



CITY OF HOBART

SUPPORTING INFORMATION

PLANNING AUTHORITY COMMITTEE MEETING

OPEN PORTION OF THE MEETING

WEDNESDAY, 24 JUNE 2026

AT 4.00PM

VENUE: COUNCIL CHAMBER, TOWN HALL

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PlanBuild
 TASMANIA

PLANNING APPLICATION

Status:

Reference

PLN-HOB-2025-0658

Address

65 BRISBANE ST HOBART TAS 7000

Titles

174045/1

Before you start

Before you start your application, you will need to know if you require planning approval or not.

If you are unsure if you require a permit, use the [PlanBuild Tasmania Enquiry Service](#) to lodge a request for advice from the relevant Council.

Once your application has been submitted the Council will review your application. If payment has not been made, you will be sent a request for the payment of application fees via PlanBuild Tasmania.

Once the fees have been paid and the Council is satisfied with the information provided, the application will be assessed and you will be notified of the outcome.

If further action is required to assess your application you will receive an email notification containing a task to complete.

Pre-Application Advice

Have you spoken with anyone at Council about this application?

Yes - enter details below

No - continue to the next section

If yes, provide the name of the person you contacted

Applicant

Name	Email	Phone	Address	Involvement
Personal Information Removed				

Owners

Name	Email Address	Address
Personal Information Removed		

Certificate(s) of Title

Selected Titles

Total Area: 0m²

174045/1

Owner Notification

Are you the sole owner of the land?

Yes - continue to the next section

No - answer question below

If no, have you notified all owners, joint or part owners of your intention to submit this application?

Yes - enter owner details below

No - you must notify all owners before proceeding with this application

List all owners, joint or part owners as recorded on the Title documents notified:

Daniel Gardner - Co Director

Enter the date that the last owner, joint or part owner was notified

16/12/2025

Declaration

I declare that all land owners, joint or part owners have been notified of this planning application.

Crown Land Consent

Is Crown Land involved in the proposed use or development?

- Yes - complete question below
 No - continue to the next section - see further information below
 Unsure

If yes, has written Crown Land consent been obtained?

- Yes - upload written consent
 No - application will not be progressed until consent has been provided

General Manager Consent

Is Council-owned or administered land involved in the proposed use or development?

- Yes - complete question below
 No - continue to the next section
 Unsure

If yes, has written consent been obtained from the Council General Manager?

- Yes - upload written consent
 No - application will not be progressed until consent has been provided

Proposed Use or Development

What is the reason for your planning application?

- I want to change how the property is used
 I want to use the property for visitor accommodation
 I want to subdivide
 I want to undertake a new development or alteration
 I want to do a minor boundary adjustment
 I want to put up a sign(s)
 I want to demolish
 I want to do works only
 Other

If your application is to subdivide, please enter the number of proposed lots.

0

If your application is for signage, please enter the number of signs.

Is the property a Tasmanian Heritage Listed Property?

- Yes
 No

Is the application for an EPA Activity under the Environmental Management and Pollution Control Act 1994?

- Yes
 No
 Unsure

Is the proposed use or development permitted or discretionary?

- Permitted
 Discretionary
 Unsure if permitted or discretionary

Provide a full description of the proposed use or development

Mixed use development of the vacant on grade carpark for a 6 storey building including ground floor retail, two floors of office and 8 residential apartments and associated carparking and works

Will the proposed use or development involve a road reserve?

Yes - complete the section below

No - continue to the next section

Unsure

If yes, enter the address(es) or locations below:

If yes, how will the road reserve be affected?

Value of Works

What is the estimated value of the works?

1000000

Supporting Documents

Version	Document Date	Document Type	Description	Prepared By
1	16 Dec 2025	Architectural Plans	Architectural Plans	Lara Maesele
1	16 Dec 2025	Planning Assessment Report	Planning Report	Frazer Read
1	1 Oct 2025	Accessibility Report	Traffic Impact Assessment	Peter Hubble
1	9 Jan 2025	Geotechnical Report	Environmental Assessment Report	GES
1	15 Dec 2025	Planning Assessment Report	Waste Management Report	Mark Gardner
1	17 Sept 2025	Engineering Drawings	Civil Engineering Drawings	Mark Gardner
1	2 Nov 2025	Heritage Report	Historic Heritage and Archaeological Management Strategy & Heritage/Archaeological Impact Assessment	Brad Williams / Frazer Read
1	16 Dec 2025	Architectural Plans	Architectural Planning Submission	Lara Maesele

Next steps

When you have completed all the necessary fields and attached all required documents to support your application, click on the green 'Save & Submit' button at the top right of this form.

Once submitted, the Council will review your application. A request for the payment of application fees will be sent to you via PlanBuild Tasmania.

Once the fees have been paid and the Council is satisfied with the information provided, the application will be assessed and you will be notified of the outcome.

If further action is required to assess your application you will receive an email notification from PlanBuild which will tell you what you need to provide to continue the application.



Tasmanian Heritage Council
GPO Box 618 Hobart Tasmania 7000
Tel: 1300 850 332
enquiries@heritage.tas.gov.au
www.heritage.tas.gov.au

PLANNING REF: PLN-HOB-2025-0658
THC WORKS REF: 8796
REGISTERED PLACE NO: 2182
APPLICANT: Mark Gardner
DATE: 10 June 2026

NOTICE OF HERITAGE DECISION

(Historic Cultural Heritage Act 1995)

The Place: 'Sydney Lodge', 65 Brisbane St, Hobart.
Proposed Works: Additional storey.

Under section 39(6)(b) of the *Historic Cultural Heritage Act 1995*, the Heritage Council gives notice that it consents to the discretionary permit being granted in accordance with the documentation submitted with Development Application PLN-HOB-2025-0658, advertised on 14/05/2026, subject to the following conditions:

1. (i) **The locations of the proposed new electricity substation and accessible car parking spaces within the curtilage of Sydney lodge, as described in figure 8.1 of the *Historic Heritage and Archaeological Management Strategy & Heritage/Archaeological Impact Assessment (Praxis Environment, version 2, November 2025)*, are not approved. The design must be modified to locate these elements outside of the area nominated as the curtilage for the heritage building; and,**
(ii) **A site plan demonstrating compliance with this condition must be provided to Heritage Tasmania, and be to the satisfaction of the Works Manager, prior to the commencement of works.**

Reason for condition

To conserve an appropriate curtilage and setting for the heritage building.

2. (i) **A landscaping plan must be prepared for the area around the heritage building that is indicated as "proposed landscaping" on drawing no. A1004 REV B by LM; and,**
(ii) **The proposed landscaping materials and plantings must be sympathetic to the place's historic cultural heritage significance; and,**
(iii) **The landscaping plan must be submitted to Heritage Tasmania and be to the satisfaction of the Works Manager prior to the commencement of the landscaping works.**

Reason for condition

To ensure that works are in keeping with the character of the place, consistent with the appropriate outcomes in Section 13 of the *Works Guidelines*.

3. (i) The archaeological processes recommended in the *Historic Heritage and Archaeological Management Strategy & Heritage/Archaeological Impact Assessment* by Praxis Environment must be implemented; and, (ii) A report detailing the findings of the archaeological investigations, in digital format, must be submitted to the Heritage Council within 6 months of the completion of excavations and before practical completion of the building.

Reason for condition

To ensure that the endorsed archaeological program is delivered in accordance with the Archaeological Method Statement.

Should you require clarification of any matters contained in this notice, please contact Russell Dobie on 1300 850 332.



Peter Scott

Chair – Works Committee

Under delegation of the Tasmanian Heritage Council

MARCH 2026

57-65 BRISBANE STREET
HOBART, TASMANIA



LARA MAESEELE



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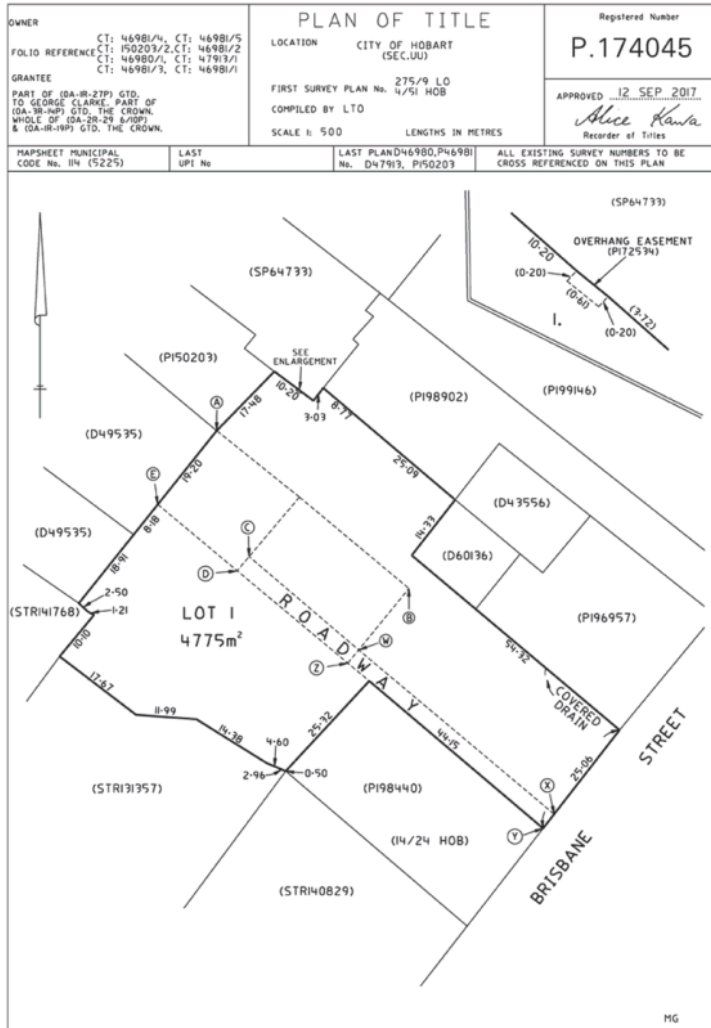
FOLIO PLAN
 RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



RESULT OF SEARCH
 RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



SEARCH OF TORRENS TITLE

VOLUME	FOLIO
174045	1
EDITION	DATE OF ISSUE
1	13-Sep-2017

SEARCH DATE : 13-Mar-2024
 SEARCH TIME : 05.37 AM

DESCRIPTION OF LAND

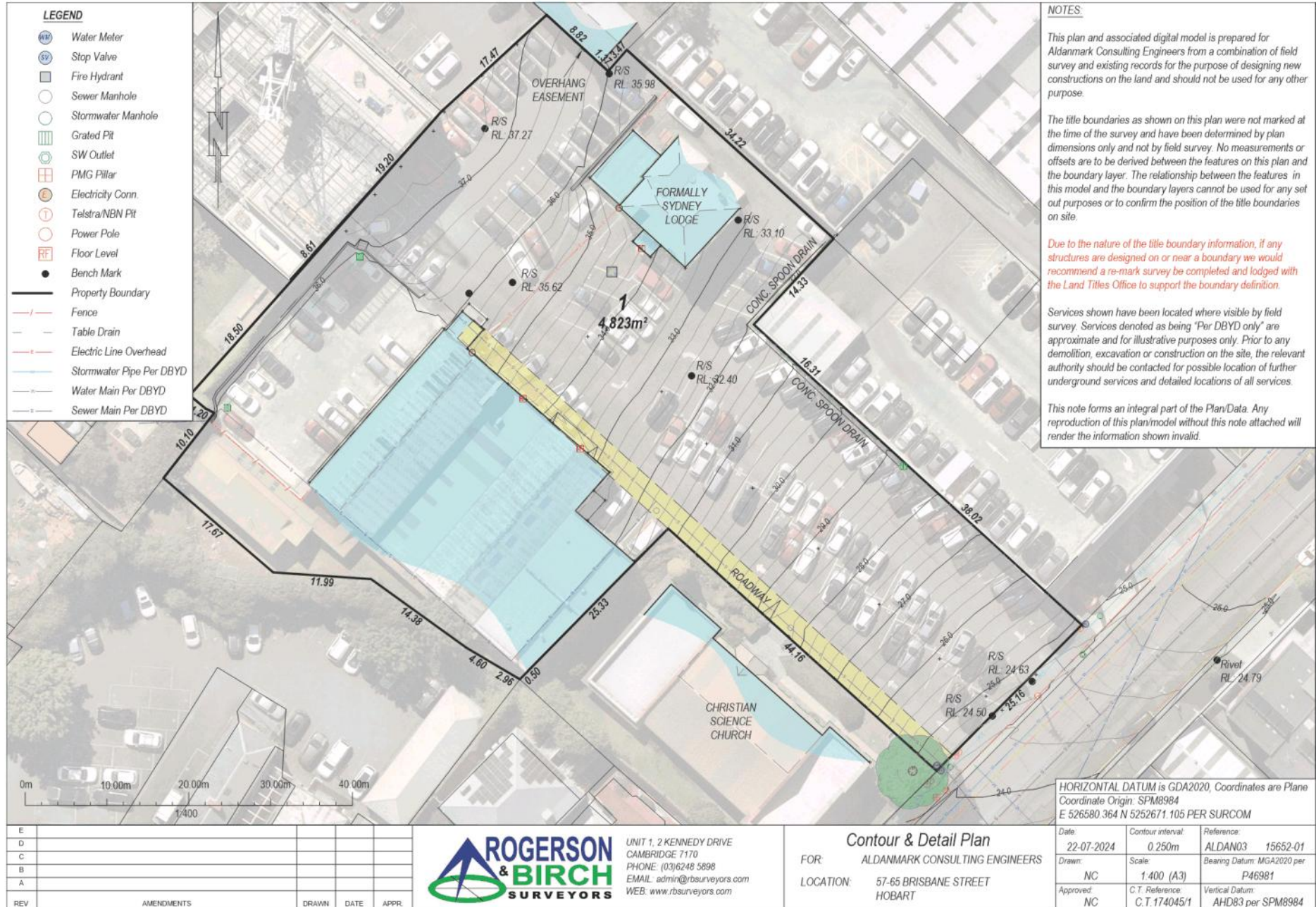
City of HOBART
 Lot 1 on Plan 174045
 Derivation : Part of (0A-1R-37P) Granted to George Clarke,
 Part of (0A-3R-14P) Granted to The Crown and Whole of
 (0A-2R-29 6/10P) & (0A-1R-19P) Granted to The Crown
 Prior CTs 46981/4, 46981/5, 150203/2, 46981/2, 46980/1,
 47913/1, 46981/3 and 46981/1

SCHEDULE 1

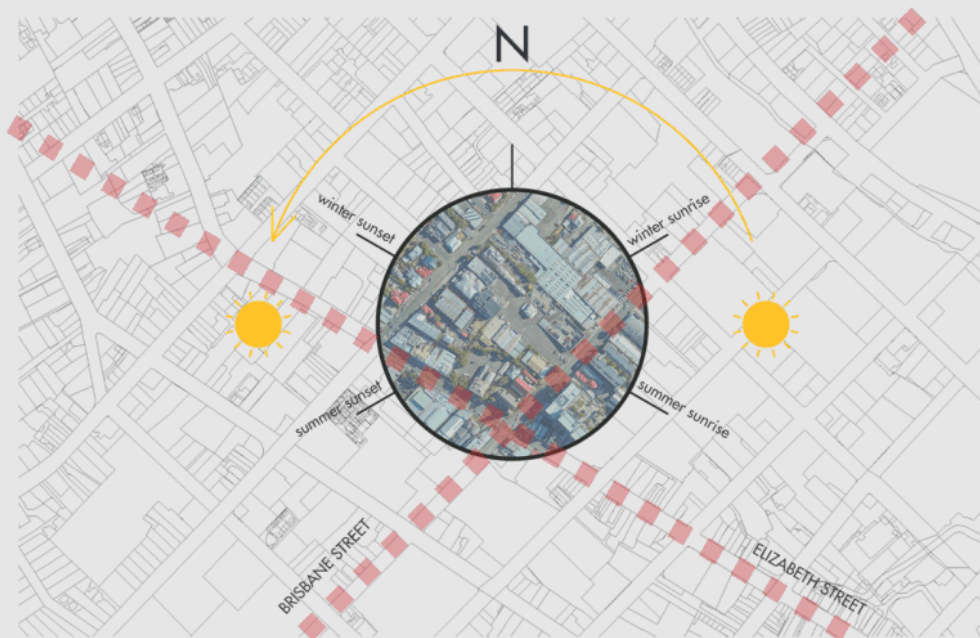
C766429 & M632949 TRANSFER to G & L MANAGEMENT PTY LTD
 Registered 13-Sep-2017 at noon

SCHEDULE 2

- M632949 Land is limited in depth to 15 metres, excludes minerals and is subject to reservations relating to drains sewers and waterways in favour of the Crown
- B469026 INSTRUMENT creating restrictive covenants burdening the land within described pursuant to Section 19 of the National Trust of Australia (Tasmania) Act 1975 and Section 102 of the Land Titles Act 1980 (excepting thereout Certificate of Title Volume 150203 Folio 2) Registered 19-Dec-1991 at noon
- BENEFITING EASEMENT (appurtenant to the land marked ABWCDE on pLAN 174045) a full free and uninterrupted right of way and passage for the registered proprietor his heirs and assigns and his and their tenants and servants and other by his or their permission in common with all other persons entitled thereto from time to time and at all times hereafter and for all purposes whatsoever to go return pass and repass with or without horses carts and other carriages laden or unladen in through over along and upon the Roadway marked CWZD on Plan 174045
- BURDENING EASEMENT: full and free right and liberty for National Executors and Trustees Company of Tasmania Limited its successors transferees and assigns (as



1.0 PROJECT DETAILS



1.1 SITE

The site is located within the Central Business Zone of Hobart, characterised by a mix of commercial, office, and residential uses with varying building heights and architectural styles - refer to section 3.0 with images of surrounding buildings.

Brisbane Street is a key urban corridor linking the CBD with surrounding residential precincts. The site benefits from:

- Proximity to public transport routes
- Pedestrian connectivity to the Hobart city centre
- Established commercial frontage along Brisbane Street

The design responds to the evolving urban character, supporting increased density while respecting the scale and rhythm of the streetscape.

1.2 DESIGN INTENT

The proposed development seeks to:

- Deliver a high-quality mixed-use building that contributes positively to the CBD
- Activate the street frontage with commercial uses
- Provide flexible office accommodation
- Introduce contemporary residential living opportunities within the city
- Integrate parking in a manner that minimises visual and functional impacts

The design balances commercial vitality, workplace functionality, and urban residential amenity within a cohesive architectural form.

1.0 PROJECT DETAILS

1.3 BUILT FORM AND MASSING

The building is organised vertically as follows:

- Ground Floor: 246m² commercial tenancy with strong street interface, plus area for amenities for offices and apartments above.
- First & Second Floors: 587m² and 427m² office spaces respectively designed for flexible occupancy.
- Second Floor (partial): 16 covered residential car parking spaces.
- Third & Fourth Floors: Five apartments per level ranging from 100 to 125m² floor areas.
- Fifth Floor: Three penthouse apartments ranging from 185m² to 200m² floor areas

The massing strategy:

- Reinforces the established street wall along Brisbane Street
- Steps and articulates upper levels to reduce perceived bulk
- Minimises overshadowing and visual dominance

The upper-level setbacks contribute to improved solar access and reduced streetscape impact.

1.4 ARCHITECTURAL EXPRESSION AND MATERIALS

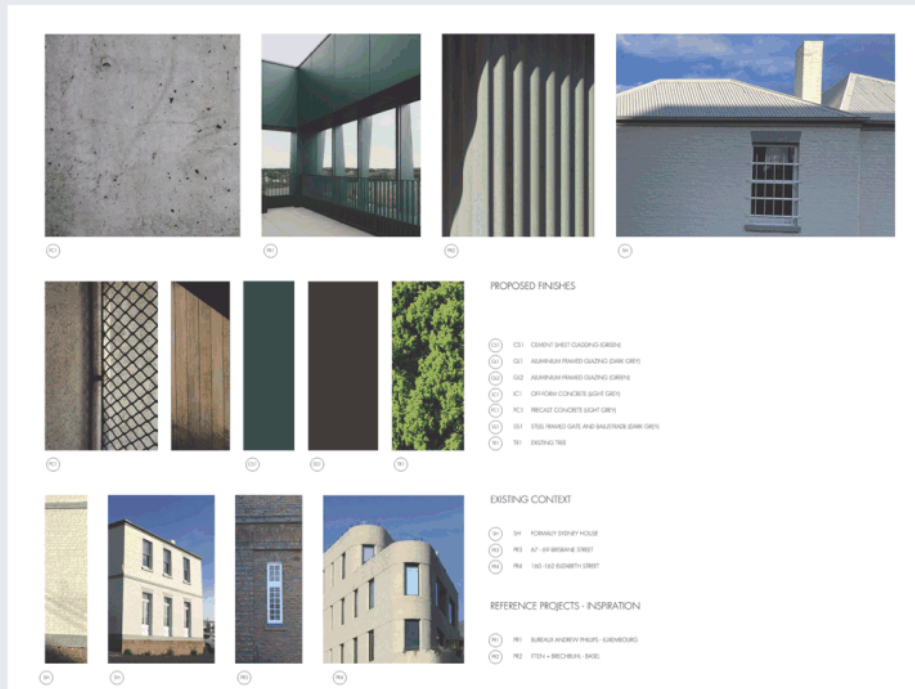
The architectural expression is contemporary and responsive to the urban character of Hobart, with a focus on articulation, material contrast, and natural light.

A robust concrete frame defines the lower commercial and office levels, stepping with the site's topography to establish a street presence. Above, lighter-weight residential forms create a clear transition in scale and use.

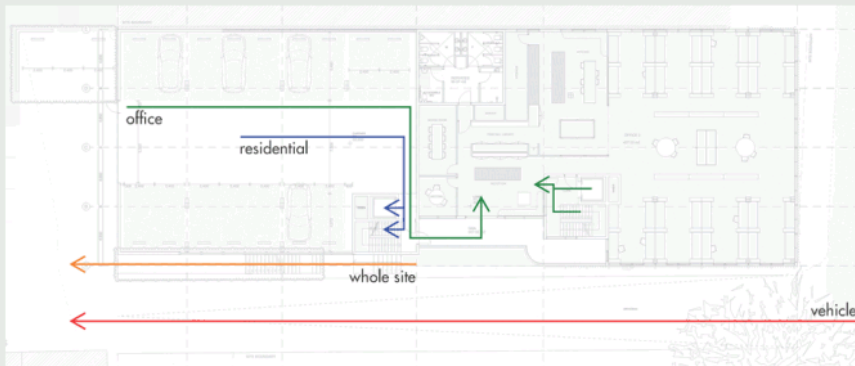
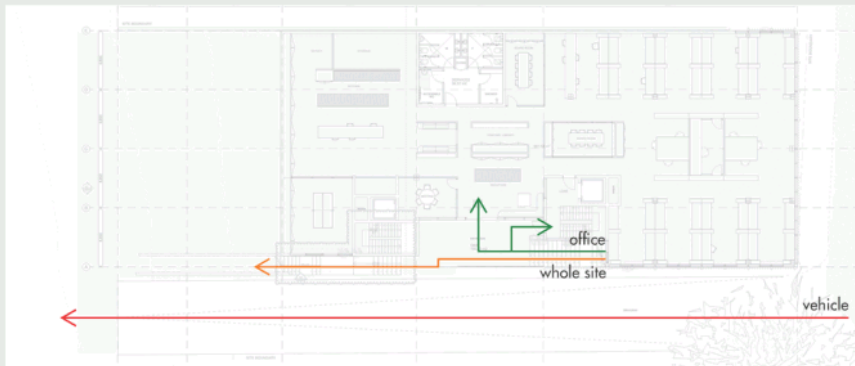
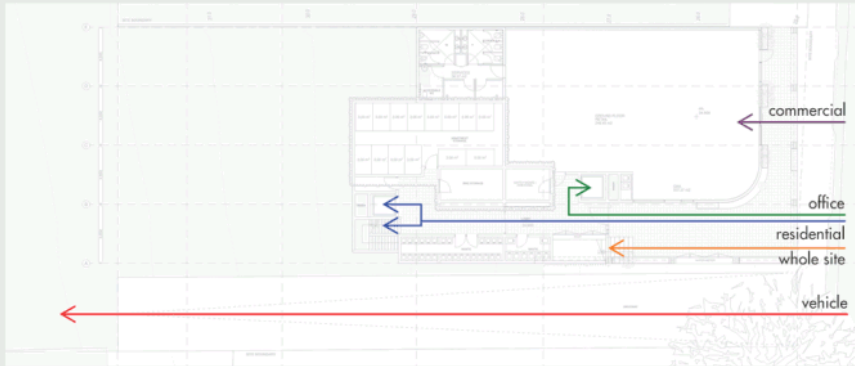
Key features include:

- Vertical and horizontal articulation to reduce perceived bulk, including an internal courtyard to open up the volume and provide communal space
- Stepping of the form in response to the site
- Generous glazing to maximise daylight and visual connection
- A setback on ground level provides shelter and street level activation
- Generous deck areas provide a greening relief and outdoor breakout spaces to the residents

Material selection prioritises compatibility with the surrounding context, durability and ease of maintenance, and visual richness through texture and contrast.



1.0 PROJECT DETAILS



1.5 STREETScape AND PUBLIC REALM INTERFACE

The ground floor is designed to enhance pedestrian experience through:

- Active frontages with transparent glazing
- Clearly defined access for commercial, office and residential uses
- Generous lobby entrance with access from Brisbane Street
- Weather protection elements by articulating building
- Commercial space setback to provide street level activation

The proposal strengthens the vibrancy of Brisbane Street and contributes to a safer, more engaging public realm.

1.6 ACCESS AND CIRCULATION

Pedestrian Access

- Primary pedestrian access is provided directly from Brisbane Street
- Separate, secure residential and office entries are clearly identifiable
- Universal access is provided in accordance with relevant standards

Vehicular Access

- Vehicular access to parking is integrated discreetly to minimise streetscape disruption
- Car parking is located at rear of site to reduce visual impact

Internal Circulation

- Vertical circulation cores (lifts and stairs) provide efficient and equitable access to all levels
- Separation between commercial, office, and residential uses ensures functional clarity

1.0 PROJECT DETAILS

1.7 ACCESSIBILITY

The development is designed to meet the requirements of:

- Disability access standards
- Equitable access principles

Key accessibility features include:

- Step-free access from street level
- Accessible paths of travel throughout the building
- Lift access to all levels
- Adaptable residential apartments

The proposal promotes inclusivity for all users, including those with mobility impairments.

1.8 ENVIRONMENTAL CONSIDERATIONS

The design has been conceived with the objective of incorporating best practice sustainable design initiatives.

Key environmentally sustainable design principles which have guided the building design are:

- Minimise building energy use and greenhouse emissions through passive design strategies.
- Manage solar access to enhance amenity and function throughout the year.
- Natural light penetration to all spaces and access to generous outdoor deck areas for all residential units
- Energy-efficient building systems
- Optimise indoor environmental quality through cross-ventilation in residential units
- Installation of electric vehicle charging stations
- Gas free site
- Provide landscaping which enhances amenity and contributes to biodiversity

The building form and façade design contribute to improved environmental performance.

2.0 PROJECT FIGURES

Site Area	4775 m ²			
Existing Buildings		Proposed Building	GBA	NLA
Warehouse 01 (leased by Vibrance)	155 m ²	Grd Floor - Commerical	505	246 m ²
Warehouse 02 (leased by Westend Mechanical)	200 m ²	1st Floor - Office	694	578 m ²
Warehouse 03 (leased by Toe Kwan Do Tasmania)	170 m ²	2nd Floor - Office	925	427 m ²
Office 01 (leased by Hype TV)	225 m ²	- Parking		385 m ²
Office 02 - Sydney House (leased by Drug Education Network)	240 m ²	3rd Floor - Residential Aparments	672	602 m ²
Amenities Block	65 m ²	4th Floor - Residential Apartments	655	583 m ²
Existing Carpark		5th Floor - Residential Apartments	640	573 m ²
Carparks	134	Proposed Parking - Site		
Accessible Carparks (DDA)	0	Carparks	59	
Bikes	0	Accessible Carparks	0	
		Motor Bikes	4	
		Proposed Parking - New Building (2nd Floor)		
		Carparks	16	
		Accessible Carparks	2	
		Bikes	20	

3.0 SURROUNDING BUILDINGS



FORMALLY SYDNEY HOUSE



34 PATRICK STREET



160 - 162 ELIZABETH STREET

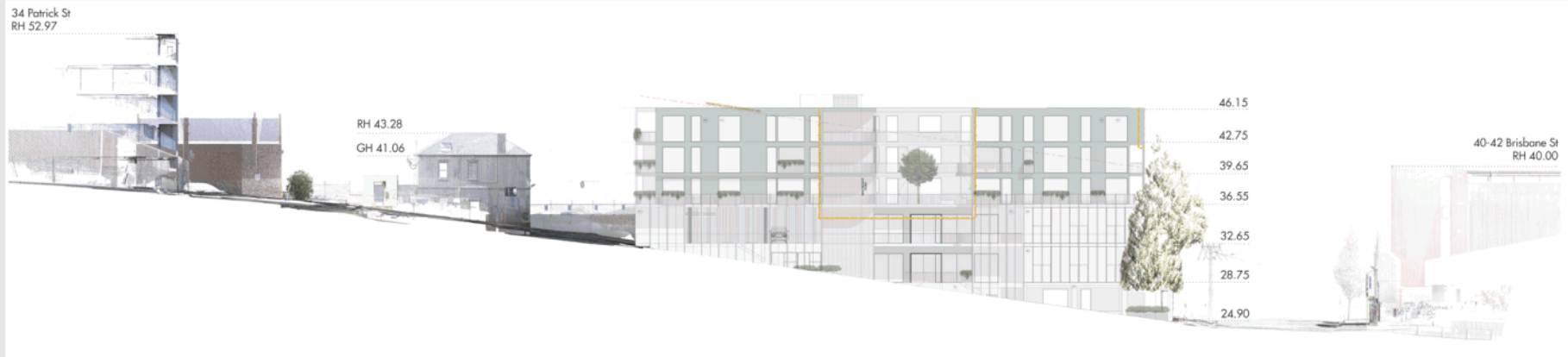


40 - 42 BRISBANE STREET



157 ELIZABETH STREET

4.1 POINT CLOUD DATA



Western Elevation 01



Western Elevation 02

4.1 POINT CLOUD DATA

34 Patrick St
RH 52.97

46.15

42.75

39.65

36.55

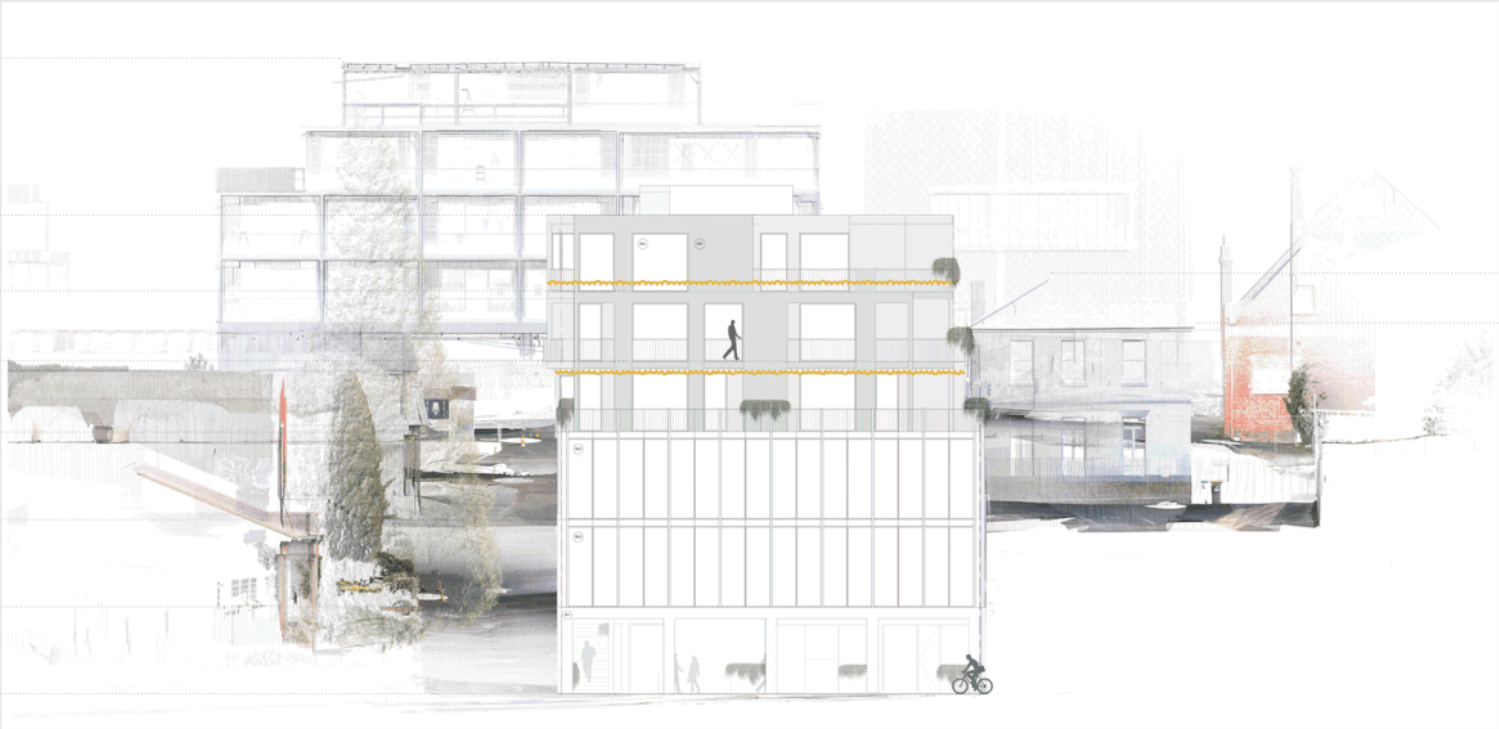
32.65

28.75

24.90

RH 43.28

GH 41.06



Southern Elevation 01

4.2 SHADOW DIAGRAMS



21st JUNE 9am



21st JUNE 12noon

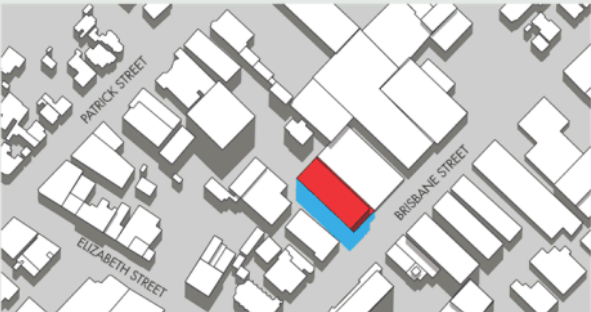


21st JUNE 3pm

4.2 SHADOW DIAGRAMS



21st SEPTEMBER 9am

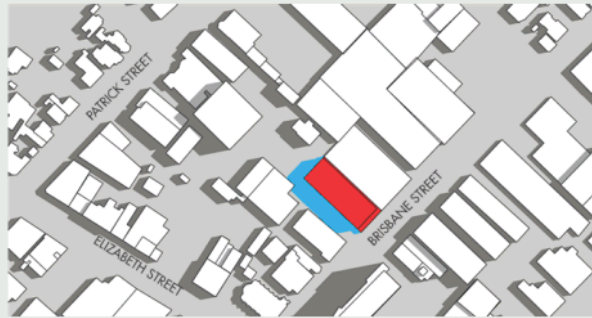


21st SEPTEMBER 12noon



21st SEPTEMBER 3pm

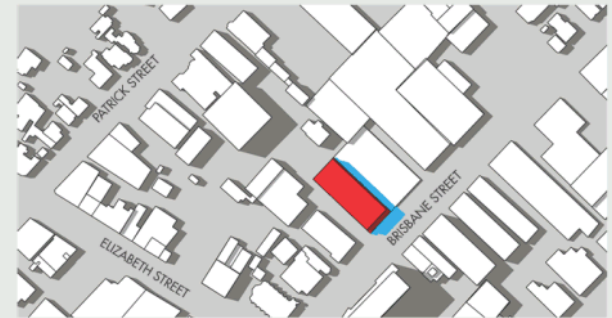
4.2 SHADOW DIAGRAMS



21st DECEMBER 9am



21st DECEMBER 12noon



21st DECEMBER 3pm

4.3 BRISBANE STREET SCAPE



5.0 ARCHITECTURAL DRAWINGS



- CHANGES**
- CH1 NEW LEVEL
 - CH2 NEW SUBSTATION + PARKING ADJUSTMENT
 - CH3 RETAINING FOOTPATHWAYS
 - CH4 WASTE AND STORAGE ADJUSTMENT
 - CH5 WALKWAY
 - CH6 EXTENDED COURTYARD
 - CH7 ENCLOSED CORE

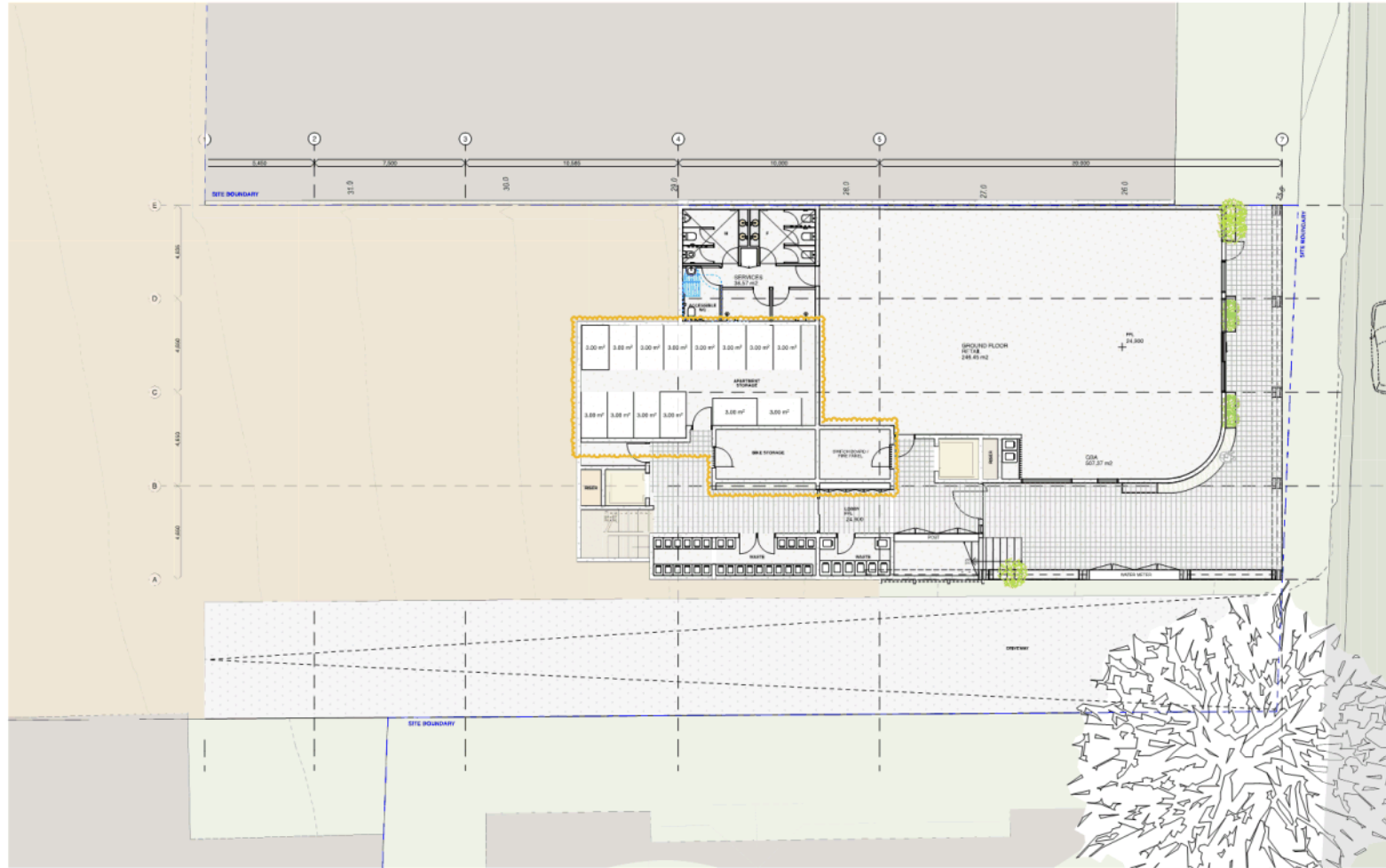


BRISBANE ST
Client
BRISBANE STREET APARTMENTS
Site
37-51 BRISBANE ST

SITE - PROPOSED
Date: 30/4/2025
Scale: 1/200
Status: FOR DA-DEUT



A1004
REV B



1. GROUND FLOOR ENTRY

- CHANGES:
- CH1 NEW LEVEL
 - CH2 NEW SUBSTATION + PARKING ADJUSTMENT
 - CH3 SETBACKS PENTHOUSE 3
 - CH4 WALKWAY AND COURTYARD ADJUSTMENT
 - CH5 WALKWAY
 - CH6 EXTENDED COURTYARD
 - CH7 ENCLOSED CORE

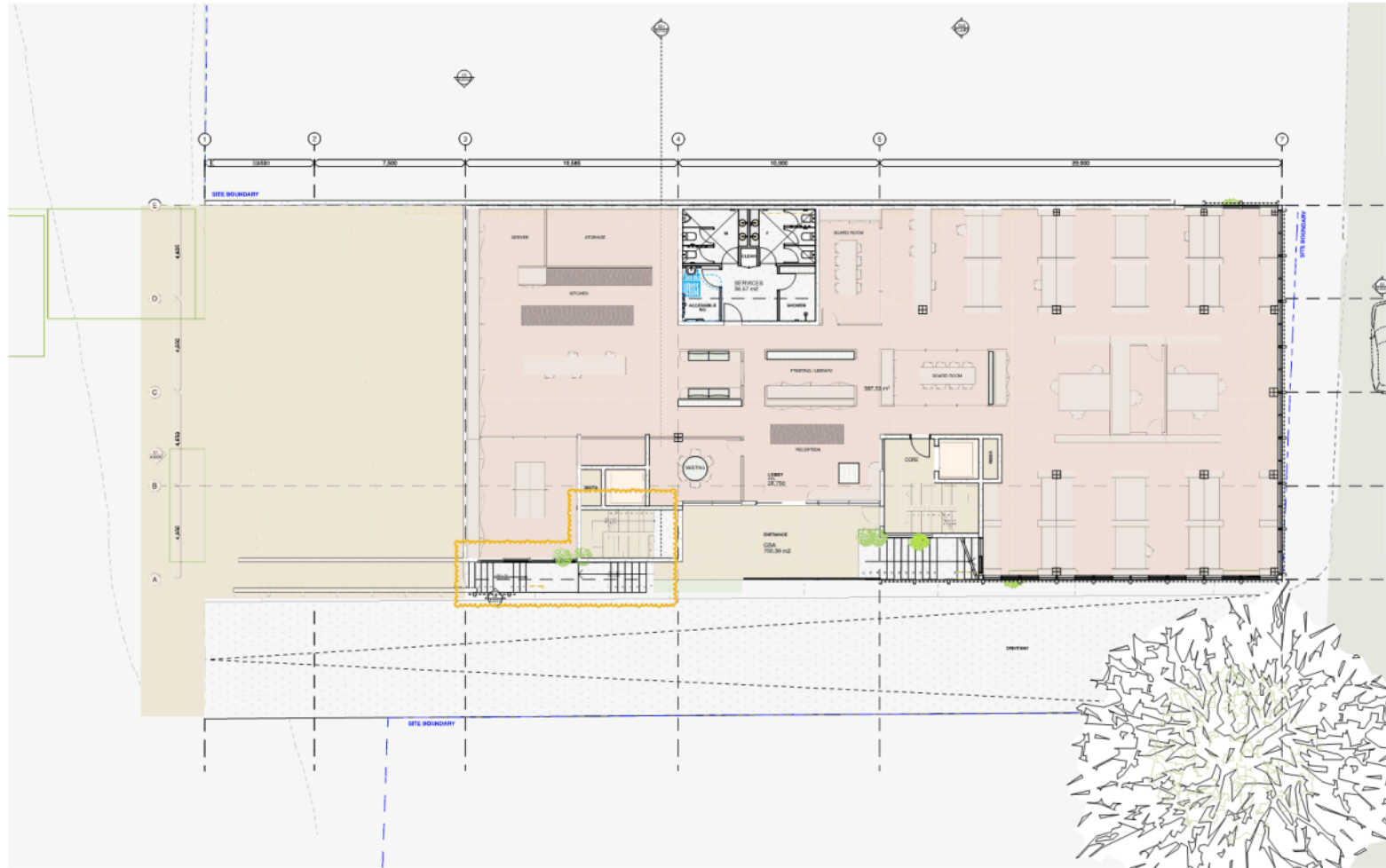


BRISBANE ST
Client
BRISBANE STREET APARTMENTS
Site
SPAC 02/24/16 02/2022

PLANS - GROUND
Date: 27/2/2025
Scale: 1/100
Drawn: FOR DA-DRAFT



A2200
REV B



2. LEVEL 1 - OFFICE

- CHANGES:
- CH1 NEW LEVEL
 - CH2 NEW SUBSTATION + PARKING ADJUSTMENT
 - CH3 SETBACKS PROTHOUSE 3
 - CH4 WASTE AND STORAGE ADJUSTMENT
 - CH5 SETBACKS
 - CH6 EXTENDED COURTYARD
 - CH7 ENCLOSED CORE

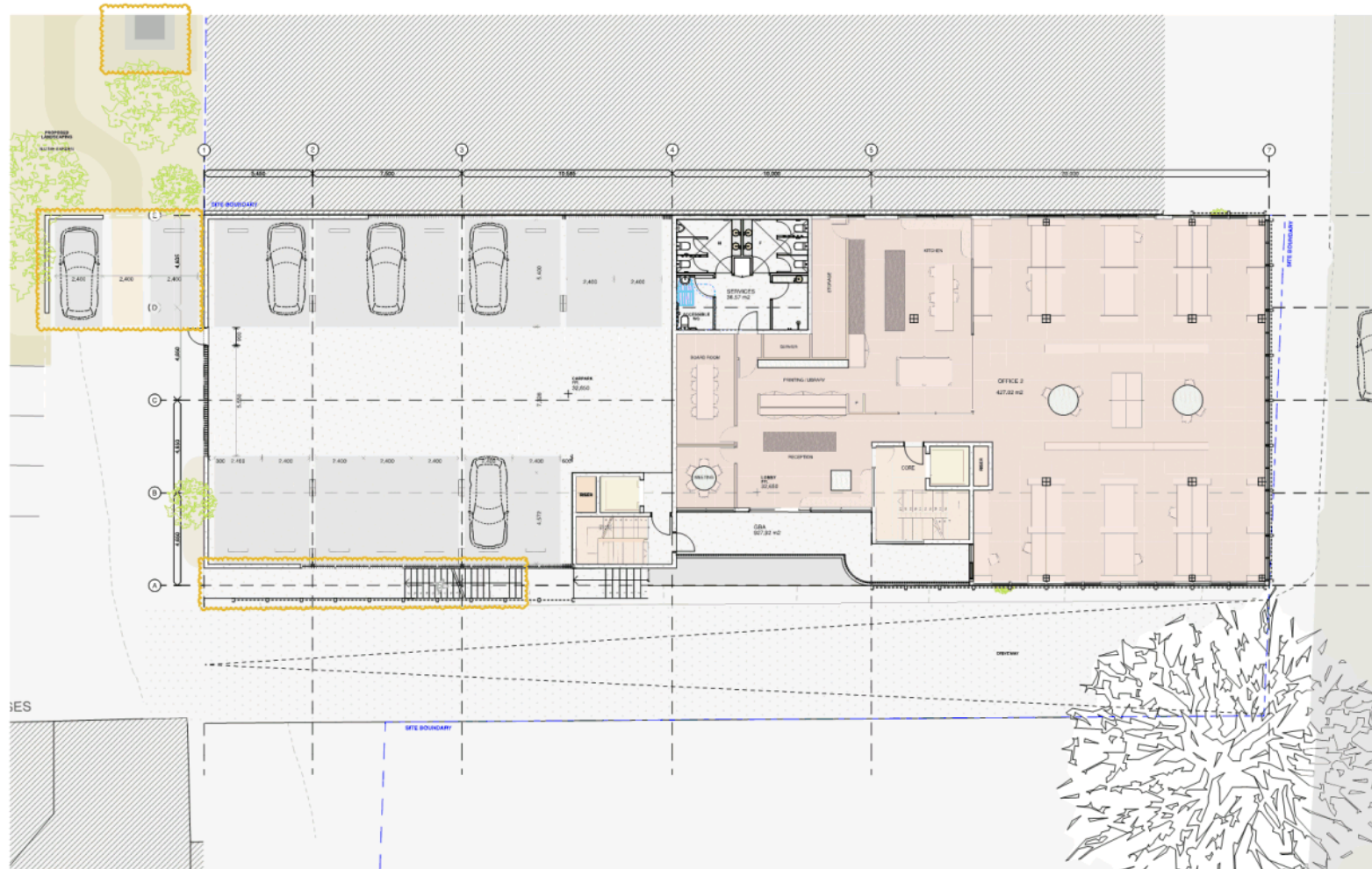


BRISBANE ST
Client
BRISBANE STREET APARTMENTS
Site
SPAT DREIN-14 07/2021

PLANS - LEVEL 1
Date: 27/2/2025
Scale: 1/100
Status: FOR CA-DEVT



A2201
REV B



3. LEVEL 2 - OFFICE

- CHANGES:
- CH1 NEW LEVEL
 - CH2 NEW LEVEL TOP/OK - PARKING ADJUSTMENT
 - CH3 SETBACKS PERMITHOUSE 3
 - CH4 WASTE AND STORAGE ADJUSTMENT
 - CH5 PERMITHOUSE 3
 - CH6 EXTENDED COURTYARD
 - CH7 ENCLOSED CORE

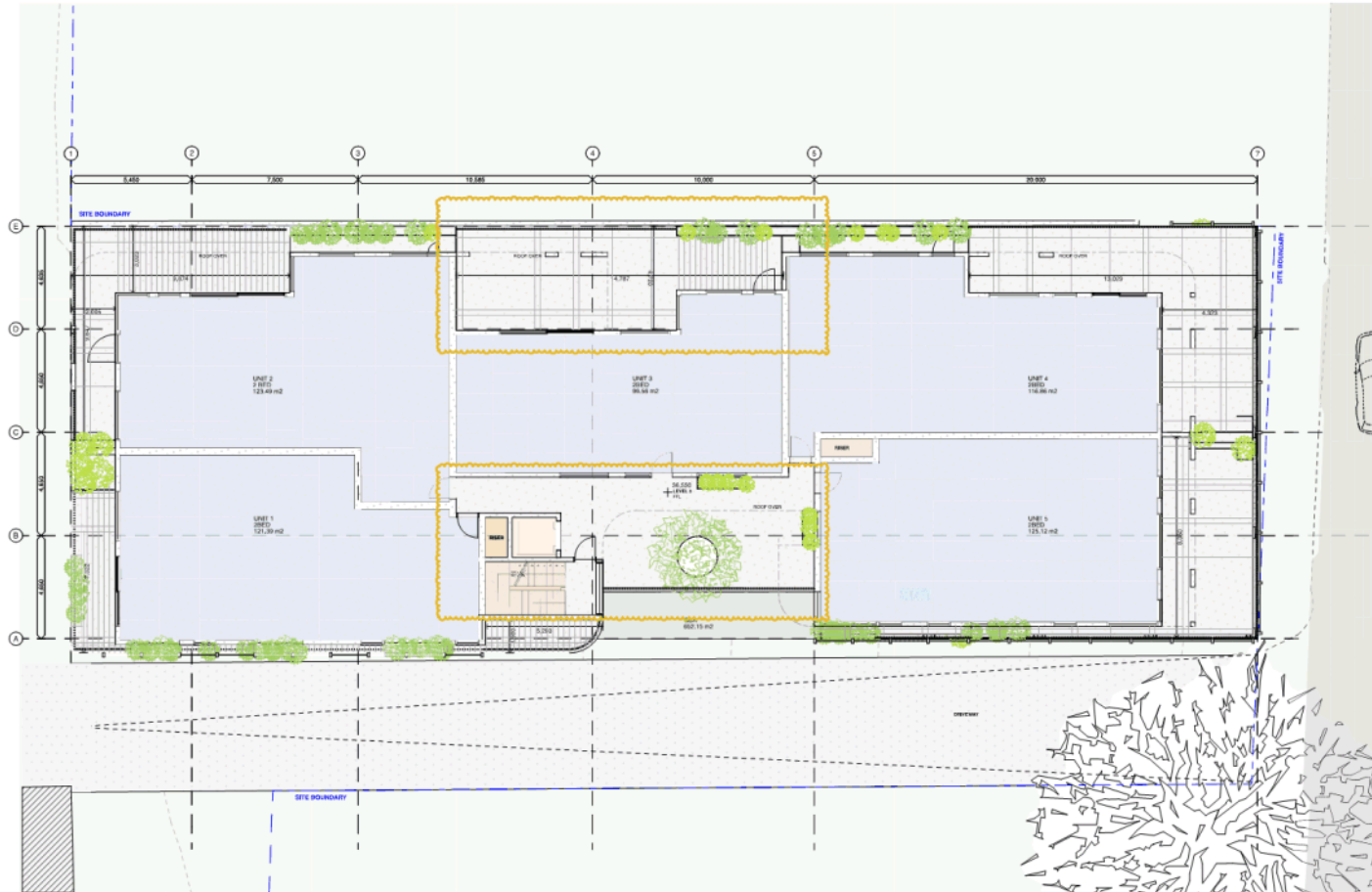


BRISBANE ST
Client
BRISBANE STREET APARTMENTS
Site
SPAC BRISBANE 14 07 2025

PLANS - LEVEL 2
Date: 27/2/2025
Scale: 1/100
Status: FOR CA-DEVT



A2202
REV B



4. LEVEL 3 - RESIDENTIAL

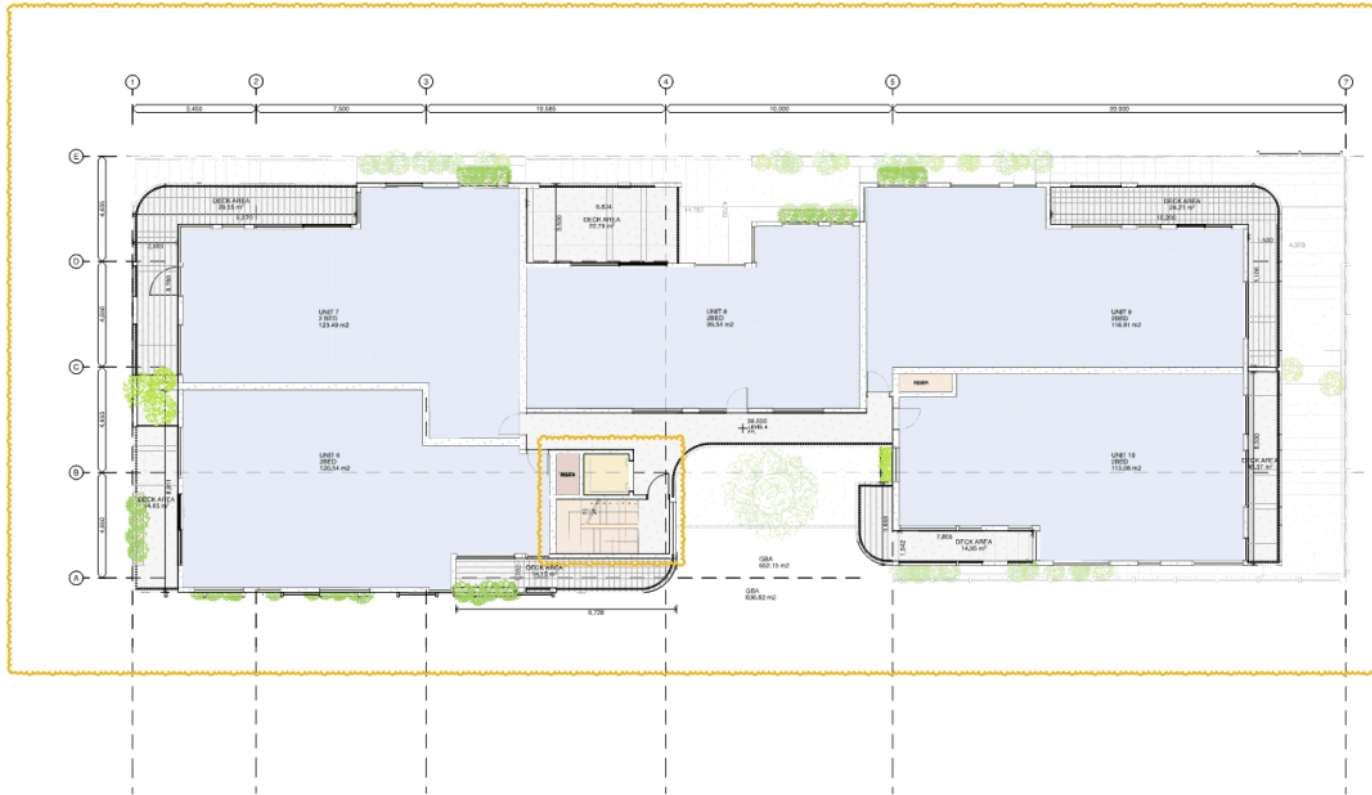
- CHANGES:
- CH1 - NEW LEVEL
 - CH2 - NEW SUBSTATION + PARKING ADJUSTMENT
 - CH3 - SETBACKS - UNIT 1
 - CH4 - WASTE AND STORAGE ADJUSTMENT
 - CH5 - WALKWAY
 - CH6 - EXTENDED COURTYARD
 - CH7 - ENCLOSED CORE



BRISBANE ST
Client
BRISBANE STREET APARTMENTS
Site
SP161 BRISBANE ST 27/02

PLANS - LEVEL 3
Date: 27/02/2025
Scale: 1/100
Status: FOR CA-DEVELOP

A2203
REV B



5. LEVEL 4 - RESIDENTIAL 2

- CHANGES
- CH1 NEW LEVEL
 - CH2 NEW SUBSTATION + PARKING ADJUSTMENT
 - CH3 SETBACKS PENTHOUSE 3
 - CH4 WASTE AND STORAGE ADJUSTMENT
 - CH5 WALKWAY
 - CH6 EXTENDED COURTYARD
 - CH7 ENCLOSED CORSE

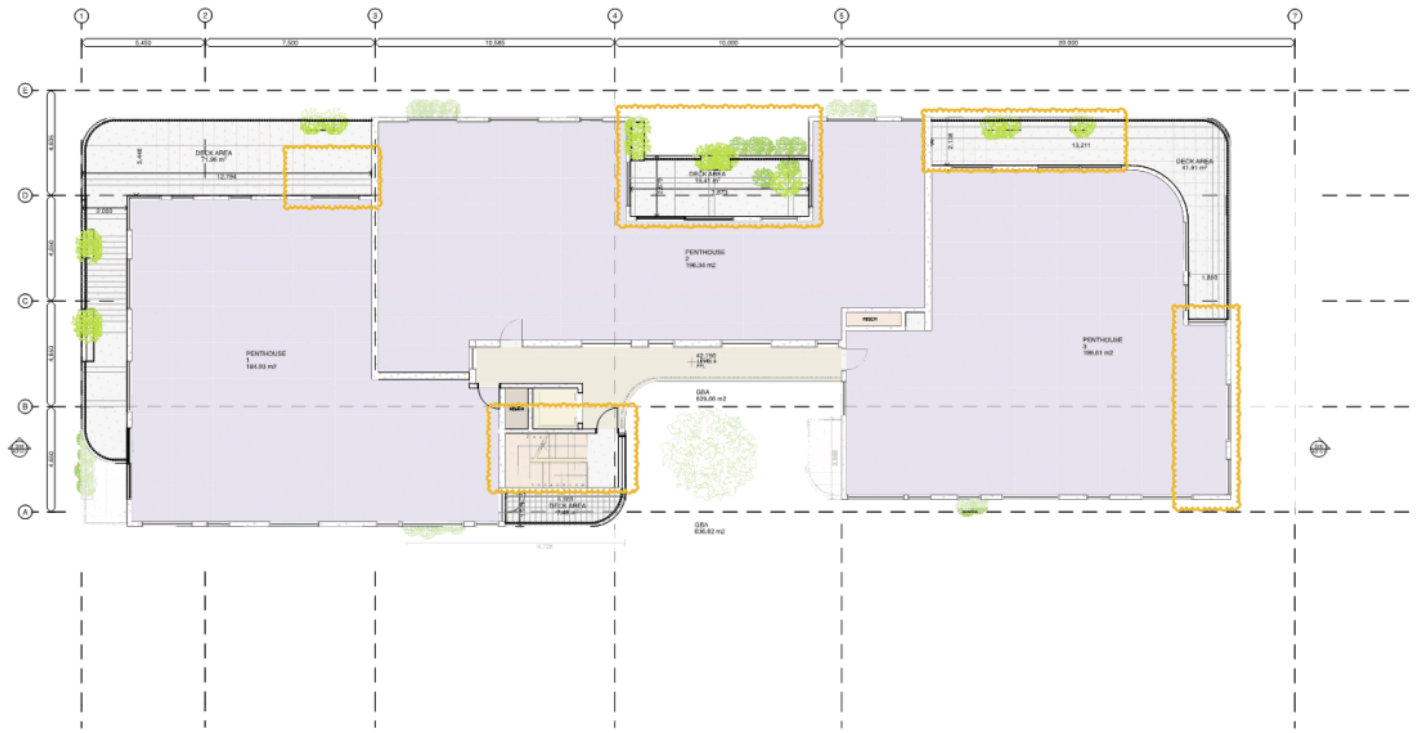


BRISBANE ST
Client
RESIDUAL STREET APARTMENTS
Site
SPAC BRISBANE 14 07 2025

PLANS - LEVEL 4
Date: 27/01/2025
Scale: 1/100
Status: FOR DA-DRAW



A2204
REV B



6. LEVEL 5 - PENTHOUSE

- CHANGES:
- CH1 NEW LEVEL
 - CH2 NEW SUBSTATION + PARKING ADJUSTMENT
 - CH3 RETAINERS - PENTHOUSE 2&3
 - CH4 WASTE AND STORAGE ADJUSTMENT
 - CH5 WALKWAY
 - CH6 EXTENDED COURTYARD
 - CH7 ENCLOSED CORE

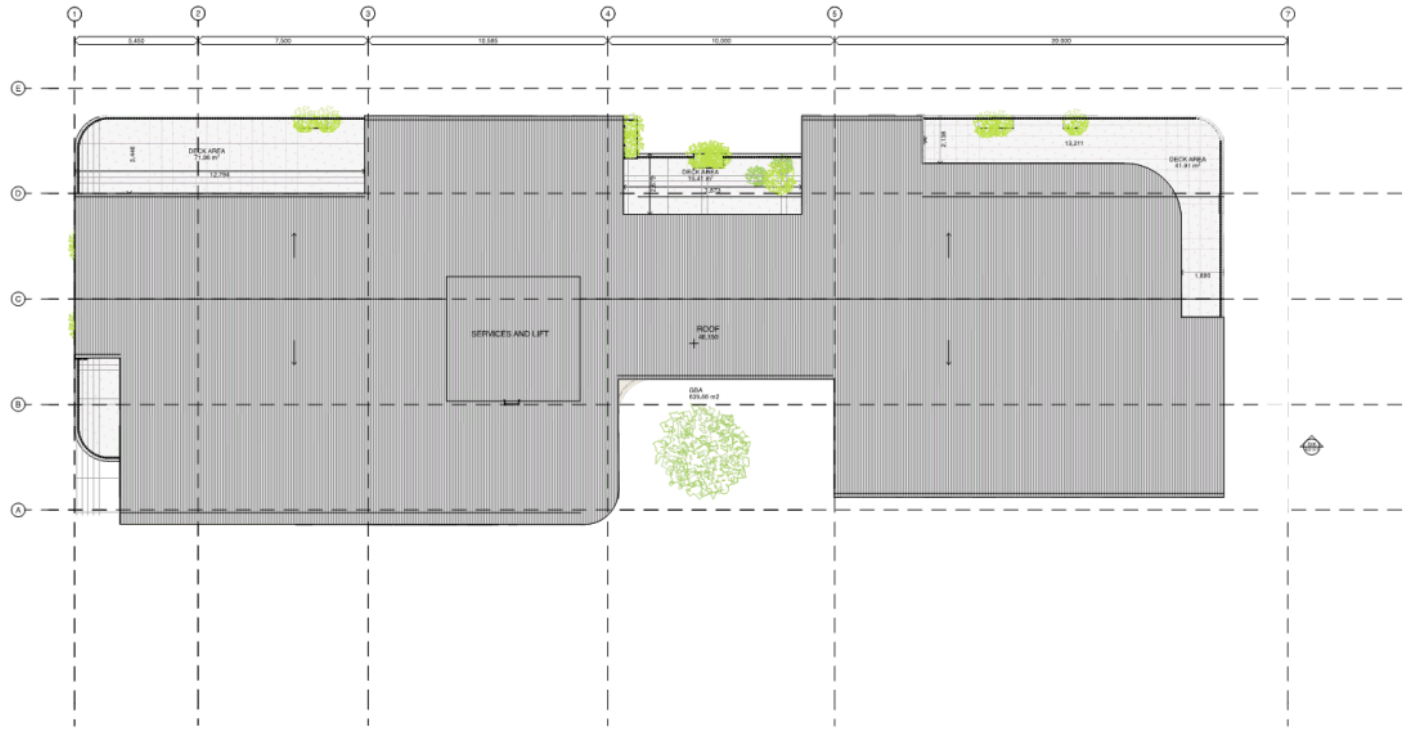


BRISBANE ST
 Client: BRISBANE STREET APARTMENTS
 Site: SPAT 02634-12-02

PLANS - LEVEL 5
 Date: 27/2/2025
 Scale: 1/100
 Status: FOR DA-DEVT



A2205
 REV B



7. ROOF

- CHANGES:
- CH1 - NEW LEVEL
 - CH2 - NEW SUBSTATION + PARKING ADJUSTMENT
 - CH3 - SETBACKS PENTHOUSE 3
 - CH4 - WASTE AND STORAGE ADJUSTMENT
 - CH5 - WALKWAY
 - CH6 - EXTENDED COURTYARD
 - CH7 - ENCLOSED CORE



BRISBANE ST
Client
BRISBANE STREET APARTMENTS
Site
SPATIAL DESIGN & ARCHITECTURE

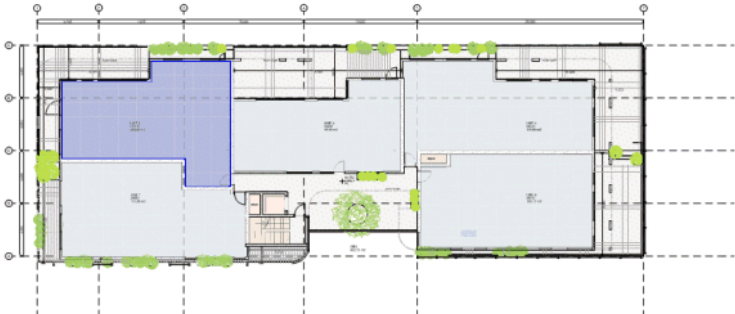
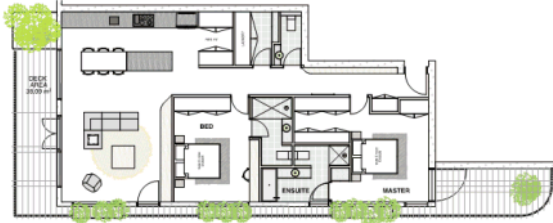
PLANS - ROOF
Date: 27/3/2025
Scale: 1/100
Status: FOR DA-DREV



A2206
REV B



LEVEL 3
UNIT 1-2 BEDROOM
1/100 @ A1
TOTAL INDOOR AREA 121 SQM
DECK AREA 31 SQM



LEVEL 3
UNIT 2-2 BEDROOM
1/100 @ A1
TOTAL INDOOR AREA 123 SQM
DECK AREA 32 SQM



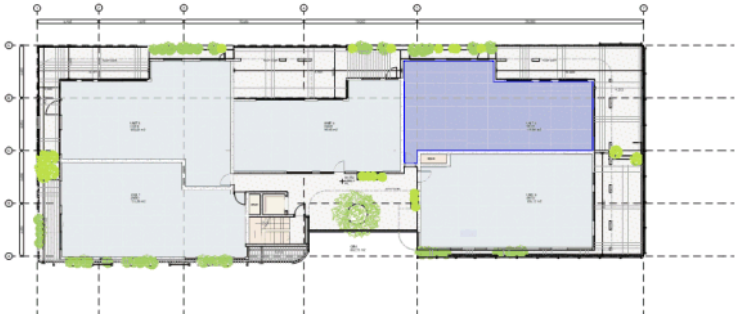
BRISBANE ST
Client
BESMANE STREET APARTMENTS
Site
SPATIAL DESIGN & INTERIORS

PARTITION PLANS - UNIT 1 & 2
Date: 27/2/2025
Scale: AS 1:5000
Status: FOR DA-DEVT

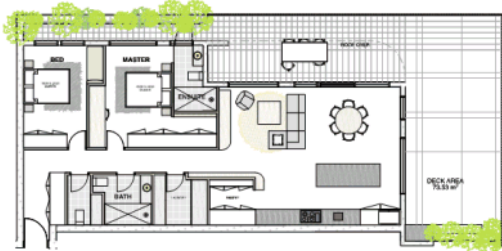
A2031
REV B



UNIT 3 - 2 BEDROOM
1/100 @ A1
TOTAL INDOOR AREA 100 SQM
DECK AREA 96 SQM



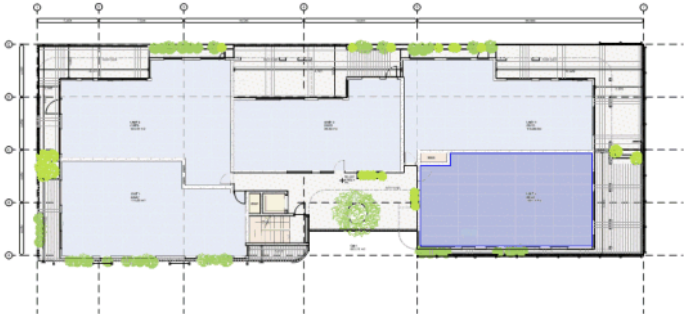
UNIT 4 - 2 BEDROOM
1/100 @ A1
TOTAL INDOOR AREA 117 SQM
DECK AREA 74 SQM



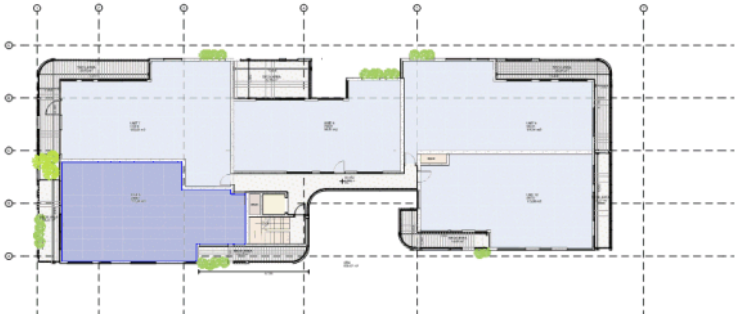
BRISBANE ST
Client
BESMARE STREET APARTMENTS
Site

PARTITION PLANS - UNIT 3 & 4
Date: 27/3/2025
Scale: AS 1:4000
Status: FOR DA-DEVT

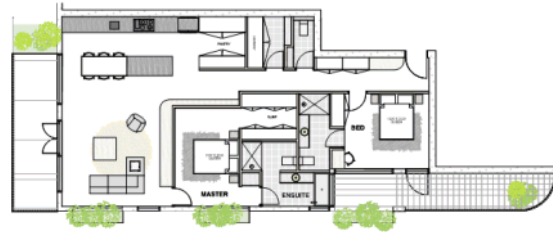
A2032
REV B



LEVEL 3
UNIT 5 - 2 BEDROOM
1100 @ A2
TOTAL INDOOR AREA 125 SQM
DECK AREA 41 SQM



LEVEL 4
UNIT 6 - 2 BEDROOM
1100 @ A1
TOTAL INDOOR AREA 118 SQM
DECK AREA 29 SQM

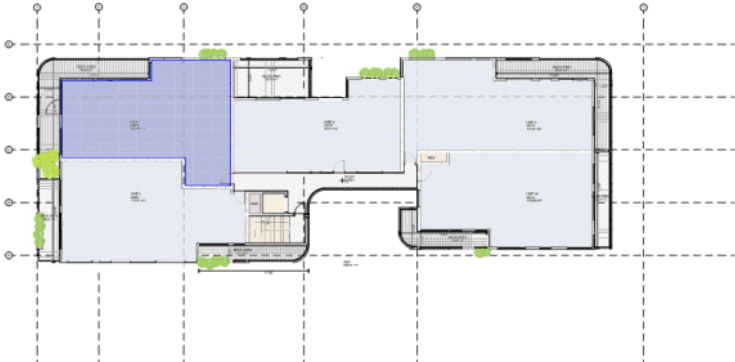


BRISBANE ST
Client
BESMARE STREET APARTMENTS
Site
SPAT DESIGN & ARCHITECT

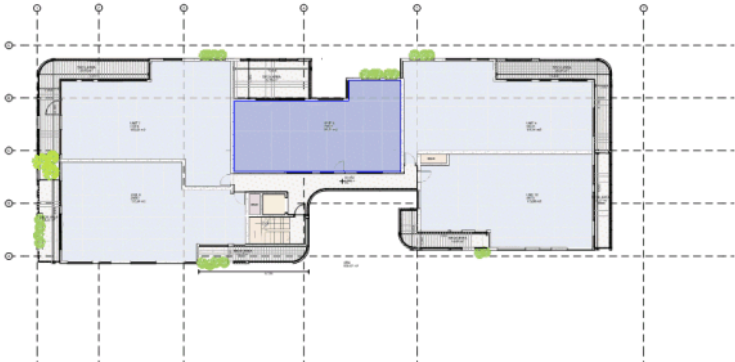
PARTITION PLANS - UNIT 5 & 6
Date: 27/3/2025
Scale: AS 1:5000
Status: FOR DA-DEUT



A2033
REV B



LEVEL 4
UNIT 7 - 2 BEDROOM
1100 SQM
TOTAL INDOOR AREA 123 SQM
DECK AREA 30 SQM



UNIT 8 - 2 BEDROOM
1100 SQM
TOTAL INDOOR AREA 100 SQM
DECK AREA 24 SQM

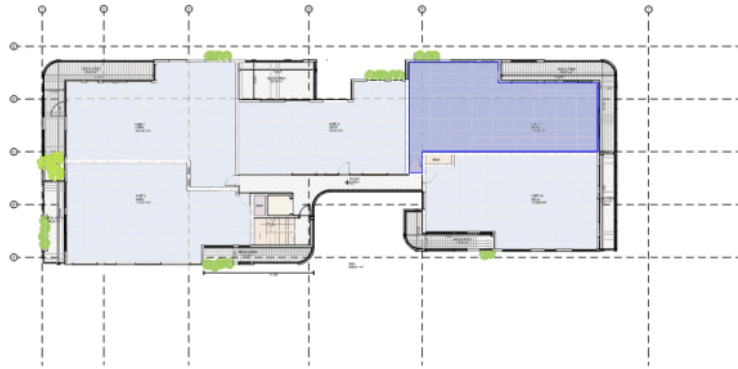


BRISBANE ST
Client
RESIDUAL STREET APARTMENTS
Site
3745 DREBIN VE 27/02

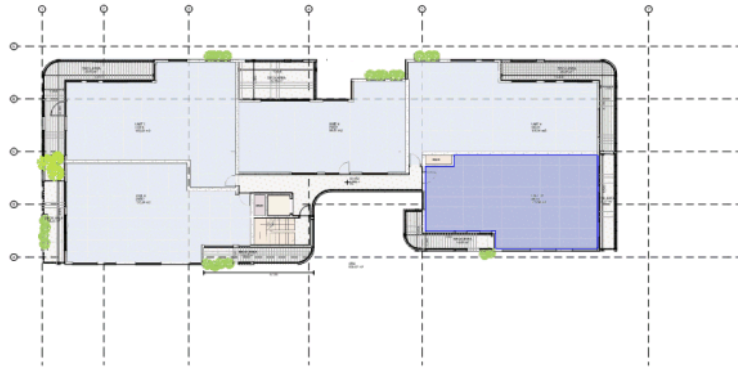
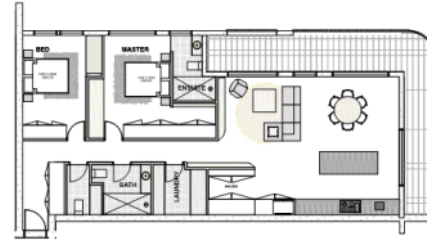
PARTITION PLANS - UNIT 7 & 8
Date: 27/2/2025
Scale: AS 1:4000
Status: FOR DA-DEVT



A2034
REV B



LEVEL 4
UNIT 9 - 2 BEDROOM
1/100 @ A1
TOTAL INDOOR AREA 117 SQM
DECK AREA 25 SQM



LEVEL 4
UNIT 10 - 2 BEDROOM
1/100 @ A1
TOTAL INDOOR AREA 113 SQM
DECK AREA 25 SQM



BRISBANE ST
Client
BRISBANE STREET APARTMENTS
Site
57-61 BRISBANE ST

PARTITION PLANS - UNIT 9 & 10
Date: 27/3/2025
Scale: AS 1:5000
Status: FOR DA-DEUT



A2035
REV B



PENTHOUSE 1
1/100 @ A1
TOTAL INDOOR AREA 190 SQM
DECK AREA 66 SQM



PENTHOUSE 2
1/100 @ A1
TOTAL INDOOR AREA 187 SQM
DECK AREA 39 SQM



PENTHOUSE 3
1/100 @ A1
TOTAL INDOOR AREA 203 SQM
DECK AREA 39 SQM



1:200
4. LEVEL 4 - PENTHOUSE



BRISBANE ST
Client
BESMARE STREET APARTMENTS
Site
37/43 BRESKIN VE STREET

PARTITION PLANS - PENTHOUSE
Date: 27/3/2025
Scale: AS 5:43064
Drawn: FOR DA-DRAFT



A2036
REV B



FINISHES

- C01 CEMENT SHEET GLAZING (GREEN)
- C01 ALUMINUM FRAMED GLAZING (DARK GREY)
- G02 ALUMINUM FRAMED GLAZING (GREEN)
- K0 OFF-FORM CONCRETE (LIGHT GREY)
- PC1 PRECAST CONCRETE (LIGHT GREY)
- S01 STEEL FRAMED GATE (DARK GREY)

CHANGES

- CH1 NEW LEVEL
- CH2 NEW SUBSTATION + PARKING ADJUSTMENT
- CH3 SETBACKS (PENTHOUSE)
- CH4 WASTE AND STORAGE ADJUSTMENT
- CH5 WALLS/DOOR
- CH6 EXTENDED COURTYARD
- CH7 ENCLOSED CORE



BRISBANE ST
 BRISBANE STREET APARTMENTS
 5/6-8 BRISBANE STREET

ELEVATION - NORTH & SOUTH
 Date: 25/3/2025
 Scale: 1/100
 Status: FOR ICA - DRAFT

A3003
 REV B



03 EAST

- Finishes schedule
- CS1 CEMENT SHEET CLADDING (GREEN)
 - GL1 ALUMINIUM FRAMED GLAZING (DARK GREY)
 - GL2 ALUMINIUM FRAMED GLAZING (GREEN)
 - PC1 OFF-FORM CONCRETE (LIGHT GREY)
 - PC1 PRECAST CONCRETE (LIGHT GREY)
 - SS1 STEEL FRAMED GATE (DARK GREY)

- CHANGES
- CH1 NEW LEVEL
 - CH2 NEW SUBSTATION + PARKING ADJUSTMENT
 - CH3 SETBACKS PROVISIONS
 - CH4 WASTE AND STORAGE ADJUSTMENT
 - CH5 WALKWAY
 - CH6 EXTENDED COURTYARD
 - CH7 ENCLOSED CORE



BRISBANE ST
 Client: BRISBANE STREET APARTMENTS
 Site: 51-53 BRISBANE STREET

ELEVATION - EAST
 Date: 25/3/2025
 Scale: 1/100
 Status: FOR QA - DWG

A3005
 REV B



S06 SECTION 01



BRISBANE ST
Client
BRISBANE STREET APARTMENTS
Site
SPATIAL DESIGN & STREET

SECTION - S06
Date: 27/3/2025
Scale: 1/100
Status: FOR DA-DRAW

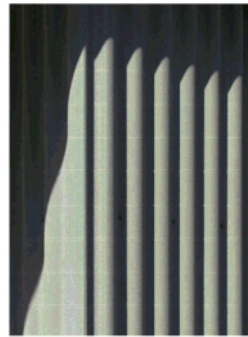
A3101
REV B



PC1



PR1



PR2



SH1



PC1



CS1



SS1



TR1

PROPOSED FINISHES

- CS1 CEMENT SHEET CLADDING (GREEN)
- GR1 ALUMINIUM FRAMED GLAZING (DARK GREY)
- GR2 ALUMINIUM FRAMED GLAZING (GREEN)
- IC1 OFF-FORM CONCRETE (LIGHT GREY)
- PC1 PRECAST CONCRETE (LIGHT GREY)
- SS1 STEEL FRAMED GATE AND BALUSTRADE (DARK GREY)
- TR1 EXISTING TREE



SH1



SH1



PR3



PR4

EXISTING CONTEXT

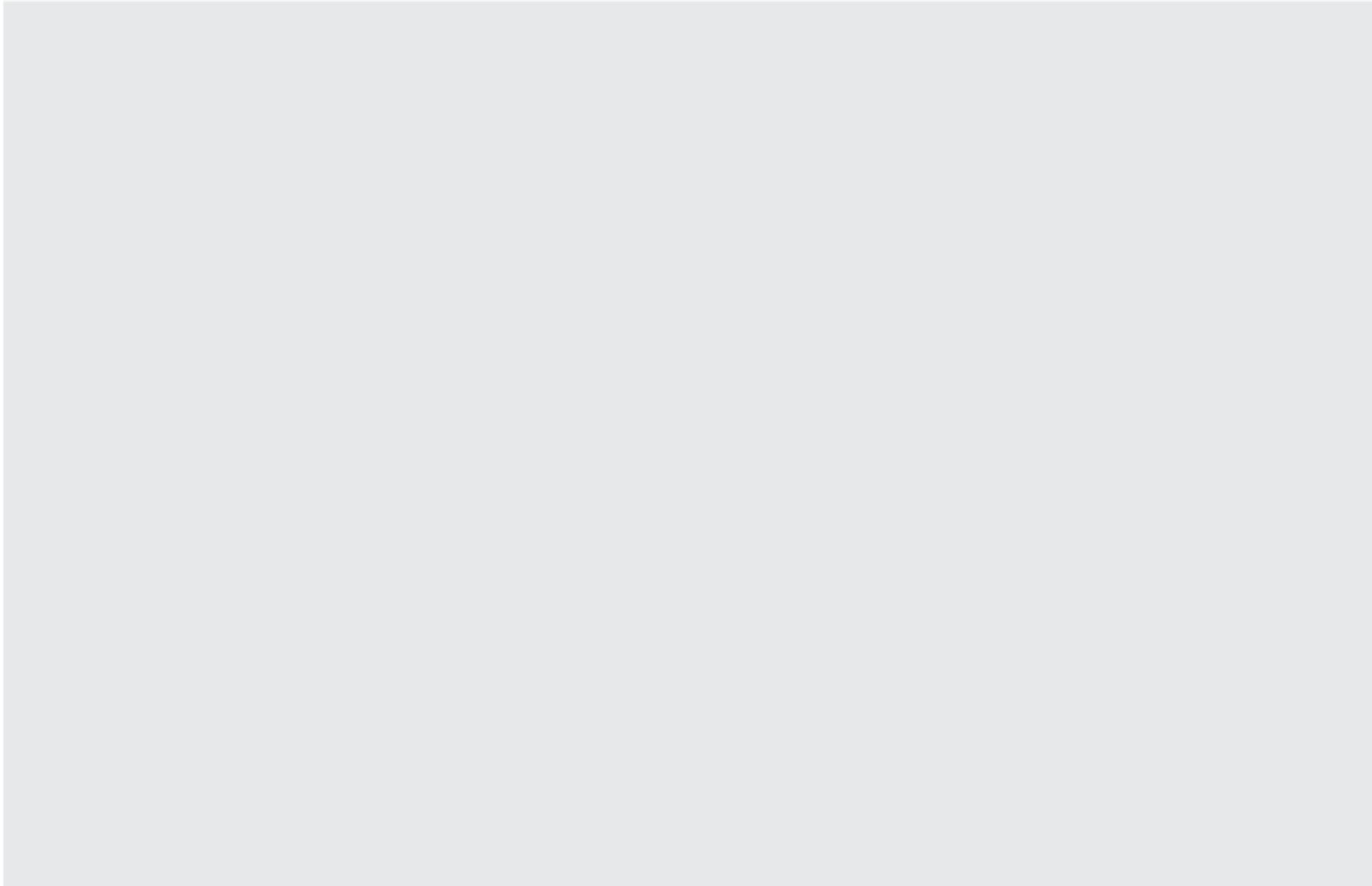
- SH1 FORMALLY SYDNEY HOUSE
- PR3 67-69 BRISBANE STREET
- PR4 160-162 ELIZABETH STREET

REFERENCE PROJECTS - INSPIRATION

- PR1 BUREAUX ANDREW PHILIPS - LUXEMBOURG
- PR2 ITTEN + BRECHBUHL - BASEL



6.0 3D VISUALISATIONS



NORTH ELEVATION



WEST ELEVATION



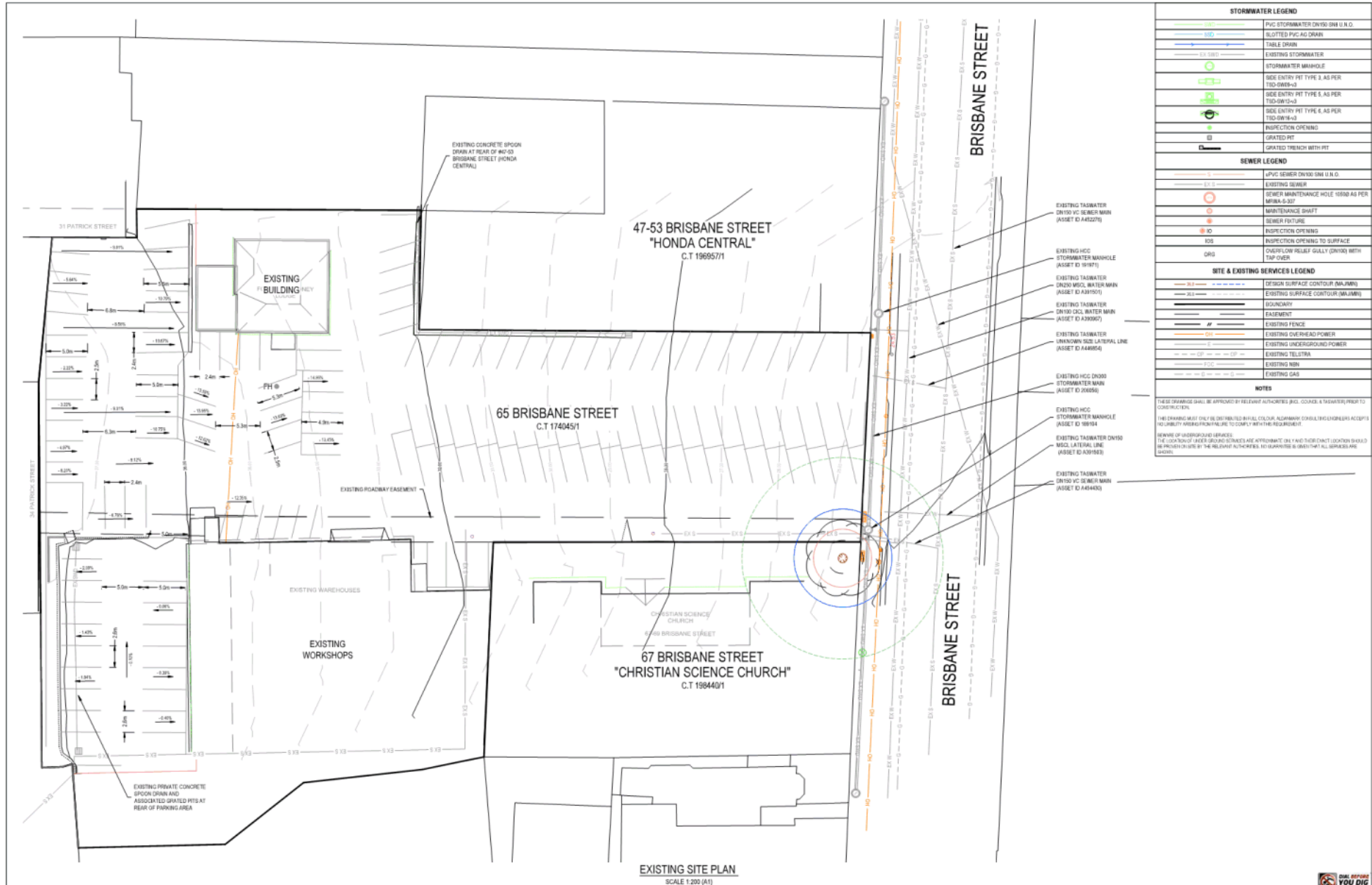
BRISBANE STREET ELEVATION



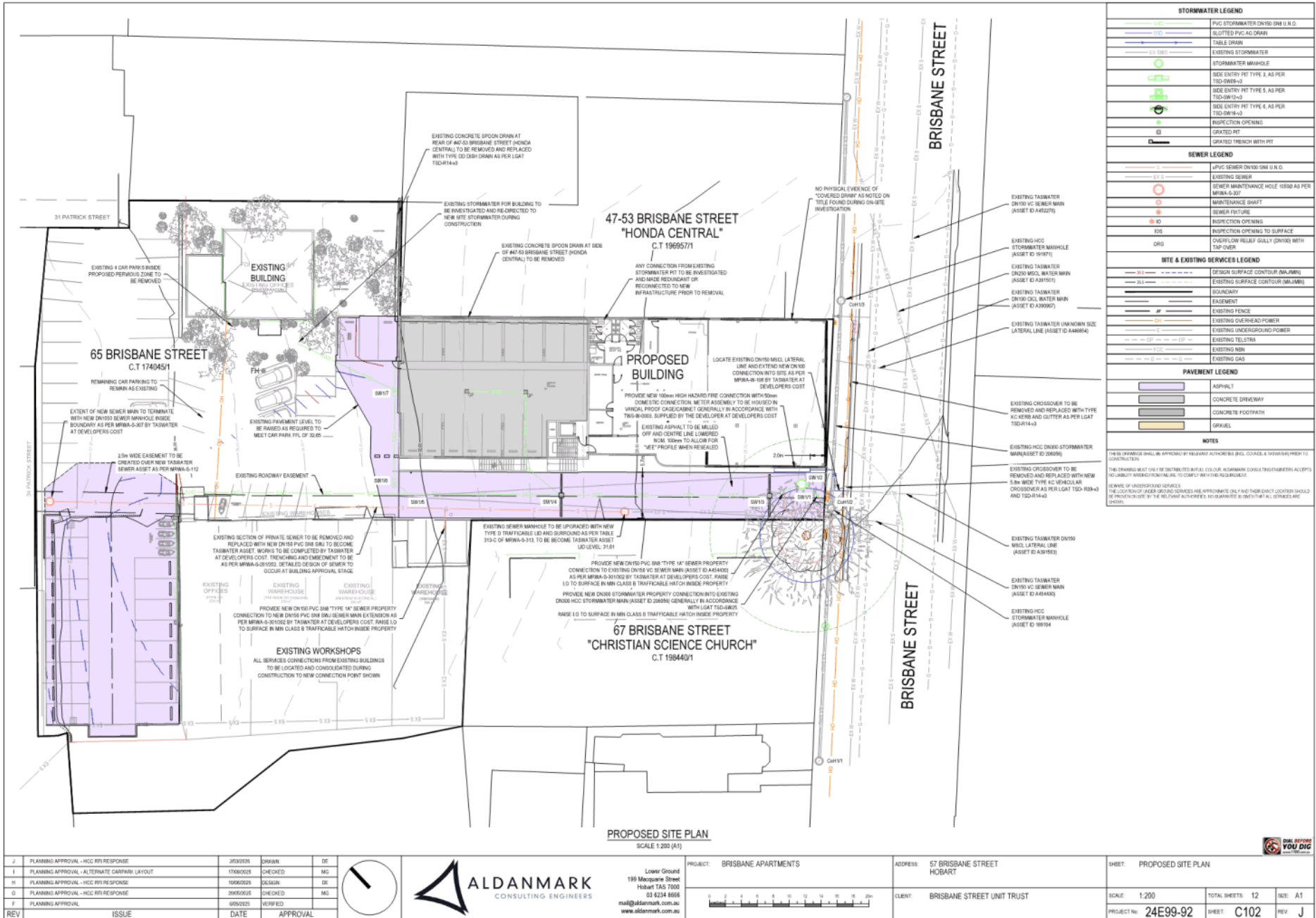
CIVIL DRAWINGS
 BRISBANE APARTMENTS
 57 BRISBANE STREET
 HOBART

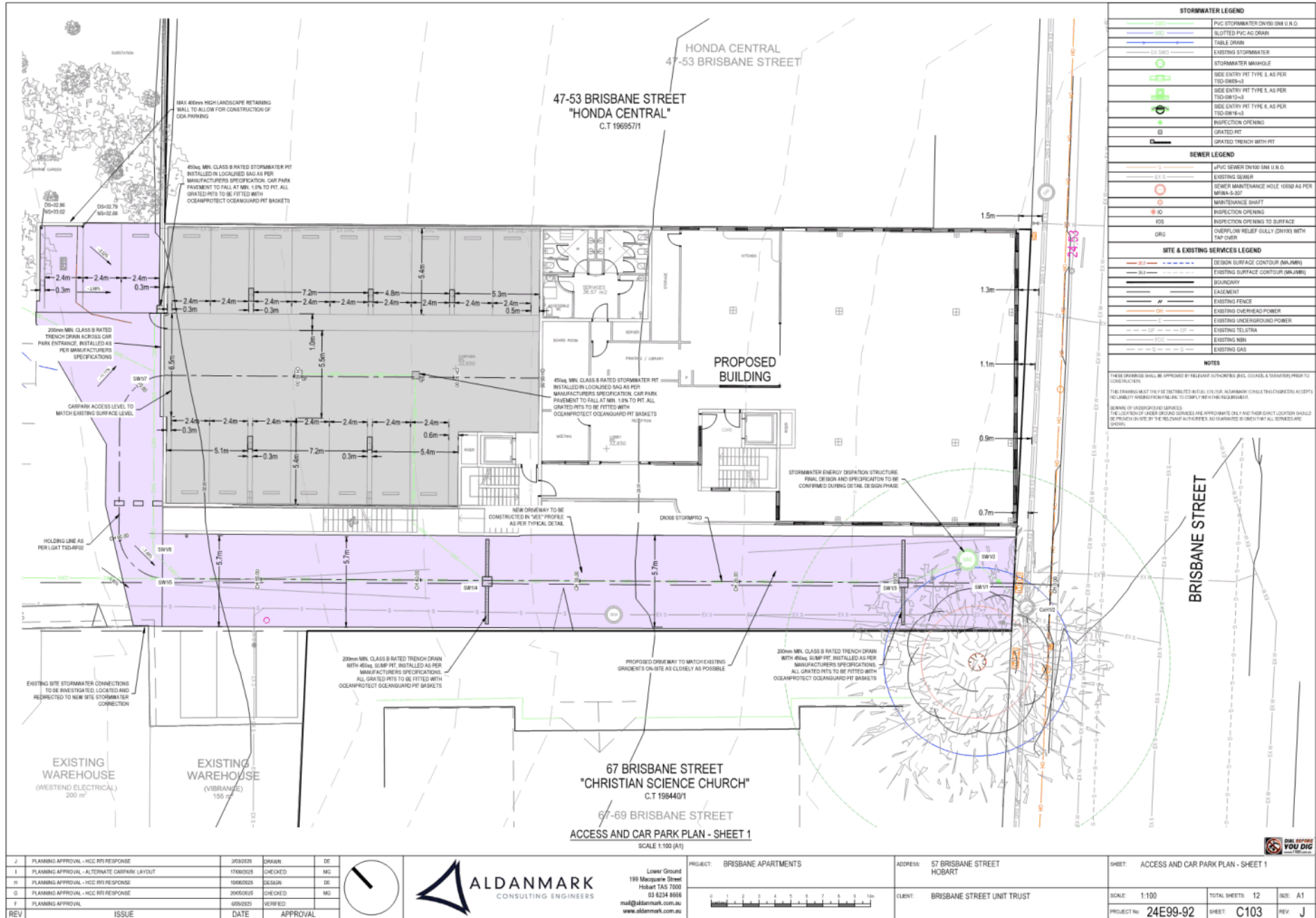
C001	COVER	J	2/03/2026
C101	EXISTING SITE PLAN	J	2/03/2026
C102	PROPOSED SITE PLAN	J	2/03/2026
C103	ACCESS AND CAR PARK PLAN - SHEET 1	J	2/03/2026
C104	ACCESS AND CAR PARK PLAN - SHEET 2	J	2/03/2026
C105	TURNPATH PLAN - SHEET 1	J	2/03/2026
C106	TURNPATH PLAN - SHEET 2	J	2/03/2026
C107	CAR PARK COMPARISON PLAN	J	2/03/2026
C201	ACCESS LONG SECTION	J	2/03/2026
C301	STORMWATER LONG SECTIONS	J	2/03/2026
C401	CONSTRUCTION DETAILS	J	2/03/2026
C701	OVERLAND FLOW PATH PLAN	J	2/03/2026

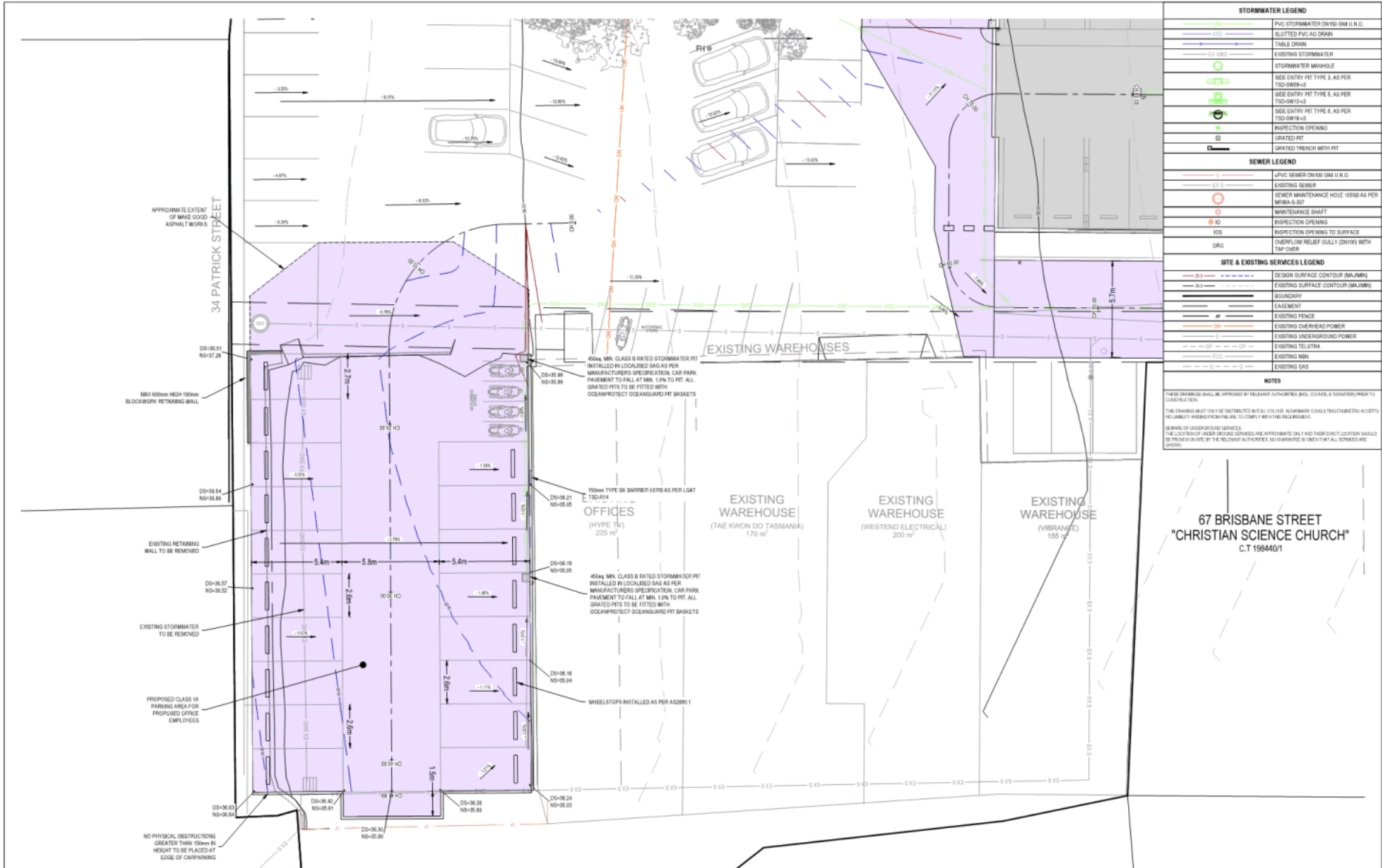
J	PLANNING APPROVAL - HCC RIR RESPONSE	2/03/2026	DRAWN	DE	 <p>ALDANMARK CONSULTING ENGINEERS</p> <p>Lower Ground 190 Macquarie Street Hobart TAS 7500 03 6234 8666 mail@aldanmark.com.au www.aldanmark.com.au</p>	PROJECT: BRISBANE APARTMENTS	ADDRESS: 57 BRISBANE STREET HOBART	SHEET: COVER		
I	PLANNING APPROVAL - ALTERNATE CARPARK LAYOUT	17/06/2025	CHECKED	MG						
II	PLANNING APPROVAL - HCC RIR RESPONSE	10/06/2025	DESIGN	DE						
III	PLANNING APPROVAL - HCC RIR RESPONSE	20/05/2025	CHECKED	MG						
F	PLANNING APPROVAL	05/02/25	VERIFIED				CLIENT: BRISBANE STREET UNIT TRUST	SCALE: AS INDICATED	TOTAL SHEETS: 12	SIZE: A1
REV	ISSUE	DATE	APPROVAL					PROJECT No: 24E99-92	SHEET: C001	REV: J



<table border="1"> <thead> <tr> <th>REV</th> <th>ISSUE</th> <th>DATE</th> <th>APPROVAL</th> </tr> </thead> <tbody> <tr> <td>J</td> <td>PLANNING APPROVAL - HCC RIR RESPONSE</td> <td>20230228</td> <td>DRAWN</td> </tr> <tr> <td>I</td> <td>PLANNING APPROVAL - ALTERNATE CARPARK LAYOUT</td> <td>17/06/2025</td> <td>CHECKED</td> </tr> <tr> <td>II</td> <td>PLANNING APPROVAL - HCC RIR RESPONSE</td> <td>10/06/2025</td> <td>DESIGN</td> </tr> <tr> <td>G</td> <td>PLANNING APPROVAL - HCC RIR RESPONSE</td> <td>20/05/2025</td> <td>CHECKED</td> </tr> <tr> <td>F</td> <td>PLANNING APPROVAL</td> <td>09/02/25</td> <td>VERIFIED</td> </tr> </tbody> </table>	REV	ISSUE	DATE	APPROVAL	J	PLANNING APPROVAL - HCC RIR RESPONSE	20230228	DRAWN	I	PLANNING APPROVAL - ALTERNATE CARPARK LAYOUT	17/06/2025	CHECKED	II	PLANNING APPROVAL - HCC RIR RESPONSE	10/06/2025	DESIGN	G	PLANNING APPROVAL - HCC RIR RESPONSE	20/05/2025	CHECKED	F	PLANNING APPROVAL	09/02/25	VERIFIED		<p>ALDANMARK CONSULTING ENGINEERS</p> <p>Lower Ground 190 Macquarie Street Hobart TAS 7500 03 6234 8666 mail@aldanmark.com.au www.aldanmark.com.au</p>	PROJECT: BRISBANE APARTMENTS ADDRESS: 57 BRISBANE STREET HOBART CLIENT: BRISBANE STREET UNIT TRUST	SHEET: EXISTING SITE PLAN SCALE: 1:200 PROJECT No: 24E99-92	TOTAL SHEETS: 12 SHEET: C101 SIZE: A1 REV: J
REV	ISSUE	DATE	APPROVAL																										
J	PLANNING APPROVAL - HCC RIR RESPONSE	20230228	DRAWN																										
I	PLANNING APPROVAL - ALTERNATE CARPARK LAYOUT	17/06/2025	CHECKED																										
II	PLANNING APPROVAL - HCC RIR RESPONSE	10/06/2025	DESIGN																										
G	PLANNING APPROVAL - HCC RIR RESPONSE	20/05/2025	CHECKED																										
F	PLANNING APPROVAL	09/02/25	VERIFIED																										



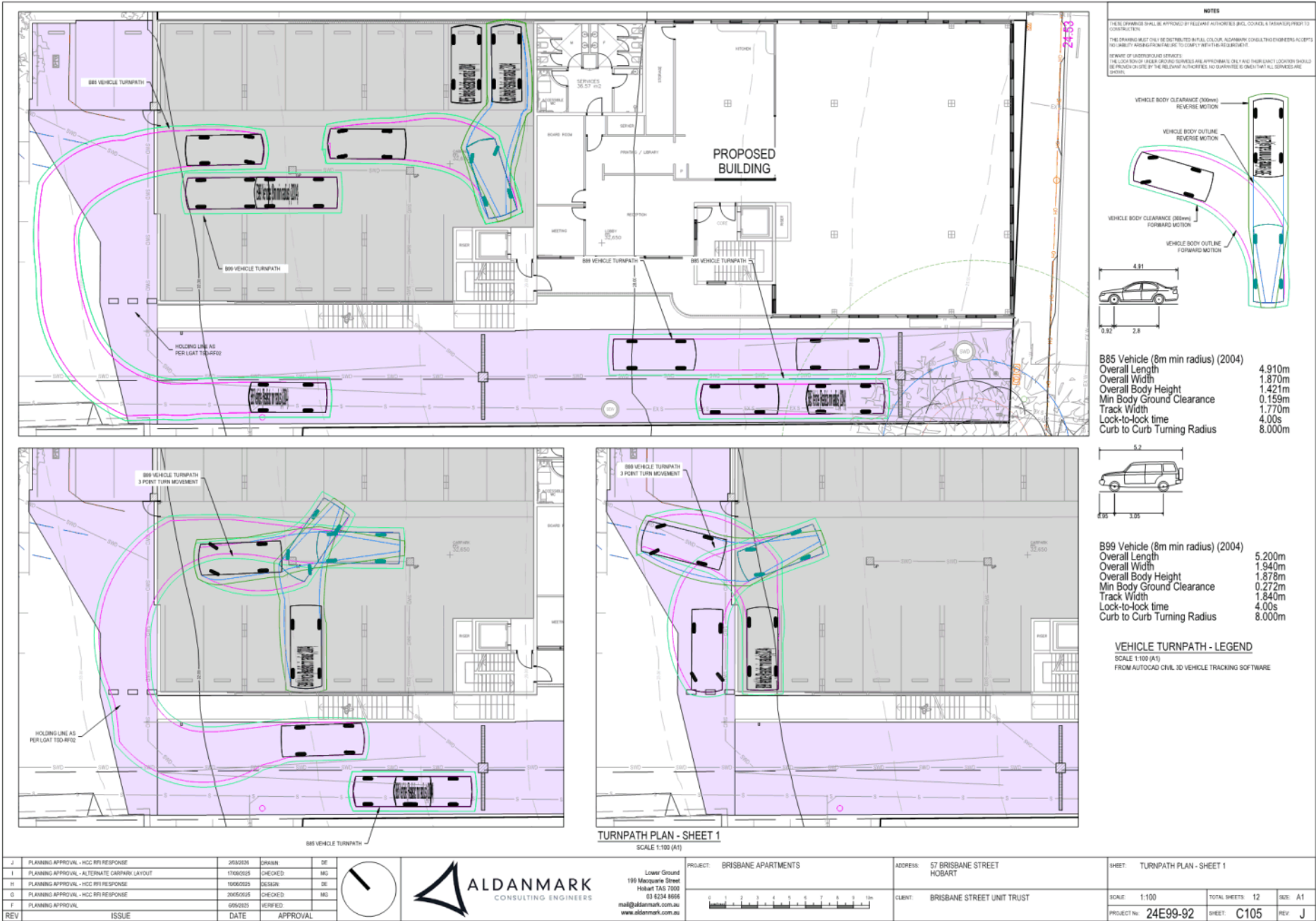


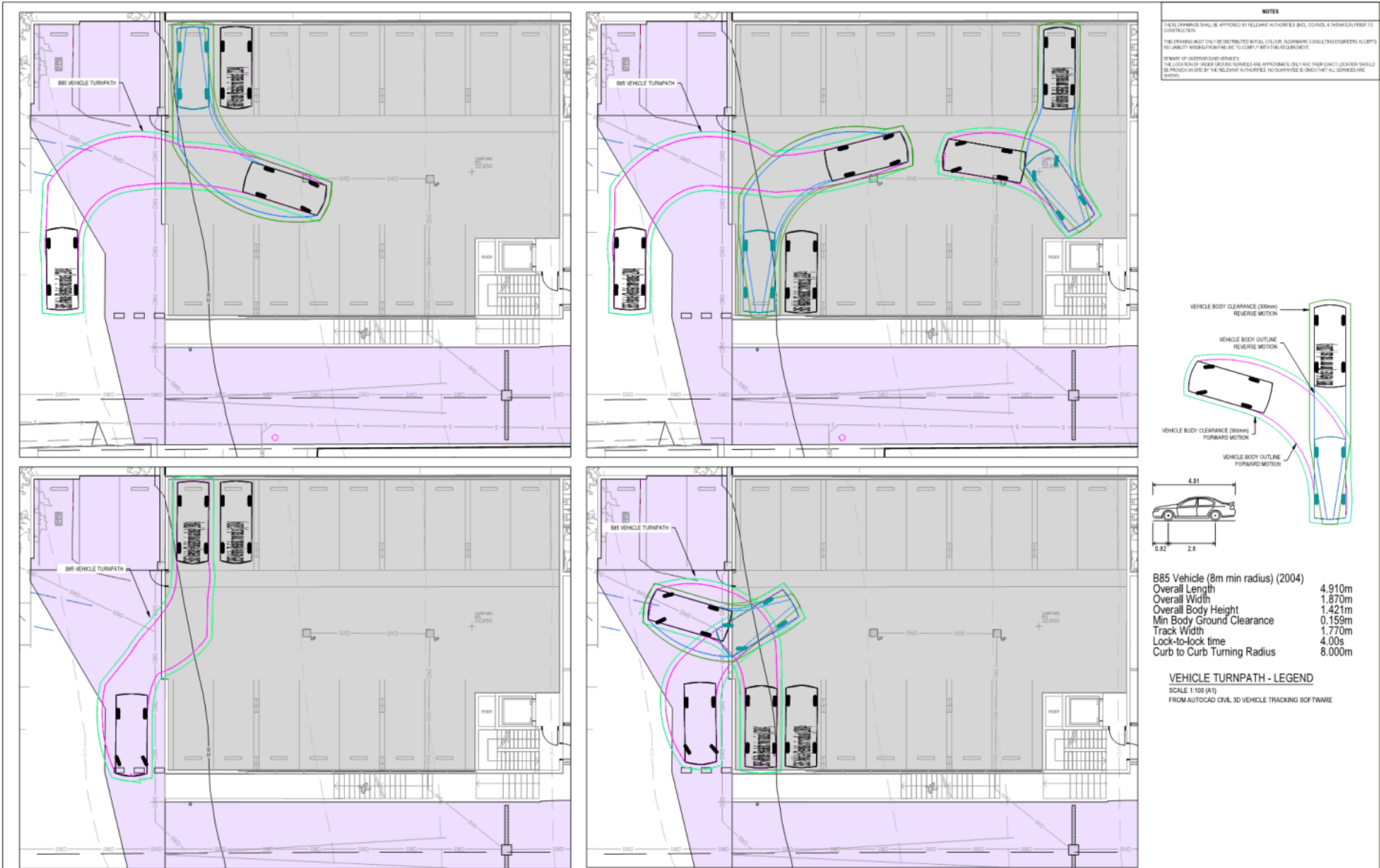


ACCESS AND CAR PARK PLAN - SHEET 2
 SCALE 1:100 (A1)

J	PLANNING APPROVAL - HCC RIR RESPONSE	2023/02/28	DRAWN	DE			Lower Ground 190 Macquarie Street Hobart TAS 7500 03 6234 8666 mail@aldanmark.com.au www.aldanmark.com.au	PROJECT: BRISBANE APARTMENTS ADDRESS: 57 BRISBANE STREET HOBART CLIENT: BRISBANE STREET UNIT TRUST	SHEET: ACCESS AND CAR PARK PLAN - SHEET 2 SCALE: 1:100 PROJECT No: 24E99-92 TOTAL SHEETS: 12 SHEET: C104 SIZE: A1 REV: J
I	PLANNING APPROVAL - ALTERNATE CARPARK LAYOUT	17/06/2025	CHECKED	MG					
H	PLANNING APPROVAL - HCC RIR RESPONSE	10/06/2025	DESIGN	DE					
G	PLANNING APPROVAL - HCC RIR RESPONSE	20/05/2025	CHECKED	MG					
REV	ISSUE	DATE	APPROVAL						

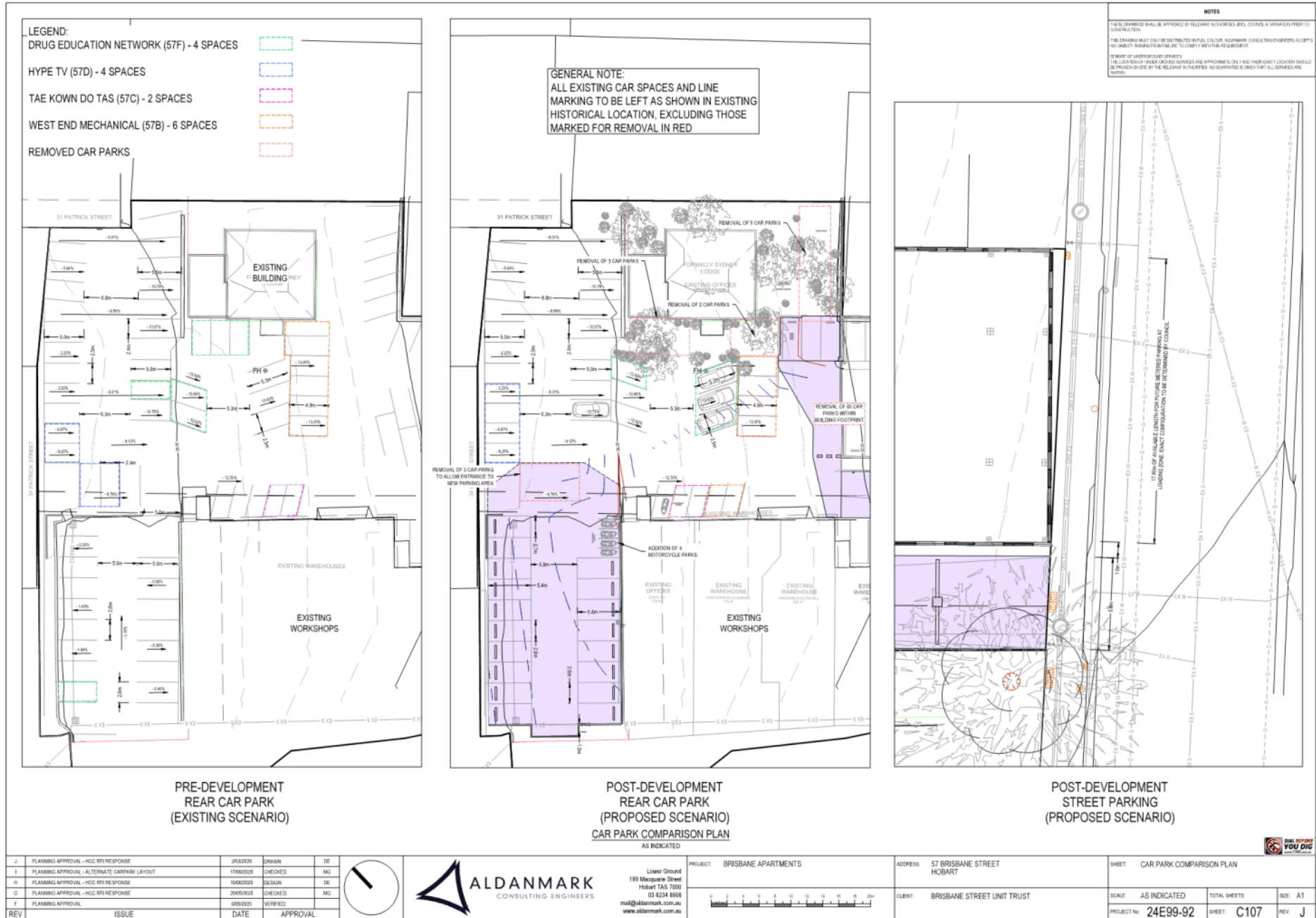


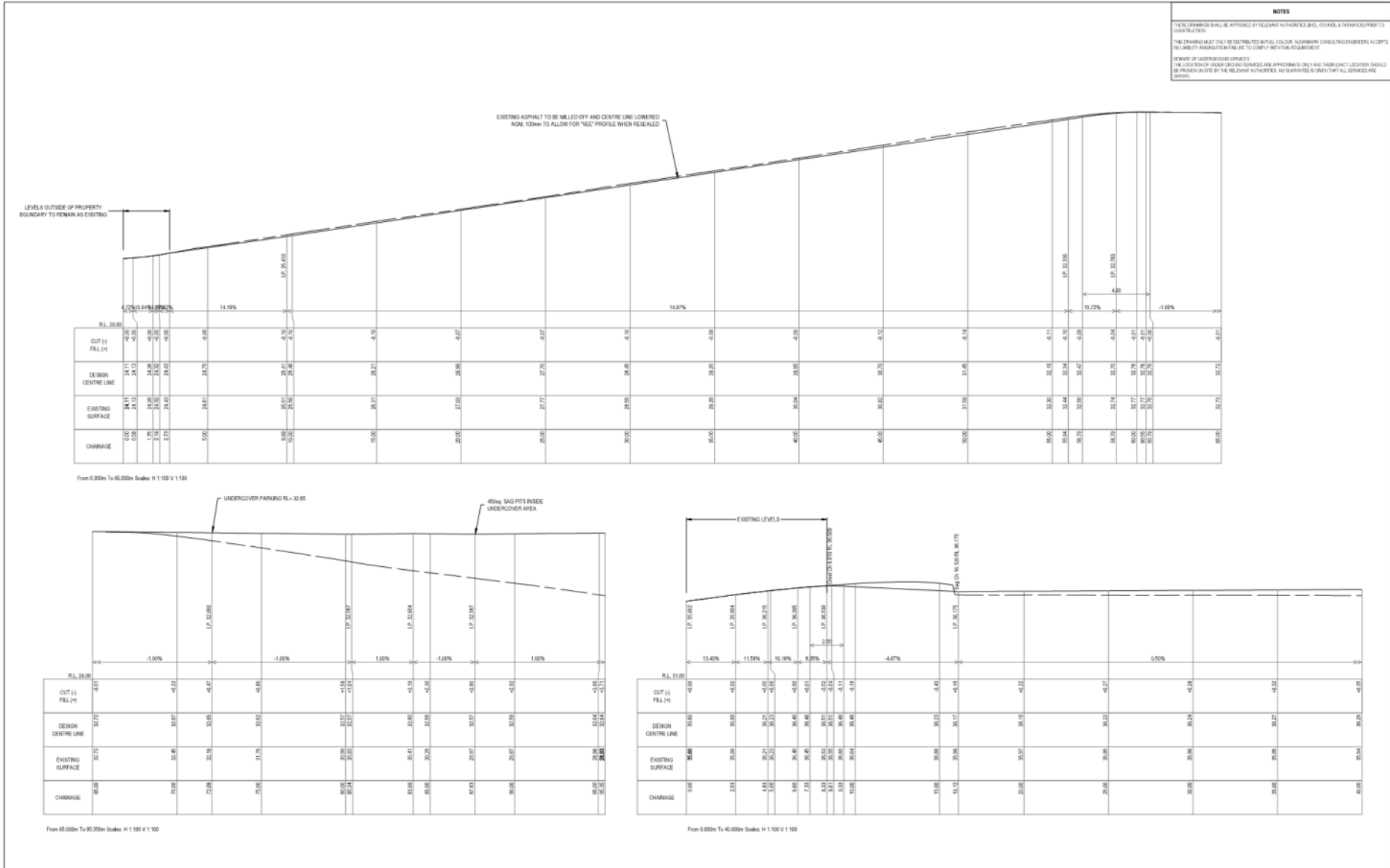




TURNPATH PLAN - SHEET 2
 SCALE 1:100 (A1)

<table border="1"> <tr><td>J</td><td>PLANNING APPROVAL - HCC R/R RESPONSE</td><td>JOS3026</td><td>DRAWN</td><td>DE</td></tr> <tr><td></td><td></td><td></td><td>CHECKED</td><td>MG</td></tr> <tr><td></td><td></td><td></td><td>DESIGN</td><td>DE</td></tr> <tr><td></td><td></td><td></td><td>CHECKED</td><td>MG</td></tr> <tr><td></td><td></td><td></td><td>VERIFIED</td><td></td></tr> <tr><td>REV</td><td>ISSUE</td><td>DATE</td><td>APPROVAL</td><td></td></tr> </table>	J	PLANNING APPROVAL - HCC R/R RESPONSE	JOS3026	DRAWN	DE				CHECKED	MG				DESIGN	DE				CHECKED	MG				VERIFIED		REV	ISSUE	DATE	APPROVAL			<p>ALDANMARK CONSULTING ENGINEERS</p> <p>Lower Ground 190 Macquarie Street Hobart TAS 7500 03 6234 8666 mail@aldanmark.com.au www.aldanmark.com.au</p>	<p>PROJECT: BRISBANE APARTMENTS</p> <p>ADDRESS: 57 BRISBANE STREET HOBART</p> <p>CLIENT: BRISBANE STREET UNIT TRUST</p>	<p>SHEET: TURNPATH PLAN - SHEET 2</p> <p>SCALE: 1:100</p> <p>PROJECT No: 24E99-92</p>	<p>TOTAL SHEETS: 12</p> <p>SHEET: C106</p>	<p>SIZE: A1</p> <p>REV: J</p>
J	PLANNING APPROVAL - HCC R/R RESPONSE	JOS3026	DRAWN	DE																																
			CHECKED	MG																																
			DESIGN	DE																																
			CHECKED	MG																																
			VERIFIED																																	
REV	ISSUE	DATE	APPROVAL																																	





NOTES

1. THESE DRAWINGS SHALL BE APPROVED BY RELEVANT AUTHORITIES (B.C.L, COACHES & TAXIWAYS) PRIOR TO CONSTRUCTION.

2. THE DRAWING MUST ONLY BE DISTRICTED BY FULL COLOUR. ALDANMARK CONSULTING ENGINEERS ACCEPTS NO LIABILITY FOR ANY DAMAGE TO PROPERTY OR PERSONS ARISING FROM THE USE OF THESE DRAWINGS.

3. THE LOCATION OF THESE DRAWINGS IS APPROXIMATE. ONLY THE EXACT LOCATION SHOULD BE PROVIDED ON-SITE BY THE RELEVANT AUTHORITIES TO GUARANTEE ALL SERVICES ARE SPHERE.

LONG SECTIONS
 SCALE 1:100 (A1)

J	PLANNING APPROVAL - HCC RIR RESPONSE	2023/02/28	DRAWN	DE
I	PLANNING APPROVAL - ALTERNATE CARPARK LAYOUT	17/06/2025	CHECKED	MG
H	PLANNING APPROVAL - HCC RIR RESPONSE	10/06/2025	DRAWN	DE
G	PLANNING APPROVAL - HCC RIR RESPONSE	20/05/2025	CHECKED	MG
F	PLANNING APPROVAL	04/05/2025	VERIFIED	
REV	ISSUE	DATE	APPROVAL	

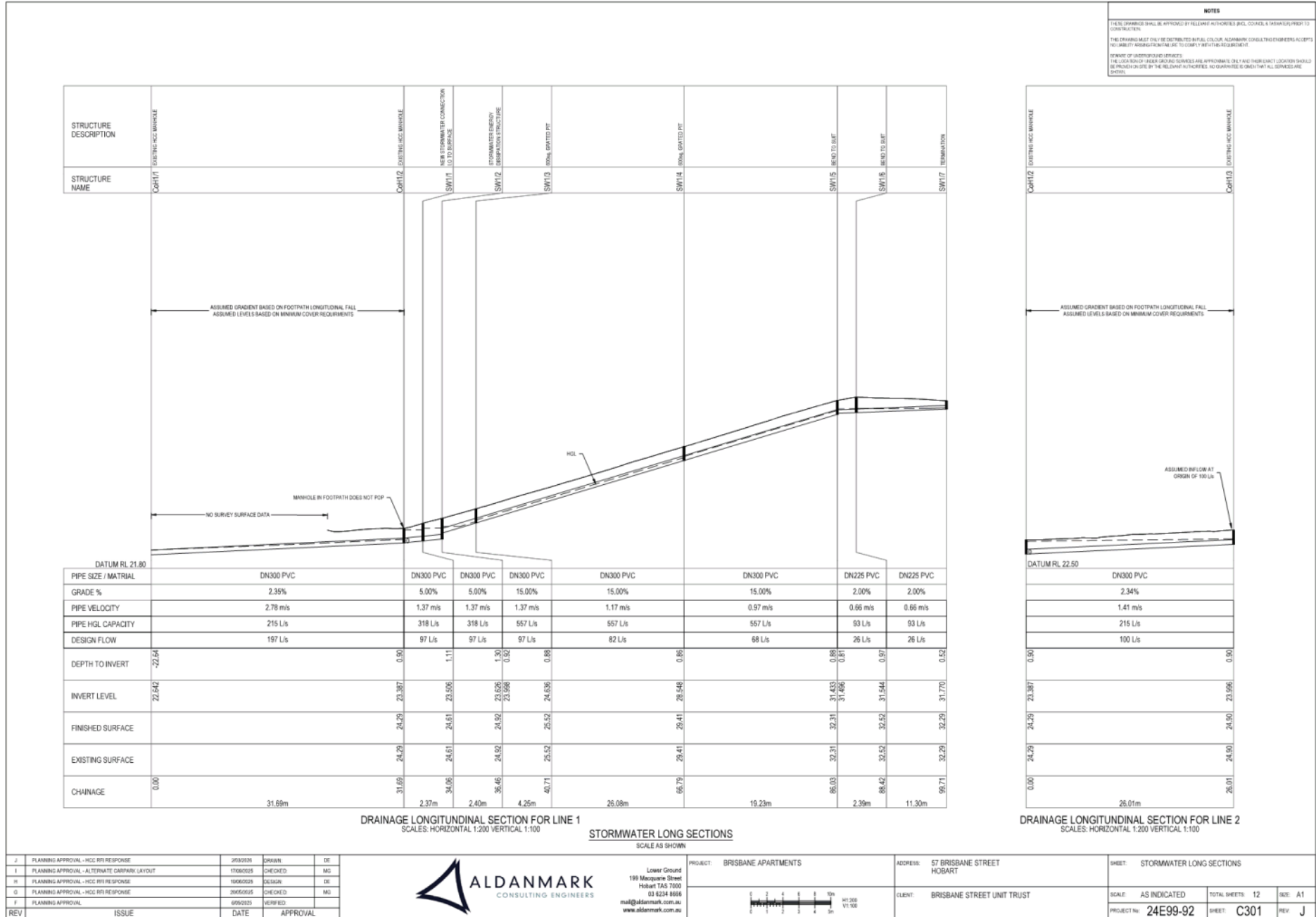


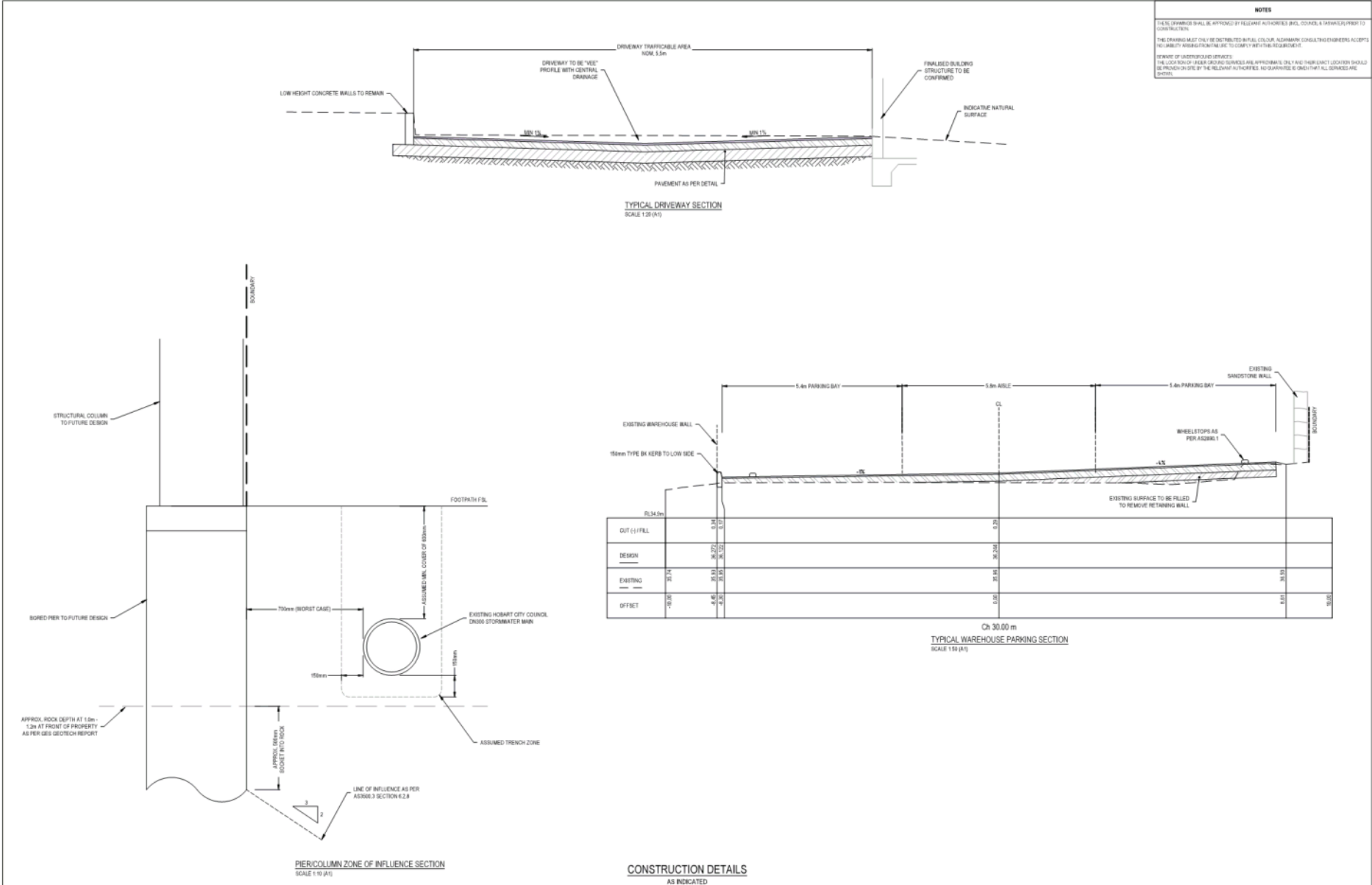
Lower Ground
 199 Macquarie Street
 Hobart TAS 7500
 03 6234 8666
 mail@aldanmark.com.au
 www.aldanmark.com.au

PROJECT: BRISBANE APARTMENTS
 ADDRESS: 57 BRISBANE STREET HOBART
 CLIENT: BRISBANE STREET UNIT TRUST



SHEET: ACCESS LONG SECTION
 SCALE: AS INDICATED
 PROJECT No: 24E99-92
 TOTAL SHEETS: 12
 SHEET: C201
 SIZE: A1
 REV: J





NOTES

1. THESE DRAWINGS SHALL BE APPROVED BY RELEVANT AUTHORITIES (B/C/L, COACHES & TAXIWAYS) PRIOR TO CONSTRUCTION.

2. THE DRAWINGS MUST ONLY BE OBTAINED BY FULLY QUALIFIED, ACCREDITED CONSULTANTS WHO ACCEPTS THE LIABILITY AND IS OBLIGED TO COMPLY WITH THE REQUIREMENT.

3. THE LOCATION OF THESE EXISTING SERVICES ARE APPROXIMATE. B/L & ARE THEREFORE LOCATIONS SHOULD BE CHECKED ON-SITE BY THE RELEVANT AUTHORITIES TO GUARANTEE ALL SERVICES ARE SPHERE.

	0+00	0+10	0+20	0+30	0+40	0+50	0+60	0+70	0+80	0+90	1+00
OUT (+) FILL		0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
DESIGN		0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
EXISTING	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
OFF SET	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

CONSTRUCTION DETAILS
 AS INDICATED

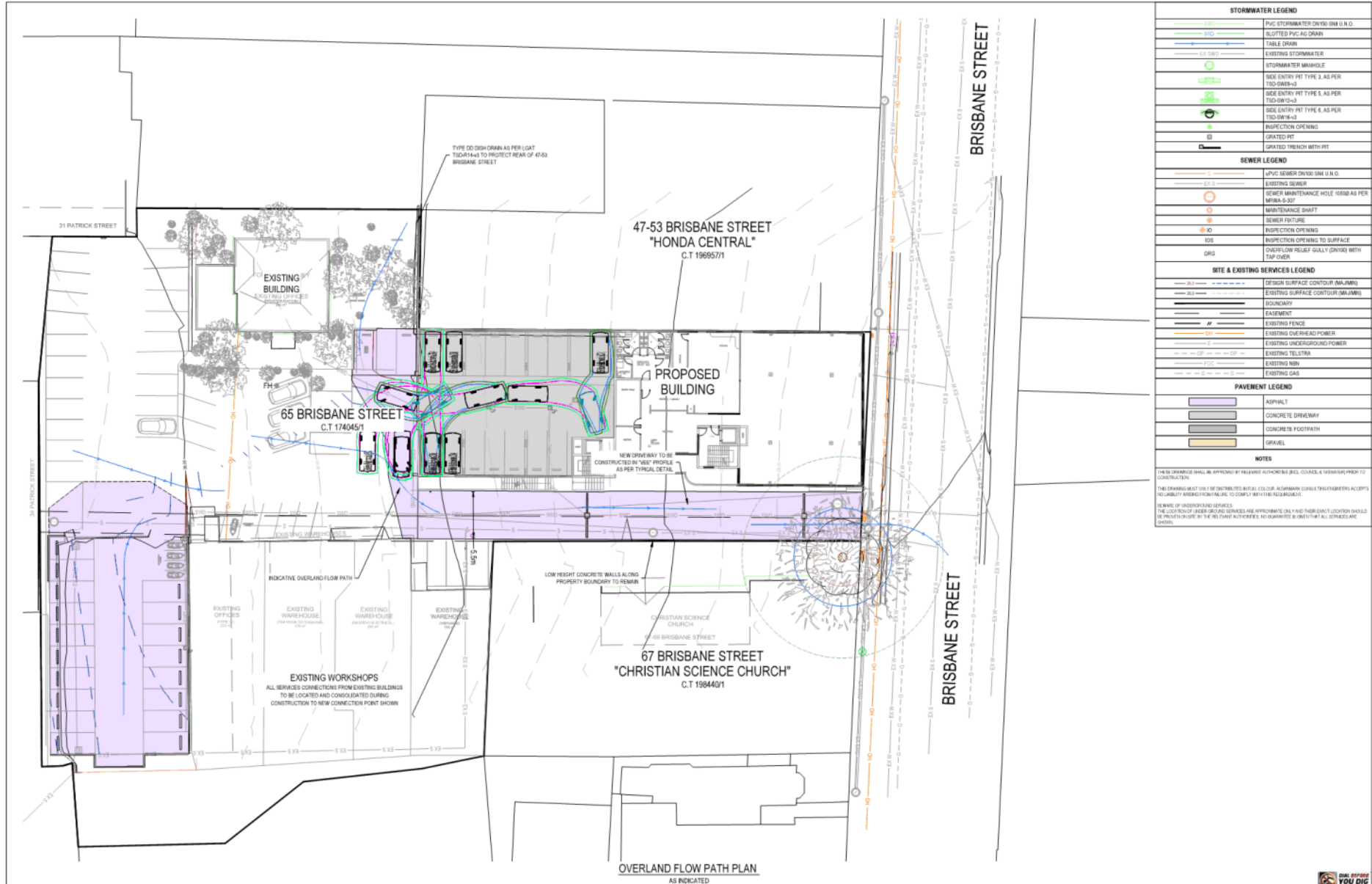
REV	ISSUE	DATE	APPROVAL
J	PLANNING APPROVAL - HCC R/R RESPONSE	20250228	DRAWN: DE
I	PLANNING APPROVAL - ALTERNATE CARPARK LAYOUT	17/06/2025	CHECKED: MG
II	PLANNING APPROVAL - HCC R/R RESPONSE	10/06/2025	DESIGN: DE
G	PLANNING APPROVAL - HCC R/R RESPONSE	20/05/2025	CHECKED: MG
F	PLANNING APPROVAL	05/02/25	VERIFIED:

ALDANMARK
 CONSULTING ENGINEERS

Lower Ground
 190 Macquarie Street
 Hobart TAS 7500
 03 6234 8666
 mail@aldanmark.com.au
 www.aldanmark.com.au

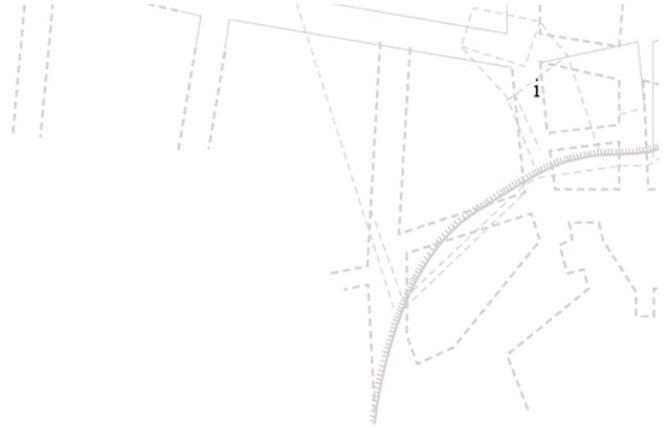
PROJECT: BRISBANE APARTMENTS	ADDRESS: 57 BRISBANE STREET HOBART	SHEET: CONSTRUCTION DETAILS
AS INDICATED	CLIENT: BRISBANE STREET UNIT TRUST	SCALE: AS INDICATED
		TOTAL SHEETS: 12
		SHEET: C401
		PROJECT No: 24E99-92
		REV: J

TOTAL SHEETS: 12	SIZE: A1
SHEET: C401	REV: J



STORMWATER LEGEND	
	PVC STORMWATER DN150 (9M) U.I.D.
	SLOTTED PVC 40 DRAIN
	TABLE DRAIN
	EXISTING STORMWATER
	STORMWATER MANHOLE
	SIDE ENTRY PIT TYPE 3, AS PER TSD-DW18-03
	SIDE ENTRY PIT TYPE 5, AS PER TSD-DW18-03
	SIDE ENTRY PIT TYPE 6, AS PER TSD-DW18-03
	INSPECTION OPENING
	GRATED PIT
	GRATED TRENCH WITH PIT
SEWER LEGEND	
	PVC SEWER DN150 (9M) U.I.D.
	EXISTING SEWER
	SEWER MAINTENANCE HOLE 1000D AS PER NBS16-007
	MAINTENANCE SHAFT
	SEWER FIXTURE
	INSPECTION OPENING
	INSPECTION OPENING TO SURFACE
	OVERFLOW RELIEF GULLY (ORTG) WITH TAP OVER
	DESIGN SURFACE CONTOUR (MAJORS)
	EXISTING SURFACE CONTOUR (MAJORS)
	BOUNDARY
	EASEMENT
	EXISTING FENCE
	EXISTING OVERHEAD POWER
	EXISTING UNDERGROUND POWER
	EXISTING TELSTRA
	EXISTING NBN
	EXISTING GAS
PAVEMENT LEGEND	
	ASPHALT
	CONCRETE DRIVEWAY
	CONCRETE FOOTPATH
	GRAVEL
NOTES	
1. ALL SERVICES SHALL BE APPROVED BY RELEVANT AUTHORITIES (E.G. COUNCIL & TRANSFER PRIOR TO CONSTRUCTION). 2. THIS DRAWING MEET ONE (1) OF DISTRIBUTED BUT NOT ALL OF OUR ALDANMARK CONSULTANTS' RESPONSIBILITIES. ACCEPTS LIABILITY FOR THE DESIGN AND CONSTRUCTION OF THE PROJECT. 3. THE LOCATION OF UNDERGROUND SERVICES ARE APPROXIMATE ONLY AND THEIR EXACT LOCATION SHOULD BE PROVIDED BY THE RELEVANT AUTHORITIES TO GUARANTEE A SUCCESSFUL INSTALLATION.	

<table border="1"> <thead> <tr> <th>REV</th> <th>ISSUE</th> <th>DATE</th> <th>APPROVAL</th> </tr> </thead> <tbody> <tr> <td>J</td> <td>PLANNING APPROVAL - HCC RIR RESPONSE</td> <td>20230228</td> <td>DRAWN</td> </tr> <tr> <td>I</td> <td>PLANNING APPROVAL - ALTERNATE CARPARK LAYOUT</td> <td>17060025</td> <td>CHECKED</td> </tr> <tr> <td>H</td> <td>PLANNING APPROVAL - HCC RIR RESPONSE</td> <td>10060025</td> <td>DESIGN</td> </tr> <tr> <td>G</td> <td>PLANNING APPROVAL - HCC RIR RESPONSE</td> <td>20050025</td> <td>CHECKED</td> </tr> <tr> <td>F</td> <td>PLANNING APPROVAL</td> <td>04052025</td> <td>VERIFIED</td> </tr> </tbody> </table>	REV	ISSUE	DATE	APPROVAL	J	PLANNING APPROVAL - HCC RIR RESPONSE	20230228	DRAWN	I	PLANNING APPROVAL - ALTERNATE CARPARK LAYOUT	17060025	CHECKED	H	PLANNING APPROVAL - HCC RIR RESPONSE	10060025	DESIGN	G	PLANNING APPROVAL - HCC RIR RESPONSE	20050025	CHECKED	F	PLANNING APPROVAL	04052025	VERIFIED		<p>ALDANMARK CONSULTING ENGINEERS</p>	<p>Lower Ground 190 Macquarie Street Hobart TAS 7500 03 6234 8666 mail@aldanmark.com.au www.aldanmark.com.au</p>	PROJECT: BRISBANE APARTMENTS ADDRESS: 57 BRISBANE STREET HOBART CLIENT: BRISBANE STREET UNIT TRUST	SHEET: OVERLAND FLOW PATH PLAN SCALE: AS INDICATED PROJECT NO: 24E99-92	TOTAL SHEETS: 12 SHEET: C701 SIZE: A1 REV: J
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Planning Report

57-65 Brisbane Street

Additional Storey for Approved Mixed Use Redevelopment



14 March 2026

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1. Introduction

All Urban Planning Pty Ltd has been engaged by Aldanmark to provide a planning assessment of a proposal for alterations and extensions to an approved but not yet constructed mixed use development at 57-65 Brisbane Street under the *Hobart Local Provisions Schedule of the Tasmanian Planning Scheme* (planning scheme).

The proposal involves adding an additional storey to the approved development PLN-HOB-2025-0147 along with some other relatively minor alterations as ‘clouded’ on the accompanying plans.

This report has been updated 14 March 2026 to include the substantial modifications to the Hobart Commercial Zone Specific Area Plan that are in effect but not available to view on the Tasmanian Planning Scheme Viewer.

1.1 Approved Development

Planning Permit PLN-HOB-2025-0147, issued on 14 August 2025, approved the construction of a new five-storey mixed-use building on the site. The approved development includes a ground-floor general retail tenancy and service facilities, beneath two levels of commercial office space on the first and second floors. The upper two levels (Third and Fourth Floors) are dedicated to residential use, comprising eight multiple dwellings (five 2-bedroom and three 3-bedroom units).

The approved design incorporates 72 on-site car parking spaces including 59 existing external spaces at the rear of the site and 13 enclosed spaces on the second floor of the building. The approval also provides for 26 bicycle spaces and heritage conservation works, specifically the reinstatement of a landscaped curtilage around the existing Sydney Lodge building.

1.2 Site & Surrounds

The site has an area of 4775m² and fronts the northwestern side of Brisbane Street on the northern fringe of the CBD between Nos. 47-53 and 67 Brisbane Street. The site has a comparatively narrow street frontage (approximately 24 metres) which widens internally. The street front is vacant of buildings and comprises of a 146 space carpark, with the buildings at the rear of the site including the c1860 *Sydney Lodge* building and a series of 1960s workshop buildings, separated and surrounded by carparking areas.

Address	CT
65 Brisbane Street	174045/1



Figure 1– Site Plan (Source: Annotated from theLIST)

There is an existing road easement over the southwestern side of the title as shown in Figure 2 below.



Level 3:

Residential level comprising 5 x two bedroom dwellings. An extended shared courtyard space is proposed, with a corresponding reduction in the floor area of Unit 3.

Level 4:

An additional residential level comprising 5 x two-bedroom dwellings and an enclosed fire stair.

Level 5:

The approved penthouse level, containing 3 x three-bedroom dwellings, is elevated from Level 4 to Level 5. This level also includes a reduced setback to Brisbane Street, the enclosed fire stair and alterations to the private deck areas of Penthouse Apartments 1 and 3.

The introduction of the new Level 4 increases the overall height of the building by one storey, resulting in a six-storey building above street level.

The proposal also includes the reinstatement of a soft landscaped setting for Sydney Lodge, rationalisation of nearby parking including two DDA-compliant car parking spaces, and a new substation located to the south-east of Sydney Lodge.

3. The Planning Scheme

The subject site is zoned Commercial under the planning scheme. Surrounding land is also zoned Commercial to the east, Inner Residential to the north-west, Central Business to the west, and Community Purpose to the south-west, associated with the Christian Science Church at 67 Brisbane Street, as shown in Figure 3.

The mix of surrounding zones reflects the site’s transitional urban context and supports the ongoing use of the land for a mixed-use development incorporating commercial and residential activities. In this regard, the proposal is consistent with the established and emerging character of the locality and the intent of the Commercial Zone to accommodate a diverse range of uses in accessible innercity locations.

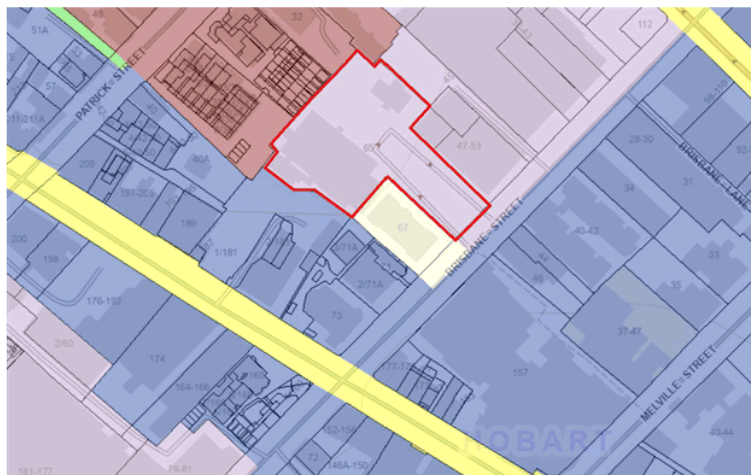


Figure 3 - Zoning plan (Source: annotated plan from theLIST)



Part of the subject site is included in Table C6.1 of the planning scheme as a place of local heritage significance. However, the site is also partly included on the Tasmanian Heritage Register and, by virtue of Clause C6.2.2, the Local Historic Heritage Code applies only to those parts of the site not included on the Register. This is discussed further in the assessment against the Code below.

A Bhutan cypress tree located on the adjoining property to the south-west at 67 Brisbane Street is also listed as a Significant Tree (HOB-C6.5.20) under the Local Historic Heritage Code. The canopy and root zone of this tree extend onto the subject site in the vicinity of the approved vehicle access.

The Tasmanian Planning Commission's decision on the new planning scheme included a direction for substantial modifications under section 35KB of the Act, resulting in the inclusion of the Hobart Commercial Zone Specific Area Plan (HOB-S6.0). It is understood that, pursuant to section 51(3AC) of the Act, this application is to be assessed on the basis that the Specific Area Plan is in effect.

3.1 Zoning

This site is zoned Commercial. The provisions of the Hobart Commercial Zone Specific Area Plan apply in substitution for the provisions of the Commercial Zone in the State Planning Provisions (Clause HOB-S6.2.2).

3.2 HOB-S6.0 Hobart Commercial Zone Specific Area Plan

Purpose of the SAP is:

- HOB-S6.1.1 To provide for a diversity of generally non-residential uses that reflect the transition between land in the Central Business Zone and residential areas.*
- HOB-S6.1.2 To provide for residential use primarily above ground floor level.*
- HOB-S6.1.3 To provide that building height reflects the transition between the Central Business Zone and the lower scale of the surrounding Urban Mixed Use and Inner Residential Zones.*
- HOB-S6.1.4 To provide for Hotel Industry use that does not cause an unreasonable loss of amenity for surrounding residential areas or sensitive uses.*

The proposal to add an additional residential floor above ground level aligns with HOB-S6.1.2. The additional level will increase the approved height of the building by the floor to floor height of 3.1m from 15.8m to 18.9 m. This additional height is considered compatible with existing buildings and the streetscape in the surrounding area as discussed further below.

3.3 Use

The proposed extension to residential use above ground floor level is a Permitted Use under the SAP Use Table. There are no applicable Use Standards.

3.4 Development Standards for Buildings and Works (HOB-S6.7.1)

Building Height (HOB-S6.7.1)

Objective:

That building height:

(a) provides a transition between the Central Business Zone and the lower scale of the surrounding Urban Mixed Use and Inner Residential Zones;;



- (b) does not cause an unreasonable loss of important views from public spaces;
- (c) does not unreasonably overshadow public open space; and
- (d) encourages residential use above ground floor level.

Development Standard	Assessment
<p>A1</p> <p><i>Building height must not be more than:</i></p> <p>(a) 12m; or</p> <p>(b) 15m, if the development provides at least 50% of the floor space above ground level for residential use.</p>	<p>The proposal involves 1758m² of residential floor space on Levels 3,4 and 5. The total floor space of the building above ground floor is 3148m². This equates to approximately 55% of the floor area above ground floor level being in residential use. The permitted height under Clause A1 is therefore 15m.</p> <p>The proposed additional storey exceeds 15m with a proposed maximum height of 18.9m and therefore requires assessment under P1.</p>
<p>P1</p> <p><i>Buildings must make a positive contribution to the streetscape and character of the area, having regard to:</i></p> <p>(a) the topography of the site;</p> <p>(b) preventing unreasonable loss of views within the view lines and view cones shown Figure HOB-S6.2 and on the landscape horizons to Kunanyi / Mt Wellington and the Wellington Range when viewed from public spaces within the Commercial Zone;</p> <p>(c) the height, bulk and form of proposed buildings providing a transition between the Central Business Zone and the lower scale of the surrounding Urban Mixed Use and Inner Residential Zones; (</p> <p>(d) the apparent height when viewed from the adjoining road and public places; and</p> <p>(e) the need to minimise unreasonable impacts on the amenity of public open space from overshadowing.</p>	<p>The proposal is considered to satisfy Performance Criterion P1 for the following reasons:</p> <p><i>a) Topography of the site</i> The site slopes downward from the north-west, such that nearby residential properties in this direction are elevated above the subject site. This existing topography assists in mitigating the visual impact of the proposed building height when viewed from those properties.</p> <p><i>b) Views and view lines</i> The proposed additional storey and associated alterations will not impact views within View Line A1 or View Cones B1 or B2, nor will they result in unreasonable impact on views to Kunanyi / Mt Wellington and the Wellington Range from Brisbane or Argyle Street or other public spaces within the Commercial Zone.</p> <p><i>c) Height, bulk and form of proposed buildings</i> The proposed building height will provide an appropriate transition to the Inner Residential Zone to adjoining to the north and notably:</p> <ul style="list-style-type: none"> • to the five-storey residential apartment building at 34 Patrick Street, located to the north-west and adjoining the rear boundary of the site within the Inner Residential Zone. This building has a relative height approximately 7 metres greater than the proposal, with an existing parapet level of 52.97 m



	<p>AHD, compared to the proposed height of 46.15 m AHD.</p> <ul style="list-style-type: none"> • Other adjoining buildings to the north-west within the Inner Residential Zone, which are higher than the proposed. <p><i>d) Apparent height from public spaces</i> When viewed from Brisbane Street and other adjoining public places, the apparent height of the proposed building, including the additional storey, is considered to provide a positive contribution to the established and emerging streetscape character.</p> <p><i>e) Overshadowing of public open space</i> The submitted shadow diagrams demonstrate that the proposal results in minimal additional overshadowing of Brisbane Street.</p>
<p>A2</p> <p><i>Heritage-adjacent buildings set back less than 15m from a frontage must:</i></p> <p><i>(a) not be more than 1 storey or 4m (whichever is the lesser) higher than the façade height of a heritage building on an adjacent heritage place (refer to Figures HOB-S6.3 and HOB-S6.4); and</i></p> <p><i>(b) if the development is between two heritage places (refer to HOB-S6.4), not be more than the façade height of the highest heritage building on an adjacent heritage place.</i></p>	<p>The proposal for an additional storey on the approved development is more than a storey or 4m higher than the adjacent heritage listed place of the Christian Science Church building at 67 Brisbane Street and is therefore to be assessed under P2.</p>
<p>P2</p> <p><i>Heritage-adjacent buildings set back less than 15m from a frontage must:</i></p> <p><i>(a) not unreasonably dominate existing heritage buildings on an adjacent heritage place; and</i></p> <p><i>(b) be compatible with and not detract from the historic heritage significance of an adjacent heritage place.</i></p>	<p>The proposal for the additional storey to the approved building is not considered to unreasonably dominate the existing heritage Christian Science building adjacent to the west at 67 Brisbane Street and satisfy P2 as follows:</p> <p><i>a) Not unreasonably dominate the adjacent heritage building</i></p> <p>While the additional storey results in the proposed building exceeding the façade height of the adjacent heritage place, the overall scale and built form of the development will not unreasonably dominate the Christian Science Church building.</p> <p>The church building maintains a strong and distinctive presence within the streetscape due to its architectural form, materials and detailing.</p>



	<p>Importantly, the surrounding locality already contains a number of substantial multi-storey buildings that establish the prevailing urban scale for this part of Brisbane Street and the northern fringe of the CBD. These include:</p> <ul style="list-style-type: none"> • the five-storey residential apartment building at 34 Patrick Street to the north-west; • the five-storey residential building at 40-42 Brisbane Street diagonally opposite the site; and • the seven-storey UTAS student accommodation building at 157 Elizabeth Street nearby. <p>These buildings demonstrate that development of a comparable or greater height already forms part of the established and emerging character of the area. Within this context, the proposed development will sit comfortably within the broader streetscape and will not visually overwhelm the adjacent heritage building.</p> <p>Furthermore, the additional storey forms part of the upper portion of the building and does not alter the established street-level relationship between the proposed development and the church building. The heritage building will continue to retain visual prominence within the streetscape.</p> <p><i>b) Compatibility with the historic heritage significance of the adjacent place</i></p> <p>The proposed development adopts a contemporary architectural design that is clearly distinguishable from the historic fabric of the Christian Science Church building. This approach is consistent with accepted heritage design principles which seek to ensure that new development is identifiable as contemporary while remaining respectful of the heritage context.</p> <p>The design does not obscure or diminish the ability to appreciate the architectural form and historic character of the church building when viewed from Brisbane Street. The proposed building also maintains appropriate spatial separation from the church building and does not interfere with key elements of its heritage significance.</p> <p>Having regard to the above matters, the proposal is considered compatible with the heritage values of the</p>
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adjacent heritage place and will not detract from its historic heritage significance.





Façade Design (HOB-S6.7.2)

Objective:

That building façades promote and maintain high levels of pedestrian interaction, amenity, and safety and are compatible with the streetscape.

Development Standard	Assessment
<p>A1</p> <p><i>New buildings must be designed to satisfy all of the following:</i></p> <p><i>(a) mechanical plant and other service infrastructure, such as heat pumps, air conditioning units, switchboards, hot water units and the like, must be screened from the street and other public places;</i></p> <p><i>(b) roof-top mechanical plant and service infrastructure, including lift structures, must be contained within the roof;</i></p>	<p>The proposal is considered to comply with A1(a), (c) and (d) as follows:</p> <p>a) Mechanical plant and other service infrastructure, including air-conditioning units and associated equipment, will be screened from view from the street and other public places.</p> <p>b) A rooftop plant enclosure is proposed and therefore the proposal requires assessment against Performance Criterion P1.</p> <p>c) No security shutters or grilles are proposed on façades facing the street or other public places.</p>



<p><i>(c) not include security shutters or grilles over windows or doors on a façade facing the frontage or other public places; and</i></p> <p><i>(d) provide external lighting to illuminate external vehicle parking areas and pathways.</i></p>	<p>d) External lighting will be provided to vehicle parking areas and pedestrian pathways to ensure safe access and visibility for users.</p>
<p>P1</p> <p><i>New buildings must be designed to be compatible with the streetscape having regard to:</i></p> <p><i>(a) minimising the visual impact of mechanical plant and other service infrastructure, such as heat pumps, air conditioning units, switchboards, hot water units and the like, when viewed from the street or other public places;</i></p> <p><i>(b) minimising the visual impact of security grilles and shutters and rooftop service infrastructure, including lift structures; and</i></p> <p><i>(c) providing suitable lighting to vehicle parking areas and pathways for the safety and security of users.</i></p>	<p>Where A1 is not met, new buildings must be designed to be compatible with the streetscape having regard to minimising the visual impact of plant and services and providing suitable lighting.</p> <p>The proposal is considered to satisfy the requirements of P1(a)–(c) as follows:</p> <p>The rooftop plant enclosure will screen plant equipment and is sited well back from the Brisbane Street frontage and side walls so that it will not be visible within the streetscape. Accordingly, the visual impact of rooftop service infrastructure will be negligible when viewed from the street or other public places.</p> <p>Mechanical plant and associated infrastructure will also be screened from public view.</p> <p>Vehicle parking areas and pedestrian pathways will be illuminated to provide appropriate safety and security for users.</p> <p>The proposal is therefore considered to satisfy Performance Criterion P1.</p>
<p>A2</p> <p><i>New buildings or alterations to an existing façade must be designed to satisfy all of the following:</i></p> <p><i>(a) provide a pedestrian entrance to the building that is visible from the road or publicly accessible areas of the site;</i></p> <p><i>(b) if for a ground floor level façade facing a frontage:</i></p> <p><i>(i) have not less than 40% of the total surface area consisting of windows or doorways; or</i></p> <p><i>(ii) not reduce the surface area of windows or doorways of an existing building, if the surface area is already less than 40%;</i></p>	<p>The proposal is considered to comply with A2 as follows:</p> <p>a) The main pedestrian entrance will be via the lobby on the southern side of the site and will be clearly visible from Brisbane Street.</p> <p>b) The ground floor frontage includes three of the four bays in the front colonnade as either glazing or the open lobby entry space and will therefore exceed the requirement for at least 40% glazing or openings on the Brisbane Street façade.</p> <p>c) The ground level façade includes large areas of glazing and a curved wall behind a colonnade. This design avoids large expanses of blank wall greater than 30% of the length of the Brisbane Street façade.</p>



<p>(c) if for a ground floor level façade facing a frontage must:</p> <p>(i) not include a single length of blank wall greater than 30% of the length of façade on that frontage; or</p> <p>(ii) not increase the length of an existing blank wall, if already greater than 30% of the length of the façade on that frontage; and</p> <p>(d) provide awnings over a public footpath if existing on the site or on adjoining properties.</p>	<p>d) Pedestrian awnings are not a characteristic feature of the area and do not exist on the site or adjoining properties.</p>
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Residential and Visitor Accommodation amenity (HOB-S6.7.3)

Objective

That buildings for residential or visitor accommodation uses provide reasonable levels of amenity and safety in terms of noise, access to daylight and natural ventilation, open space and storage.

Development Standard	Assessment
<p>A1</p> <p>Residential or Visitor Accommodation development must demonstrate that design elements are able to achieve internal noise levels in accordance with the Australian Standards AS2107:2016 - Acoustics (Recommended design sound levels and reverberation times for building interiors).</p>	<p>Brisbane Street is a relatively low traffic street and it is considered that, similar to the approved development, the proposed additional residential floor of this new build development will be able to achieve internal noise levels to meet this standard.</p>
<p>A2</p> <p>Residential or serviced apartment components of a new building (including external elements such as a balcony, roof garden, terrace or deck) must:</p> <p>(a) if the building includes any single aspect dwellings or single aspect serviced apartments, be set back at least 5m from all side or rear boundaries and other buildings on the same site (refer Figure HOB-S6.5); or</p> <p>(b) if the building includes no single aspect dwellings and no single aspect serviced apartments, have at least two elevations of the building, and all habitable room windows, that are either:</p>	<p>All dwellings on the proposed additional residential level have more than one aspect. The proposal however is to be assessed under P2 in that the 3 apartments adjacent to the north-eastern side boundary do not have at least two elevations that are either facing a frontage or 5m from a side or rear boundary.</p>



<p>(i) set back at least 5m from a side or rear boundary or other building on the same site; or</p> <p>(ii) facing a frontage (refer Figure HOB-S6.6).</p>	
<p>P2</p> <p><i>Residential or serviced apartment components of a new building must be designed to allow for reasonable access to daylight into habitable rooms and private open space, and reasonable opportunity for air circulation and natural ventilation, having regard to:</i></p> <p>(a) proximity to side and rear boundaries;</p> <p>(b) proximity to other buildings on the same site;</p> <p>(c) the height and bulk of other buildings on the same site;</p> <p>(d) the size of any internal courtyard or void;</p> <p>(e) the use of light wells or air shafts;</p> <p>(f) development potential on adjacent sites, considering the zones and codes that apply to those sites; and</p> <p>(g) any assessment by a suitably qualified person.</p>	<p>The proposed layout for the proposed dual aspect dwellings is considered to provide good opportunities for access to daylight and natural ventilation. When assessed against the matters in P2 (a)–(g), the design is considered to comply after having regard to the following:</p> <p><i>(a) Proximity to side and rear boundaries</i></p> <p>The proposal is well-setback from the rear boundary, maintaining openness to the north and north westerly aspects and promoting good solar access to living areas and private open space.</p> <p><i>(b) Proximity to other buildings on the same site</i></p> <p>The building is well separated from existing buildings within the site, ensuring that overshadowing and airflow obstruction are minimal.</p> <p><i>(c) Height and bulk of other buildings on the same site</i></p> <p>The proposed five-storey building is higher than the existing one and two storey structures on site, allowing its upper levels to enjoy unobstructed exposure to daylight and ventilation.</p> <p><i>(d) Size of internal courtyard or void</i></p> <p>The proposal does not rely on internal courtyards or voids to achieve adequate amenity.</p> <p><i>(e) Use of light wells or air shafts</i></p> <p>The relatively open nature of the site allows for good natural light and ventilation without reliance on light wells or air shafts.</p> <p><i>(f) Development potential on adjacent sites</i></p> <p>The upper residential levels will sit above the height of the adjoining Honda building at 47–53 Brisbane Street, also located in the Commercial</p>



	<p>Zone. It is considered that the proposed setback to this shared boundary in addition to the second aspect for all dwellings provides adequate protection from any potential future development impacts consistent with the zone standards.</p> <p><i>(g) Assessment by a suitably qualified person</i></p> <p>Given the building orientation, open site context, and design measures outlined above, it is considered that the proposed dwellings will achieve good access to daylight and ventilation. A specialist daylight and ventilation assessment is therefore not required.</p> <p>Having regard to the matters in (a)–(g), it is considered that the proposed additional residential level will receive good daylight and natural ventilation to habitable rooms and private open spaces. The design is considered to satisfy Performance Criterion P2.</p>
<p>A3</p> <p><i>Every habitable room in a dwelling:</i></p> <p><i>(a) must have at least one external window;</i></p> <p><i>(b) must have at least one external window visible from all points of the room if a living room;</i></p> <p><i>(c) where the only external window in the room is located within a recess, that recess must be:</i></p> <p><i>(i) a minimum width of 1.2m; and</i></p> <p><i>(ii) a maximum depth of 1.5 times the width, measured from the external surface of the external window; and</i></p> <p><i>(d) must have a room depth from an external window of:</i></p> <p><i>(i) not more than 2.5 times the ceiling height; or</i></p>	<p>The proposal complies with A3 in that all bedrooms and living areas have external windows and good access to natural light.</p>



<p><i>(ii) if an open plan layout (where the living, dining and kitchen are combined), not more than 8m.</i></p>	
<p>A4 <i>Private open space must be provided for each dwelling or serviced apartment on a site.</i></p>	<p>Complies.</p>
<p>A5 <i>Private open space for a dwelling or serviced apartment must:</i></p> <p><i>(a) have an area not less than:</i></p> <p><i>(i) 8m² for 1 bedroom dwellings or serviced apartments;</i></p> <p><i>(ii) 10m² for 2 bedroom dwellings or serviced apartments;</i></p> <p><i>(iii) 12m² for 3 or more bedroom dwellings or serviced apartments;</i></p> <p><i>(b) not include plant and equipment such as outdoor components of an air conditioning unit;</i></p> <p><i>(c) unless drying facilities are provided elsewhere on the site, include a clothes drying area of at least 2m² in addition to minimum area in (a) above, that may be in a separate location, and is screened from public view;</i></p> <p><i>(d) have a minimum horizontal dimension of 2m, or 1.5m for a 1 bedroom dwelling or serviced apartment;</i></p> <p><i>(e) where above ground floor level, not be located within 5m of private open space of any other dwelling or serviced apartment in another building (excluding between conjoined terrace-style dwellings or serviced apartments); and</i></p> <p><i>(f) be screened visually and acoustically from mechanical plant and equipment, service structures and lift motor rooms.</i></p>	<p>Each of the proposed 3 bedroom dwellings is provided with a private deck area exceeding 12 m², and each 2 bedroom dwelling is provided with a private deck area exceeding 10 m², satisfying the minimum area requirements of A5(a).</p> <p>The private open space areas do not include plant or equipment such as outdoor air-conditioning units, satisfying A5(b).</p> <p>Each dwelling incorporates an internal laundry and dedicated clothes drying facilities, such that a separate external drying area is not required, satisfying A5(c).</p> <p>The private open space areas are visually and acoustically separated from mechanical plant, service structures, and lift motor rooms, satisfying A5(f).</p> <p>However, the minimum horizontal dimension of the private open space areas associated with the new Level 4 dwellings and Penthouse Apartment 3 (amended) is less than 2 metres, and therefore does not satisfy A5(d).</p> <p>Accordingly, while the proposal satisfies A5(a), (b), (c) and (f), it does not fully satisfy A5(d) and is therefore assessed against Performance Criterion P5.</p>
<p>P5 <i>Private open space for a dwelling or serviced apartment must be capable of meeting the</i></p>	<p>P5 The proposed private open spaces of the new dwellings are considered appropriate for the projected future outdoor recreation needs of</p>



<p><i>projected outdoor recreation requirements of occupants, having regard to:</i></p> <p><i>(a) the size and minimum dimensions of the space, excluding space occupied by plant and equipment such as outdoor components of an air conditioning unit;</i></p> <p><i>(b) the amount of space available for furniture or plantings;</i></p> <p><i>(c) the potential for significant noise intrusion;</i></p> <p><i>(d) proximity and overlooking to the private open space of existing adjacent residential and serviced apartment development;</i></p> <p><i>(e) screening where necessary for privacy that does not unreasonably restrict access to daylight;</i></p> <p><i>(f) screening where necessary for noise and wind protection that does not unreasonably restrict access to daylight;</i></p> <p><i>(g) screening from public view for clothes drying areas; and</i></p> <p><i>(h) any assessment from a suitably qualified person.</i></p>	<p>the occupants having regard to each of the following criterion:</p> <p><i>a) Size and minimum dimensions of the space</i></p> <p>While the minimum horizontal dimension of the private open space areas associated with the Level 4 dwellings and Penthouse Apartment 3 (as amended) is less than 2 metres, each deck provides an area that exceeds the minimum size requirements specified in A5(a), excluding space occupied by plant and equipment. The reduced width does not preclude functional use of the space for outdoor dining or passive recreation purposes typical of innercity apartment living.</p> <p><i>b) Space available for furniture or plantings</i></p> <p>The decks are capable of accommodating outdoor furniture such as small tables, seating, and planter boxes, enabling use for sitting, relaxation, and casual outdoor dining. The configuration and overall area of the decks support practical and meaningful use notwithstanding the reduced minimum dimension.</p> <p><i>c) Potential for significant noise intrusion</i></p> <p>The private open space areas are elevated above street level and are not located in close proximity to major noise-generating activities beyond those typical of an inner-city commercial environment. Any ambient noise experienced is consistent with the established context and is not considered to unreasonably compromise the usability of the spaces for their intended purpose.</p> <p><i>d) Proximity and overlooking</i></p> <p>The decks are appropriately separated from the private open space of existing adjacent residential and serviced apartment development. Given the urban context, surrounding built form, and setbacks incorporated into the design, overlooking impacts are limited and consistent with expectations for a mixed-use development</p>
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	<p>within a Commercial and Central Business zone interface.</p> <p><i>e) Screening for privacy</i></p> <p>Where required, privacy screening is incorporated into the design of the private open space areas. This screening is designed to balance privacy outcomes while maintaining reasonable access to daylight and outlook, ensuring that the spaces remain pleasant and functional.</p> <p><i>f) Screening for noise and wind protection</i></p> <p>The building form, balustrades, and partial enclosure provided by the surrounding structure offer a degree of shelter from wind and noise. Any screening required for amenity purposes does not unreasonably restrict access to daylight or ventilation.</p> <p><i>g) Screening for clothes drying</i></p> <p>Each dwelling includes internal laundry and clothes drying facilities, and no external clothes drying areas are proposed within the private open space. Accordingly, screening from public view for clothes drying is not required.</p> <p><i>h) Assessment from a suitably qualified person</i></p> <p>No specialist assessment has been provided in relation to private open space. However, based on the design response, location, and urban context of the proposal, the private open space areas are considered capable of meeting the reasonable outdoor recreation needs of future occupants.</p>
<p>A6</p> <p><i>Sites with 10 or more dwellings or serviced apartments must provide on-site communal open space on the site that:</i></p> <p><i>(a) is at least 70m², with an additional 2m² for every dwelling or serviced apartment over 10;</i></p> <p><i>(b) if provided in multiple locations has least one single area not less than a minimum of 40m²;</i></p>	<p>The proposed additional floor follows the approved apartment concept with a preference for private open space for each dwelling rather than large communal areas. The proposal therefore does not comply with A6 and is to be assessed under P6.</p>



<p>(c) <i>has a horizontal dimension of not less than 3m;</i></p> <p>(d) <i>includes not less than 20% of the total area for plantings (including food growing), being deep soil planting if at ground level;</i></p> <p>(e) <i>is directly accessible from common entries and pathways;</i></p> <p>(f) <i>screens any communal clothes drying facilities from public view;</i></p> <p>(g) <i>may be above ground floor level, including rooftops;</i></p> <p>(h) <i>is screened visually and acoustically from mechanical plant and equipment, service structures and lift motor rooms;</i></p> <p>(i) <i>does not include vehicle driveways, manoeuvring or hardstand areas; and</i></p> <p>(j) <i>includes no more than 20% of the total area located between 30 degrees East of South and 30 degrees West of South of:</i></p> <p>(i) <i>a building on the site with a height more than 3m; or</i></p> <p>(ii) <i>a side or rear boundary within 5m.</i></p>	
<p>P6</p> <p><i>Sites with 10 or more dwellings or serviced apartments must provide on-site communal open space on the site that provides reasonable amenity and outdoor recreation opportunities for occupants, having regard to:</i></p> <p>(a) <i>the area and dimensions of the space;</i></p> <p>(b) <i>the total number of dwellings or serviced apartments on the site;</i></p> <p>(c) <i>the accessibility of the space;</i></p> <p>(d) <i>the flexibility of the space and opportunities for various forms of recreation;</i></p> <p>(e) <i>the availability and location of common facilities within the space;</i></p> <p>(f) <i>any proposed landscaping;</i></p>	<p>The proposal including a generous private open space area for each apartment is considered to satisfy exception c) of P6.</p>



<p>(g) the provision of gardens, trees and plantings (including food gardens) appropriate in area to the size of the communal area;</p> <p>(h) accessibility to daylight, taking into account the development potential of adjacent sites;</p> <p>(i) the outlook from the space;</p> <p>(j) the level of noise intrusion from external noise sources; and</p> <p>(k) any advice from a suitably qualified person;</p> <p>unless:</p> <p>(a) the dwellings or serviced apartments are located in an existing building where communal open space cannot be reasonably achieved due to site constraints, or impacts on historic heritage significance of a heritage place or local heritage precinct; or</p> <p>(b) open space, accessible by the public, that is of high quality in terms of location, access to sunlight, outlook, facilities, landscaping and accessibility and that can adequately accommodate the needs of occupants, is provided on the site; or</p> <p>(c) private open space is provided for all dwellings or serviced apartments on the site, that provides a reasonable level of amenity in terms of access to sunlight and outlook, and sufficiently caters for flexible outdoor recreation needs including relaxation, entertainment, planting, outdoor dining and children’s play.</p>	
<p>A7</p> <p>Each multiple dwelling must be provided with a dedicated and secure storage space of not less than 6m³ and located externally to the dwelling.</p>	<p>Each of the proposed dwellings will have a 3m² storage room that will have an internal volume greater than 6m³ adjacent to the ground floor foyer to the building. The proposal complies with A7.</p>

Waste Storage and Collection (HOB-S6.7.4)

Objective:

That storage and collection of waste provides for a reasonable level of amenity and safety for surrounding occupants and for traffic, cyclists, pedestrians and other road and footpath users.



Development Standard	Assessment
<p>A1</p> <p><i>Bulk waste bins that are commercially serviced must be provided for sites:</i></p> <p>(a) <i>with more than one commercial tenancy;</i></p> <p>(b) <i>with one commercial tenancy that is greater than 100m²;</i></p> <p>(c) <i>with more than 4 dwellings or visitor accommodation units (or 3 if a mixed use site); and</i></p> <p>(d) <i>with more than 2 dwellings or visitor accommodation units (or 1 if a mixed use site);</i></p> <p><i>unless:</i></p> <p>(i) <i>there are no more than 4 individual bins for kerbside collection at any one time per commercial site;</i></p> <p>(ii) <i>there are no more than 8 individual bins for kerbside collection at any one time per residential or mixed use site; or</i></p> <p>(iii) <i>individual bins are commercially serviced without being placed on the kerbside for collection.</i></p>	<p>The proposal is accompanied by a waste management plan confirming the commercial bulk waste servicing arrangements for the site. The proposal complies with A1.</p>
<p>A2</p> <p><i>An on-site storage area, with an impervious surface (unless for compostables), must be provided for bins that:</i></p> <p>(a) <i>if for individual bins per dwelling, Visitor Accommodation unit or commercial tenancy:</i></p> <p>(i) <i>provides an area for the exclusive use of each dwelling, accommodation unit or tenancy, and is not located between the building and a frontage;</i></p> <p>(ii) <i>is set back not less than 4.5m from a frontage unless within a fully enclosed building;</i></p> <p>(iii) <i>is not less than 5.5m horizontally from any dwelling or accommodation unit unless for bins associated with that dwelling, or within a fully enclosed building; and</i></p>	<p>The design includes separate waste storage areas for the apartments and commercial tenancies on the ground floor and complies with A2.</p>



<p><i>(iv) is screened from the frontage and any dwelling or accommodation unit by a wall with a height of not less than 1.2m above the finished surface level of the storage area.</i></p> <p><i>(b) if for bulk waste bins:</i></p> <p><i>(i) is located on common property;</i></p> <p><i>(ii) includes dedicated areas for storage and management of recycling and compostables;</i></p> <p><i>(iii) is not less than 5.5m from any dwelling or accommodation unit unless within a fully enclosed building;</i></p> <p><i>(iv) is screened from any public road, dwelling or accommodation unit by a wall with a height of not less than 1.8m above the finished surface level of the storage area;</i></p> <p><i>(v) is accessible to each dwelling, accommodation unit or tenancy without the requirement to travel off-site; and</i></p> <p><i>(vi) where the development is mixed use, have separate storage spaces for commercial and residential bins with separate access to each.</i></p>	
<p>A3</p> <p><i>Bulk waste bins must be collected on site by private commercial vehicles, and access to storage areas must:</i></p> <p><i>(a) in terms of the location, sight distance, geometry and gradient of an access, as well as off-street parking, manoeuvring and service area, be designed and constructed to comply with AS2890.2:2018: Parking Facilities - Off-Street Commercial Vehicle Facilities;</i></p> <p><i>(b) ensure the vehicle is located entirely within the site when collecting bins; and</i></p> <p><i>(c) include a dedicated pedestrian walkway, alongside or independent of vehicle access ways.</i></p>	<p>It is proposed that waste bins will be wheeled for collection from the colonnade at the front of the site. Given that waste collection vehicles will be parked on the street for this collection the proposal is to be assessed under P3.</p>
<p>P3</p> <p><i>A waste collection plan demonstrates that arrangements for collecting waste do not compromise the amenity and safety for</i></p>	<p>The proposed waste collection arrangement is discussed in Section 9.10 of the traffic impact assessment.</p>

<p><i>surrounding occupants, vehicular traffic, cyclists, pedestrians and other road and footpath users, having regard to:</i></p> <p><i>(a) the number of bins;</i></p> <p><i>(b) the method of collection;</i></p> <p><i>(c) the time of day of collection;</i></p> <p><i>(d) the frequency of collection;</i></p> <p><i>(e) access for vehicles to bin storage areas, including consideration of gradient, site lines, manoeuvring, direction of vehicle movement and pedestrian access;</i></p> <p><i>(f) distance from vehicle stopping point to bins if not collected on site;</i></p> <p><i>(g) the traffic volume, geometry and gradient of the street; and</i></p> <p><i>(h) the volume of pedestrians using the street.</i></p>	
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4. Codes

4.1 Signs Code

The proposal does not involve any signage to be read from off the site. This code therefore does not apply.

4.2 Parking and Sustainable Transport Code

The requirements of this Code are addressed in the accompanying TIA prepared by Hubble Traffic.

4.3 Road and Railway Assets Code

The access and parking requirements of this Code are assessed in a TIA.

4.4 Historic Heritage Code

As discussed in the accompanying Heritage Impact Assessment, given the Tasmanian Heritage Registration of most of the site, only the following areas of the site (shaded red) are affected by Clause C6.6:



<p><i>Tasmanian Heritage Register listing – ID 2182</i></p>	<p><i>Parts of the site that are subject to the provisions for Local Heritage Places (Source: Praxis HIA)</i></p>

To the extent that the provisions of the Local Historic Heritage Code of the planning scheme applies, the proposal is assessed against the relevant provisions in the accompanying Heritage Impact Assessment.

This proposal for inclusion of an additional storey and other alterations to the approved development does not involve any work within the tree protection zone of the adjacent significant tree.

4.5 Potentially Contaminated Land Code

The proposal is accompanied by the original environmental site assessment prepared by GES that formed part of the approved development. The findings from this investigation confirm that there is currently no risk to Human Health or the Environment as part of the planned works at the site. The proposal is considered to satisfy Clause C14.5.1. P1 providing the recommendations of the ESA are followed.

5. Conclusion

This application seeks approval for alterations and extensions to the mixed-use development previously approved under Planning Permit PLN-HOB-2025-0147, including the addition of one additional residential level and associated design refinements. Importantly, the proposal builds upon an established approval and does not alter the fundamental land use mix, access arrangements, or development intent endorsed by the existing permit.

The proposed additional storey and related amendments have been carefully designed to respond to the site’s topography, surrounding built form, and streetscape context. Assessment against the relevant provisions of the planning scheme demonstrates that the proposal satisfies the applicable Performance Criteria. In particular, the revised building height, private open space arrangements, and proposed built form is considered compatible with the established and emerging character of Brisbane Street and its interface with surrounding Commercial, Central Business, and Inner Residential zones.

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The proposal does not result in unreasonable impacts on public or private amenity, including with respect to views, overshadowing, privacy, or noise. Shadow impacts remain within acceptable limits for an inner-city mixed-use environment, and the design response appropriately balances residential amenity with the expectations of development within a Commercial Zone and the Hobart Commercial Zone Specific Area Plan.

Overall, the proposed alterations and extensions represent a logical and orderly evolution of the approved development. The application is recommended for approval, subject to appropriate conditions including those that applied to the original permit.



Frazer Read

Principal

14 March 2026



Planning Report

57-65 Brisbane Street

Additional Storey for Approved Mixed Use Redevelopment



16 December 2025

19 Mawhera Ave, Sandy Bay Tasmania 7005 **Call** 0400 109 582
Email frazer@allurbanplanning.com.au allurbanplanning.com.au

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1. Introduction

All Urban Planning Pty Ltd has been engaged by Aldanmark to provide a planning assessment of a proposal for alterations and extensions to an approved but not yet constructed mixed use development at 57-65 Brisbane Street under the *Hobart Local Provisions Schedule of the Tasmanian Planning Scheme* (planning scheme).

The proposal involves adding an additional storey to the approved development PLN-HOB-2025-0147 along with some other relatively minor alterations as 'clouded' on the accompanying plans.

1.1 Approved Development

Planning Permit PLN-HOB-2025-0147, issued on 14 August 2025, approved the construction of a new five-storey mixed-use building on the site. The approved development includes a ground-floor general retail tenancy and service facilities, beneath two levels of commercial office space on the first and second floors. The upper two levels (Third and Fourth Floors) are dedicated to residential use, comprising eight multiple dwellings (five 2-bedroom and three 3-bedroom units).

The approved design incorporates 72 on-site car parking spaces including 59 existing external spaces at the rear of the site and 13 enclosed spaces on the second floor of the building. The approval also provides for 26 bicycle spaces and heritage conservation works, specifically the reinstatement of a landscaped curtilage around the existing Sydney Lodge building.

1.2 Site & Surrounds

The site has an area of 4775m² and fronts the northwestern side of Brisbane Street on the northern fringe of the CBD between Nos. 47-53 and 67 Brisbane Street. The site has a comparatively narrow street frontage (approximately 24 metres) which widens internally. The street front is vacant of buildings and comprises of a 146 space carpark, with the buildings at the rear of the site including the c1860 *Sydney Lodge* building and a series of 1960s workshop buildings, separated and surrounded by carparking areas.

Address	CT
65 Brisbane Street	174045/1

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Figure 1– Site Plan (Source: Annotated from theLIST)

There is an existing road easement over the southwestern side of the title as shown in Figure 2 below.

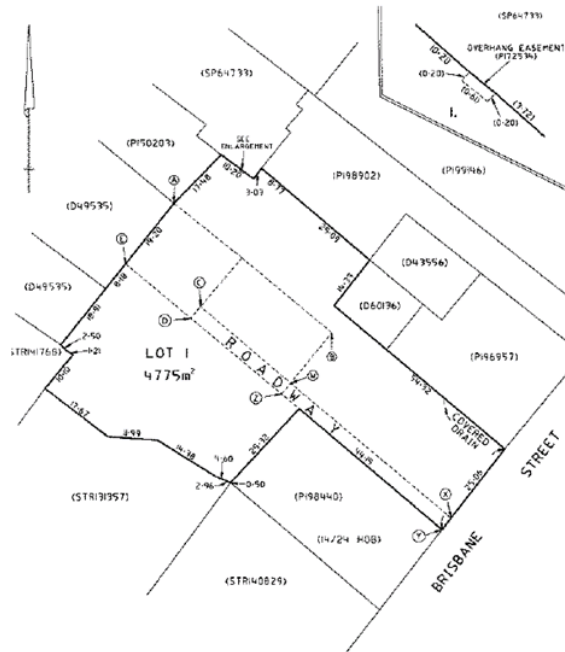


Figure 2 – Folio Plan (Source: Certificate of Title 174045/1)

2. Proposal

The proposal seeks approval for alterations and extensions to the mixed-use development approved under Planning Permit PLN-HOB-2025-0147, including the addition of one extra floor.

The proposal comprises the following floor-by-floor layout:

Ground Floor:

The floor plan remains substantially consistent with the approved development, comprising the residential lobby, bicycle storage, waste facilities, and a General Retail tenancy (246 m²). Minor internal alterations are proposed to the size and orientation of the waste storage room and residential storage areas to service the additional five residential units proposed above.

Level 1:

578 m² office tenancy, with alterations to include a new pedestrian pathway along the south-western side of the building.

Level 2:

427 m² office tenancy and 16 car parking spaces, with the pedestrian walkway from the Ground Floor and Level 1 continuing along the south-western side of the building. The previously approved transformer location has been relocated external to the building, and the number of car parking spaces has increased from 13 to 16.

**Level 3:**

Residential level comprising 5 x two bedroom dwellings. An extended shared courtyard space is proposed, with a corresponding reduction in the floor area of Unit 3.

Level 4:

An additional residential level comprising 5 x two-bedroom dwellings and an enclosed fire stair.

Level 5:

The approved penthouse level, containing 3 x three-bedroom dwellings, is elevated from Level 4 to Level 5. This level also includes a reduced setback to Brisbane Street, the enclosed fire stair and alterations to the private deck areas of Penthouse Apartments 1 and 3.

The introduction of the new Level 4 increases the overall height of the building by one storey, resulting in a six-storey building above street level.

The proposal also includes the reinstatement of a soft landscaped setting for Sydney Lodge, rationalisation of nearby parking including two DDA-compliant car parking spaces, and a new substation located to the south-east of Sydney Lodge.

3. The Planning Scheme

The subject site is zoned Commercial under the planning scheme. Surrounding land is also zoned Commercial to the east, Inner Residential to the north-west, Central Business to the west, and Community Purpose to the south-west, associated with the Christian Science Church at 67 Brisbane Street, as shown in Figure 3.

The mix of surrounding zones reflects the site's transitional urban context and supports the ongoing use of the land for a mixed-use development incorporating commercial and residential activities. In this regard, the proposal is consistent with the established and emerging character of the locality and the intent of the Commercial Zone to accommodate a diverse range of uses in accessible innercity locations.

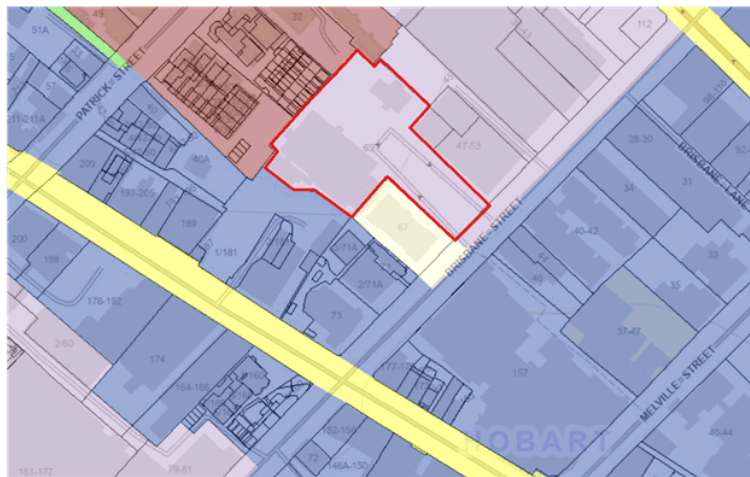


Figure 3 - Zoning plan (Source: annotated plan from theLIST)



Part of the subject site is included in Table C6.1 of the planning scheme as a place of local heritage significance. However, the site is also partly included on the Tasmanian Heritage Register and, by virtue of Clause C6.2.2, the Local Historic Heritage Code applies only to those parts of the site not included on the Register. This is discussed further in the assessment against the Code below.

A Bhutan cypress tree located on the adjoining property to the south-west at 67 Brisbane Street is also listed as a Significant Tree (HOB-C6.5.20) under the Local Historic Heritage Code. The canopy and root zone of this tree extend onto the subject site in the vicinity of the approved vehicle access.

The Tasmanian Planning Commission's decision on the new planning scheme included a direction for substantial modifications under section 35KB of the Act, resulting in the inclusion of the Hobart Commercial Zone Specific Area Plan (HOB-S6.0). It is understood that, pursuant to section 51(3AC) of the Act, this application is to be assessed on the basis that the Specific Area Plan is in effect.

3.1 Zoning

This site is zoned Commercial. The provisions of the Hobart Commercial Zone Specific Area Plan apply in substitution for the provisions of the Commercial Zone in the State Planning Provisions (Clause HOB-S6.2.2).

3.2 HOB-S6.0 Hobart Commercial Zone Specific Area Plan

Purpose of the SAP is:

- HOB-S6.1.1 To provide for a diversity of generally non-residential uses that reflect the transition between land in the Central Business Zone and residential areas.*
- HOB-S6.1.2 To provide for residential use primarily above ground floor level.*
- HOB-S6.1.3 To provide that building height is compatible with existing development in the streetscape and surrounding area.*
- HOB-S6.1.4 To provide for Hotel Industry use that does not cause an unreasonable loss of amenity for surrounding residential areas or sensitive uses.*

The proposal to add an additional residential floor above ground level aligns with HOB-S6.1.2. The additional level will increase the approved height of the building by the floor to floor height of 3.1m from 15.8m to 18.9 m. This additional height is considered compatible with existing buildings and the streetscape in the surrounding area as discussed further below.

3.3 Use

The proposed extension to residential use above ground floor level is a Permitted Use under the SAP Use Table. There are no applicable Use Standards.

3.4 Development Standards for Buildings and Works (HOB-S6.7.1)

Building Height (HOB-S6.7.1)

Objective:

That building height:

- (a) is compatible with existing development in the streetscape and surrounding area;*
- (b) does not unreasonably impact on views from public spaces;*

(c) does not unreasonably overshadow public open space; and

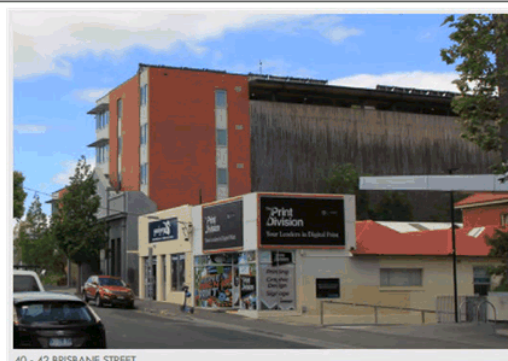
(d) encourages residential use above ground floor level.

Development Standard	Assessment
<p>A1</p> <p>Building height must not be more than:</p> <p>(a) 12m; or</p> <p>(b) 15m, if the development provides at least 50% of the floor space above ground level for residential use.</p>	<p>The proposal involves 1758m² of residential floor space on Levels 3,4 and 5. The total floor space of the building above ground floor is 3148m². This equates to approximately 55% of the floor area above ground floor level being in residential use. The permitted height under Clause A1 is therefore 15m.</p> <p>The proposed additional storey exceeds 15m with a proposed maximum height of 18.9m and therefore requires assessment under P1.</p>
<p>P1</p> <p>Building height must be compatible with the streetscape and character of development existing on established properties in the area, having regard to:</p> <p>(a) the topography of the site;</p> <p>(b) preventing unreasonable loss of views within the view lines and view cones in Figure HOB-S6. 1 and on the landscape horizons to Kunanyi / Mt Wellington and the Wellington Range from public spaces within the Commercial Zone;</p> <p>(c) the height, bulk and form of existing buildings on the site and surrounding area;</p> <p>(d) the height, bulk and form of proposed buildings;</p> <p>(e) the apparent height when viewed from the adjoining road and public places;</p> <p>(f) the need to minimise unreasonable impacts on the amenity of public open space from overshadowing.</p>	<p>The proposal is considered to satisfy Performance Criterion P1 for the following reasons:</p> <p>a) <i>Topography of the site</i> The site slopes downward from the north-west, such that nearby residential properties in this direction are elevated above the subject site. This existing topography assists in mitigating the visual impact of the proposed building height when viewed from those properties.</p> <p>b) <i>Views and view lines</i> The proposed additional storey and associated alterations will not impact views within View Line A1 or View Cones B1 or B2, nor will they result in unreasonable impact on views to Kunanyi / Mt Wellington and the Wellington Range from Brisbane or Argyle Street or other public spaces within the Commercial Zone.</p> <p>c) <i>Height, bulk and form of existing buildings</i> The proposed building height is compatible with the height, bulk and form of existing development on the site and in the surrounding area, including:</p> <ul style="list-style-type: none"> The five-storey residential apartment building at 34 Patrick Street, located to the north-west and adjoining the rear boundary of the site within the Inner Residential Zone. This building has a relative height approximately 7 metres greater than the proposal, with an existing parapet level of 52.97 m AHD,



	<p>compared to the proposed height of 46.15 m AHD.</p> <ul style="list-style-type: none"> • The five-storey residential apartment building diagonally opposite the site at 40–42 Brisbane Street. • The seven-storey UTAS student accommodation podium building at 157 Elizabeth Street downgrade from the proposal with a parapet height of 45m AHD. • Other adjoining buildings to the north-west within the Inner Residential Zone, which are higher than the proposed. <p><i>d) Height, bulk and form of the proposed building</i> The proposal is located at the interface between the Central Business Zone and Commercial Zone and has been designed and sited with appropriate separation distances and setbacks to adjoining sensitive buildings, including the Sydney Lodge heritage building and the Christian Science Church at 67 Brisbane Street.</p> <p>A transitional reduction in height to the adjoining commercial building at 47–53 Brisbane Street (Honda) is not considered necessary, having regard to its commercial zoning and potential for future redevelopment.</p> <p><i>e) Apparent height from public spaces</i> When viewed from Brisbane Street and other adjoining public places, the apparent height of the proposed building, including the additional storey, is not considered incompatible with the established and emerging streetscape character.</p> <p><i>f) Overshadowing of public open space</i> The submitted shadow diagrams demonstrate that the proposal results in minimal additional overshadowing of Brisbane Street.</p>
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Residential and Visitor Accommodation amenity (HOB-S6.7.2)

Objective

That buildings for residential or visitor accommodation uses provide reasonable levels of amenity and safety in terms of noise, access to daylight and natural ventilation, open space and storage.

Development Standard	Assessment
<p>A1</p> <p><i>Residential or Visitor Accommodation development must demonstrate that design elements are able to achieve internal noise levels in accordance with the Australian Standards AS2107:2016 - Acoustics (Recommended design sound levels and reverberation times for building interiors).</i></p>	<p>Brisbane Street is a relatively low traffic street and it is considered that, similar to the approved development, the proposed additional residential floor of this new build development will be able to achieve internal noise levels to meet this standard.</p>
<p>A2</p> <p><i>Residential or serviced apartment components of a new building (including external elements such as a balcony, roof garden, terrace or deck) must:</i></p>	<p>All dwellings on the proposed additional residential level have more than one aspect. The proposal however is to be assessed under P2 in that the 3 apartments adjacent to the north-eastern side boundary do not have at</p>



<p>(a) if the building includes any single aspect dwellings or single aspect serviced apartments, be set back at least 5m from all side or rear boundaries and other buildings on the same site (refer Figure HOB-S6.2); or</p> <p>(b) if the building includes no single aspect dwellings and no single aspect serviced apartments, have at least two elevations of the building, and all habitable room windows, that are either:</p> <p>(i) set back at least 5m from a side or rear boundary or other building on the same site; or</p> <p>(ii) facing a frontage (refer Figure HOB-S6.3).</p>	<p>least two elevations that are either facing a frontage or 5m from a side or rear boundary.</p>
<p>P2</p> <p>Residential or serviced apartment components of a new building must be designed to allow for reasonable access to daylight into habitable rooms and private open space, and reasonable opportunity for air circulation and natural ventilation, having regard to:</p> <p>(a) proximity to side and rear boundaries;</p> <p>(b) proximity to other buildings on the same site;</p> <p>(c) the height and bulk of other buildings on the same site;</p> <p>(d) the size of any internal courtyard or void;</p> <p>(e) the use of light wells or air shafts;</p> <p>(f) development potential on adjacent sites, considering the zones and codes that apply to those sites; and</p> <p>(g) any assessment by a suitably qualified person.</p>	<p>The proposed layout for the proposed dual aspect dwellings is considered to provide good opportunities for access to daylight and natural ventilation. When assessed against the matters in P2 (a)–(g), the design is considered to comply after having regard to the following:</p> <p>(a) Proximity to side and rear boundaries</p> <p>The proposal is well-setback from the rear boundary, maintaining openness to the north and north westerly aspects and promoting good solar access to living areas and private open space.</p> <p>(b) Proximity to other buildings on the same site</p> <p>The building is well separated from existing buildings within the site, ensuring that overshadowing and airflow obstruction are minimal.</p> <p>(c) Height and bulk of other buildings on the same site</p> <p>The proposed five-storey building is higher than the existing one and two storey structures on site, allowing its upper levels to enjoy unobstructed exposure to daylight and ventilation.</p> <p>(d) Size of internal courtyard or void</p>



	<p>The proposal does not rely on internal courtyards or voids to achieve adequate amenity.</p> <p><i>(e) Use of light wells or air shafts</i></p> <p>The relatively open nature of the site allows for good natural light and ventilation without reliance on light wells or air shafts.</p> <p><i>(f) Development potential on adjacent sites</i></p> <p>The upper residential levels will sit above the height of the adjoining Honda building at 47–53 Brisbane Street, also located in the Commercial Zone. It is considered that the proposed setback to this shared boundary in addition to the second aspect for all dwellings provides adequate protection from any potential future development impacts consistent with the zone standards.</p> <p><i>(g) Assessment by a suitably qualified person</i></p> <p>Given the building orientation, open site context, and design measures outlined above, it is considered that the proposed dwellings will achieve good access to daylight and ventilation. A specialist daylight and ventilation assessment is therefore not required.</p> <p>Having regard to the matters in (a)–(g), it is considered that the proposed additional residential level will receive good daylight and natural ventilation to habitable rooms and private open spaces. The design is considered to satisfy Performance Criterion P2.</p>
<p>A3</p> <p><i>Every habitable room in a dwelling:</i></p> <p><i>(a) must have at least one external window;</i></p> <p><i>(b) must have at least one external window visible from all points of the room if a living room;</i></p> <p><i>(c) where the only external window in the room is located within a recess, that recess must be:</i></p>	<p>The proposal complies with A3 in that all bedrooms and living areas have external windows and good access to natural light.</p>



<p>(i) a minimum width of 1.2m; and</p> <p>(ii) a maximum depth of 1.5 times the width, measured from the external surface of the external window; and</p> <p>(d) must have a room depth from an external window of:</p> <p>(i) not more than 2.5 times the ceiling height; or</p> <p>(ii) if an open plan layout (where the living, dining and kitchen are combined), not more than 8m.</p>	
<p>A4</p> <p>Private open space must be provided for each dwelling or serviced apartment on a site.</p>	<p>Complies.</p>
<p>A5</p> <p>Private open space for a dwelling or serviced apartment must:</p> <p>(a) have an area not less than:</p> <p>(i) 8m² for 1 bedroom dwellings or serviced apartments;</p> <p>(ii) 10m² for 2 bedroom dwellings or serviced apartments;</p> <p>(iii) 12m² for 3 or more bedroom dwellings or serviced apartments;</p> <p>(b) not include plant and equipment such as outdoor components of an air conditioning unit;</p> <p>(c) unless drying facilities are provided elsewhere on the site, include a clothes drying area of at least 2m² in addition to minimum area in (a) above, that may be in a separate location, and is screened from public view;</p> <p>(d) have a minimum horizontal dimension of 2m, or 1.5m for a 1 bedroom dwelling or serviced apartment;</p> <p>(e) where above ground floor level, not be located within 5m of private open space of any other dwelling or serviced apartment in another</p>	<p>Each of the proposed 3 bedroom dwellings is provided with a private deck area exceeding 12 m², and each 2 bedroom dwelling is provided with a private deck area exceeding 10 m², satisfying the minimum area requirements of A5(a).</p> <p>The private open space areas do not include plant or equipment such as outdoor air-conditioning units, satisfying A5(b).</p> <p>Each dwelling incorporates an internal laundry and dedicated clothes drying facilities, such that a separate external drying area is not required, satisfying A5(c).</p> <p>The private open space areas are visually and acoustically separated from mechanical plant, service structures, and lift motor rooms, satisfying A5(f).</p> <p>However, the minimum horizontal dimension of the private open space areas associated with the new Level 4 dwellings and Penthouse Apartment 3 (amended) is less than 2 metres, and therefore does not satisfy A5(d).</p> <p>Accordingly, while the proposal satisfies A5(a), (b), (c) and (f), it does not fully satisfy A5(d) and is therefore assessed against Performance Criterion P5.</p>



<p><i>building (excluding between conjoined terrace-style dwellings or serviced apartments); and</i></p> <p><i>(f) be screened visually and acoustically from mechanical plant and equipment, service structures and lift motor rooms.</i></p>	
<p>P5</p> <p><i>Private open space for a dwelling or serviced apartment must be capable of meeting the projected outdoor recreation requirements of occupants, having regard to:</i></p> <p><i>(a) the size and minimum dimensions of the space, excluding space occupied by plant and equipment such as outdoor components of an air conditioning unit;</i></p> <p><i>(b) the amount of space available for furniture or plantings;</i></p> <p><i>(c) the potential for significant noise intrusion;</i></p> <p><i>(d) proximity and overlooking to the private open space of existing adjacent residential and serviced apartment development;</i></p> <p><i>(e) screening where necessary for privacy that does not unreasonably restrict access to daylight;</i></p> <p><i>(f) screening where necessary for noise and wind protection that does not unreasonably restrict access to daylight;</i></p> <p><i>(g) screening from public view for clothes drying areas; and</i></p> <p><i>(h) any assessment from a suitably qualified person.</i></p>	<p>P5</p> <p>The proposed private open spaces of the new dwellings are considered appropriate for the projected future outdoor recreation needs of the occupants having regard to each of the following criterion:</p> <p><i>a) Size and minimum dimensions of the space</i></p> <p>While the minimum horizontal dimension of the private open space areas associated with the Level 4 dwellings and Penthouse Apartment 3 (as amended) is less than 2 metres, each deck provides an area that exceeds the minimum size requirements specified in A5(a), excluding space occupied by plant and equipment. The reduced width does not preclude functional use of the space for outdoor dining or passive recreation purposes typical of innercity apartment living.</p> <p><i>b) Space available for furniture or plantings</i></p> <p>The decks are capable of accommodating outdoor furniture such as small tables, seating, and planter boxes, enabling use for sitting, relaxation, and casual outdoor dining. The configuration and overall area of the decks support practical and meaningful use notwithstanding the reduced minimum dimension.</p> <p><i>c) Potential for significant noise intrusion</i></p> <p>The private open space areas are elevated above street level and are not located in close proximity to major noise-generating activities beyond those typical of an inner-city commercial environment. Any ambient noise experienced is consistent with the established context and is not considered to unreasonably compromise the usability of the spaces for their intended purpose.</p>



	<p><i>d) Proximity and overlooking</i></p> <p>The decks are appropriately separated from the private open space of existing adjacent residential and serviced apartment development. Given the urban context, surrounding built form, and setbacks incorporated into the design, overlooking impacts are limited and consistent with expectations for a mixed-use development within a Commercial and Central Business zone interface.</p> <p><i>e) Screening for privacy</i></p> <p>Where required, privacy screening is incorporated into the design of the private open space areas. This screening is designed to balance privacy outcomes while maintaining reasonable access to daylight and outlook, ensuring that the spaces remain pleasant and functional.</p> <p><i>f) Screening for noise and wind protection</i></p> <p>The building form, balustrades, and partial enclosure provided by the surrounding structure offer a degree of shelter from wind and noise. Any screening required for amenity purposes does not unreasonably restrict access to daylight or ventilation.</p> <p><i>g) Screening for clothes drying</i></p> <p>Each dwelling includes internal laundry and clothes drying facilities, and no external clothes drying areas are proposed within the private open space. Accordingly, screening from public view for clothes drying is not required.</p> <p><i>h) Assessment from a suitably qualified person</i></p> <p>No specialist assessment has been provided in relation to private open space. However, based on the design response, location, and urban context of the proposal, the private open space areas are considered capable of meeting the reasonable outdoor recreation needs of future occupants.</p>
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<p>A6</p> <p><i>Sites with 10 or more dwellings or serviced apartments must provide on-site communal open space on the site that:</i></p> <p>(a) <i>is at least 70m², with an additional 2m² for every dwelling or serviced apartment over 10;</i></p> <p>(b) <i>if provided in multiple locations has least one single area not less than a minimum of 40m²;</i></p> <p>(c) <i>has a horizontal dimension of not less than 3m;</i></p> <p>(d) <i>includes not less than 20% of the total area for plantings (including food growing), being deep soil planting if at ground level;</i></p> <p>(e) <i>is directly accessible from common entries and pathways;</i></p> <p>(f) <i>screens any communal clothes drying facilities from public view;</i></p> <p>(g) <i>may be above ground floor level, including rooftops;</i></p> <p>(h) <i>is screened visually and acoustically from mechanical plant and equipment, service structures and lift motor rooms;</i></p> <p>(i) <i>does not include vehicle driveways, manoeuvring or hardstand areas; and</i></p> <p>(j) <i>includes no more than 20% of the total area located between 30 degrees East of South and 30 degrees West of South of:</i></p> <p>(i) <i>a building on the site with a height more than 3m; or</i></p> <p>(ii) <i>a side or rear boundary within 5m.</i></p>	<p>The proposed additional floor follows the approved apartment concept with a preference for private open space for each dwelling rather than large communal areas. The proposal therefore does not comply with A6 and is to be assessed under P6.</p>
<p>P6</p> <p><i>Sites with 10 or more dwellings or serviced apartments must provide on-site communal open space on the site that provides reasonable amenity and outdoor recreation opportunities for occupants, having regard to:</i></p> <p>(a) <i>the area and dimensions of the space;</i></p>	<p>The proposal including a generous private open space area for each apartment is considered to satisfy exception c) of P6.</p>



<p>(b) the total number of dwellings or serviced apartments on the site;</p> <p>(c) the accessibility of the space;</p> <p>(d) the flexibility of the space and opportunities for various forms of recreation;</p> <p>(e) the availability and location of common facilities within the space;</p> <p>(f) any proposed landscaping;</p> <p>(g) the provision of gardens, trees and plantings (including food gardens) appropriate in area to the size of the communal area;</p> <p>(h) accessibility to daylight, taking into account the development potential of adjacent sites;</p> <p>(i) the outlook from the space;</p> <p>(j) the level of noise intrusion from external noise sources; and</p> <p>(k) any advice from a suitably qualified person;</p> <p>unless:</p> <p>(a) the dwellings or serviced apartments are located in an existing building where communal open space cannot be reasonably achieved due to site constraints, or impacts on historic heritage significance of a heritage place or local heritage precinct; or</p> <p>(b) open space, accessible by the public, that is of high quality in terms of location, access to sunlight, outlook, facilities, landscaping and accessibility and that can adequately accommodate the needs of occupants, is provided on the site; or</p> <p>(c) private open space is provided for all dwellings or serviced apartments on the site, that provides a reasonable level of amenity in terms of access to sunlight and outlook, and sufficiently caters for flexible outdoor recreation needs including relaxation, entertainment, planting, outdoor dining and children’s play.</p>	
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<p>A7</p> <p><i>Each multiple dwelling must be provided with a dedicated and secure storage space of not less than 6m³ and located externally to the dwelling.</i></p>	<p>Each of the proposed dwellings will have a 3m² storage room that will have an internal volume greater than 6m³ adjacent to the ground floor foyer to the building. The proposal complies with A7.</p>
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Waste Storage and Collection (23.4.9)

Objective:

That storage and collection of waste provides for a reasonable level of amenity and safety for surrounding occupants and for traffic, cyclists, pedestrians and other road and footpath users.

Development Standard	Assessment
<p>A1</p> <p><i>Bulk waste bins that are commercially serviced must be provided for sites:</i></p> <p><i>(a) with more than one commercial tenancy;</i></p> <p><i>(b) with one commercial tenancy that is greater than 100m²;</i></p> <p><i>(c) with more than 4 dwellings or visitor accommodation units (or 3 if a mixed use site); and</i></p> <p><i>(d) with more than 2 dwellings or visitor accommodation units (or 1 if a mixed use site);</i></p> <p><i>unless:</i></p> <p><i>(i) there are no more than 4 individual bins for kerbside collection at any one time per commercial site;</i></p> <p><i>(ii) there are no more than 8 individual bins for kerbside collection at any one time per residential or mixed use site; or</i></p> <p><i>(iii) individual bins are commercially serviced without being placed on the kerbside for collection.</i></p>	<p>The proposal is accompanied by a waste management plan confirming the commercial bulk waste servicing arrangements for the site. The proposal complies with A1.</p>
<p>A2</p> <p><i>An on-site storage area, with an impervious surface (unless for compostables), must be provided for bins that:</i></p> <p><i>(a) if for individual bins per dwelling, Visitor Accommodation unit or commercial tenancy:</i></p>	<p>The design includes separate waste storage areas for the apartments and commercial tenancies on the ground floor and complies with A2.</p>



<p>(i) provides an area for the exclusive use of each dwelling, accommodation unit or tenancy, and is not located between the building and a frontage;</p> <p>(ii) is set back not less than 4.5m from a frontage unless within a fully enclosed building;</p> <p>(iii) is not less than 5.5m horizontally from any dwelling or accommodation unit unless for bins associated with that dwelling, or within a fully enclosed building; and</p> <p>(iv) is screened from the frontage and any dwelling or accommodation unit by a wall with a height of not less than 1.2m above the finished surface level of the storage area.</p> <p>(b) if for bulk waste bins:</p> <p>(i) is located on common property;</p> <p>(ii) includes dedicated areas for storage and management of recycling and compostables;</p> <p>(iii) is not less than 5.5m from any dwelling or accommodation unit unless within a fully enclosed building;</p> <p>(iv) is screened from any public road, dwelling or accommodation unit by a wall with a height of not less than 1.8m above the finished surface level of the storage area;</p> <p>(v) is accessible to each dwelling, accommodation unit or tenancy without the requirement to travel off-site; and</p> <p>(vi) where the development is mixed use, have separate storage spaces for commercial and residential bins with separate access to each.</p>	
<p>A3</p> <p>Bulk waste bins must be collected on site by private commercial vehicles, and access to storage areas must:</p> <p>(a) in terms of the location, sight distance, geometry and gradient of an access, as well as off-street parking, manoeuvring and service area, be designed and constructed to comply with</p>	<p>It is proposed that waste bins will be wheeled for collection from the colonnade at the front of the site. Given that waste collection vehicles will be parked on the street for this collection the proposal is to be assessed under P3.</p>



<p><i>AS2890.2:2018: Parking Facilities - Off-Street Commercial Vehicle Facilities;</i></p> <p><i>(b) ensure the vehicle is located entirely within the site when collecting bins; and</i></p> <p><i>(c) include a dedicated pedestrian walkway, alongside or independent of vehicle access ways.</i></p>	
<p><i>P3</i></p> <p><i>A waste collection plan demonstrates that arrangements for collecting waste do not compromise the amenity and safety for surrounding occupants, vehicular traffic, cyclists, pedestrians and other road and footpath users, having regard to:</i></p> <p><i>(a) the number of bins;</i></p> <p><i>(b) the method of collection;</i></p> <p><i>(c) the time of day of collection;</i></p> <p><i>(d) the frequency of collection;</i></p> <p><i>(e) access for vehicles to bin storage areas, including consideration of gradient, site lines, manoeuvring, direction of vehicle movement and pedestrian access;</i></p> <p><i>(f) distance from vehicle stopping point to bins if not collected on site;</i></p> <p><i>(g) the traffic volume, geometry and gradient of the street; and</i></p> <p><i>(h) the volume of pedestrians using the street.</i></p>	<p>The proposed waste collection arrangement is discussed in Section 9.10 of the traffic impact assessment.</p>

4. Codes

4.1 Signs Code

The proposal does not involve any signage to be read from off the site. This code therefore does not apply.

4.2 Parking and Sustainable Transport Code

The requirements of this Code are addressed in the accompanying TIA prepared by Hubble Traffic.

4.3 Road and Railway Assets Code

The access and parking requirements of this Code are assessed in a TIA.



4.4 Historic Heritage Code

As discussed in the accompanying Heritage Impact Assessment, given the Tasmanian Heritage Registration of most of the site, only the following areas of the site (shaded red) are affected by Clause C6.6:

<p><i>Tasmanian Heritage Register listing – ID 2182</i></p>	<p><i>Parts of the site that are subject to the provisions for Local Heritage Places (Source: Praxis HIA)</i></p>

To the extent that the provisions of the Local Historic Heritage Code of the planning scheme applies, the proposal is assessed against the relevant provisions in the accompanying Heritage Impact Assessment.

This proposal for inclusion of an additional storey and other alterations to the approved development does not involve any work within the tree protection zone of the adjacent significant tree.

4.5 Potentially Contaminated Land Code

The proposal is accompanied by the original environmental site assessment prepared by GES that formed part of the approved development. The findings from this investigation confirm that there is currently no risk to Human Health or the Environment as part of the planned works at the site. The proposal is considered to satisfy Clause C14.5.1. P1 providing the recommendations of the ESA are followed.

5. Conclusion

This application seeks approval for alterations and extensions to the mixed-use development previously approved under Planning Permit PLN-HOB-2025-0147, including the addition of one additional residential level and associated design refinements. Importantly, the proposal builds upon an established approval and does not alter the fundamental land use mix, access arrangements, or development intent endorsed by the existing permit.

The proposed additional storey and related amendments have been carefully designed to respond to the site’s topography, surrounding built form, and streetscape context. Assessment against the relevant provisions of the planning scheme demonstrates that the proposal satisfies the applicable Performance



Criteria. In particular, the revised building height, private open space arrangements, and proposed built form is considered compatible with the established and emerging character of Brisbane Street and its interface with surrounding Commercial, Central Business, and Inner Residential zones.

The proposal does not result in unreasonable impacts on public or private amenity, including with respect to views, overshadowing, privacy, or noise. Shadow impacts remain within acceptable limits for an inner-city mixed-use environment, and the design response appropriately balances residential amenity with the expectations of development within a Commercial Zone and the Hobart Commercial Zone Specific Area Plan.

Overall, the proposed alterations and extensions represent a logical and orderly evolution of the approved development. The application is recommended for approval, subject to appropriate conditions including those that applied to the original permit.

A handwritten signature in blue ink, appearing to read "Frazer Read".

Frazer Read

Principal

16 December 2025



SUBMISSION TO PLANNING AUTHORITY NOTICE - SPAN

Reference
TASWASPAN-HOB-2025-0264

Application Reference
PLN-HOB-2025-0658

Address
65 BRISBANE ST HOBART TAS 7000

Titles
174045/1

Applicant

Name	Email	Phone	Address	Involvement
Mark Gardner	business@aldanmark.com.au	0362348666	199 Macquarie Street, Hobart, Tasmania, Australia, 7000	Applicant

Owner

Name	Email Address	Address
Mark Gardner	business@aldanmark.com.au	199 Macquarie Street, Hobart, Tasmania, Australia, 7000
Brisbane Street Apartments Pty Ltd	mgardner@aldanmark.com.au	17 Bicheno Street, Clifton Beach, 7020, Australia

Relevant Property Title/s

Selected Titles Total Area: 0m²
174045/1

Council Description of Use and/or Development

Council Proposed Use or Development Description

Alterations, Extension to Previously Approved Development (PLN-HOB-2024-0147) and Thirteen Multiple Dwellings (Eight Approved and Five Proposed)

Response

Pursuant to the Water and Sewerage Industry Act 2008 (TAS)

Section 56P(1) - TasWater imposes the following conditions on the permit for this application:

Conditions

#	Condition	Origin
1	<p>Connections, Metering, Backflow - connections</p> <p>A suitably sized water supply with metered connections and sewerage system and connections to the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit.</p> <p>Advice: TasWater will not accept direct fire boosting from the network unless it can be demonstrated that the periodic testing of the system will not have a significant negative effect on our network and the minimum service requirements of other customers serviced by the network. To this end break tanks may be required with the rate of flow into the break tank controlled so that peak flows to fill the tank do not also cause negative effect on the network.</p>	Water and Sewerage Industry Act 2008
2	<p>Connections, Metering, Backflow - Removal/installation</p> <p>Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost.</p>	Water and Sewerage Industry Act 2008
3	<p>Connections, Metering, Backflow - Use</p> <p>Prior to commencing construction/use of the development, any water connection utilised for construction/the development must have a backflow prevention device and water meter installed, to the satisfaction of TasWater.</p>	Water and Sewerage Industry Act 2008

4	Condition (Water) Prior to TasWater issuing a Consent to Register a Legal Document /Certificate(s) for Certifiable Work (Building) and/or (Plumbing), the applicant or landowner as the case may be, must pay a developer charge totalling \$4,143.00 to TasWater for water infrastructure for 2,358 additional Equivalent Tenements, indexed by the Consumer Price Index All groups (Hobart) from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater.	Water and Sewerage Industry Act 2008
5	Condition (Sewerage) Prior to TasWater issuing a Consent to Register a Legal Document /Certificate(s) for Certifiable Work (Building) and/or (Plumbing), the applicant or landowner as the case may be, must pay a developer charge totalling \$6,223.30 to TasWater for sewerage infrastructure for 3,542 additional Equivalent Tenements, indexed by the Consumer Price Index All groups (Hobart) from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater.	Water and Sewerage Industry Act 2008
6	Fees payment required The applicant or landowner as the case may be, must pay a development assessment fee of \$417.63, to TasWater, as approved by the Economic Regulator and the fee will be indexed, until the date paid to TasWater. The payment is required within 30 days of the issue of an invoice by TasWater.	Water and Sewerage Industry Act 2008
7	Declaration The drawings/documents and conditions stated above constitute TasWater's Submission to Planning Authority Notice.	Water and Sewerage Industry Act 2008
8	Advice - General For information on TasWater development standards, please visit https://www.taswater.com.au/building-and-development/technical-standards For application forms please visit https://www.taswater.com.au/building-and-development/development-application-form	
9	Advice - Service Locations Please note that the developer is responsible for arranging to locate the existing TasWater infrastructure and clearly showing it on the drawings. Existing TasWater infrastructure may be located by a surveyor and/or a private contractor engaged at the developers cost to locate the infrastructure. A copy of the GIS is included in email with this notice and should aid in updating of the documentation. The location of this infrastructure as shown on the GIS is indicative only. <ul style="list-style-type: none">• A permit is required to work within TasWater's easements or in the vicinity of its infrastructure. Further information can be obtained from TasWater• TasWater has listed a number of service providers who can provide asset detection and location services should you require it. Visit www.taswater.com.au/Development/Service-location for a list of companies• TasWater will locate residential water stop taps free of charge• Sewer drainage plans or Inspection Openings (IO) for residential properties are available from your local council.	Water and Sewerage Industry Act 2008
10	Advice - Boundary Trap Area The proposed development is within a boundary trap area and the developer will need to provide a boundary trap that prevents noxious gases or persistent odours back venting into the property's sanitary drain. The boundary trap is to be contained within the property boundaries and the property owner remains responsible for the ownership, operation and maintenance of the boundary trap.	Water and Sewerage Industry Act 2008
11	Water Submetering As of July 1 2022, TasWater's Sub-Metering Policy no longer permits TasWater sub-meters to be installed for new developments. Please ensure plans submitted with the application for Certificate(s) for Certifiable Work (Building and/or Plumbing) reflect this. For clarity, TasWater does not object to private sub-metering arrangements. Further information is available on our website (www.taswater.com.au) within our Sub-Metering Policy and Water Metering Guidelines.	Water and Sewerage Industry Act 2008
12	Advice - Developer Charges For information on Developer Charges please visit the following webpage - https://www.taswater.com.au/building-and-development/developer-charges	

Referral Documents

Version	Document Date	Document Type	Description	Prepared By
1	16 Dec 2025	Architectural Plans	Architectural Plans	Lara Maesele
1	16 Dec 2025	Planning Assessment Report	Planning Report	Frazer Read
1	1 Oct 2025	Accessibility Report	Traffic Impact Assessment	Peter Hubble
1	17 Dec 2025	General	Application Form (redacted)	Sandie Wickham
1	9 Jan 2025	Geotechnical Report	Environmental Assessment Report	GES
1	15 Dec 2025	Planning Assessment Report	Waste Management Report	Mark Gardner

1	17 Sept 2025	Engineering Drawings	Civil Engineering Drawings	Mark Gardner
1	2 Nov 2025	Heritage Report	Historic Heritage and Archaeological Management Strategy & Heritage/Archaeological Impact Assessment	Brad Williams / Frazer Read
1	16 Dec 2025	Architectural Plans	Architectural Planning Submission	Lara Maeseele

Schedule of Drawings/Documents

Schedule of Drawings/Documents

Prepared By	Drawings / Document No.	Revision No.	Date of Issue
Aldanmark	Proposed Site Plan	C	10/04/2025
Lara Maeseele	Site Plans	B	16/12/2025

TasWater Details

Officer Name Al Cole	Officer Title Senior Assessment Officer	Officer Contact Number 0439605108
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Submitted on 12/01/2026

Form published: 08/10/2025 08:54



Tasmanian Heritage Council
GPO Box 618 Hobart Tasmania 7000
Tel: 1300 850 332
enquiries@heritage.tas.gov.au
www.heritage.tas.gov.au

PLANNING REF: PLN-HOB-2025-0658
THC WORKS REF: 8796
REGISTERED PLACE NO: 2182
APPLICANT: Mark Gardner
DATE THC RECEIVED: 18 December 2025
DATE OF THIS NOTICE: 00 Month 2024

NOTICE OF INTEREST AND REQUEST FOR ADDITIONAL INFORMATION

(Historic Cultural Heritage Act 1995)

The Place: Sydney Lodge, 65 Brisbane St, Hobart
Proposed Works: Additional Storey

Under s36(3)(b) of the *Historic Cultural Heritage Act 1995* (the Act) the Tasmanian Heritage Council provides notice that it wishes to be involved in determining the discretionary permit application.

Under s36(4) of the Act, the Tasmanian Heritage Council gives notification that it requires the following additional information:

1. Please provide details of the size and external colour of the proposed substation.
2. Please provided details of the facing materials to be used for the car park retaining wall details.
3. Please provide detail of the proposed landscaping adjacent to the heritage building, including any screening (fences, planting, etc.) to the substation and surface materials for footpaths.

Please submit the above information to the planning authority who will forward it to the Heritage Council.

Please note, the assessment period for the permit application ceases to run from the date of this notice until the Heritage Council provides notice to the relevant planning authority that it is satisfied that the above requirement has been answered. The procedure for requiring additional information to consider the permit application is outlined under s37 of the *Historic Cultural Heritage Act 1995*.

Please contact Russell Dobie on 0458 326828 if you would like to discuss any matters relating to this application or this notice.

A handwritten signature in black ink, appearing to read 'RD'.

Russell Dobie
Regional Heritage Advisor – Heritage Tasmania
Under delegation of the Tasmanian Heritage Council



Tasmanian Heritage Council
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PLANNING REF: PLN-HOB-2025-0658
THC WORKS REF: 8796
REGISTERED PLACE NO: 2182
APPLICANT: Mark Gardner
DATE: 08 May 2026

RECEIPT OF ADDITIONAL INFORMATION

(Historic Cultural Heritage Act 1995)

The Place: Sydney Lodge, 65 Brisbane St, Hobart
Proposed Works: Additional Storey

Under section 37(4) of the *Historic Cultural Heritage Act 1995*, the Heritage Council gives notice that it is satisfied with the additional information received by our office on 04/405/2026, required by our Notice of 18/12/2025.

The clock can accordingly re-start on this date (08/05/2026) subject to any other information being required by your planning authority.

Please contact Russell Dobie on 1300 850 332 if you require clarification of any matter contained in this letter.

A handwritten signature in black ink, appearing to read 'RD'.

Russell Dobie
Regional Heritage Advisor – Heritage Tasmania
Under delegation of the Tasmanian Heritage Council



TRAFFIC IMPACT ASSESSMENT

Hubble Traffic

Updated October 2025

MULTI-USE DEVELOPMENT, 57-65 BRISBANE STREET, HOBART

Disclaimer: This report has been prepared based on and in reliance upon the information provided to Hubble Traffic Pty Ltd by the client and gathered by Hubble Traffic Pty Ltd during the preparation of the report. Whilst all reasonable skill, care and diligence has been used in preparation of the report, Hubble Traffic Pty Ltd take no responsibility for errors or omissions arising from misstatements by third parties.

This report has been prepared specifically for the exclusive use of the client named in the report and to the extent necessary, Hubble Traffic Pty Ltd disclaim responsibility for any loss or damage occasioned by use of or reliance upon this report, or the date produced herein, by any third party.

Version	Date	Reason for Issue
Draft	March 2025	Draft issued for client feedback
Final	October 2025	Final issued



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1. Introduction

Aldanmark (developer) has engaged Hubble Traffic to prepare an independent Traffic Impact Assessment to consider the traffic impacts of a proposed six-storey multi-use building at 57-65 Brisbane Street, Hobart (development site).

This assessment is based on Aldanmark project plans 24E99-92 revision I. Considered the functional requirements of the proposed uses, the size, topography, and position of the lot, the current surrounding road network, and the need to provide adequate on-site parking while ensuring safe and efficient access.

The site has five existing tenancies operating on the sites perimeter, which will remain unchanged with the development.

The development has been assessed against the Hobart Interim Planning Scheme E5 Road and Railway Assets Code, E6 Parking and Access Code, and the Australian Standard 2890.1:2004.

This report has been prepared to satisfy the requirements of Austroads' Guide to Traffic Management Part 12: Traffic Impacts of Developments, 2019, and has referred to the following information and resources:

- Hobart Interim Planning Scheme (planning scheme)
- Road Traffic Authority NSW (RTA) Guide to Traffic Generating Developments
- Australian Standards AS2890 parts 1, 2 and 6
- Austroads series of Traffic Management and Road Design
 - Part 4: Intersection and crossings, General
 - Part 4a: Unsignalised and Signalised Intersections
 - Part 12: Traffic Impacts of Development
- Department of State Growth crash database
- Autoturn Online vehicle turning software
- LIST Land Information System Tasmania Database

This Traffic Impact Assessment has been revised to reflect the inclusion of an additional storey within the proposed development, resulting in an increased number of residential apartments. The updated assessment also incorporates feedback received through Council's previous Request for Further Information.



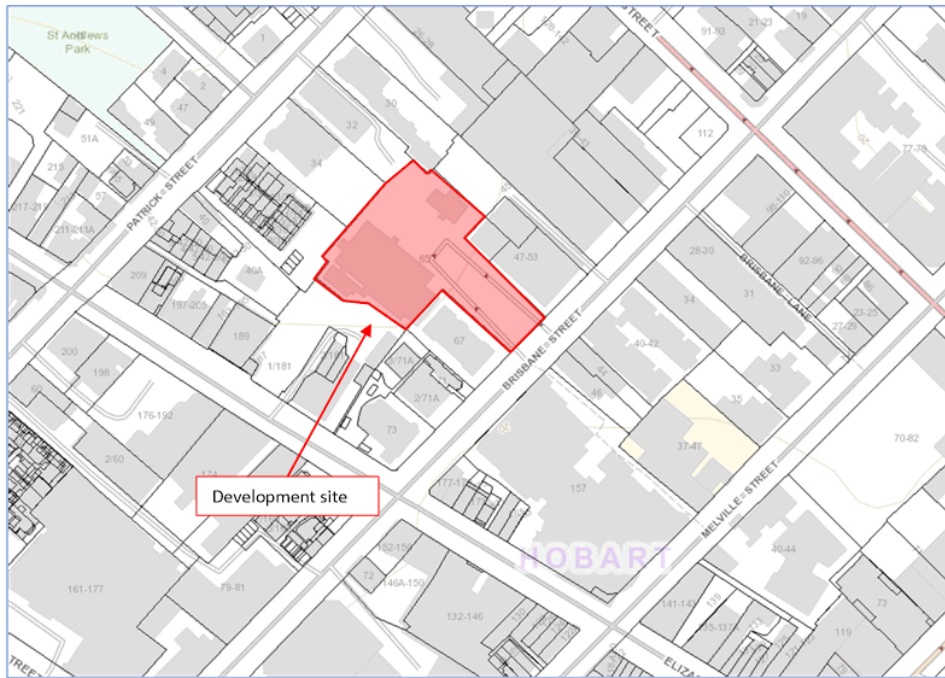
2. Site Description

The development site, located at 57-65 Brisbane Street, currently operates as a car parking station with approximately 146 spaces and five tenancies situated on the perimeter of the site.

The site is positioned on a steep incline, with separate entry and exit access onto Brisbane Street.

According to the Land Information System Tasmania Database (LIST), the development site is within an established Commercial zone, in close proximity to the Hobart Central Business District (CBD).

Diagram 2.0 – Extract from LIST Database



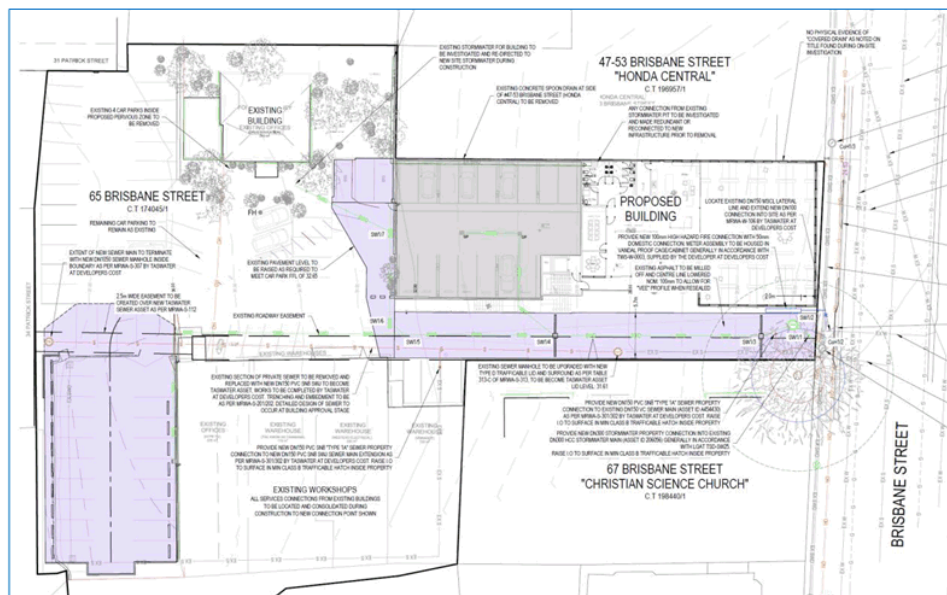
3. Development proposal

The development proposal is to construct a new six-storey building, without any changes to the existing five tenancies, or changes to the parking spaces located at the rear of the site. The building will include:

- Ground floor: One large tenancy suitable for general retail use, bicycle parking with associated change room and lockers, waste storage, lobby and pedestrian access onto Brisbane Street.
- First and second floors: Each floor will be suitable to accommodate a commercial use, specifically office use, with final floor layouts to be completed at a later date.
- Third and fourth floors: Each containing five residential apartments, comprising of two-bedroom apartments.
- Fifth floor: Three residential penthouse apartments, each with a minimum of three bedrooms.

In total, 72 on-site car parking spaces will be provided. These will be achieved by retaining 56 of the existing parking spaces located at the rear of the site and providing 16 new parking spaces on the second floor of the new building, as a new enclosed parking module, which will be allocated to the residential apartments.

Diagram 3.0A – Proposed development layout



T: 0416 064 755
 E: Hubbletraffic@outlook.com

Diagram 3.0B – Proposed ground floor layout

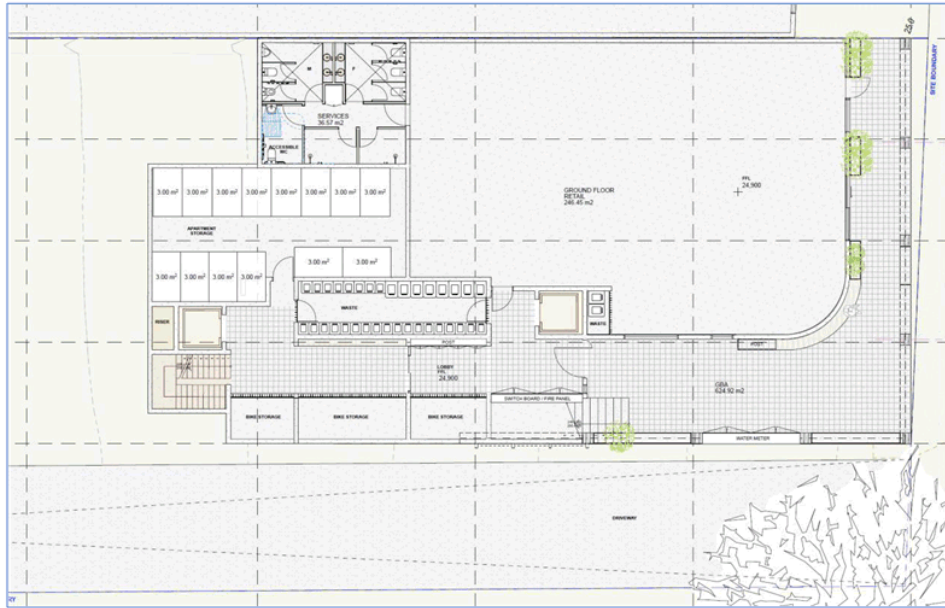


Diagram 3.0C – Proposed first floor layout

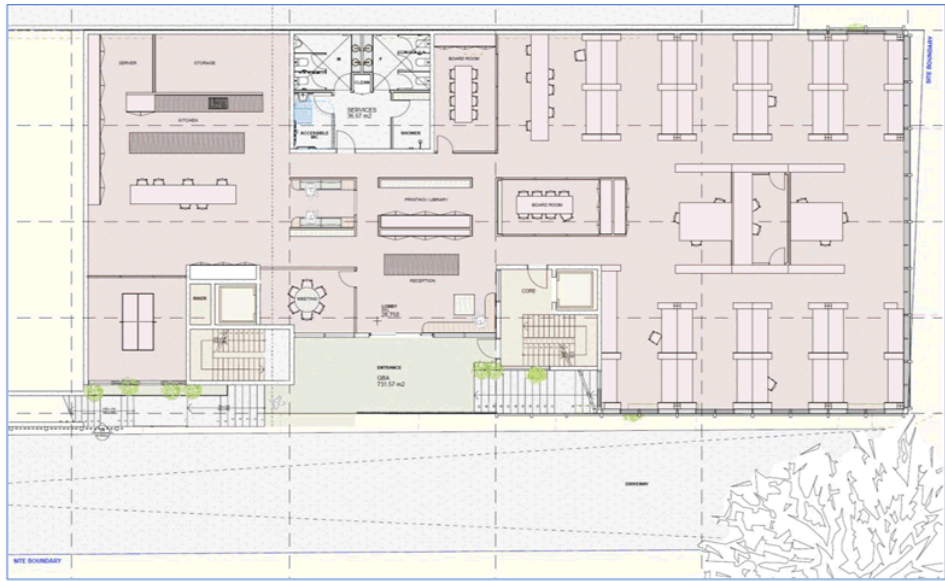


Diagram 3.0D – Proposed second floor layout

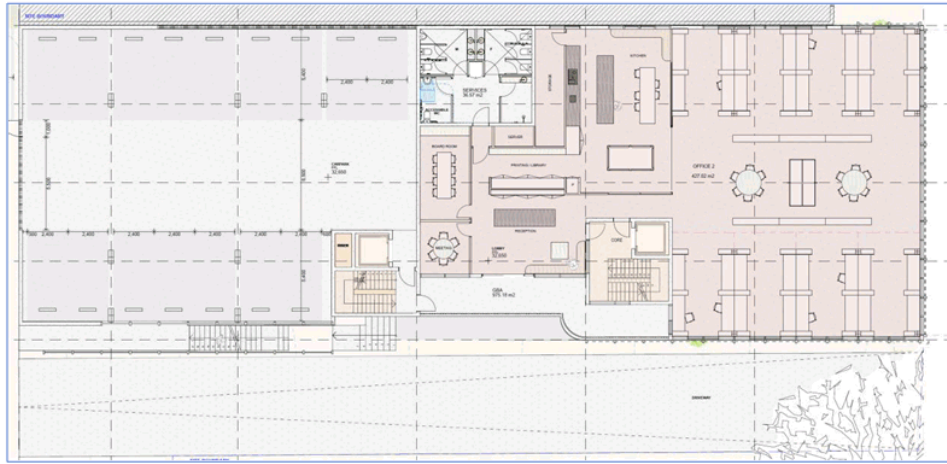


Diagram 3.0E – Proposed third floor layout

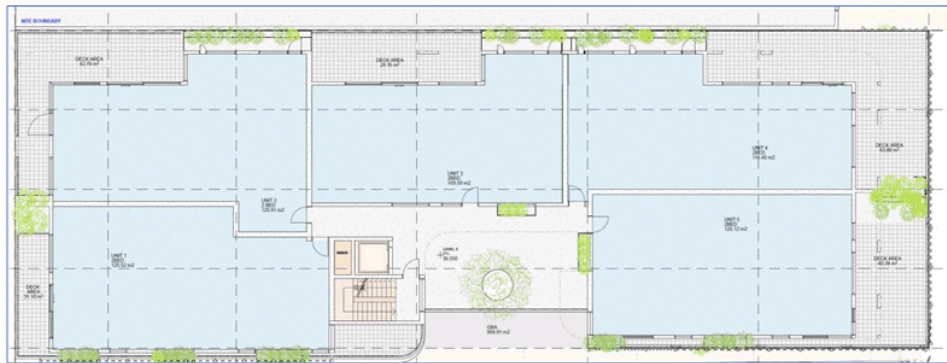


Diagram 3.0F – Proposed fourth floor layout

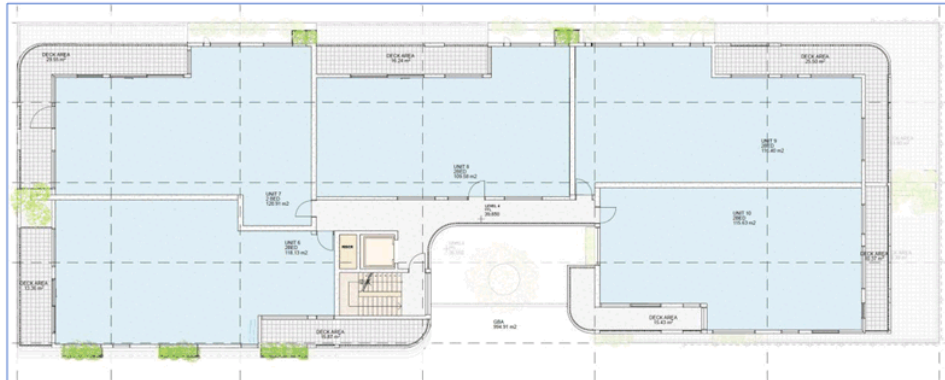
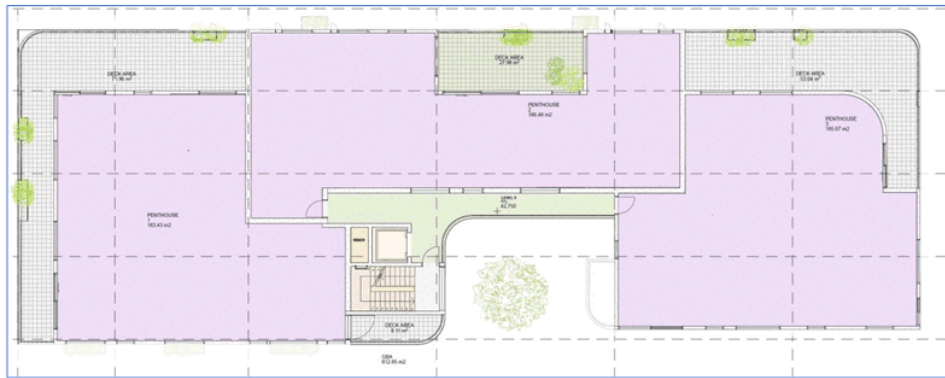


Diagram 3.0G – Proposed fifth floor layout



4. Trip generation by this development

A trip in this report is defined as a one way vehicular movement from one point to another excluding the return journey, therefore, a return trip to and from a land use is counted as two trips.

To determine the impact of the development, it is necessary to compare the trips currently generated by the parking station with the estimated trips from the new building. Noting that the number of trips generated by the existing tenancies will remain the same.

The RTA Guide to Traffic Generating Developments (RTA Guide), has been used to estimate the number of trips generated by general retail, residential and office uses, while all other on-site uses have been estimated.

Table 4.0 –Requirements as per RTA Guide

Land use	RTA land use	Generation Rate
General retail	Specialty shop	<ul style="list-style-type: none"> Daily vehicle trips of 40 trips per 100m² of floor area, and Peak trips of 5.6 per 100m² of floor area.
Residential	Medium density residential units, smaller units (up to two bedrooms):	<ul style="list-style-type: none"> Daily vehicle trips of 5 per unit, and Weekday peak trips of 0.5 per unit.
	Medium density residential units, larger units (three or more bedrooms):	<ul style="list-style-type: none"> Daily vehicle trips of 6.5 per unit, and Weekday peak trips of 0.65 per unit.
Office	Office and commercial	<ul style="list-style-type: none"> Daily vehicle trips of 10 trips per 100m² of floor area, and Peak trips of 2 per 100m² of floor area.

4.1. Existing trips generated by the parking station

Of the 146 on-site car parking spaces, 125 spaces are available for lease. Each space generates two trips per day, one arriving and one leaving, with an estimated 10 percent of spaces generating an intermediate trip during the day. Based on this, the 125 leased parking spaces are estimated to generate 275 daily trips when fully occupied.

Based on a recent traffic survey undertaken at the site, 41 vehicle movements were generated in the morning peak hour and 58 vehicle movements in the evening peak hour. With the site providing 146 parking spaces, this equates to each space generating 0.28 vehicle movements in the morning peak hour and 0.4 vehicle movements in the evening peak hour.

It is estimated that the leased parking spaces generate 275 daily trips, with 35 trips occurring during the morning peak period and 50 trips during the evening peak period.

Table 4.1 – Number of trips generated by the parking station

Trips	Daily	Morning peak	Evening peak
Parking station	275	35	50



4.2. Existing trips generated by existing tenancies

Of the five existing tenancies, two are office land use, while the others are general retail, service industry, and sports and recreation use.

General retail and Office Land use

Based on the floor areas, the RTA Guide estimates that the general retail use has the potential to generate 62 daily trips, with 9 trips likely to occur in the peak periods. While the office use is estimated to generate 50 daily trips, with 10 likely to occur in the peak periods.

Service Industry use

The number of trips generated by a service industry use is generally determined by the number of vehicles that can be serviced in one day and the number of employees. Assuming a worst-case scenario of three service bays operating, with each generating three vehicles per day, three employees, and one flatbed truck per day, this use could generate a total of 13 vehicles per day.

From the 13 vehicles per day, it is estimated all employees travel during the peak periods, and 50 percent of customers drop off and pick up their vehicles within the peak periods, generating eight trips within both peak periods.

While the trip assessment accounts for two trips by a flatbed truck delivering a repair vehicle outside of peak periods, this scenario is unlikely due to the infrequent nature of these deliveries.

Sports and Recreation use

It is difficult to estimate the number of trips generated by this use, which has two dedicated parking spaces, and relies on sharing the parking spaces of other tenancies when they are not operating.

Based on a floor area of 170 square metres, its estimated this type of use could generate eight parking spaces. In a worst-case scenario where three one-hour classes were operating at full capacity, it could generate eight vehicles in any one hour, totalling 24 vehicles, or 48 daily trips.

Based on the RTA Guide trip generation rates, the existing tenancies has the potential to generate up to 186 daily trips, with 43 occurring within the busiest peak hour. A comparison of the recent manual survey data indicates that these tenancies generate most of their trips outside of the normal commuter peak hours, with the morning survey recording a small number of peak hour trips as shown in table 4.2.

Table 4.2 – Estimated number of trips

Use	RTA Guide trips		Manual survey	
	Daily	Peak	Morning peak	Evening peak
Retail	62	9	1	2
Office	50	10	5	6
Service industry	26	8	2	2
Sports and recreation	48	16	1	1
Total	186	43	9	11

4.3. Trips generated by new building tenancies

Residential trips

Based on the RTA Guide trip generation rates, section 3.3 residential housing, 10 two-bedroom and 3 three-bedroom residential apartments are estimated to generate 70 daily trips, with 7 trips likely to occur during the peak periods.

Table 4.3A – Residential trips

Dwelling Type	RTA Generation rate	Number of units	Daily trips	Peak trips
Two bedrooms	5 per day 0.5 per peak	10	50	5
Three bedrooms	6.5 per day 0.65 per peak	3	20	2
Total		13	70	7

Retail trips

Based on a floor area of 247 square metres, the retail tenancy is estimated to generate a total of 99 daily trips, with 14 trips likely to occur within the busiest peak hour.

The general retail is located on the ground floor, with customers likely to use the supply of on-street parking rather than the on-site parking located at the rear of the development. This will generate trips along Brisbane Street, with customers unlikely to generate a trip using the site's vehicular access.

For the purpose of this assessment, these trips will be assessed as trips generated by the development.

Office trips

The office areas have a combined floor area of 961 square metres over two floors, with the RTA Guide indicating this use could generate 96 daily trips, with 19 trips occurring during the peak periods.

Table 4.3B – Number of trips generated by the office accommodation

Trips	Daily	Morning peak	Evening peak
Office use	96	19	19

Alternative transport mode

The development is within close proximity to a variety of alternative transport modes that reduce the reliance on private vehicles. These include the Hobart CBD Bus Interchange with high-frequency bus services and on-road cycle facilities that promotes active transport

To understand the use of alternative transport modes, a parking survey was conducted at the formerly named Vodafone multi-storey building, located at the corner of Argyle and Bathurst Streets in Hobart. This site was chosen as it operates in a similar manner to the proposed development, providing 184 casual parking spaces, 144 permanent parking spaces, and four floors of office accommodation.

The survey data demonstrates that where suitable alternative transport modes are available, the use of private vehicles is significantly reduced, with 40% of the occupants using alternative transport options like ridesharing, walking, and cycling.

This survey data can be found within Appendix C.

Total trips generated by the new building:

When the new building is fully operational, based on the RTA Guide the building could generate 265 daily trips, with 40 trips likely to occur within the busiest peak hour. However, a portion of these trips can be discounted, with some employees likely to use alternative transport.

Overall, with the discounts applied to the office use, the new building is estimated to generate 227 daily trips, with 32 trips likely to occur during the busiest peak hour.

Table 4.3C – Estimated number of trips

Use	RTA Guide trips		Discount	Estimated trips	
	Daily	Peak		Daily	Peak
Residential	70	7	None	70	7
Retail	99	14	None	99	14
Office	96	19	40% using alternative transport	58	11
Total				227	32

4.4. Estimate of change in trips generated by the development

As previously discussed, with no proposed changes to the existing tenancies, it is expected that they will continue to generate the same number of trips. The change in trips will result from converting the parking station into the new building.

The parking station (125 leased parking spaces) is estimated to be generating 275 daily trips, with 35 of these trips occurring in the morning peak, and 50 trips in the evening. The new building is predicted to generate 227 daily trips, with 32 trips likely to occur in the morning and evening peak periods. Even without the alternative transport discount for the office accommodation, the new building is expected to generate less trips than the parking station.

The table below demonstrates that the new building is likely to reduce the number of trips generated per day and in the peak periods, compared to the parking station.

Table 4.4 – Estimated trip generation

Trips	Daily	Morning peak	Evening peak
Parking station	275	35	50
New building	227	32	32
Difference	-48	-3	-18

5. Existing road network

5.1. Brisbane Street characteristics

Brisbane Street operates in an east-to-west orientation past the development site, extending between Brooker Avenue and Hill Street. The horizontal road alignment is straight and situated within mostly flat terrain.

Adjacent to the development site, the road has been constructed to an urban standard, with a 12.1 metre wide bitumen surface, concrete kerb and channel, footpaths along both sides, and street lighting. Due to the close proximity to the CBD, there is a high demand for on-street parking, with short-term parking restrictions installed to provide a high turnover of on-street parking spaces.

With no posted speed limit signs, the urban default 50 km/h speed limit applies by regulation.

Photograph 5.1 – Brisbane Street standard



5.2. Elizabeth Street and Argyle Street intersections

Brisbane Street intersects both Elizabeth Street and Argyle Street, forming two four-way intersections. The traffic signals operate as isolated sites, where the lights change based on demand, including pedestrian demand, providing an efficient level of service. The surrounding roads already carry significant traffic flows through these two signal controlled intersections ensuring motorists receive an appropriate level of traffic efficiency.

Traffic modelling at the signal sites is not considered necessary, as the phasing of signals will adapt to traffic flows through the intersection and continue to balance the competing needs of users.

5.3. Traffic flow on the surrounding road network

To evaluate the traffic impact from the development, it is important to understand the current traffic flow on the surrounding road network. Recent manual traffic surveys were undertaken at the development site's entry and exit accesses with Brisbane Street, during the morning and evening peak periods of Wednesday, 18th December. This data is available in Appendix A.

The surveys indicate that Brisbane Street is moderately trafficked, with slightly higher two-way traffic flows captured in the evening peak compared to the morning peak.

Overall, the development site was observed to be a low traffic generator, with 41 two-way vehicles captured entering and leaving during the morning peak, and 58 vehicles in the evening peak.

Based on the manual traffic survey data, the following diagrams illustrate the turning movements and two-way traffic flows along Brisbane Street.

Diagram 5.3A – Morning peak hour traffic movements

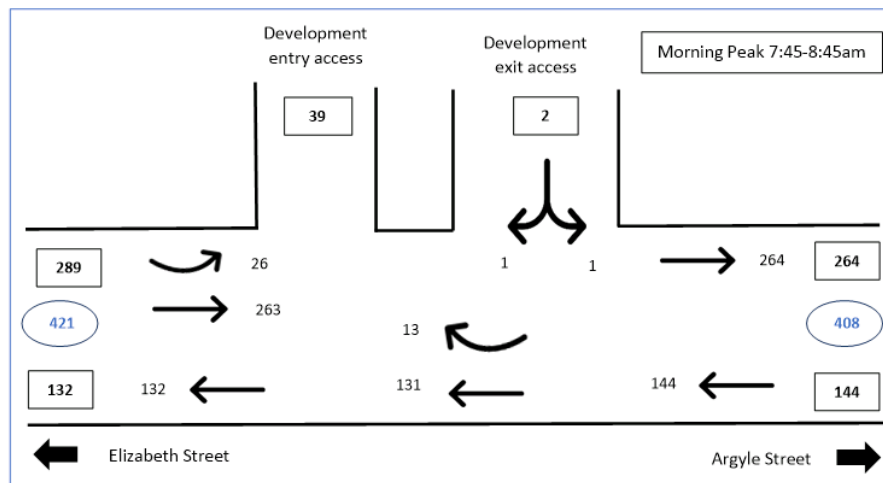
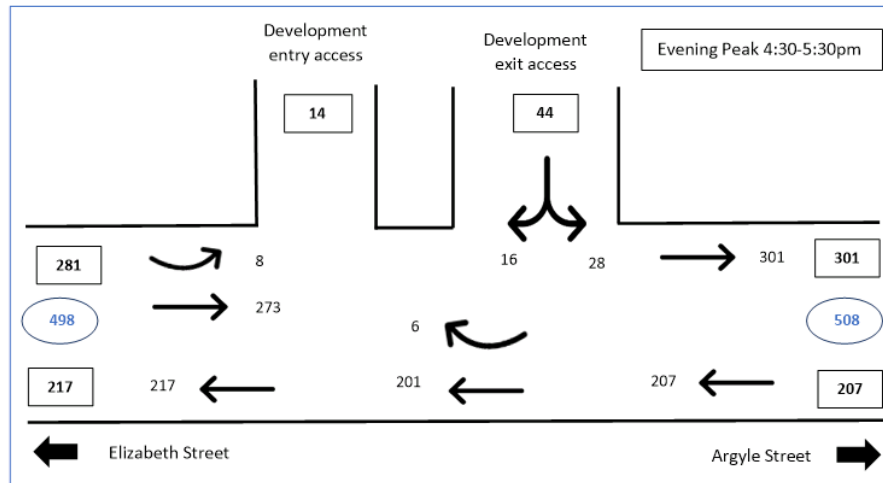


Diagram 5.3B – Evening peak hour traffic movements



5.4. Road safety of surrounding road network

The Department of State Growth maintains a database of reported road crashes. A check of this database for the last completed five years found a total of 17 accidents occurring along Brisbane Street between Elizabeth and Argyle Streets.

Most of the accidents occurred at the signalised intersections, mainly resulting in property damage only.

Five crashes occurred between the intersections, all resulting in property damage, where a vehicle:

- reversed into a fixed object or parked vehicle
- rear-ended the vehicle in front
- hit an object on the footpath
- hit an object while manoeuvring
- hit an object while parking

The type and number of crashes are considered acceptable for this type of road, with the low operating speeds minimising the crash severity. This number of crashes does not indicate motorists are having any difficulty negotiating the road layout.

6. Impact from traffic generated by this development

As outlined in Section 4, the proposed development is anticipated to result in a net reduction of approximately 48 daily vehicle trips, including 3 fewer trips during the AM peak and 18 fewer trips during the PM peak period. Given this development is unlikely to generate an increase in trips, the following section assesses the current traffic performance experienced by motorists within the surrounding road network.

The manual surveys indicated that Brisbane Street is moderately trafficked, 433 two-way vehicle movements observed in the morning peak period and 508 in the evening.

According to the RTA Guide, Brisbane Street currently provides a high level of service (LOS A–B) within an urban context. This indicates stable traffic flow conditions, with motorists able to select their operating speed and sufficient gaps in the traffic stream to facilitate safe entry and exit movements.

Temporary construction activity between Elizabeth and Murray Streets has altered traffic conditions, potentially reducing observed volumes by up to 30% during recent manual surveys. Notwithstanding this reduction, the underlying capacity of Brisbane Street remains sufficient to accommodate increased traffic volumes without compromising performance, as demonstrated in the table below.

The presence of signalised intersections at either end of Brisbane Street further supports efficient traffic flow and network resilience.

Table 6.0 – Traffic performance for Brisbane Street users

Brisbane Street	Existing flows			
	Morning		Evening	
	EB	WB	EB	WB
Existing flows	289	144	301	217
Level of Service	B	A	B	B
30% additional traffic	376	187	391	282
Level of service	B	A	C	B

Diagram 6.0 – Extract from the RTA Guide

Level of Service	One Lane (veh/hr)	Two Lanes (veh/hr)
A	200	900
B	380	1400
C	600	1800
D	900	2200
E	1400	2800

7. Access arrangement to and from the development site

7.1. Access into and out of the development

The development site has two existing vehicular accesses onto Brisbane Street, operating as separate entry and exit. The development will operate with a single two-way access point, using the current access on the western side of the property, which is approximately 10 metres wide.

The removal of the exit access will increase the supply of on-street parking.

Photograph 7.1 – Existing vehicular accesses onto Brisbane Street



7.2. Sight distance leaving the development site

At the proposed two-way access point, Brisbane Street has a straight horizontal road alignment, and a slight vertical grade on the eastern side that does not inhibit sight distance for motorists leaving the site.

With Brisbane Street located within a 50 km/h speed environment, planning scheme table E5.1 specifies that the Safe Intersection Sight Distance (SISD) is 80 metres. On-site measurements of the available sight distance were taken, with motorists having 90 metres of available sight distance in both directions, enabling vehicles to enter and leave the development site in a safe and efficient manner, without impacting other road users.

It is important to note that due to the high demand for on-street parking, parked vehicles on the right side of the vehicular access have the potential to limit sight lines. Parked vehicles are a temporary obstruction and have the potential to limit SISD for a driver located 2.5 metres back from the edge of the kerb. However, when there is a parked vehicle, the driver can move beyond the kerb face to increase sight distance, knowing the parked vehicle provides suitable protection.

The following two photographs below demonstrate that a driver positioned beyond the kerb face can achieve appropriate sight distance when on-street parking is occurring.

Photograph 7.2A – Available sight distance to the left



Photograph 7.2B – Available sight distance to the right

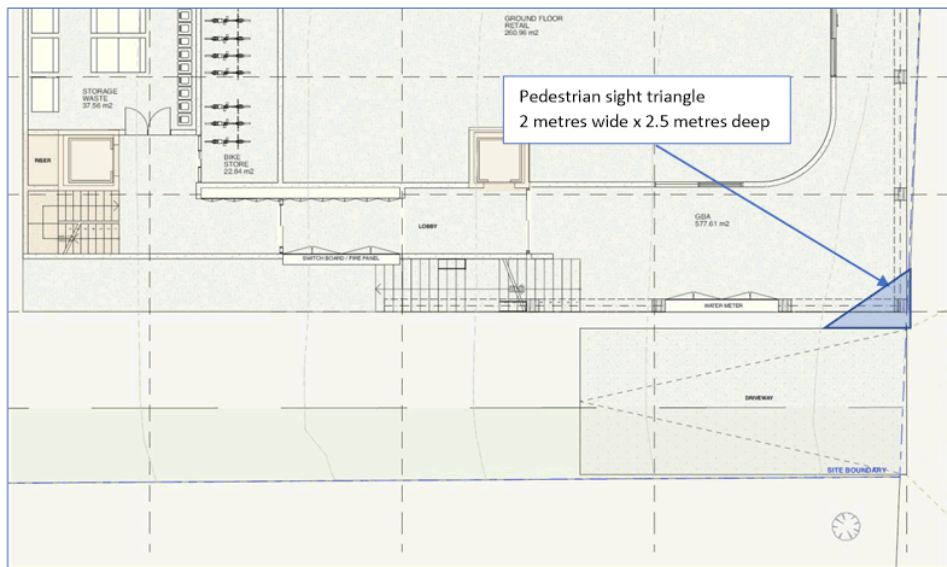


7.3. Pedestrian sight distance

It is important for drivers leaving the development site to have adequate sight lines to pedestrians using the footpath along Brisbane Street. Adequate sight lines between a driver leaving the site and pedestrians using the footpath will be achieved by minimising physical obstacles in the pedestrian sight triangle.

The ground floor building is set back from the edge of the footpath, with columns located at the back of the footpath and to the side of the driveway to support the first floor. The supporting columns will be well set apart, and a single column located within the pedestrian sight triangle is not expected to inhibit pedestrian sight lines.

Diagram 7.3 – Pedestrian sight triangle



8. Parking requirements

8.1. Number of car parking spaces required for the new building

Planning scheme table E6.1 prescribes the number of on-site car parking spaces required, based on the type of land use. The new building has three different uses, Business and professional services (Office use), General retail and hire (except as otherwise specified in this table), and Residential (multiple dwelling containing two or more bedrooms).

Based on 13 residential apartments and the floor areas of other uses, the new building requires 70 on-site car parking spaces under table E6.1 of the planning scheme, as illustrated in the table below.

Table 8.1 – Planning scheme on-site car parking spaces for the new building

Activity	Use	Planning scheme requirements	Floor area /dwellings	Number of parking spaces
Office	Business	One space per 30 square metres of floor area.	961m ²	32
Retail	General Retail	One space per 30 square metres of floor area.	247m ²	8
Residential	Residential	Two spaces per dwelling	13 dwellings	26
	Visitor parking	One dedicated visitor space per four dwellings (rounded up to the nearest whole number)		4
Total for new building				70

8.2. Number of parking spaces required for the existing five tenancies

The five existing tenancies located along the site's perimeter are to be retained, with no changes proposed to their current on-site parking allocations. These tenancies comprise a mix of land uses, including Business and Professional Services (Office), General Retail and Hire, Service Industry, and Sports and Recreation. Based on the respective floor areas and applying the relevant use-specific parking rates under Table E6.1 of the Tasmanian Planning Scheme, a total of 29 on-site car parking spaces are required to achieve compliance.

Table 8.2 – Number of on-site car parking spaces required for the existing tenancies

Activity	Use	Planning scheme requirements	Floor area	Number of parking spaces
Drug Education Network	Office	One space per 30 square metres of floor area.	240m ²	8
Hype TV	Office	One space per 30 square metres of floor area.	225m ²	8
Vibrance	General Retail	One space per 30 square metres of floor area.	155m ²	5
West End Mechanics	Service industry	Three spaces per 100 square metres of floor area, or three spaces for each work bay, whichever is greater	200m ²	6
Tae Kwon Do TAS	Sports and recreation	Subject to traffic and parking impact assessment	170m ²	2
Total for existing businesses				29

The existing tenancies currently operate with a total allocation of 21 on-site parking spaces. One tenant has relinquished three spaces, reducing the effective provision to 18 spaces. While Table E6.1 of the Tasmanian Planning Scheme specifies a higher number of spaces based on floor area and land use, the current allocation has proven sufficient to meet operational demand, with no reported issues relating to parking availability or site functionality. Given the spatial constraints and the absence of adverse impacts, it is considered appropriate to retain the existing parking arrangement for these tenancies.

8.3. Functional parking demand

Apartments

The demand for on-site parking is moderated by the site's location on the fringe of the Hobart CBD, where proximity to public transport, active travel networks, and employment centres supports reduced car dependency. Each two-bedroom apartment will be allocated one dedicated parking space, while three-bedroom apartments will be allocated two spaces. All 16 residential parking spaces will be accommodated within an enclosed parking module, ensuring secure and weather-protected access for residents.

Existing five tenancies

In accordance with the developer's direction, 18 on-site parking spaces will be allocated to the five existing tenancies. Although this provision falls below the number specified in Table E6.1 of the Tasmanian Planning Scheme, the tenancies have historically operated without reported parking-related issues. Given the established usage patterns and absence of operational constraints, the proposed allocation is considered sufficient to meet practical demand.

Retail use

The retail space has a floor area of 246m² and is situated on the ground floor with direct access to Brisbane Street. Given its inner-city location, many customers are expected to arrive on foot, while those traveling by vehicle are likely to use the supply of on-street parking, as access to the retail on-site parking spaces may be more convenient.

Under planning scheme table E6.1, eight on-site parking spaces are required for the retail use. This number of spaces is considered excessive when taking into consideration the inner city location, the supply of on-street parking, and access convenience. The development will provide 4 on-site spaces for this retail use.

Office accommodation

The development will provide 32 on-site parking spaces allocated to office employees. This is likely to create an oversupply of parking for this use, as given the inner-city location, it is expected that a reasonable number of employees will use an alternative transport mode.

8.4. Number of spaces being provided by the development

In accordance with table E6.1 of the planning scheme, the proposed development generates a requirement for 99 on-site car parking spaces. Due to site constraints, only 72 spaces can be accommodated within the development footprint. Notwithstanding this shortfall, functional parking demand analysis, summarised in table 8.4, indicates that the provision of 72 spaces is expected to adequately support the operational needs of the development without adverse impacts.

Table 8.4 – On-site parking being provided by the development

Type of use	Planning scheme requirement	Function demand	Reasoning of reduce parking demand
Existing five tenancies	29	18	These uses are currently operating with 18 spaces, with no operational issues raised
Office accommodation	32	32	No discount. This might result in an oversupply, with employees using alternative transport modes
Retail Use	8	4	Inner-city location, customers likely to arrive on foot, while vehicles are likely to use the on-street parking supply
Residential	30	16	Two-bedroom apartments provided with one parking space, due to the inner-city location.
Accessible parking		2	Located adjacent to the apartment parking module
Total	99	72	

8.5. Other parking requirements

Bicycle parking spaces

Table E6.2 prescribes the number and class of bicycle parking spaces required, based on the type of use.

- Residential use has no requirement
- Office use requires one space for each 250 square metres of floor area, after the first 250 square metres for employees, with one space per 1,000 square metres for customers if the floor area exceeds 1,000 square metres.
- General retail (Shop) use requires one space for each 500 square metres of floor area, after the first 500 square metres for employees, and one for each 500 square metres for customers.

Based on the floor areas, the development site is required to provide six bicycle spaces for employees and two spaces for customers. A total of eight bicycle spaces will be provided, complying with the planning scheme acceptable solution.

Motorcycle parking spaces

Based on the development providing 72 on-site parking spaces, three motorcycle parking spaces are required. Four motorcycle parking spaces will be provided, exceeding the requirement and complying with the planning scheme acceptable solution.

Accessible parking spaces

In accordance with the National Construction Code (NCC), the development site comprises multiple classifications: Class 2 (Residential), which does not trigger a requirement for accessible parking; Class 5 (Office) and Class 8 (Mechanic Workshop), which require one accessible space per 100 car parking spaces; and Class 6 (General Retail), which requires one accessible space per 50 car parking spaces.

Applying the most stringent rate across the site yields a requirement for two accessible parking spaces. These will be provided within the site and supported by a compliant shared zone.

The two accessible parking spaces will be located adjacent to the enclosed residential parking module and supported by a compliant shared zone. A delineated pedestrian pathway will connect these spaces to the lift lobby on the second floor, facilitating accessible vertical circulation throughout the building and direct access to Brisbane Street. The design includes a dedicated doorway between the accessible parking area and the lift lobby, ensuring safe, convenient, and equitable access for all users.

9. Internal development layout

9.1. Design vehicle

The primary type of vehicle accessing this site will be light vehicles, based on the developer's information. Among the existing tenants, only West End Mechanics generate occasional heavy vehicle movements. Occasionally, a flatbed truck is used to drop off and pick up repair vehicles, which is comparable to a heavy rigid vehicle (12.5 metres in length).

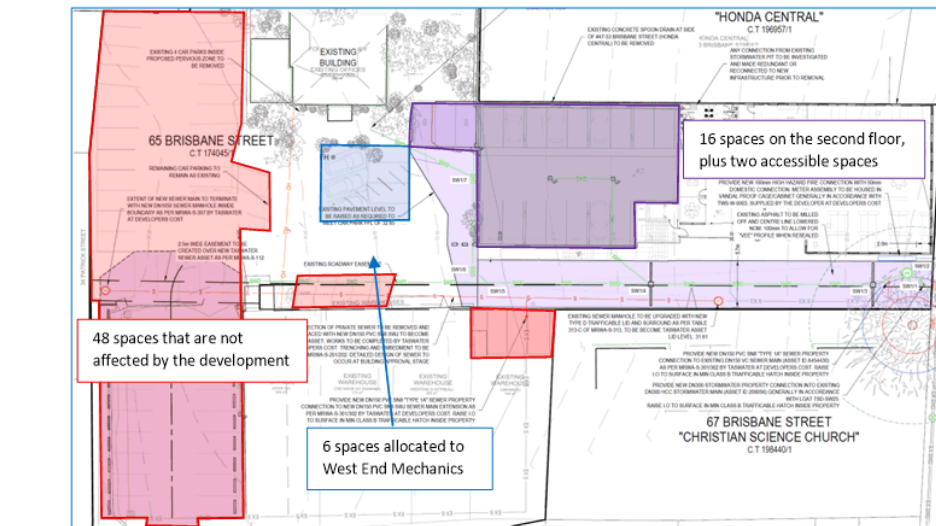
West End Mechanic has six parking spaces opposite their workshop. When a flatbed truck arrives, the operator rearranges vehicles within these spaces to allow the truck to turn around. As this process does not affect other parking spaces or users and will continue with the development.

No other heavy vehicles are expected, with any heavy vehicles associated with the new building to use on-street parking on Brisbane Street, while employees and visitors to the site are expected to generate light vehicle movements, measuring less than 5.5 metres in length.

9.2. Layout of parking spaces

The design features two distinct parking modules. The first is an enclosed parking module situated on the second floor of the new building, providing 16 parking spaces specifically allocated to the apartment's residents, with two accessible spaces located adjacent to this module. The second parking module offers 54 existing parking spaces.

Diagram 9.2 - Layout of on-site car parking



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9.3. Layout and dimensions of existing parking spaces

The majority of parking spaces at the rear of the new building will remain untouched in their current position, and although some of the spaces are situated on non-compliant vertical grades, they are considered fit-for-purpose.

The section highlighted in purple in the diagram below will be reconfigured and designed to comply with Australian Standards 2890.1:2004 (the Standard), user class 3, suitable for short-term city parking. Each space will be a minimum of 2.6 metres wide, 5.4 metres long, and supported with a minimum 5.8 metre manoeuvring aisle.

All parking spaces will be situated on an asphalt surface, line marked and some operating with wheel stops. Diagram 9.3 below illustrates the number of allocated parking spaces for the existing tenancies and their location.

Diagram 9.3 – Allocated parking spaces for existing tenancies



9.4. Enclosed parking module for the apartment’s residents

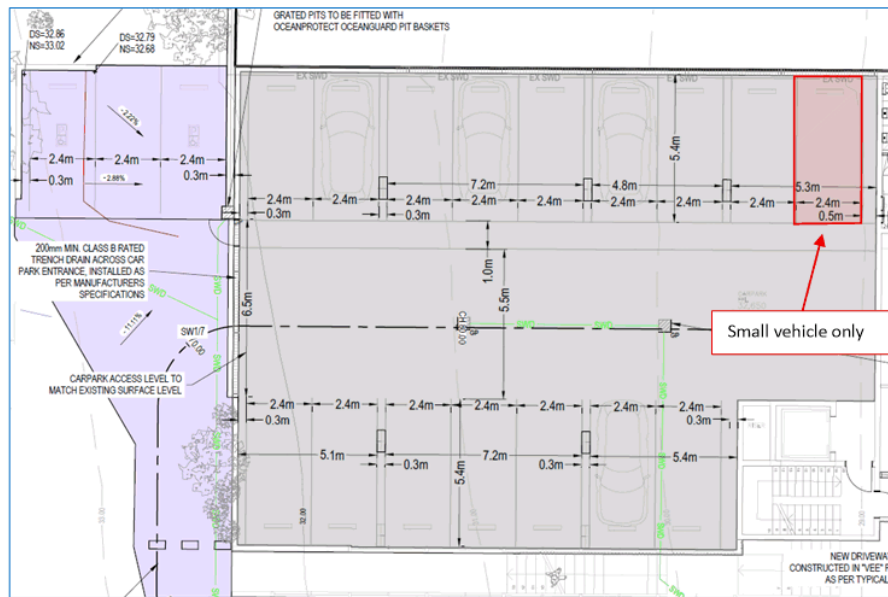
The parking spaces have been designed in accordance with AS 2890.1, User Class 1A, appropriate for residential and employee use. Each space will be a minimum of 2.4 metres in width and 5.4 metres in length, supported by a manoeuvring aisle of at least 5.8 metres.

Due to vertical constraints imposed by the building structure, the parking bay located at the right end of the top row will be designated for small car use only. Vertical obstructions adjacent to and opposite the bay restrict manoeuvring clearance; however, the space exceeds the minimum dimensional requirements for a compliant small car bay under Section 2.4.1(a)(iii) of AS 2890.1. Within the total allocation of 16 residential parking spaces, the inclusion of one small car bay is considered appropriate, particularly as it will be assigned to a three-bedroom apartment that is otherwise provided with two spaces. This arrangement ensures the small car bay supplements a standard space, mitigating any functional limitations and maintaining overall usability for the resident.

Where a parking space is located adjacent to a vertical obstruction higher than 150 millimetres, an additional 0.3 metres of width will be provided between the space and the obstruction. If there is a vertical obstruction on both sides, 0.3 metres will be provided on both sides of the space. The location of the vertical columns complies with the Standard section 5 and Figure 5.2, ensuring they do not inhibit vehicles entering or leaving, or opening of doors.

All parking spaces will be delineated with pavement markings and supported with wheel stops where possible. A minimum headroom clearance of 2.3 metres will be provided.

Diagram 9.4 – Layout of enclosed parking module



9.5. Gradient of car parking spaces

Section 2.4.6 of the Standard prescribes that parking spaces shall be designed to a maximum of five percent in both longitudinal and transverse directions. All reconfigured and new spaces within the enclosed parking module and have been designed with less than five percent gradients.

Due to the natural topography of the land, some of the existing parking spaces located behind the new building, currently operate with non-compliant grades, but are considered fit-for-purpose. The developer has advised the extent of infrastructure works to achieve compliant grade for these spaces is beyond the capacity of this project.

9.6. On-site turning and vehicle manoeuvrability

The new parking spaces have been designed with sufficient manoeuvring area to allow cars to enter and leave the spaces in a safe and efficient manner. Vehicle swept path software has been used to demonstrate a selection of the new on-site parking spaces located within the enclosed parking, available in Appendix B.

The developer has advised that the door to the enclosed resident parking module will remain open during daytime hours to assist with the manoeuvring of vehicles accessing the two designated accessible parking spaces. This operational arrangement supports ease of access and complies with functional requirements for accessible parking. The swept path diagrams provided in Appendix B demonstrate that vehicle movements into and out of these spaces

9.7. Internal driveway and stormwater

The design will retain the existing internal driveway layout, with some minor modifications. The main driveway, extending from Brisbane Street, will be a minimum of 5.5 metres, accommodating two-way traffic movements.

The driveway connecting Brisbane Street to the enclosed parking module will feature a new asphalt surface. A catch drain will be installed across the entrance to the enclosed parking module, with another catch drain positioned as close as possible to Brisbane Street. These drains are designed to collect and direct stormwater to an approved stormwater system.

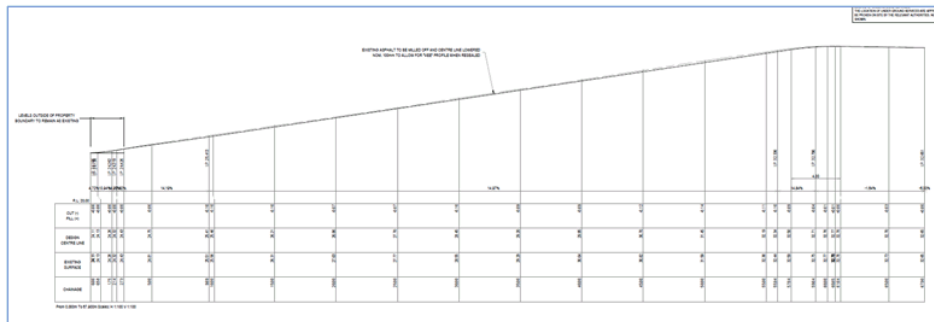
9.8. Internal gradients

The site's steep topography means the existing driveway's vertical grades will mostly stay the same, with an asphalt overlay fixing any deformations. A handheld electronic level found that most of the driveway has a grade of less than 15 percent, except for a five-metre section at 16 percent. According to the Standard, these grades are suitable for this type of development, with most vehicles being light and under 5.5 metres in length.

Although the existing Brisbane Street footpath grade at the vehicular crossover is higher than desirable, there is no scope for modification due to the presence of a Heritage Listed Tree adjacent to this driveway and access. Retaining the current footpath grade is not expected to cause any adverse impact on vehicles entering or leaving, or pedestrians.

The Aldanmark plans indicate that the gradient of the driveway is less than 15 percent, with a change in grade at the 60-meter chainage supplemented by a 4-meter long vertical curve. This driveway design is anticipated to offer an acceptable level of service for users.

Diagram 9.8 – Internal driveway gradients



9.9. Loading and unloading activity

All loading and unloading activities associated with the general retail will be conducted from on-street, utilising the available on-street parking along Brisbane Street.

The only commercial vehicle expected to use the site is an occasional flatbed truck delivering a repair vehicle to the West End Mechanics. This operator currently possesses six parking spaces directly opposite the workshop. When the flatbed truck is needed to deliver a vehicle, on-site vehicle rearrangement occurs to facilitate the truck's manoeuvring and turnaround, ensuring the vehicle enters and departs in a forward driving direction. The developer has advised this arrangement occurs without causing adverse impact, and will continue under the redevelopment of the site. Overall, all vehicles are expected to enter and leave in a forward-driving direction.

9.10. Waste collection

The design includes a communal waste storage area on the ground floor. The bins will be wheeled out to the front of the building for kerbside collection. It is proposed that the bins will be lined up underneath the building, between the columns, to ensure there is no adverse impact on pedestrians using the footpath.

The developer has advised that the existing tenancies will continue to manage their own waste collection.

10. Planning scheme

10.1. E5.0 Road and Railway Assets Code

E5.5.1 Existing road accesses and junctions

In comparing the existing car parking station with the proposed new building, the development is estimated to generate less daily and peak hour vehicle movements and complies with the acceptable solution A3.

E5.6.2 Road accesses and junctions

The entry access has sufficient width to accommodate two-way traffic flow and will be used as the main entry and exit, with the site's existing exit to be closed. This complies with the acceptable solution A2.

E5.6.4 Sight distances at accesses, junctions, and level crossings

The available sight distance at the entry access onto Brisbane Street exceeds the planning scheme requirements for a 50 km/h speed environment. This means vehicles will be able to enter and leave in a safe and efficient manner, complying with the acceptable solution A1.

10.2. E6.0 Parking and Access Code

E6.6.1 Number of parking spaces

To meet the acceptable solution under the planning scheme, the development is required to provide 99 on-site car parking spaces. Due to site constraints, the development site is able to provide a maximum of 72 on-site car parking spaces, meeting the functional parking demand, and is required to be assessed against the performance criteria P1.

Performance criteria	Assessment
The number of on-site car parking spaces must be sufficient to meet the reasonable needs of users, having regard to all of the following:	
a) car parking demand;	A functional car parking demand assessment has determined that the site is expected to generate a requirement for 72 on-site car parking spaces, which is below the prescribed 99 spaces under the planning scheme. This reduced demand reflects the allocation of one dedicated space per two-bedroom residential apartment, the continued operation of the five existing tenancies with 18 on-site spaces, and the expectation that the majority of

	retail customers will utilise available on-street parking in the surrounding area. The proposed provision is considered sufficient to meet the practical parking needs of the development without compromising operational efficiency.
b) the availability of on-street and public car parking in the locality;	Brisbane Street has been constructed with sufficient width to support on-street parking, while maintaining two-way traffic flow. Kerbside restrictions have been installed to generate a high turnover of vehicles using the on-street parking spaces.
c) the availability and frequency of public transport within a 400m walking distance of the site;	The closest bus stops are located on Elizabeth Street and Argyle Street, which are both within 200 metres of the development site. Both streets are high frequency routes, with regular services operating every 10 minutes.
d) the availability and likely use of other modes of transport;	Due to the close proximity to Hobart CBD, walking and cycling provide suitable alternative transport modes. Particularly with on-road cycle lanes operating along Argyle Street.
e) the availability and suitability of alternative arrangements for car parking provision;	None.
f) any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces;	None.
g) any car parking deficiency or surplus associated with the existing use of the land;	Aware of none.
h) any credit which should be allowed for a car parking demand deemed to have been provided in association with a use which existed before the change of parking requirement, except in the case of substantial redevelopment of a site;	The five existing tenancies currently operate with 18 on-site car parking spaces without operational issues. Retaining this number of spaces for these tenancies is considered appropriate.
i) the appropriateness of a financial contribution in	None.

lieu of parking towards the cost of parking facilities or other transport facilities, where such facilities exist or are planned in the vicinity;	
j) any verified prior payment of a financial contribution in lieu of parking for the land;	None.
k) any relevant parking plan for the area adopted by council;	Aware of none.
l) the impact on the historic cultural heritage significance of the site if subject to the Local Heritage Code;	The development has maximised the number of parking spaces on-site, while maintaining the historic cultural heritage significance of the land.
m) whether the provisions of the parking would result in the loss, directly or indirectly, of one or more significant trees listed in the Significant Trees Code.	A significant tree is located adjacent to the developments access; however, this will not result in any loss of on-site car parking spaces.

E6.6.2 Number of accessible parking spaces

The design includes two accessible parking spaces, supported with a shared zone, adjacent to the apartment parking module. These spaces are located adjacent to the second floor of the new building, and connected to the lifts by a marked pedestrian pathway.

E6.6.3 Number of motorcycle parking spaces

Four dedicated motorcycle parking spaces will be provided by the development, complying with the acceptable solution.

E6.6.4 Number of bicycle parking spaces

In total the development is providing eight bicycle spaces complying with the planning scheme acceptable solution.



E6.7. Development standards

Development standards	Comment
6.7.1 Number of Vehicular Accesses;	The development site will use the existing entry only access with Brisbane Street as a two-way access, while removing the exit only access, which complies with the acceptable solution.
6.7.2 Design of Vehicular Access;	The existing entry access measures 10 metres wide and is suitable to accommodate two-way traffic flow, complying with the acceptable solution.
6.7.3 Vehicular Passing Areas Along an Access;	The access, driveway and internal roads will be of sufficient width to allow for two-way traffic movements, complying with the acceptable solution.
6.7.4 On-site Turning;	The design allows for all vehicles to enter and leave the development site in a forward-driving direction.
6.7.5 Layout of Parking Areas;	The parking spaces at the rear of the building will remain unaffected by the development, with no changes planned. The layout of the new parking spaces within the enclosed parking module has been designed to comply with AS2890.1:2004, ensuring that vehicles can enter and leave in a safe and efficient manner, in accordance with the acceptable solution.
6.7.6 Surface Treatment of Parking Areas;	An asphalt surface will be provided for the internal driveways, while the new spaces will be located on a concrete surface.
6.7.7 Lighting of Parking Areas;	Lighting of the new parking spaces will be provided to meet the acceptable solution.
6.7.8 Landscaping of Parking Areas;	Landscaping will be provided in the development site.
6.7.9 Design of Motorcycle Parking Areas;	The development is providing four motorcycle parking spaces which will be designed to comply with section 2.4.7 "Provision for Motorcycles" of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking.
6.7.10 Design of Bicycle Parking Facilities;	Secure bicycle parking will be provided on the ground floor for both employees and visitors.
6.7.11 Bicycle End of Trip Facilities;	In total the development is providing eight bicycle parking spaces, with associated changerooms and amenities located on each floor.
6.7.12 Siting of Car Parking;	Not applicable for a development located within a Commercial zone.
6.7.13 Facilities for Commercial Vehicles;	West End Mechanic will continue to operate with six parking spaces directly opposite the workshop. Vehicle shuffling within these spaces will allow for the occasional flatbed truck to turnaround. No other commercial vehicles are expected on-site and would use the on-street parking along Brisbane Street.
6.7.14 Access to a Road;	The development has direct access onto Brisbane Street.

11. Conclusion

From a traffic engineering and road safety perspective, the additional traffic generated from this development is not expected to create any adverse safety, amenity, or traffic efficiency problems, as:

- the development site is predicted not to intensify the number of vehicle movements,
- the surrounding road network is highly managed with traffic signals that adapt to increases in traffic demand, ensuring an appropriate level of service is provided, minimising delays and traffic queues,
- a total of 72 on-site car parking spaces will be provided, which is expected to meet the reasonable needs of the new building and existing tenancies, minimising the potential for overflow parking,
- parking spaces at the rear of the property will remain unaffected by the development, and are considered fit-for-purpose,
- the entry access is of sufficient width to accommodate two-way traffic flow and has sufficient available sight distance to allow vehicles to enter and leave the site in a safe and efficient manner.

This Traffic Impact Assessment found no reason for this development not to proceed

12. Appendix A – Manual traffic surveys

Manual traffic surveys undertaken on Wednesday 18th of December

Morning traffic survey data

Time AM	Brisbane Street				Development site	
	Straight towards Elizabeth St	Straight towards Argyle St	Right into development site	Left into development site	Left onto Brisbane St	Right onto Brisbane St
7:15 – 7:30	20	37	1	7	0	0
7:30 – 7:45	33	44	1	11	0	0
7:45 – 8:00	21	53	6	11	0	0
8:00 – 8:15	31	67	0	7	0	0
8:15 – 8:30	43	68	4	5	1	0
8:30 – 8:45	36	75	3	3	0	1
Total	184	344	15	44	1	1
Peak total	131	263	13	26	1	1

*10 vehicles parked before the start of the survey, with a total of 67 vehicles parked at the conclusion

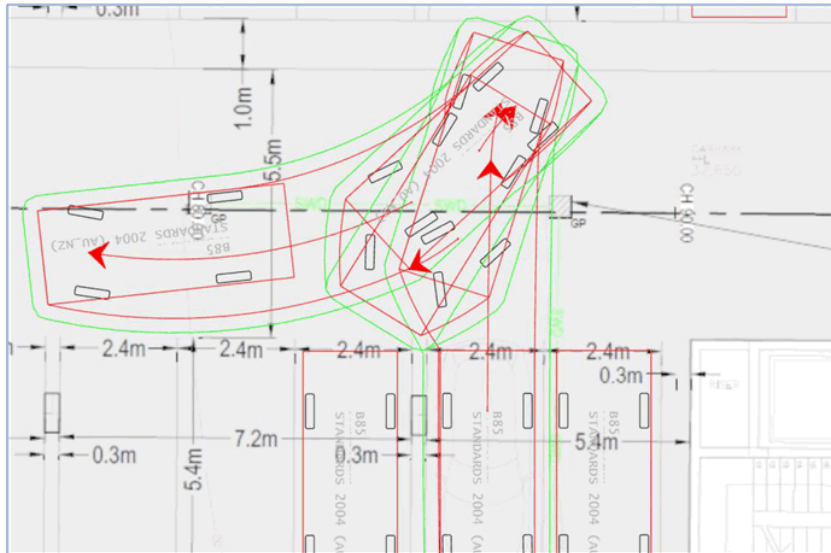
Evening traffic survey data

Time PM	Brisbane Street				Development site	
	Straight towards Elizabeth St	Straight towards Argyle St	Right into development site	Left into development site	Left onto Brisbane St	Right onto Brisbane St
4:00 – 4:15	71	67	2	0	7	3
4:15 – 4:30	45	58	0	2	4	1
4:30 – 4:45	56	55	0	0	6	2
4:45 – 5:00	38	55	4	2	7	4
5:00 – 5:15	48	80	1	3	6	6
5:15 – 5:30	59	83	1	3	9	4
Total	317	398	8	10	39	20
Peak total	201	273	6	8	28	16

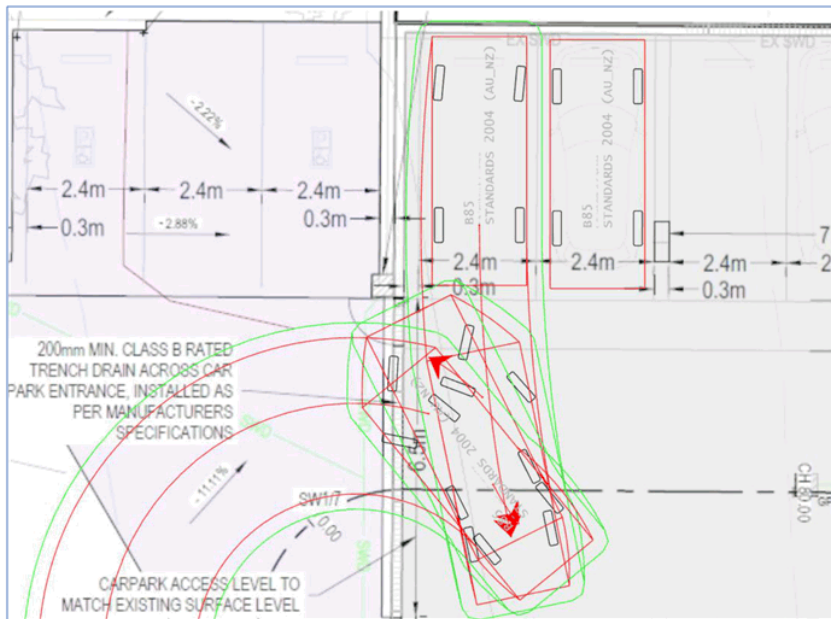
*80 vehicles parked before the start of the survey, with a total of 39 vehicles parked at the conclusion

13. Appendix B – Vehicle swept paths

Swept path of a B85 vehicle leaving parking space

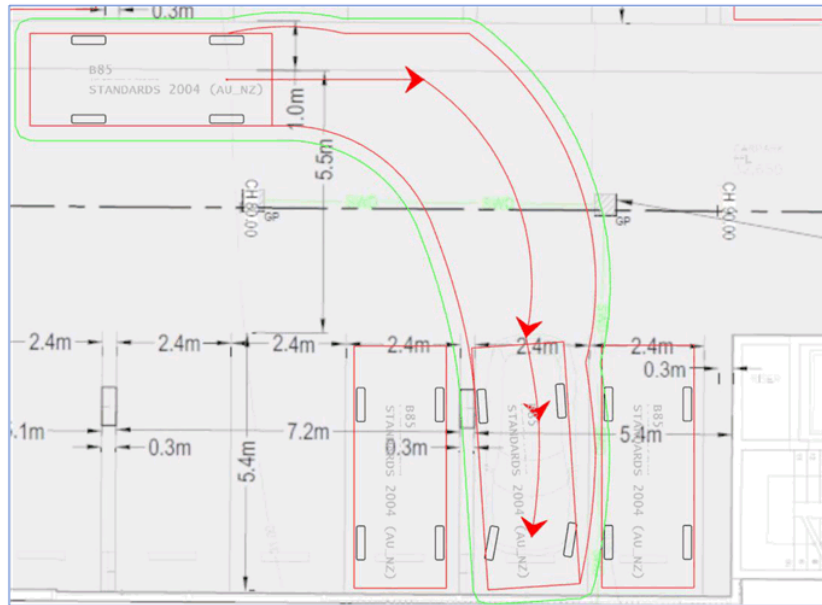


Swept path of a B85 vehicle leaving parking space

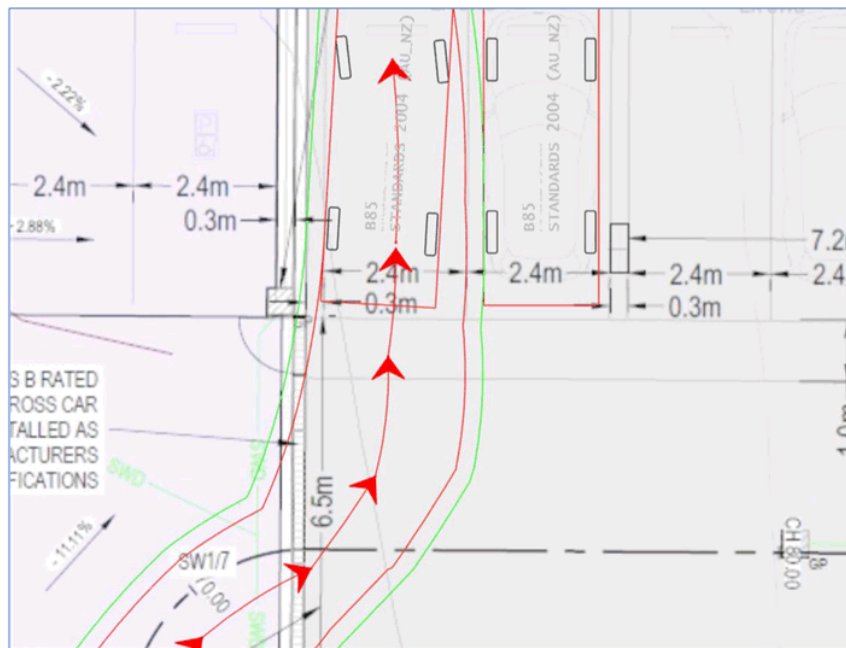


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Swept path of a B85 vehicle entering parking spaces

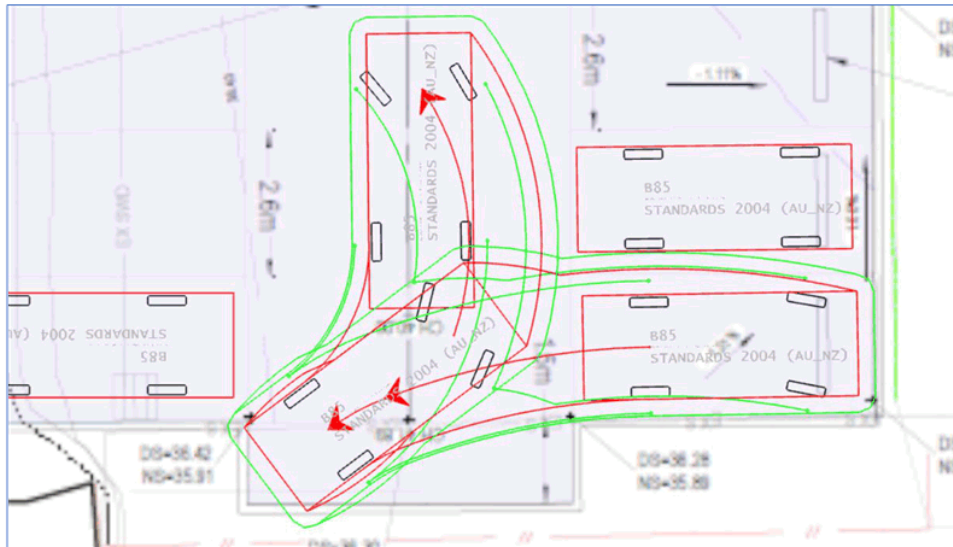


Swept path of a B85 vehicle entering parking spaces

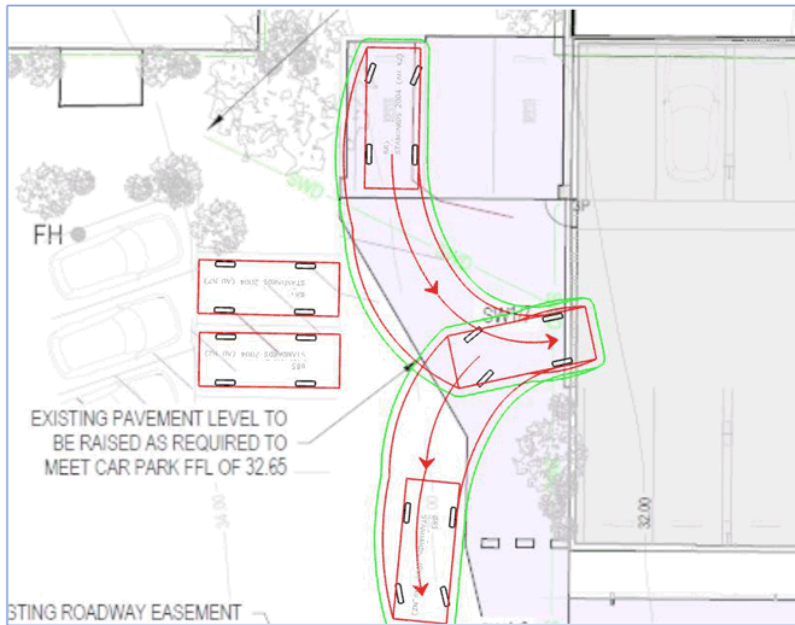


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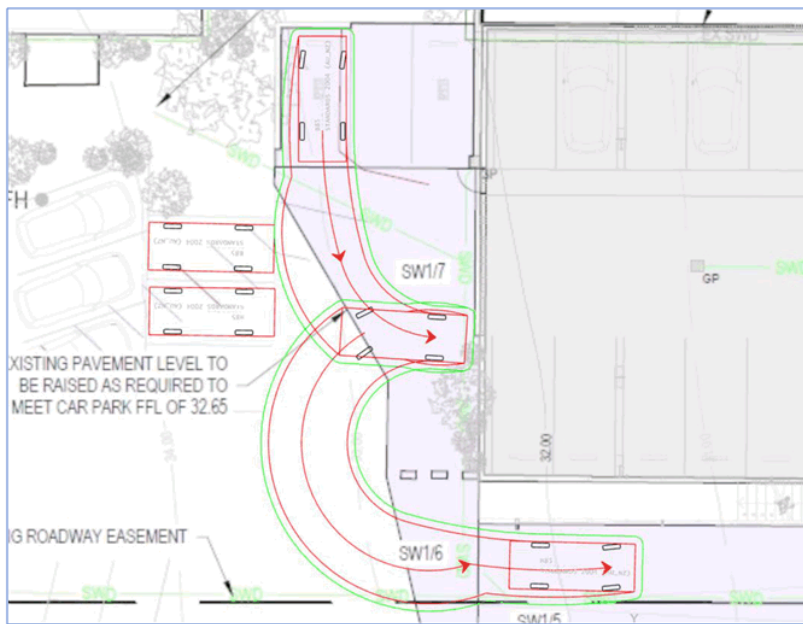
Swept path of B85 vehicle leaving the last parking space, using 1.5 metre blind aisle extension, with the kerbing adjacent to the spaces being less than 150 millimetres in height, allowing vehicle overhang.



Swept path of a B85 vehicle leaving the first accessible space (enclosed parking module door open)



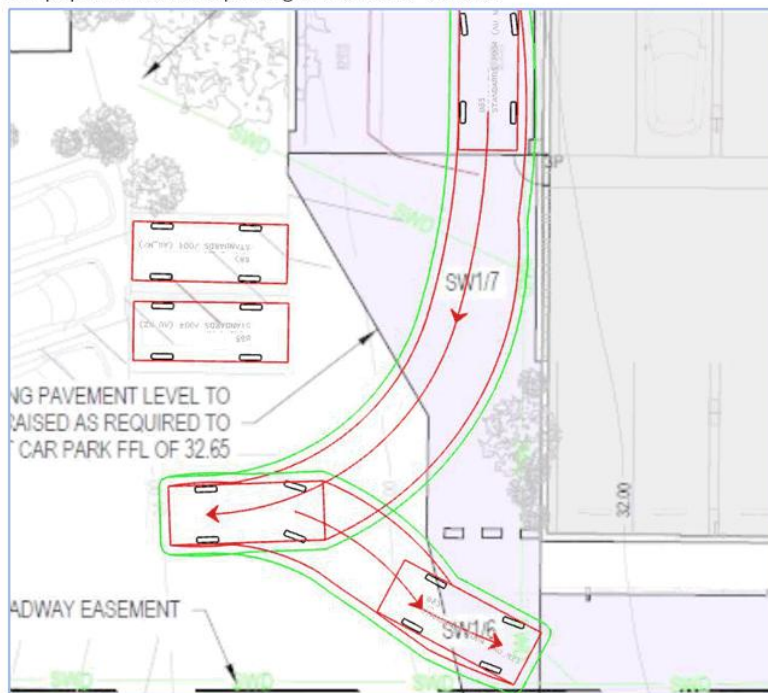
Swept path if enclosed parking module door is closed



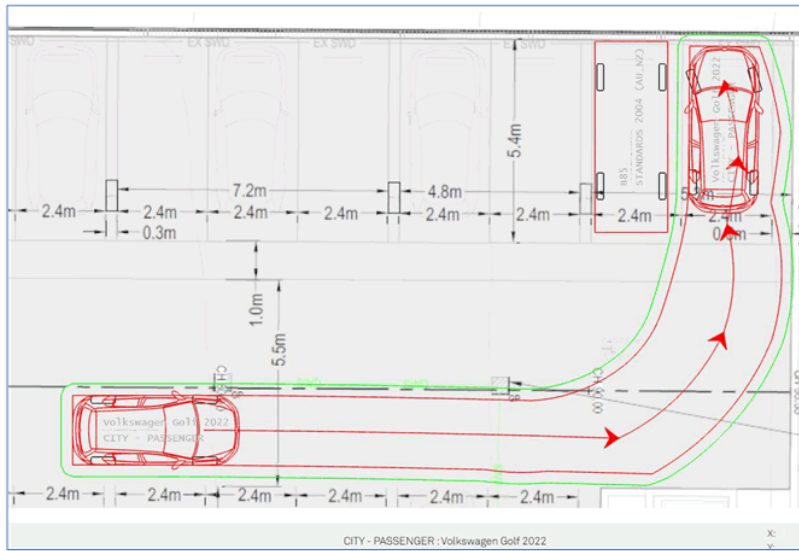
Swept path of a vehicle leaving the second accessible space (enclosed parking module door open)



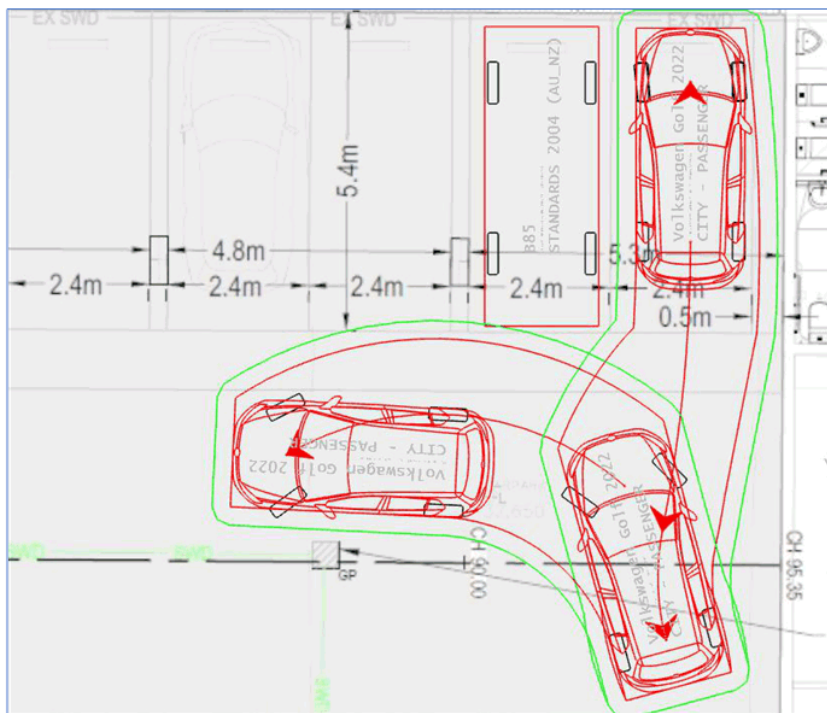
Swept path if enclosed parking module door is closed



Swept path of a small vehicle, (4.3 metres long and 1.8 metres wide) entering the end parking space



Swept path of a small vehicle, (4.3 metres long and 1.8 metres wide), leaving the end parking space



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14. Appendix C – Manual survey of Vodafone building

A parking survey was conducted on Wednesday, October 23, 2024, at the formerly named Vodafone building, to determine the transport modes staff used to arrive. This building is similar to the proposed development, as it provides on-site parking and office accommodation, and operates within close proximity of the Central Business District. It is within walking distance to high-frequency public transport services, surrounded by residential areas, and accessible by cycling.

The building is located at the corner of Argyle Street and Bathurst Street, with vehicles able to enter and exit from both streets, while pedestrians and cyclists can use three entrances: Argyle Street, Bathurst Street, and Liverpool Street. It was noted that pedestrians used the building to walk through and exit onto different streets.

The multi-storey building provides three levels of parking, with 184 spaces allocated to casual users, and this supply was fully occupied before 7:45 am, meaning that any vehicles arriving after that time were associated with the 144 permanent spaces.

The 90-minute survey commenced at 7:30 am and captured the following data:

- Total of 102 vehicles arriving, with 90% being single occupant (10% ridesharing).
- 4 individuals arrived using a bicycle (2%)
- 56 individuals arrived by walking to the site (33%)

This survey data clearly demonstrates that about 40% of users arrived using alternative transport modes, reducing the reliance on private vehicles.

Table 14.0A – Vehicles arriving and leaving

Time AM	Vehicles in				Vehicles out		
	Single occupant		Multi occupant	Total	Single occupant	Multi occupant	Total
	Argyle St	Bathurst St					
7:30 – 7:45	12	7	3	22	0	0	0
7:45 – 8:00	11	9	0	20	0	0	0
8:00 – 8:15	6	2	1	9	0	0	0
8:15 – 8:30	5	8	2	15	3	0	3
8:30 – 8:45	13	10	3	26	7	1	8
8:45 – 9:00	9	0	1	10	1	0	1
Total	56	36	10	102	11	1	12

Table 14.0B – Pedestrians and cyclists arriving

Time AM	Argyle St	Bathurst St	Liverpool St	Walk through	Total accessing the building
7:30 – 7:45	1	11	4	6	10
7:45 – 8:00	2 (1)*	12	10	12	12
8:00 – 8:15	1 (2)*	6 (1)*	5	4	8
8:15 – 8:30	4	8	4	7	9
8:30 – 8:45	2	7	2	4	7
8:45 – 9:00	1	6	9	6	10
Total	11 (3)*	50 (1)*	34	39	56 (4)*

*Cyclists are shown in brackets



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ENVIRONMENTAL SITE ASSESSMENT

65 Brisbane Street, Hobart, Tasmania

January 2025

For Brisbane Street Apartments Pty Ltd

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

DOCUMENT CONTROL

Title	Version	Author	Date Written	Reviewed By	Date Reviewed
<i>Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania</i>	Version 1	Sarah Joyce	8 th January 2025	Mark Downie	9 th January 2025

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

EXECUTIVE SUMMARY

This report presents the findings of an Environmental Site Assessment (ESA) undertaken by Geo-Environmental Solutions Pty. Ltd. (GES) at 65 Brisbane Street, Hobart, Tasmania - hereby referred to as 'The Site'. GES was commissioned by Brisbane Street Apartments Pty Ltd to conduct the site assessment. This ESA has been prepared by a suitably qualified and experienced person in accordance with the procedures and practices detailed in the National Environmental Protection [Assessment of Site Contamination] Measure 1999 as amended 2013 (NEPM ASC 2013).

The client has requested the ESA report for the potential future site redevelopment which includes a multi-level building with carparking, commercial premises on the lower levels and tenanted residential spaces on the upper levels. The objective of this ESA was to investigate the site for contamination, by addressing E2.6.2 P1 performance criteria P1 of the Hobart Interim Planning Scheme of 2015.

The following information was gathered during the desktop investigation:

- The site currently features a large workshop building, a historic building formally *Sydney Lodge* and a large car parking area. The investigation area and proposed footprint of the new building is a single level open-air, asphalt covered carpark. The proposed development is a new multi-story building featuring commercial use such as offices and carparking on the ground level and residential apartments on the upper levels. This ESA report was requested as part of the planning process for this site redevelopment.
- The site is zoned Commercial land use under the *Hobart Interim Planning Scheme of 2015*.
- The geology of the site is mapped as being underlain by Late Carboniferous - Triassic Upper Fluvio-lacustrine Sequence of quartz sandstone sequence. The elevation of the investigation area ranges from 26 to 32m above sea level and the site dips steeply to the southeast. The closest downgradient ecosystem receptors are Hobart Rivulet (contained in culverts) located approximately 400m southeast from the site, and the River Derwent at Constitution Dock which is located approximately 800m southeast from the site.
- The historical aerial photographs confirm that prior to 1946, the site housed a town house building on the Brisbane Street, street-front and the former *Sydney Lodge* at the rear of the property, by the 1960's this house on the street front was demolished and the area covered by asphalt and used for vehicle parking; the former *Sydney Lodge* remains on site to this day. Also, in the 1965's a large industrial shed, thought to be a vehicle workshop was constructed. This workshop has had several small out buildings added to it. It also remains on site today.
- The Environment Protection Authority Tasmania layers on the LIST were reviewed and there were no regulated premises and underground petroleum storage systems located either upgradient from the site or directly adjacent to the site.
- Contaminants of potential concern at the site include the following: total petroleum/recoverable hydrocarbons; mono aromatic hydrocarbons: (benzene, toluene, ethylbenzene, xylene, naphthalene); polynuclear aromatic hydrocarbons; and metals.

From the soil assessment, it is concluded that:

- Environment There were two samples, BH2 0.5-0.6 and BH3 0.1-0.2 that had ESL guideline limit exceedance for benzo(a)pyrene for commercial / industrial land use and the same two samples had an exceedance above the EILs for zinc; BH3 0.1-0.2 also had an EIL exceedance for copper. However, there is an absence of an ecological receptors at this location.
- Human Health For commercial/industrial land use guidelines, there were no human health guideline exceedances for dermal contact, dust inhalation and soil ingestion risk, vapour intrusion or trench worker specific guidelines. Therefore, no risk was identified for human receptors.
- Excavated Soil Management There were eight primary samples that returned a Level 1 Material (clean fill) classification. There were eight primary samples that returned a Level 2 Material (low level contaminated soil) classification for elevated levels of either arsenic, beryllium, chromium, cobalt, lead, manganese, mercury, nickel, zinc or benzo(a)pyrene. There was one primary sample BH6 0.5-0.6 that returned a Level 3 Material (contaminated soil) classification for elevated levels of manganese.
- Perched water was encountered in BH6 at 0.7m. A water sample was unable to be collected. Water was noted as clean in appearance and odour free.

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The following is recommended at the site:

- Environmental Risk Given exceedances of ESL guideline limits were observed, a Soil and Water Management Plan (SWMP) is required to minimise the movement of soil and sediment off-site during the construction phase. Correct implementation of the SWMP will mitigate the identified environmental risks.
- Excavated Material The soil classified on site ranges from Level 1 Material (Clean Fill) to Level 3 Material (contaminated soil). For any future excavation of soil at the site, future waste classification should be conducted in accordance with EPA Tasmania IB105.
- Groundwater If groundwater is encountered during the site redevelopment works and is suspected to be contaminated (observed odour, sheen or staining) then a suitably qualified and experienced person should conduct groundwater sampling in accordance with NEPM ASC 2013 to determine if there is a risk to human health or the environment.
- Statement of Suitability The findings from this investigation confirm that there is currently no risk to Human Health or the Environment as part of the planned works at the site providing that the above recommendations are followed.

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ABREVIATIONS

AEC	Areas of Environmental Concern
AHD	Australian Height Datum
ALS	Analytical Laboratory Services
ANZG	Australia and New Zealand Guidelines for Fresh and Marine Water Quality
AST	Above ground Storage Tank
B(a)P	Benzo(a)pyrene
BGS	Below Ground Surface
BH	Borehole
BTEXN	Benzene Toluene Ethylbenzene Xylene Naphthalene
COA	Certificate of Analysis
COC	Chain of Custody
COPC	Contaminant of Potential Concern
CRC CARE	Corporative Research Centre for Contamination Assessment and Remediation of the Environment
CSM	Conceptual Site Model
DQO	Data Quality Objectives
EOH	End Of Hole
EIL	Ecological Investigation Levels
ESL	Ecological Screening Levels
EPA	Environmental Protection Authority
ESA	Environmental Site Assessment
GDA94	Geocentric Datum of Australia 1994
GES	Geo-Environmental Solutions Pty. Ltd.
HIL	Health Investigation Levels
HSL	Health Screening Levels
IL	Investigation Levels
LOR	Limits of Reporting
MDL	Method Detection Limit
NATA	National Association of Testing Authorities
NEPM ASC	National Environmental Protection (Assessment of Site Contamination) Measure
NHMRC	National Health and Medical Research Council
NL	Non Limiting
NRMMC	Natural Resource Management Ministerial Council
PAH	Polycyclic Aromatic Hydrocarbons
PHC	Petroleum Hydrocarbons
PID	Photo-Ionisation Detector
PPA	Preferential (PVI) Pathways Assessment
PVI	Petroleum Vapour Intrusion
TPH	Total Petroleum Hydrocarbons
TRH	Total Recoverable Hydrocarbons

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UPSS	Underground Petroleum Storage Systems
USCS	Unified Soil Classification System
UST	Underground Storage Tank
VOC	Volatile Organic Compounds

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1 INTRODUCTION

1.1 General

This report presents the findings of an Environmental Site Assessment (ESA) undertaken by Geo-Environmental Solutions Pty. Ltd. (GES) at 65 Brisbane Street, Hobart, Tasmania - hereby referred to as 'The Site'. GES was commissioned by Brisbane Street Apartments Pty Ltd to conduct the site assessment. The site location in context of the greater Hobart area is presented in Figure 1.

This ESA has been prepared by a suitably qualified and experienced person in accordance with the procedures and practices detailed in the National Environmental Protection [Assessment of Site Contamination] Measure 1999 as amended 2013 (NEPM ASC 2013). Guidelines and key regulations and policies are identified in the References section of this document. Personnel engaged in preparing this ESA are listed in Appendix 1 along with their relevant qualifications and years of experience.

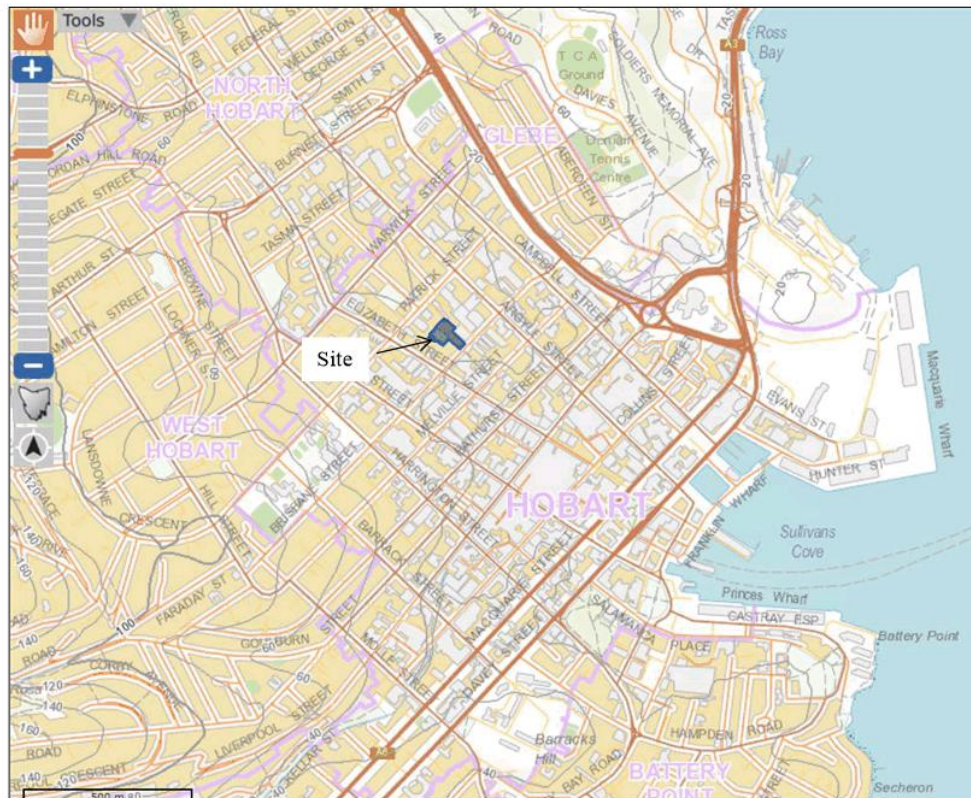


Figure 1 Site Location (Image source TheLIST)

1.2 Site Layout

An aerial image of the existing site layout and the investigation area is presented in Figure 2.

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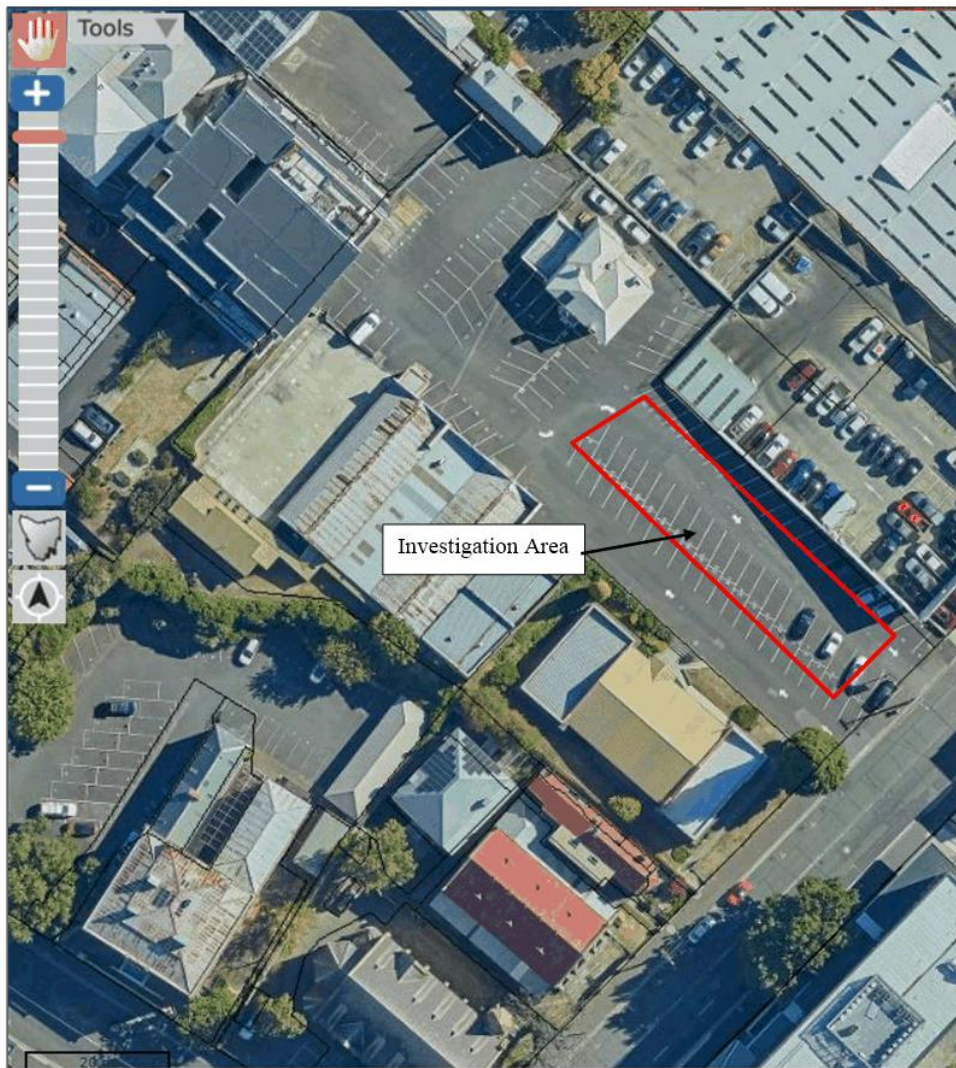


Figure 2 Existing Site Layout (Image source The LIST)

1.3 Investigation Objectives

The client has requested the ESA report for the proposed site redevelopment of a multi-level building with carparking, commercial premises and tenanted apartment spaces. The objective of this ESA was to investigate the site for contamination, by addressing performance criteria under the *Hobart's Interim Planning scheme 2015* for potential excavation. To assess the suitability and safety of the soil for excavation at a typical depth for potential foundation excavation, and any human or environmental risks that may be currently present in the soil.

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1.4 Scope of Works

The scope of work for this ESA was to conduct a desktop review and an invasive soil investigation at the site. Work included the:

- Desktop review including gathering data from the LIST and the aerial photo viewer.
- Drilling of six soil boreholes to collect 17 primary samples, which were selected for analysis.
- Samples were sent for analysis to ALS Global laboratory in Springvale Victoria which is a National Association of Testing Authorities (NATA) accredited laboratory.
- Analysis to determine the presence or absence, and if present the level of contamination of the site included Total Recoverable Hydrocarbons (TRH) Benzene Toluene Ethylbenzene Xylene Naphthalene (BTEXN), Polynuclear Aromatic Hydrocarbons (PAH), and a suit of fifteen (15) metals.
- Soil samples were sent with quality assurance/ quality control (QA/QC) samples including a duplicate split sample (DUPLIACTE), a trip blank sample (Trip Blank) and one rinsate blank sample (RINSATE).
- Results were compared against the relevant guidelines.
- A risk assessment, known as a Conceptual Site Model (CSM) was developed for the site; and
- Findings were presented in this ESA Report, detailing any specific onsite human health and environmental risks.

1.5 Site Details

Site details are presented in Table 1.

Table 1 Site Details

SITE LOCATION	65 Brisbane Street, Hobart, Tasmania.
TITLE REFERENCES	Title Reference 174045/1, Property ID 3537209.
INVESTIGATION AREA	The investigation area is the Brisbane Street front of the site in the footprint of the proposed commercial building.
SITE AREA	The site is 4722m ² and the proposed footprint of the proposed building is 700m ² .
SITE ELEVATION & GRADIENT	The Site is situated 26-31m above sea level and falls towards the southeast.
SITE SURFACING	The surface of the site is an asphalt carpark.
SITE OWNER	G & L Management Pty Ltd
PREVIOUS AND CURRENT LANDUSE	A commercial premises with a large shed that has been present on site since the 1960s, used for vehicle maintenance. The investigation area has been an asphalt carpark since that time.
SITE & SURROUNDING LAND ZONING	The site is zoned <i>commercial</i> .
PLANNING REQUIREMENTS	Potentially contaminated land code applies
PROPOSED LAND USE	Commercial Building with tenanted apartment spaces on the upper levels.

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2 PLANNING

2.1 Proposed Development

The client is proposing a multi-level building with carparking, commercial premises on the lower levels and tenanted residential apartment spaces on the upper levels. This plan will be submitted to the Hobart City Council through the Development Application process. The relevant building plans are presented in Appendix 2.

2.2 Hobart Interim Planning Scheme

The site is considered potentially contaminated land under the Hobart Interim Planning Scheme 2015 (HIPS) due to the historical uses of the site which housed a mechanics workshop and therefore the development application must include an assessment against the code because it includes excavation and ground disturbance.

2.3 Environmental Site Assessment

As there are proposed excavation works at the site, there are no acceptable solutions to proposed works, E2.6.2 P1 performance criteria P1 of the HIPS are to be addressed which includes

- (a) an environmental site assessment that demonstrates there is no evidence the land is contaminated;*
- (b) an environmental site assessment that demonstrates that the level of contamination does not present a risk to human health or the environment; or*
- (c) an environmental site assessment, including a plan to manage contamination and associated risk to human health and the environment, that includes:*
 - (i) any specific remediation and protection measures required to be implemented before excavation commences; and*
 - (ii) a statement that the excavation does not adversely impact on human health or the environment.*

2.3.1 Remediation and Protection Measures

If the ESA report concludes that remediation and/or protection measures are necessary to avoid risks to human health or the environment, a proposed remediation and/or management plan must be submitted. Any Remediation or management plan involving soil disturbance must include a detailed soil and water management plan to prevent offsite transfer of potentially contaminated soil or stormwater.

2.3.2 Statement of Suitability

A statement based on the results of the ESA that the excavation as part of the planned works will not adversely impact on human health or the environment is to be provided (subject to implementation of any identified remediation and/or protection measures as required).

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3 DESKTOP STUDY

3.1 Site Walkover

During the site walkover, photographs were taken and observations made with regards to current site conditions, relevant photographs are presented in Appendix 3. The current conditions of the investigation area were in line with the sites current use of vehicle parking. The surface of the investigation area is covered by an asphalt. There is the large workshop building on the northwestern side of the site, the historical Former *Sydney Lodge* is in the northeast portion of the site.

3.2 Site Zoning

The site is zoned Commercial under the Hobart Interim Planning Scheme of 2015, the investigation area is outlined in red in Figure 3. The land use surrounding the site is commercial, inner residential, central business and utilities. The proposed use of the site is a multi-level building with commercial landuses including carparking and offices on ground levels, and tenanted apartments spaces on upper levels.

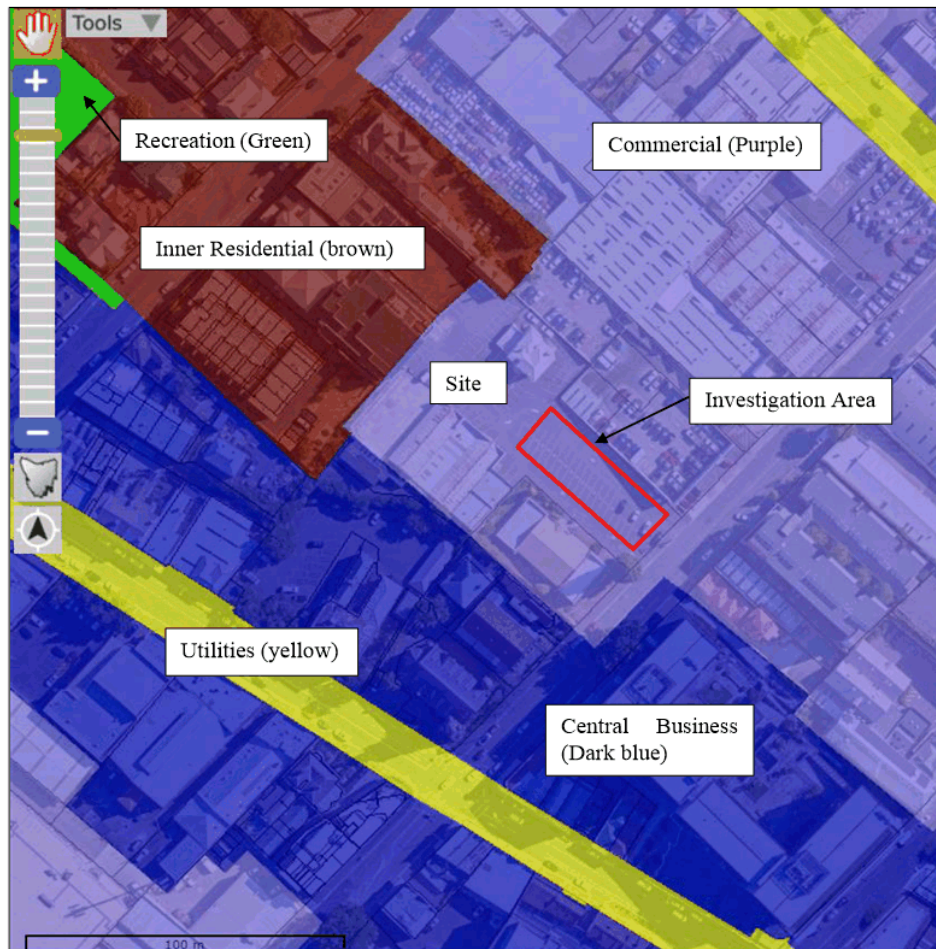


Figure 3 Hobart Councils Interim Planning Scheme Zones 2015 (Image source: The LIST)

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3.3 MRT Geology Mapping

The 1:25,000 scale geology map of the Greater Hobart area is shown in Figure 4. The mapping indicates that the site is underlain by Late Carboniferous - Triassic Upper Fluvialacustrine Sequence of Quartz Sandstone Sequence (Rq). The mapped units surrounding the site include;

R – Triassic/Perman Undifferentiated upper parmeener supergroup rocks. Late Carboniferous - Triassic Upper Fluvialacustrine Sequence of Quartz Sandstone Sequence

Rq – Late Carboniferous to Triassic sedimentary sequences, Parmeener Supergroup, Upper Parmeener Supergroup, Upper Fluvialacustrine Sequence - Quartz Sandstone Sequence (Rq)

Jd – Jurassic dolerite -Dolerite (tholeiitic) with locally developed granophyre.

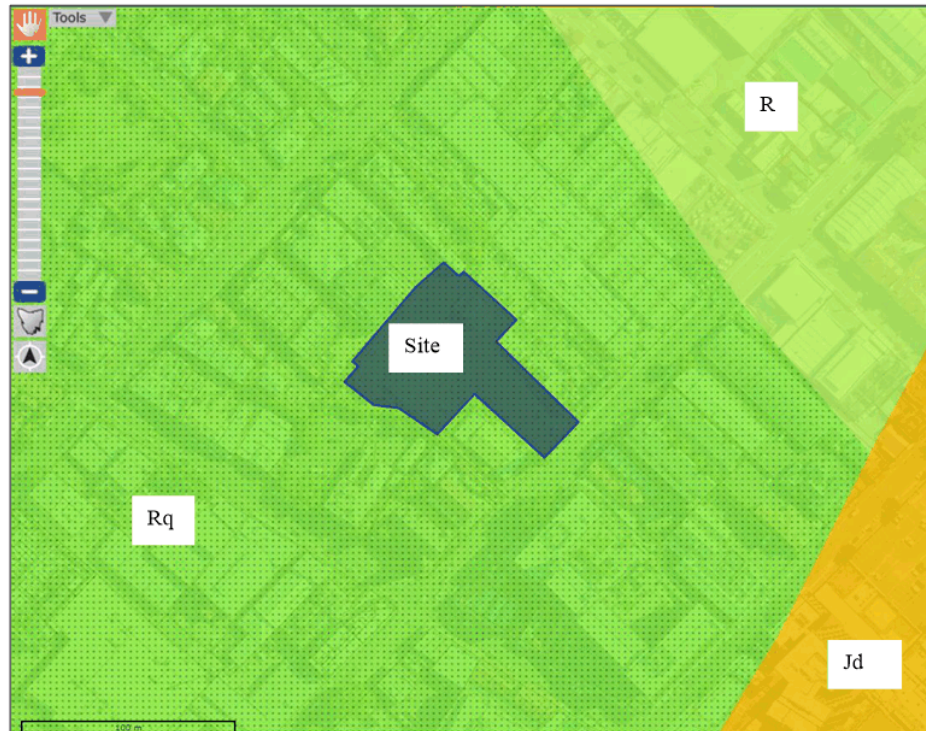


Figure 4 Mineral Resources Tasmania 1:25,000 Scale Mapping (Image source: The LIST).

3.4 Historical Aerial Photography Interpretation

The 2018-2019, 2011, 1982, 1973, 1973 (close up), 1965, 1965 (close up), 1957 and 1946 historical aerial photographs were viewed as part of this ESA, along with illustrations from Sprent's Book mosaic of Hobart c1845. The historical photographs are presented in Appendix 4 and summarised in Table 2.

In summary, the following observations on site were made from the photographs.

- The long strip on Brisbane Street (the investigation area) was originally granted to William Foord.
- Prior to 1946, there was town house building on the Brisbane Street street-front of the site and a historical house, formally *Sydney Lodge* on the northeastern part of the site, this building is still present in January 2025.
- By 1965 the Brisbane Street, street-front residential building had been demolished, the investigation area was converted into a carpark / thoroughfare. The surface of the site appears to be asphalt. The northwest corner of the site has a large industrial shed. Two other small sheds are also present. Multiple vehicles on site, suggesting that a mechanical workshop is operating.

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- By 1973 a small extension was added to the new shed. House at the rear of the property remains. By 1982 the extension on the main shed was again added to and by 2011 the old free standing shed was removed, new parking lines painted and a new building behind the main shed which was present by 1990.

Off site the following observations have been made:

- In 1946 there is a residential property at 47-53 Brisbane Street. By 1957 this property houses a commercial building / warehouse which is now currently car sales room. By 2011 the building was redeveloped to include parking on the roof.
- The property behind 47-53 Brisbane Street, which is part of 45 Brisbane street houses untidy vehicle storage, likely wrecked vehicles. By 1982 the surface of this property was tidied up and by 2011 the surface to the site was concreted.
- Properties to the south-west house historical buildings that are not thought to house potentially contaminating activities which include a Chapel, apartments, a Scholars Hall and the First Church of Christ Scientist.

Table 2 Historical Aerial Photograph Review

Photo	Observations
c1845	<ul style="list-style-type: none"> • There was a long strip on Brisbane Street (the investigation area) which was granted to William Foord. 1797-1850. (Sprents Book Mosaic). According to Groups (accessed Jan 2025); He was a building contractor who superintended the construction of St. Luke's Church at Bothwell and was also Clerk of Works during the construction of the new Government House in Hobart Town.
1946	<ul style="list-style-type: none"> • <u>On site</u>: The site appears to be a residential building, the street-front appears to be divided into lots suggesting it is a town house arrangement with multiple occupants. There is a second historical house at the rear of the property, northeastern side. • <u>Off site</u>: 47-53 Brisbane Street residential.
1957	<ul style="list-style-type: none"> • <u>On site</u>: Site unchanged. Still present in 1958. • <u>Off site</u>: 47-53 Brisbane Street commercial building / warehouse. Currently car sales room.
1965	<ul style="list-style-type: none"> • <u>On site</u>: residential building demolished, investigation area is a carpark / thoroughfare. The surface of the site appears to be asphalt. The northwest corner of the site has a large industrial shed. Two other small sheds are also present. Multiple vehicles on site. • <u>Off site</u>: Vehicle storage at 45 Brisbane street, at the rear of 47-53 Brisbane Street. Messy and untidy, could be wreckers.
1973	<ul style="list-style-type: none"> • <u>On site</u>: A small extension was added to the new shed. House at the rear of the property remains. • <u>Off site</u>: Properties on Patrick Street (up gradient) cleared for site redevelopment. The property at 45 Brisbane Street, is unchanged.
1982	<ul style="list-style-type: none"> • <u>On site</u>: The extension on the main shed was again added to. • <u>Off site</u>: The property at 45 Brisbane Street, has been tidied up.
2011	<ul style="list-style-type: none"> • <u>On site</u>: Old free standing shed removed. New parking lines. New building behind the main shed (present by 1990). • <u>Off site</u>: The building at 47-53 Brisbane Street has been redeveloped and now has a flat roof that is used for carparking and car maintenance or vehicle washing. The surface of property at 45 Brisbane Street, has been concreted.
2018-2025	<ul style="list-style-type: none"> • <u>On site</u>: Site Unchanged. • <u>Off site</u>: To the south: 67 Brisbane Street – First Church of Christ Scientist, Hobart; 71 Brisbane Street Serviced apartments (71), Scholars Hall Hobart (2/71A) and Unit (3/71A), 73 Brisbane Street A Chapel. None of the activities on these sites are considered to host potentially contaminating activities.

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3.5 Previous Site Investigations

At the time of reporting GES was unaware of any previous site investigations at the site.

3.6 Environmental Protection Authority

The Environmental Protection Authority (EPA) Regulated Premises and Underground Petroleum Storage Systems layers on The LIST were consulted; one abandoned UPSS (green pin) and five permanently decommissioned UPSS (black pins) were identified within 350m radius of the site as illustrated in Figure 5, details are below. None of these systems are considered likely to impact the soil or groundwater at the site.

Abandoned UPSS: At the site: 37-47 Melville Street. Located 114.5m down gradient from the site.

Permanently Decommissioned UPSS:

- 98-110 Argyle Street – Located 110m down gradient from the site.
- 157 Elizabeth Street, Hobart – Located 115m down gradient from the site.
- 98 Campbell Street - Located 115m down gradient from the site.
- 'Temple House' 55 Argyle Street, Hobart – Located 350 down gradient from the site.
- 171 Argyle Street– Located 350 upgradient from the site, in a separate water catchment area.

None of the identified UPSS are considered to have an impact on the site.



Figure 5 EPA regulated premises and registered UPSS (The LIST)

3.7 Hobart City Council

The Hobart City Council (HCC) is likely to consider the site to be potentially contaminated land because on the northwestern corner of the site houses a vehicle maintenance workshop.

3.8 WorkSafe Tasmania

It was not deemed necessary to request a WorkSafe Tasmania (WST) Dangerous Goods File because historical use of the investigation area was originally residential and then has only housed an asphalt carpark from the 1960s.

3.9 Acid sulfate soils

According to the Land Information Service Tasmania (LIST) database, the soils at the site are not mapped as Potentially Acid Sulfate Soils (PASS). The closest mapped Low Probability PASS is 85m south and downgradient from the site, see Figure 6, with Low Probability PASS illustrated in orange. Therefore, the site is not considered to house any PASS.

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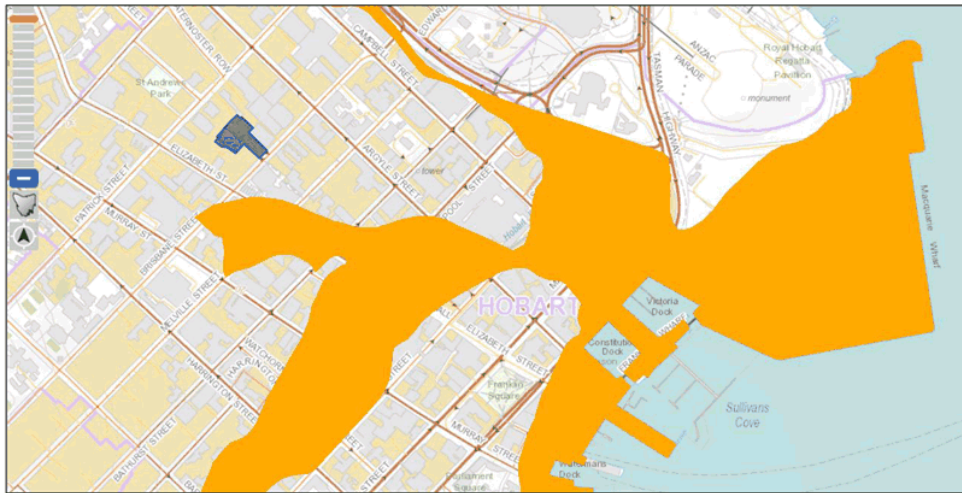


Figure 6 Coastal Acid Sulfate Soils (0-20m AHD) (The List, 2025)

3.10 Geomorphology

The Lidar Hillshade Basemap layer for the site is shown in Figure 7, the investigation area is shown with a red border. The image shows that the area of the investigation as has had very little to no surface disturbance, in the areas where building buildings are present, cuttings for the footprint can be seen. It is likely that building rubble was spread across the site post demolition at the front of Brisbane Street.



Figure 7 Hillshade Colour Basemap (sourced from The LIST)

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3.11 Drainage & Hydrogeology

The elevation of the investigation area ranges from 26 to 32m above sea level and the site falls steeply to the southeast. The surface of the site has been modified and the surface covered in asphalt. Therefore, given the modification to the site surface and the urban setting, surface water is expected to be channelled into stormwater drain systems which feeds into Hobart Rivulet and the River Derwent. The hypothetical groundwater flow direction is inferred in Figure 8 and Figure 9.

3.12 Groundwater

3.12.1 Potential Up-Gradient Contamination Sources

The workshop on site may have housed potentially contaminating activities. Any potential contamination from that building is likely to travel in a southerly direction and is unlikely to impact the proposed development area. Refer to Figure 9.

3.12.2 Downgradient Ecosystem Receptors

The closest downgradient ecosystem receptors are the receiving waters Hobart Rivulet (which is contained in a culver) located approximately 400m southeast from the site, and the River Derwent at Constitution Dock which is located approximately 800m southeast from the site.

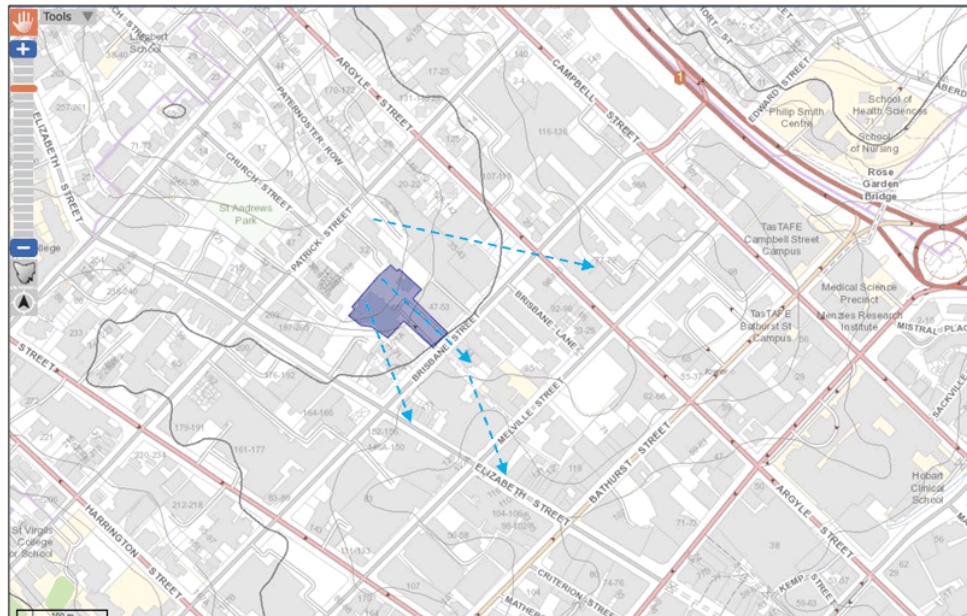


Figure 8 5m Contour Elevations and Inferred Surface and Groundwater Flow Direction (image source the LIST)

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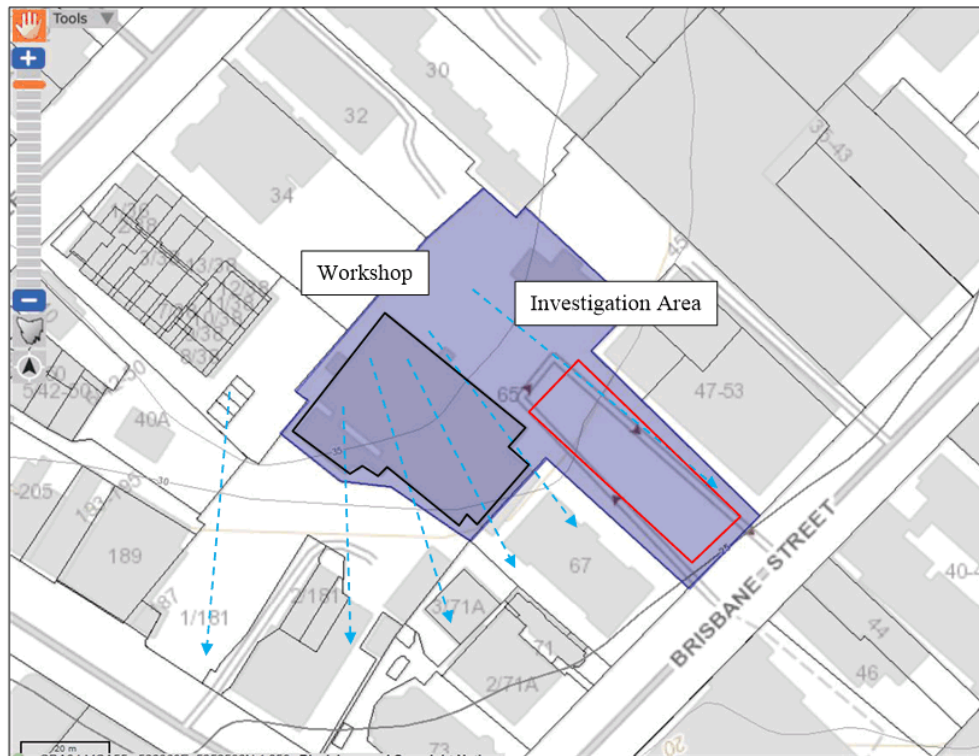


Figure 9 Close Up of the 5m Contour Elevations and Inferred Surface and Groundwater Flow Direction (image source the LIST)

3.13 Potential Contamination Issues

3.13.1 Areas of Potential Concern

Given the urban setting, and the long commercial and industrial uses of the site and surrounding properties, the entire site is an area of potential concern.

3.13.2 Contaminants of Potential Concern

Potential contaminants of potential concern (COPC) that have been considered include a suite of common urban contaminants suitable for Hobart:

- Petroleum Hydrocarbons: (including Total Petroleum/Recoverable Hydrocarbons (TPH/TRH),
- Mono Aromatic hydrocarbons: Benzene, Toluene, Ethylbenzene, Xylene, Naphthalene (BTEXN),
- Polynuclear Aromatic Hydrocarbons (PAHs), and
- Metals.

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4 FIELD INVESTIGATION PROCEDURES

4.1 Works Summary

Site works are summarised in Table 3 and Figure 10. Site photographs are shown in Appendix 3. Site investigation works comprised of drilling with a Geoprobe direct push drill rig at 6 bore hole locations and collecting representative soil samples. A total of 17 representative primary soil samples were collected and selected for analysis. Excess material was placed back into the holes.

Table 3 Summary of Site Investigation Work Dates

Scope	Data	Lab Report	Details
Drilling and sampling	4 th December 2024	EM2421460	A total of 6 soil bores were drilled, a total of 17 primary soil samples were collected and selected for analysis plus three QC samples were collected.



Figure 10 Borehole Plan

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4.2 Soil Investigation

4.2.1 Borehole Drilling

A total of six boreholes were drilled for assessing site geology and sampling potential contamination impact. Soil sampling was conducted per the National Environmental Protection [Assessment of Site Contamination] Measure (NEPM ASC 2013) and AS4482 sampling guidelines. Table 4 presents a summary of the soil assessment methodology adopted at the site.

Table 4 Summary of Soil Sampling Methods

Activity	Details / Comments
Sampling Methods	Geoprobe direct push drilling.
Soil Logging	Logging the soil was conducted in accordance with the Unified Soil Classification System (USCS) as detailed in AS1726 (1993).
Decontamination of Sampling Equipment	Quantum Clean Laboratory Detergent (R213) was used to decontaminate reusable sampling equipment between each borehole sampling location. Fresh liners were used in the Geoprobe direct push sampling system between each location.
Soil Screening	In accordance with AS4482.2, individual soil samples were collected at 0.5 intervals below ground surface (bgs) and/or change in geology. Soil screening was not conducted as there was no soil odours discernable.
Laboratory Soil Sample Collection	In accordance with AS4482.2. All samples were collected using disposable nitrile gloves. Samples were collected for laboratory analysis where possible at the following intervals 0.1-0.2, 0.5-0.6, & 1.0-1.1 below ground surface.
Sample preservation	Samples were placed into laboratory provided collection jars for analysis. Soil jars were placed in a pre-chilled eski with ice bricks.
Sample holding times	Sample holding times were within acceptable range (based on NEPM ASC B3-2013) from collection to extraction for the primary analysis. Samples arrived at the laboratory at 7.8°C. Ideally, they would have arrived at 6 °C or below.

4.2.2 Soil Analysis

Chain of Custody (COC) and Sample Receipt Notification (SRN) documentation is provided in Appendix 5. Table 5 presents a summary of the laboratory analyses undertaken. The primary samples and QC samples were submitted to Analytical Laboratory Services (ALS), Springvale, Melbourne for analysis. All 17 primary samples were selected for analysis.

Table 5 Overview of Soil Analysis and Quality Control

Analytes	Primary Samples	Duplicate ^a	Rinse Blank ^b	Trip Blank ^c
TPH	17	1	1	1
BTEXN	17	1	1	1
PAH	17	1	1	1
Suite 15 Metals	17	1	1	1

Sampling Quality Control Standards (AS4482):

a - Duplicate one (1) in twenty (20) primary samples

b- Single rinse sample per piece of equipment per day

c- Trip blank one per eski

Given metals were analysed, there was a requirement to assess the following soil physical properties to determine soil threshold investigation levels: Soil grain class (sand/silt or clay); % Clay content; Cation exchange capacity (CEC); and Soil pH. The soil physical properties were based on knowledge of similar soil types encountered around the Greater Hobart area.

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5 QUALITY CONTROL

All field and laboratory Quality Assurance and Quality Control (QA/QC); Quality Control Report (QC) and Quality Control Interpretive Report (QCI) details and outputs are presented in Appendix 6.

5.1 Field

It is standard to expect up to 10% error in field duplication and up to 10% laboratory error. Therefore, in theory up to 20% error can be assumed on duplicate analysis. Some variation may exist in soil and groundwater because even though all efforts are made to split samples homogeneously, fragments of materials may bias samples in certain elements.

Relative Percentage Differences (RPDs) for the duplicate samples where applicable are calculated using the method outlined below.

The acceptance criteria used for the RPDs depend on the levels of contaminants detected and the laboratory's Method Detection Limits. The closer the levels detected are to the MDL the greater the acceptable RPD. RPDs are calculated as follows:

- RPD <50% for low level results (<20 * MDL)
- RPD <30% for medium level results (20-100 * MDL)
- RPD <15% for high level results (>100 * MDL)
- No limit applies at <2 * MDL (Method Detection Limit)

Field QA/QC procedures and compliance are summarised in Table 6.

Table 6 Soil Field QA/QC procedures and Compliance

QA/QC Requirement	Compliance	Comments
Appropriate sampling strategy used and representative samples collected	Yes	Sampling program was undertaken in accordance with AS4482.1-2005.
Appropriate and well documented sample collection, handling, logging and transportation procedures.	Yes	Appropriate and well documented.
Decontamination	Yes	Appropriate decontamination such as cleaning tools before sampling and between sample locations was undertaken and fresh liners were used in the Geoprobe direct push sampling system between each location.
Chain-of-custody documentation completed	Yes	COC were completed in accordance with NEPM ASC Schedule B2, Section 5.4.5 and transported under strict COC procedures. The signed COC documents are included in this report, which includes the condition report on arrival of samples to the Laboratory, cross checking of sample identification and paperwork and preservation method.
Required number of splits: Duplicate & inter-lab splits: 1 per 20 primary samples	Yes / No	One duplicate sample was collected and tested, for 17 primary sample collected as per AS4482.1-2005. No inter-laboratory split sample was collected.
QA/QC samples reported RPD's within indicated MDL guidelines.	Yes	For BH2 0.1-0.2 and Duplicate pairs, there were no non-compliances.
Required numbers of rinse blank samples collected with no laboratory detections?	Yes	One rinse blank was collected as per AS4482.1-2005. There were no detections of contaminants in the rinsate sample.
Trip blanks collected with no laboratory detections?	Yes	According to AS4482.2-1999, soil trip blanks are only required where volatile hydrocarbons are likely. One trip blank was collected. There were no detection of contaminants in that sample.
Field blanks collected with no laboratory detections?	NA	According to Australian Standards, there is no requirement to collect field blanks, unless there is concern with cross contamination risks. No field blanks were collected.
Samples delivered to the laboratory within sample holding times and with correct preservative	Yes / No	All primary samples were sent to the laboratory within holding times and correct preservative. Samples did arrive at the laboratory at 7.8 degrees Celsius (°C), ideally, they should arrive 6 °C or below.

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5.2 Laboratory

Soil laboratory QA/QC procedures and compliance are summarised in Table 7.

Table 7 Soil Laboratory QA/QC Procedures and Compliance, QCI Report EM2316193

QA/QC Requirement	Compliance	Comments
All analyses NATA accredited	Yes	ALS Laboratories is NATA Accredited. Appropriate analytical methods used, in accordance with Schedule B(3) of the NEPM ASC 2013. Acceptable laboratory limits of reporting (LORs) adopted.
Method Blanks: zero to <Practical Quantitation Limit (PQL)	Yes	There were no method blank value outliers in the QCI report.
Laboratory Control Samples: 70% to 130% recovery for soil.	Yes	There were no laboratory control outliers in the QCI report.
Matrix spikes: 70% to 130% recovery for organics or 80%-120% recovery for inorganics	Yes	There were no matrix spike outliers in the QCI report.
Duplicate Samples: 0% to <20% RPD.	No	Soil: Duplicate RPD's for BH1 1.0-1.1 for Cobalt (95.8%) and Manganese (71.6%) were above LOR limit of 20%. Rinsate Water: Duplicate RPD's for Anonymous sample for C6-C9 (44.0%) and C6-C10 (44.8%) were above LOR limit of 20%.
Surrogates: 70% to 130% recovery	Yes	There were no surrogate recovery outliers in QCI report.
Analysis holding time outliers	Yes	There were no hold time outliers in the QCI report.
Quality Control Sample Frequency Outliers	No	Water. Rinsate – laboratory duplicates for PAH/Phenols (0.00%) and TRH semivolatle fraction (5.88%) did not meet NEPM ASC 2013 B3 or ALS QC Standard of 10.00%. Matrix spikes for PAH/Phenols (0.00%) did not meet NEPM ASC 2013 B3 or ALS QC Standard of 5.00%.

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6 FIELD INVESTIGATION FINDINGS

6.1 Soil Bores

6.1.1 Geological Interpretation

The geology of the site is consistent with the geological mapping of the area of Triassic / Permian deposits, of quartz rich sandstone. The material encountered for each borehole is presented in Table 8. Most of the material encountered during the drilling was fill of Sandy CLAY and Clayey SAND with some gravels.

Table 8 Material Encountered at the Site

BH	From	To	Description	USCS
BH1	0	0.2	FILL: Gravelly SAND, dark grey / light brown, very dense	GP
BH1	0.2	0.5	Sandy CLAY, DG/DB, moist firm with gravels	CH
BH1	0.5	1.2	Sandy CLAY low plastic, light brown / light grey, mottled, firm with gravels	CL
			Refusal on rock	
BH2	0	0.3	FILL: Gravelly SAND, dark grey / light brown, very dense	GP
BH2	0.3	0.5	Sandy CLAY, DG/DB, moist firm with gravels	CH
BH2	0.5	1.5	Clayey SAND, LB, moist, stiff	CH
BH2	1.5	1.6	Sandy CLAY low plastic, light brown / light grey, mottled, firm with gravels	CL
BH2			Refusal on rock	
BH3	0	0.2	FILL: Gravelly SAND, dark grey / light brown, very dense	GP
BH3	0.2	0.6	Sandy CLAY, DG/DB, moist firm with gravels	CH
BH3	0.6	1	Silty CLAY h plastic, light brown, moist, stiff	CH
BH3	1	1.7	Sandy CLAY low plastic, light brown / light grey, mottled, firm with gravels	CL
BH3			Refusal on rock	
BH4	0	0.2	FILL: Gravelly SAND, dark grey / light brown, very dense	GP
BH4	0.2	0.5	Sandy CLAY, DG/DB, moist firm with gravels	CH
BH4	0.5	1	Silty CLAY h plastic, light brown, moist, stiff	CH
BH4	1	1.2	Sandy CLAY low plastic, light brown / light grey, mottled, firm with gravels	CL
BH4			Refusal on rock	
BH5	0	0.2	FILL: Gravelly SAND, dark grey / light brown, very dense	GP
BH5	0.2	0.5	Sandy CLAY, DG/DB, moist firm with gravels	CH
BH5	0.5	1	Silty CLAY h plastic, light brown, moist, stiff	CH
BH5	1	1.8	Sandy CLAY low plastic, light brown / light grey, mottled, firm with gravels	CL
BH5			Refusal on Gravels	
BH6	0	0.2	FILL: Gravelly SAND, dark grey / light brown, very dense	GP
BH6	0.2	1	Gravelly CLAY, LB moist to wet, very dense	GC
BH6			perched water at 0.7m; Refusal on gravels	

6.1.2 Grain & Depth Class Interpretation

Grain size classifications are applied to all soils at the site to determine threshold screening level concentrations for hydrocarbons (and chromium) to assess soil ecological and human health risks.

Grain class threshold values are determined based on either the:

- sample grain size (in the case of ecological screening levels or chromium limits); or
- average grain class overlying the sample point (when assessing petroleum vapour screening levels) relative to the proposed finished floor level.

Table 9 provides a summary of the grain class averages for material overlying the sample.

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Table 9 Summary of Grain Class Based on USCS Classification

Sample	Footing Excavation Depth [^] - Red Fill Thickness [^] - Green	Sample PVI Depth (m) Relative to Slab/Cut Depth	Soil Grain Size Class Averaging Above Soil Sample													Attenuation			Petroleum Vapour Intrusion HSL Grain Class*	SAMPLE USCS			
			GW	GP	GM	GC	SW	SP	SM	SC	ML	CL	OL	MH	CH	OH	CI	Rock (R)			Existing Pavement (P)	Crawl Space Thickness (m)	Proposed CONCRETE (CH)
BH1 0.1-0.2	0.5	0.2	0.0															NA	0.2	1.0	1.0	CLAY	GP
BH1 0.5-0.6	0.5	0.5	0.0											0.3				NA	0.2	1.0	1.0	CLAY	CL
BH1 1.0-1.1	0.5	1.0	0.0							0.5				0.3				NA	0.2	1.0	1.0	CLAY	CL
BH2 0.1-0.2	3.5	<																NA	0.2	1.0	1.0	CLAY	GP
Duplicate	3.5	<																NA	0.2	1.0	1.0	CLAY	GP
BH2 0.5-0.6	3.5	<																NA	0.2	1.0	1.0	CLAY	CH
BH2 1.0-1.1	3.5	<																NA	0.2	1.0	1.0	CLAY	CH
BH3 0.1-0.2	6.5	<																NA	0.2	1.0	1.0	CLAY	GP
BH3 0.5-0.6	6.5	<																NA	0.2	1.0	1.0	CLAY	CH
BH3 1.0-1.1	6.5	<																NA	0.2	1.0	1.0	CLAY	CL
BH4 0.1-0.2	6.5	<																NA	0.2	1.0	1.0	CLAY	GP
BH4 0.5-0.6	6.5	<																NA	0.2	1.0	1.0	CLAY	CH
BH4 1.0-1.1	6.5	<																NA	0.2	1.0	1.0	CLAY	CL
BH5 0.1-0.2	3.5	<																NA	0.2	1.0	1.0	CLAY	GP
BH5 0.5-0.6	3.5	<																NA	0.2	1.0	1.0	CLAY	CH
BH5 1.0-1.1	3.5	<																NA	0.2	1.0	1.0	CLAY	CL
BH6 0.1-0.2	0.5	0.2	0.0															NA	0.2	1.0	1.0	CLAY	GP
BH6 0.5-0.6	0.5	0.5	0.0	0.3														NA	0.2	1.0	1.0	SAND	GC

Footnotes:

* Grain class is modified based on proposed building construction: concrete is interpreted to have similar vapour intrusion properties to clay and is therefore designated as CLAY within the grain size averaging assessment; backfill is inferred to comprise of gravel (GW)

< Sample has been collected from above the proposed excavation (base of slab or proposed ground level) and is not relevant in PVI risk assessment

^ Excavation depths are approximate and may vary due to change in services depths or overall building/footing construction design

6.1.3 Soil Contamination Observations

During the site walkover, photographs were taken and observations made with regards to current site conditions, see photographs in Appendix 3. The surface of the investigation area is covered by an asphalt. There is the large workshop building on the northwestern side of the site, the historical Former Sydney Lodge is in the northeast portion of the site.

There was no surface staining or hydrocarbon odours observed in the investigation area.

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7 SOIL ECOLOGICAL IMPACT ASSESSMENT

7.1 Protected Environmental Values

The requirement for protecting soil from contaminated activities in Tasmania is managed under the Environmental Management and Pollution Control Act 1994 (EMPCA) which states in Part 5A:

(2) An area of land is a contaminated site if –

(a) there is in, on or under that area of land a pollutant in a concentration that –

(i) is above the background concentration; and

(ii) is causing or is likely to be causing serious or material environmental harm or environmental nuisance, or is likely to cause serious or material environmental harm or environmental nuisance in the future if not appropriately managed;

Potential soil impact at the site is assessed through application of the following environmental investigation guidelines.

7.2 NEPM ASC (2013) Guidelines

The following ecological investigation guidelines are to be addressed in order to assess acceptable levels of risk to terrestrial ecosystems:

- NEPM ASC 2013: Ecological Investigation Levels (EILs) – have been developed for selected metal and organic substances. EILs depend on specific soil and physicochemical properties and land use scenarios and generally apply to the top two (2) metres of the soil profile (NEPM ASC 2013);
- NEPM ASC 2013: Ecological Screening Levels (ESLs) – have been developed for selected petroleum hydrocarbon compounds and total petroleum hydrocarbon fractions. ESL's broadly apply to coarse- and fine-grained soils and various land use scenarios within the top two (2) metres of the soil profile.

Soil analytical results are compared against ESLs and EILs limits presented in Table 10.

Table 10 Summary of Soil Contaminates Considered as part of this investigation, based on NEPM ASC 2013

Investigation Levels (IL)	Analytes Investigated						
	Hydrocarbons				Metals		DDT
	BTEX	TRH (F1 to F4)	Benzo(a) pyrene (PAH)	Naphthalene (PAH)	Zn, Cu, Cr (III), Ni & As	Lead	
ESLs	Analysed	Analysed	Analysed				
EILs				Analysed	Analysed	Analysed	Not Analysed

7.3 Guidelines

7.3.1 Ecological Screening Levels

The following compounds were compared against NEPM ASC 2013, ESLs:

- BTEXN
- F1 to F4 TRH and
- Benzo(a)pyrene (PAH)

Selection of ESL threshold investigation limits are set out in the NEPM ASC 2013 guidelines and require classification of the soil according to:

- Land use sensitivity:
 - Areas of ecological significance

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- Urban residential and public open space; and
 - Commercial and industrial.
- Dominant particle size passing through a 2 mm sieve into:
 - Coarse – sand sizes and greater; and
 - Fine – clay and silt sizes.

Adopted NEPM ASC 2013 guidelines for soil and land use classifications are presented below.

7.3.2 Ecological Investigation Levels

The following compounds were compared against EILs:

- Arsenic, Chromium, Copper, Lead, Nickel, Zinc and Naphthalene

There was a requirement to classify the soil according to physicochemical properties given that the above listed compounds. Adopted physicochemical parameters are presented in the results tables.

Selection of EIL threshold investigation limits are set out in the NEPM ASC 2013 guidelines and require classification of the soil per specific soil and physicochemical properties which are presented in the results tables. The adopted land use scenarios presented in Table 11.

Table 11 Adopted Land Use Scenario for the Soil Bores

Land Use Scenario	Applicable Soil Bores
Areas of Ecological Significance	
Urban Residential & Public Open Space	<i>The proposed upper level of the building is residential but there will be no soil access for residents, therefore no assessment required.</i>
Commercial & Industrial	<i>Guidelines for Commercial and Industrial land use were applied to all borehole samples because the ground floor levels will be either carparking or commercial spaces for assessment purposes is commercial.</i>

Based on a preliminary assessment of site soil conditions, the following physicochemical properties are applied to assess guideline EILs:

- Clay content consistent with field observations
- A soil pH and cation exchange capacity (CEC) consistent with Table 12.

Table 12 Cation Exchange and Clay content, Adopted For the Site

Soil Physicochemical Properties			
USCS	Clay %	CEC	pH
R	100	10	4.5
GW	0	10	4.5
GP	0	10	4.5
GM	10	15	4.5
GC	30	20	4.5
SW	0	10	4.5
SP	0	10	4.5
SM	10	15	4.5
SC	20	20	4.5
ML	30	20	4.5
CL	100	35	4.5
OL	50	35	4.5
MH	30	35	4.5
CH	100	45	4.5
OH	100	60	4.5
PT	100	80	4.5
P	0	0	4.5
CM	100	35	4.5
CI	100	35	4.5
Rock	0	10	4.5

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7.4 Findings

7.4.1 Ecological Screening Levels

Laboratory analytical results are presented in Appendix 7. Table 13 compares soil analytical results against relevant NEPM ASC (2013) ESLs for commercial and industrial land use. Concentrations which exceeded laboratory limit of reporting (LOR) are highlighted in bold, ESL exceedances are highlighted with a coloured cell, and samples within a potential excavation zone for any future potential excavations are marked with an X. Of the 17 primary samples, there were four samples that had a range of detections for TRH fractions. There were two samples, BH2 0.5-0.6 and BH3 0.1-0.2 that had ESL guideline limit exceedance for benzo(a)pyrene.

Table 13 Summary of Soil Analytical Results Compared with ESL's for Commercial / Industrial Land Use.

NEPM Ecological Screening Levels for Soil				BTEX				PAH	TRH			
Bold - Indicates LOR Exceedances X - Indicates Sample has been Excavated				Benzene mg/kg	Toluene mg/kg	Ethylbenzene mg/kg	Xylenes mg/kg	Benzo(a)pyrene mg/kg	F1 (C6 - C10) mg/kg	F2 (>C10 - C16) mg/kg	F3 (>C16 - C34) mg/kg	F4 (>C34 - C40) mg/kg
Colour Shading - Indicates ESL Exceedances: >1 x, * 2-5 x, ** 5-20 x, *** 20-50 x, **** >50 x												
Sample ID	Sample Date	Soil Texture Class (fine / coarse)	Land Use	LOR 0.2	LOR 0.5	LOR 0.5	LOR 0.5	LOR 0.5	LOR 10	LOR 50	LOR 100	LOR 100
BH1 0.1-0.2 X	4/12/24	C	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	230	200
BH1 0.5-0.6	4/12/24	F	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH1 1.0-1.1	4/12/24	F	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH2 0.1-0.2 X	4/12/24	C	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
Duplicate X	4/12/24	C	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH2 0.5-0.6	4/12/24	F	COM/IND	<0.2	<0.5	<0.5	<0.5	1.4	<10	<50	130	<100
BH2 1.0-1.1	4/12/24	F	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH3 0.1-0.2 X	4/12/24	C	COM/IND	<0.2	<0.5	<0.5	<0.5	0.8	<10	<50	170	<100
BH3 0.5-0.6	4/12/24	F	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH3 1.0-1.1	4/12/24	F	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH4 0.1-0.2 X	4/12/24	C	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	160
BH4 0.5-0.6	4/12/24	F	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH4 1.0-1.1	4/12/24	F	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH5 0.1-0.2 X	4/12/24	C	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH5 0.5-0.6	4/12/24	F	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH5 1.0-1.1	4/12/24	F	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH6 0.1-0.2 X	4/12/24	C	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100
BH6 0.5-0.6	4/12/24	C	COM/IND	<0.2	<0.5	<0.5	<0.5	<0.5	<10	<50	<100	<100

7.4.2 Ecological Investigation Levels

Laboratory analytical results are presented in Appendix 7. Table 14 compares soil analytical results against relevant EILs for commercial / industrial land use. Concentrations which exceeded laboratory LOR are reported in the table, EIL exceedances are highlighted with a coloured cell, and samples within a potential excavation zone for any future potential excavations are marked with an X. There were two samples, BH2 0.5-0.6 and BH3 0.1-0.2 that had an exceedance above the EILs commercial / industrial land use guideline limits for zinc; BH3 0.1-0.2 also had an exceedance for copper.

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Table 14 Soil Analytical Results Compared Against EILs for Commercial / Industrial Land Use.

NEPM Ecological Investigation Levels for Soil						Copper (CEC) mg/kg	Copper (pH) mg/kg	Nickel mg/kg	Zinc mg/kg	Chromium III mg/kg	Lead mg/kg	Arsenic mg/kg	Naphthalene mg/kg
Sample ID	Sample Date	EIL Land Use Sensitivity Class	Soil CEC (cmolc/kg)	Soil pH	Soil Texture Class (fine /coarse)								
BH1 0.1-0.2 X	4/12/24	COM/IND	10	4.5 (3)	C	18	18	16	69	14	32	9	<1
BH1 0.5-0.6	4/12/24	COM/IND	35	4.5 (3)	F	<5	<5	18	41	5	14	<5	<1
BH1 1.0-1.1	4/12/24	COM/IND	35	4.5 (3)	F	<5	<5	18	32	7	5	<5	<1
BH2 0.1-0.2 X	4/12/24	COM/IND	10	4.5 (3)	C	18	18	16	57	12	11	10	<1
Duplicate X	4/12/24	COM/IND	10	4.5 (3)	C	13	13	14	49	12	9	8	<1
BH2 0.5-0.6	4/12/24	COM/IND	45	4.5 (3)	F	75	75	12	519*	18	445	8	<1
BH2 1.0-1.1	4/12/24	COM/IND	45	4.5 (3)	F	24	24	8	116	24	119	<5	<1
BH3 0.1-0.2 X	4/12/24	COM/IND	10	4.5 (3)	C	87	87	19	275	17	289	12	<1
BH3 0.5-0.6	4/12/24	COM/IND	45	4.5 (3)	F	<5	<5	4	20	6	19	<5	<1
BH3 1.0-1.1	4/12/24	COM/IND	35	4.5 (3)	F	17	17	96	87	6	11	8	<1
BH4 0.1-0.2 X	4/12/24	COM/IND	10	4.5 (3)	C	29	29	8	113	10	145	<5	<1
BH4 0.5-0.6	4/12/24	COM/IND	45	4.5 (3)	F	<5	<5	6	14	10	13	<5	<1
BH4 1.0-1.1	4/12/24	COM/IND	35	4.5 (3)	F	9	9	24	18	6	8	5	<1
BH5 0.1-0.2 X	4/12/24	COM/IND	10	4.5 (3)	C	8	8	7	51	10	17	12	<1
BH5 0.5-0.6	4/12/24	COM/IND	45	4.5 (3)	F	13	13	8	125	6	134	<5	<1
BH5 1.0-1.1	4/12/24	COM/IND	35	4.5 (3)	F	<5	<5	28	69	4	42	13	<1
BH6 0.1-0.2 X	4/12/24	COM/IND	10	4.5 (3)	C	17	17	9	42	11	14	12	<1
BH6 0.5-0.6	4/12/24	COM/IND	20	4.5 (3)	C	72	72	232	58	71	6	27	<1

pH Designation:

(1) Using 0.01M CaCl₂ extract. Rayment, G.E. and Lyons, D.J. (2011). "Soil Chemical Methods – Australasia". 495+20 pp. CSIRO Publishing, Melbourne.(2) pH_f (1:5). Adjusted by subtracting 0.75 with +/- 0.25 error to calibrate to the CaCl₂ method (per comm. ALS Brisbane Acid Sulphate Soils Laboratory). Methods in accordance with Ahern, C.R., Stone Y., and Blunden B. (1998b). 'Acid Sulfate Soils Assessment Guidelines'. Acid Sulfate Soils Management Advisory Committee, Wollongbar, NSW, Australia.

(3) Classified in accordance with parent material typical soil pH as per the Tasmanian soils database

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8 SOIL HUMAN HEALTH DIRECT CONTACT ASSESSMENT

8.1 Guidelines

Guidelines presented herein are based on potential exposure of human receptors to soil impact which may include:

- Trench workers repairing or building services (typically to 1 m bgs). This classification is not dependent on the land use class.
- On Site inhabitants which may be exposed to potential shallow soil impact in non-paved areas of the Site; and
- On Site excavation works which may include potential swimming pools (up to 3 m bgs); basement carparks; and deep foundations.

8.1.1 Land Use Classification

The NEPM ASC 2013 guidelines have been referenced to ensure that the correct land use and density category has been adopted for the site and the surrounding properties (where applicable). As per NEPM ASC 2013 guidelines, the adopted land use class is dependent on the building density and the opportunity for soil access by site occupants (exposure to potentially impacted soil). Aspects needing to be considered include:

- Whether the site is of sensitive land use such as a childcare centre, preschool, primary school or aged care facility in which case land use Class A is applicable;
- The percentage of paved area to determine direct contact exposure risk and therefore classification as low or high density; and
- Classifications are based on residential, recreational or commercial/industrial setting.

8.1.2 Adopted Land Use Classification

The adopted land use class is presented in Table 15. Land use class is based on the opportunity for soil access, as per NEPM ASC 2013 guidelines. Soil access is anticipated to include future construction workers during site redevelopment, offsite commercial workers during the site redevelopment works and future trench workers conducting routine maintenance or site upgrades. Once the redevelopment is completed it is anticipated that there will be no access to soil to commercial site users or residents in the tenanted spaces on the upper levels of the building.

Table 15 Summary of Land Use Setting and Density for Determining Exposure Risk

Soil Bores	Construction Phase	Location	Land Use	Pathway	Land Use Class
All soil	During	Site	Construction worker and trench workers	ALL	D and trench worker specific
		Off Site	Nearby commercial land users	DI	D
	Post	Site	Future trench workers Future commercial site users	ALL	D and trench worker specific

DC – Dermal Contact - Trench Worker Guidelines (CRC CARE 2013)

DI – Dust Inhalation - HIL Guidelines (NEPM ASC 2013); SI – Soil Ingestion - HIL Guidelines (NEPM ASC 2013); ALL – All of above

8.1.3 Health Investigation & Screening Levels

The main exposure pathways and methods for assessing health risk from contaminated soils are presented in Table 16.

Table 16 Summary of Exposure Pathways and Preliminary (Tier 1) Methods for Assessing Human Exposure Risk

Exposure Scenario	Contaminant Type	Tier 1 Assessment Method	Reference
Vapour Inhalation – Indoor (PVI)	Petroleum Hydrocarbons	HSLs (addressed in PVI sections)	NEPM ASC 2013
Vapour Inhalation – Trench (PVI)			CRC CARE (Friebel & Nadebaum, 2011)
Dermal Contact		HSLs	
Dust Inhalation	Metals PAH's	Health Investigation Levels (HILs)	NEPM ASC 2013
Soil Ingestion			

PVI – Petroleum Vapour Intrusion

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8.2 Findings

8.2.1 Dermal Contact - Petroleum Hydrocarbons

Laboratory analytical results are presented in Appendix 7. Table 17 presents soil hydrocarbon analytical results compared against CRC CARE (Friebel & Nadebaum, 2011) HSL guidelines for assessing dermal contact risk. Concentrations which exceeded laboratory LOR are highlighted in bold, HSL exceedances would be highlighted with a coloured cell indicating the highest HSL land used class which is exceeded, and samples within potential excavation zones for any future excavations are marked with an X.

A total of four samples has detections of hydrocarbons and there were no exceedances for commercial / industrial land use or trench worker specific guidelines.

Table 17 Soil Analytical Results Compared Against CRC CARE (Friebel & Nadebaum, 2011) Guidelines for Dermal Contact

CRC CARE Health Screening Level		EP080: BTEXN					EP080/071: TRH			
		Benzene	Toluene	Ethylbenzene	Total Xylenes	Naphthalene	C6 - C10 Fraction	>C10 - C16 Fraction	>C16 - C34 Fraction	>C34 - C40 Fraction
Dermal Contact Hazard from Soil Hydrocarbons'		mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	
Units		mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	
LOR		0.2	0.5	0.5	0.5	1	10	50	100	
HSL D Commercial/Industrial		430	99000	27000	81000	11000	26000	20000	27000	
Intrusive Maintenance Worker		1100	120000	85000	130000	29000	82000	62000	85000	
Date	Sample									
4/12/2024	BH1 0.1-0.2 X	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	230	
4/12/2024	BH1 0.5-0.6	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH1 1.0-1.1	<0.2	<0.5		<0.5	<1	<10	<50	<100	
4/12/2024	BH2 0.1-0.2 X	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	Duplicate X	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH2 0.5-0.6	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	130	
4/12/2024	BH2 1.0-1.1	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH3 0.1-0.2 X	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	170	
4/12/2024	BH3 0.5-0.6	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH3 1.0-1.1	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH4 0.1-0.2 X	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH4 0.5-0.6	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH4 1.0-1.1	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH5 0.1-0.2 X	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH5 0.5-0.6	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH5 1.0-1.1	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH6 0.1-0.2 X	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	
4/12/2024	BH6 0.5-0.6	<0.2	<0.5	<0.5	<0.5	<1	<10	<50	<100	

8.2.2 Dust Inhalation & Soil Ingestion

Laboratory analytical results are presented in Appendix 7. Soil analytical results are compared against combined dust inhalation and soil ingestion risk is assessed through the application of NEPM ASC 2013 Health Investigation Levels (HILs) for exposure to soil contaminants are presented in Table 18.

Concentrations which exceeded laboratory LOR are highlight in bold (except for the metals), and HIL exceedances would be highlighted with a coloured cell indicating the highest HIL land used class and samples within a potential excavation zone for any future excavations are marked with an X. There were no HIL D, commercial / industrial guideline exceedances for dust inhalation and soil ingestion risk.

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9 INDOOR INHABITANT PVI ASSESSMENT – HSL's

This PVI assessment has been conducted in accordance with relevant CRC CARE Technical Documentation and NEPM ASC 2013 guidelines presented in references section of this report. The HSL assessment approach is generally the first (Tier 1) investigation phase adopted for assessing PVI risk at petroleum hydrocarbon (PHC) impacted sites.

HSL guidelines have been applied for samples collected from the site to account for risks that may be associated with volatile hydrocarbon vapour intrusion into confined spaces where there may be an inhalation risk through longer term exposure. This does not constitute a full vapour risk assessment but provides additional information from which to further quantify any risk.

A detailed investigation (Tier 2 to 3) is recommended over an HSL assessment where an acute risk has been identified at the site (CRC CARE 2013) because of:

- Migrating product on surface soils beneath buildings;
- Strong PHC odours;
- Flammable risk in confined spaces; and/or
- Health complaints from occupants.

Based on the site visits, none of the above conditions have been identified at the site. If the outcome of this Tier 1 assessment reveals HSL exceedances for hydrocarbon vapour intrusion, a more detailed (Tier 2) assessment will be required to further evaluate the human health risk.

PVI risk is initially interpreted through the development of HSL threshold limits from the following classifications:

- The geology and or hydrogeology of the investigation point; and
- Land use sensitivity;

The resulting HSL threshold limits are compared with laboratory analytical results.

9.1 Selected Media for Assessing PVI Risk

Table 19 presents a summary of the preferred HSL approach to assessing PVI risk. In this case, soil PHC concentrations were assessed.

Table 19 Preferred Methods for Determining Site PVI Risk

Media Analysed	Method	Limitations	Order of Preference
Soil Gas	Concentrations of a soil gas through a soil vapor probe	This approach provides the most reliable data in interpreting PVI risk, although direct modelling should be applied if concentrations exceed HSL threshold limits.	Primary
Groundwater	Concentrations of PHC in groundwater through deployment of monitoring wells	More robust and reliable than soil in determining onsite and in particular, offsite risks. Determining PVI risk based on groundwater is inherently conservative when interpreting vapour risk to account for not readily discernible preferential pathways. Reference may be drawn to alternative assessment approaches: <ol style="list-style-type: none"> 1) Application of site-specific conditions to the CRC CARE model for assessing PVI risk 2) Soil gas interpretation for areas where a PVI risk is identified from groundwater analysis. 	Secondary
Soil	Concentrations of PHC in soil	Concentrations in soil may be subject variability due to soil moisture, organic content and oxygen ingress all which create significant bias in threshold values. Reliance is placed on utilizing groundwater analysis over soil. Soil results provide localised information.	Tertiary

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9.2 Land Use Class

For surrounding properties, the potential PVI risk is characterised through application of CRC CARE HSL's for each individual property based on their existing land use (NEPM ASC 2013; Friebel & Nadebaum 2010). The CRC CARE guidelines have been referenced to ensure that the correct land use and density category has been adopted for surrounding land use to ensure health risks are consistent with the HSL models. Aspects considered include the:

- Sensitivity of the existing or potential land use;
- Percentage of paved area for defining potential vapour migration risk;
- Type of basement garage which may influence the confinement of PHC vapours;
- Presence of a slab or cavity for discerning vapour intrusion risk.

If hydrocarbon impacted soil is discerned at the site, consideration is given to downgradient receptors. Where applicable, land use class therefore considers:

- Downgradient receptors where onsite HSL exceedances have been identified in soil; and
- Variations in land use for different parts of the proposed development.

The following land use classes are applied:

- *HSL D for commercial and industrial land use.*

9.3 Findings

Laboratory analytical results are presented in Appendix 7. Table 20 presents the results against a potential indoor vapour risk based on land use setting guidelines. Concentrations which exceeded laboratory LOR would be highlighted in bold. HSL exceedances would be highlighted with a coloured cell. There were no exceedances above HSL D guidelines for indoor vapour risk for commercial / industrial land use.

Table 20 Soil Analytical Results Compared Against HSL D for Indoor Vapour Risk

Soil Hydrocarbon HSL's for Assessing Indoor Vapour Intrusion (NEPM 2013)					EP080: BTEXN					EP080/071: TRH	
Soil Sample Analysis					Benzene	Toluene	Ethylbenzene	Total Xylenes	Naphthalene	F1	F2
Colour Shading - Indicates HSL Exceedances: >1 x, * 2-5 x, ** 5-20 x, *** 20-50 x, **** >50 x											
Sample ID	Sample Date	Depth Class	Grain Class	HSL	mg/kg LOR 0.2	mg/kg LOR 0.5	mg/kg LOR 0.5	mg/kg LOR 0.5	mg/kg LOR 1	mg/kg LOR 10	mg/kg LOR 50
BH1 0.1-0.2	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH1 0.5-0.6	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH1 1.0-1.1	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH2 0.1-0.2	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
Duplicate	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH2 0.5-0.6	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH2 1.0-1.1	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH3 0.1-0.2	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH3 0.5-0.6	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH3 1.0-1.1	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH4 0.1-0.2	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH4 0.5-0.6	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH4 1.0-1.1	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH5 0.1-0.2	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH5 0.5-0.6	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH5 1.0-1.1	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH6 0.1-0.2	4/12/2024	0 - 1	CLAY	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH6 0.5-0.6	4/12/2024	0 - 1	SAND	D	<0.2	<0.5	<0.5	<0.5	<1	<10	<50

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10 TRENCH WORKER PVI ASSESSMENT – HSL’s

10.1 Classification

The following Health Screening Assessment is based on hydrocarbon vapour intrusion risk to subsurface excavation workers within excavations. This is assessed through analysis of vapours from soil and soil vapours. Groundwater is generally not used to assess risk as threshold limits for all depth and grain classes are non-limiting. Land use classes are not applicable when assessing vapour intrusion into trenches.

Soil and soil vapour HSL’s for assessing hydrocarbon risk to maintenance workers are based on CRC CARE Technical Report 10 guidelines (Friebel & Nadebaum 2011) and the following variables:

- Dominant grain size class of material at the soil sample depth or based on the dominant grain class of the backfill material based on US Agriculture Soil Classification System (SCS) and partitioning into either sand, silt or clay; and
- Classifying soil according to depth ranges: 0 to 2 m; 2 to 4 m; 4 to 8 m; and greater than 8 m.

10.2 Findings

Laboratory analytical results are presented in Appendix 7. Summary of Soil Analytical Results Compared against HSL’s for Assessing PVI Risk to Trench Workers are presented in Table 21. Concentrations that exceeded laboratory LOR would be highlighted in bold, and if there were any HSL exceedances they would be highlighted with a coloured cell.

There were no volatile hydrocarbons detection and no guideline exceedances above of the CRC CARE HSL guidelines for Assessing PVI Risk to Trench Workers and no risk identified.

Table 21 Summary of Soil Analytical Results Compared against HSL’s for Assessing PVI Risk to Trench Workers

CRC CARE Health Screening Level Assessment for PHC Inhalation Risk To Trench Workers From Soil Sample Analysis				EP080: BTEXN					EP080/071: TRH	
Bold - Indicates LOR Exceedances				Benzene mg/kg LOR 0.2	Toluene mg/kg LOR 0.5	Ethylbenzene mg/kg LOR 0.5	Total Xylenes mg/kg LOR 0.5	Naphthalene mg/kg LOR 1	C5 - C10 Fraction mg/kg LOR 10	>C10 - C16 Fraction mg/kg LOR 50
Dark Grey Shading - Indicates HSL Exceedances: >1 x, + 2-5 x, ** 5-20 x, *** 20-50 x, **** >50 x										
Sample ID	Sample Date	Depth Class	Grain Class							
BH1 0.1-0.2	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH1 0.5-0.6	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH1 1.0-1.1	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH2 0.1-0.2	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
Duplicate	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH2 0.5-0.6	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH2 1.0-1.1	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH3 0.1-0.2	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH3 0.5-0.6	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH3 1.0-1.1	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH4 0.1-0.2	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH4 0.5-0.6	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH4 1.0-1.1	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH5 0.1-0.2	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH5 0.5-0.6	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH5 1.0-1.1	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH6 0.1-0.2	4/12/2024	0 to 2m	CLAY	<0.2	<0.5	<0.5	<0.5	<1	<10	<50
BH6 0.5-0.6	4/12/2024	0 to 2m	SAND	<0.2	<0.5	<0.5	<0.5	<1	<10	<50

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11 SOIL DISPOSAL ASSESSMENT

11.1 Guidelines

Soil which is excavated from the site for landfill disposal is to be assessed against Information Bulletin 105 (IB105) for Classification and Management of Contaminated Soil for Disposal. The EPA uses four categories to classify contaminated soil as per Table 22:

- (Level 1) Fill Material;
- (Level 2) Low Level Contaminated Soil;
- (Level 3) Contaminated Soil; and
- (Level 4) Contaminated Soil for Remediation.

Fixed numerical values are presented for soil concentrations and leachable fraction concentrations.

Table 22 Summary of IB105 Classification Guidelines

	Classification (with reference to Table 2)	Controlled Waste ¹	Comments
Fill Material² (Level 1)	Soil that exhibits levels of contaminants below the limits defined under <i>Fill Material</i> in Table 2.	Unlikely	Soil classified as <i>Fill Material</i> can still be a 'pollutant' under the <i>Environmental Management and Pollution Control Act 1994</i> and needs to be responsibly managed.
Low Level Contaminated Soil (Level 2)	Soil that exhibits levels of contaminants above the limits defined under <i>Fill Material</i> but below the limits defined under <i>Low Level Contaminated Soil</i> in Table 2.	Likely	Where leachable concentrations have not been prescribed, maximum total concentrations will be used to classify the soil.
Contaminated Soil (Level 3)	Soil that exhibits levels of contaminants above the limits defined under <i>Low Level Contaminated Soil</i> but below the limits defined under <i>Contaminated Soil</i> in Table 2.	Yes	Where leachable concentrations have not been prescribed, maximum total concentrations will be used to classify the soil.
Contaminated Soil for Remediation (Level 4)	Soil that exhibits levels of contaminants above the limits defined under <i>Contaminated Soil</i> in Table 2 (regardless of the maximum total concentrations) is generally not considered acceptable for off-site disposal without prior treatment.	Yes	Soil that contains contaminants that do not have criteria for leachable concentrations (e.g. petroleum hydrocarbons), and the levels of contaminants exceed the maximum total concentrations listed in <i>Contaminated Soil</i> , are generally classified as <i>Contaminated Soil for Remediation</i> .

¹ Controlled Waste is defined in the *Environmental Management and Pollution Control Act 1994*.
² Criteria for *Fill Material* are the limits set by the Director for the purposes of R.9(2)(a)(ii) in the *Regulations*.

11.2 Findings

The soil samples have been compared against IB105 guidelines for potential future soil disposal, see Table 23. For solid waste classification, material ranged from Level 1 Material (clean fill) to Level 3 Material (contaminated soil).

There were eight primary samples that returned a Level 1 Material (clean fill) classification. There were eight primary samples that returned a Level 2 Material (low level contaminated soil) classification for elevated levels of either arsenic, beryllium, chromium, cobalt, lead, manganese, mercury, nickel, zinc or benzo(a)pyrene. There was one primary sample BH6 0.5-0.6 that returned a Level 3 Material (contaminated soil) classification for elevated levels of manganese.

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Table 23 Soil Analytical Results Compared Against IB105 Investigation Limits for Maximum Total Solids in Soil for Disposal (Dry Weight).

Information Bulletin 105 Classification and Management of Contaminated Soil For Disposal		Arsenic	Barium	Beryllium	Cadmium	Chromium Total	Copper	Cobalt	Lead	Manganese	Mercury	Nickel	Selenium	Zinc	Benzo(a)pyrene	C6 - C9 Fraction	C10 - C36 Fraction (sum)	Sum of polycyclic aromatic hydrocarbons	Benzene	Toluene	Ethylbenzene	Total Xylenes
Unit		mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg	mg/kg
LOR		50	1	2	5	5	2	5	5	5	0.1	2	5	5	0.5	10	50	0.5	0.2	0.5	0.5	0.5
Investigation Level Selected																						
IB105 Level 1		<20	<300	<2	<3	<50	<100	<100	<300	<500	<1	<60	<10	<200	<0.08	<65	<1000	<20	<1	<1	<3	<14
IB105 Level 2		20	300	2	3	50	100	100	300	500	1	60	10	200	0.08	65	1000	20	1	1	3	14
IB105 Level 3		200	3000	40	40	500	2000	200	1200	5000	30	600	50	14000	2	650	5000	40	5	100	100	180
IB105 Level 4		750	30000	400	400	5000	7500	1000	3000	25000	110	3000	200	50000	20	1000	10000	200	50	1000	1080	1800
4/12/2024	BH1 0.1-0.2 X	9	60	2	<1	14	18	14	32	275	0.1	16	<5	69	<0.5	<10	300	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH1 0.5-0.6	<5	20	<1	<1	5	<5	18	14	172	<0.1	18	<5	41	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH1 1.0-1.1	<5	<10	4	<1	7	<5	26	5	98	<0.1	18	<5	32	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH2 0.1-0.2 X	10	40	2	<1	12	18	10	11	282	<0.1	16	<5	57	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	Duplicate X	8	30	2	<1	12	13	13	9	269	<0.1	14	<5	49	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH2 0.5-0.6 X	8	290	<1	<1	18	75	8	445	352	3.5	12	<5	519	1.4	<10	<50	10.6	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH2 1.0-1.1 X	<5	150	<1	<1	24	24	7	119	368	1.8	8	<5	116	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH3 0.1-0.2 X	12	210	<1	<1	17	87	8	289	113	0.9	19	<5	275	0.8	<10	130	6.1	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH3 0.5-0.6 X	<5	40	<1	<1	6	<5	3	19	18	0.1	4	<5	20	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH3 1.0-1.1 X	8	10	9	<1	6	17	91	11	710	<0.1	96	<5	87	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH4 0.1-0.2 X	<5	110	<1	<1	10	29	5	145	134	0.1	8	<5	113	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH4 0.5-0.6 X	<5	150	<1	<1	10	<5	6	13	26	<0.1	6	<5	14	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH4 1.0-1.1 X	5	80	4	<1	6	9	108	8	300	<0.1	24	<5	18	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH5 0.1-0.2 X	12	40	<1	<1	10	8	3	17	60	0.5	7	<5	51	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH5 0.5-0.6 X	<5	210	<1	<1	6	13	10	134	720	<0.1	8	<5	125	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH5 1.0-1.1 X	13	20	3	<1	4	<5	32	42	126	<0.1	28	<5	69	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH6 0.1-0.2 X	12	30	<1	<1	11	17	3	14	70	<0.1	9	<5	42	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5
4/12/2024	BH6 0.5-0.6	27	300	7	<1	71	72	53	6	5230	<0.1	232	<5	58	<0.5	<10	<50	<0.5	<0.2	<0.5	<0.5	<0.5

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

12 CONCEPTUAL SITE MODEL

12.1 Overview

This CSM has been developed based on the current desktop investigation and invasive soil assessment. The site to be potentially contaminated land because on the northwestern corner of the site houses a vehicle maintenance workshop. The site is proposed to be redeveloped to a more sensitive land use from a single level open-air, carpark to a multi-level building with carparking, commercial premises on the lower levels and tenanted residential spaces on the upper levels.

Figure 11 illustrates the Conceptual Site Model (CSM) with potential risks identified during this investigation that may be associated with site contamination. All potential current and future contamination pathways have been considered.

Potential contaminants of potential concern (COPC) that have been considered included Total Petroleum/Recoverable Hydrocarbons (TPH/TRH); Mono Aromatic Hydrocarbons (MAHs); Benzene, Toluene, Ethylbenzene, Xylene, Naphthalene (BTEXN); Polynuclear Aromatic Hydrocarbons (PAHs), Metals.

12.2 Potential Ecological Receptors

The closest ecological receptors are Hobart Rivulet (contained in a culver) approximately 400m south east from the site, and the River Derwent at Constitution Dock which is located approximately 800m southeast from the site. This receptor has been discounted as a possible receptor due to the spatial separation from the site.

12.3 Potential Human Receptors

Potential human receptors considered during this investigation include; commercial workers and trench workers during the site redevelopment plus offsite commercial workers during the site redevelopment.

Future commercial and residential site users, have been discounted for the following reasons

- post development there will be no access to the soil
- there are no residential units on the lower levels of the building
- there were no detections of contamination above human health guidelines

12.4 Potential Transport Mechanisms and Exposure Pathways

12.4.1 Ecological Receptors

The closest ecological receptors are Hobart Rivulet and the River Derwent, these receptors have been discounted as a possible receptor due to the spatial separation from the site, and given the requirement for a Soil and Water Management Plan (SWMP) to minimise movement of soil and sediment off-site.

12.4.2 Commercial Site Users

No risk to commercial site workers has been identified.

12.4.3 Indoor Vapour Risk

There were no guideline exceedances, and no indoor vapour risk identified.

12.4.4 Trench Workers

No risk to trench workers was identified in accordance with trench worker specified guidelines.

12.4.5 Groundwater Impact

Although groundwater was encountered in borehole 6 at 0.7m below ground surface, groundwater assessment was not conducted as part of this investigation.

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

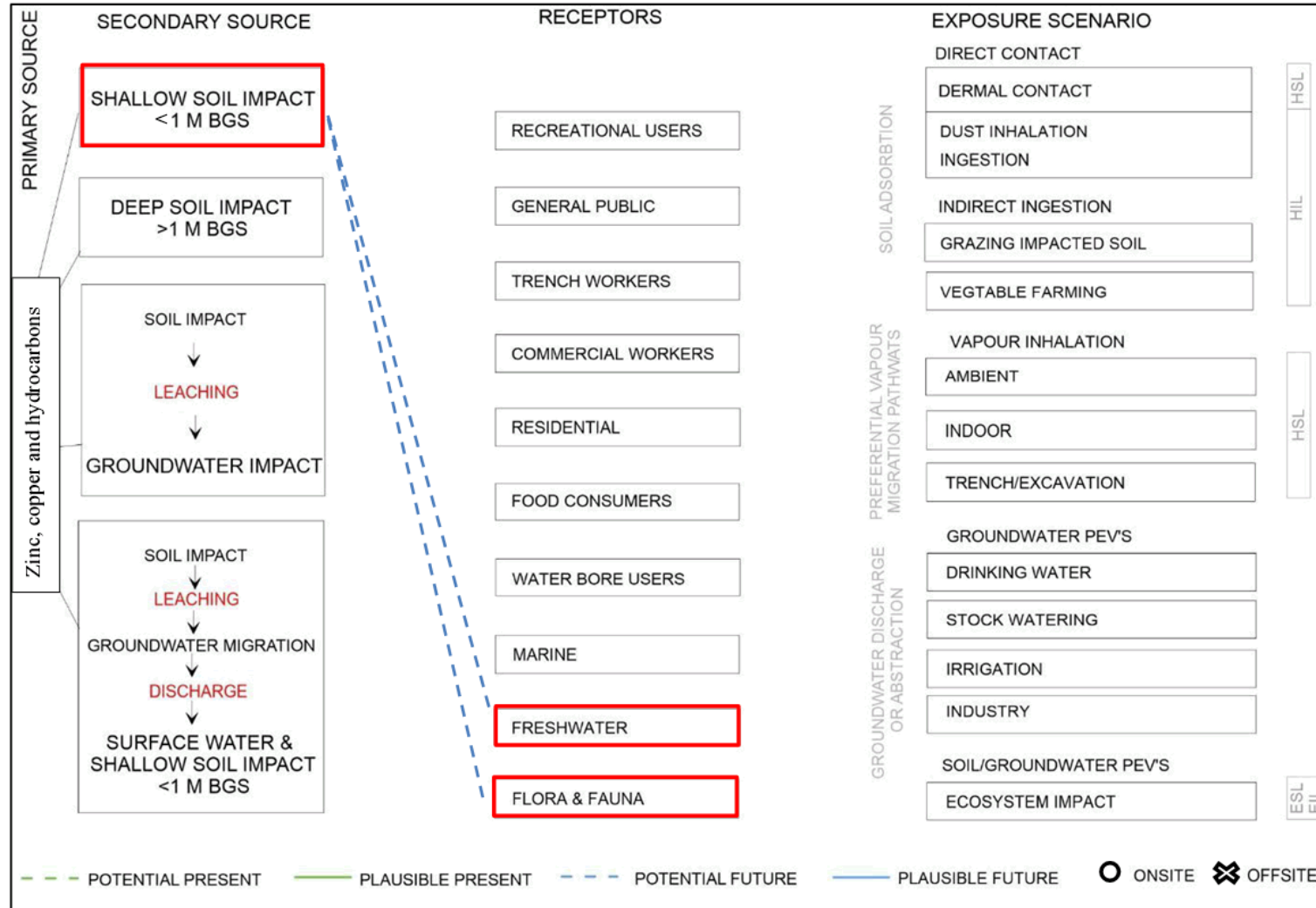


Figure 11 Conceptual Site Model – Flow Chart

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

13 CONCLUSIONS & RECOMMENDATIONS

13.1 Desktop Assessment

The following information was gathered during the desktop investigation:

- The site currently features a large workshop building, a historic building formally *Sydney Lodge* and a large car parking area. The investigation area and proposed footprint of the new building is a single level open-air, asphalt covered carpark. The proposed development is a new multi-story building featuring commercial use such as offices and carparking on the ground level and residential apartments on the upper levels. This ESA report was requested as part of the planning process for this site redevelopment.
- The site is zoned Commercial land use under the *Hobart Interim Planning Scheme of 2015*.
- The geology of the site is mapped as being underlain by Late Carboniferous - Triassic Upper Fluvio-lacustrine Sequence of quartz sandstone sequence. The elevation of the investigation area ranges from 26 to 32m above sea level and the site dips steeply to the southeast. The closest downgradient ecosystem receptors are Hobart Rivulet (contained in culverts) located approximately 400m southeast from the site, and the River Derwent at Constitution Dock which is located approximately 800m southeast from the site.
- The historical aerial photographs confirm that prior to 1946, the site housed a town house building on the Brisbane Street, street-front and the former *Sydney Lodge* at the rear of the property, by the 1960's this house on the street front was demolished and the area covered by asphalt and used for vehicle parking; the former *Sydney Lodge* remains on site to this day. Also, in the 1965's a large industrial shed, thought to be a vehicle workshop was constructed. This workshop has had several small out buildings added to it. It also remains on site today.
- The Environment Protection Authority Tasmania layers on the LIST were reviewed and there were no regulated premises and underground petroleum storage systems located either upgradient from the site or directly adjacent to the site.
- Contaminants of potential concern at the site include the following: total petroleum/recoverable hydrocarbons; mono aromatic hydrocarbons: (benzene, toluene, ethylbenzene, xylene, naphthalene); polynuclear aromatic hydrocarbons; and metals.

13.2 Adopted Guideline Settings

The following investigation limits were adopted for the site:

- Ecosystem receptor
 - Commercial land use ESL and EILs; River Derwent at Constitution Dock is located 800m down gradient. Potential impact unlikely to reach this receptor.
- Human Receptors
 - HSL D for soil direct contact risk to dermal contact to construction workers and offsite commercial site users during construction and trench works post construction.
 - HIL D for soil ingestion and dust inhalation risk to construction workers and offsite commercial site users during construction and trench works post construction.
 - HSL D indoor vapour risk to construction workers and trench works post construction.
 - Trench Worker specific guidelines.

13.3 Soil Assessment Conclusions

From the soil assessment, it is concluded that:

- Environment There were two samples, BH2 0.5-0.6 and BH3 0.1-0.2 that had ESL guideline limit exceedance for benzo(a)pyrene for commercial / industrial land use and the same two samples had an exceedance above the EILs for zinc; BH3 0.1-0.2 also had an EIL exceedance for copper. However, there is an absence of an ecological receptors at this location.

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

- Human Health For commercial/industrial land use guidelines, there were no human health guideline exceedances for dermal contact, dust inhalation and soil ingestion risk, vapour intrusion or trench worker specific guidelines. Therefore, no risk was identified for human receptors.
- Excavated Soil Management There were eight primary samples that returned a Level 1 Material (clean fill) classification. There were eight primary samples that returned a Level 2 Material (low level contaminated soil) classification for elevated levels of either arsenic, beryllium, chromium, cobalt, lead, manganese, mercury, nickel, zinc or benzo(a)pyrene. There was one primary sample BH6 0.5-0.6 that returned a Level 3 Material (contaminated soil) classification for elevated levels of manganese.
- Perched water was encountered in BH6 at 0.7m. A water sample was unable to be collected. Water was noted as clean in appearance and odour free.

13.4 Recommendations

The following is recommended at the site

- Environmental Risk Given exceedances of ESL guideline limits were observed, a Soil and Water Management Plan (SWMP) is required to minimise the movement of soil and sediment off-site during the construction phase. Correct implementation of the SWMP will mitigate the identified environmental risks.
- Excavated Material The soil classified on site ranges from Level 1 Material (Clean Fill) to Level 3 Material (contaminated soil). For any future excavation of soil at the site, future waste classification should be conducted in accordance with EPA Tasmania IB105.
- Groundwater If groundwater is encountered during the site redevelopment works and is suspected to be contaminated (observed odour, sheen or staining) then a suitably qualified and experienced person should conduct groundwater sampling in accordance with NEPM ASC 2013 to determine if there is a risk to human health or the environment.
- Statement of Suitability The findings from this investigation confirm that there is currently no risk to Human Health or the Environment as part of the planned works at the site. Providing the above recommendations are followed, then it is acceptable for the proposed work to proceed.

Yours faithfully,



Sarah Joyce BSc (Hons)
Senior Environmental Scientist

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

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Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

LIMITATIONS STATEMENT

This *Environmental Site Assessment* Report has been prepared in accordance with the scope of services between Geo-Environmental Solutions Pty. Ltd. (GES) and Brisbane Street Apartments Pty Ltd ('the Client'). To the best of GES's knowledge, the information presented herein represents the Client's requirements at the time of printing of the Report. However, the passage of time, manifestation of latent conditions or impacts of future events may result in findings differing from that described in this Report. In preparing this Report, GES has relied upon data, surveys, analyses, designs, plans and other information provided by the Client and other individuals and organisations referenced herein. Except as otherwise stated in this Report, GES has not verified the accuracy or completeness of such data, surveys, analyses, designs, plans and other information.

The scope of this study does not allow for the review of every possible soil and groundwater contaminant over the whole area of the site. Samples collected from the investigation area are assumed to be representative of the areas from where they were collected and indicative of the contamination status of the site at that point in time. The conclusions described within this report are based on these samples, the results of their analysis and an assessment of their contamination status.

This report does not purport to provide legal advice. Readers of the report should engage professional legal practitioners for this purpose as required.

No responsibility is accepted for use of any part of this report in any other context or for any other purpose by third party.

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

Appendix 1 GES Staff

Geo-Environmental Solutions (GES) is a specialist geotechnical and environmental consultancy providing advice on all aspects of soils, geology, hydrology, and soil and groundwater contamination across a diverse range of industries.

Geo Environmental Solutions Pty Ltd:

- ACN – 115 004 834
- ABN – 24 115 004 834

GES STAFF - ENGAGED IN SITE INVESTIGATION WORKS

Dr John Paul Cumming B.Agr.Sc (Hons) Phd CPSS GAICD

- Principle Author and Principle Environmental Consultant
- PhD in Environmental Soil Chemistry from the University of Tasmania in 2007
- 22 years' experience in environmental contamination assessment and site remediation.

Ms Sarah Joyce BSc (Hons)

- Senior Environmental Scientist
- Honours in Geography and Environmental Science at the University of Tasmania in 2003
- 20 years professional work experience and 12 years contaminated site assessment.

Mr Callum Cooper BSc

- Field Technician/ Geologist
- 1 year experience in contamination assessment

Mr Mark Downie B.Agr.Sc

- Soil Scientist – 20 years' professional work experience
- 12 years' experience in contamination assessment and reporting of soils and groundwater.

GES STAFF – CONTAMINATED SITES EXPERIENCE

Mr Aaron Plummer (Cert. IV)

- Senior Geotechnical Technician
- 10 years' experience in hydrocarbon and heavy metal contamination sampling of soils and groundwater.

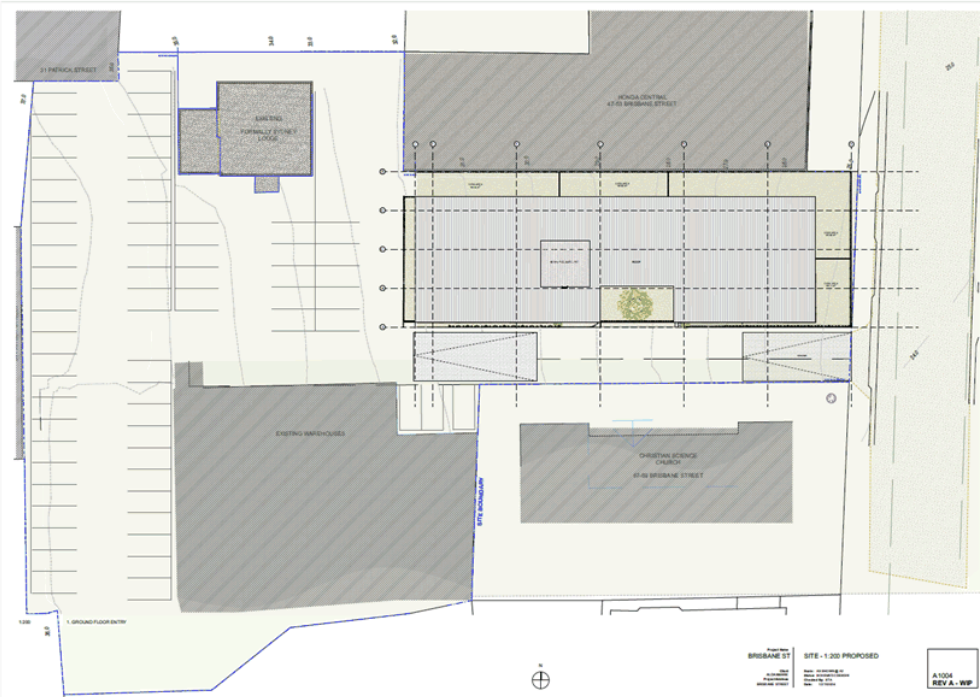
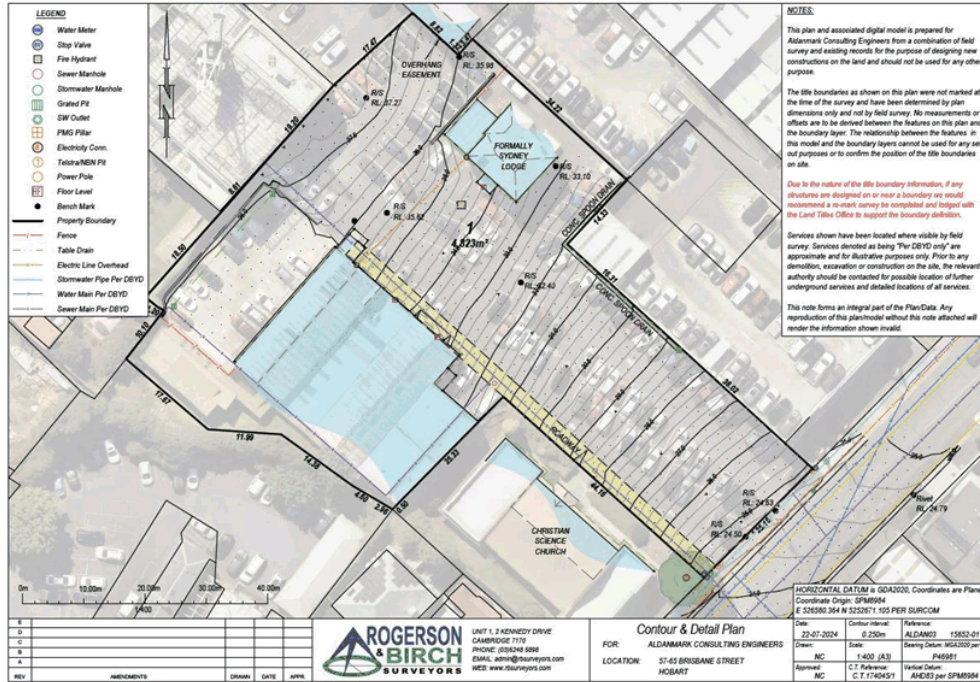
Mr Grant McDonald (Adv. cert. hort.)

- Field Technician
- 15 years' experience in hydrocarbon and heavy metal contamination sampling of soils and groundwater.

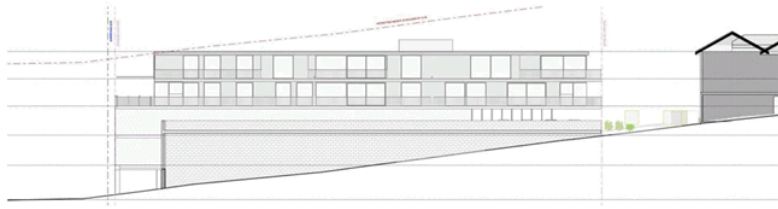
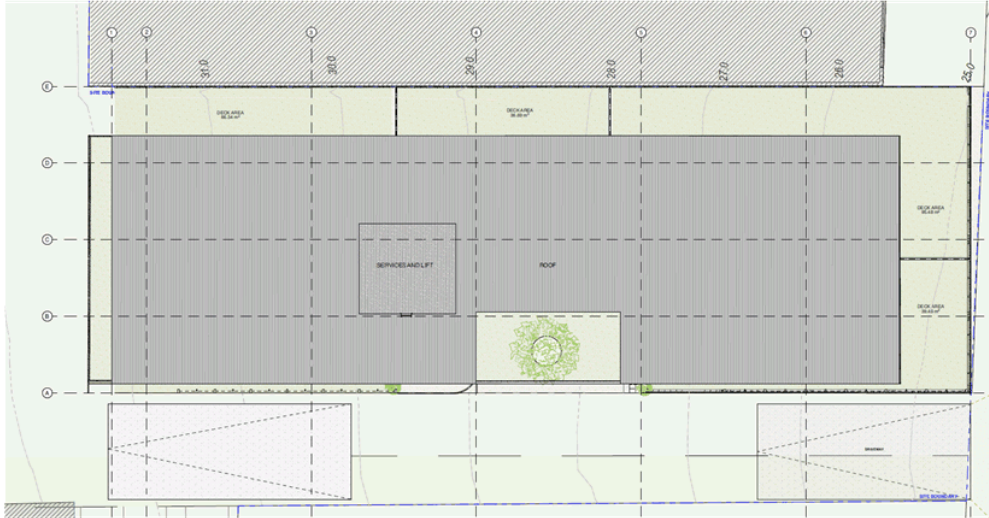
Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

Appendix 2 Development Plans

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



BRISBANE ST ELEVATION - SITE SECTIONS
A202
REV A - WP

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



4.0 PLANNING

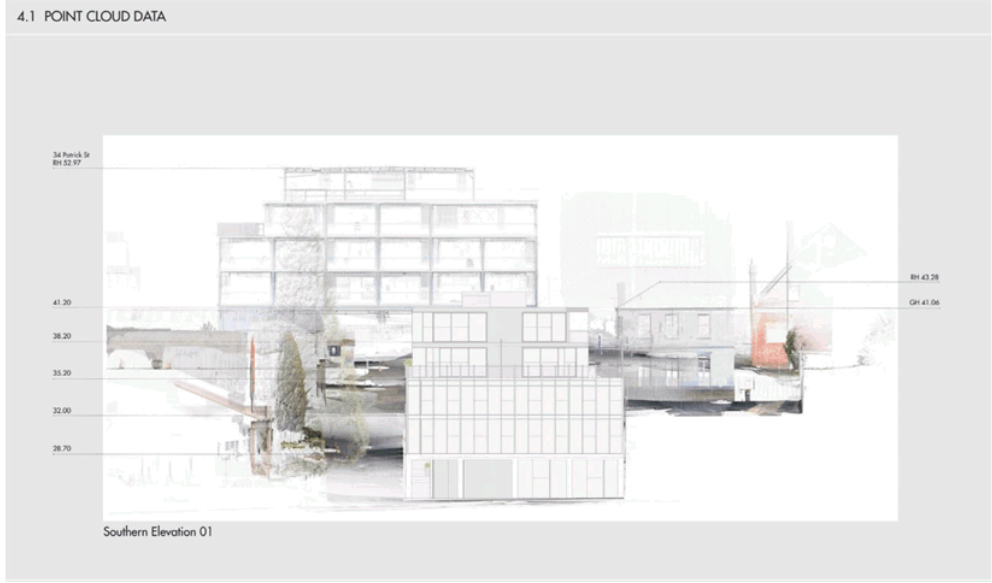
The site at 57-65 Brisbane Street is zoned as 'Commercial' under the current Hobart Interim Planning Scheme 2015.

23.4.1 Building Height

A1 - Building height must be no more than:
(a) 11.5m high and a maximum of 3 storeys; or
(b) 15m high and a maximum of 4 storeys, if the development provides at least 50% of the floor space above ground level for residential use.

A2 - Building height within 10m of a residential zone must be no more than 8.5m

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

Appendix 3 Site Photographs



Photograph 1. The site view from Campbell Street (Google Maps).



Photograph 2. View of the site from Brisbane Street, Heritage building on the right.

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



Photograph 3. View south across the investigation area.



Photograph 4. View southeast across the investigation area.

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



Photograph 5. Heritage building on site, formally Sydney Lodge.



Photograph 6. Workshop and extension on site.



Photograph 7. BH1 Profile.



Photograph 8. BH2 Profile.

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Photograph 9. BH3 Profile.



Photograph 10. BH4 Profile.



Photograph 11. BH5 Profile.



Photograph 12. BH6 Profile.

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Appendix 4 Historical Aerial Photographs

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



Historical Aerial Photograph 1. 2018-2019 - Source (TheLIST)



Historical Aerial Photograph 2. 6 March 2011 - Source NRE

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



Historical Aerial Photograph 3. 24 March 1982 - Source NRE



Historical Aerial Photograph 4. 11 December 1973 - Source NRE

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



Historical Aerial Photograph 5. 11 December 1973 - Source NRE – Close Up site next door.



Historical Aerial Photograph 6. 17 February 1965 - Source NRE

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Historical Aerial Photograph 7. 17 February 1965 - Source NRE Close up neighbouring property



Historical Aerial Photograph 8. 12 February 1957 - Source NRE

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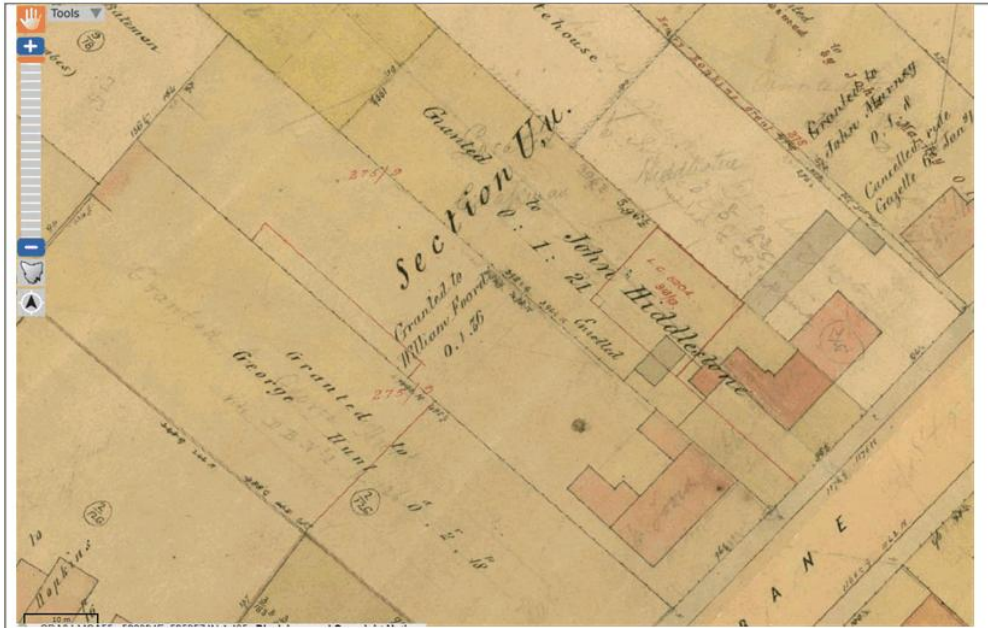


Historical Aerial Photograph 9. 2 April 1946 – Source NRE



Historical Aerial Photograph 10. c.1846 – Source - Sprent's Book Mosaic, accessed via The LIST.

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Historical Aerial Photograph 11. c. 1846 (close up) – Source - Sprent’s Book Mosaic, accessed via The LIST. Land Granted to William Foord. 1797-1850, Hobart. According to <https://groups.io/g/AUS-Tasmania/topic/71746725>; He was a building contractor who superintended the construction of St. Luke's Church at Bothwell and was also Clerk of Works during he construction of the new Government House in Hobart Town.

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Appendix 5 Chain of Custody (COC) and Sample Receipt Notification (SRN)

COC EM2421460

SRN EM2421460

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

ALS ENVIRONMENTAL
 ALS Laboratory
 please see →

CHAIN OF CUSTODY
 GEOENVIRONMENTAL SOLUTIONS
 29 Kirkcubey Pl, Battery Point TAS 7004

FREIGHT

ALS Laboratory
 177 Collins St, Melbourne VIC 3000
 Tel: +61 3 9593 8200
 Fax: +61 3 9593 8201
 Email: als@als.com.au
 Website: www.als.com.au

CLIENT: 65 Brisbane St
PROJECT: 29 Kirkcubey Pl, Battery Point TAS 7004
ORDER NUMBER:
PROJECT MANAGER: JP Community
SAMPLER: CC
CONTACT PH:
SAMPLER MOBILE:
EDD FORMAT (or default): ✓
 Email Reports to (will default to PM if no other addresses are listed):
 Email Invoices to (will default to PM if no other addresses are listed):
COMMENTS/SPECIAL HANDLING/STORAGE OR DISPOSAL:

TURNAROUND REQUIREMENTS: Standard TAT (List due date) Non Standard or urgent TAT (List due date)
ALS QUOTE NO.:
FOR LABORATORY USE ONLY (Circle)
 Caddy Seal Used? Yes No N/A
 Washed from ice packs present upon receipt? Yes No N/A
 Random Sample Temperature on Receipt: C
 Other comment:
RECEIVED BY: Manu (M)
DATE/TIME: 4-12-24 1500
RELINQUISHED BY:
DATE/TIME:
RECEIVED BY: Manu (M)
DATE/TIME: 5/12 1020

LAB ID	SAMPLE ID	DATE / TIME	MATRIX	TYPE & PRESERVATIVE (refer to container)	CONTAINER INFORMATION		ANALYSIS REQUIRED (includes SUITES (N/A), also Codes must be used to attract sale price) Where Metals are required specify Total (pretreat steps required) or Dissolved (pretreat steps required)	Additional Information
					TOTAL	(refer to CONTAINERS)		
1	Rinsate	4-12-24	W	2V (P) A	4	4	TPH, BTEX, PAH, 15 Metals	
2	Trip Blank		S	Jar	1	1		
3	BH1 0.5-0.6				1	1		
4	BH1 0.5-0.6				1	1		
5	BH1 1.0-1.1				1	1		
6	BH2 0.1-0.2				1	1		
7	Duplicate				1	1		
8	BH2 0.5-0.6				1	1		
9	BH2 1.0-1.1				1	1		
10	BH3 0.1-0.2				1	1		
11	BH3 0.5-0.6				1	1		
12	BH3 1.0-1.1				1	1		
TOTAL					1	1		

Environmental Division Melbourne Work Order Reference EM2421460
 Telephone : - 61-3-8649 9600

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

CHAIN OF CUSTODY
 ENVIRONMENTAL SOLUTIONS
 ALS Laboratory
 please tick →

CLIENT: ENVIRONMENTAL SOLUTIONS
 28 Kirkway Pl, Battery Point TAS 7004

PROJECT: 65 Brisbane

ORDER NUMBER: SP Cumming

SAMPLER: CC/CC

PROJECT MANAGER: SP Cumming

SAMPLER MOBILE: SP Sarah Joyce

CONTACT PH: 0413 541 531

EDD FORMAT (or default): accounts, JP, Sarah

TURNAROUND REQUIREMENTS:
 (Standard TAT may be longer for some tests e.g. Ultra Trace Organics)
 Standard TAT (Lit due date)
 Non Standard or urgent TAT (Lit due date)

FOR LABORATORY USE ONLY (Circle)
 Custody Seal Intact? Yes No
 Free ice / frozen ice blocks present upon receipt? Yes No
 Random Sample Temperature on Receipt? Yes No
 Other comment: C

COC SEQUENCE NUMBER (Circle)
 COC: 1 2 3 4 5 6 7
 or: 1 2 3 4 5 6 7

RECEIVED BY: MAM
DATE/TIME:

RELINQUISHED BY:
DATE/TIME: 4-12-24 15:00

COMMENTS/SPECIAL HANDLING/STORAGE OR DISPOSAL:

LAB ID	SAMPLE ID	DATE / TIME	MATRIX	TYPE & PRESERVATIVE (codes below)	TOTAL CONTAINERS (refer to)	ANALYSIS REQUIRED INCLUDING SUITES (NB. Suite Codes must be listed to attract suite price) Where Metals are required, specify Total (unfiltered bottle required) or Dissolved (filtered bottle required)	Additional Information
13	B44 0.1-0.2	4-12-24	5	Jar	1	TPH, BTEX, PAH, 15 Metals	Comments on likely contaminant levels, dilutions, or samples requiring specific GC analysis etc.
14	B44 0.5-0.6						
15	B44 1.0-1.1						
16	B45 0.1-0.2						
17	B45 0.5-0.6						
18	B45 1.0-1.1						
19	B46 0.1-0.2						
20	B46 0.5-0.6						
TOTAL							

Water Containment Codes: P = Unpreserved Plastic; N = N2O2 Preserved Plastic; QSS = N2O2 Preserved QSS; S = Sealed Hydrophobic Preserved; B = Sodium Hydroxide Preserved; AD = Amber Glass Unpreserved; ZP = Purple Unpreserved Plastic; V = VOA Volatile Solvent; B = BTEX Preserved Plastic; VS = VOA Volatile Solvent; BTEX Preserved Plastic; AV = Air Tight Unpreserved Vial; SG = Surah Preserved Amber Glass; H = HCl Preserved Plastic; H2 = HCl Preserved Plastic; SP = Surah Preserved Plastic; F = Formaldehyde Preserved Glass; Z = Zinc Acetate Preserved Bottle; E = EDTA Preserved Bottle; ST = Sterile Bottle; ASS = Plastic Bag for Acid Substrate Sol; S = Unpreserved Bag.

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



SAMPLE RECEIPT NOTIFICATION (SRN)

Work Order	: EM2421460		
Client	: GEO-ENVIRONMENTAL SOLUTIONS	Laboratory	: Environmental Division Melbourne
Contact	: DR JOHN PAUL CUMMING	Contact	: Katie Davis
Address	: 29 KIRKSWAY PLACE BATTERY POINT TASMANIA, AUSTRALIA 7004	Address	: 4 Westall Rd Springvale VIC Australia 3171
E-mail	: jcumming@geosolutions.net.au	E-mail	: katie.davis@alsglobal.com
Telephone	: +61 03 6223 1839	Telephone	: +61-3-8549 9600
Facsimile	: +61 03 6223 4539	Facsimile	: +61-3-8549 9626
Project	: 65 Brisbane St	Page	: 1 of 3
Order number	: ----	Quote number	: EB2017GEOENVSOL0001 (EN/222)
C-O-C number	: ----	QC Level	: NEPM 2013 B3 & ALS QC Standard
Site	: ----		
Sampler	: cc		

Dates

Date Samples Received	: 05-Dec-2024 10:30	Issue Date	: 06-Dec-2024
Client Requested Due Date	: 12-Dec-2024	Scheduled Reporting Date	: 12-Dec-2024

Delivery Details

Mode of Delivery	: Carrier	Security Seal	: Intact.
No. of coolers/boxes	: 1	Temperature	: 7.8°C - Ice Bricks present
Receipt Detail	:	No. of samples received / analysed	: 20 / 20

General Comments

- This report contains the following information:
 - Sample Container(s)/Preservation Non-Compliances
 - Summary of Sample(s) and Requested Analysis
 - Proactive Holding Time Report
 - Requested Deliverables
- **Please direct any queries related to sample condition / numbering / breakages to Client Services.**
- Sample Disposal - Aqueous (3 weeks), Solid (2 months) from receipt of samples.
- Unless otherwise stated, analytical work for this work order will be conducted at ALS Melbourne, NATA accreditation no. 825, site no. 13778.
- **Please refer to the Proactive Holding Time Report table below which summarises breaches of recommended holding times that have occurred prior to samples/instructions being received at the laboratory. The laboratory will process these samples unless instructions are received from you indicating you do not wish to proceed. The absence of this summary table indicates that all samples have been received within the recommended holding times for the analysis requested.**
- Please be aware that APHA/NEPM recommends water and soil samples be chilled to less than or equal to 6°C for chemical analysis, and less than or equal to 10°C but unfrozen for Microbiological analysis. Where samples are received above this temperature, it should be taken into consideration when interpreting results. Refer to ALS EnviroMail 85 for ALS recommendations of the best practice for chilling samples after sampling and for maintaining a cool temperature during transit.

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Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

Issue Date : 06-Dec-2024
 Page : 2 of 3
 Work Order : EM2421460 Amendment 0
 Client : GEO-ENVIRONMENTAL SOLUTIONS



Sample Container(s)/Preservation Non-Compliances

All comparisons are made against pretreatment/preservation AS, APHA, USEPA standards.

- No sample container / preservation non-compliance exists.

Summary of Sample(s) and Requested Analysis

Some items described below may be part of a laboratory process necessary for the execution of client requested tasks. Packages may contain additional analyses, such as the determination of moisture content and preparation tasks, that are included in the package. If no sampling time is provided, the sampling time will default 00:00 on the date of sampling. If no sampling date is provided, the sampling date will be assumed by the laboratory and displayed in brackets without a time component

Matrix: SOIL

Laboratory sample ID	Sampling date / time	Sample ID	SOIL - EA055-103 Moisture Content	SOIL - S-03	15 Metals (NEPM 2013 Suite - Incl. Digestion)	SOIL - S-07 TRH(BTEX)MPAH (SM)	SOIL - S-18 TRH(CFC9)BTEXN
EM2421460-002	04-Dec-2024 00:00	Trip Blank	✓	✓	✓	✓	✓
EM2421460-003	04-Dec-2024 00:00	BH1 0.1-0.2	✓	✓	✓	✓	✓
EM2421460-004	04-Dec-2024 00:00	BH1 0.5-0.6	✓	✓	✓	✓	✓
EM2421460-005	04-Dec-2024 00:00	BH1 1.0-1.1	✓	✓	✓	✓	✓
EM2421460-006	04-Dec-2024 00:00	BH2 0.1-0.2	✓	✓	✓	✓	✓
EM2421460-007	04-Dec-2024 00:00	Duplicate	✓	✓	✓	✓	✓
EM2421460-008	04-Dec-2024 00:00	BH2 0.5-0.6	✓	✓	✓	✓	✓
EM2421460-009	04-Dec-2024 00:00	BH2 1.0-1.1	✓	✓	✓	✓	✓
EM2421460-010	04-Dec-2024 00:00	BH3 0.1-0.2	✓	✓	✓	✓	✓
EM2421460-011	04-Dec-2024 00:00	BH3 0.5-0.6	✓	✓	✓	✓	✓
EM2421460-012	04-Dec-2024 00:00	BH3 1.0-1.1	✓	✓	✓	✓	✓
EM2421460-013	04-Dec-2024 00:00	BH4 0.1-0.2	✓	✓	✓	✓	✓
EM2421460-014	04-Dec-2024 00:00	BH4 0.5-0.6	✓	✓	✓	✓	✓
EM2421460-015	04-Dec-2024 00:00	BH4 1.0-1.1	✓	✓	✓	✓	✓
EM2421460-016	04-Dec-2024 00:00	BH5 0.1-0.2	✓	✓	✓	✓	✓
EM2421460-017	04-Dec-2024 00:00	BH5 0.5-0.6	✓	✓	✓	✓	✓
EM2421460-018	04-Dec-2024 00:00	BH5 1.0-1.1	✓	✓	✓	✓	✓
EM2421460-019	04-Dec-2024 00:00	BH6 0.1-0.2	✓	✓	✓	✓	✓
EM2421460-020	04-Dec-2024 00:00	BH6 0.5-0.6	✓	✓	✓	✓	✓

Matrix: WATER

Laboratory sample ID	Sampling date / time	Sample ID	WATER - W-03 15 Metals (NEPM Suite)	WATER - W-07 TRH(BTEX)MPAH
EM2421460-001	04-Dec-2024 00:00	Rinsate	✓	✓

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

Issue Date : 06-Dec-2024
Page : 3 of 3
Work Order : EM2421460 Amendment 0
Client : GEO-ENVIRONMENTAL SOLUTIONS



Proactive Holding Time Report

Sample(s) have been received within the recommended holding times for the requested analysis.

Requested Deliverables

All Invoices

- A4 - AU Tax Invoice (INV)

Email accounts@geosolutions.net.au

JOHN PAUL CUMMING

- *AU Certificate of Analysis - NATA (COA)
- *AU Interpretive QC Report - DEFAULT (Anon QCI Rep) (QCI)
- *AU QC Report - DEFAULT (Anon QC Rep) - NATA (QC)
- A4 - AU Sample Receipt Notification - Environmental HT (SRN)
- A4 - AU Tax Invoice (INV)
- Chain of Custody (CoC) (COC)
- EDI Format - ENMRG (ENMRG)
- EDI Format - ESDAT (ESDAT)

Email jcumming@geosolutions.net.au
Email jcumming@geosolutions.net.au
Email jcumming@geosolutions.net.au
Email jcumming@geosolutions.net.au
Email jcumming@geosolutions.net.au
Email jcumming@geosolutions.net.au
Email jcumming@geosolutions.net.au
Email jcumming@geosolutions.net.au

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

Appendix 6 Quality Assurance and Quality Control

Table with columns for Duplicate Comparison, Sample, and various chemical analytes. Includes rows for BH2 0.1-0.2 and Duplicate pairs, with data for Relative Percentage Difference (RPD) % and RPD Compliance Limit %.

For BH2 0.1-0.2 and Duplicate pairs, there were no non-compliances.

Table for Quality Control Blanks. Columns include analyte names (e.g., Arsenic, Barium, Bismuth) and detection limits. Includes a 'Date' column with the value 07/12/2024.

For rinsate sample, there were no detections <LOR.

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

QC EM2421460

QCI EM2421460

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.



QUALITY CONTROL REPORT			
Work Order	EM2421460	Page	1 of 15
Client	GEO-ENVIRONMENTAL SOLUTIONS	Laboratory	Environmental Division Melbourne
Contact	DR JOHN PAUL CUMMING	Contact	Katie Davis
Address	29 KIRKSWAY PLACE BATTERY POINT TASMANIA, AUSTRALIA 7004	Address	4 Westall Rd Springvale VIC Australia 3171
Telephone	+61 03 6223 1839	Telephone	+61-3-8549 9600
Project	65 Brisbane St	Date Samples Received	05-Dec-2024
Order number	----	Date Analysis Commenced	09-Dec-2024
C-O-C number	----	Issue Date	13-Dec-2024
Sampler	cc		
Site	----		
Quote number	EN/222		
No. of samples received	20		
No. of samples analysed	20		



Accreditation No. 825
 Accredited for compliance with
 ISO/IEC 17023 - Testing

This report supersedes any previous report(s) with this reference. Results apply to the sample(s) as submitted, unless the sampling was conducted by ALS. This document shall not be reproduced, except in full.

This Quality Control Report contains the following information:

- Laboratory Duplicate (DUP) Report; Relative Percentage Difference (RPD) and Acceptance Limits
- Method Blank (MB) and Laboratory Control Spike (LCS) Report; Recovery and Acceptance Limits
- Matrix Spike (MS) Report; Recovery and Acceptance Limits

Signatories

This document has been electronically signed by the authorized signatories below. Electronic signing is carried out in compliance with procedures specified in 21 CFR Part 11.

Signatories	Position	Accreditation Category
Dilani Fernando	Laboratory Coordinator	Melbourne Inorganics, Springvale, VIC
Eric Chau	Metals Team Leader	Melbourne Inorganics, Springvale, VIC
Nancy Wang	2IC Organic Chemist	Melbourne Inorganics, Springvale, VIC
Nancy Wang	2IC Organic Chemist	Melbourne Organics, Springvale, VIC
Niki Stepniwski	Senior Organic Instrument Chemist	Melbourne Inorganics, Springvale, VIC

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Project	65 Brisbane St



General Comments

The analytical procedures used by ALS have been developed from established internationally recognised procedures such as those published by the USEPA, APHA, AS and NEPM. In house developed procedures are fully validated and are often at the client request.

Where moisture determination has been performed, results are reported on a dry weight basis.

Where a reported less than (<) result is higher than the LOR, this may be due to primary sample extract/digestate dilution and/or insufficient sample for analysis. Where the LOR of a reported result differs from standard LOR, this may be due to high moisture content, insufficient sample (reduced weight employed) or matrix interference.

- Key:
- Anonymous = Refers to samples which are not specifically part of this work order but formed part of the QC process lot
 - CAS Number = CAS registry number from database maintained by Chemical Abstracts Services. The Chemical Abstracts Service is a division of the American Chemical Society.
 - LOR = Limit of reporting
 - RPD = Relative Percentage Difference
 - # = Indicates failed QC
 - * = The final LOR has been raised due to dilution or other sample specific cause; adjusted LOR is shown in brackets. The duplicate ranges for Acceptable RPD% are applied to the final LOR where applicable.

Laboratory Duplicate (DUP) Report

The quality control term Laboratory Duplicate refers to a randomly selected intralaboratory split. Laboratory duplicates provide information regarding method precision and sample heterogeneity. The permitted ranges for the Relative Percent Deviation (RPD) of Laboratory Duplicates are specified in ALS Method QW-EN38 and are dependent on the magnitude of results in comparison to the level of reporting: Result < 10 times LOR: No Limit; Result between 10 and 20 times LOR: 0% - 50%; Result > 20 times LOR: 0% - 20%.

Laboratory sample ID	Sample ID	Method: Compound	CAS Number	LOR	Unit	Laboratory Duplicate (DUP) Report					
						Original Result	Duplicate Result	RPD (%)	Acceptable RPD (%)		
EG005(ED093)T: Total Metals by ICP-AES (QC Lot: 6244350)											
EM2421423-021	Anonymous	EG005T: Manganese	7439-96-6	5	mg/kg	65	87	28.7	0% - 50%		
EM2421423-001	Anonymous	EG005T: Beryllium	7440-41-7	1	mg/kg	<1	<1	0.0	No Limit		
		EG005T: Cadmium	7440-43-9	1	mg/kg	<1	<1	0.0	No Limit		
		EG005T: Barium	7440-39-3	10	mg/kg	20	20	0.0	No Limit		
		EG005T: Chromium	7440-47-3	2	mg/kg	8	10	13.5	No Limit		
		EG005T: Cobalt	7440-48-4	2	mg/kg	<2	2	0.0	No Limit		
		EG005T: Nickel	7440-02-0	2	mg/kg	5	7	31.9	No Limit		
		EG005T: Arsenic	7440-38-2	5	mg/kg	<5	<5	0.0	No Limit		
		EG005T: Copper	7440-50-8	5	mg/kg	7	8	14.4	No Limit		
		EG005T: Lead	7439-92-1	5	mg/kg	51	54	7.4	0% - 50%		
		EG005T: Manganese	7439-96-6	5	mg/kg	30	39	24.6	No Limit		
		EG005T: Selenium	7782-49-2	5	mg/kg	<5	<5	0.0	No Limit		
		EG005T: Vanadium	7440-62-2	5	mg/kg	14	13	0.0	No Limit		
		EG005T: Zinc	7440-66-6	5	mg/kg	37	38	0.0	No Limit		
		EG005T: Boron	7440-42-6	50	mg/kg	<50	<50	0.0	No Limit		
		EM2421423-021	Anonymous	EG005T: Beryllium	7440-41-7	1	mg/kg	<1	<1	0.0	No Limit
				EG005T: Cadmium	7440-43-9	1	mg/kg	<1	<1	0.0	No Limit
EG005T: Barium	7440-39-3			10	mg/kg	30	30	0.0	No Limit		
EG005T: Chromium	7440-47-3			2	mg/kg	10	9	16.7	No Limit		
EG005T: Cobalt	7440-48-4			2	mg/kg	3	3	0.0	No Limit		
EG005T: Nickel	7440-02-0	2	mg/kg	9	10	0.0	No Limit				

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

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 Work Order : EM2421460
 Client : GEO-ENVIRONMENTAL SOLUTIONS
 Project : 65 Brisbane St



Sub-Matrix: SOIL				Laboratory Duplicate (DUP) Report							
Laboratory Sample ID	Sample ID	Method/Compound	CAS Number	LOR	Unit	Original Result	Duplicate Result	RPD (%)	Acceptable RPD (%)		
EG005(ED093): Total Metals by ICP-AES (QC Lot: 6244350) - continued											
EM2421423-021	Anonymous	EG005T: Arsenic	7440-38-2	5	mg/kg	<5	<5	0.0	No Limit		
		EG005T: Copper	7440-50-8	5	mg/kg	9	8	16.6	No Limit		
		EG005T: Lead	7439-92-1	5	mg/kg	52	55	5.5	0% - 50%		
		EG005T: Selenium	7782-49-2	5	mg/kg	<5	<5	0.0	No Limit		
		EG005T: Vanadium	7440-62-2	5	mg/kg	13	15	17.5	No Limit		
		EG005T: Zinc	7440-66-6	5	mg/kg	39	38	3.4	No Limit		
		EG005T: Boron	7440-42-8	50	mg/kg	<50	<50	0.0	No Limit		
EG005(ED093): Total Metals by ICP-AES (QC Lot: 6244351)											
EM2421460-005	BH1 1.0-1.1	EG005T: Cobalt	7440-48-4	2	mg/kg	26	# 75	65.8	0% - 20%		
		EG005T: Manganese	7439-96-5	5	mg/kg	58	# 208	71.6	0% - 20%		
EM2421460-005	BH1 1.0-1.1	EG005T: Beryllium	7440-41-7	1	mg/kg	4	5	29.4	No Limit		
		EG005T: Cadmium	7440-43-9	1	mg/kg	<1	<1	0.0	No Limit		
		EG005T: Barium	7440-39-3	10	mg/kg	<10	<10	0.0	No Limit		
		EG005T: Chromium	7440-47-3	2	mg/kg	7	7	0.0	No Limit		
		EG005T: Nickel	7440-02-0	2	mg/kg	18	25	31.7	0% - 50%		
		EG005T: Arsenic	7440-38-2	5	mg/kg	<5	<5	0.0	No Limit		
		EG005T: Copper	7440-50-8	5	mg/kg	<5	<5	0.0	No Limit		
		EG005T: Lead	7439-92-1	5	mg/kg	5	8	40.6	No Limit		
		EG005T: Selenium	7782-49-2	5	mg/kg	<5	<5	0.0	No Limit		
		EG005T: Vanadium	7440-62-2	5	mg/kg	9	13	33.4	No Limit		
		EG005T: Zinc	7440-66-6	5	mg/kg	32	40	20.8	No Limit		
		EG005T: Boron	7440-42-8	50	mg/kg	<50	<50	0.0	No Limit		
		EM2421460-014	BH4 0.5-0.6	EG005T: Beryllium	7440-41-7	1	mg/kg	<1	<1	0.0	No Limit
				EG005T: Cadmium	7440-43-9	1	mg/kg	<1	<1	0.0	No Limit
				EG005T: Barium	7440-39-3	10	mg/kg	150	130	11.9	0% - 50%
EG005T: Chromium	7440-47-3			2	mg/kg	10	11	0.0	No Limit		
EG005T: Cobalt	7440-48-4			2	mg/kg	6	6	0.0	No Limit		
EG005T: Nickel	7440-02-0			2	mg/kg	8	8	0.0	No Limit		
EG005T: Arsenic	7440-38-2			5	mg/kg	<5	<5	0.0	No Limit		
EG005T: Copper	7440-50-8			5	mg/kg	<5	<5	0.0	No Limit		
EG005T: Lead	7439-92-1			5	mg/kg	13	12	0.0	No Limit		
EG005T: Manganese	7439-96-5			5	mg/kg	26	26	0.0	No Limit		
EG005T: Selenium	7782-49-2			5	mg/kg	<5	<5	0.0	No Limit		
EG005T: Vanadium	7440-62-2			5	mg/kg	20	21	5.6	No Limit		
EG005T: Zinc	7440-66-6			5	mg/kg	14	14	0.0	No Limit		
EG005T: Boron	7440-42-8			50	mg/kg	<50	<50	0.0	No Limit		
EA055: Moisture Content (Dried @ 105-110°C) (QC Lot: 6248561)											

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 Client : GEO-ENVIRONMENTAL SOLUTIONS
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Sub-Matrix: SOIL				Laboratory Duplicate (DUP) Report							
Laboratory Sample ID	Sample ID	Method/Compound	CAS Number	LOR	Unit	Original Result	Duplicate Result	RPD (%)	Acceptable RPD (%)		
EA055: Moisture Content (Dried @ 105-110°C) (QC Lot: 6248561) - continued											
EM2421402-001	Anonymous	EA055: Moisture Content	---	0.1 (1.0)*	%	7.4	7.1	4.1	No Limit		
EM2421402-011	Anonymous	EA055: Moisture Content	---	0.1 (1.0)*	%	20.6	22.5	8.9	0% - 20%		
EA055: Moisture Content (Dried @ 105-110°C) (QC Lot: 6248562)											
EM2421460-007	Duplicate	EA055: Moisture Content	---	0.1 (1.0)*	%	7.5	7.7	2.0	No Limit		
EM2421460-017	BH5 0.5-0.6	EA055: Moisture Content	---	0.1 (1.0)*	%	16.9	15.6	7.9	0% - 50%		
EG035: Total Recoverable Mercury by FIMS (QC Lot: 6244349)											
EM2421423-001	Anonymous	EG035T: Mercury	7439-97-6	0.1	mg/kg	<0.1	<0.1	0.0	No Limit		
EM2421423-021	Anonymous	EG035T: Mercury	7439-97-6	0.1	mg/kg	<0.1	<0.1	0.0	No Limit		
EG035T: Total Recoverable Mercury by FIMS (QC Lot: 6244352)											
EM2421460-005	BH1 1.0-1.1	EG035T: Mercury	7439-97-6	0.1	mg/kg	<0.1	<0.1	0.0	No Limit		
EM2421460-014	BH4 0.5-0.6	EG035T: Mercury	7439-97-6	0.1	mg/kg	<0.1	<0.1	0.0	No Limit		
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons (QC Lot: 6244177)											
EM2421460-003	BH1 0.1-0.2	EP075(SIM): Naphthalene	91-20-3	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Acenaphthylene	208-96-8	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Acenaphthene	83-32-9	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Fluorene	86-73-7	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Phenanthrene	85-01-8	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Anthracene	120-12-7	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Fluoranthene	206-44-0	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Pyrene	129-00-0	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Benz[a]anthracene	56-55-3	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Chrysene	218-01-9	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Benzo[b]fluoranthene	205-99-2	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Benzo[k]fluoranthene	205-82-3	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Benzofluoranthene	207-06-9	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Benzofluorene	50-32-8	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Indeno[1,2,3-cd]perylene	193-39-5	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Dibenz[a,h]anthracene	53-70-3	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EP075(SIM): Benzo[g,h,i]perylene	191-24-2	0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
		EM2421460-013	BH4 0.1-0.2	EP075(SIM): Naphthalene	91-20-3	0.5	mg/kg	<0.5	<0.5	0.0	No Limit
				EP075(SIM): Acenaphthylene	208-96-8	0.5	mg/kg	<0.5	<0.5	0.0	No Limit
				EP075(SIM): Acenaphthene	83-32-9	0.5	mg/kg	<0.5	<0.5	0.0	No Limit
				EP075(SIM): Fluorene	86-73-7	0.5	mg/kg	<0.5	<0.5	0.0	No Limit
EP075(SIM): Phenanthrene	85-01-8			0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
EP075(SIM): Anthracene	120-12-7			0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
EP075(SIM): Fluoranthene	206-44-0			0.5	mg/kg	<0.5	<0.5	0.0	No Limit		
EP075(SIM): Pyrene	129-00-0			0.5	mg/kg	<0.5	<0.5	0.0	No Limit		

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Table with columns: Laboratory sample ID, Sample ID, Method/Compound, CAS Number, LOR, Unit, Original Result, Duplicate Result, RPD (%), Acceptable RPD (%). Sub-Matrix: SOIL. Laboratory Duplicate (DUP) Report. EP075(SIM): Polynuclear Aromatic Hydrocarbons (QC Lot: 6244177) - continued. EP080(071): Total Petroleum Hydrocarbons (QC Lot: 6244024).

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Table with columns: Laboratory sample ID, Sample ID, Method/Compound, CAS Number, LOR, Unit, Original Result, Duplicate Result, RPD (%), Acceptable RPD (%). Sub-Matrix: SOIL. Laboratory Duplicate (DUP) Report. EP080: BTEXN (QC Lot: 6244024). Sub-Matrix: WATER. Laboratory Duplicate (DUP) Report. EG020A-F: Dissolved Metals by ICP-MS (QC Lot: 6247118).

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Sub-Matrix: WATER				Laboratory Duplicate (DUP) Report					
Laboratory sample ID	Sample ID	Method/Compound	CAS Number	LOR	Unit	Original Result	Duplicate Result	RPD (%)	Acceptable RPD (%)
EG020F: Dissolved Metals by ICP-MS (QC Lot: 6247118) - continued									
EM2421401-017	Anonymous	EG020A-F: Lead	7439-92-1	0.001	mg/L	<0.001	<0.001	0.0	No Limit
		EG020A-F: Manganese	7439-96-5	0.001	mg/L	0.004	0.004	0.0	No Limit
		EG020A-F: Nickel	7440-02-0	0.001	mg/L	0.003	0.003	0.0	No Limit
		EG020A-F: Zinc	7440-66-6	0.005	mg/L	<0.005	0.005	0.0	No Limit
		EG020A-F: Selenium	7782-49-2	0.01	mg/L	<0.01	<0.01	0.0	No Limit
		EG020A-F: Vanadium	7440-62-2	0.01	mg/L	<0.01	<0.01	0.0	No Limit
		EG020A-F: Boron	7440-42-8	0.05	mg/L	0.09	0.09	0.0	No Limit
		EG020A-F: Cadmium	7440-43-9	0.0001	mg/L	<0.0001	<0.0001	0.0	No Limit
EM2421523-044	Anonymous	EG020A-F: Arsenic	7440-38-2	0.001	mg/L	<0.001	<0.001	0.0	No Limit
		EG020A-F: Beryllium	7440-41-7	0.001	mg/L	<0.001	<0.001	0.0	No Limit
		EG020A-F: Barium	7440-39-3	0.001	mg/L	<0.001	<0.001	0.0	No Limit
		EG020A-F: Chromium	7440-47-3	0.001	mg/L	<0.001	<0.001	0.0	No Limit
		EG020A-F: Cobalt	7440-48-4	0.001	mg/L	<0.001	<0.001	0.0	No Limit
		EG020A-F: Copper	7440-50-8	0.001	mg/L	<0.001	<0.001	0.0	No Limit
		EG020A-F: Lead	7439-92-1	0.001	mg/L	<0.001	<0.001	0.0	No Limit
		EG020A-F: Manganese	7439-96-5	0.001	mg/L	<0.001	0.001	0.0	No Limit
		EG020A-F: Nickel	7440-02-0	0.001	mg/L	<0.001	<0.001	0.0	No Limit
		EG020A-F: Zinc	7440-66-6	0.005	mg/L	<0.005	<0.005	0.0	No Limit
		EG020A-F: Selenium	7782-49-2	0.01	mg/L	<0.01	<0.01	0.0	No Limit
		EG020A-F: Vanadium	7440-62-2	0.01	mg/L	<0.01	<0.01	0.0	No Limit
EG020A-F: Boron	7440-42-8	0.05	mg/L	<0.05	<0.05	0.0	No Limit		
EG035F: Dissolved Mercury by FIMS (QC Lot: 6247119)									
EM2421460-001	Rinsate	EG035F: Mercury	7439-97-6	0.0001	mg/L	<0.0001	<0.0001	0.0	No Limit
EM2421523-009	Anonymous	EG035F: Mercury	7439-97-6	0.0001	mg/L	<0.0001	<0.0001	0.0	No Limit
EP08071: Total Petroleum Hydrocarbons (QC Lot: 6243067)									
EM2421404-019	Anonymous	EP071: C15 - C28 Fraction	---	100	µg/L	<100	<100	0.0	No Limit
		EP071: C10 - C14 Fraction	---	50	µg/L	<50	<50	0.0	No Limit
		EP071: C29 - C36 Fraction	---	50	µg/L	<50	<50	0.0	No Limit
EP08071: Total Petroleum Hydrocarbons (QC Lot: 6246370)									
EM2421455-001	Anonymous	EP080: C6 - C9 Fraction	---	20 (200)*	µg/L	8150	# 12800	44.0	0% - 20%
EM2421402-016	Anonymous	EP080: C6 - C9 Fraction	---	20	µg/L	<20	<20	0.0	No Limit
EP08071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QC Lot: 6243067)									
EM2421404-019	Anonymous	EP071: >C10 - C16 Fraction	---	100	µg/L	<100	<100	0.0	No Limit
		EP071: >C16 - C34 Fraction	---	100	µg/L	<100	<100	0.0	No Limit
		EP071: >C34 - C40 Fraction	---	100	µg/L	<100	<100	0.0	No Limit
EP08071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QC Lot: 6246370)									
EM2421455-001	Anonymous	EP080: C6 - C10 Fraction	C6_C10	20 (200)*	µg/L	9220	# 14500	44.8	0% - 20%

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Sub-Matrix: WATER				Laboratory Duplicate (DUP) Report					
Laboratory sample ID	Sample ID	Method/Compound	CAS Number	LOR	Unit	Original Result	Duplicate Result	RPD (%)	Acceptable RPD (%)
EP08071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QC Lot: 6246370) - continued									
EM2421402-016	Anonymous	EP080: C6 - C10 Fraction	C6_C10	20	µg/L	<20	<20	0.0	No Limit
EP080: BTEXN (QC Lot: 6246370)									
EM2421455-001	Anonymous	EP080: Benzene	71-43-2	1 (10)*	µg/L	314	314	0.0	0% - 20%
		EP080: Toluene	108-88-3	2 (10)*	µg/L	<10	<10	0.0	No Limit
		EP080: Ethylbenzene	100-41-4	2 (10)*	µg/L	218	234	7.8	0% - 20%
		EP080: meta- & para-Xylene	108-38-3	2 (20)*	µg/L	73	81	10.7	No Limit
		EP080: ortho-Xylene	95-47-6	2 (10)*	µg/L	23	24	5.5	No Limit
		EP080: Naphthalene	91-20-3	5 (10)*	µg/L	179	210	15.9	0% - 20%
		EP080: Benzene	71-43-2	1	µg/L	<1	<1	0.0	No Limit
		EP080: Toluene	108-88-3	2	µg/L	<2	<2	0.0	No Limit
EM2421402-016	Anonymous	EP080: Ethylbenzene	100-41-4	2	µg/L	<2	<2	0.0	No Limit
		EP080: meta- & para-Xylene	108-38-3	2	µg/L	<2	<2	0.0	No Limit
		EP080: ortho-Xylene	95-47-6	2	µg/L	<2	<2	0.0	No Limit
		EP080: Naphthalene	91-20-3	5	µg/L	<5	<5	0.0	No Limit

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Method Blank (MB) and Laboratory Control Sample (LCS) Report

The quality control term Method / Laboratory Blank refers to an analyte free matrix to which all reagents are added in the same volumes or proportions as used in standard sample preparation. The purpose of this QC parameter is to monitor potential laboratory contamination. The quality control term Laboratory Control Sample (LCS) refers to a certified reference material, or a known interference free matrix spiked with target analytes. The purpose of this QC parameter is to monitor method precision and accuracy independent of sample matrix. Dynamic Recovery Limits are based on statistical evaluation of processed LCS.

Sub-Matrix: SOL

Table with columns: Method, Concentration, CAS Number, LOR, Unit, Result, Spike Concentration, Laboratory Control Spike (LCS) Report (LCS, Low, High), and Acceptable Limits (%). Includes sections for Total Metals by ICP-AES (QCLot: 6244350) and Total Recoverable Mercury by FIMS (QCLot: 6244349).

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Sub-Matrix: SOL

Table with columns: Method, Concentration, CAS Number, LOR, Unit, Result, Spike Concentration, Laboratory Control Spike (LCS) Report (LCS, Low, High), and Acceptable Limits (%). Includes sections for Total Recoverable Mercury by FIMS (QCLot: 6244349) and Total Petroleum Hydrocarbons (QCLot: 6244177).

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Sub-Matrix: SOIL				Method Blank (MB) Report	Laboratory Control Spike (LCS) Report			
Method/Concentration	CAS Number	LOR	Unit	Result	Spike Concentration	Spike Recovery (%)	Acceptable Limits (%)	
EP080071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QCLot: 6244178) - continued								
EP071: <C10 - C16 Fraction	---	50	mg/kg	<50	1080 mg/kg	99.4	77.0	130
EP071: >C16 - C34 Fraction	---	100	mg/kg	<100	3640 mg/kg	97.2	81.5	120
EP071: >C34 - C40 Fraction	---	100	mg/kg	<100	270 mg/kg	98.7	73.3	137
EP080: BTEXN (QCLot: 6240989)								
EP080: Benzene	71-43-2	0.2	mg/kg	<0.2	2 mg/kg	74.3	61.6	117
EP080: Toluene	108-88-3	0.5	mg/kg	<0.5	2 mg/kg	77.7	65.8	125
EP080: Ethylbenzene	100-41-4	0.5	mg/kg	<0.5	2 mg/kg	76.6	65.8	124
EP080: meta- & para-Xylene	106-38-3	0.5	mg/kg	<0.5	4 mg/kg	80.4	64.8	134
EP080: ortho-Xylene	106-42-3	0.5	mg/kg	<0.5	2 mg/kg	84.9	68.7	132
EP080: Naphthalene	95-47-8	1	mg/kg	<1	0.5 mg/kg	111	61.8	123
EP080: BTEXN (QCLot: 6244024)								
EP080: Benzene	71-43-2	0.2	mg/kg	<0.2	2 mg/kg	90.0	61.6	117
EP080: Toluene	108-88-3	0.5	mg/kg	<0.5	2 mg/kg	86.9	65.8	125
EP080: Ethylbenzene	100-41-4	0.5	mg/kg	<0.5	2 mg/kg	86.4	65.8	124
EP080: meta- & para-Xylene	106-38-3	0.5	mg/kg	<0.5	4 mg/kg	85.5	64.8	134
EP080: ortho-Xylene	106-42-3	0.5	mg/kg	<0.5	2 mg/kg	87.7	68.7	132
EP080: Naphthalene	95-47-8	1	mg/kg	<1	0.5 mg/kg	94.6	61.8	123
Sub-Matrix: WATER				Method Blank (MB) Report	Laboratory Control Spike (LCS) Report			
Method/Concentration	CAS Number	LOR	Unit	Result	Spike Concentration	Spike Recovery (%)	Acceptable Limits (%)	
EG020F: Dissolved Metals by ICP-MS (QCLot: 6247118)								
EG020A-F: Arsenic	7440-38-2	0.001	mg/L	<0.001	0.1 mg/L	93.1	89.0	111
EG020A-F: Beryllium	7440-41-7	0.001	mg/L	<0.001	0.1 mg/L	103	85.0	112
EG020A-F: Barium	7440-39-3	0.001	mg/L	<0.001	0.1 mg/L	92.4	83.6	113
EG020A-F: Cadmium	7440-43-9	0.0001	mg/L	<0.0001	0.1 mg/L	88.0	83.5	111
EG020A-F: Chromium	7440-47-3	0.001	mg/L	<0.001	0.1 mg/L	95.3	83.2	109
EG020A-F: Cobalt	7440-48-4	0.001	mg/L	<0.001	0.1 mg/L	91.4	84.3	110
EG020A-F: Copper	7440-50-8	0.001	mg/L	<0.001	0.1 mg/L	91.3	83.1	107
EG020A-F: Lead	7439-92-1	0.001	mg/L	<0.001	0.1 mg/L	93.1	84.6	108
EG020A-F: Manganese	7439-96-5	0.001	mg/L	<0.001	0.1 mg/L	90.8	84.8	110
EG020A-F: Nickel	7440-02-0	0.001	mg/L	<0.001	0.1 mg/L	92.6	84.3	110
EG020A-F: Selenium	7782-49-2	0.01	mg/L	<0.01	0.1 mg/L	95.5	82.3	113
EG020A-F: Vanadium	7440-62-2	0.01	mg/L	<0.01	0.1 mg/L	95.4	83.7	110

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Sub-Matrix: WATER				Method Blank (MB) Report	Laboratory Control Spike (LCS) Report			
Method/Concentration	CAS Number	LOR	Unit	Result	Spike Concentration	Spike Recovery (%)	Acceptable Limits (%)	
EG020F: Dissolved Metals by ICP-MS (QCLot: 6247118) - continued								
EG020A-F: Zinc	7440-66-8	0.005	mg/L	<0.005	0.1 mg/L	97.8	86.3	112
EG020A-F: Boron	7440-42-8	0.05	mg/L	<0.05	0.5 mg/L	100	85.4	115
EG035F: Dissolved Mercury by FIMS (QCLot: 6247119)								
EG035F: Mercury	7439-97-6	0.0001	mg/L	<0.0001	0.01 mg/L	97.7	71.6	116
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons (QCLot: 6243063)								
EP075(SIM): Naphthalene	91-20-3	1	µg/L	<1.0	5 µg/L	88.8	42.9	114
EP075(SIM): Acenaphthylene	208-96-6	1	µg/L	<1.0	5 µg/L	93.0	48.6	119
EP075(SIM): Acenaphthene	83-32-9	1	µg/L	<1.0	5 µg/L	92.9	47.0	117
EP075(SIM): Fluorene	86-73-7	1	µg/L	<1.0	5 µg/L	96.9	49.5	119
EP075(SIM): Phenanthrene	85-01-8	1	µg/L	<1.0	5 µg/L	101	49.4	121
EP075(SIM): Anthracene	120-12-7	1	µg/L	<1.0	5 µg/L	108	48.4	122
EP075(SIM): Fluoranthene	206-44-0	1	µg/L	<1.0	5 µg/L	102	50.3	124
EP075(SIM): Pyrene	129-00-0	1	µg/L	<1.0	5 µg/L	104	50.0	126
EP075(SIM): Benz(a)anthracene	56-55-3	1	µg/L	<1.0	5 µg/L	93.4	49.4	127
EP075(SIM): Chrysene	218-01-9	1	µg/L	<1.0	5 µg/L	106	48.7	126
EP075(SIM): Benzo(b)fluoranthene	205-99-2	1	µg/L	<1.0	5 µg/L	78.7	54.5	134
EP075(SIM): Benzo(k)fluoranthene	205-82-3	1	µg/L	<1.0	5 µg/L	113	56.1	134
EP075(SIM): Benzo(a)pyrene	50-32-8	0.5	µg/L	<0.5	5 µg/L	101	55.6	135
EP075(SIM): Indeno(1,2,3-cd)pyrene	193-39-5	1	µg/L	<1.0	5 µg/L	102	54.4	126
EP075(SIM): Dibenzo(a,h)anthracene	53-70-3	1	µg/L	<1.0	5 µg/L	99.8	54.5	126
EP075(SIM): Benzo(g,h,i)perylene	191-04-2	1	µg/L	<1.0	5 µg/L	93.7	54.4	126
EP080071: Total Petroleum Hydrocarbons (QCLot: 6243067)								
EP071: C10 - C14 Fraction	---	50	µg/L	<50	4421 µg/L	72.5	47.2	122
EP071: C15 - C28 Fraction	---	100	µg/L	<100	15219 µg/L	78.5	52.9	131
EP071: C29 - C36 Fraction	---	50	µg/L	<50	7904 µg/L	80.7	50.4	127
EP080071: Total Petroleum Hydrocarbons (QCLot: 6246378)								
EP080: C6 - C9 Fraction	---	20	µg/L	<20	360 µg/L	101	66.2	134
EP080071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QCLot: 6243067)								
EP071: <C10 - C16 Fraction	---	100	µg/L	<100	6085 µg/L	79.8	49.1	125
EP071: >C16 - C34 Fraction	---	100	µg/L	<100	20300 µg/L	78.0	51.6	128
EP071: >C34 - C40 Fraction	---	100	µg/L	<100	1456 µg/L	88.7	47.2	130
EP080071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QCLot: 6246378)								
EP080: C6 - C10 Fraction	C6_C10	20	µg/L	<20	450 µg/L	99.1	66.2	132

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Sub-Matrix: WATER				Method/Blank (MS) Report	Laboratory Control Spike (LCS) Report			
Method/Contaminant	CAS Number	LOR	Unit	Result	Spike Concentration	Spike Recovery (%)	Acceptable Limits (%)	
							Low	High
EP080: BTEXN (QCLot: 6246370)								
EP080: Benzene	71-43-2	1	µg/L	<1	20 µg/L	102	68.8	127
EP080: Toluene	108-88-3	2	µg/L	<2	20 µg/L	102	72.9	129
EP080: Ethylbenzene	100-41-4	2	µg/L	<2	20 µg/L	100	71.7	130
EP080: meta- & para-Xylene	108-38-3	2	µg/L	<2	40 µg/L	108	72.3	136
EP080: ortho-Xylene	95-47-6	2	µg/L	<2	20 µg/L	110	75.9	134
EP080: Naphthalene	91-20-3	5	µg/L	<5	5 µg/L	107	68.3	131

Matrix Spike (MS) Report

The quality control term Matrix Spike (MS) refers to an intralaboratory split sample spiked with a representative set of target analytes. The purpose of this QC parameter is to monitor potential matrix effects on analyte recoveries. Static Recovery Limits as per laboratory Data Quality Objectives (DQOs). Ideal recovery ranges stated may be waived in the event of sample matrix interference.

Sub-Matrix: SOIL				Matrix Spike (MS) Report				
Laboratory sample ID	Sample ID	Method/Contaminant	CAS Number	Spike Concentration	Spike Recovery (%)	MS	Acceptable Limits (%)	
							Low	High
EG005(ED099)T: Total Metals by ICP-AES (QCLot: 6244350)								
EM2421423-003	Anonymous	EG005T: Arsenic	7440-38-2	50 mg/kg	105		78.0	124
		EG005T: Cadmium	7440-43-9	50 mg/kg	102		79.7	116
		EG005T: Chromium	7440-47-3	50 mg/kg	105		79.0	121
		EG005T: Copper	7440-50-8	250 mg/kg	103		80.0	120
		EG005T: Lead	7439-92-1	250 mg/kg	90.1		80.0	120
		EG005T: Nickel	7440-02-0	50 mg/kg	103		78.0	120
		EG005T: Zinc	7440-66-6	250 mg/kg	101		80.0	120
EG005(ED099)T: Total Metals by ICP-AES (QCLot: 6244351)								
EM2421460-006	BH2 0.1-0.2	EG005T: Nickel	7440-02-0	50 mg/kg	99.3		78.0	120
EM2421460-006	BH2 0.1-0.2	EG005T: Arsenic	7440-38-2	50 mg/kg	102		78.0	124
		EG005T: Cadmium	7440-43-9	50 mg/kg	102		79.7	116
		EG005T: Chromium	7440-47-3	50 mg/kg	102		79.0	121
		EG005T: Copper	7440-50-8	250 mg/kg	105		80.0	120
		EG005T: Lead	7439-92-1	250 mg/kg	103		80.0	120
		EG005T: Zinc	7440-66-6	250 mg/kg	104		80.0	120
EG035T: Total Recoverable Mercury by FIMS (QCLot: 6244349)								
EM2421423-003	Anonymous	EG035T: Mercury	7439-97-6	0.5 mg/kg	105		70.0	130
EG035T: Total Recoverable Mercury by FIMS (QCLot: 6244352)								
EM2421460-006	BH2 0.1-0.2	EG035T: Mercury	7439-97-6	0.5 mg/kg	107		70.0	130
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons (QCLot: 6244177)								
EM2421460-004	BH1 0.5-0.6	EP075(SIM): Acenaphthene	83-32-9	3 mg/kg	91.7		77.2	116

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Sub-Matrix: SOIL				Matrix Spike (MS) Report				
Laboratory sample ID	Sample ID	Method/Contaminant	CAS Number	Spike Concentration	Spike Recovery (%)	MS	Acceptable Limits (%)	
							Low	High
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons (QCLot: 6244177) - continued								
EM2421460-004	BH1 0.5-0.6	EP075(SIM): Pyrene	129-00-0	3 mg/kg	94.0		65.5	136
EP080/071: Total Petroleum Hydrocarbons (QCLot: 6240989)								
EM2421460-004	BH1 0.5-0.6	EP080: C6 - C9 Fraction	---	28 mg/kg	56.6		33.4	124
EP080/071: Total Petroleum Hydrocarbons (QCLot: 6244024)								
EM2421237-003	Anonymous	EP080: C6 - C9 Fraction	---	28 mg/kg	67.9		33.4	124
EP080/071: Total Petroleum Hydrocarbons (QCLot: 6244178)								
EM2421460-005	BH1 1.0-1.1	EP071: C10 - C14 Fraction	---	790 mg/kg	93.9		71.2	125
		EP071: C15 - C28 Fraction	---	2730 mg/kg	95.8		75.6	122
		EP071: C29 - C36 Fraction	---	1410 mg/kg	94.8		78.9	120
EP080/071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QCLot: 6240989)								
EM2421460-004	BH1 0.5-0.6	EP080: C6 - C10 Fraction	OR, C10	33 mg/kg	54.0		30.8	120
EP080/071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QCLot: 6244024)								
EM2421237-003	Anonymous	EP080: C6 - C10 Fraction	OR, C10	33 mg/kg	55.5		30.8	120
EP080/071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QCLot: 6244178)								
EM2421460-005	BH1 1.0-1.1	EP071: >C19 - C16 Fraction	---	1080 mg/kg	95.6		72.2	128
		EP071: >C16 - C34 Fraction	---	3640 mg/kg	94.0		76.5	119
		EP071: >C34 - C40 Fraction	---	270 mg/kg	97.1		66.8	138
EP080: BTEXN (QCLot: 6240989)								
EM2421460-004	BH1 0.5-0.6	EP080: Benzene	71-43-2	2 mg/kg	64.3		54.4	127
		EP080: Toluene	108-88-3	2 mg/kg	67.1		57.1	131
EP080: BTEXN (QCLot: 6244024)								
EM2421237-003	Anonymous	EP080: Benzene	71-43-2	2 mg/kg	91.5		54.4	127
		EP080: Toluene	108-88-3	2 mg/kg	89.7		57.1	131
Sub-Matrix: WATER								
Sub-Matrix: WATER				Matrix Spike (MS) Report				
Laboratory sample ID	Sample ID	Method/Contaminant	CAS Number	Spike Concentration	Spike Recovery (%)	MS	Acceptable Limits (%)	
							Low	High
EG020F: Dissolved Metals by ICP-MS (QCLot: 6247118)								
EM2421461-017	Anonymous	EG020A-F: Arsenic	7440-38-2	0.2 mg/L	95.9		76.6	124
		EG020A-F: Beryllium	7440-41-7	0.2 mg/L	109		73.0	120
		EG020A-F: Barium	7440-39-3	0.2 mg/L	95.7		75.0	127
		EG020A-F: Cadmium	7440-43-9	0.05 mg/L	93.7		74.6	118
		EG020A-F: Chromium	7440-47-3	0.2 mg/L	100		71.0	135
		EG020A-F: Cobalt	7440-49-4	0.2 mg/L	96.4		78.0	132
		EG020A-F: Copper	7440-50-8	0.2 mg/L	96.1		76.0	130
		EG020A-F: Lead	7439-92-1	0.2 mg/L	95.7		75.0	133

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Sub-Matrix: WATER				Matrix Spike (MS) Report			
Laboratory sample ID	Sample ID	Method/Compound	CAS Number	Spike	Acceptable Limits (%)		
				Concentration	MS	Low	High
EG020F: Dissolved Metals by ICP-MS (QCLot: 6247116) - continued							
EM2421401-017	Anonymous	EG020A-F: Manganese	7439-96-5	0.2 mg/L	96.6	64.0	134
		EG020A-F: Nickel	7440-02-0	0.2 mg/L	97.6	73.0	131
		EG020A-F: Vanadium	7440-62-2	0.2 mg/L	96.0	73.0	131
		EG020A-F: Zinc	7440-66-6	0.2 mg/L	101	75.0	131
EG035F: Dissolved Mercury by FIMS (QCLot: 6247119)							
EM2421523-008	Anonymous	EG035F: Mercury	7439-97-6	0.01 mg/L	107	70.0	120
EP080/071: Total Petroleum Hydrocarbons (QCLot: 6243067)							
EM2421404-020	Anonymous	EP071: C10 - C14 Fraction	---	4421 µg/L	62.5	48.0	126
		EP071: C15 - C28 Fraction	---	15219 µg/L	67.7	51.7	132
		EP071: C29 - C36 Fraction	---	7954 µg/L	68.3	50.5	127
EP080/071: Total Petroleum Hydrocarbons (QCLot: 6246370)							
EM2421402-017	Anonymous	EP080: C6 - C9 Fraction	---	290 µg/L	67.9	33.9	126
EP080/071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QCLot: 6243067)							
EM2421404-020	Anonymous	EP071: >C10 - C16 Fraction	---	6995 µg/L	68.7	48.0	129
		EP071: >C16 - C34 Fraction	---	20300 µg/L	68.3	50.4	130
		EP071: >C34 - C40 Fraction	---	1456 µg/L	74.8	47.4	131
EP080/071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions (QCLot: 6246370)							
EM2421402-017	Anonymous	EP080: C6 - C10 Fraction	C6, C10	330 µg/L	64.7	34.0	122
EP080: BTEXN (QCLot: 6246370)							
EM2421402-017	Anonymous	EP080: Benzene	71-43-2	20 µg/L	82.9	56.3	133
		EP080: Toluene	108-88-3	20 µg/L	83.6	60.4	132



QA/QC Compliance Assessment to assist with Quality Review

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Client : GEO-ENVIRONMENTAL SOLUTIONS	Laboratory : Environmental Division Melbourne
Contact : DR JOHN PAUL CUMMING	Telephone : +61-3-9549 9600
Project : 65 Brisbane St	Date Samples Received : 05-Dec-2024
Site : ---	Issue Date : 13-Dec-2024
Sampler : CC	No. of samples received : 20
Order number : ---	No. of samples analysed : 20

This report is automatically generated by the ALS LIMS through interpretation of the ALS Quality Control Report and several Quality Assurance parameters measured by ALS. This automated reporting highlights any non-conformances, facilitates faster and more accurate data validation and is designed to assist internal expert and external Auditor review. Many components of this report contribute to the overall DQO assessment and reporting for guideline compliance.

Brief method summaries and references are also provided to assist in traceability.

Summary of Outliers

Outliers : Quality Control Samples

This report highlights outliers flagged in the Quality Control (QC) Report.

- **NQ** Method Blank value outliers occur.
- **NQ** Laboratory Control outliers occur.
- **NQ** Matrix Spike outliers occur.
- Duplicate outliers exist - please see following pages for full details.
- For all regular sample matrices, where applicable to the methodology, **NQ** surrogate recovery outliers occur.

Outliers : Analysis Holding Time Compliance

- **NQ** Analysis Holding Time Outliers exist.

Outliers : Frequency of Quality Control Samples

- Quality Control Sample Frequency Outliers exist - please see following pages for full details.

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Outliers : Quality Control Samples

Duplicates, Method Blanks, Laboratory Control Samples and Matrix Spikes

Matrix: **SOIL**

Compound Group Name	Laboratory Sample ID	Client Sample ID	Analyte	CAS Number	Date	Limits	Comment
Duplicate (DUP) RPDs							
EG005(ED093)T: Total Metals by ICP-AES	EM2421460-005	BH1 1.0-1.1	Cobalt	7440-48-4	95.8 %	0% - 20%	RPD exceeds LOR based limits
EG005(ED093)T: Total Metals by ICP-AES	EM2421460-005	BH1 1.0-1.1	Manganese	7439-96-5	71.6 %	0% - 20%	RPD exceeds LOR based limits

Matrix: **WATER**

Compound Group Name	Laboratory Sample ID	Client Sample ID	Analyte	CAS Number	Date	Limits	Comment
Duplicate (DUP) RPDs							
EPO80071: Total Petroleum Hydrocarbons	EM2421455-001	Anonymous	C6 - C9 Fraction	---	44.0 %	0% - 20%	RPD exceeds LOR based limits
EPO80071: Total Recoverable Hydrocarbons - NEPM	EM2421455-001	Anonymous	C6 - C10 Fraction	C6, C10	44.8 %	0% - 20%	RPD exceeds LOR based limits

Outliers : Frequency of Quality Control Samples

Matrix: **WATER**

Quality Control Sample Type	Method	QC	Count	Rate (%)	Quality Control Specification
			Regular	Actual	Expected
Laboratory Duplicates (DUP)					
PAH/Phenols (GC/MS - SIM)	EP075(SIM)	0	8	0.00	10.00
TRH - Semivolatile Fraction	EP071	1	17	5.88	10.00
Matrix Spikes (MS)					
PAH/Phenols (GC/MS - SIM)	EP075(SIM)	0	8	0.00	5.00

Analysis Holding Time Compliance

If samples are identified below as having been analysed or extracted outside of recommended holding times, this should be taken into consideration when interpreting results. This report summarizes extraction / preparation and analysis times and compares each with ALS recommended holding times (referencing USEPA SW 846, APHA, AS and NEPM) based on the sample container provided. Dates reported represent first date of extraction or analysis and preclude subsequent dilutions and returns. A listing of breaches (if any) is provided herein. Holding time for leachate methods (e.g. TCLP) vary according to the analytes reported. Assessment compares the leach date with the shortest analyte holding time for the equivalent soil method. These are: organics 14 days, mercury 28 days & other metals 180 days. A recorded breach does not guarantee a breach for all non-volatile parameters. Holding times for VOC in soils vary according to analytes of interest. Vinyl Chloride and Styrene holding time is 7 days; others 14 days. A recorded breach does not guarantee a breach for all VOC analytes and should be verified in case the reported breach is a false positive as Vinyl Chloride and Styrene are not key analytes of interest/concern.

Matrix: **SOIL**

Method	Sample Date	Extraction / Preparation			Analysis		
		Date extracted	Due for extraction	Evaluation	Date analysed	Due for analysis	Evaluation

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Matrix: **SOIL**

Method	Sample Date	Extraction / Preparation			Analysis			
		Date extracted	Due for extraction	Evaluation	Date analysed	Due for analysis	Evaluation	
EAO55: Moisture Content (Dried @ 105-110°C)								
Soil Glass Jar - Unpreserved (EAO55)								
Trip Blank,								
BH1 0.5-0.6,	BH1 0.1-0.2,	04-Dec-2024	---	---	---	10-Dec-2024	18-Dec-2024	✓
BH2 0.1-0.2,	BH1 1.0-1.1,							
BH2 0.5-0.6,	Duplicate,							
BH3 0.1-0.2,	BH2 1.0-1.1,							
BH3 1.0-1.1,	BH3 0.5-0.6,							
BH4 0.5-0.6,	BH4 1.0-1.1,							
BH5 0.1-0.2,	BH5 0.5-0.6,							
BH5 1.0-1.1,	BH6 0.1-0.2,							
BH6 0.5-0.6,								
EG005(ED093)T: Total Metals by ICP-AES								
Soil Glass Jar - Unpreserved (EG005T)								
BH1 0.1-0.2,	BH1 0.5-0.6,	04-Dec-2024	10-Dec-2024	02-Jun-2025	✓	11-Dec-2024	02-Jun-2025	✓
BH1 1.0-1.1,	BH2 0.1-0.2,							
Duplicate,	BH2 0.5-0.6,							
BH2 1.0-1.1,	BH3 0.1-0.2,							
BH3 0.5-0.6,	BH3 1.0-1.1,							
BH4 0.1-0.2,	BH4 0.5-0.6,							
BH4 1.0-1.1,	BH5 0.1-0.2,							
BH5 0.5-0.6,	BH5 1.0-1.1,							
BH6 0.1-0.2,	BH6 0.5-0.6							
EG035T: Total Recoverable Mercury by FIMS								
Soil Glass Jar - Unpreserved (EG035T)								
BH1 0.1-0.2,	BH1 0.5-0.6,	04-Dec-2024	10-Dec-2024	01-Jan-2025	✓	11-Dec-2024	01-Jan-2025	✓
BH1 1.0-1.1,	BH2 0.1-0.2,							
Duplicate,	BH2 0.5-0.6,							
BH2 1.0-1.1,	BH3 0.1-0.2,							
BH3 0.5-0.6,	BH3 1.0-1.1,							
BH4 0.1-0.2,	BH4 0.5-0.6,							
BH4 1.0-1.1,	BH5 0.1-0.2,							
BH5 0.5-0.6,	BH5 1.0-1.1,							
BH6 0.1-0.2,	BH6 0.5-0.6							

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Method	Sample Date	Extraction / Preparation			Analysis		
		Date extracted	Due for extraction	Evaluation	Date analysed	Due for analysis	Evaluation
Matrix: SOIL							
EP075(SIMB): Polynuclear Aromatic Hydrocarbons							
Soil Glass Jar - Unpreserved (EP075(SIM))							
Trip Blank							
BH1 0.1-0.2,	04-Dec-2024	10-Dec-2024	18-Dec-2024	✓	11-Dec-2024	19-Jan-2025	✓
BH1 1.0-1.1,							
Duplicate,							
BH2 1.0-1.1,							
BH3 0.5-0.6,							
BH4 0.1-0.2,							
BH4 1.0-1.1,							
BH5 0.5-0.6,							
BH6 0.1-0.2,							
BH6 0.5-0.6,							
EP080(71): Total Petroleum Hydrocarbons							
Soil Glass Jar - Unpreserved (EP080)							
Trip Blank							
BH1 0.1-0.2,	04-Dec-2024	10-Dec-2024	18-Dec-2024	✓	10-Dec-2024	18-Dec-2024	✓
BH1 1.0-1.1,							
Duplicate,							
BH2 1.0-1.1,							
BH3 0.5-0.6,							
BH4 0.1-0.2,							
BH4 1.0-1.1,							
BH5 0.5-0.6,							
BH6 0.1-0.2,							
BH6 0.5-0.6,							
EP080(71): Total Recoverable Hydrocarbons - NEPM 2013 Fractions							
Soil Glass Jar - Unpreserved (EP080)							
Trip Blank							
BH1 0.1-0.2,	04-Dec-2024	10-Dec-2024	18-Dec-2024	✓	10-Dec-2024	18-Dec-2024	✓
BH1 1.0-1.1,							
Duplicate,							
BH2 1.0-1.1,							
BH3 0.5-0.6,							
BH4 0.1-0.2,							
BH4 1.0-1.1,							
BH5 0.5-0.6,							
BH6 0.1-0.2,							
BH6 0.5-0.6,							

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Method	Sample Date	Extraction / Preparation			Analysis		
		Date extracted	Due for extraction	Evaluation	Date analysed	Due for analysis	Evaluation
Matrix: SOIL							
EP080: BTEXN							
Soil Glass Jar - Unpreserved (EP080)							
Trip Blank							
BH1 0.1-0.2,	04-Dec-2024	10-Dec-2024	18-Dec-2024	✓	10-Dec-2024	18-Dec-2024	✓
BH1 1.0-1.1,							
Duplicate,							
BH2 1.0-1.1,							
BH3 0.5-0.6,							
BH4 0.1-0.2,							
BH4 1.0-1.1,							
BH5 0.5-0.6,							
BH6 0.1-0.2,							
BH6 0.5-0.6,							
Matrix: WATER							
EQ202F: Dissolved Metals by ICP-MS							
Clear Plastic Bottle - Filtered; Lab-acidified (EQ202A-F)							
Rinsate							
	04-Dec-2024	---	---	---	10-Dec-2024	02-Jan-2025	✓
EQ203F: Dissolved Mercury by FIMS							
Clear Plastic Bottle - Filtered; Lab-acidified (EQ203F)							
Rinsate							
	04-Dec-2024	---	---	---	10-Dec-2024	01-Jan-2025	✓
EP075(SIMB): Polynuclear Aromatic Hydrocarbons							
Amber Glass Bottle - Unpreserved (EP075(SIM))							
Rinsate							
	04-Dec-2024	09-Dec-2024	11-Dec-2024	✓	10-Dec-2024	18-Jan-2025	✓
EP080(71): Total Petroleum Hydrocarbons							
Amber Glass Bottle - Unpreserved (EP071)							
Rinsate							
	04-Dec-2024	09-Dec-2024	11-Dec-2024	✓	10-Dec-2024	18-Jan-2025	✓
Amber VDC Vial - Sulfuric Acid (EP080)							
Rinsate							
	04-Dec-2024	10-Dec-2024	18-Dec-2024	✓	10-Dec-2024	18-Dec-2024	✓
EP080(71): Total Recoverable Hydrocarbons - NEPM 2013 Fractions							
Amber Glass Bottle - Unpreserved (EP071)							
Rinsate							
	04-Dec-2024	09-Dec-2024	11-Dec-2024	✓	10-Dec-2024	18-Jan-2025	✓
Amber VDC Vial - Sulfuric Acid (EP080)							
Rinsate							
	04-Dec-2024	10-Dec-2024	18-Dec-2024	✓	10-Dec-2024	18-Dec-2024	✓
EP080: BTEXN							
Amber VDC Vial - Sulfuric Acid (EP080)							
Rinsate							
	04-Dec-2024	10-Dec-2024	18-Dec-2024	✓	10-Dec-2024	18-Dec-2024	✓

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Quality Control Parameter Frequency Compliance

The following report summarises the frequency of laboratory QC samples analysed within the analytical lot(s) in which the submitted sample(s) was(were) processed. Actual rate should be greater than or equal to the expected rate. A listing of breaches is provided in the Summary of Outliers.

Matrix: **SOIL** Evaluation: * = Quality Control frequency not within specification; ✓ = Quality Control frequency within specification

Quality Control Sample Type	Analytical Methods	Method	Count		Rate (%)			Quality Control Specification
			QC	Regular	Actual	Expected	Evaluation	
Laboratory Duplicates (DUP)								
Moisture Content		EA255	4	40	10.00	10.00	✓	NEPM 2013 B3 & ALS QC Standard
PAH/Phenols (SIM)		EP075(SIM)	2	18	11.11	10.00	✓	NEPM 2013 B3 & ALS QC Standard
Total Mercury by FIMS		EG035T	4	37	10.81	10.00	✓	NEPM 2013 B3 & ALS QC Standard
Total Metals by ICP-AES		EG005T	6	38	15.79	10.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH - Semivolatile Fraction		EP071	2	18	11.11	10.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH Volatiles/BTEX		EP080	4	35	11.43	10.00	✓	NEPM 2013 B3 & ALS QC Standard
Laboratory Control Samples (LCS)								
PAH/Phenols (SIM)		EP075(SIM)	1	18	5.56	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Total Mercury by FIMS		EG035T	2	37	5.41	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Total Metals by ICP-AES		EG005T	2	38	5.26	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH - Semivolatile Fraction		EP071	1	18	5.56	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH Volatiles/BTEX		EP080	2	35	5.71	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Method Blanks (MB)								
PAH/Phenols (SIM)		EP075(SIM)	1	18	5.56	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Total Mercury by FIMS		EG035T	2	37	5.41	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Total Metals by ICP-AES		EG005T	2	38	5.26	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH - Semivolatile Fraction		EP071	1	18	5.56	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH Volatiles/BTEX		EP080	2	35	5.71	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Matrix Spikes (MS)								
PAH/Phenols (SIM)		EP075(SIM)	1	18	5.56	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Total Mercury by FIMS		EG035T	2	37	5.41	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Total Metals by ICP-AES		EG005T	3	38	7.89	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH - Semivolatile Fraction		EP071	1	18	5.56	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH Volatiles/BTEX		EP080	2	35	5.71	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Matrix: WATER Evaluation: * = Quality Control frequency not within specification; ✓ = Quality Control frequency within specification								
Quality Control Sample Type	Analytical Methods	Method	Count		Rate (%)			Quality Control Specification
			QC	Regular	Actual	Expected	Evaluation	
Laboratory Duplicates (DUP)								
Dissolved Mercury by FIMS		EG035F	2	10	20.00	10.00	✓	NEPM 2013 B3 & ALS QC Standard
Dissolved Metals by ICP-MS - Suite A		EG020A-F	2	12	16.67	10.00	✓	NEPM 2013 B3 & ALS QC Standard
PAH/Phenols (GC/MS - SIM)		EP075(SIM)	0	8	0.00	10.00	✗	NEPM 2013 B3 & ALS QC Standard
TRH - Semivolatile Fraction		EP071	1	17	5.88	10.00	✗	NEPM 2013 B3 & ALS QC Standard
TRH Volatiles/BTEX		EP080	2	20	10.00	10.00	✓	NEPM 2013 B3 & ALS QC Standard
Laboratory Control Samples (LCS)								
Dissolved Mercury by FIMS		EG035F	1	10	10.00	5.00	✓	NEPM 2013 B3 & ALS QC Standard

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Matrix: **WATER** Evaluation: * = Quality Control frequency not within specification; ✓ = Quality Control frequency within specification

Quality Control Sample Type	Analytical Methods	Method	Count		Rate (%)			Quality Control Specification
			QC	Regular	Actual	Expected	Evaluation	
Laboratory Control Samples (LCS) - Continued								
Dissolved Metals by ICP-MS - Suite A		EG020A-F	1	12	8.33	5.00	✓	NEPM 2013 B3 & ALS QC Standard
PAH/Phenols (GC/MS - SIM)		EP075(SIM)	1	8	12.50	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH - Semivolatile Fraction		EP071	1	17	5.88	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH Volatiles/BTEX		EP080	1	20	5.00	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Method Blanks (MB)								
Dissolved Mercury by FIMS		EG035F	1	10	10.00	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Dissolved Metals by ICP-MS - Suite A		EG020A-F	1	12	8.33	5.00	✓	NEPM 2013 B3 & ALS QC Standard
PAH/Phenols (GC/MS - SIM)		EP075(SIM)	1	8	12.50	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH - Semivolatile Fraction		EP071	1	17	5.88	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH Volatiles/BTEX		EP080	1	20	5.00	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Matrix Spikes (MS)								
Dissolved Mercury by FIMS		EG035F	1	10	10.00	5.00	✓	NEPM 2013 B3 & ALS QC Standard
Dissolved Metals by ICP-MS - Suite A		EG020A-F	1	12	8.33	5.00	✓	NEPM 2013 B3 & ALS QC Standard
PAH/Phenols (GC/MS - SIM)		EP075(SIM)	0	8	0.00	5.00	✗	NEPM 2013 B3 & ALS QC Standard
TRH - Semivolatile Fraction		EP071	1	17	5.88	5.00	✓	NEPM 2013 B3 & ALS QC Standard
TRH Volatiles/BTEX		EP080	1	20	5.00	5.00	✓	NEPM 2013 B3 & ALS QC Standard

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Brief Method Summaries

The analytical procedures used by the Environmental Division have been developed from established internationally recognized procedures such as those published by the US EPA, APHA, AS and NEPM. In house developed procedures are employed in the absence of documented standards or by client request. The following report provides brief descriptions of the analytical procedures employed for results reported in the Certificate of Analysis. Sources from which ALS methods have been developed are provided within the Method Descriptions.

Analytical Methods	Method	Matrix	Method Description
Moisture Content	EA055	SOIL	In house: A gravimetric procedure based on weight loss over a 12 hour drying period at 105-110 degrees C. This method is compliant with NEPM Schedule B(3).
Total Metals by ICP-AES	EG005T	SOIL	In house: Referenced to APHA 3120; USEPA SW 846 - 6010. Metals are determined following an appropriate acid digestion of the soil. The ICPAES technique ionises samples in a plasma, emitting a characteristic spectrum based on metals present. Intensities at selected wavelengths are compared against those of matrix matched standards. This method is compliant with NEPM Schedule B(3).
Total Mercury by FIMS	EG035T	SOIL	In house: Referenced to APHA 3112 Hg - B (Flow-injection (SnCl2) (Cold Vapour generation) AAS) FIM-AAS is an automated flameless atomic absorption technique. Mercury in solids are determined following an appropriate acid digestion. Ionic mercury is reduced online to atomic mercury vapour by SnCl2 which is then purged into a heated quartz cell. Quantification is by comparing absorbance against a calibration curve. This method is compliant with NEPM Schedule B(3).
TRH - Semivolatile Fraction	EP071	SOIL	In house: Referenced to USEPA SW 846 - 8015 Sample extracts are analysed by Capillary GC/FID and quantified against alkane standards over the range C10 - C40. Compliant with NEPM Schedule B(3).
PAH/Phenols (SIM)	EP075(SIM)	SOIL	In house: Referenced to USEPA SW 846 - 8270. Extracts are analysed by Capillary GC/MS in Selective Ion Mode (SIM) and quantification is by comparison against an established 5 point calibration curve. This method is compliant with NEPM Schedule B(3).
TRH Volatiles/BTEX	EP080	SOIL	In house: Referenced to USEPA SW 846 - 8260. Extracts are analysed by Purge and Trap, Capillary GC/MS. Quantification is by comparison against an established 5 point calibration curve. Compliant with NEPM Schedule B(3) amended.
Dissolved Metals by ICP-MS - Suite A	EG020A-F	WATER	In house: Referenced to APHA 3125; USEPA SW846 - 6020, ALS QWI-ENE/EG020. Samples are 0.45µm filtered prior to analysis. The ICPMS technique utilizes a highly efficient argon plasma to ionize selected elements. Ions are then passed into a high vacuum mass spectrometer, which separates the analytes based on their distinct mass to charge ratios prior to their measurement by a discrete dynode ion detector.
Dissolved Mercury by FIMS	EG035F	WATER	In house: Referenced to APHA 3112 Hg - B (Flow-injection (SnCl2)(Cold Vapour generation) AAS) Samples are 0.45µm filtered prior to analysis. FIM-AAS is an automated flameless atomic absorption technique. A bromate/bromide reagent is used to oxidise any organic mercury compounds in the filtered sample. The ionic mercury is reduced online to atomic mercury vapour by SnCl2 which is then purged into a heated quartz cell. Quantification is by comparing absorbance against a calibration curve. This method is compliant with NEPM Schedule B(3).
TRH - Semivolatile Fraction	EP071	WATER	In house: Referenced to USEPA SW 846 - 8015 The sample extract is analysed by Capillary GC/FID and quantification is by comparison against an established 5 point calibration curve of n-Alkane standards. This method is compliant with the OC requirements of NEPM Schedule B(3).
PAH/Phenols (GC/MS - SIM)	EP075(SIM)	WATER	In house: Referenced to USEPA SW 846 - 8270 Sample extracts are analysed by Capillary GC/MS in SIM Mode and quantification is by comparison against an established 5 point calibration curve. This method is compliant with NEPM Schedule B(3).

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Analytical Methods	Method	Matrix	Method Description
TRH Volatiles/BTEX	EP080	WATER	In house: Referenced to USEPA SW 846 - 8260 Water samples are directly purged prior to analysis by Capillary GC/MS and quantification is by comparison against an established 5 point calibration curve. Alternatively, a sample is equilibrated in a headspace vial and a portion of the headspace determined by GCMS analysis. This method is compliant with the OC requirements of NEPM Schedule B(3).
Preparation Methods	Method	Matrix	Method Description
Hot Block Digest for metals in soils sediments and sludges	EN69	SOIL	In house: Referenced to USEPA 200.2. Hot Block Acid Digestion 1.0g of sample is heated with Nitric and Hydrochloric acids, then cooled. Peroxide is added and samples heated and cooled again before being filtered and bulked to volume for analysis. Digest is appropriate for determination of selected metals in sludge, sediments, and soils. This method is compliant with NEPM Schedule B(3).
Methanolic Extraction of Soils for Purge and Trap	ORG16	SOIL	In house: Referenced to USEPA SW 846 - 5030A. 5g of solid is shaken with surrogate and 10mL methanol prior to analysis by Purge and Trap - GC/MS.
Tumbler Extraction of Solids	ORG17	SOIL	In house: Mechanical agitation (tumbler). 10g of sample, Na2SO4 and surrogate are extracted with 30mL 1:1 DCM/Acetone by end over end tumble. The solvent is decanted, dehydrated and concentrated (by KD) to the desired volume for analysis.
Separatory Funnel Extraction of Liquids	ORG14	WATER	In house: Referenced to USEPA SW 846 - 3510 100 mL to 1L of sample is transferred to a separatory funnel and serially extracted three times using DCM for each extract. The resultant extracts are combined, dehydrated and concentrated for analysis. This method is compliant with NEPM Schedule B(3) - ALS default excludes sediment which may be resident in the container.
Volatiles Water Preparation	ORG16-W	WATER	A 5 mL aliquot or 5 mL of a diluted sample is added to a 40 mL VOC vial for purging.

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Appendix 7 Certificate of Analysis

COA EM2421460

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CERTIFICATE OF ANALYSIS

Work Order	: EM2421460	Page	: 1 of 18
Client	: GEO-ENVIRONMENTAL SOLUTIONS	Laboratory	: Environmental Division Melbourne
Contact	: DR JOHN PAUL CUMMING	Contact	: Katie Davis
Address	: 29 KIRKSWAY PLACE BATTERY POINT TASMANIA, AUSTRALIA 7004	Address	: 4 Westall Rd Springvale VIC Australia 3171
Telephone	: +61 03 6223 1839	Telephone	: +61-3-8549 9600
Project	: 65 Brisbane St	Date Samples Received	: 05-Dec-2024 10:30
Order number	: ----	Date Analysis Commenced	: 09-Dec-2024
C-O-C number	: ----	Issue Date	: 13-Dec-2024 13:14
Sampler	: CC		
Site	: ----		
Quote number	: EN/222		
No. of samples received	: 20		
No. of samples analysed	: 20		



This report supersedes any previous report(s) with this reference. Results apply to the sample(s) as submitted, unless the sampling was conducted by ALS. This document shall not be reproduced, except in full.

This Certificate of Analysis contains the following information:

- General Comments
- Analytical Results
- Surrogate Control Limits

Additional information pertinent to this report will be found in the following separate attachments: Quality Control Report, QA/QC Compliance Assessment to assist with Quality Review and Sample Receipt Notification.

Signatories

This document has been electronically signed by the authorized signatories below. Electronic signing is carried out in compliance with procedures specified in 21 CFR Part 11.

Signatories	Position	Accreditation Category
Dilani Fernando	Laboratory Coordinator	Melbourne Inorganics, Springvale, VIC
Eric Chau	Metals Team Leader	Melbourne Inorganics, Springvale, VIC
Nancy Wang	2IC Organic Chemist	Melbourne Inorganics, Springvale, VIC
Nancy Wang	2IC Organic Chemist	Melbourne Inorganics, Springvale, VIC
Nikki Stepniewski	Senior Inorganic Instrument Chemist	Melbourne Inorganics, Springvale, VIC

right solutions. right partner.

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General Comments

The analytical procedures used by ALS have been developed from established internationally recognised procedures such as those published by the USEPA, APHA, AS and NEPM. In house developed procedures are fully validated and are often at the client request.

Where moisture determination has been performed, results are reported on a dry weight basis.

Where a reported less than (<) result is higher than the LOR, this may be due to primary sample extract/digestate dilution and/or insufficient sample for analysis.

Where the LOR of a reported result differs from standard LOR, this may be due to high moisture content, insufficient sample (reduced weight employed) or matrix interference.

When sampling time information is not provided by the client, sampling dates are shown without a time component. In these instances, the time component has been assumed by the laboratory for processing purposes.

Where a result is required to meet compliance limits the associated uncertainty must be considered. Refer to the ALS Contract for details.

Key: CAS Number = CAS registry number from database maintained by Chemical Abstracts Services. The Chemical Abstracts Service is a division of the American Chemical Society.
 LOR = Limit of reporting
 * = This result is computed from individual analyte detections at or above the level of reporting
 @ = ALS is not NATA accredited for these tests.
 = indicates an estimated value.

- EQ005T: EM2421460 #5. Poor duplicate precision for manganese due to sample heterogeneity. Confirmed by re-extraction and re-analysis. The following results were observed: 98 mg/kg, 208 mg/kg 90 mg/kg and 148 mg/kg.
- EP075 (SIM): Where reported, Benzo(a)pyrene Toxicity Equivalent Quotient (TEQ) per the NEPM (2013) is the sum total of the concentration of the eight carcinogenic PAHs multiplied by their Toxicity Equivalence Factor (TEF) relative to Benzo(a)pyrene. TEF values are provided in brackets as follows: Benz(a)anthracene (0.1), Chrysene (0.01), Benzo(b+h) & Benzo(k)fluoranthene (0.1), Benzo(a)pyrene (1.0), Indeno(1,2,3-cd)pyrene (0.1), Dibenz(a,h)anthracene (1.0), Benzo(g,h,i)perylene (0.01). Less than LOR results for 'TEQ Zero' are treated as zero.
- Benzo(a)pyrene Toxicity Equivalent Quotient (TEQ) per the NEPM (2013) is the sum total of the concentration of the eight carcinogenic PAHs multiplied by their Toxicity Equivalence Factor (TEF) relative to Benzo(a)pyrene. TEF values are provided in brackets as follows: Benz(a)anthracene (0.1), Chrysene (0.01), Benzo(b+h) & Benzo(k)fluoranthene (0.1), Benzo(a)pyrene (1.0), Indeno(1,2,3-cd)pyrene (0.1), Dibenz(a,h)anthracene (1.0), Benzo(g,h,i)perylene (0.01). Less than LOR results for 'TEQ Zero' are treated as zero, for 'TEQ 1/2LOR' are treated as half the reported LOR, and for 'TEQ LOR' are treated as being equal to the reported LOR. Note: TEQ 1/2LOR and TEQ LOR will calculate as 0.6mg/Kg and 1.2mg/Kg respectively for samples with non-detects for all of the eight TEQ PAHs.
- EP080: Where reported, Total Xylenes is the sum of the reported concentrations of m&p-Xylene and o-Xylene at or above the LOR.
- EP075(SIM): Where reported, Total Cresol is the sum of the reported concentrations of 2-Methylphenol and 3- & 4-Methylphenol at or above the LOR.
- EP080: Poor duplicate precision for EM2421455_001 due to sample heterogeneity. Confirmed by re-analysis.
- EQ005T: EM2421460 #5. Poor duplicate precision for cobalt due to sample heterogeneity. Confirmed by re-extraction and re-analysis. The following results were observed: 19 mg/kg, 26 mg/kg 75 mg/kg and 37 mg/kg.

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)				Sample ID	Trip Blank	BH1 0.1-0.2	BH1 0.5-0.6	BH1 1.0-1.1	BH2 0.1-0.2
				Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00
Compound	CAS Number	LOR	Unit	Result	Result	Result	Result	Result	Result
EAD55: Moisture Content (Dried @ 105-110°C)									
Moisture Content	---	1.0	%	<1.0	8.3	11.0	10.1	7.9	
EQ005(ED003)T: Total Metals by ICP-AES									
Arsenic	7440-39-2	5	mg/kg	---	9	<5	<5	10	
Barium	7440-39-3	10	mg/kg	---	60	20	<10	40	
Beryllium	7440-41-7	1	mg/kg	---	2	<1	4	2	
Boron	7440-42-8	50	mg/kg	---	<50	<50	<50	<50	
Cadmium	7440-43-9	1	mg/kg	---	<1	<1	<1	<1	
Chromium	7440-47-3	2	mg/kg	---	14	5	7	12	
Cobalt	7440-48-4	2	mg/kg	---	14	18	26	10	
Copper	7440-50-8	5	mg/kg	---	18	<5	<5	18	
Lead	7439-92-1	5	mg/kg	---	32	14	5	11	
Manganese	7439-96-5	5	mg/kg	---	275	172	98	282	
Nickel	7440-02-0	2	mg/kg	---	16	18	18	16	
Selenium	7782-49-2	5	mg/kg	---	<5	<5	<5	<5	
Vanadium	7440-62-2	5	mg/kg	---	24	9	9	25	
Zinc	7440-66-6	5	mg/kg	---	69	41	32	57	
EQ035T: Total Recoverable Mercury by FIMS									
Mercury	7439-97-6	0.1	mg/kg	---	0.1	<0.1	<0.1	<0.1	
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons									
Naphthalene	91-20-3	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Acenaphthylene	208-96-8	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Acenaphthene	83-32-9	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Fluorene	86-73-7	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Phenanthrene	85-01-8	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Anthracene	120-12-7	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Fluoranthene	206-44-0	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Pyrene	129-00-0	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)				Sample ID	Trip Blank	BH1 0.1-0.2	BH1 0.5-0.6	BH1 1.0-1.1	BH2 0.1-0.2
				Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00
Compound	CAS Number	LOR	Unit	Result	Result	Result	Result	Result	Result
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons - Continued									
Benzo(a)anthracene	56-55-3	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Chrysene	218-01-9	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Benzo(b)fluoranthene	205-99-2	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Benzo(k)fluoranthene	207-08-9	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Benzo(a)pyrene	50-32-8	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Indeno(1,2,3-cd)pyrene	193-39-5	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Dibenz(a,h)anthracene	53-70-3	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
Benzo(g,h)perylene	191-24-2	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
^a Sum of polycyclic aromatic hydrocarbons	---	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
^a Benzo(a)pyrene TEQ (zero)	---	0.5	mg/kg	---	<0.5	<0.5	<0.5	<0.5	
^a Benzo(a)pyrene TEQ (half LOR)	---	0.5	mg/kg	---	0.6	0.6	0.6	0.6	
^a Benzo(a)pyrene TEQ (LOR)	---	0.5	mg/kg	---	1.2	1.2	1.2	1.2	
EP080(071): Total Petroleum Hydrocarbons									
C6 - C9 Fraction	---	10	mg/kg	<10	<10	<10	<10	<10	
C10 - C14 Fraction	---	50	mg/kg	---	<50	<50	<50	<50	
C15 - C28 Fraction	---	100	mg/kg	---	150	<100	<100	<100	
C29 - C36 Fraction	---	100	mg/kg	---	150	<100	<100	<100	
^a C10 - C36 Fraction (sum)	---	50	mg/kg	---	300	<50	<50	<50	
EP080(071): Total Recoverable Hydrocarbons - NEPM 2013 Fractions									
C6 - C10 Fraction	C6_C10	10	mg/kg	<10	<10	<10	<10	<10	
^a C6 - C10 Fraction minus BTEX (F1)	C6_C10-BTEX	10	mg/kg	<10	<10	<10	<10	<10	
>C10 - C16 Fraction	---	50	mg/kg	---	<50	<50	<50	<50	
>C16 - C34 Fraction	---	100	mg/kg	---	230	<100	<100	<100	
>C34 - C40 Fraction	---	100	mg/kg	---	200	<100	<100	<100	
^a >C10 - C40 Fraction (sum)	---	50	mg/kg	---	430	<50	<50	<50	
^a >C10 - C16 Fraction minus Naphthalene (F2)	---	50	mg/kg	---	<50	<50	<50	<50	

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)				Sample ID	Trip Blank	BH1 0.1-0.2	BH1 0.5-0.6	BH1 1.0-1.1	BH2 0.1-0.2
				Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00
Compound	CAS Number	LOR	Unit		EM2421460-002	EM2421460-003	EM2421460-004	EM2421460-005	EM2421460-006
					Result	Result	Result	Result	Result
EP080: BTEXN									
Benzene	71-43-2	0.2	mg/kg		<0.2	<0.2	<0.2	<0.2	<0.2
Toluene	108-88-3	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Ethylbenzene	100-41-4	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
meta- & para-Xylene	106-38-3	106-42-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	<0.5
ortho-Xylene	95-47-6	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
^ Sum of BTEX	---	0.2	mg/kg		<0.2	<0.2	<0.2	<0.2	<0.2
^ Total Xylenes	---	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Naphthalene	91-20-3	1	mg/kg		<1	<1	<1	<1	<1
EP075(SIM): Phenolic Compound Surrogates									
Phenol-d6	13127-88-3	0.5	%		---	92.5	86.3	87.5	84.9
2-Chlorophenol-d4	93951-73-6	0.5	%		---	90.5	99.0	99.5	95.3
2,4,6-Tribromophenol	118-79-6	0.5	%		---	74.0	72.8	72.6	74.6
EP075(SIM): PAH Surrogates									
2-Fluorobiphenyl	321-60-8	0.5	%		---	92.6	93.8	93.6	94.4
Anthracene-d10	1719-06-8	0.5	%		---	99.6	102	102	102
4-Terphenyl-d14	1718-51-0	0.5	%		---	93.6	95.6	96.3	95.2
EP0805: TPH(V)/BTEX Surrogates									
1,2-Dichloroethane-D4	17060-07-0	0.2	%		96.7	72.0	62.0	70.9	73.2
Toluene-d8	2037-26-5	0.2	%		89.1	75.5	66.9	73.2	76.8
4-Bromofluorobenzene	460-00-4	0.2	%		89.5	90.6	80.1	88.9	93.4

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)				Sample ID	Duplicate	BH2 0.5-0.6	BH2 1.0-1.1	BH3 0.1-0.2	BH3 0.5-0.6
				Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00
Compound	CAS Number	LOR	Unit		EM2421460-007	EM2421460-008	EM2421460-009	EM2421460-010	EM2421460-011
					Result	Result	Result	Result	Result
EA055: Moisture Content (Dried @ 105-110°C)									
Moisture Content	---	1.0	%		7.5	22.0	13.9	14.3	12.9
EQ005(ED003)T: Total Metals by ICP-AES									
Arsenic	7440-39-2	5	mg/kg		8	8	<5	12	<5
Barium	7440-39-3	10	mg/kg		30	290	150	210	40
Beryllium	7440-41-7	1	mg/kg		2	<1	<1	<1	<1
Boron	7440-42-8	50	mg/kg		<50	<50	<50	<50	<50
Cadmium	7440-43-9	1	mg/kg		<1	<1	<1	<1	<1
Chromium	7440-47-3	2	mg/kg		12	18	24	17	6
Cobalt	7440-48-4	2	mg/kg		13	8	7	8	3
Copper	7440-50-8	5	mg/kg		13	75	24	87	<5
Lead	7439-92-1	5	mg/kg		9	445	119	289	19
Manganese	7439-96-5	5	mg/kg		269	352	368	113	18
Nickel	7440-02-0	2	mg/kg		14	12	8	19	4
Selenium	7782-49-2	5	mg/kg		<5	<5	<5	<5	<5
Vanadium	7440-62-2	5	mg/kg		22	18	14	26	14
Zinc	7440-66-6	5	mg/kg		49	519	116	275	20
EQ025T: Total Recoverable Mercury by FIMS									
Mercury	7439-97-6	0.1	mg/kg		<0.1	3.5	1.8	0.9	0.1
EP075(SIM): Polynuclear Aromatic Hydrocarbons									
Naphthalene	91-20-3	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Acenaphthylene	208-96-8	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Acenaphthene	83-32-9	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Fluorene	86-73-7	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Phenanthrene	85-01-8	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Anthracene	120-12-7	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Fluoranthene	206-44-0	0.5	mg/kg		<0.5	1.5	<0.5	1.0	<0.5
Pyrene	129-00-0	0.5	mg/kg		<0.5	1.6	<0.5	1.0	<0.5

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)		Sample ID	Duplicate	BH2 0.5-0.6	BH2 1.0-1.1	BH3 0.1-0.2	BH3 0.5-0.6
		Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00
Compound	CAS Number	LOR	Unit	EM2421460-007	EM2421460-008	EM2421460-009	EM2421460-011
				Result	Result	Result	Result
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons - Continued							
Benzo(a)anthracene	56-55-3	0.5	mg/kg	<0.5	1.1	<0.5	<0.5
Chrysene	218-01-9	0.5	mg/kg	<0.5	1.1	<0.5	<0.5
Benzo(b)fluoranthene	205-99-2	0.5	mg/kg	<0.5	1.6	<0.5	<0.5
Benzo(k)fluoranthene	207-08-9	0.5	mg/kg	<0.5	0.5	<0.5	<0.5
Benzo(a)pyrene	50-32-8	0.5	mg/kg	<0.5	1.4	<0.5	<0.5
Indeno(1,2,3-cd)pyrene	193-39-5	0.5	mg/kg	<0.5	0.8	<0.5	<0.5
Dibenz(a,h)anthracene	53-70-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Benzo(g,h)perylene	191-24-2	0.5	mg/kg	<0.5	1.0	<0.5	<0.5
^A Sum of polycyclic aromatic hydrocarbons	---	0.5	mg/kg	<0.5	10.6	<0.5	<0.5
^A Benzo(a)pyrene TEQ (zero)	---	0.5	mg/kg	<0.5	1.8	<0.5	<0.5
^A Benzo(a)pyrene TEQ (half LOR)	---	0.5	mg/kg	0.6	2.1	0.6	1.3
^A Benzo(a)pyrene TEQ (LOR)	---	0.5	mg/kg	1.2	2.3	1.2	1.2
EP080(071): Total Petroleum Hydrocarbons							
C6 - C9 Fraction	---	10	mg/kg	<10	<10	<10	<10
C10 - C14 Fraction	---	50	mg/kg	<50	<50	<50	<50
C15 - C28 Fraction	---	100	mg/kg	<100	<100	<100	<100
C29 - C36 Fraction	---	100	mg/kg	<100	<100	<100	<100
^A C10 - C36 Fraction (sum)	---	50	mg/kg	<50	<50	<50	<50
EP080(071): Total Recoverable Hydrocarbons - NEPM 2013 Fractions							
C6 - C10 Fraction	C6_C10	10	mg/kg	<10	<10	<10	<10
^A C6 - C10 Fraction minus BTEX (F1)	C6_C10-BTEX	10	mg/kg	<10	<10	<10	<10
>C10 - C16 Fraction	---	50	mg/kg	<50	<50	<50	<50
>C16 - C34 Fraction	---	100	mg/kg	<100	130	<100	170
>C34 - C40 Fraction	---	100	mg/kg	<100	<100	<100	<100
^A >C10 - C40 Fraction (sum)	---	50	mg/kg	<50	130	<50	170
^A >C10 - C16 Fraction minus Naphthalene (F2)	---	50	mg/kg	<50	<50	<50	<50

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)		Sample ID	Duplicate	BH2 0.5-0.6	BH2 1.0-1.1	BH3 0.1-0.2	BH3 0.5-0.6
		Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00
Compound	CAS Number	LOR	Unit	EM2421460-007	EM2421460-008	EM2421460-009	EM2421460-011
				Result	Result	Result	Result
EP080: BTEXN							
Benzene	71-43-2	0.2	mg/kg	<0.2	<0.2	<0.2	<0.2
Toluene	108-88-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Ethylbenzene	100-41-4	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
meta- & para-Xylene	108-38-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
ortho-Xylene	95-47-6	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
^A Sum of BTEX	---	0.2	mg/kg	<0.2	<0.2	<0.2	<0.2
^A Total Xylenes	---	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Naphthalene	91-20-3	1	mg/kg	<1	<1	<1	<1
EP075(SIM)S: Phenolic Compound Surrogates							
Phenol-d6	13127-69-3	0.5	%	81.0	90.1	81.9	102
2-Chlorophenol-d4	93951-73-6	0.5	%	88.0	93.4	91.8	106
2,4,6-Tribromophenol	118-79-6	0.5	%	73.6	75.3	74.2	93.7
EP075(SIM)T: PAH Surrogates							
2-Fluorobiphenyl	321-60-8	0.5	%	92.6	82.0	93.2	93.0
Anthracene-d10	1719-06-8	0.5	%	99.5	103	102	115
4-Terphenyl-d14	1718-51-0	0.5	%	93.3	90.2	95.8	99.3
EP080S: TPH(V)/BTEX Surrogates							
1,2-Dichloroethane-d4	17060-07-0	0.2	%	73.6	75.3	68.3	71.1
Toluene-d8	2037-26-5	0.2	%	77.0	75.8	67.3	72.7
4-Bromofluorobenzene	460-00-4	0.2	%	92.6	91.9	83.3	88.7

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)				Sample ID	BH3 1.0-1.1	BH4 0.1-0.2	BH4 0.5-0.6	BH4 1.0-1.1	BH5 0.1-0.2
				Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00
Compound	CAS Number	LOR	Unit		EM2421460-012	EM2421460-013	EM2421460-014	EM2421460-015	EM2421460-016
					Result	Result	Result	Result	Result
EAD55: Moisture Content (Dried @ 105-110°C)									
Moisture Content	---	1.0	%		15.7	12.3	16.0	11.3	10.5
EG005(ED003)T: Total Metals by ICP-AES									
Arsenic	7440-39-2	5	mg/kg		8	<5	<5	5	12
Barium	7440-39-3	10	mg/kg		10	110	150	80	40
Beryllium	7440-41-7	1	mg/kg		9	<1	<1	4	<1
Boron	7440-42-8	50	mg/kg		<50	<50	<50	<50	<50
Cadmium	7440-43-9	1	mg/kg		<1	<1	<1	<1	<1
Chromium	7440-47-3	2	mg/kg		6	10	10	6	10
Cobalt	7440-48-4	2	mg/kg		91	5	6	108	3
Copper	7440-50-8	5	mg/kg		17	29	<5	9	8
Lead	7439-92-1	5	mg/kg		11	145	13	8	17
Manganese	7439-96-5	5	mg/kg		710	134	26	300	60
Nickel	7440-02-0	2	mg/kg		96	8	6	24	7
Selenium	7782-49-2	5	mg/kg		<5	<5	<5	<5	<5
Vanadium	7440-62-2	5	mg/kg		12	28	20	6	15
Zinc	7440-66-6	5	mg/kg		87	113	14	18	51
EG035T: Total Recoverable Mercury by FIMS									
Mercury	7439-97-6	0.1	mg/kg		<0.1	0.1	<0.1	<0.1	0.5
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons									
Naphthalene	91-20-3	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Acenaphthylene	208-96-8	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Acenaphthene	83-32-9	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Fluorene	86-73-7	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Phenanthrene	85-01-8	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Anthracene	120-12-7	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Fluoranthene	206-44-0	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Pyrene	129-00-0	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)				Sample ID	BH3 1.0-1.1	BH4 0.1-0.2	BH4 0.5-0.6	BH4 1.0-1.1	BH5 0.1-0.2
				Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00
Compound	CAS Number	LOR	Unit		EM2421460-012	EM2421460-013	EM2421460-014	EM2421460-015	EM2421460-016
					Result	Result	Result	Result	Result
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons - Continued									
Benzo(a)anthracene	56-55-3	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Chrysene	218-01-9	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Benzo(b)fluoranthene	205-99-2	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Benzo(k)fluoranthene	207-08-9	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Benzo(a)pyrene	50-32-8	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Indeno(1,2,3-cd)pyrene	193-39-5	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Dibenzo(a,h)anthracene	53-70-3	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
Benzo(g,h)perylene	191-24-2	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
^a Sum of polycyclic aromatic hydrocarbons	---	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
^a Benzo(a)pyrene TEQ (zero)	---	0.5	mg/kg		<0.5	<0.5	<0.5	<0.5	<0.5
^a Benzo(a)pyrene TEQ (half LOR)	---	0.5	mg/kg		0.6	0.6	0.6	0.6	0.6
^a Benzo(a)pyrene TEQ (LOR)	---	0.5	mg/kg		1.2	1.2	1.2	1.2	1.2
EP080/071: Total Petroleum Hydrocarbons									
C6 - C9 Fraction	---	10	mg/kg		<10	<10	<10	<10	<10
C10 - C14 Fraction	---	50	mg/kg		<50	<50	<50	<50	<50
C15 - C28 Fraction	---	100	mg/kg		<100	<100	<100	<100	<100
C29 - C36 Fraction	---	100	mg/kg		<100	<100	<100	<100	<100
^a C10 - C36 Fraction (sum)	---	50	mg/kg		<50	<50	<50	<50	<50
EP080/071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions									
C6 - C10 Fraction	C6_C10	10	mg/kg		<10	<10	<10	<10	<10
^a C6 - C10 Fraction minus BTEX (F1)	C6_C10-BTEX	10	mg/kg		<10	<10	<10	<10	<10
>C10 - C16 Fraction	---	50	mg/kg		<50	<50	<50	<50	<50
>C16 - C34 Fraction	---	100	mg/kg		<100	<100	<100	<100	<100
>C34 - C40 Fraction	---	100	mg/kg		<100	160	<100	<100	<100
^a >C10 - C40 Fraction (sum)	---	50	mg/kg		<50	160	<50	<50	<50
^a >C10 - C16 Fraction minus Naphthalene (F2)	---	50	mg/kg		<50	<50	<50	<50	<50

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)				Sample ID	BH3 1.0-1.1	BH4 0.1-0.2	BH4 0.5-0.6	BH4 1.0-1.1	BH5 0.1-0.2
				Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00
Compound	CAS Number	LOR	Unit	EM2421460-012	EM2421460-013	EM2421460-014	EM2421460-015	EM2421460-016	
				Result	Result	Result	Result	Result	
EP080: BTEXN									
Benzene	71-43-2	0.2	mg/kg	<0.2	<0.2	<0.2	<0.2	<0.2	
Toluene	108-88-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	<0.5	
Ethylbenzene	100-41-4	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	<0.5	
meta- & para-Xylene	106-38-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	<0.5	
ortho-Xylene	95-47-6	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	<0.5	
^ Sum of BTEX	---	0.2	mg/kg	<0.2	<0.2	<0.2	<0.2	<0.2	
^ Total Xylenes	---	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	<0.5	
Naphthalene	91-20-3	1	mg/kg	<1	<1	<1	<1	<1	
EP075(SIM): Phenolic Compound Surrogates									
Phenol-d6	13127-88-3	0.5	%	87.2	79.7	85.6	83.6	83.6	
2-Chlorophenol-d4	93951-73-6	0.5	%	99.2	84.1	97.0	93.0	95.4	
2,4,6-Tribromophenol	118-79-6	0.5	%	71.7	75.2	73.0	70.4	69.6	
EP075(SIM): PAH Surrogates									
2-Fluorobiphenyl	321-60-8	0.5	%	93.7	93.3	91.8	90.6	90.4	
Anthracene-d10	1719-06-8	0.5	%	100	99.0	100.0	95.7	98.9	
4-Terphenyl-d14	1718-51-0	0.5	%	94.9	94.0	93.8	91.4	93.2	
EP080: TPH(V)/BTEX Surrogates									
1,2-Dichloroethane-d4	17060-07-0	0.2	%	75.1	74.9	72.7	73.2	75.2	
Toluene-d8	2037-26-5	0.2	%	74.9	78.1	72.4	75.7	74.2	
4-Bromofluorobenzene	460-00-4	0.2	%	93.3	96.0	90.0	92.3	94.4	

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)				Sample ID	BH5 0.5-0.6	BH5 1.0-1.1	BH6 0.1-0.2	BH6 0.5-0.6	---
				Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	---
Compound	CAS Number	LOR	Unit	EM2421460-017	EM2421460-018	EM2421460-019	EM2421460-020	---	
				Result	Result	Result	Result	---	
EA055: Moisture Content (Dried @ 105-110°C)									
Moisture Content	---	1.0	%	16.9	14.6	6.1	20.2	---	
EG005(ED003)T: Total Metals by ICP-AES									
Arsenic	7440-39-2	5	mg/kg	<5	13	12	27	---	
Barium	7440-39-3	10	mg/kg	210	20	30	300	---	
Beryllium	7440-41-7	1	mg/kg	<1	3	<1	7	---	
Boron	7440-42-8	50	mg/kg	<50	<50	<50	<50	---	
Cadmium	7440-43-9	1	mg/kg	<1	<1	<1	<1	---	
Chromium	7440-47-3	2	mg/kg	6	4	11	71	---	
Cobalt	7440-48-4	2	mg/kg	10	32	3	53	---	
Copper	7440-50-8	5	mg/kg	13	<5	17	72	---	
Lead	7439-92-1	5	mg/kg	134	42	14	6	---	
Manganese	7439-96-5	5	mg/kg	720	126	70	5230	---	
Nickel	7440-02-0	2	mg/kg	8	28	9	232	---	
Selenium	7782-49-2	5	mg/kg	<5	<5	<5	<5	---	
Vanadium	7440-62-2	5	mg/kg	15	8	15	72	---	
Zinc	7440-66-6	5	mg/kg	125	69	42	58	---	
EG025T: Total Recoverable Mercury by FIMS									
Mercury	7439-97-6	0.1	mg/kg	<0.1	<0.1	<0.1	<0.1	---	
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons									
Naphthalene	91-20-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	---	
Acenaphthylene	208-96-8	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	---	
Acenaphthene	83-32-9	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	---	
Fluorene	86-73-7	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	---	
Phenanthrene	85-01-8	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	---	
Anthracene	120-12-7	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	---	
Fluoranthene	206-44-0	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	---	
Pyrene	129-00-0	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5	---	

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)		Sample ID	BH5 0.5-0.6	BH5 1.0-1.1	BH6 0.1-0.2	BH6 0.5-0.6	---
		Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	---
Compound	CAS Number	LOR	Unit	EM2421460-017	EM2421460-018	EM2421460-019	EM2421460-020
				Result	Result	Result	Result
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons - Continued							
Benzo(a)anthracene	56-55-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Chrysene	218-01-9	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Benzo(b)fluoranthene	205-99-2	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Benzo(k)fluoranthene	207-08-9	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Benzo(a)pyrene	50-32-8	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Indeno(1,2,3-cd)pyrene	193-39-5	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Dibenzo(a,h)anthracene	53-70-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Benzo(g,h)perylene	191-24-2	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
^A Sum of polycyclic aromatic hydrocarbons	---	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
^A Benzo(a)pyrene TEQ (zero)	---	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
^A Benzo(a)pyrene TEQ (half LOR)	---	0.5	mg/kg	0.6	0.6	0.6	0.6
^A Benzo(a)pyrene TEQ (LOR)	---	0.5	mg/kg	1.2	1.2	1.2	1.2
EP080(071): Total Petroleum Hydrocarbons							
C6 - C9 Fraction	---	10	mg/kg	<10	<10	<10	<10
C10 - C14 Fraction	---	50	mg/kg	<50	<50	<50	<50
C15 - C28 Fraction	---	100	mg/kg	<100	<100	<100	<100
C29 - C36 Fraction	---	100	mg/kg	<100	<100	<100	<100
^A C10 - C36 Fraction (sum)	---	50	mg/kg	<50	<50	<50	<50
EP080(071): Total Recoverable Hydrocarbons - NEPM 2013 Fractions							
C6 - C10 Fraction	C6_C10	10	mg/kg	<10	<10	<10	<10
^A C6 - C10 Fraction minus BTEX (F1)	C6_C10-BTEX	10	mg/kg	<10	<10	<10	<10
>C10 - C16 Fraction	---	50	mg/kg	<50	<50	<50	<50
>C16 - C34 Fraction	---	100	mg/kg	<100	<100	<100	<100
>C34 - C40 Fraction	---	100	mg/kg	<100	<100	<100	<100
>C10 - C40 Fraction (sum)	---	50	mg/kg	<50	<50	<50	<50
^A >C10 - C16 Fraction minus Naphthalene (F2)	---	50	mg/kg	<50	<50	<50	<50

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Analytical Results

Sub-Matrix: SOIL (Matrix: SOIL)		Sample ID	BH5 0.5-0.6	BH5 1.0-1.1	BH6 0.1-0.2	BH6 0.5-0.6	---
		Sampling date / time	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	04-Dec-2024 00:00	---
Compound	CAS Number	LOR	Unit	EM2421460-017	EM2421460-018	EM2421460-019	EM2421460-020
				Result	Result	Result	Result
EP080: BTEXN							
Benzene	71-43-2	0.2	mg/kg	<0.2	<0.2	<0.2	<0.2
Toluene	108-88-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Ethylbenzene	100-41-4	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
meta- & para-Xylene	108-38-3	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
ortho-Xylene	95-47-6	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
^A Sum of BTEX	---	0.2	mg/kg	<0.2	<0.2	<0.2	<0.2
^A Total Xylenes	---	0.5	mg/kg	<0.5	<0.5	<0.5	<0.5
Naphthalene	91-20-3	1	mg/kg	<1	<1	<1	<1
EP075(SIM)S: Phenolic Compound Surrogates							
Phenol-d6	13127-88-3	0.5	%	85.8	84.5	82.5	78.6
2-Chlorophenol-d4	93951-73-6	0.5	%	97.4	95.7	89.1	88.7
2,4,6-Tribromophenol	118-79-6	0.5	%	69.6	67.0	71.0	43.6
EP075(SIM)T: PAH Surrogates							
2-Fluorobiphenyl	321-60-8	0.5	%	92.0	91.8	91.7	90.0
Anthracene-d10	1719-06-8	0.5	%	98.8	101	99.2	97.9
4-Terphenyl-d14	1718-51-0	0.5	%	93.9	93.7	93.2	93.0
EP080S: TPH(V)/BTEX Surrogates							
1,2-Dichloroethane-d4	17060-07-0	0.2	%	79.8	74.8	76.6	72.9
Toluene-d8	2037-26-5	0.2	%	82.1	74.9	75.6	76.1
4-Bromofluorobenzene	460-00-4	0.2	%	100	94.0	93.7	100

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

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Analytical Results				Sample ID	Rinsate			
Sub-Matrix: WATER (Matrix: WATER)				Sampling date / time	04-Dec-2024 00:00			
Compound	CAS Number	LOR	Unit	EM2421460-001				
EQ020F: Dissolved Metals by ICP-MS								
Arsenic	7440-38-2	0.001	mg/L	<0.001				
Boron	7440-42-8	0.05	mg/L	<0.05				
Barium	7440-39-3	0.001	mg/L	<0.001				
Beryllium	7440-41-7	0.001	mg/L	<0.001				
Cadmium	7440-43-9	0.0001	mg/L	<0.0001				
Cobalt	7440-48-4	0.001	mg/L	<0.001				
Chromium	7440-47-3	0.001	mg/L	<0.001				
Copper	7440-50-8	0.001	mg/L	<0.001				
Manganese	7439-96-5	0.001	mg/L	<0.001				
Nickel	7440-02-0	0.001	mg/L	<0.001				
Lead	7439-92-1	0.001	mg/L	<0.001				
Selenium	7782-49-2	0.01	mg/L	<0.01				
Vanadium	7440-62-2	0.01	mg/L	<0.01				
Zinc	7440-66-6	0.005	mg/L	<0.005				
EQ035F: Dissolved Mercury by FIMS								
Mercury	7439-97-6	0.0001	mg/L	<0.0001				
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons								
Naphthalene	91-20-3	1.0	µg/L	<1.0				
Acenaphthylene	208-96-8	1.0	µg/L	<1.0				
Acenaphthene	83-32-9	1.0	µg/L	<1.0				
Fluorene	86-73-7	1.0	µg/L	<1.0				
Phenanthrene	85-01-8	1.0	µg/L	<1.0				
Anthracene	120-12-7	1.0	µg/L	<1.0				
Fluoranthene	209-44-0	1.0	µg/L	<1.0				
Pyrene	129-00-0	1.0	µg/L	<1.0				
Benzo(a)anthracene	56-55-3	1.0	µg/L	<1.0				
Chrysene	218-01-9	1.0	µg/L	<1.0				

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Analytical Results				Sample ID	Rinsate			
Sub-Matrix: WATER (Matrix: WATER)				Sampling date / time	04-Dec-2024 00:00			
Compound	CAS Number	LOR	Unit	EM2421460-001				
EP075(SIM)B: Polynuclear Aromatic Hydrocarbons - Continued								
Benzo(b)fluoranthene	205-99-2	205-82-3	1.0	µg/L	<1.0			
Benzo(k)fluoranthene	207-08-9	1.0	µg/L	<1.0				
Benzo(a)pyrene	50-32-8	0.5	µg/L	<0.5				
Indeno(1,2,3-cd)pyrene	193-39-5	1.0	µg/L	<1.0				
Dibenz(a,h)anthracene	53-70-3	1.0	µg/L	<1.0				
Benzo(g,h)perylene	191-24-2	1.0	µg/L	<1.0				
^A Sum of polycyclic aromatic hydrocarbons		0.5	µg/L	<0.5				
^A Benzo(a)pyrene TEQ (zero)		0.5	µg/L	<0.5				
EP080071: Total Petroleum Hydrocarbons								
C8 - C9 Fraction		20	µg/L	<20				
C10 - C14 Fraction		50	µg/L	<50				
C15 - C28 Fraction		100	µg/L	<100				
C29 - C36 Fraction		50	µg/L	<50				
^A C10 - C36 Fraction (sum)		50	µg/L	<50				
EP080071: Total Recoverable Hydrocarbons - NEPM 2013 Fractions								
C6 - C10 Fraction	C6, C10	20	µg/L	<20				
^A C6 - C10 Fraction minus BTEX (F1)	C6, C10-BTEX	20	µg/L	<20				
>C10 - C16 Fraction		100	µg/L	<100				
>C16 - C34 Fraction		100	µg/L	<100				
>C34 - C40 Fraction		100	µg/L	<100				
^A >C10 - C40 Fraction (sum)		100	µg/L	<100				
^A >C10 - C16 Fraction minus Naphthalene (F2)		100	µg/L	<100				
EP080: BTEXN								
Benzene	71-43-2	1	µg/L	<1				
Toluene	108-88-3	2	µg/L	<2				
Ethylbenzene	100-41-4	2	µg/L	<2				

Environmental Site Assessment: 65 Brisbane Street, Hobart, Tasmania. January 2025.

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Analytical Results

Sub-Matrix: WATER (Matrix: WATER)				Sample ID	Rinsate				
				Sampling date / time	04-Dec-2024 00:00				
Compound	CAS Number	LOR	Unit		EM2421460-001				
				Result					
EP080: BTEXN - Continued									
meta- & para-Xylene	105-35-3 106-42-3	2	µg/L	<2					
ortho-Xylene	95-47-6	2	µg/L	<2					
* Total Xylenes		2	µg/L	<2					
* Sum of BTEX		1	µg/L	<1					
Naphthalene	91-20-3	5	µg/L	<5					
EP075(SIMIS): Phenolic Compound Surrogates									
Phenol-d6	13127-88-3	1.0	%	26.6					
2-Chlorophenol-D4	93951-73-6	1.0	%	43.4					
2,4,6-Tribromophenol	118-79-6	1.0	%	52.9					
EP075(SIMIT): PAH Surrogates									
2-Fluorobiphenyl	321-60-8	1.0	%	84.0					
Anthracene-d10	1719-06-8	1.0	%	107					
4-Terphenyl-d14	1718-51-0	1.0	%	91.4					
EP080S: TPH(V)/BTEX Surrogates									
1,2-Dichloroethane-D4	17060-07-0	2	%	86.9					
Toluene-D8	2037-26-5	2	%	90.6					
4-Bromofluorobenzene	460-00-4	2	%	109					

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Surrogate Control Limits

Sub-Matrix: SOIL		Recovery Limits (%)	
Compound	CAS Number	Low	High
EP075(SIMIS): Phenolic Compound Surrogates			
Phenol-d6	13127-88-3	54	125
2-Chlorophenol-D4	93951-73-6	65	123
2,4,6-Tribromophenol	118-79-6	34	122
EP075(SIMIT): PAH Surrogates			
2-Fluorobiphenyl	321-60-8	61	125
Anthracene-d10	1719-06-8	62	130
4-Terphenyl-d14	1718-51-0	67	133
EP080S: TPH(V)/BTEX Surrogates			
1,2-Dichloroethane-D4	17060-07-0	51	125
Toluene-D8	2037-26-5	55	125
4-Bromofluorobenzene	460-00-4	56	124
Sub-Matrix: WATER		Recovery Limits (%)	
Compound	CAS Number	Low	High
EP075(SIMIS): Phenolic Compound Surrogates			
Phenol-d6	13127-88-3	10	51
2-Chlorophenol-D4	93951-73-6	30	114
2,4,6-Tribromophenol	118-79-6	26	133
EP075(SIMIT): PAH Surrogates			
2-Fluorobiphenyl	321-60-8	35	127
Anthracene-d10	1719-06-8	44	122
4-Terphenyl-d14	1718-51-0	44	124
EP080S: TPH(V)/BTEX Surrogates			
1,2-Dichloroethane-D4	17060-07-0	73	129
Toluene-D8	2037-26-5	70	125
4-Bromofluorobenzene	460-00-4	71	129

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Historic Heritage and Archaeological
Management Strategy
& Heritage/Archaeological Impact
Assessment

57-65 Brisbane Street
HOBART TASMANIA

Brad Williams
Heritage Consultant

For AllUrban Planning, on behalf of Brisbane Street
Apartments Pty. Ltd.

Version 2, November 2025
(Heritage Impact Assessment November 2025)

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This document was written by Brad Williams (BA.Hons Archaeology, G.Dip Maritime Archaeology, MA Cultural Heritage Management, G.Dip Environmental Planning) Historical Archaeologist, Heritage Consultant and Director of Praxis Environment – a division of Praxis Synergy Pty. Ltd. Part of the historical background in Section 4 was authored by Alan Townsend, consultant historian.

Unless otherwise stated, the north point (or approximate) of maps and plans is to the top of the page.

Cadastral information depicted in this document must not be relied upon without verification by a Surveyor.

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Abbreviations

TAHO – Tasmanian Archive and Heritage Office
THC – Tasmanian Heritage Council
THR – Tasmanian Heritage Register
LTO – Lands Titles Office
DSG – Department of State Growth
HIPS15 – Hobart Interim Planning Scheme 2015
DPIPWE – (former) Department of Primary Industry, Parks, Water and Environment.

1. Introduction, rationale and brief

This report has been commissioned by AllUrban Planning on behalf of Brisbane Street Apartments Pty. Ltd. (owners), to consider the likely historic heritage issues and statutory heritage requirements for any future development on the property known as 57-65 Brisbane Street (*the site*), Hobart to provide a sound conservation planning policy basis for the management of heritage values of the site. A focus of this report is to provide an understanding of the historic cultural heritage significance of the setting, curtilage and streetscape values of the building known as *Sydney Lodge* to guide any future development and is considered a critical part of managing the heritage values of the place and its wider setting. Further, this document aims to provide an archaeological management plan for the overall site. Note that Version 1 of this document was formulated in March 2025 – since that time the former Hobart Interim Planning scheme has been replaced by the Tasmanian Planning Scheme – Hobart, therefore Version 2 of this document is aligned with the new scheme.

The place is comprised of Certificate of Title 174045/1 (PID 3537209) and is a single allotment of 0.4775 hectares on the northern fringe of the Hobart CBD. It has a comparatively narrow street frontage (approximately 24 metres) which widens internally. The streetfront is vacant of buildings and comprises of a carpark, with the buildings at the rear of the site – the c1860 *Sydney Lodge* building as well as a series of 1960s (and later) workshop buildings, separated and surrounded by carparking areas.

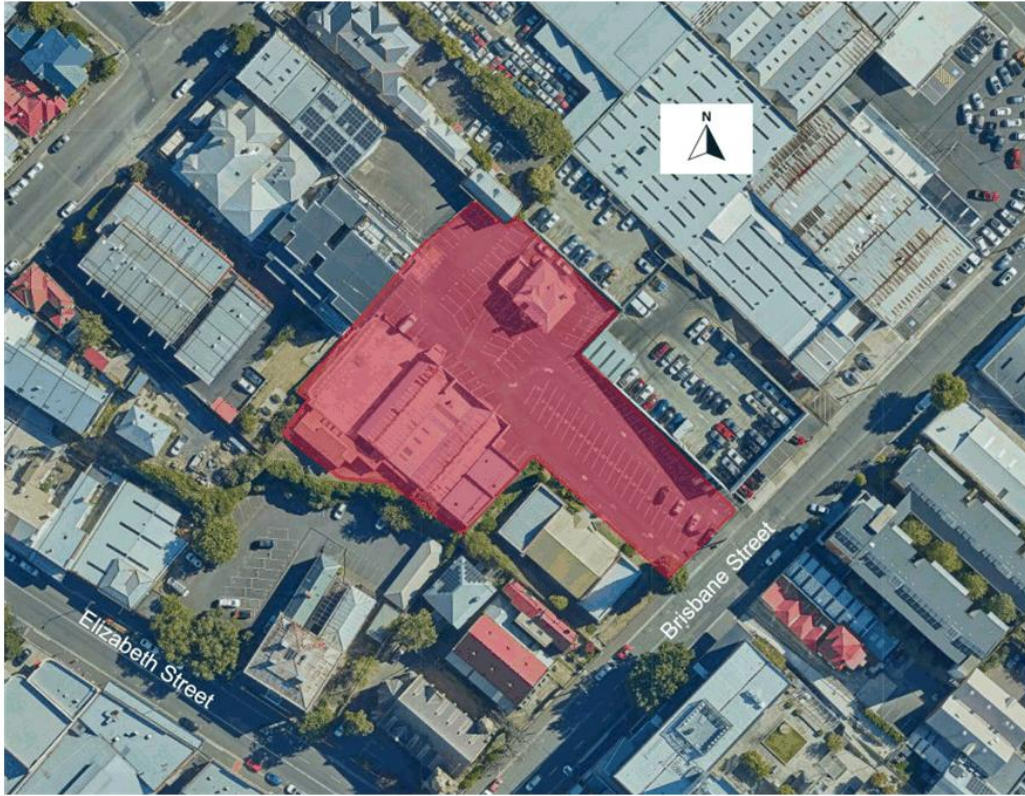


Figure 1.1.– Aerial photograph of the area (the site denoted in red) Adapted from www.thelist.tas.gov.au

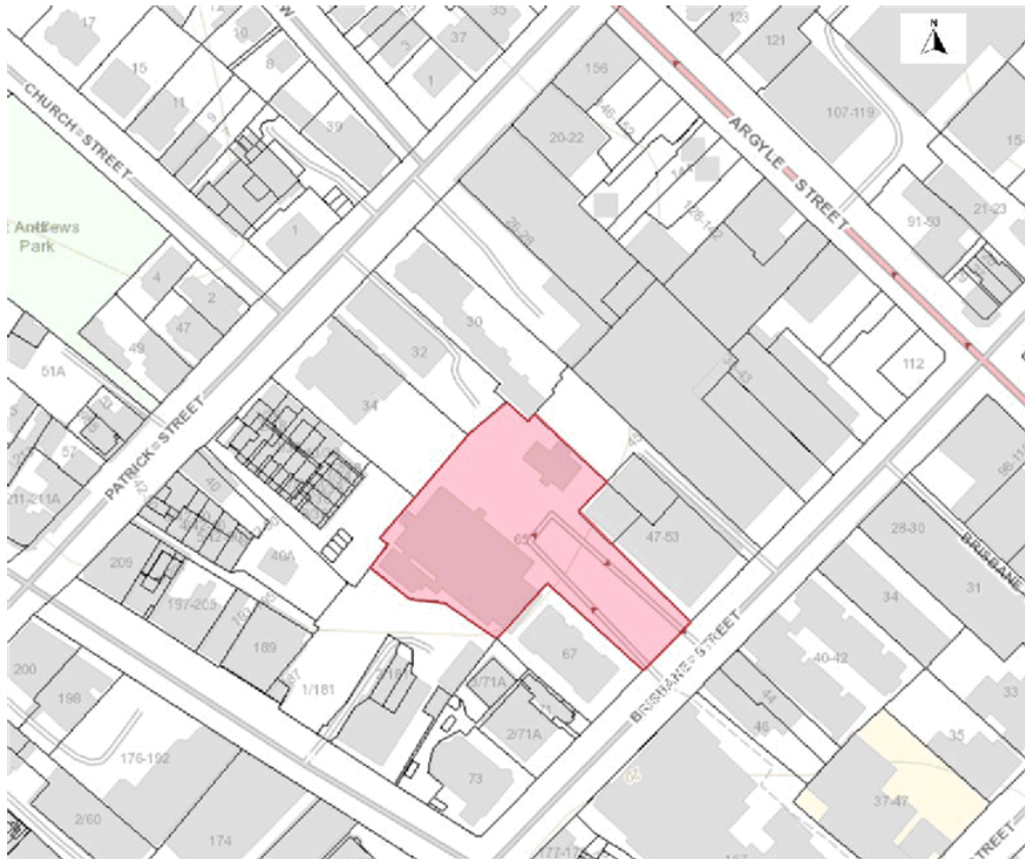


Figure 1.2 – Cadastral arrangement of the area (the site denoted in red) Adapted from www.thelist.tas.gov.au

Part of the site is listed as a Local Historic Heritage Place on Table C6.1 of the Tasmanian Planning Scheme – Hobart (the *scheme*). It is not within any Heritage Precinct as defined by the scheme. Part of the site is permanently registered on the Tasmanian Heritage Register (*THR*) and is adjacent to a place also registered on the THR (*Ingomar*, 34 Patrick Street). The site is included on Table C.6.4 (Places of Archaeological Potential) of the scheme.

Accordingly, the owner recognises the responsibility to appropriately manage the heritage values of the place in any forthcoming development. The brief for this project was to:

The first stage of the project (i.e. pre-development design) is to include:

1. An overview site history which is the essential basis for (2-3) below.

2. A **conservation brief** for the building itself, which would broadly look at the overall form and curtilage/streetscape values of the building (but not include a detailed fabric survey or internal assessment). This would inform a **streetscape and townscape setting analysis** of the building which would assist in the formulation of development guidelines as proposed below.
3. A **statement of historical archaeological potential** (SoHAP) for the site, which would consider the site history, key historic themes and the possibility that the site may contain any significant archaeological remains.

A second stage of the project (i.e. following development design) is to include:

4. If required by (3) – i.e. if archaeological potential is determined an **archaeological impact assessment** against any proposed development, and if archaeological impact is likely the formulation of an **archaeological method statement** to be implemented in any future works program. Note that if no archaeological potential is determined, this step will not be necessary.
5. A **heritage impact assessment** (including a statement of compliance) for any future development plan which is to be measured against the policies of the archaeological and conservation briefs as well as the statutory heritage requirements.

It is important to note that the above assessment of the place has been formulated independently of the proposed development – and the policies of which have then been applied to assess the merits of the proposal – this is consistent with the ICOMOS Australia *Burra Charter* practice, as well as the conservation planning process espoused by J.S. Kerr, which are considered to be sound industry best-practice in understanding and consequently managing historic heritage values.

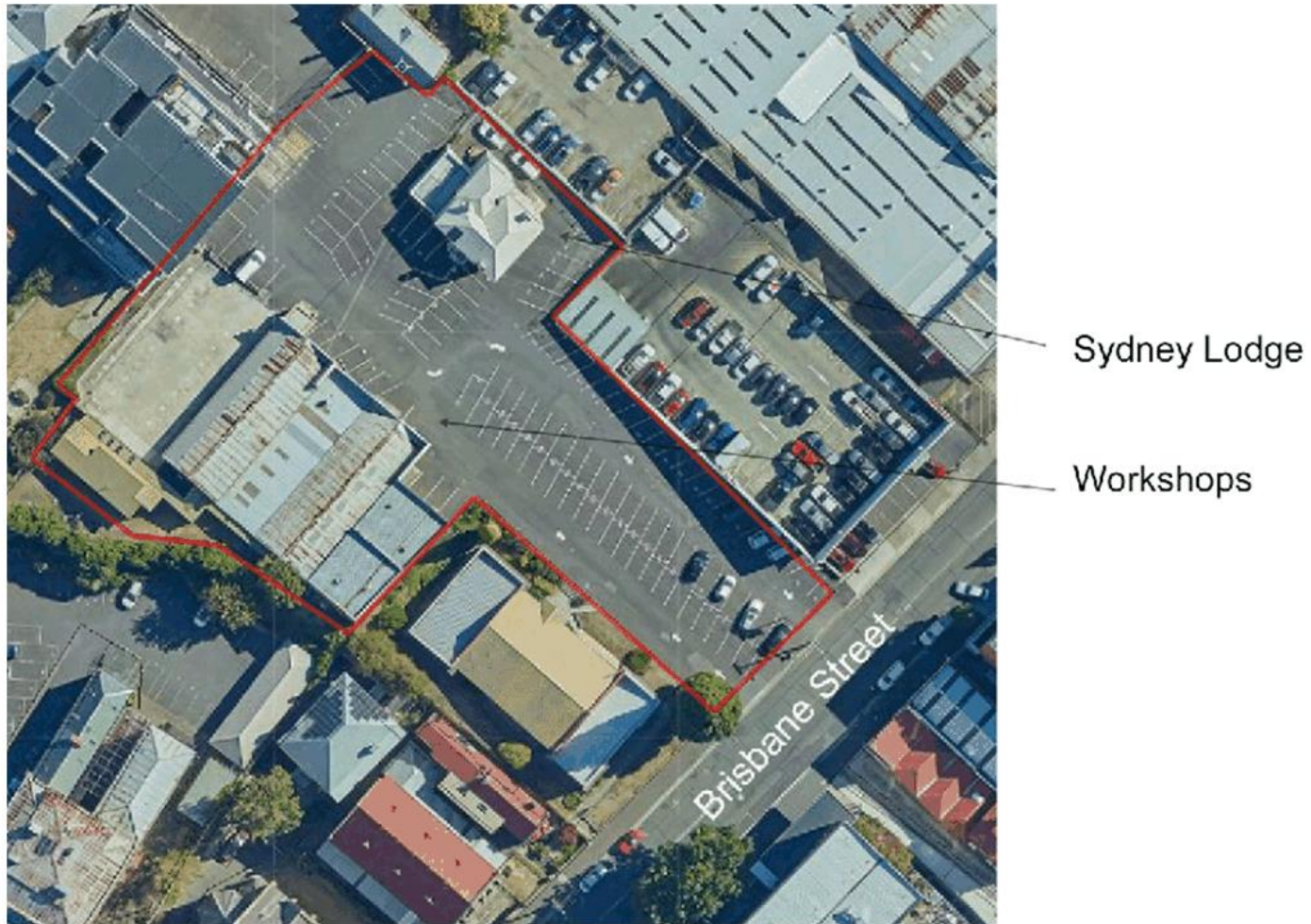


Figure 1.3 – Detailed aerial photograph of the site. Adapted from www.thelist.tas.gov.au

2. Statutory heritage requirements

The following heritage listings and overarching legislative provisions are relevant to the management of the historic cultural heritage values of 57-65 Brisbane Street, Hobart.

Tasmanian Planning Scheme - Hobart

Although part the subject site is included on Table C6.1 of the Tasmanian Planning Scheme – Hobart, as it is also partly included on the Tasmanian Heritage Register, by virtue of Clause C6.2.2 the Local Historic Heritage Code only applies to parts of the site (See Figure 2.1).



Figure 2.1 – Only the red shaded areas of the site are subject to Clause C6.2.2.

Therefore those parts of the site are subject to the following clauses of the scheme:

C6.1	Local Heritage Places	C6.6.1	Demolition
		C6.6.2	Site coverage
		C6.6.3	Height and bulk of buildings
		C6.6.4	Siting of building and structures
		C6.6.5	Fences
		C6.6.6	Roof form and materials
		C6.6.7	Building alterations exc. roof
		C6.6.8	Outbuildings and structures
		C6.6.9	Driveways and parking (non-res)
		C6.6.10	Tree removal (specifically part)
		C6.10.1	Lot Design (Subdivision)
		C1.6.4	Signs on local heritage places and in local heritage precincts and local historic landscape precincts

Note that the Tasmanian Heritage Register inclusion does not override the application of Clause C6.6.10 (tree removal) – the subject site does not contain any significant trees, however the adjacent site at 67 Brisbane Street contains a significant tree, therefore consideration of any development of the subject site upon that tree must be given.

The site is not included on Table C6.2 of the LPS, therefore is not subject to Clauses C6.7.1, C6.7.3, C6.10.2 or C1.6.4 of the planning scheme which relate to Local Heritage Precincts.

The subject site is included within an area defined by Table C6.4 of the LPS (REF LPS), therefore is subject to the following clauses of the planning scheme:

C6.4	Places of Archaeological Potential	C6.8.1	Building and works
		C6.10.3	Lot Design (Subdivision)

Whilst the current document does not constitute a full conservation management plan, it aims to fulfill the baseline requirements of managing the heritage values of the place, and to flag where any further input may be required and to allow the planning authority to make an informed assessment against the performance criteria of the Scheme. Note that as the owner does not foreshadow any changes to the Sydney Lodge building in the foreseeable future, the scope for the current project only includes the external building envelope (as well as spaces, curtilage, views etc.) in order to gain a context of the overall site.

Historic Cultural Heritage Act 1995

The subject site is listed on the Tasmanian Heritage Register; therefore is subject to the provisions of the *Historic Cultural Heritage Act 1995* (HCHA).

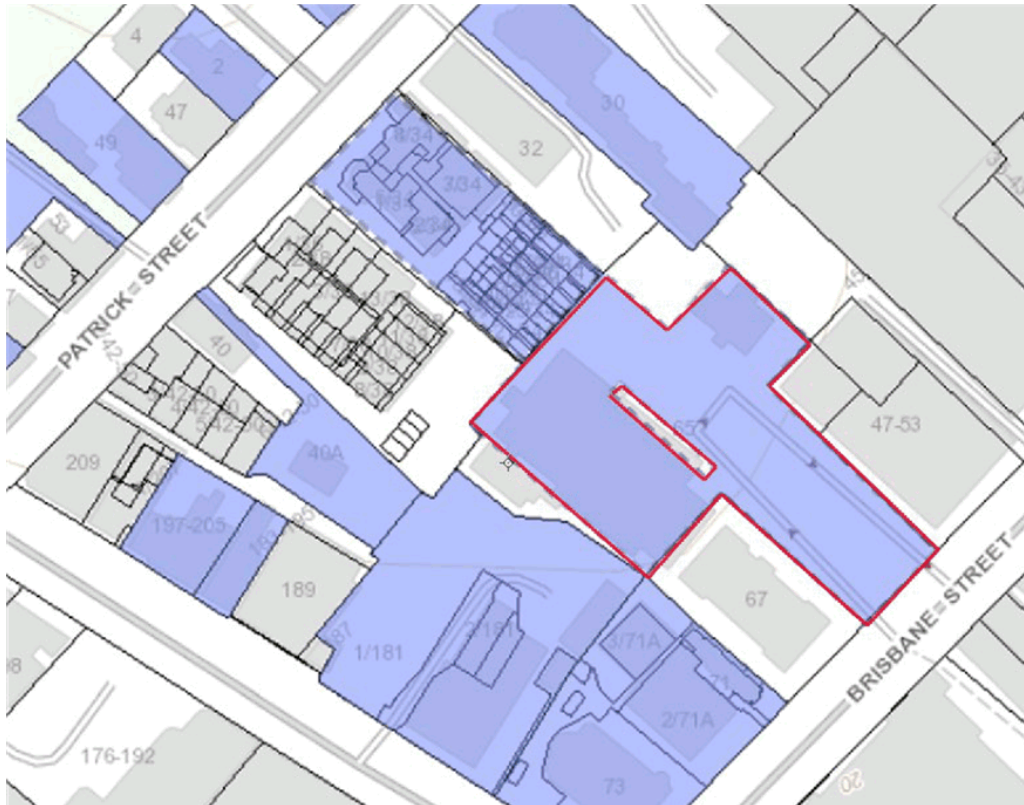


Figure 2.1 – Area affected by the THR registration (outlined red and shaded blue). Adapted from www.thelist.tas.gov.au

Note that there are some anomalies in the registered area (e.g. northern and western corners, and a strip in the centre) – these arise from legacy titles which were not part of the originally registered place, which have not been amended (if necessary) to include those area upon consolidation of the earlier titles.

Part 6 of the HCHA (Heritage Works) sets the process by which approvals for works may be gained from the Tasmanian Heritage Council (THC):

35. Heritage works require heritage approval

- (1) A person must not carry out any heritage works unless those heritage works have heritage approval.
- (2) For the purposes of subsection (1), heritage works are taken to have heritage approval if, and only if –
- (a) in a case where a minor works approval has been issued, the heritage works are carried out in accordance with –
 - (i) that minor works approval; and
 - (ii) if a discretionary permit or other permit is required for the heritage works under the Planning Act, that discretionary permit or other permit; or
 - (b) in a case where a certificate of exemption has not been issued, the heritage works are carried out in accordance with a discretionary permit.
- (3) It is a defence in proceedings for an offence under subsection (1) if the defendant establishes that –
- (a) the heritage works were carried out in response to an emergency; and
 - (b) the heritage works were, both as to nature and extent, reasonably necessary for the purposes of responding to the emergency; and
 - (c) in the circumstances, it was not practicable to seek a minor works approval; and
 - (d) the defendant, before, while or as soon as practicable after carrying out the heritage works, notified the Heritage Council, in writing, of the emergency and the details of the heritage works.

Sections 36-41 set the process for the lodgment and assessment of applications for a heritage works permit, via a Discretionary Development Application under the Land Use Planning and Approvals Act 1993.

Section 42 describes the process whereby certain works may be exempt from the requirement of s.35:

42. Minor Works Approval for heritage works

- (1) A person may apply to the Heritage Council for a minor works approval for heritage works.
- (2) The minor works application –
- (a) is to be in a form provided or approved by the Heritage Council; and
 - (b) is to be supported by such information as the Heritage Council requires, either at the time of lodgment or subsequently.
- (3) The Heritage Council may –
- (a) approve the minor works application; or
 - (b) refuse the minor works application.
- (4) Without limiting its discretion, the Heritage Council must approve the minor works application if it is reasonably satisfied that the heritage works –

(a) are identified in the works guidelines as works that will have no impact or only negligible impact on the historic cultural heritage significance of the relevant registered place or heritage area; and
(b) are capable of being carried out in accordance with the works guidelines.

Whilst the HCHA provides no specific detail as to how particular proposals are considered, nor does it provide any indicative thresholds of what may be considered to have *no or negligible* heritage impact, the THC/Tasmanian Government publication *Works Guidelines for Historic Heritage Places* (November 2015)¹ provides further detail on the application process, guiding principles and the basis for decisions made by the THC.

In addition, the THC has a series of practice notes and technical guides, available via www.heritage.tas.gov.au which provide useful guiding principles for how the THC are expected to assess and determine applications for heritage works.

In addition to the above, Section 39(2) of the HCHA requires the THC to consider the likely impact that proposed works on a registered place may have on adjoining registered places:

39. Procedure if Heritage Council wishes to be involved in determining discretionary permit application

(2) The Heritage Council is to further consider the discretionary permit application and in so doing –
(a) is to have regard to the likely impact of the relevant heritage works on the historic cultural heritage significance of the relevant registered place or heritage area and any registered place that adjoins the relevant registered place or heritage area; and

The site adjoins 34 Patrick Street (Ingomar) and 30 Patrick Street (Roydon) therefore technically the THC can consider impact of any proposed development upon those places (see Figure 1.4).

Environment Protection & Biodiversity Conservation Act 1999

The site is not included on the National or Commonwealth Heritage Lists, therefore the historic cultural heritage provisions of the Environment Protection and Biodiversity Conservation Act 1999 are not applicable.

Aboriginal Heritage Act 1975

An assessment of any possible Aboriginal heritage values is not part of the brief for this report; nonetheless the provisions of the *Aboriginal Heritage Act 1975* are applicable to the place. A search of the Tasmanian Aboriginal Heritage sites register (Job # 38755858) did not identify any registered Aboriginal relics or apparent risk of impacting Aboriginal relics (search valid

¹ http://heritage.tas.gov.au/Documents/Works_Guidelines_FINAL_Nov2015.pdf

until 27/8/25). The Tasmanian Government's *Unanticipated Discovery Plan – Procedure for the management of unanticipated discoveries of Aboriginal relics in Tasmania* must be adhered to in the event that any Aboriginal heritage items are discovered during the course of any works or survey.

3. Assessment methodology

This assessment has been undertaken in accordance with the ICOMOS Australia *Burra Charter*, which is considered to be the Australian heritage industry’s benchmark for assessing, understanding and managing heritage values. Figure 3.1 depicts this process:

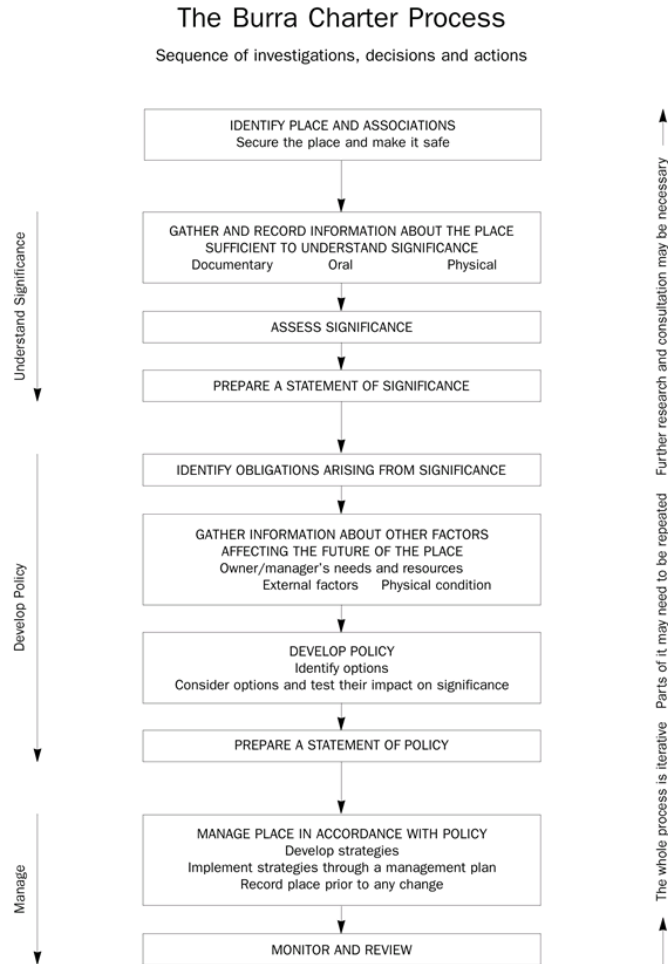


Figure 3.1 – The Burra Charter Process. ICOMOS Australia.

The statutory provisions and consequent responsibilities as outlined in Section 2 have also been considered in formulating this document.

Whilst this document does not intend to be a comprehensive conservation management plan, it takes the basic principles of conservation planning, as outlined in J.S. Kerr's *The Conservation Plan*², in order to develop the policies upon which the conservation of the place (and assessment of development impact) is based. This document has also been developed with regard to the standard content of conservation management plans as detailed by the New South Wales Heritage Office's *A Suggested Table of Contents for a Conservation Management Plan*³, as well as the New South Wales Heritage Office guidelines for the preparation of brief conservation management strategies.⁴ Note that the brief for the current project did not include assessment and consideration of the internal form, spaces and fabric of the Sydney Lodge building.

It is intended that this document be used by the design team in any forthcoming development of the place and this sets the benchmark of understanding the significance of the place against which a heritage impact assessment for any proposed development can be undertaken. Figure 3.2 depicts this process:

² KERR, J. (2000): *The Conservation Plan*. National Trust of NSW, Sydney.

³ http://www.heritage.nsw.gov.au/docs/cmp_contents2.pdf

⁴ http://www.heritage.nsw.gov.au/docs/CMS_part1investigation.pdf

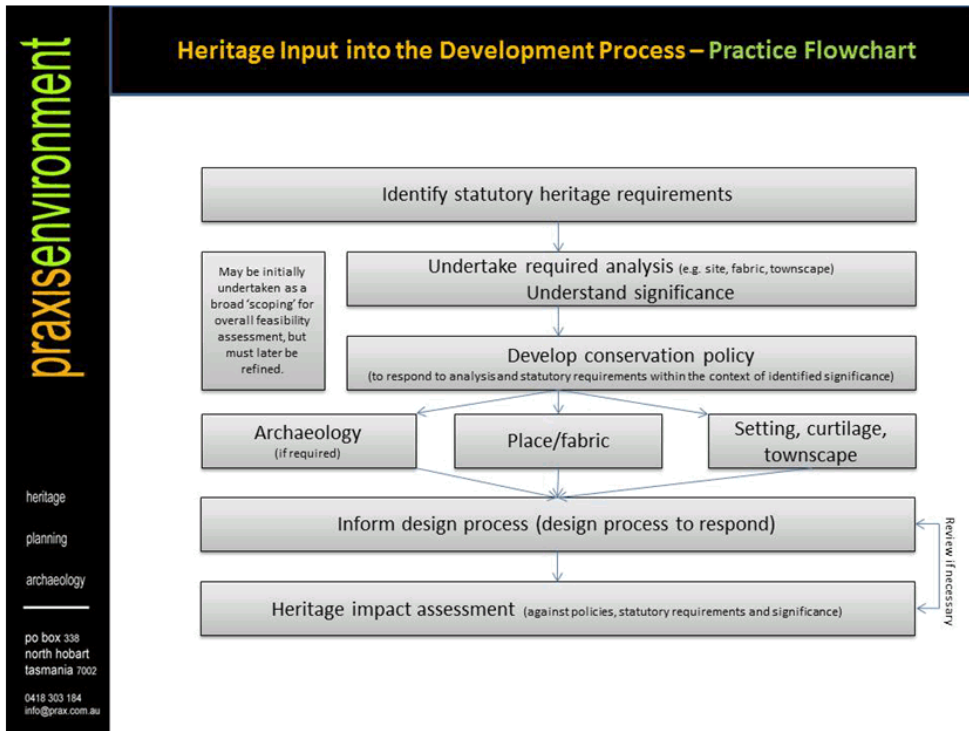


Figure 3.2 – Practice flowchart for the application of this conservation planning document.

4. Brief overview of the history and historical context of the place

4.1. Historical research methodology

As outlined in the methodology in Section 1.3, the key to assessing historic heritage significance is to gain an understanding of the history of the place, the context of it within its surrounds, associated thematic contexts, and other intangible values (e.g. community value, value associated with people, events etc.). Whilst the scope of the current assessment did not allow a full and comprehensive historical research project into the place, it has been commissioned to provide sufficient understanding of the physical context of the building and surrounds upon which decisions regarding the management of heritage values can be made. Where relevant, this document suggests where further research may be required to further clarify or articulate particular points (e.g. associative values).

The following sources were consulted as part of this project, in relation to the place, and wider environs:

- Land Titles Records
- Newspaper articles, sketches and photographs from a variety of primary (and secondary) sources, including the Tasmanian Archive and Heritage Office (TAHO), Trove (National Library of Australia) etc.
- Early town maps/surveys of Hobart – held by Lands Tasmania and TAHO.
- Aerial photographs – held by Lands Tasmania (DPIPWE GIS ArcGIS system).
- Hobart Valuation Rolls.
- Hobart City Council building files (AE417 series, Tasmanian Archive and Heritage Office).

Previous heritage studies and other secondary source material

There are no known previous conservation planning documents, heritage/streetscape assessments or archaeological investigations specifically regarding the place.

4.2. Overview of the history of the place

The indigenous inhabitants of the Hobart coastal zone at the time of British contact in 1803-1804 were the Mouheneenner people of the South East tribe of Aboriginal Tasmanians. It is likely that the area was also visited by members of the Big River tribe from the Central Plateau region who would travel down the west bank of the Derwent.

The Mouheneenner, and the South East tribe more generally, were a maritime people living upon the rich resources of the coastline and its immediate hinterland, moving up and down the coast according to the seasonal availability of food and other

resources. Their diet consisted largely of shellfish augmented by seabirds, eggs, seals, kangaroos and wallabies and marine and terrestrial plants. Sheltered coves and bays provided favoured camping spots.

The place now known as Hobart was known to the Aborigines as Nipaluna.

The British arrival at Sullivan's Cove in 1804 had a major impact on the indigenous people, with their camps, hunting grounds and other key resources being effectively taken over by the British who were seeking to establish a productive foothold in their new southern colony.⁵

Hobart was first settled by the British in 1804 when a small camp of officers, convicts and free settlers was made at Sullivan's Cove under the command of Lt-Governor David Collins. The Hobart Rivulet, which runs through the current study site, was vital to the success of the settlement, providing a continual stream of water for domestic and industrial use.

In 1811 Governor Macquarie of New South Wales ordered a town plan be made with streets laid out in a grid pattern. By this time Hobart had several shops, hotels, a church, hospital and some developing industries.⁶ The 1820s - 1830s was a time of major growth in commerce and industry with an influx of free settlers and the enterprising spirit of ex-convicts. Some substantial Georgian buildings of brick and stone were erected and settlement extended to outlying suburbs.⁷

The subject site on the fringe of the Hobart CBD was first mapped and allotted in the late 1810s, as depicted on plans Hobart 12 and 13 (Lands Tasmania). These surveys do not show any development on the subject site, although were generally limited to major public buildings at that time.

⁵ McConnell A. and Evans K., Hobart Coastal Heritage Study, 2016, a report for the City of Hobart.

⁶ Alexander A and Petrow S, 'Hobart' in Alexander A (ed) *The Companion to Tasmanian History*, Centre for Tasmanian Historical Studies, 2005, p. 176

⁷ Alexander A and Petrow S, 'Hobart' in Alexander A (ed) *The Companion to Tasmanian History*, Centre for Tasmanian Historical Studies, 2005, p. 176

Figure 4.1 - Original Hobart Town grants.



The subject site largely comprises of all/portions of three land grants (refer to Figure 4.4). These are referred to as:

1. The whole of a grant to William Foord (the central portion of the site).
2. Part of a grant to John Hiddlestone (the eventual site of Sydney Lodge).
3. Part of a grant to George Hunt.

Note that these may not represent the very earliest 'owners' of the land, however these represent the first formal allocation of land from the Crown. Earlier owners where applicable are discussed below.

The western edge of the site, as well as the northern corner are later adhesions to the subject site, the former being a portion of the rear garden of *Westella* and the latter being the rear portion of a Patrick Street allotment. These are not further explored here in terms of title history (due to some quite complicated transactions which have little bearing on site history, however the physical evolution of these is included in the figures below and these areas are relevant to the assessment of archaeological potential.

Figure 4.2 - C1829 survey of Hobart. Lands Tasmania H5.



The Foord grant

William Foord, a carpenter and builder, came free to Van Diemen’s Land on the *Thalias* in 1823⁸. In an undated memorial to the Governor (probably 1825) Foord stated that:

“Since the above period [arriving in VDL] your Honour’s memorialist has built two substantial two storey brick houses situate in Brisbane Street, which are let to tenants of the Most respectability”

⁸ Tasmanian Archives Colonial Secretary’s Office (CSO) 1/1/55/1117

Foord is referenced as living in Brisbane Street in July 1824 when his house was burgled⁹; the following year, when writing to the Governor for a grant of land in Elizabeth Street, Foord dated his letter from Brisbane Street¹⁰.

In October 1832 Foord applied to have his 1 rood 36 perch location on Brisbane Street recognised as a grant from the Crown¹¹. The location was duly recognised by grant in January 1833¹² and matches the title depicted on Sprent's ca 1843 survey.

The description of the houses as 'two substantial brick houses' suggests that this was either an earlier building, or had additional terraces added later – noting later descriptions of three (and depictions in later photographs and the 1908 Metropolitan Drainage Board plan).

The Hiddlestone grant

John Hiddlestone and his family came free to Van Diemen's Land, arriving in September 1822 on the *Heroine*¹³ with a quantity of "ironmongery, tools, oils, colors, turpentine and glass"¹⁴. Almost immediately he went into business with Isaac Chapman as Chapman & Hiddlestone, carpenters, painters, plumbers and builders at a location in Harrington Street.

In June 1825 Hiddlestone and Chapman sent a memorial to Governor Arthur requesting additional grants of land for each. According to the memorial, Hiddlestone and Chapman:

*"...about seventeen months ago [January 1823] your Memorialists were favoured by His Honour Lt Governor Sorell with two building allotments in the outskirts of the town, on which they have since erected two commodious brick dwelling houses each two stories high, and their land, though extremely rugged and forbidding when they entered upon it, they have now brought into a state of productive cultivation as a kitchen garden. As a mark of the approbation with which your Honour's predecessor was pleased to view the proceedings of your Memorialists, he added to their land two other allotments on which they intend in a short time to erect two respectable dwelling houses, having already enclosed it with a neat and substantial fence"*¹⁵

Around the same time (1825) Chapman petitioned the Governor, giving his address as Brisbane Street¹⁶, which implies that the "allotments on the outskirts of the town" are in fact Hiddlestone and Chaman's Brisbane Street grants, although Hiddlestone's brick house would likely be the one depicted as outside the study area.

⁹ *Hobart Town Gazette & Van Diemen's Land Advertiser* 30 July 1824 p2

¹⁰ TA CSO 1/1/140/3464

¹¹ *Hobart Town Gazette* 19 October 1832 p5

¹² Department of State Growth (DSG) Land Titles Office (LTO) Memorial (Mem) 5/5524

¹³ State Library of Tasmania Names Index 435936

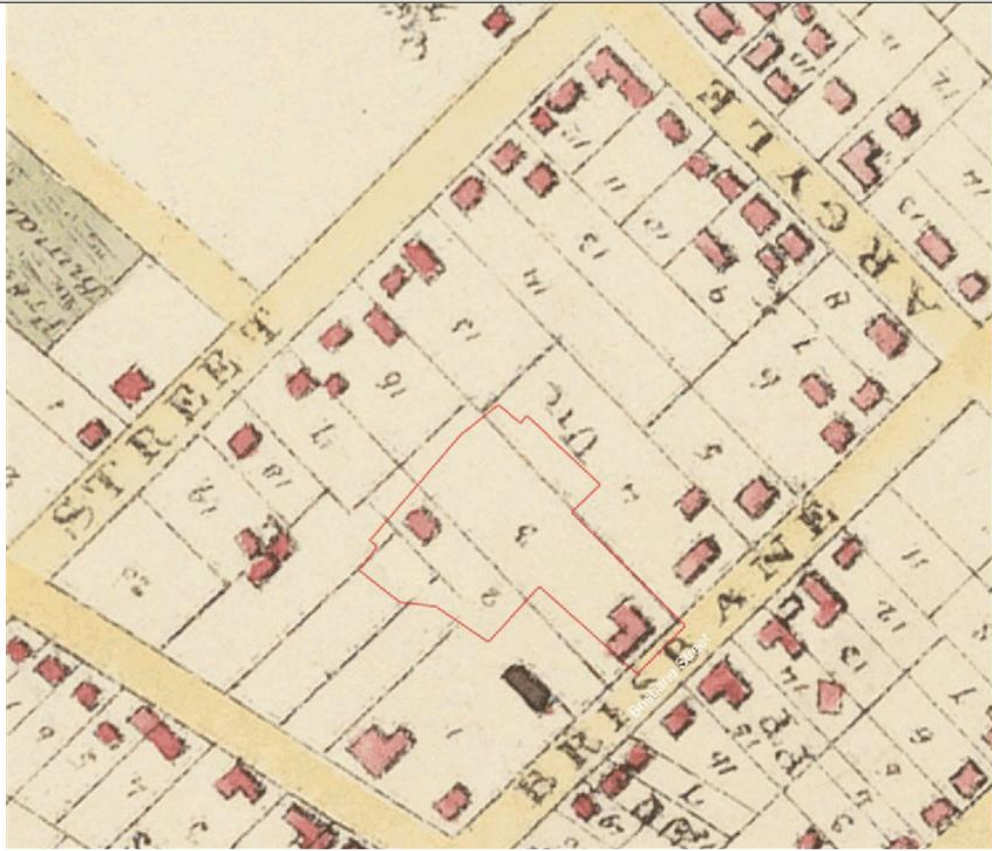
¹⁴ TA CSO1/1/68/1402

¹⁵ TA CSO1/1/49/922

¹⁶ TA CSO12/1/468/10387

The Hunt grant
<p>The first reference to this grant discovered during this research occurs in October 1835, when Archibald Macarthur sold a property which matches the 2 roods 18 perches shown on Sprent. Macarthur arrived free in Hobart Town in 1822 on the Skelton and in January 1823 held the first Presbyterian service in Hobart. He is closely associated with the construction of the first Scots Kirk (now St Andrew’s Hall) and later the Scots Kirk in Bathurst Street.</p> <p>Baptismal records indicate that Macarthur and his wife Mary were living in Brisbane Street in 1829. That building is depicted on this survey. In October 1835 Macarthur sold the property to George Hunt, a Hobart merchant, for £1,058. The description of land conveyed included “together with the Stone Messuage or Tenement and all or singular other the erections and buildings now standing thereon”.</p>

Figure 4.3 - Frankland's 1839 map of Hobart.



The Foord grant

This survey shows the Foord building on the Brisbane Street frontage, as per the earlier depiction.

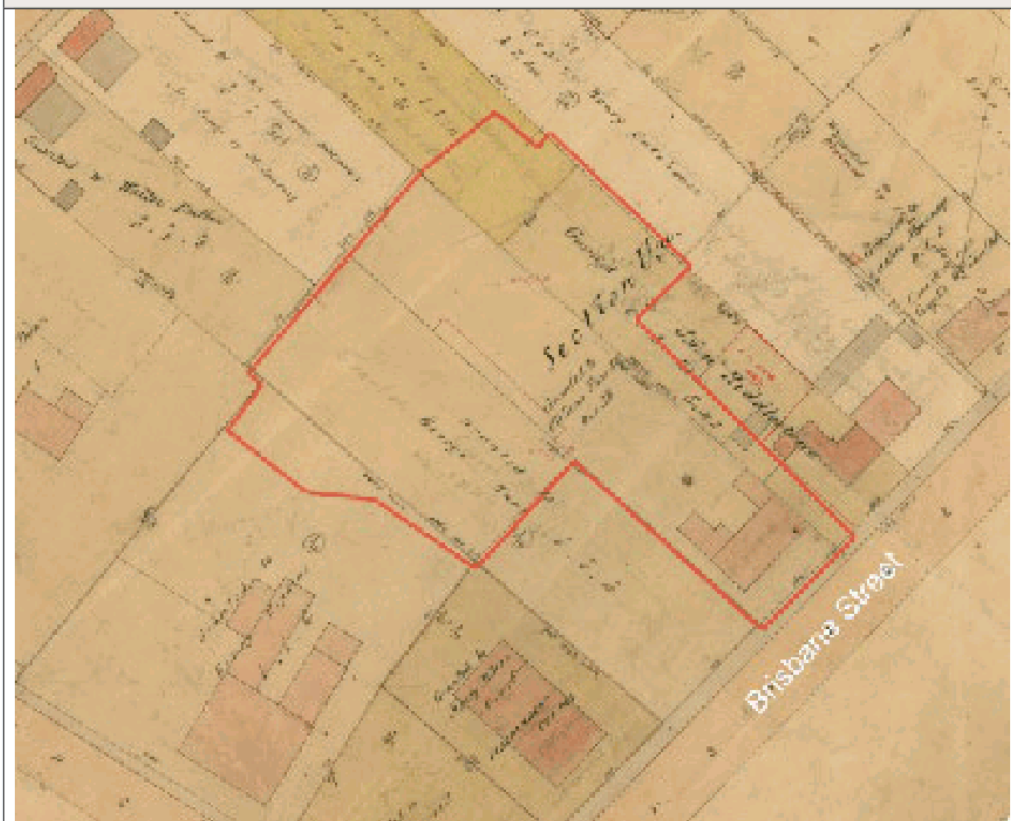
The Hiddlestone grant

Although not particularly accurate this survey shows the buildings on the Hiddlestone grant outside the study area, as per the earlier depiction.

The Hunt grant

This survey shows the Hunt building on the rear of the site, as per the earlier depiction.

Figure 4.4 - Sprent's c1845 survey of Hobart. TAHO AF393-1-28.



The Foord grant

William Foord died in January 1850, leaving the property to his widow, Mary Ford¹⁷. The following year Mary Foord married Peter Hughes¹⁸, who in 1851 mortgaged the 1 rood 26 perch allotment together with “the four several messuages thereon”¹⁹. The property was put up for auction in April 1852, and described as:

“In Brisbane Street, Hobart Town – Those three well finished Brick Houses situate near Elizabeth Street, with stables, gardens, carriage entrance and other conveniences AND ALSO a very delightfully situated Cottage adjoining the above and having an entrance from Brisbane Street”²⁰ This indicates that another terrace house had been added to the Brisbane Street frontage (as depicted on the Sprent survey) and another dwelling added to the rear, post-1845 but pre-1852.

¹⁷ TA AD960/1/3 Will number 404; see also DSG LTO Mem 5/5524

¹⁸ TAHO RGD37/1/10 no 130

¹⁹ DSG LTO Mem 3/6937

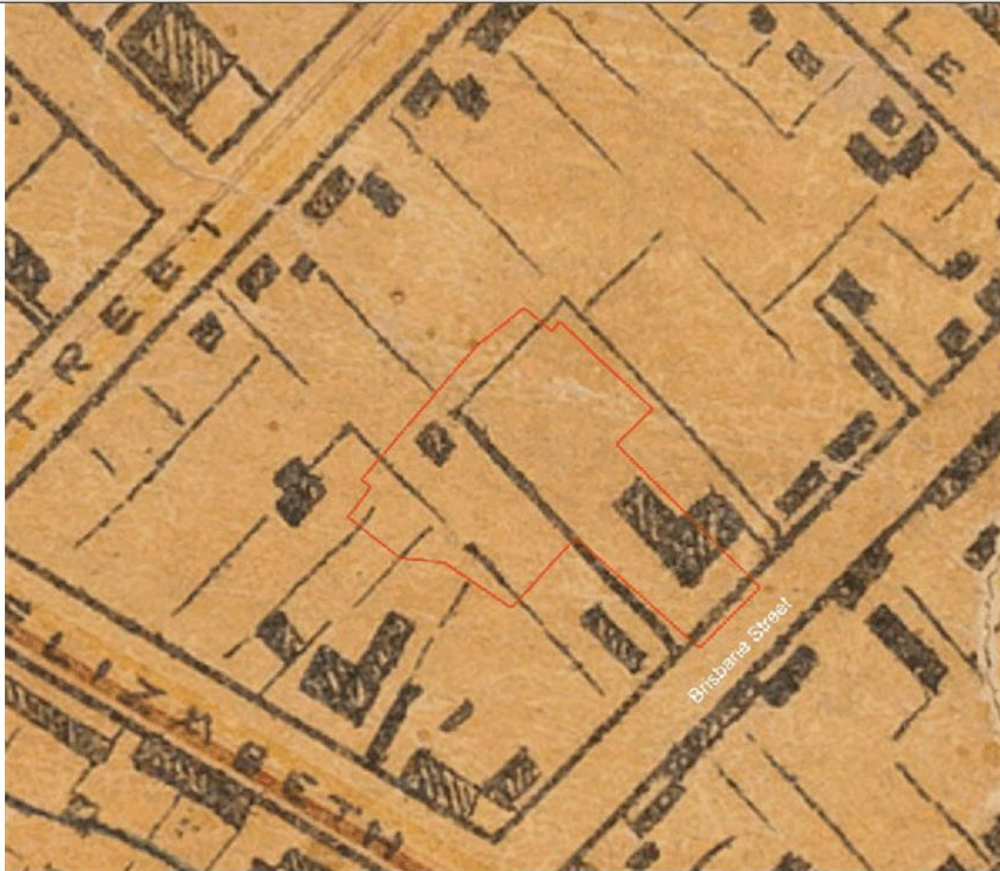
²⁰ Colonial Times 30 April 1852 p1

<p>This description (as well as the following auction description) appears to match the buildings shown on the Metropolitan Drainage Board Plans as 57,59 &61 Brisbane Street (the terraces) and 63 Brisbane Street (the 'Verandah Cottage'). The property sold at auction with both lots (i.e. the full 1 rood 36 perches) to Agnes Sommerville, a widow, for £1280²¹. Sommerville died in November 1855, leaving the property to Joseph Allport and William Knight in trust for the benefit of her daughter, Margaret Lamb. The property was retained by Lamb until April 1868, when it was put up for auction. As previously, the property was sold as two lots:</p> <p><i>“Lot 1: THREE BRICK HOUSES in Brisbane Street, between the properties of Messrs Salier and Murray, and near the Brisbane Street Chapel, one of which contains 9 well arranged rooms, 2 kitchens and outhouses, etc; the other two contain each 6 rooms and all have a fair-sized plot of ground at the back of each. The frontage on Brisbane Street is 54 feet 8 inches, by a depth of 297 feet. These houses are at present let to respectable tenants, and from their central position are rarely, if ever, empty. Water laid on.</i></p> <p><i>Lot 2: A VERANDAH COTTAGE, at the back of Lot 1, occupied by Mr Windsor, containing 3 rooms, kitchen, outhouses, etc; it has a large garden in good order and is altogether a most desirable residence. The view from this house is grand, and from its raised position is looked upon as very healthily situated; it possesses a right of way from Brisbane Street through Lot 1, and has a depth of 118 feet, by a width of 63 feet”</i></p> <p>Lot 1 (the terraces with Brisbane Street frontage) sold to James Ebenezer Salier for £480; Lot 2 (the 'verandah cottage' above lot 1) sold to Angus Cumming, a draper, for £265²². Four years later, in May 1872, Cumming sold Lot 2 to Salier, thus putting him in possession of Foord’s entire 1 rood 36 perches²³.</p>
<p>The Hiddlestone grant</p>
<p>As per the earlier depictions, this survey shows the Hiddlestone buildings outside the subject site. The rear portion of site had been adhered to the backyard of a Patrick Street fronting property, with no development depicted.</p>
<p>The Hunt grant</p>

²¹ DSG LTO Mem 3/67937
²² DSG LTO Mem 5/5524 and 5/5529
²³ DSG LTO Mem 5/9170

Hunt had his ownership recognised by the issuance of a title from the Crown in June 1840. In February 1850 Hunt mortgaged the property to John Murray for £550. The next transfer occurred in April 1870, when Murray conveyed the property to William Nicolson, Alexander Ireland, John Redman Bateman and Robert Galt Gray. The building at the rear of the site is not depicted on the Spret survey, although is known to have been present at that time – this is not unusual as the Spret survey only depicted buildings which were visible from the public domain, therefore it is not unusual that an internal building such as this was omitted.

Figure 4.5 - Hood's 1854 map of Hobart.



The Foord grant

The Hood survey of Hobart is known to be highly inaccurate in terms of boundaries and locations of buildings, however it is useful in depicting the *presence* of major buildings. This shows the terrace houses on the Brisbane Street frontage with no development at the rear.

The Hiddlestone grant

Hiddlestone applied to have his 1 rood 21 perch grant re-issued in July 1853²⁴ which was duly issued by the Crown in October 1853²⁵. Hiddlestone died at Brisbane Street in June 1859²⁶, leaving a life interest to Jane Elizabeth Hiddlestone,

²⁴ Colonial Times 7

²⁵ TA RD1/1/27 p70

²⁶ TA RGD35/1/6 Number 1543

his widow²⁷. Jane Hiddlestone died in October 1861²⁸; in February 1862 the 1 rood 21 perch grant was put up for auction in two lots:

“LOT 1 will comprise a well-built two-storey brick dwelling, with a neat and convenient cottage adjoining, together with coach house, stables, garden, yards, and drying ground, the whole having a frontage on Brisbane Street of 63ft or thereabouts..

LOT 2 that substantial, well-finished family residence, with numerous outbuildings and every convenience for a respectable family, beautifully situated in the centre of a large and productive garden, and commanding a delightful view of the harbour and river”²⁹

Both lots were purchased by James Ebenezer Salier (price unknown)³⁰. The advertisement above indicates that Lot 1 was the brick c1824 house (outside the study area) on Brisbane Street, with Lot 2 being the house which James Ebenezer Salier would name Sydney Lodge³¹. Given that Sydney Lodge was not depicted on the 1854 Hood survey, and was described in an 1862 auction notice, that would indicate a c1860 construction which is consistent with the styling of the building.

The Hunt grant

The 1820s Hunt building is depicted on this survey approximately as per previous depictions.

²⁷ TAHO AD960/1/4 No 797

²⁸ TA RGD35/1/6 No 3011

²⁹ *The Mercury* 7 February 1862 p4

³⁰ DSG LTO Certificate of Title (CT) 1/1112

³¹ The first reference to 'Sydney Lodge' in Bathurst Street found via Trove occurs in August 1864 – see *the Mercury* 9 August 1864 p1

Figure 4.6 - C1882 panorama of Hobart from the east. TAHO NS1697-1-142.



The eastern elevation of Sydney Lodge can be seen in this image (red arrow), showing it in much the same form as it remains today, however a two-storey extension is visible on this elevation. Note that there is no veranda. The terrace houses on the street frontage cannot be seen clearly (blue arrow depicting the approximate location).

Figure 4.7 - C1900 Brisbane Street streetscape. TAHO NS392-1-752.

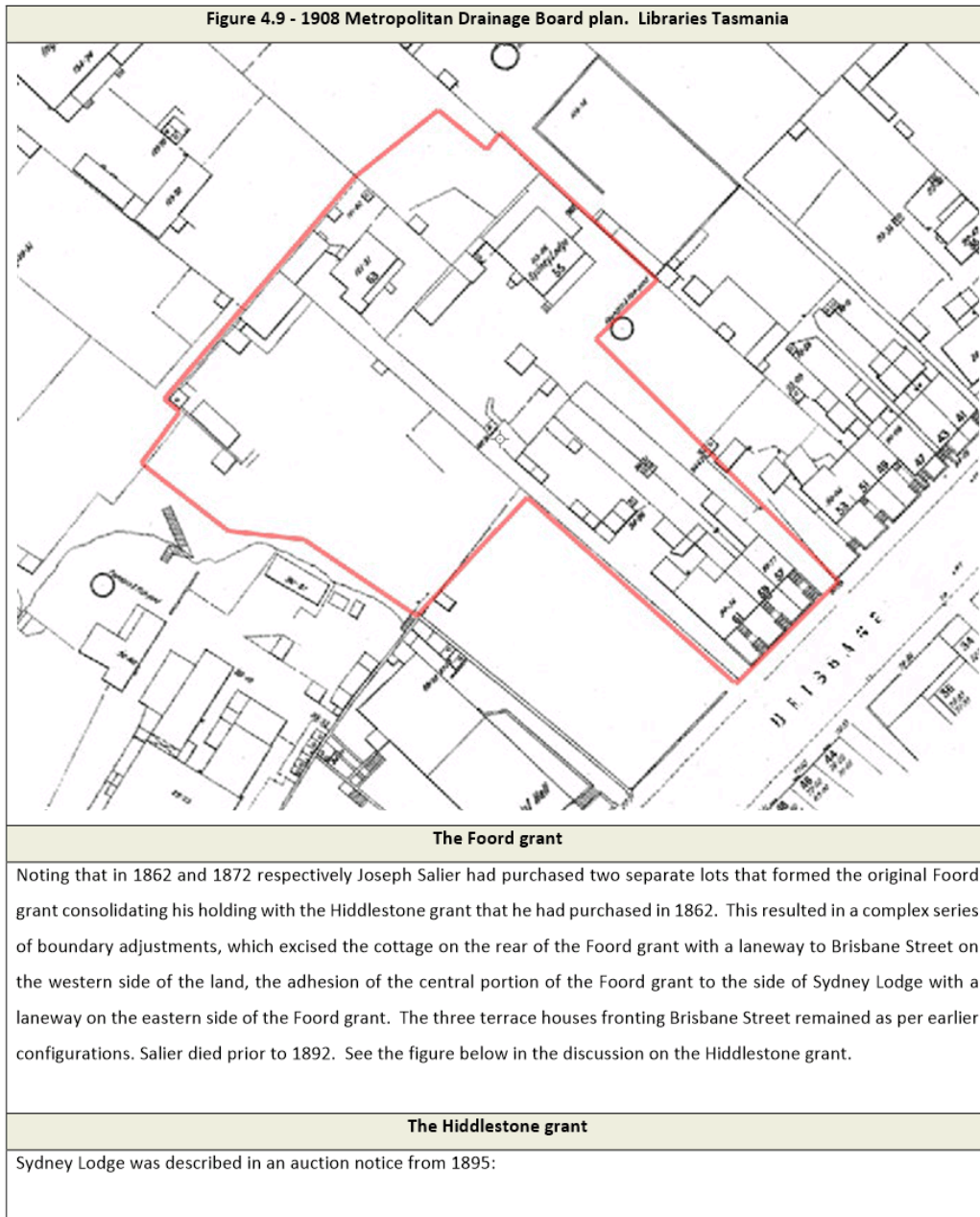


The terrace houses on the street frontage can be seen obliquely, showing the building as a two-storey brick terrace with a hipped roof and cast-iron balconies. Note that it appears to be raised considerably from street level.

Figure 4.8 - 1905 Brisbane Street streetscape. TAHO PH30-1-3706.



Whilst this image only depicts a small portion of the subject site (i.e. the western front corner of the Hiddlestone grant (i.e. the terrace houses) it demonstrates that the front of the site had a retaining wall therefore the floor level of the houses was raised well above street level.



'that charming family residence, Sydney Lodge, known for many years as the residence of Mr JE Salier. The house is built of brick, and contains large drawing room, dining room, smoking room, study, four bedrooms, kitchen, pantry and outside kitchen, conservatory etc. The grounds are extensive and tastefully laid out, and although the property is situated in the heart of the city, it has all the seclusion and retirement of a country residence'³²

As noted above, by 1872 Salier was also in possession of the entire Foord grant³³. Sydney Lodge was leased following Salier's death in 1895³⁴ and by 1900 was being operated as a "private residence for visitors"³⁵. In March 1901 Salier's property was put up for auction, including:

"Lot 1: Sydney Lodge, Brisbane Street, comprising commodious family residence with extensive grounds, for many years in the occupation of the late Mr Salier

Lot 5: Brick cottage, No 53 Brisbane Street, Rental £15/12s per annum,

Lot 6: Brick terrace of 3 two-storey houses, Nos 57,59 and 61 Brisbane Street, rental £78/18s per annum³⁶

The property was eventually sold by Salier's estate to Caleb Pryor Tapping for a total of £1,225 in March and April 1902³⁷. The figure below depicts the configuration of the Hiddlestone grant at that time – the central portion adhered to Sydney Lodge and the terrace houses at front (note that the rear cottage is not depicted on this survey, having been sold to F.H. Crisp prior.

³² *The Mercury* 16 January 1895 p4

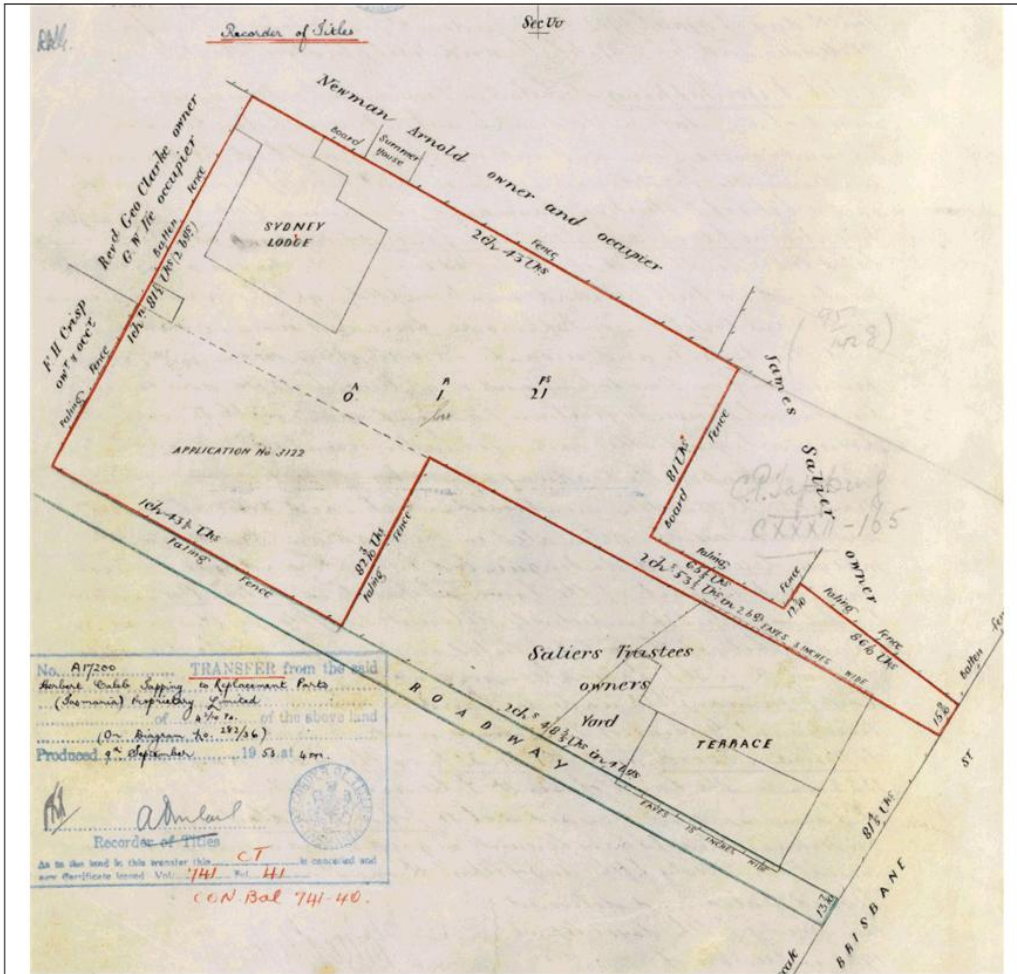
³³ DSG LTO Mem 5/9170

³⁴ *The Mercury* 30 September 1895 p1

³⁵ *Daily Telegraph* 13 December 1900 p4

³⁶ *The Mercury* 11 December 1901 p4

³⁷ DSG LTO CT1/11 and 126/137



Configuration of the Sydney Lodge property in April 1902. DSG C/T 126/137.

The Hunt grant

In May 1908 the property was in the possession of Frederick Henry Crisp, a Hobart timber merchant. In November 1908, Crisp's estate subdivided the property and sold the Brisbane Street frontage to William Blockey Drake, leaving the remaining 1 rood 15 perches in the possession of the Crisp estate, which forms part of the subject area³⁸.

³⁸ DSG LTO CT173/143

This remaining portion of the Hunt grant was sold in April 1913 to Alfred Thomas Davis for £3,000³⁹. By this time, the allotment had taken on a very different shape, being the northern half of the Hunt grant and adjoining land fronting Patrick Street (outside the subject site). The MDB survey shows that the 1820s Hunt house had been demolished by this time – it is unclear when. Some outbuildings of unknown vintage remained and may have been associated with the Patrick Street property – although noting that an early sandstone wall remains on this boundary which suggests that those outbuildings may be early.

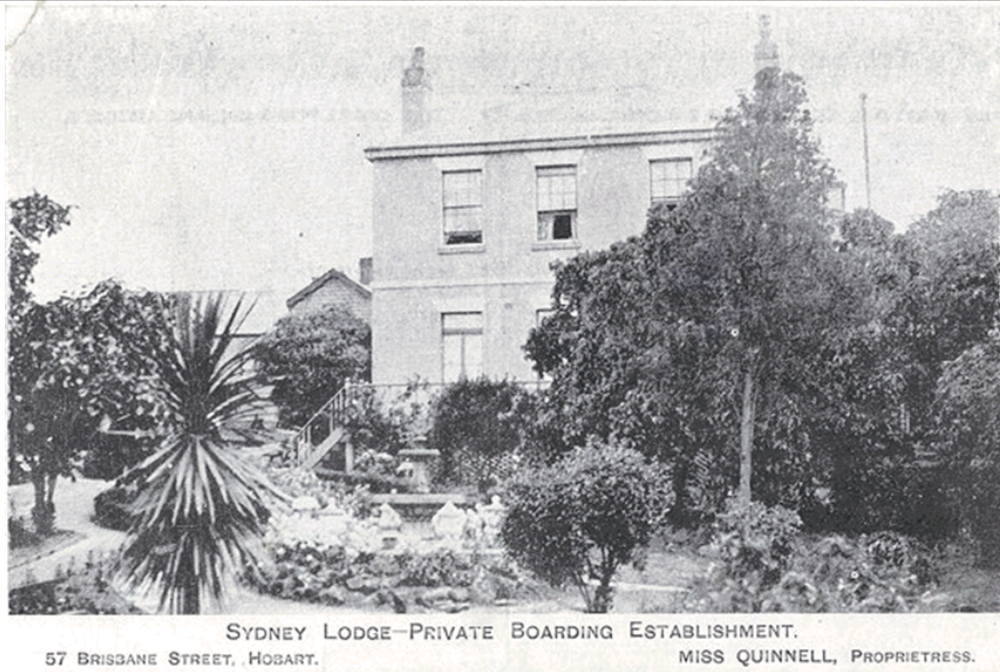
³⁹ DSG LTO 173/142

Figure 4.10 - C1915 Brisbane Street streetscape. TAHO NS1553-1-344



The terrace houses on the street frontage can be seen obliquely, showing the building as a two-storey brick terrace with a hipped roof and cast-iron balconies. Note that it appears to be raised considerably from street level.

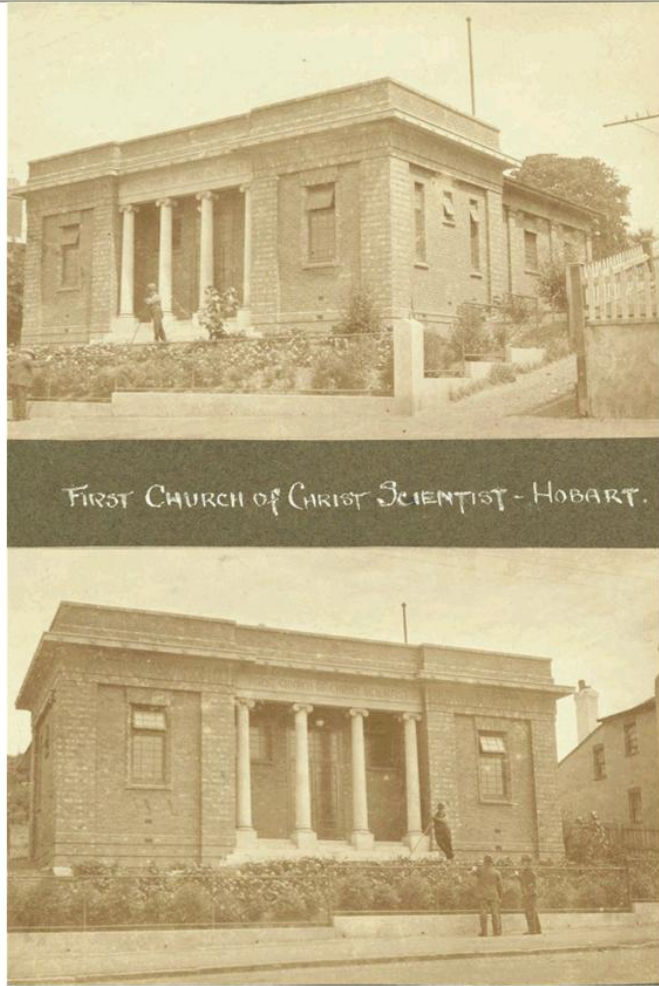
Figure 4.11 - Tasmanian Boarding House Directory 1933-34. Libraries Tasmania TAS_INDEX_MARC:196160.



Sydney Lodge operated as a boarding house for the first three decades of the 20th century:

'Sydney Lodge: Hobart. Sydney Lodge Guest House, 57 Brisbane Street, Hobart, is five doors from Elizabeth Street, and near the trams, trains and boats. There is an excellent view, with nice grounds and modern comforts. Cleanliness is the motto of Mrs Grove, the proprietress, late of the west coast, and a daughter of Mr AT Anderson...'

Figure 4.12 - Adjacent First Church of Christ Scientist 1928. TAHO NS5175-1-71.



The First Church of Christ Scientist was built on the front of the Hunt grant (outside the subject site) in 1928. These images show the building newly completed, and include a portion of the terrace houses on the front of the Foord grant. Again this shows the building elevated from the street, and it shows a substantial two-storey skillion at the rear.

Figure 4.13 - 1946 aerial photograph of Hobart. DPIPWE 0015-916.



The Foord grant

This image shows the buildings on the Foord grant much as they were for most of the c19th, with the terrace houses fronting Brisbane Street and the cottage at rear. The central portion being the side garden of Sydney Lodge.

The Hiddlestone grant

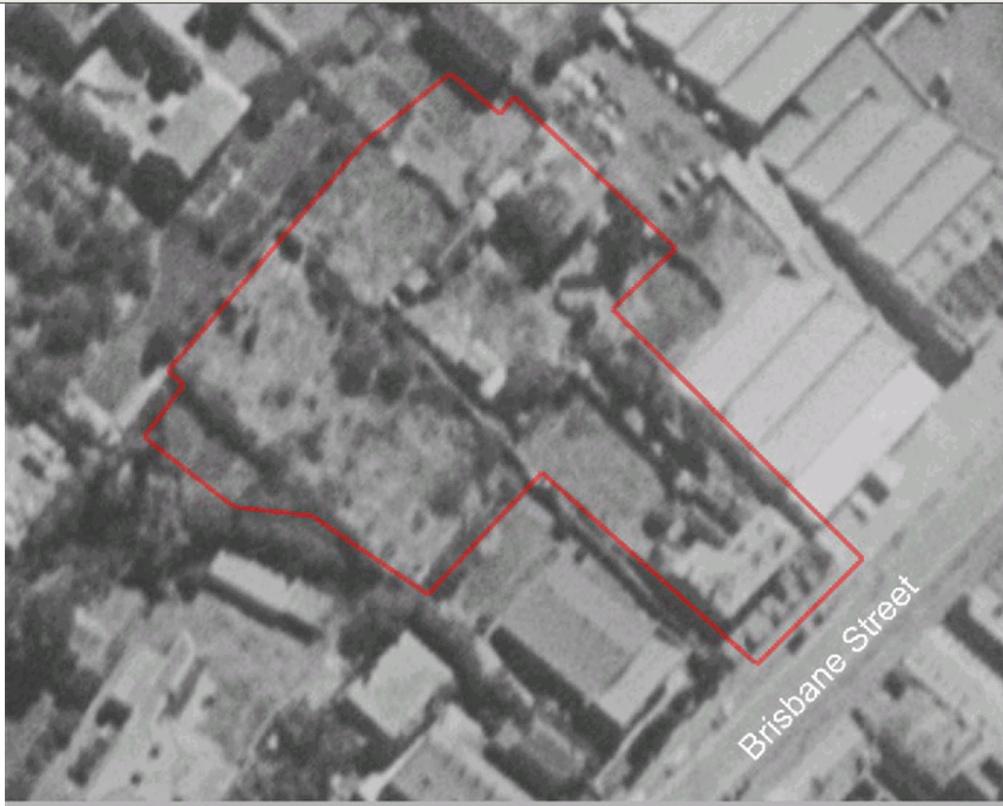
This image shows Sydney Lodge with the front garden and fountain as well as expansive side and rear gardens.

The Hunt grant

Davis sold had sole the rear of the Hunt grant to Emma Bessier in December 1922. During those decades the land appears to have been well-landscaped gardens probably associated with her wider landholdings in Patrick Street. The property remained in her possession until December 1955, when a portion of her enlarged holdings (outside the subject area) was sold to Margaret Lansdell; the bulk of her holdings, part of which forms the study area, was sold to the Director of Housing in February 1956⁴⁰

⁴⁰ DSG LTO CT803/83 and CT 837/39

Figure 4.14 - 1957 aerial photograph of Hobart. DPIPWE 0326-140



The Foord grant

The terrace houses fronting Brisbane Street remained in 1957, although the cottage at the rear had been demolished since 1946. The central section of the Foord grant remained as the side garden of Sydney Lodge.

The Hiddlestone grant

In 1953 a portion of the front garden of Sydney Lodge was sold to Replacement Parts Tasmania⁴¹, who had (between 1946 and 1953) purchased the front section of the Hiddlestone grant, demolished the earlier buildings and had established a warehouse. At that time Sydney Lodge was owned by H.C. Tapping.

⁴¹ DSG LTO CT741/41

Sydney Lodge and an area of 1rood 19perches were sold to the Director of Public Housing in July 1956 and then to The Crown in April 1958. ⁴²
The Hunt grant
By 1957, the Bessier holding appears to have been neglected, with the landscaped gardens seen in the 1946 aerial largely denuded.

In 1957 the Education Department expressed an interest in purchasing the Sydney Lodge site in conjunction with neighbouring *Ingomar* to create a new Domestic Arts School, thus leaving the old building in Murray Street ⁴³. Correspondence from the Education Department noted:

'the property acquired includes among other buildings, 3-conjoined residences fronting on Brisbane Street in respect of which the Department has received notice from the City Council to effect repairs. The Department has taken no action in this connection as the buildings under its scheme were due for demolition.' ⁴⁴

The total cost to acquire the Brisbane Street site was £13,500. Tenders for demolition of the three buildings was advertised in late 1957 and early 1958⁴⁵.

In 1959 the Education Department was seeking accommodation for the Visual Aids Branch/Visual Aids and Science Equipment Centre and proposed to construct three pre-fabricated 'Cyclone' units on the land acquired between Patrick and Brisbane Streets. In 1962, the Education Department negotiated with Replacement Parts Pty Ltd (see above) to exchange parcels of land to enhance the site. Once agreement had been reached, the Education Department negotiated with Hobart City Council for the construction of temporary prefabricated "Cyclone" units for a period of three years. In 1963, to comply with building regulations, tenders were called for the erection of a brick veneer skin around the temporary structures, then known as the Teaching Aids and Science Centre⁴⁶.

A new store was constructed adjacent to the Visual Aids and Science Centre in 1964.⁴⁷ Note that for the sake of consistency, the term *Teaching Aids Centre* will be used in the discussion below.

⁴² DSG LTO CT741/41
⁴³ TA ED183/1/2966
⁴⁴ TA ED 183/1/2966
⁴⁵ *The Mercury* 21 December 1957 & 29 March 1958
⁴⁶ TA ED 183/1/2966
⁴⁷ *ibid.*

Figure 4.15 - Looking down the site prior to the construction of the Teaching Aids Centre 1959. TAHO AB713-1-7034



This image shows the neglected gardens at the rear of the Hunt grant, towards the back of the First Church of Christ Scientist.

Figure 4.16 - Looking up the site prior to the construction of the Teaching Aids Centre 1959. TAHO AB713-1-7034



This image shows the neglected gardens at the rear of the Hunt grant, towards the back of the Ingomar (Patrick Street) – the building depicted being off the subject site.

Figure 4.17 - Clearing for construction of the Teaching Aids Centre 1959. TAHO AB713-1-7041.



This image depicts the rear of the subject site, looking across the rear of the Foord grant from the Hunt grant (facing east). Sydney Lodge can be seen in the background. Note the flattening of the site, with a distinct edge of the cut behind the bulldozer.

Figure 4.18 - Clearing for construction of the Teaching Aids Centre 1959. TAHO AB713-1-7036.



This image shows the boundary between the Foord and Hunt grants at the rear of the site (facing north, former Ingomar rear extension in the background). Note the demolition of the stone wall (a portion of which remains on the boundary) and bulldozing of the site.

Figure 4.19 - Clearing for construction of the Teaching Aids Centre 1959. TAHO AB713-1-7037.



This image shows the rear boundary wall of the Hunt grant (facing west). Note the stone boundary wall – a portion of which remains. Note also the distinct edge of the cut behind the bulldozer and apparent early building (privy?) on the rear corner of the site.

Figure 4.20 - Clearing for construction of the Teaching Aids Centre 1959. TAHO AB713-1-7038.



This image shows the rear of the Hunt grant (facing south-west).

Figure 4.21 - Clearing for construction of the Teaching Aids Centre 1959. TAHO AB713-1-7042.



This image shows the rear of the Hunt grant (facing south).

Figure 4.22 - Construction of the Teaching Aids Centre 1959. TAHO AB713-1-7043.



This image shows the rear of the Hunt grant (facing west). Note the distinct cut to level the site to the right of the image. There appears to be a small outbuilding (possibly a privy) on the corner of the site.

Figure 4.23 - Construction of the Teaching Aids Centre 1959. TAHO AB713-1-7048.



This image depicts the rear of the subject site, looking across the rear of the Foord grant from the Hunt grant (facing east). Sydney Lodge can be seen in the background. Note the flattening of the site, with a distinct edge of the cut behind the work site.

Figure 4.24 - Construction of the Teaching Aids Centre 1960. TAHO AB713-1-7011.



This image shows the rear boundary wall of the Hunt grant (facing west). Note the stone boundary wall – a portion of which remains. Note also the distinct edge of the cut at the edge of the works site. The frames of the temporary 'cyclone' building are under erection.

Figure 4.25 - Construction of the Teaching Aids Centre 1960. TAHO AB713-1-7098.



This image shows the boundary line between the Foord and Hunt grants, facing south. Note the deep service trench, flattening of the site and the frames of the temporary 'cyclone' building under erection.

Figure 4.26 - Construction of the Teaching Aids Centre 1960. TAHO AB713-1-7098.



This image shows the rear of the Foord grant, from the Hunt grant. Note the timber building to the side of Sydney Lodge.

Figure 4.27 - Construction of the Teaching Aids Centre 1960. TAHO AB713-1-7158.



This image shows the recently completed Teaching Aids Centre on the rear of the Hunt grant. Note the distinct cut on the left of the image.

Figure 4.28 - Completion of the Teaching Aids Centre 1960. TAHO AB713-1-7283.



This image shows the recently completed Teaching Aids Centre on the rear of the Hunt grant across the rear of the Foord grant. Note the distinct cut in the foreground of the image.

Figure 4.29 - Completion of the Teaching Aids Centre 1960. TAHO AB713-1-7285.



This image shows the recently completed Teaching Aids Centre on the rear of the Hunt grant across the rear of the Foord grant.

Figure 4.30 - Clearing around Sydney Lodge, shortly after construction of the Teaching Aids Centre 1960. TAHO AB713-1-7284



Note the earlier glazing arrangement of the building.



Figure 4.32 - Teaching Aids Centre 1966. TAHO AB713-1-9837.



Note the upgrade of the buildings with the brick-veneer skin added in 1963.

Figure 4.33 - Teaching Aids Centre 1966. TAHO AB713-1-9839.



Note the upgrade of the buildings with the brick-veneer skin added in 1963.

Figure 4.35 - Teaching Aids Centre 1966. TAHO AB713-1-9836p1.



Figure 4.36 - 1967 aerial photograph of Hobart. DPIPWE 0442-143



This image shows the site in much the same arrangement as currently exists, noting that the Teaching Aids entre has had some extensions since.

Figure 4.37 - 1977 aerial photograph of Hobart. DPIPWE 0727-046



This image shows the site in much the same arrangement as currently exists, noting that the Teaching Aids entre had been extended to the south since 1967.

Figure 4.38 - Sydney Lodge 1977. Margaret Bryant Collection, TAHO NS3373-1-407.



Figure 4.39 - 1987 aerial photograph of Hobart. DPIPWE 1118-040



This image shows the site in much the same arrangement as currently exists, noting that the Teaching Aids entre had been extended to the west since 1977.

The above historical summary shows that:

- Two presumably conjoined terrace houses were constructed on the Foord grant prior to 1825. It is not clear if these were demolished to make way for a later 3 x terrace house development prior to 1845, or if another was added by that date. These were demolished c1960. Another freestanding dwelling was constructed at the very rear of the Foord grant between 1845 and 1852. This was demolished between 1947 and 1957.
- A residence was constructed on the Hiddlestone grant pre-1824, which was outside portion of that grant which now forms part of the subject site. Sydney Lodge was built on the rear of the site between 1854 and 1862, and that part of the site was subdivided with a laneway to Brisbane Street prior to 1862.
- A residence was constructed on the very rear of the Hunt grant prior to 1829. This was demolished prior to 1908 but may have had outbuildings which remained on the site after that date.
- The entire site was acquired by the Tasmanian Government/Crown in the late 1950s and all buildings except Sydney Lodge were demolished to make way for the Teaching Aids Centre which was constructed in 1960.

The following figures show overlay plans of known historic development in relation to the current layout of the site:



Figure 4.40 - Footprint of features as depicted on the c1829 survey plan (green) over a recent aerial photograph of the subject site (adapted from www.thelist.tas.gov.au).




Figure 4.41 - Footprint of features as depicted on the c1845 Sprent survey plan (yellow) over a recent aerial photograph of the subject site. The footprint of the building depicted on the c1829 survey on the rear of the Hunt grant is also depicted, as it is likely that the Sprent survey did not include this because of lack of visibility from public vantage points. Adapted from www.thelist.tas.gov.au.



Figure 4.42 - Footprint of features as depicted on the 1908 Metropolitan Drainage Board survey plan (blue) over a recent aerial photograph of the subject site (adapted from www.thelist.tas.gov.au).

5. General description and evolution of the place and surrounds


5.1 Sydney Lodge

Element	Image	Brief description
Form		<p>The main portion of Sydney Lodge is an 'L-shaped' two storey building with a medium-pitched hipped roof. Adjoined (and likely contemporary) service wing is tucked into the rear which extends slightly eastward of the front eastern wall forming an overall near-square plan. The building has four plain brick chimneys (some servicing multiple fireplaces).</p>

Southern elevation (principal façade)




The southern elevation of Sydney Lodge is symmetrical and somewhat Georgian in appearance (despite being much later than that period). The walls are painted brick on a rough-ashlar foundation. The ground floor has three sets of glazed French doors with top-lights and the top floor has three double-hung sash windows which were replaced in the 1970s (formerly being 12-pane). There is a masonry stringcourse and the building has narrow eaves. There has previously been a terrace at the front which has been removed.


<p>Eastern elevation</p>		<p>The eastern elevation of the front portion of Sydney Lodge includes only a later fire exit door and modern steel stair. A larger opening has been blocked which was associated with a former eastern extension (see Figure 4.6). The rear wing which extends further eastward than the front section has an original window on the first floor (bottom sash replaced) and modified windows on the ground floor. A modern extension at the rear can be seen on this elevation.</p>
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<p>Western elevation</p>		<p>The western elevation includes the main entrance door obscured by a modern porch. This has probably always been the main entrance with early images showing an earlier form of porch in this location. The ground floor has a single double-hung sash window to the rear of the main portion of the building and the upper floor has two windows. These would have originally been 12-pane but have been replaced in the 1970s. The rear extension can be seen on this elevation.</p>
<p>Northern (rear) elevation</p>		<p>The rear (northern) elevation shows the original roof form in the two sections and is blank except for an original 12-pane double-hung sash window on the upper floor of the wing. The ground floor is obscured by a modern extension.</p>

5.2 The former Teaching Aids building

Image	Brief description
	<p>This building is not the focus of the current report, as the statutory heritage listings for the site focus on Sydney Lodge and the archaeological potential of the site. Whilst not internally inspected, it is known that the 1960 prefabricated ‘cyclone’ buildings were temporary, and that it was required that these were upgraded soon after construction – with a brick skin added. There have certainly been extensive modifications since. See Figures 4.24 – 4.34. The northernmost bay of the 1960 building has been removed to slab level (now a carpark). A southern extension was constructed between 1967 and 1977 and a rear extension was constructed between 1977 and 1987. The outward appearance of the building in no way resembles the 1960 building.</p>

5.3 The carpark


Image	Brief description
	<p>With the exception of the workshops (former Teaching Aids building) and Sydney Lodge, the site is predominantly used for carparking. The entire site is asphalted and has a distinct slope upwards from the street. The carpark is further described in Section 7.5.</p>

5.4. Key views and vistas

Although the place is not within any heritage precinct under the Hobart Interim Planning Scheme 2015, the setting and curtilage of the listed place itself is a key consideration in any further development of the place. Accordingly, it is relevant to consider the key views to and from the place and the wider vistas within which the place is set. As per the Burra Charter process detailed in Figure 3.1, in order to understand and measure any possible heritage impact, an understanding of the significance of the setting and streetscape presence of the place is required. The following figure depicts the vey vantage points to the site which will be considered here – both on a nearby lens and on a wider visual catchment. This assessment will consider the importance of any residual historic views to the place and the possible impact that any development may have upon such:



Figure 5.1 – Key public vantage points towards 57-65 Brisbane Street from near environs. Adapted from www.thelist.tas.gov.au

Point	Importance to the listed place & setting (both historically and current)	Image	Possible impact of any proposed development on the site
1	<p>Sydney Lodge has never been a prominent element in the streetscape from this vantage point, being an internal building. This view would have been an important viewline to the earlier terrace houses on the Brisbane Street frontage.</p>		<p>It is considered that any development on the subject site would not have any adverse impact upon the streetscape qualities of Sydney Lodge from this vantage point.</p>

<p>2</p>	<p>Sydney Lodge has some townscape presence from this vantage point, however traditionally this would not have been the case. The building post-dates the two-storey terrace houses which once occupied the frontage of the site and was accessed by a narrow laneway – it would not have been a visible nor prominent</p>		<p>Whilst development on the Brisbane Street frontage would obscure this view, it would reinstate a more traditional overall urban form which would reinforce the original townscape context of Sydney Lodge being an internal lot/building.</p>
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<p>3</p>	<p>Sydney Lodge has some townscape presence from this vantage point, however traditionally this would not have been the case. The building post-dates the two-storey terrace houses which once occupied the frontage of the site and was accessed by a narrow laneway – it would not have been a visible nor prominent</p>		<p>Whilst development on the Brisbane Street frontage would obscure this view, it would reinstate a more traditional overall urban form which would reinforce the original townscape context of Sydney Lodge being an internal lot/building.</p>
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<p>4</p>	<p>Sydney Lodge is not a visible streetscape element from this vantage point. Historically with domestic-scale development on the street frontage, Sydney Lodge and its front garden would have been a visible (but still not prominent) streetscape element however subsequent neighbouring development has obscured any such views.</p>		<p>It is considered that any development on the subject site would not have any adverse impact upon the streetscape qualities of Sydney Lodge from this vantage point.</p>
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5.5. Description of the townscape development of the area

Whilst the primary objective of this document is to consider the historic heritage significance of 57-65 Brisbane Street and to provide conservation policy to guide any future development of that place, as part of understanding the townscape and streetscape presence of Sydney Lodge, and the possibility of appropriate development elsewhere on the site, it is necessary that the evolution of that wider place be considered. Further, to understand whether the possibility of infill development on any part of the site may be appropriate, it is necessary to understand the evolution of the wider environs of the subject site. Accordingly, an understanding of the evolution of the physical attributes of that setting be gained within which the significance of the setting of the place can be better understood.

Section 4 has provided an overview history of the site, from which a wider lens can be cast to understand the historical evolution of the immediate surrounds. The following figures provide an approximation of historic titles that may have influenced the pattern of townscape development from early European settlement – based on the block bounded by Brisbane, Elizabeth, Argyle and Patrick Streets (the subject site shaded). Note that these may not necessarily represent precise title configurations but are based on both known title configuration and *apparent* configuration (e.g. where there may be several buildings on the same title, conversely where multiple titles may have comprised one apparent place).

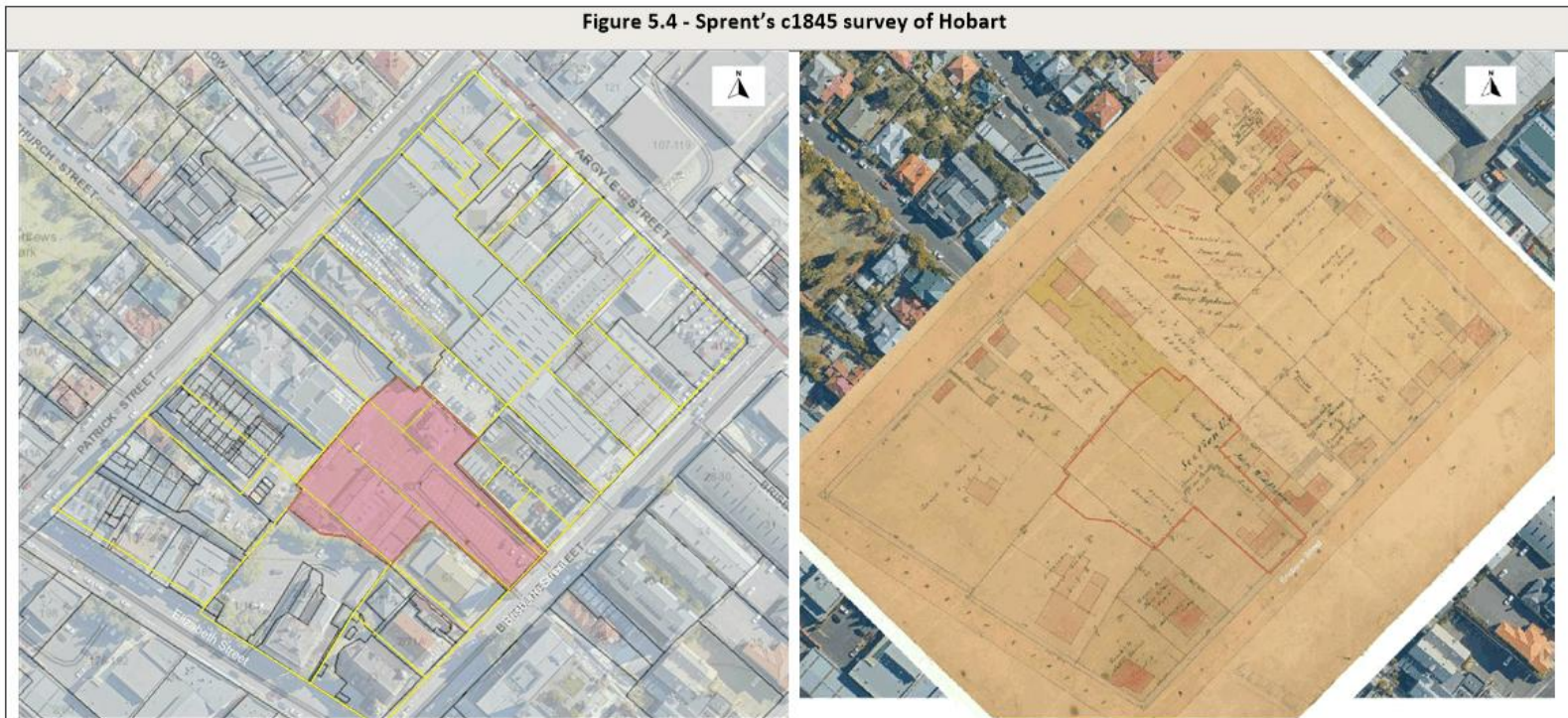


Figure 5.3 - C1829 survey of Hobart



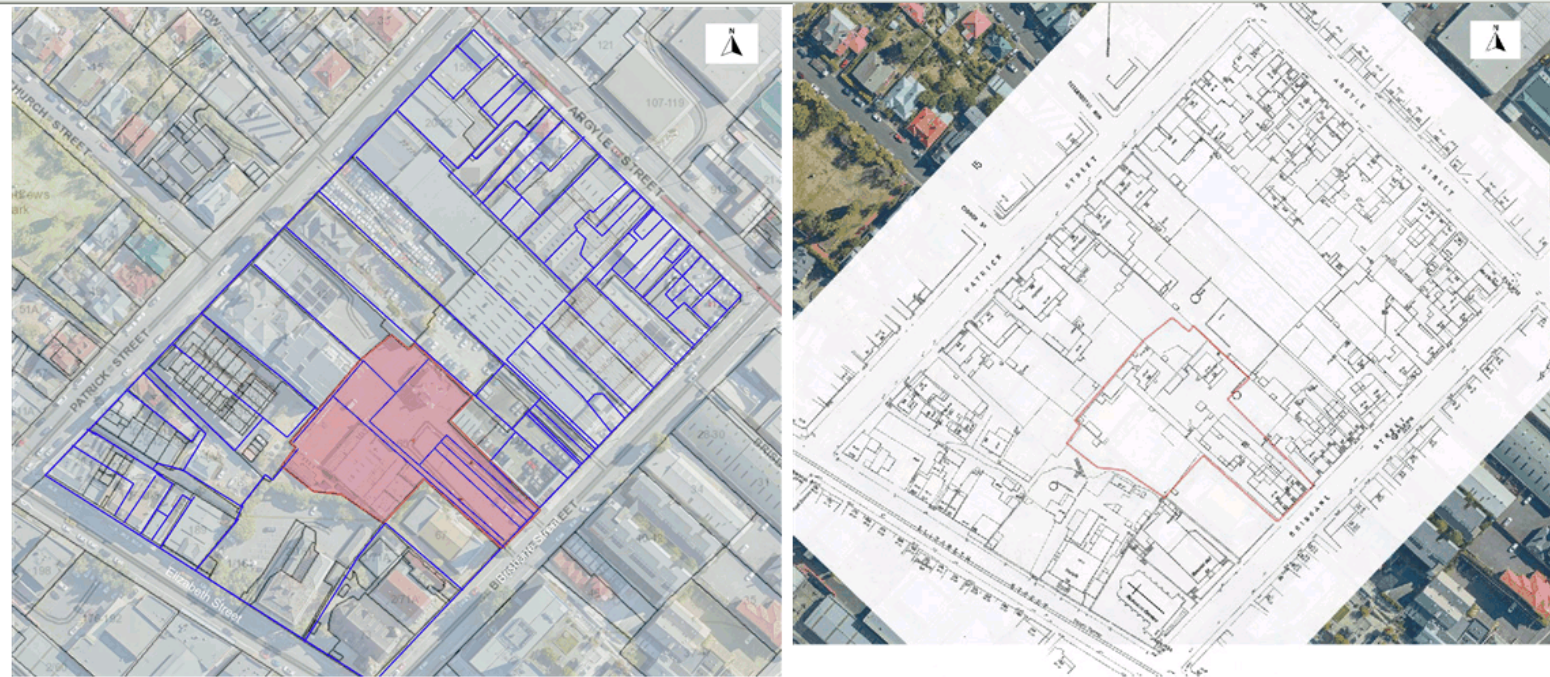
The c1829 survey of Hobart shows a similar arrangement to the grants map of larger allotments although by this time some had been consolidated into even larger lots. Some internalization of development (e.g. the Hunt grant) had occurred by this time.

Figure 5.4 - Sprent's c1845 survey of Hobart

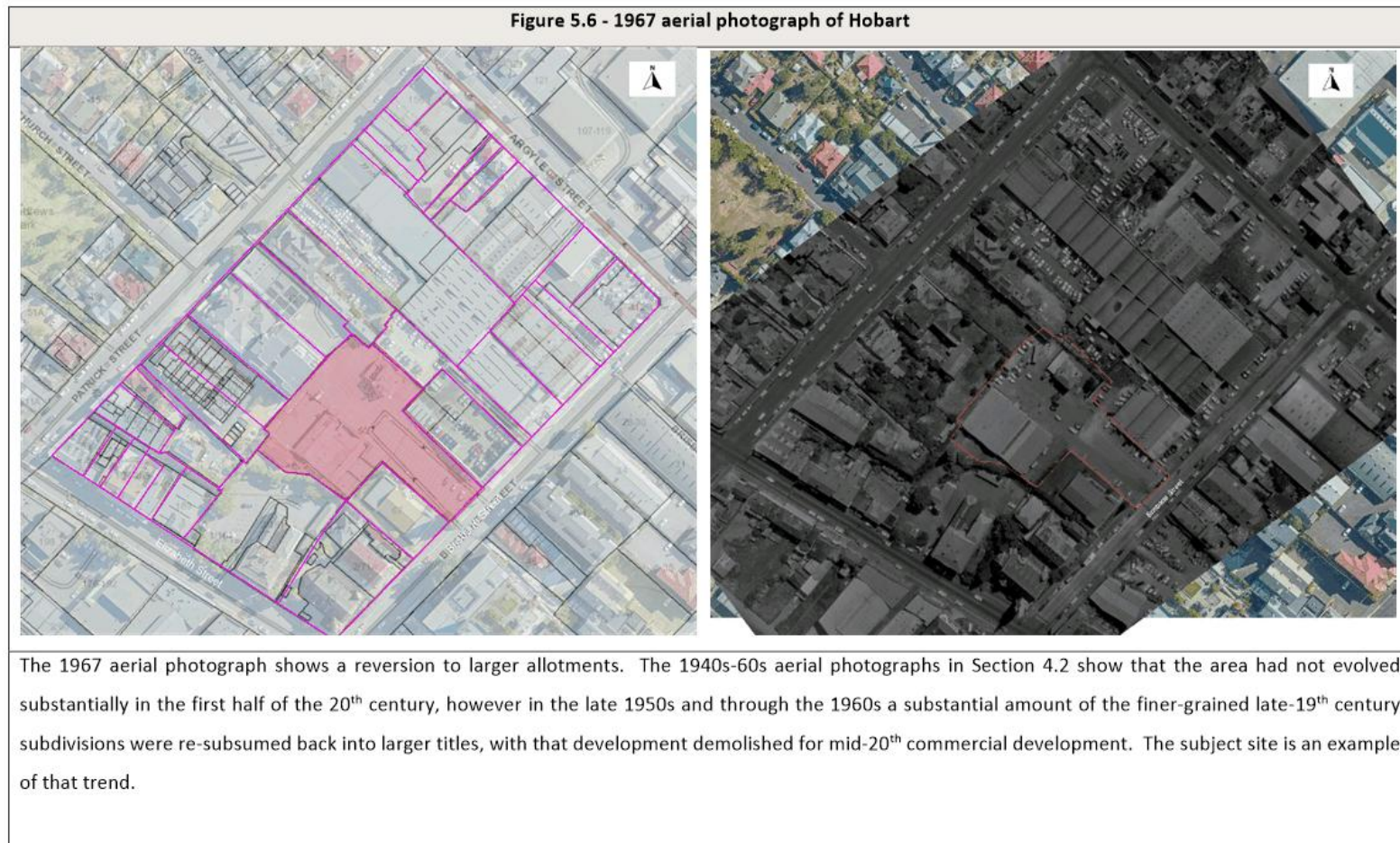


Sprent's survey shows a similar arrangement of large allotments, although some subdivision had occurred on Argyle and Brisbane Street frontages. The Sprent survey is not particularly useful in demonstrating internal development patterns, as it relied on streetfront/publicly visible buildings, therefore some known internal development is not depicted.

Figure 5.5 - 1908 Metropolitan Drainage Board survey of Hobart



By 1908 a substantial amount of subdivision and infill had occurred, with the larger allotments generally only surviving on the Patrick Street frontage as well as those associated with Westella and the church on the corner of Elizabeth and Patrick Streets. Not a substantial amount infill had occurred in the latter-half of the 19th century with development maximising much narrower street frontages – Sydney Lodge had been built during that period, with the internal development of the block largely limited to subject site as well as some smaller-scale developments off Argyle Street.



The above analysis demonstrates the following points:

- The block is typical of the larger city-fringe allotments designated around 1820, with long and narrow allotments with narrow street frontages common.
- Generally early development was near the street frontage, with large backyards, although internal development was common.
- The latter-half of the 19th century saw substantial subdivision of these larger allotments with some internal development accessed by laneways (i.e. battle-axe lots). Sydney Lodge is an example of such development.
- That pattern of development and subdivision existed for the first half of the 20th century.
- In the 1950s and 60s, much of that finer-grained development of the later 19th century was re-subsumed into larger allotments to make way for commercial development.

The subject site is typical of that pattern of development. Sydney Lodge is a rare survivor of the mid-19th century infill development, but the overall subject site is typical of the mid-20th century consolidation of earlier titles into larger commercial lots.

5.6. Architectural tenor of the area

That ad-hoc development of this part of the CBD fringe has resulted in the area representing a diverse range of architectural styles reflecting different periods and scales of subdivision patterns periods – as per the discussion on subdivision and infill development above, the environs of the subject site are an example of that ad-hoc development that has evolved in the way it demonstrates townscape form and architectural character, which is probably demonstrated by the fact that the area is not recognised as any heritage precinct.

This analysis considers the general architectural tenor of the buildings in the surrounding area as defined by Figure 5.7 as a means of understanding the wider architectural character of the immediate environs of the subject site. Note that this assessment was done via online sources and observations from public spaces - no private property was accessed and no photographs of private residences were taken.



Figure 5.6 - Study area for the 'immediate environs' description below. Adapted from www.thelist.tas.gov.au

Colour	Architectural style/era
Yellow	Post-mid c20th larger-scale apartment/commercial development.
Orange	Later c20th smaller-scale apartment/commercial/residential development.
Blue	Early-mid c20th residential/commercial development.
Green	C19th residential/commercial development and c19th-early c20th ecclesiastical buildings (heritage listed).

Figure 5.6 depicts the following points on the tenor of the architectural forms of the surrounds of the subject site:

- Few 19th century building survive in the area – mostly clustered around the corner of Elizabeth and Brisbane Street and fronting Patrick Street. Sydney Lodge and 40a Patrick Street) are the only remaining internal examples of such a building.
- Of the 19th century buildings, there is no cohesive style, setback or function – e.g. there are a range of architectural styles, periods and sizes of residential buildings, and a range of commercial and ecclesiastical buildings.
- Early 20th century buildings are uncommon – most of these (together with much of the earlier 19th century building stock) were cleared in the mid 20th century for commercial development.
- The Argyle Street half of the block is typified by mid-late 20th century commercial buildings with whole-of-site footprints, which is common on the north-eastern portion of the Brisbane Street frontage.
- The area has no coherent or consistent streetfront setback, and the immediate environs of the subject site is on the border between larger scale commercial development and the cluster of 19th-early 20th century ecclesiastical buildings.

This evolution of the area is a key principle which will be considered in the following section regarding significance of the subject site and surrounds.

5.7. Adjacent state heritage listed places

As per the statutory heritage provisions detailed in Section 2, the Tasmanian Heritage Council has the ability to consider the possible impact of development of a site registered on the Tasmanian Heritage Register upon adjacent places that are also registered. As depicted on Figure 2.1, the site is adjacent to *Ingomar* (34 Patrick Street, THR 2566).

An apartment block at the rear of *Ingomar* has recently been constructed which provides complete visual separation of the heritage building of *Ingomar* itself from the subject site. It is concluded that any development on the subject site permissible under the planning scheme would have no impact whatsoever on *Ingomar*.



Figure 5.7 – Ingomar Apartments from the subject site.

6. Statement of historic heritage significance

No detailed statements of significance are known to exist for 57-65 Brisbane Street. The Tasmanian Heritage Register datasheet for Sydney Lodge only gives very brief statements, which are expanded upon here. The following statements of significance are based on the national HERCON standard for statements of significance, based on the amount of information currently at-hand. Note that natural history values have not been assessed here, as these are beyond the scope of this assessment. This statement of significance has been based upon the history of the place as outlined above, however could be further refined with more detailed investigations into the site history. The statement has also been formulated with consideration to the site and environs analysis in Section 5.

A. Importance to the course, or pattern of our cultural or natural history.

Sydney Lodge is of historical significance in demonstrating the second-generation infill development of a larger allotment on the Hobart city fringe and the creation of internal allotments demonstrating the increasing density of the city in the mid-19th century.

B. Possession of uncommon, rare or endangered aspects of our cultural or natural history.

The site, nor the immediate surrounds are considered to exhibit any uncommon, rare or endangered aspects of our cultural history.

C. Potential to yield information that will contribute to an understanding of our cultural or natural history.

The site is known to have been the site of at least two 1820s buildings at the rear, as well as three pre-1840s terrace houses on the Brisbane Street frontage (possible replacing an earlier 1820s building. This is further considered in Section 7.

D. Important in demonstrating the principal characteristics of a class of cultural or natural places or environments.

Sydney Lodge is considered to be a fine example of a c1860s larger-scale city fringe residence representing second-generation infill development of the site. The western and northern additions are of no significance. Although largely lost, the garden setting of the building was a key factor in its significance. The building has little streetscape significance – having always been set on an internal allotment, however that internal lot arrangement has some significance in demonstrating mid-19th century townscape evolution of the Hobart city fringe.

E. Importance in exhibiting particular aesthetic characteristics

It is not considered that the site nor the immediate surrounds demonstrate any remarkable aesthetic characteristics.

F. Importance in demonstrating a high degree of creative or technical achievement at a particular period.

The site, nor the immediate surrounds are considered to demonstrate any high degree of creative or technical achievement.

G. Strong or special association with a particular community or cultural group for social, cultural or spiritual reasons. This includes the significance of a place to Indigenous peoples as part of their continuing and developing cultural traditions.

The site is not considered to have any strong nor special association with any cultural group for social, cultural or spiritual reasons.

H. Special association with the life or works of a person, or group of persons, of importance in our history.

The site has no known direct special associations with the life or works of a person or persons of importance in our history.

7. Statement of historical archaeological potential.

7.1. Statutory requirements for historical archaeology

As the subject site is within a *Place of Archaeological Potential* as defined by Table C6.4 of the scheme, Further to Clauses C6.8.1 and C6.10.3.

Additionally, the Tasmanian Heritage Council may consider archaeological impact in any proposal for the site – although no entry for Criterion C (Research Potential) is articulated in the Tasmanian Heritage Register datasheet, the council can require an assessment of archaeological potential to inform their assessment of any proposed development.

7.2. Archaeological methodology

This statement of archaeological potential is derived from a process which identifies the potential of the site to yield archaeological remains, the significance of any remains, and their potential to yield meaningful information about the site, and which might contribute to relevant key archaeological and historical themes. The following briefly outlines the methodology followed:

Determining general archaeological potential: Through a desktop analysis of historical data and secondary sources, as well as non-invasive site observations, an understanding of the evolution of the site has been gained which has allowed an assessment of the archaeological potential (however significant) of any part of the site - resulting in substantiated predictions of the likelihood of finding *something* upon any particular part of the site.

This has been done by analysing primary source material, summarizing the developmental history of the site and developing a chronological narrative detailing an overview of the history of all known features to have ever existed on the site. Where possible, developmental overlays have been developed from historic maps, plans, photographs and other visual documentation. This overlay has been supported by other observations providing supplementary information and also includes processes such as demolition and disturbance which may have removed or destroyed potential remains – and may have diminished the archaeological potential.

Assessing the significance and potential of any likely archaeological resources to yield meaningful information: Upon understanding the archaeological potential through desktop and site analysis, the next step was to understand its relationship to any aspect of the identified significance of the site – e.g. do the remains have the potential to demonstrate an aspect of the significance of the site or related key historic theme? The potential for any of the archaeological remains to demonstrate important aspects of the history of the site, whether in a state, regional or thematic context, is to be considered.

Understanding possible impact of development and formulation of management strategies: Based on any identified archaeological potential and significance of the site, consideration will be given as to whether the proposed development will impact upon any likely archaeological remains and if necessary broad management strategies will be proposed to manage any impact.

7.3. Determining general archaeological potential and significance

As per the historical background of the site presented here in Section 4, the following is known:

- Two presumably conjoined terrace houses were constructed on the Foord grant prior to 1825. It is not clear if these were demolished to make way for a later 3 x terrace house development prior to 1845, or if another was added by that date. These were demolished c1960. Another freestanding dwelling was constructed at the very rear of the Foord grant between 1845 and 1852. This was demolished between 1947 and 1957.
- A residence was constructed on the Hiddlestone grant pre-1824, which was outside portion of that grant which now forms part of the subject site. Sydney Lodge was built on the rear of the site between 1854 and 1862, and that part of the site was subdivided with a laneway to Brisbane Street prior to 1862.
- A residence was constructed on the very rear of the Hunt grant prior to 1829. This was demolished prior to 1908 but may have had outbuildings which remained on the site after that date.
- The entire site was acquired by the Tasmanian Government/Crown in the late 1950s and all buildings except Sydney Lodge were demolished to make way for the Teaching Aids Centre which was constructed in 1960.

These depictions of the historic features are presented in Figure 7.1:



Figure 7.1 – Composite overlay of outlines of features from the c1832 survey (green), Spren's 1840s survey (yellow) and 1908 MDB survey (blue). Note that the c1839 Frankland depiction is not presented here as its accuracy is dubious. Adapted from www.thelist.tas.gov.au

7.4. Consideration of prior disturbance

Disturbance from demolition of c19th buildings

There are several known demolition events that have occurred on the site:

- Demolition of the pre-1829 house on the rear of the Hunt grant. The date of demolition is unknown but between 1854 and 1908.

- Possible demolition of the pre-1824 house on the Brisbane Street frontage of the Foord grant (note that it is not clear if this was demolished, or added to in order to form the three pre-1845 terrace houses).
- Demolition of the c1845-1852 house on the rear of the Foord grant sometime between 1946 and 1957.
- Demolition of the three terrace houses on the Brisbane Street frontage of the Foord grant around 1958.

No documentation regarding the demolition of these buildings has been found in the course of this research (which is not unexpected). Another demolition event occurred in 1959 with the clearing of remnant outbuildings on the rear of the Hunt grant and the later c19th outbuildings surrounding Sydney Lodge – see below for commentary on the demolition of these, which is documented in a series of photographs from that time.

Figures 4.7, 4.8, 4.10 and 4.12 depict that the terraces fronting Brisbane Street were set higher than the street with a retaining wall at the front. That fact has implications for archaeological potential as it implies that their demolition would have included demolition of the retaining wall and the fill behind, which *may* have resulted in a more thorough demolition of sub-floor structure and deposits. The 1908 Metropolitan Drainage Board survey provides height data which has been found across the city to be highly accurate when compared (from consistent points) with current height data. Figure 7.2 depicts several height datum across the site from 1908, which is compared to current data in Figure 7.3. This shows that the floor level of the terrace houses were approximately 1600mm above the current ground level. This indicates that there has been a substantial amount of material removed from the Brisbane Street frontage – noting however that this diminishes further rearward, where levels are currently similar to 1908 – therefore the floor/yard level of rear portion of those buildings as well as the buildings to the rear of the site would have been similar to the current levels:

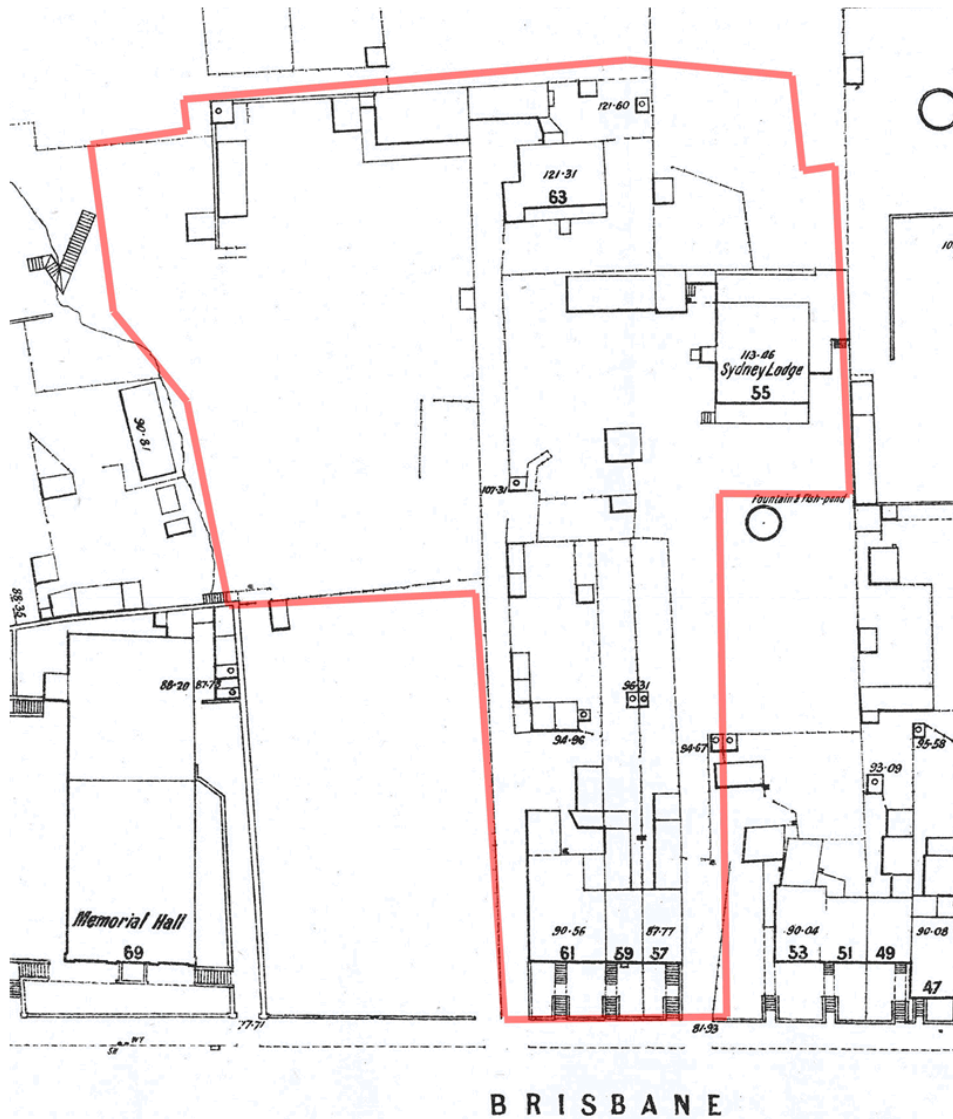


Figure 7.2 – Level data from the 1908 Metropolitan Drainage Board plans.

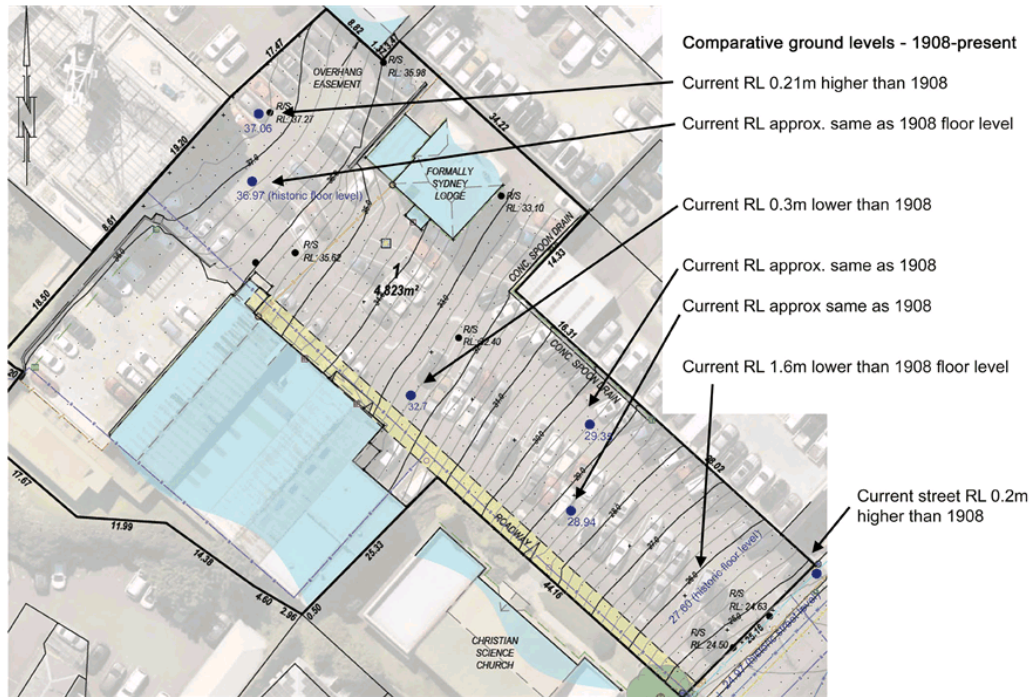


Figure 7.3 – Comparative levels, 1908 and present. Baseplan by Rogerson and Birch Surveyors.

Disturbance from construction of mid-c20th buildings

Figures 4.17-4.21 depict the clearing of the site in 1959 ahead of the 1960 construction of the teaching aids centre. These images depict the demolition of a building on the rear of the Hunt grant – of which a portion of the boundary wall remains. The images also depict that an estimated metre of depth (at the top of the site) was bulldozed to provide a level building platform which would certainly have impacted any shallow archaeological remains – i.e. the pre-1829 building on the Hunt grant and the pre-1845 building on the rear of the Foord grant. This work would also have impacted the footprints of the outbuildings surrounding Sydney Lodge which were demolished at that time.

Disturbance from service trenches

There appears to have been limited disturbance from underground services. A search of public underground asset registers via the byda.com.au system only indicates a Tas Networks LV cable running from Brisbane Street to the workshop building along the edge of the Church of Christ Scientist site. That line appears to take a similar line to the substantial trench depicted

on Figure 4.25 being the c1960 stormwater line from the Teaching Aids Centre. This line does not cross any historic building footprints as depicted on Figure 7.1, therefore would probably have had limited archaeological impact.

Note that the byda.com.au system does not depict privately owned underground assets, and it is unclear where underground services run to and from Sydney Lodge, therefore there is likely to be a range of linear disturbance across the site. The archaeological impact of such is however likely to be limited, given that services would likely be limited to areas that did not previously contain buildings (e.g. the terrace houses that were demolished in 1960) and would logically be located in traditionally open spaces and laneways which would have lower archaeological potential in any case.

7.5. Site observations

Site observations give little insight into the possible archaeological potential of the site. The site follows a gentle slope which as per Figure 7.3 is close to the historically recorded topography of the site (with the exception of the Brisbane Street frontage). The entire carpark is asphalted (with some concrete – e.g. the remnant floor of the northern bay of the Teaching Aids Centre). Note that it is possible that there were parts of the site which were historically terraced – for example the lines between buildings and in gardens on Figure 7.1 may indicate fences, or they may represent retaining walls indicating terracing.



Figure 7.4 – The gentle slope of the site which is similar to the historic topography.



Figure 7.5 – The gentle slope of the site which is similar to the historic topography.

The only obvious archaeological feature is the remnant stone wall at the rear of the site (i.e. the rear of the Hunt grant) which borders onto Ingomar (Patrick Street). This is the wall that can be seen in Figures 4.16, 4.18, 4.19, 4.22 and 4.24. The ground level adjacent to the wall is up to 1.0 metre below the base course of the wall and is retained by modern blockwork and a battered garden bed – consistent with the depictions of bulldozing in front of that wall to form a flatter terrace at the time of construction of the Teaching Aids Centre. This would have heavily impacted/destroyed any archaeological remains associated with that wall.



Figure 7.5 – Retaining wall at the base of the historic stone wall, indicating excavation in front of that wall.



Figure 7.6 – Excavation at the rear of the site (top of the Hunt grant) to form a flat terrace for the northern bay of the Teaching Aids Building in 1960 – the slab from that building now used for parking.

As part of the Environmental site analysis for the site⁴⁸, six boreholes were sampled on the Brisbane Street frontage of the site, which have given an indication of subsurface site conditions in that area as per Figure 7.7. A review of the borelogs within that report indicates that no cultural material was encountered, with 200-300mm of fill on the surface (likely to be modern carpark surface and base) and refusal from 1.2-1.8m. Whilst this does suggest that the site has been cleared to sterile ground, it must be acknowledged that this testing was not intended to seek cultural material and the cores themselves were not archaeologically inspected. It is also probable that the boreholes (being such small diameter) could miss any cultural material altogether.

⁴⁸ Geo-Environmental Solutions: Environmental Site Analysis, 65 Brisbane Street, Hobart, Tasmania. January 2025.



Figure 7.7 – Borehole locations January 2025. Geo-Environmental Solutions (p12).

7.6. Archaeological zoning plan

Given the above discussion, the following archaeological zoning plan for the site is proposed (Figure 7.7) to be read in conjunction with the following table:



Figure 7.8 – Area of high archaeological potential (red). Adapted from www.thelist.tas.gov.au

Colour	Likely remains	Likely Disturbance	Significance	Potential
Red	<ul style="list-style-type: none"> - Remains of the three pre-1845 terrace houses and possible remains of what might be an earlier (pre-1824) house. - Possible basement spaces. - Occupation debris associated with those houses. 	Disturbance more likely closer to Brisbane Street however as the site rises disturbance is likely to be less.	Significant in representing several early residences and associated material culture.	High
Orange	<ul style="list-style-type: none"> - Remains of the pre-1829 residence on the rear of the Hunt grant. - Possible basement spaces. - Occupation debris associated with that house. 	Likely widespread shallow disturbance associated with site works at the time of construction of the Visual Aids Centre.	Significant in representing a very early residence and associated material culture.	Medium
Blue	<ul style="list-style-type: none"> - Underfloor deposits associated with Sydney Lodge. - Remains of the former terrace in front of Sydney Lodge. 	Likely to be undisturbed.	Of historical interest due to association with early occupation of Sydney Lodge.	Low
Remainder of the site	<ul style="list-style-type: none"> - Remains of later minor structures (e.g. sheds, privies) and possible features such as wells, cesspits and rubbish pits. - Remains of landscape elements (e.g. paths, garden edging etc.). 	Probably ephemeral features likely to have been subject to shallow disturbance. If present, deeper features and deposits may remain.	Of historical interest in understanding the minor infrastructure of the site and associated material culture.	Low

Section 8 will include archaeological management policies further to the discussion above and the archaeological zoning plan.

7.7. The likely significance and research potential of any archaeological remains

As depicted above, the subject site has a reasonable simple built development history (albeit a complex title history), with several c1820s development sites (plus the extant Sydney lodge site) – which mostly are the only layer of development prior to their demolition. Those portions of the subject site which was subject to that c19th development were wholly residential and appears to have remained as such until the time of demolition.

Given the demolition of the buildings and formation of a carpark over any remains, any such remains would be limited to low-level structure (i.e. foundations, possible lower courses of the buildings) and any subsurface features such as basements, wells, cesspits etc. – although no such structures have been determined through historical research (i.e. no such structures are described in early accounts of the buildings), although are considered possible. There is also the possibility of artefactual remains relating to the habitation and use of the buildings as per the thematic discussion below.

The site may also yield information on site formation processes which have acted upon the site, both pre and during construction (e.g. alteration of the natural landform, construction rubble), use (e.g. occupation deposits), demolition (e.g. demolition rubble) and post-demolition use (e.g. fill and disturbance).

Much of the significance of these sites are their comparative earliness – the 1820s representing an important formative period in Hobart's post-European settlement history. The residential sites and their domestic occupation are considered to be of high archaeological potential due to their earliness and have the potential to demonstrate 19th century domestic life in the area (and wider Tasmania for that matter) in an archaeological context. These represent a small contiguous section of a Hobart city-fringe community from the 1820s onwards which can provide a valuable additional and comparative dataset to other such archaeological investigations undertaken in Hobart in the last two decades.

From a wider regional perspective, archaeological data and remains yielded from the subject site, whether coupled with other Hobart/Tasmanian data, has the potential to strengthen a comparative dataset for research into intra-colonial society through comparison with mainland (and indeed inter-colonial society on an international level).

From a temporal perspective, any remains from the investigation of such colonial communities represent a formative period of the settlement of Hobart and are likely to be of significance when considering their research potential.

Consistent with the 'Tiered research question' approach outlined in the Tasmanian Heritage Council's *Guidelines for Historical Archaeological Research on Registered Places*⁴⁹, the following questions could be investigated in the archaeological remains expected to be present within the subject site:

⁴⁹ <http://www.heritage.tas.gov.au/media/pdf/Archae%20ResGlines%20%20FINAL%20-%20June%202009.pdf>

Tier 1 Questions: These questions outline the essential knowledge base needed for any site research or significance evaluations. Such questions are often empirical in nature, and straightforward answers can be sought and often identified – generally limited to a physical knowledge of that particular place. Questions relevant to the subject site may include:

- How closely did the buildings and site features conform to the historic plans?
- What construction methods were used in the buildings and other infrastructure?
- What evidence of alteration of the natural landscape and cultural interventions to the site is archaeologically determinable (e.g. filling of the site, demolition events, site formation processes etc.).
- Are the distinct use/development phases of the buildings distinguishable?
- Can the layout and function of the buildings, and indeed individual rooms or yard spaces be ascertained?
- How thoroughly were the buildings demolished?

Answers to these questions provide a foundation of information about the structure, type, use and duration of site occupation which enables the researcher to consider a second tier of questions.

Tier 2 Questions: Conclusions that can be drawn about a site that connect the material remains found on a site to specific behavior. For instance, do artifacts relate to the lifeways of the households that lived and/or worked on the site? For instance, do any artifacts represent class, gender, taste and health/hygiene of those living/working on the site? Particularly if artifacts can be specifically dated, and with supplementary historical research, artifact assemblages from this site may contribute knowledge and provide tangible connectedness to known residents, etc. and how they lived.

Tier 3 Questions: These questions represent the highest level of inquiry. Such questions associate the activities and behavior at individual sites with broad social, technological and cultural developments – which can be of interest on local, national or global lines of enquiry. Whilst these questions posed for a single site may not reach conclusions in the short term (as Tier 1 and 2 questions might) – the collection of data can contribute to future research by the provision of a comparable dataset. The goal of such research is to develop increasingly refined and tested understandings of human cultures within broader theoretical or comparative contexts. Lines of wider enquiry that findings from within the subject site may contribute to are:

- Do any activities archaeologically apparent on the site (e.g. drinking, food, hygiene, entertainment) provide meaningful comparisons on aspects of those themes with other contemporary residential Hobart enclaves or wider Hobart/Tasmania or for that matter Australian or international 1840s+ residential sites?
- Do the conclusions on gender, class, economic and social status of the inhabitants of the residences and associated buildings conform to historical perspectives of Colonial households?

- Are there class or status differences evident in the material culture of the inhabitants of this area (subject to further historical research) when compared to, say, other early residential enclaves or sites in contemporary rural areas and/or other cities?
- Did any changes in material culture through time in the residences coincide with wider Tasmanian or local events or technology (e.g. urbanisation/development of Hobart, railway/port upgrades, start of rubbish collection etc.)?

8. Heritage management policies

Based on the statements of significance outlined in Section 6, as well as the discussion on archaeological significance in Section 7, and in-light of the statutory heritage requirements detailed in Section 2, the following sets policies for the future management of the heritage values of the place.

Policy #	Policy	Reason for policy
General policies		
1.	The approach to managing any works on the place must be guided by the principles of the ICOMOS Australia <i>Burra Charter</i> . ⁵⁰	To ensure that the future management of the heritage values of the place is undertaken to heritage industry best-practice.
2.	Any use or development of the place must not have any unreasonable adverse heritage impact upon identified values of the place.	To acknowledge that the place has heritage significance and consequent statutory heritage requirements.
The Existing building – Sydney Lodge		
3.	If any major works or any extensions are proposed to the Sydney Lodge building (excluding the provisions of Policy 4), then a detailed conservation management plan must be formulated for the building to inform any planning/proposals.	To acknowledge the significance of the building and that the level of detailed analysis of that building was not part of the scope for the current project.
4.	The western entrance porch, rear extension and eastern fire stairs on Sydney Lodge may be removed or modified (within existing building envelopes) as desired.	Noting the clear non-significance of these elements.
5.	The minimum garden curtilage for Sydney Lodge should be as per Figure 8.1 and a landscape design is to be produced by persons with experience in heritage landscapes. Ideally this should be informed by, or interpretive of, original landscape	To restore some of the original garden curtilage of Sydney Lodge and to provide an appropriate domestic setting for the building. This should include backyard space, but not to preclude parking which may support ongoing use of the building.

⁵⁰ <http://australia.icomos.org/wp-content/uploads/The-Burra-Charter-2013-Adopted-31.10.2013.pdf>

	elements of the building, and implemented as part of any major site development.	
6.	Parking immediately adjacent to the building should not occur (with the exception of the rear extension). Parking at the rear of the building is likely to be acceptable, however a softer landscape treatment or separation from wider parking areas should be utilised.	
7.	Viewlines from Brisbane Street to Sydney Lodge are not considered critical and compliance with Policies 5 and 6 are considered sufficient to maintain its townscape qualities.	Noting that Sydney Lodge has always been on an internal lot and has never had prominence from Brisbane Street.
Historical archaeology		
8.	If excavation is proposed in areas marked in red on Figure 7.8, then these excavations are to be preceded by an archaeological impact assessment and archaeological method statement in accordance with the Tasmanian Heritage Council's Practice Note 2 (Managing Historical Archaeological Potential in the Works Process).	To ensure that significant archaeological remains are managed in accordance with industry standard.
9.	If excavation is proposed in areas marked in orange on Figure 7.8, then these excavations are to be monitored by an archaeologist during the works and if any significant remains are encountered then these are to be managed in accordance with the Tasmanian Heritage Council's Practice Note 2 (Managing Historical Archaeological Potential in the Works Process).	
10.	No archaeological input is required for excavations elsewhere on the site, however works crews are to be briefed on the possibility of unanticipated	

	discoveries and in the event any such remains are found these are to be assessed by an archaeologist and managed as per the provisions of Policy 9.	
New buildings, streetscape and townscape development		
11.	The height of any new development should not exceed the ridge height of Sydney Lodge.	To maintain some subservience of any new development in comparison to Sydney lodge in the wider townscape context. This would allow for higher development on the front of the site (owing to slope) but further limit height on the rear of the site that could potentially dominate Sydney Lodge in the wider townscape.
12.	It is not considered necessary for any new buildings on the site to emulate any of the heritage character of the site nor surrounds (e.g. in materiality, colour, form, fenestration etc.). It is considered sufficient that general planning provisions can guide suitable development without the need for specific heritage input.	To acknowledge the diversity of built forms, styles, periods and detailing of the surrounding area and that emulation of Sydney Lodge in particular is not warranted.
Subdivision		
13.	The site may be subdivided as desired without any adverse heritage impact (noting Policies 5 and 6 in relation to the curtilage of Sydney Lodge).	Acknowledging that the history of the subdivision and consolidation of the site is complex and that the current title configuration is not considered to be significant.
14.	Sydney Lodge need not retain its own individual street access – it may be accessed via a shared access/right-of-way.	Noting that Sydney Lodge has always been on an internal lot with shared access – which is a common townscape feature in the area.

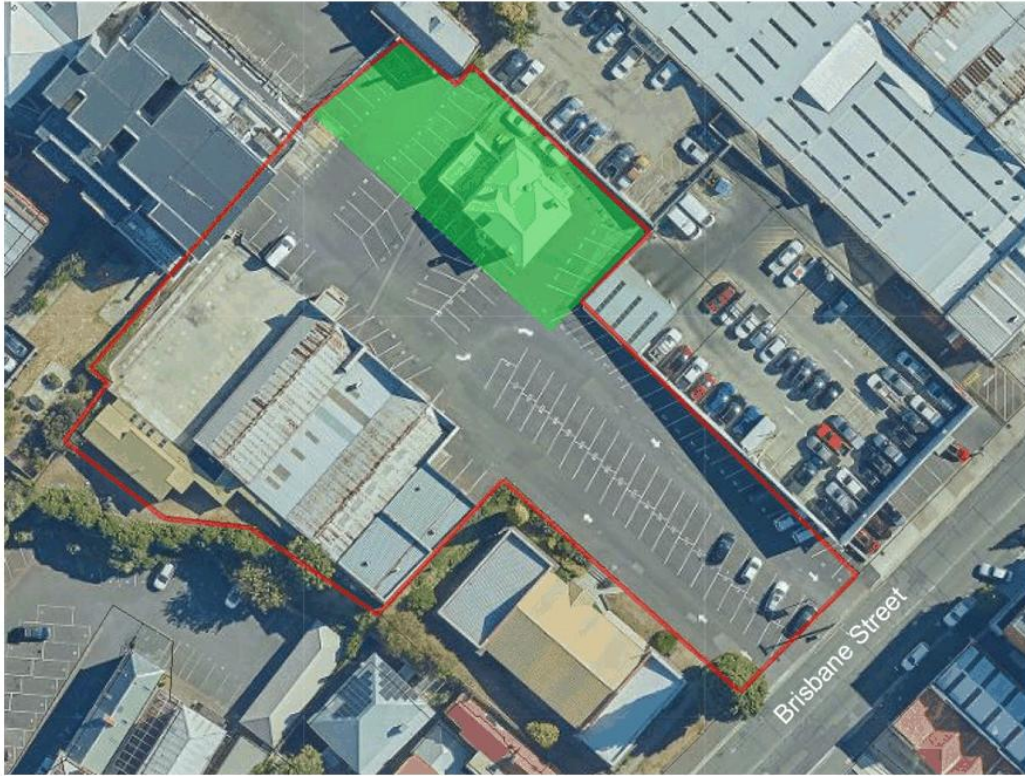


Figure 8.1 – Suggested minimum curtilage for Sydney Lodge, pursuant to Policy 6.

9. Proposed development and heritage impact assessment

As per the methodology detailed in Section 1.3, and further to the brief for this project as per Section 1.1, the following heritage impact assessment takes the conservation policies from this document as well as the statutory heritage provisions which apply to the place and applies these to a proposed development for the place.

Again, it is emphasised that the conservation policies were formulated in the absence of any foreshadowed development of the site – and have aimed to take the general characteristics of the site and surrounds and to develop conservation policies which the design process is to respond to in order to align to the relevant statutory heritage provisions. The conservation policies have been used to inform the design process and the heritage impact assessment has measured the performance of the design process in meeting the heritage objectives.

9.1. The proposed development

A proposal has been developed by Lara (architects) in conjunction with Aldanmark (Engineers) for a mixed-use development on the Brisbane Street frontage of the site. The documents relied upon for the heritage impact assessment are the set by Lara Architects dated 7/10/2025, Development Application issue A1004, A2200, A2201 – A2206, A3001 – A3006, Aldanmark Consulting Engineers proposed site plan (Drawing C102, Project 24E99-92 and elevations and pointcloud modelling associated with those drawings. Refer to the architectural documentation and planning report for full details of the proposal, which will (in brief) include:

- A six-level mixed-use building on the Brisbane Street frontage of the site, extending rearward to the point where the site widens to include Sydney Lodge, with the existing laneway to Sydney Lodge (and the workshops) retained. This will include:
 - o Ground floor retail and parking.
 - o First floor office space.
 - o Second floor office space and parking.
 - o Three floors of residential apartments.
 - The proposed new building will be approximately half a storey higher than the peak roof height of Sydney Lodge and substantially lower than the neighbouring Ingomar apartments.
- No works are proposed to the existing workshops building.
- No works are proposed to Sydney Lodge.
- The proposal includes the reinstatement of a soft landscape setting for Sydney Lodge and rationalisation of nearby parking.
- Some rearrangement of existing parking across the site.

9.2. Heritage & archaeological impact assessment and statement of compliance

This heritage impact assessment will assess the possible impact of the works outlined in Section 9.1 against the specific policies outlined in Section 8, aimed at assisting the statutory assessment and determination of the proposal as required by the relevant heritage listings detailed in Section 2 (via the assessment against the performance criteria of the planning scheme). This will include a general heritage impact assessment as well as an archaeological impact assessment.

It must be noted that the site already has an approved development for a similar 5-level building (PLN-HOB-2025-0147). The current proposal adds another level, as well as additional infrastructure at rear (namely an electrical substation and 2 accessible parking spaces). The current proposal has been assessed independently of that approval, however the precedent of that approval has been considered in this assessment in a 'test' of the capability of site development.

9.2.1 – Impact upon historic built fabric

The proposal does not include any works to Sydney Lodge, therefore there will be no impact upon historic built fabric. Note that Heritage Management Police 3 requires that if in future any major works are proposed to that building then a detailed conservation management plan must be formulated for the building to inform any planning/proposals

9.2.2. Impact upon the setting of Sydney Lodge

The proposal includes the intent to provide for a garden setting to the sides and part of the rear and front of Sydney Lodge, as a means of reinstating a more 'domestic' landscape to the building, reminiscent of its traditional front garden which is now largely lost to development off the subject site. Currently, Sydney Lodge is completely surrounded by asphalt car parking areas, therefore the intent of reinstating some green space around the building is considered to be a substantial heritage benefit. This however does not include all of the space proposed in Policy 5 (depicted on Figure 8.1).

The rear area of Sydney Lodge is to remain as parking (10 spaces) – which is considered necessary to assist in the ongoing adaptive reuse of that building – which is considered to be a reasonable expectation. This is not considered to be an adverse heritage impact and there is a proposed landscape buffer to delineate that area and to provide a sense of 'backyard' space. It is recommended that the surface treatment of that parking area be differentiated from the wider site to further promote a sense of independence and separation of that area from the wider parking area (e.g. by a narrow hedge at least) - noting that the side curtilage is slightly larger than recommended). Overall the proposal vastly improves the immediate setting of Sydney Lodge.

A substation and equal access parking is proposed within the recommended front curtilage of Sydney Lodge. This is not considered ideal, however a range of site constraints necessitates this placement. Refer to the options

discussion in the associated documentation for consideration of prudent/feasible alternatives and why this is considered necessary. This may be acceptable provided that a landscape buffer surrounds each of those elements (noting that there may be limitations on landscaping around the substation).

It is recommended that if the development is approved that a condition of approval be that a detailed landscape plan is formulated by persons with experience in heritage landscapes, in order to provide for a contextually appropriate garden setting for the building, as well as screening the substation as much as allowable and providing a soft edge to the front and rear parking spaces (particularly delineating the rear area as largely separate to the wider parking area) and that this be implemented to the satisfaction of permit authorities. A condition (HER18) to achieve such was applied to the previous approval.

9.2.3. Impact upon the streetscape/townscape qualities of Sydney Lodge

As detailed in Section 4, Sydney Lodge has always been situated on an internal lot, with precedented development on the Brisbane Street frontage which pre-dates that building. The proposed building will result in an infill of that area which has been vacant since 1960 and assists in replacing lost urban form in providing a larger building close to the Brisbane Street frontage. Whilst this will result in the loss of some (non-prominent) views of Sydney Lodge, the building will remain in its original context of an internal allotment with the benefit of the development providing for an improved landscape setting to the building (as discussed above).

The proposed building will have an overall height higher than the ridge of Sydney Lodge, which is at odds with Policy 11. This is not an ideal outcome, however that height is only 2.87m higher, which is considered acceptable as it is not a vast differentiation in height and will not largely impede any significant viewlines than the already approved development.

9.2.4. Compatibility of the style, bulk and form of the proposed building to Sydney Lodge.

As discussed above, Sydney Lodge has traditionally been an entity separate to the site of the proposed new building, therefore the relationship of these buildings should remain as 'neighbours' rather than the proposed building attempting to take design cues from Sydney Lodge. To attempt to emulate any aspect of the design of the heritage building in the new building would result in a misguided attempt to 'fit' with that building. Heritage Management Policy 12 concludes that it is not considered necessary for any new buildings on the site to emulate any of the heritage character of the site nor surrounds (e.g. in materiality, colour, form, fenestration etc.). It is considered sufficient that general planning provisions can guide suitable development without the need for specific heritage input.

The green elements of the western elevation of the proposed building provides for an improved approach to Sydney Lodge (which will also be along the traditional access pathway) which will assist (in combination with the improved landscape setting of Sydney Lodge) in reinstating a gardenesque feel to the building.

9.2.5 - Archaeological impact assessment

As per the documentation, the entire footprint of the proposed development will require excavation. Although relatively shallow at the Brisbane Street frontage, this will deepen towards the central portion of the development, but be shallower towards the rear (owing to the ground floor occupying only a portion of the footprint (cut into the slope). For the purpose of this assessment, it will be assumed that at least shallow excavation is required across the entire footprint of the proposed building.



Figure 9.1 – Area of proposed development (shaded green) in relation to the areas of archaeological potential as per Figure 7.8. Adapted from www.thelist.tas.gov.au

Figure 9.1 demonstrates that the proposed development footprint will impact upon practically all of the area designated a high archaeological potential – i.e. the footprint of the three pre-1845 terrace houses (and the potentially earlier 1820s building on that site).

As detailed in Section 2, Clause E13.10.1 (c) the HIPS15 requires consideration of *strategies to avoid, minimise and/or control impacts arising from building, works and demolition*; and E.13.10.1(e) requires consideration of *measures proposed to preserve significant archaeological evidence 'in situ'*.

The following points are made in terms of the potential impact of the proposed development upon the archaeological resource:

- Section 7 of the current document has provided a statement of historical archaeological potential, which has indicated that there are likely to be significant archaeological remains within the development footprint – albeit it is not known how thoroughly these have been previously disturbed.
- The feasibility of the proposed development requires a building of the footprint proposed. If that footprint were reduced (e.g. to avoid the area of high archaeological potential) a higher building would be required for that feasibility, which is likely to have greater impact upon Sydney Lodge. Similarly, if the proposed development footprint were moved rearward on the site (i.e. to avoid the area of high archaeological potential) then this would certainly have an adverse impact upon the setting and curtilage of Sydney Lodge.
- Whilst if archaeological remains are present, and although these are considered significant in the research potential they could yield (see Section 7.7), the level of significance is not considered to warrant in-situ retention provided that a thorough archaeological program is implemented to yield that archaeological potential.
- Archaeological potential of such sites is rarely yielded in the absence of development, which provides the cost feasibility of implementing an archaeological program as mitigation for impact.
- Subject to archaeological findings, the proponent of the development gives the undertaking that an interpretation plan which will provide meaningful communication of the archaeological results will form part of the development. It is expected that a condition of any approval will require an interpretation plan to be submitted to permit authorities and implemented ahead of practical completion.

The above points demonstrate that there is no prudent or feasible alternative to the likely impacts on the archaeological resource arising to the development – and in order to address Performance Criterion E.13.10.1(d) of the HIPS15) an archaeological method statement is necessary to realise archaeological potential. Accordingly section 9.3 will propose an archaeological method statement which the proponent commits to if the development is approved.

9.3. Archaeological method statement

Test excavation

Section 7.4 has considered the possibility of prior disturbance of the site and concludes that it is possible that the area designated as high archaeological potential (i.e. red) on Figure 7.8 *may* have been subject to extensive prior disturbance, however this is not conclusive and is thought to be less likely further rearward on the site. Ahead of the works program, it is proposed to excavate a test trench 1.0m wide and approximately 20 metres

long through the centre of the area of high archaeological potential in order to gain a better understanding of subsurface conditions and to attempt to confirm the presence of structural material and/or artifact deposits – or whether prior disturbance has resulted in all of such being removed. The approximate location of the test trench is depicted on Figure 9.2:



Figure 9.2 – Proposed test excavation (purple) in relation to the 1829 depiction of the building footprint (green) and 1845 depiction (yellow). Base image from www.thelist.tas.gov.au.

In the event that the test excavation reveals that prior disturbance has been extensive/total, then the area may be re-designated to a lower archaeological potential and managed according to the relevant policy for that level of potential. If however the test excavation reveals that there is in-situ structure and/or deposits, then the area of high archaeological potential will be further investigated as per the archaeological method statement below as part of an early works program for the development.

Excavation methodology

Bitumen and non-significant overburden will be removed via mechanical excavator under archaeological supervision to just above the expected depth of any structure as determined by the test excavation. The area will be gridded at the discretion of the archaeologist and excavation will continue by hand (as per methodology below), to expose the remains in order to gain further understanding of their nature, and to thoroughly record them (as per methodology below). Mechanical excavation in those areas will only continue if the archaeologist is satisfied that this can occur without detriment, that required outcomes can be achieved and that excavation by hand is not necessary.

The general approach to excavation will be by gridding the area in units which are responsive to the nature of the remains (e.g. in horizontal control units no greater than 1000x1000mm, or the width of the linear trench, in areas where remains appear to be complex or concentrated, or in larger control units where remains are not as complex or concentrated) and removal of each contextual unit or spit (in depths as deemed appropriate by the archaeologist, according to the nature of the strata and/or remains). Apart from non-significant overburden, all spoil will be sieved through mesh of a gauge no greater than 12mm and any significant artifacts managed as per below.

It is expected that the stratigraphic sequence will be relatively simple, (in reverse order of deposition) that of post-demolition, demolition, occupation, construction and pre-construction/natural (however noting that it is possible that there have been two early building events on this part of the site). Excavation of remains within the defined contexts in reverse order of deposition will occur and each unit/context thoroughly recorded (as per below) prior to removal to facilitate the development. It will however be considered whether in-situ structure might remain for interpretation, however the feasibility of such is not known at this stage.

It is proposed that all depositional strata be removed initially, as per above, with the aim of exposing and retaining any/all structural remains in-situ for holistic recording, prior to their removal ahead of the works excavation program. Any salvageable building materials will be retained for use elsewhere at the discretion of the site owner (possibly in interpretive installations or contemporary recycled features).

Cessation of archaeological input

Archaeological input will cease only when the archaeologist is satisfied that all significant remains have been investigated and thoroughly recorded, as per this method statement and any conditions of statutory approvals, or if sterile ground is encountered, and that adequate consultation has been undertaken with Hobart City Council's Heritage Officer and Heritage Tasmania to verify that all on-site archaeological requirements have been met (and archaeological conditions satisfied).

Recording

Any structure or significant cultural deposit encountered will be thoroughly recorded (both photographically and sketched at a scale of no smaller than 1:20 and plotted on the site plan at a scale of a scale no smaller than 1:200). The first preference will be to keep structural remains in-situ (and covered in geo-fabric, unless removal is necessary to further investigate lower strata (which may bear archaeological remains), or if there is no prudent and feasible alternative to removal to allow the development to proceed – in which case remains will be removed after thorough recording. A pointcloud survey of structure will be undertaken, as well as drone photography.

Artifacts

Given the history of the site as provided in Section 4, it is likely that any artifacts encountered will be domestic in nature. that any significant artifacts are found during excavations, these will be retained and have the required in-field conservation treatments and packaging undertaken. Artifacts will be bagged and tagged with spatial identification and removed from the site (to a secure location) daily. Trench-notes will further detail the context and initial interpretation of artifacts.

Basic post-field curation of artifacts will be undertaken. Glass and ceramic items will be washed, whilst any organics or metals will be dry-brushed. Artifacts will be packaged in acid-free archive bags, tagged with appropriate tags, and boxed in archival quality boxes (with appropriate padding if required). Should any urgent conservation treatment be required, a professional Conservator will be consulted at the earliest possible instance. A catalogue of artifacts will be included in the final report on works.

After any required analysis, these will be archived (with a copy of relevant reports) on-site of the new development (upon completion) – however at the owner’s discretion and with the approval of Hobart City Council’s Heritage Officer, alternative arrangements for storage and longer-term curation/display may be made with an appropriate repository.

Reporting requirements

Excavations and monitoring must be recorded to appropriate professional standards (for example Section 4.2 of the Tasmanian Heritage Council’s Practice Note 2). A final report must include (at a minimum):

- An executive summary of findings
- Details of the methodology employed
- Detailed interpretations of findings
- Relevant annotated photographs (including drone photography)
- Site plans at a scale of no less than 1:200
- Trench plans at a scale of no less than 1:50
- Feature plans/sketches at a scale of no less than 1:20
- Photograph log

A copy of the final report, and project archive, will be deposited with Hobart City Council (and other repositories as listed below) within 6 months of completion of the excavations and ahead of practical completion of the building.

Public benefit

Subject to the exact nature and findings of the archaeological program, the following public benefit program will be implemented during and following the works:

- The project report will be made publicly available, through appropriate repositories such as Hobart City Council, Heritage Tasmania, the State Library of Tasmania and the National Library of Australia (Trove).
- If archaeological results warrant, an academic publication may be produced (not at the proponent's expense). In any case, archaeological results will be made freely available for future archaeological research.

It is not considered feasible to have any on-site public benefit events during the works program.

Aboriginal heritage

This document deals primarily with the management of historic cultural heritage and has only briefly considered in-situ Aboriginal cultural heritage insofar as a search of Aboriginal Heritage Tasmania's register was undertaken, which has confirmed that no known Aboriginal heritage remains are within the subject site and that there is a low risk of such. There is the possibility of encountering Aboriginal heritage in a secondary context (e.g. fill). Archaeological monitoring should be mindful of this possibility, and follow the Tasmanian Government's *Unanticipated Discovery Plan – Procedure for the management of unanticipated discoveries of Aboriginal relics in Tasmania*

Site contamination

It is the responsibility of the proponent of the development to investigate the possibility of site contaminants, and to either verify that no site contaminants are present, or to take required measures to deal with any known or likely contaminants during excavation works (noting that any necessary decontamination works may require archaeological input).

9.4. Assessment against conservation policy:

As per the methodology outlined in Section 3, this document has undertaken an analysis of the form, fabric and spaces of the place in order to refine a statement of historic heritage significance and to formulate conservation policies which align to the statutory heritage requirements in order to maintain that significance in the event of any future development on the site. The proposed development as detailed in the documentation cited above will be assessed in-line with that conservation policy in order to determine the compliance of the proposal with that policy. Where any disparity in compliance occurs, commentary will be made below from the architectural statement whereby a range of other consideration (i.e. not necessarily limited to heritage) will be detailed in order to resolve any non-compliance and to attempt to justify any heritage impacts. Following that discussion against the conservation policies, the proposal will be assessed against the specific statutory heritage requirements of Section 2.

Policy #	Policy	Comment on proposal
General policies		
1.	The approach to managing any works on the place must be guided by the principles of the ICOMOS Australia <i>Burra Charter</i> . ⁵¹	As per the methodology detailed in Section 3, the current document has been formulated in accordance with the principles of the ICOMOS Australia <i>Burra Charter</i> and has been utilised to guide the proposal.
2.	Any use or development of the place must not have any unreasonable adverse heritage impact upon identified values of the place.	The current document provides a heritage management strategy to ensure that the heritage values of the site are not unreasonably impacted by the proposal and that where necessary mitigation strategies are employed to manage any impact.
The Existing building – Sydney Lodge		
3.	If any major works or any extensions are proposed to the Sydney Lodge building (excluding the provisions of Policy 4), then a detailed conservation management plan must be formulated for the building to inform any planning/proposals.	No works are proposed to Sydney Lodge. The proponent gives the undertaking that if any major works are proposed in the future that a conservation management plan will guide the planning of such.

⁵¹ <http://australia.icomos.org/wp-content/uploads/The-Burra-Charter-2013-Adopted-31.10.2013.pdf>

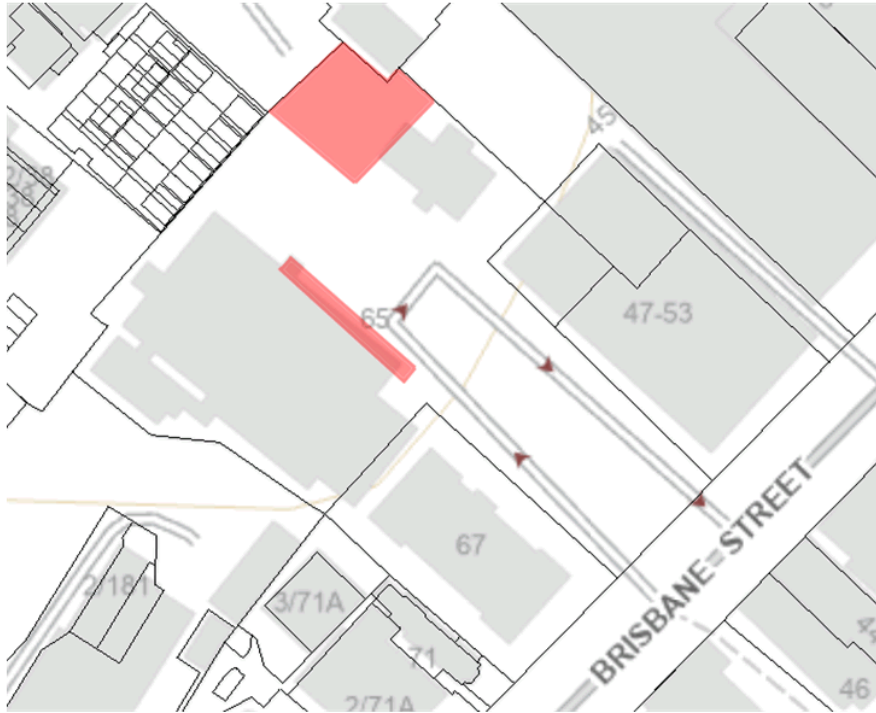
4.	The western entrance porch, rear extension and eastern fire stairs on Sydney Lodge may be removed or modified (within existing building envelopes) as desired.	No works are proposed to Sydney Lodge.
5.	The minimum garden curtilage for Sydney Lodge should be as per Figure 8.1 and a landscape design is to be produced by persons with experience in heritage landscapes. Ideally this should be informed by, or interpretive of, original landscape elements of the building, and implemented as part of any major site development.	<p>The proposal provides for a garden setting for Sydney Lodge in accordance with this policy, however the accessible parking and substation in front of the building is not in accordance with this policy. Refer to the options discussion in the associated documentation for consideration of prudent/feasible alternatives and why this is considered necessary.</p> <p>It is recommended that if the development is approved that a condition of approval be that a detailed landscape plan is formulated by persons with experience in heritage landscapes, in order to provide for a contextually appropriate garden setting for the building, and that this be implemented to the satisfaction of permit authorities.</p>
6.	Parking immediately adjacent to the building should not occur (with the exception of the rear extension). Parking at the rear of the building is likely to be acceptable, however a softer landscape treatment or separation from wider parking areas should be utilised.	<p>The proposal includes removal of parking immediately adjacent to Sydney Lodge as per this policy.</p> <p>The landscape plan as per (6) is to include a suitable surface treatment and landscaping to achieve compliance with this policy.</p>
7.	Viewlines from Brisbane Street to Sydney Lodge are not considered critical and compliance with Policies 5 and 6 are considered sufficient to maintain its townscape qualities.	The proposal complies with policies 5 and 6 and therefore complies with this policy.

Historical archaeology		
8.	If excavation is proposed in areas marked in red on Figure 8.2, then these excavations are to be preceded by an archaeological impact assessment and archaeological method statement in accordance with the Tasmanian Heritage Council's Practice Note 2 (Managing Historical Archaeological Potential in the Works Process).	The archaeological impact assessment and archaeological method statement proposed in Section 9 complies with these policies.
9.	If excavation is proposed in areas marked in orange on Figure 8.2, then these excavations are to be monitored by an archaeologist during the works and if any significant remains are encountered then these are to be managed in accordance with the Tasmanian Heritage Council's Practice Note 2 (Managing Historical Archaeological Potential in the Works Process).	
10.	No archaeological input is required for excavations elsewhere on the site, however works crews are to be briefed on the possibility of unanticipated discoveries and in the event any such remains are found these are to be assessed by an archaeologist and managed as per the provisions of Policy 9.	
New buildings, streetscape and townscape development		
11.	The height of any new development should not exceed the ridge height of Sydney Lodge.	The proposed new building has a maximum height approximately equal to the eave height of Sydney Lodge, therefore complies with this policy.
12.	It is not considered necessary for any new buildings on the site to emulate any of the	As per the discussion in Section 9.2.4 the proposal is consistent with this policy.

	heritage character of the site nor surrounds (e.g. in materiality, colour, form, fenestration etc.). It is considered sufficient that general planning provisions can guide suitable development without the need for specific heritage input.	
Subdivision		
13.	The site may be subdivided as desired without any adverse heritage impact (noting Policies 5 and 6 in relation to the curtilage of Sydney Lodge).	The proposal is consistent with this policy.
14.	Sydney Lodge need not retain its own individual street access – it may be accessed via a shared access/right-of-way.	The proposal is consistent with this policy

9.5. Assessment against Clause C6.6 Performance Criteria – Local Heritage Places

Note that as detailed in Section 2, only the following areas of the site (shaded red) are affected by Clause C6.6:



C6.6 – Development Standards for Local Heritage Places

C6.6.1 – Demolition

Objective	
To ensure that the demolition or removal of buildings do not cause an unacceptable impact on the local historic heritage significance of local heritage places.	
Performance Criteria (No Acceptable Solutions)	Response
<p>P1 Demolition or removal of buildings on a local heritage place must not cause an unacceptable impact on the local historic heritage significance of the place, having regard to:</p> <ul style="list-style-type: none"> a) the physical condition of the local heritage place; b) the extent and rate of deterioration of the building or structure; c) the safety of the building or structure; d) the streetscape or setting in which the building or structure is located; e) the historic heritage values of the local heritage place as identified in the relevant Local Provisions Schedule, or if there are no historic heritage values identified in the relevant Local Provisions Schedule, the historic heritage values as identified in a report prepared by a suitably qualified person; f) any options to reduce or mitigate deterioration; g) whether demolition is the most reasonable option to secure the long-term future of a building or structure; and h) any economic considerations. 	<p>Apart from the resurfacing of the carpark at the rear of Sydney Lodge (in accordance with a detailed landscape plan which seeks to add a ‘domestic’ feel to the area, no demolition is proposed within the areas affected by Table C6.1.</p>

C6.6.2 – Site coverage

Objective

That site coverage is compatible with the local historic heritage significance of local heritage places.	
Performance Criteria (No Acceptable Solutions)	Response
<p>P1</p> <p>The site coverage must be compatible with the local historic heritage significance of a local heritage place, having regard to:</p> <ul style="list-style-type: none"> a) the topography of the site; and b) the historic heritage values of the local heritage place as identified in the relevant Local Provisions Schedule, or if there are no historic heritage values identified in the relevant Local Provisions Schedule, the historic heritage values as identified in a report prepared by a suitably qualified person. 	<p>The proposal does not change the site coverage of the areas affected by Table C6.1.</p>

C6.6.3 – Height and Bulk of Buildings

Objective	
That the height and bulk of buildings are compatible with the local historic heritage significance of local heritage places.	
Performance Criteria	Response
<p>P1</p> <p>The height and bulk of buildings must be compatible with the local historic heritage significance of a local heritage place, having regard to:</p> <ul style="list-style-type: none"> (a) the historic heritage values of the local heritage place as identified in the relevant Local Provisions Schedule, or if there are no historic heritage values identified in the relevant Local Provisions Schedule, the historic heritage values as identified in a report prepared by a suitably qualified person; (b) the character and appearance of the existing building or place; (c) the height and bulk of other buildings in the surrounding area; and (d) the setting of the local heritage place. 	<p>No buildings are proposed within the areas affected by Table C6.1.</p>

C6.6.4 – Siting of buildings and structures

Objective	
That the siting of buildings is compatible with the local historic heritage significance of local heritage places.	
Performance Criteria (No Acceptable Solutions)	Response
<p>P1</p> <p>The front, side and rear setbacks of a building must be compatible with the local historic heritage significance of the place, having regard to:</p> <ul style="list-style-type: none"> (a) the historic heritage values of the local heritage place as identified in the relevant Local Provisions Schedule, or if there are no historic heritage values identified in the relevant Local Provisions Schedule, the historic heritage values as identified in a report prepared by a suitably qualified person; (b) the topography of the site; (b) the size, shape, and orientation of the lot; and (c) the setbacks of other buildings in the surrounding area. 	No buildings are proposed within the areas affected by Table C6.1.

C6.6.5 – Fences

Objective	
That fences are compatible with the local historic heritage significance of local heritage places.	
Acceptable Solutions/ Performance Criteria	Response
<p>A1</p> <p>New fences and gates on local heritage places must be designed and constructed to match existing original fences on the site.</p>	No fencing is proposed within the areas affected by Table C6.1.
<p>P1</p> <p>New fences and gates must be compatible with the local historic heritage significance of a local heritage place, having regard to:</p>	

<p>(a) the historic heritage values of the local heritage place as identified in the relevant Local Provisions Schedule, or if there are no historic heritage values identified in the relevant Local Provisions Schedule, the historic heritage values as identified in a report prepared by a suitably qualified person;</p> <p>(b) the architectural style of the buildings on the site;</p> <p>(c) the dominant fencing style in the setting;</p> <p>(d) the original or previous fences on the site; and</p> <p>(e) the proposed height and location of the fence.</p>	
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C6.6.6 – Roof form and materials

Objective	
That roof form and materials are compatible with the local historic heritage significance of local heritage places.	
Acceptable Solutions/Performance Criteria	Response
<p>A1 Replacement roofs on local heritage places which will be visible from any road or public open space adjoining the site, must be of a form and material to match the existing roof being replaced.</p> <p>P1 Roof form and materials must be compatible with the local historic heritage significance of a local heritage place, having regard to:</p> <p>(a) the historic heritage values of the local heritage place as identified in the relevant Local Provisions Schedule, or if there are no historic heritage values identified in the relevant Local Provisions Schedule, the historic heritage values as identified in a report prepared by a suitably qualified person;</p> <p>(b) the design, period of construction and materials of the building on the site that the roof directly relates to;</p> <p>(c) the dominant roofing style and materials in the setting; and</p> <p>(d) the streetscape.</p>	<p>No buildings are proposed within the areas affected by Table C6.1.</p>

C6.6.7 Building alterations, excluding roof form and materials

Objective	
That building alterations, excluding roof form and materials, are compatible with the local historic heritage significance of local heritage places.	
Performance Criteria (No Acceptable Solutions)	Response
<p>P1</p> <p>Building alterations, excluding roof form and materials, of an existing building that is a local heritage place must be compatible with and not detract from the local historic heritage significance of the place, having regard to:</p> <ul style="list-style-type: none"> (a) the historic heritage values of the local heritage place as identified in the relevant Local Provisions Schedule, or if there are no historic heritage values identified in the relevant Local Provisions Schedule, the historic heritage values as identified in a report prepared by a suitably qualified person; (b) the design, period of construction and materials of the building on the site that the building alterations most directly relate to; (c) the dominant external building materials in the setting; and (d) the streetscape. 	<p>No buildings are located within the areas affected by Table C6.1.</p>

C6.6.8 Outbuildings and structures

Objective	
That the siting of outbuildings and structures are compatible with the local historic heritage significance of local heritage places.	
Acceptable Solutions/Performance Criteria	Response
<p>A1</p> <p>Outbuildings and structures on local heritage places must:</p> <ul style="list-style-type: none"> (a) not be located in the front setback; (b) not be visible from any road or public open space adjoining the site; 	<p>No outbuildings are proposed within the areas affected by Table C6.1.</p>

<p>(c) not have a side that is longer than 3m; (d) have a gross floor area less than 9m²; (e) have a combined total area of all outbuildings on the site of not more than 20m²; (f) have a maximum height less than 2.4m above existing ground level; (g) not have a maximum change of level as a result of cut or fill of more than 1m; and (h) not encroach on any service easement or be located within 1m of any underground service.</p> <p>P1 Outbuildings and structures must be compatible with the local historic heritage significance of a local heritage place, having regard to:</p> <p>(a) the historic heritage values of the local heritage place as identified in the relevant Local Provisions Schedule, or if there are no historic heritage values identified in the relevant Local Provisions Schedule, the historic heritage values as identified in a report prepared by a suitably qualified person; (b) the bulk, form and size of buildings on the site; (c) the bulk, form and size of the proposed outbuilding or structure; (d) the external materials, finishes and decoration of the outbuilding or structure; and (e) the visibility of the outbuilding or structure from any road or public open space adjoining the site.</p>	
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C6.6.9 Driveways and parking for non-residential purposes

Objective	
That driveways and parking for non-residential purposes are compatible with the local historic heritage significance of local heritage places.	
Acceptable Solutions/Performance Criteria	Response

<p>A1 Driveways and parking areas for non-residential purposes on local heritage places must be located behind the building line of buildings located or proposed on a site.</p> <p>P1 Driveways and parking areas for non-residential purposes must be compatible with the local historic heritage significance of a local heritage place, having regard to:</p> <ul style="list-style-type: none"> (a) the historic heritage values of the local heritage place as identified in the relevant Local Provisions Schedule, or if there are no historic heritage values identified in the relevant Local Provisions Schedule, the historic heritage values as identified in a report prepared by a suitably qualified person; (b) the loss of any building fabric; (c) the removal of gardens or vegetated areas; (d) parking availability in the surrounding area; (e) vehicle and pedestrian traffic safety; and (f) the streetscape. 	<p>It is proposed to alter the current parking arrangements in the rear area of Sydney Lodge that is affected by Clause C6.6, however this is a positive outcome as (pending a detailed landscape plan) that will improve the setting of Sydney Lodge by providing a more 'domestic' tenor.</p> <p>The proposal satisfies this Performance Criterion.</p>
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C6.6.10 Removal, destruction or lopping of trees, or removal of vegetation, that is specifically part of a local heritage place

<p>Objective</p>	
<p>That the removal, destruction or lopping of trees or the removal of vegetation that is specifically part of a local heritage place does not impact on the local historic heritage significance of the place.</p>	
<p>Performance Criteria (No Acceptable Solutions)</p>	<p>Response</p>

<p>P1 The removal, destruction or lopping of trees or the removal of vegetation which is specifically part of a local heritage place listed in the relevant Local Provisions Schedule, must not cause an unreasonable impact on the local historic heritage significance of a local heritage place, having regard to:</p> <ul style="list-style-type: none"> (a) the historic heritage values of the local heritage place as identified in the relevant Local Provisions Schedule, or if there are no historic heritage values identified in the relevant Local Provisions Schedule, the historic heritage values as identified in a report prepared by a suitably qualified person; (b) the age and condition of the tree or vegetation; (c) the size and form of the tree or vegetation; (d) the importance of the tree or vegetation to the local historic heritage significance of a local heritage place; and (e) any advice by a suitably qualified person. 	<p>Refer to the arborist report for management of the potential impact of the proposed development upon the tree on the adjacent place (67 Brisbane Street).</p>
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C6.10.1 Lot design on a Local Heritage Place

Objective	
That subdivision does not cause an unacceptable impact on the local historic heritage significance of local heritage places.	
Performance Criteria (No Acceptable Solutions)	Response
<p>P1 Subdivision must not cause an unacceptable impact on the local historic heritage significance of a local heritage place, having regard to:</p> <ul style="list-style-type: none"> (a) the local historic heritage significance of the local heritage place identified in the relevant Local Provisions Schedule; (b) the historic development pattern of the area; (c) the separation of buildings or structures from their original setting; (d) the lot sizes, dimensions, frontage, access and orientation; (e) the suitability of the proposed lots for their intended uses; and 	<p>The lot design of places affected by Table C6.1 will not be altered by the proposal.</p>

(f) the removal of vegetation, trees or garden settings.	
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C1.6.4 Signs on local heritage places and in local heritage precincts and local historic landscape precincts

Objective	
That the size, design and siting of signs is compatible with and does not have an unacceptable impact on the local historic heritage significance of a local heritage place, a local heritage precinct or a local historic landscape precinct as listed in the Local Historic Heritage Code.	
Acceptable Solution/Performance Criteria	Response
<p>A1</p> <p>A sign located on a site that is a local heritage place, in a local heritage precinct or local historic landscape precinct listed under the Local Historic Heritage Code, must:</p> <ul style="list-style-type: none"> (a) be not more than 0.2m²; (b) not be an illuminated sign; and (c) there must be not more than 1 sign per site. <p>P1</p> <p>A sign located on a site that is a local heritage place, in a local heritage precinct or local historic landscape precinct listed under the Local Historic Heritage Code must be located in a manner that does not have an unacceptable impact on the local historic heritage significance of the place or precinct, having regard to:</p> <ul style="list-style-type: none"> (a) placement to allow the architectural details of the building to remain prominent; (b) the size and design not substantially diminishing the local historic heritage significance of the place or precinct; (c) where relevant, placement in a location on the building that would traditionally have been used as an advertising area; (d) any domination or obscuring of any historic signs forming an integral part of a building’s architectural detailing or local historic heritage significance; (e) using fixtures that do not and are not likely to damage building fabric; 	<p>No signs are proposed on places affected by Table C6.1.</p>

<p>(f) not projecting above a parapet or roof line if such a projection impacts on the local historic heritage significance of the building; and</p> <p>(g) not using internal illumination in a sign on a local heritage place unless it is demonstrated that such illumination will not detract from the local historic heritage significance of the place or precinct.</p>	
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9.6. Assessment against Clause C6.8 Performance Criteria – Development Standards for Places or Precincts of Archaeological Potential

C6.8.1 Building and Works

Objective	
That building and works on a place or precinct of archaeological potential is implemented in a manner that seeks to retain or protect, preserve or otherwise appropriately manage archaeological evidence.	
Performance Criteria (No Acceptable Solutions)	Response
<p>P1</p> <p>Building and works on places or precincts of archaeological potential must not cause an unacceptable impact on archaeological evidence, having regard to:</p> <ul style="list-style-type: none"> (a) the nature of the archaeological evidence, either known or potential; (b) measures proposed to investigate the archaeological evidence to confirm statements of potential; (c) strategies to avoid, minimise or control impacts arising from building, works and demolition; (d) measures proposed to preserve significant archaeological evidence in situ; and (e) any advice contained in a statement of archaeological potential. 	<p>The current document fulfills (a) in that it provides a predictive model of archaeological potential. The proposed test excavation in Section 9.3 provides for refinement of that predictive model in accordance with (b). Section 9.2.5 has considered options for minimising impact upon archaeological remains as required by (c) via the archaeological method statement in Section 9.3 which provides for the further investigation and realising of archaeological potential. Section 9.2.5 has considered the overall archaeological potential of the site and has formulated recommendations which inform the relevant policies as required by (e).</p>

C6.10.3 Subdivision works for places or precincts of archaeological potential

Objective	
That works associated with subdivision, including infrastructure, do not increase the likelihood of adverse impact on a place or precinct of archaeological potential.	
Performance Criteria (No Acceptable Solutions)	Response
<p>P1</p> <p>Works associated with subdivision must not increase the likelihood of adverse impact on archaeological evidence on places or precincts of archaeological potential, having regard to:</p> <ul style="list-style-type: none"> (a) the nature, extent and significance of the archaeological evidence existing on the land; (b) any significant impact upon archaeological evidence or potential; (c) any increased likelihood of future development that is incompatible with a place or precinct of archaeological potential; (d) the statement of archaeological potential for the place or precinct identified in the relevant Local Provisions Schedule; and (e) any advice contained in a statement of archaeological potential. 	<p>Any proposed subdivision or strata titling of the site is unlikely to increase the likelihood of adverse impact on the place of archaeological potential, coupled with an archaeological investigation program to yield archaeological potential.</p> <p>The proposal satisfies this Performance Criterion.</p>

9.7. Assessment against the Tasmanian Heritage Council's Works Guidelines.

The following table considers the proposal against the relevant Appropriate *Outcomes* of the Tasmanian Heritage Council's Works Guidelines (Version 2, October 2025).

4. Interpretation.	
4.1. Interpretation signboards.	It is recommended that an interpretation plan for the heritage values of the site be formulated and implemented ahead of the issue of a certificate of occupancy for the proposed development. This may include interpretation of archaeological values (as per Section 9.3).
4.2. Plaques and inscriptions.	
4.3. Outlines of vanished structures.	
4.3. Audio-visual and lighting installations.	

5. Subdivision or Boundary Adjustment.	
5.1. Adhesion.	The title configuration of the site is not considered significant and any subdivision or adhesion (that maintains the curtilage of Sydney Lodge) is likely to be acceptable.
5.2. Subdivision or boundary adjustment.	

7. Excavation and Archaeological Investigation.	
7.1. Initial investigation.	Section 7 has provided a statement of historical archaeological potential for the site, which has formed the basis of relevant archaeological management policies. These are to be implemented as part of the proposed development.
7.2. Excavation and ground disturbance.	
7.3. Archaeological excavation for research.	

8. New Buildings.	
8.1. New buildings (generally).	<p>Section 9.2.2 – 9.2.4 of the current document concludes that the proposed new building will not have any unreasonable impact upon the setting and viewlines to/from Sydney Lodge and its townscape associations. Whilst a large building, it repairs a gap in the urban form – that area having formerly been the site of comparatively large historic buildings.</p> <p>Policy 12 states that there is no need for the new building to respond to Sydney Lodge stylistically as the site of the new building has traditionally not been part of the Sydney Lodge curtilage.</p> <p>The proposed new building is slightly higher (<3m) than that recommended by the policy. It is considered that this impact is negligible and is acceptable.</p>

11. New Services (including upgrades for energy efficiency).	
11.1. New services (generally).	<p>The proposed new substation is not considered to be sited in a manner which minimises adverse heritage impact upon Sydney Lodge. Refer to the associated documentation for an analysis of site constraints and prudent/feasible alternatives to justify that impact.</p>

14. Outdoor spaces, built landscapes, gardens and plants.	
14.7. Hard landscaping.	The proposal removes much of the unsympathetic hard landscaping surrounding Sydney Lodge and therefore provides a positive heritage outcome.
14.11. Buildings or works near significant trees.	Refer to the arborist report for mitigation strategies to avoid impact upon the significant tree on the adjacent land (67 Brisbane Street). Note that that tree is not on the Tasmanian Heritage Register.
14.12. Car parking and driveways.	Although the placement of accessible parking spaces forward of Sydney Lodge is not an ideal outcome, the proposal removes much of the unsympathetic parking arrangement surrounding Sydney Lodge and therefore provides an overall positive heritage outcome. The rationalisation of parking on the wider site provides an improved outcome and (subject to a detailed landscape plan) the proposal includes a more appropriate response to parking within proximity to Sydney Lodge.

As per Section 2, the site adjoins 34 Patrick Street (Ingomar) and 30 Patrick Street (Roydon) therefore technically the THC can consider impact of any proposed development upon those places. It is concluded that the proposed development has no conceivable impact upon the values of those places as detailed on each respective THR datasheet.

57-65 Brisbane Street Waste Management.

Current Site management

The Site currently consists of 5 tenancies as per the Plan below. General waste is a 120L street side bin, Recycling is a 240L street side bin

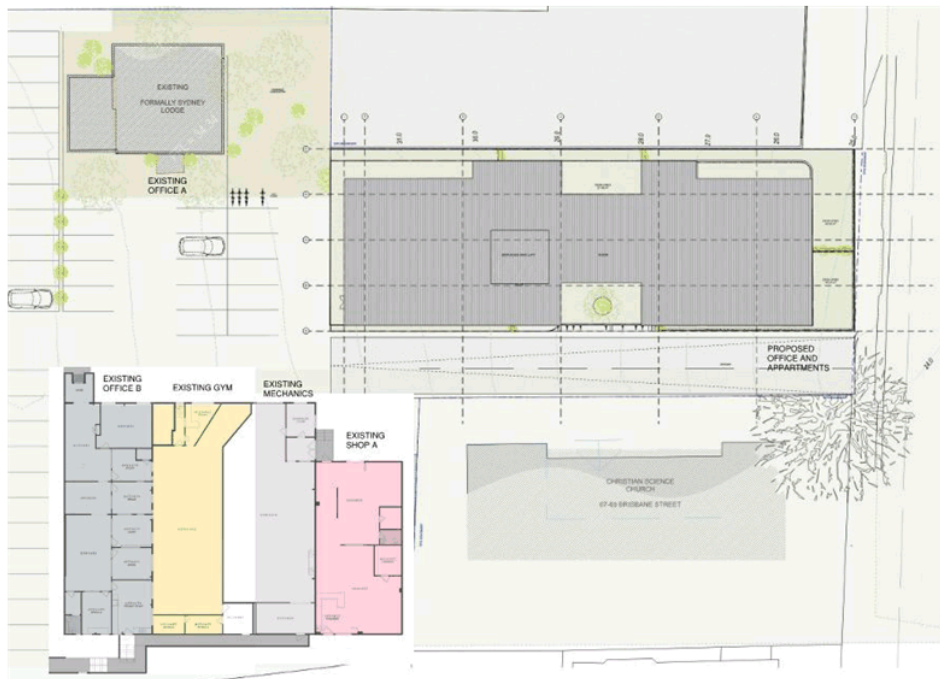
SHOP A – 1x general waste, 1x recycling

GYM – 1x general waste, 1x recycling monthly

MECHANIC – 2x general waste, 1x recycling (private collection of any heavy materials)

OFFICE A – 2x general waste, 1x recycling

OFFICE B – 2x general waste, 1x recycling



Current Waste Collection consists of Wednesday morning street side collection of general waste every week and recycling collected fortnightly. This is undertaken by HCC and is expected to remain, however this may be incorporated into a future proposed Third Party collection contract.

Proposed Development

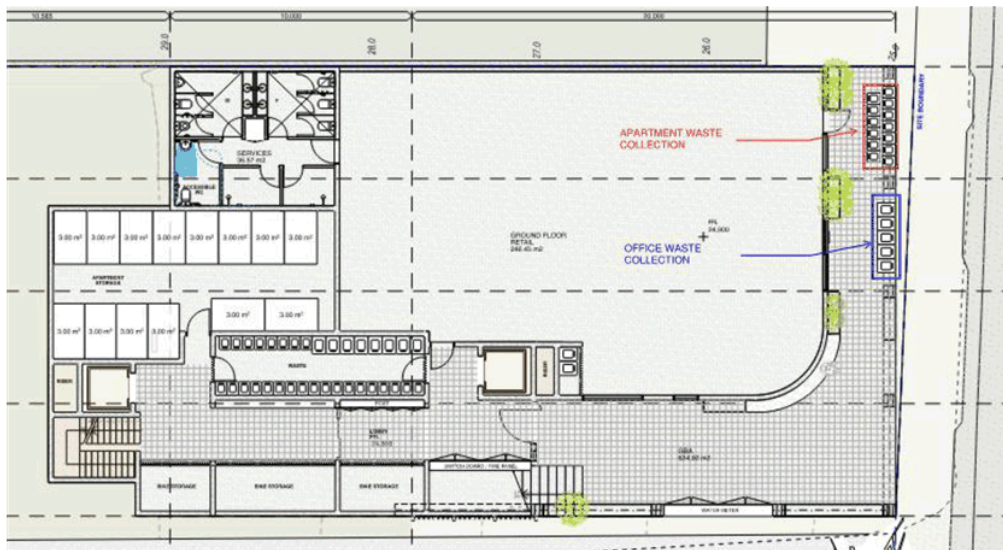
The proposed development consists of 1224m² of Offices and 16 Apartments

This is estimated to generate additional waste as indicated in the table below

Waste Source	Base Quantity	Waste m ³ /Week	Number of 120 ltr Bins	Number of 240 ltr Bins	Recycling m ³ /Week	Number of 120 ltr Bins	Number of 240 ltr Bins
Apartments	13	1.6	13.0	6.5	1.0	13.0	6.5
Offices	1224.0	1.2	10.2	5.1	1.2	10.2	5.1
	TOTAL	2.8			2.8		

Proposed Waste Collection

Waste is proposed to be collected from street pickup in the locations shown below with General waste collected on one day or the week and Recycling on an alternate day. Refer proposed collection quote by Veolia.



**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



SEARCH OF TORRENS TITLE

VOLUME 174045	FOLIO 1
EDITION 2	DATE OF ISSUE 22-Jan-2025

SEARCH DATE : 26-Mar-2025

SEARCH TIME : 05.56 PM

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Plan 174045

Derivation : Part of (0A-1R-37P) Granted to George Clarke,
Part of (0A-3R-14P) Granted to The Crown and Whole of
(0A-2R-29 6/10P) & (0A-1R-19P) Granted to The Crown
Prior CTs 46981/4, 46981/5, 150203/2, 46981/2, 46980/1,
47913/1, 46981/3 and 46981/1

SCHEDULE 1

N221826 TRANSFER to BRISBANE STREET APARTMENTS PTY LTD
Registered 22-Jan-2025 at noon

SCHEDULE 2

M632949 Land is limited in depth to 15 metres, excludes
minerals and is subject to reservations relating to
drains sewers and waterways in favour of the Crown

B469026 INSTRUMENT creating restrictive covenants burdening
the land within described pursuant to Section 19 of
the National Trust of Australia (Tasmania) Act 1975
and Section 102 of the Land Titles Act 1980
(excepting thereout Certificate of Title Volume
150203 Folio 2) Registered 19-Dec-1991 at noon

BENEFITING EASEMENT (appurtenant to the land marked ABCDE on
PLAN 174045) a full free and uninterrupted right of
way and passage for the registered proprietor his
heirs and assigns and his and their tenants and
servants and other by his or their permission in
common with all other persons entitled thereto from
time to time and at all times hereafter and for all
purposes whatsoever to go return pass and repass with
or without horses carts and other carriages laden or
unladen in through over along and upon the Roadway
marked CWZD on Plan 174045

BURDENING EASEMENT: full and free right and liberty for
National Executors and Trustees Company of Tasmania
Limited its successors transferees and assigns (as

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

- appurtenant to the land comprised in Certificate of Title Volume 354 Folio 166) and its and their tenants and servants and all persons authorised by it or them from time to time and at all times hereafter by day or by night at its and their will and pleasure jointly with any other person or persons entitled to use the same as a right of way to pass and repass with or without horses carts and other carriages of all descriptions over and along the Roadway marked WXYZ on Plan 174045
- 9/8748 CONVEYANCE - BURDENING EASEMENT: the right for Newman Arnold to use and enjoy the drain or sewer laid down by him along the strip of land marked Covered Drain on Plan 174045 as the same at present exists together with the right to repair amend cleanse and maintain the same and for this purpose to enter upon the said adjacent premises at reasonable and convenient times at in a reasonable manner
- E45541 BURDENING EASEMENT: an overhang easement (appurtenant to Lot 1 on Sealed Plan 64733) over the land marked Overhang Easement on Plan 174045 Registered 13-Sep-2017 at noon
- M632949 FENCING PROVISION in Transfer
- E45572 ADHESION ORDER under Section 110 of the Local Government (Building and Miscellaneous Provisions) Act 1993 Registered 13-Sep-2017 at noon
- E396134 MORTGAGE to Commonwealth Bank of Australia Registered 22-Jan-2025 at 12.01 PM

UNREGISTERED DEALINGS AND NOTATIONS

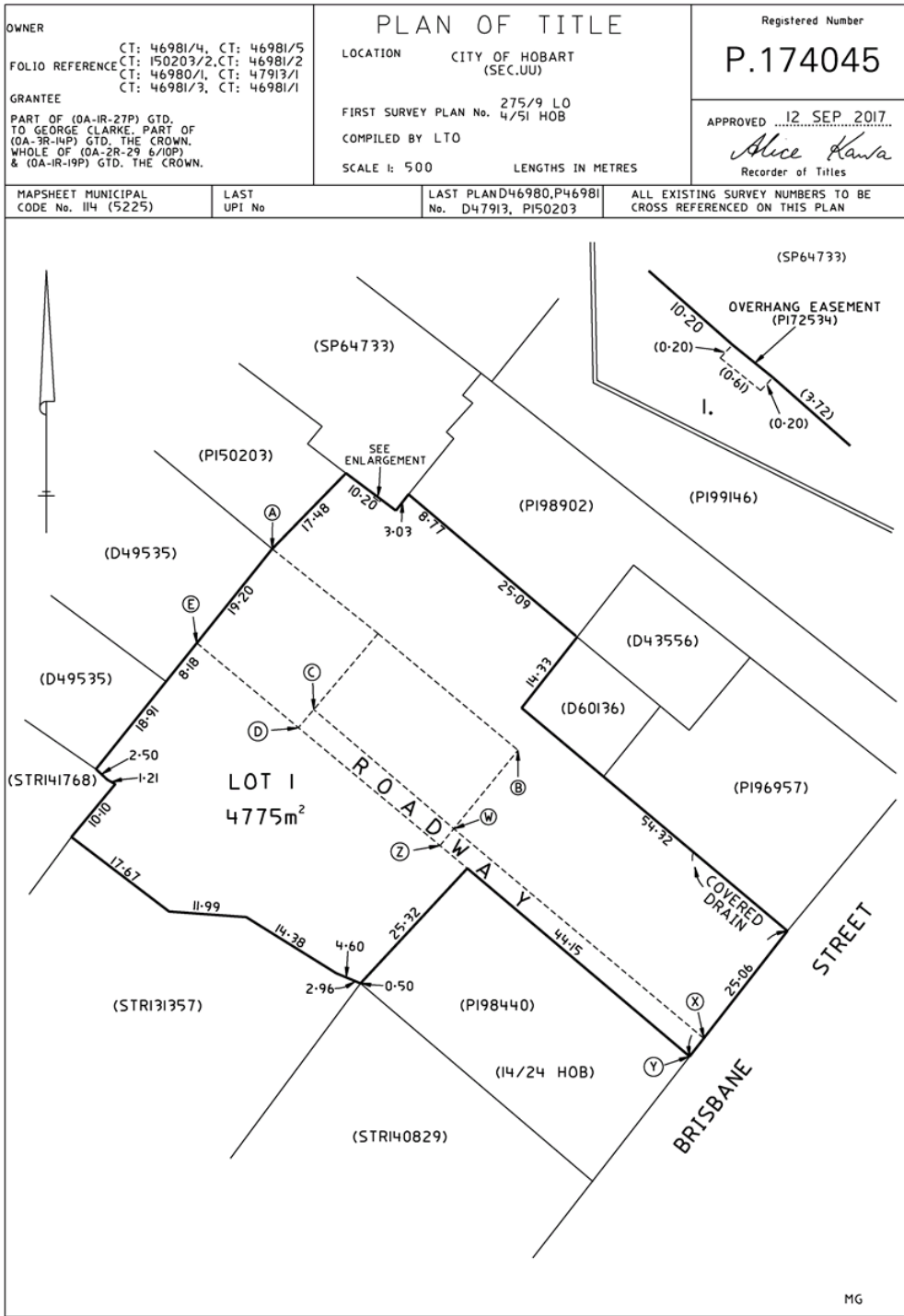
No unregistered dealings or other notations



FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



**RESULT OF URDS SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980UNREGISTERED AND RECENTLY REGISTERED DEALINGS REPORT

SEARCH DATE : 26-Mar-2025

SEARCH TIME : 05:56 pm

CT: 174045/1

<u>Lodge Date</u>	<u>Type</u>	<u>DealingNo</u>	<u>Reg Date</u>
28-Nov-2024	TRANSFER	N221826	22-Jan-2025
28-Nov-2024	MORTGAGE	E396134	22-Jan-2025

Search covers any dealings registered in the last 90 days and any dealings yet to be registered.

DEVELOPMENT ENGINEERING

ASSESSMENT SUMMARY:

Tasmanian Planning Scheme - State Planning Provisions		
CLAUSE	DESCRIPTION	ASSESSMENT DECISION
C2.0 - Parking and Sustainable Transport Code		DOES APPLY
C2.5 - Use Standards		
C2.5.1	Car parking numbers	PERFORMANCE CRITERIA
C2.5.2	Bicycle parking numbers	NOT APPLICABLE
C2.5.3	Motorcycle parking numbers (>20 car parking spaces)	NOT APPLICABLE
C2.5.4	Loading Bays (>1000m ² floor area in a single occupancy)	NOT APPLICABLE
C2.5.5	Number of car parking spaces within the General Residential Zone and Inner Residential Zone (Reuse/COU)	NOT APPLICABLE
C2.6 - Development Standards for Buildings and Works		
C2.6.1	Construction of parking areas	ACCEPTABLE SOLUTION
C2.6.2	Design and layout of parking areas	PERFORMANCE CRITERIA
C2.6.3	Number of accesses for vehicles	NOT APPLICABLE
C2.6.4	Lighting of parking areas within the General Business Zone and Central Business Zone (5 or more car parking spaces)	NOT APPLICABLE
C2.6.5	Pedestrian access (10 or more car parking spaces)	PERFORMANCE CRITERIA
C2.6.6	Loading bays (Based on C2.5.4)	NOT APPLICABLE
C2.6.7	Bicycle parking and storage facilities within the General Business Zone and Central Business Zone	NOT APPLICABLE
C2.6.8	Siting of parking and turning areas (Specific Zones)	NOT APPLICABLE
C2.7 - Parking Precinct Plan		
C2.7.1	Parking precinct plan	NOT APPLICABLE
C3.0 - Road and Railway Assets Code		
		DOES APPLY
C3.5 - Use Standards		
C3.5.1	Traffic generation at a vehicle crossing, level crossing or new junction	ACCEPTABLE SOLUTION
C3.6 - Development Standards for Buildings or Works		
C3.6.1	Habitable buildings for sensitive uses within a road or railway attenuation area	NOT APPLICABLE
C3.7 - Development Standards for Subdivision		
C3.7.1	Subdivision for sensitive uses within a road or railway attenuation area	NOT APPLICABLE
6.11 Conditions and Restrictions on a Permit		
6.11.2 (g) / C11 / C12		DO APPLY
Erosion, and stormwater volume and quality controls		
6.0 - Assessment of an Application for Use or Development		
6.11.2 (g)	Erosion, and stormwater volume and quality controls	ACCEPTABLE SOLUTION - Waterways Group Assessment
C11.0 - Coastal Inundation Hazard Code		
C11.6.1	Buildings and works, excluding coastal protection works, within a coastal inundation hazard area	NOT APPLICABLE
C11.7.1	Subdivision within a coastal inundation hazard area	NOT APPLICABLE
C12.0 - Flood-Prone Areas Hazard Code		
C12.6.1	Buildings and works within a flood-prone hazard area	NOT APPLICABLE
C12.7.1	Subdivision within a flood-prone hazard area	NOT APPLICABLE
HOB – Local Provisions Schedule		

DISCRETION SUMMARY:C2.5.1 Parking number assessment

The car parking number assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the *Tasmanian Planning Scheme - State Planning Provisions*.

Documentation submitted to date does not comply with the Acceptable Solution, therefore assessment against the Performance Criterion is relied on for clause C2.5.1.

Acceptable solution - A1: - **DOES NOT COMPLY: (d/ii).**

The proposal includes seven (7x) on-site car parking spaces for the residential / dwelling component, which is five (4x) less than required by Table C2.1.

Based on the documentation submitted to date and given the above assessment, the car parking numbers is accepted as meeting the *Performance Criteria P1.2:C2.5.1* of the Planning Scheme. This is particularly due to the actual parking demands that will be generated by the development.

C2.6.2

The design and layout of parking areas must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the *Tasmanian Planning Scheme - State Planning Provisions*.

Documentation submitted to date does not comply with the Acceptable Solution, therefore assessment against the Performance Criterion is relied on for clause C2.6.2.

Acceptable Solution - A1: - **DOES NOT COMPLY: (ai), (aia), (aiaii), (aiv),**

Parking, access ways, manoeuvring and circulation spaces must either:

(a) comply with the following:

- (i) have a gradient in accordance with [Australian Standard AS 2890- Parking facilities, Parts 1-6](#); - **NOT MET (Turning gradient > 5%)**
- (ii) have car parking space dimensions which satisfy the requirements in [Table C2.3](#); - **NOT MET (Parking spaces 2.4m in width and small car parking space)**
- (iii) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in [Table C2.3](#) where there are 3 or more car parking spaces; - **NOT MET (Parking spaces 2.4m in width)**
- (iv) have a vertical clearance of not less than 2.1m above the parking surface level; - **NOT MET (Due to vertical constraints imposed by the building structure, the parking bay located at the right end of the top row will be designated for small car use only)**

Performance Criteria - P1: - **ACCEPTED AS MEETING THE PERFORMANCE CRITERIA**

All parking, access ways, manoeuvring and circulation spaces must be designed and readily identifiable to provide convenient, safe and efficient parking

Based on the submitted documentation to date and given the assessment, the design and layout of parking areas is accepted as meeting the *Performance Criteria P1:C2.6.2* of the Planning Scheme.

C2.6.5

Pedestrian access

The pedestrian access must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the *Tasmanian Planning Scheme - State Planning Provisions*.

Documentation submitted to date does not comply with the Acceptable Solution, therefore assessment against the Performance Criterion is relied on for clause C2.6.5.

Acceptable Solution A1.1: - **DOES NOT COMPLY: (ai), (aia), (b)**

Uses that require 10 or more car parking spaces must:

(a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:

(i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or

(ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and

(b) be signed and line marked at points where pedestrians cross access ways or parking aisles.

Acceptable Solution A1.2: - **DOES NOT COMPLY**

In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building.

Performance Criteria - P1: - **ACCEPTED AS MEETING THE PERFORMANCE CRITERIA**

Safe and convenient pedestrian access must be provided within parking areas, having regard to:

- (a) the characteristics of the site;
- (b) the nature of the use;
- (c) the number of parking spaces;
- (d) the frequency of vehicle movements;
- (e) the needs of persons with a disability;
- (f) the location and number of footpath crossings;
- (g) vehicle and pedestrian traffic safety;
- (h) the location of any access ways or parking aisles; and
- (i) any protective devices proposed for pedestrian safety.

Based on the submitted documentation to date and given the assessment, the pedestrian access is accepted as meeting the *Performance Criteria P1: C2.6.5* of the Planning Scheme.

REFERRALS:

TasWater:	REFERRAL NOT REQUIRED					
Check Applicable (X)	Sewer Main Proximity	New Sewer Connection	New Water Connection	Additional loading X	Subdivision	Other

Waterways:	REFERRAL NOT REQUIRED
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Check Applicable (X)	New Connection	Detention or >100m ² Hardstand	Treatment X	Pump / Onsite	S/W Main Proximity X	Subdivision
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Roads: REFERRAL NOT REQUIRED						
Check Applicable (X)	New Crossover	Structures in Road Reservation	Excavation within Road Reservation	Adjusting Footpath Levels	Subdivision	Other

Traffic: REFERRAL REQUIRED						
Check Applicable (X)	On-street Parking Demand X	On-street Parking Controls	New Access	Parking Precinct Plan	Subdivision	Other

Survey: REFERRAL REQUIRED						
Check Applicable (X)	Title Interest	Boundary Interest X	Easement Interest X	Strata Interest	Subdivision	Other

Other: REFERRAL REQUIRED						
Check Applicable (X)	TasNetworks X	Parks	Sport & Rec.	Waste	Parking	Other

COMMENTS:

Due to the scope of the proposal, the application has been referred to the Council's Manager Waterways, Manager City Mobility and Surveying Services Manager. The delegated officers' responses, including recommendations are inserted in the respective referral reports.

RECOMMENDATION:

Approval with conditions

REPRESENTATIONS:

Any representations to be directed to the responsible referral Group / officer for a detailed response.

CONDITIONS:

- ENG 1A: Protection of Council assets
- ENG 3A / 3C: Driveways
ALDANMARK CONSULTING ENGINEERS documentation received by the Council on the 10th April 2026 and approved as part of the Final Planning Documents set associated with this permit
- ENG 4: Surface treatment
- ENG 5: The number of car parking spaces approved on the site, for use is seventy two (72)

- ENG 8: All car parking spaces for people with disabilities must be delineated to Australian/NZS Standard, Parking facilities Part 6: Off-street parking for people with disabilities AS/NZS 2890.6: 2009,
- ENG 10: Waste management
- ENG s1: Any small car parking spaces (< 5.4m but no less than 5.0m in length including minimum vertical clearance) must be designated as a space for small cars only.

ADVICE:

- Dial before you dig
- Fees and charges
- Building Permit
- Plumbing Permit
- Occupation of the Public Highway
- General Exemption (Temporary) Parking Permits

DETAILED ASSESSMENT:

C2.0 Parking and Sustainable Transport Code:

C2.1	Code Purpose:
	The purpose of the Parking and Sustainable Transport Code is: C2.1.1 To ensure that an appropriate level of parking facilities is provided to service use and development. C2.1.2 To ensure that cycling, walking and public transport are encouraged as a means of transport in urban areas. C2.1.3 To ensure that access for pedestrians, vehicles and cyclists is safe and adequate. C2.1.4 To ensure that parking does not cause an unreasonable loss of amenity to the surrounding area. C2.1.5 To ensure that parking spaces and accesses meet appropriate standards. C2.1.6 To provide for parking precincts and pedestrian priority streets.
C2.2	This Code applies to use or development of land:
Application of this Code	
C2.2.1	Unless stated otherwise in a particular purpose zone, or sub-clause C2.2.2, C2.2.3 or C2.2.4, this code applies to all use and development
C2.2.2	Clause C2.5.3 only applies to use and development in the following Use Classes: (a) Business and Professional Services; (b) Community Meeting and Entertainment; (c) Custodial Facility; (d) Crematoria and Cemeteries; (e) Educational and Occasional Care; (f) Food Services; (g) General Retail and Hire; (h) Hospital Services; (i) Hotel Industry; (j) Pleasure Boat Facility; (k) Residential if for a communal residence, multiple dwellings or hostel use; (l) Sports and Recreation; and (m) Tourist Operation.
C2.2.3	Clause C2.5.4 only applies to use and development in the following Use Classes: (a) Bulky Goods Sales; (b) General Retail and Hire; (c) Manufacturing and Processing; and (d) Storage.

C2.2.4	Clause C2.5.5 only applies to use and development in the following Use Classes: (a) Business and Professional Services; (b) Community Meeting and Entertainment; (c) Educational and Occasional Care; (d) Emergency Services; (e) Food Services; (f) General Retail and Hire; (g) Sports and Recreation; and (h) Utilities, if not for minor utilities.
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[C2.5](#) Use Standards:

C2.5	Use Standards This code applies to all use and development.
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[C2.5.1](#) Car parking numbers:

Objective:	That an appropriate level of car parking spaces are provided to meet the needs of the use.
C2.5.1	Comments / Discussion (In Bold)
Clause for Assessment	Clause C2.5.1
Car parking numbers	The car parking number assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> . <u>Documentation submitted to date does not comply with the Acceptable Solution, therefore assessment against the Performance Criterion is relied on for clause C2.5.1.</u>
PERFORMANCE CRITERIA	Acceptable solution - A1: - DOES NOT COMPLY: (d/ii). The proposal includes seven (7x) on-site car parking spaces for the residential / dwelling component, which is five (4x) less than required by Table C2.1. The number of on-site car parking spaces must be no less than the number specified in Table C2.1 , less the number of car parking spaces that cannot be provided due to the site including container refund scheme space, excluding if: (a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan; - Not Applicable, no area plan has been adopted by council. (b) the site is contained within a parking precinct plan and subject to Clause C2.7 ; - Not Applicable, no parking precinct plan has been adopted by council. (c) the site is subject to Clause C2.5.5 ; or (d) <u>it relates to an intensification</u> of an existing use or development or a change of use where: (i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or (ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows: $N = A + (C - B)$ N = Number of on-site car parking spaces required

A = Number of existing on-site car parking spaces

B = Number of on-site car parking spaces required for the existing use or development specified in [Table C2.1](#)

C= Number of on-site car parking spaces required for the proposed use or development specified in [Table C2.1](#).

N = Number of on-site car parking spaces required

A = 18 (8x dwellings x 2, plus 2x visitor parks)

B = 18 (8x dwellings x 2, plus 2x visitor parks)

C= 11 (5x dwellings x 2, plus 1x visitor park)

$N = 18 + (11 - 18)$

N = 11

- Development Engineering notes that the submitted Traffic Impact Assessment refers to clauses and demand tables from the previous planning scheme, as assessed under the previously approved planning application PLN HOB 2025 0147.

- Development Engineering has been advised the proposal is to be assessed in conjunction with the original approval, and not in isolation.

The multiple dwellings / residential component (eight previously approved and five proposed) demand rates remain consistent between the former and current scheme for the residential use.

- Key elements already approved such as land use, overall development intent, and baseline impacts are generally accepted as established.

Residential = Any Residential use in any other zone

1 space per bedroom or 2 spaces per 3 bedrooms + 1 visitor space for every 5 multiple dwellings

Five (5x) dwellings = ten (10x) spaces required, plus one (1x) visitor space

Performance Criteria - P1.1: - **NOT APPLICABLE** (Residential use)

The number of on-site car parking spaces for uses, excluding dwellings

Performance Criteria - P1.2: - **ACCEPTED AS MEETING THE PERFORMANCE CRITERIA**

The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:

(a) the nature and intensity of the use and car parking required;

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

(b) the size of the dwelling and the number of bedrooms; and

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

(c) the pattern of parking in the surrounding area.

- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment

- Statement provided by the applicant's traffic engineer contained within the Traffic Impact Assessment;

"In accordance with table E6.1 of the planning scheme, the proposed development generates a requirement for 99 on-site car parking spaces. Due to site constraints, only 72 spaces can be accommodated within the development footprint. Notwithstanding this shortfall, functional parking demand analysis, summarised in table 8.4, indicates that the provision of 72 spaces is expected to adequately support the operational needs of the development without adverse impacts."

"The demand for on-site parking is moderated by the site's location on the fringe of the Hobart CBD, where proximity to public transport, active travel networks, and employment centres supports reduced car dependency. Each two-bedroom apartment will be allocated one dedicated parking space, while three-bedroom apartments will be allocated two spaces. All 16 residential parking spaces will be accommodated within an enclosed parking module, ensuring secure and weather-protected access for residents."

"The existing tenancies currently operate with a total allocation of 21 on-site parking spaces.

One tenant has relinquished three spaces, reducing the effective provision to 18 spaces. While Table E6.1 of the Tasmanian Planning Scheme specifies a higher number of spaces based on floor area and land use, the current allocation has proven sufficient to meet operational demand, with no reported issues relating to parking availability or site functionality. Given the spatial constraints and the absence of adverse impacts, it is considered appropriate to retain the existing parking arrangement for these tenancies."

- Council's City Transport Group provided the following statement;

"A functional car parking demand analysis determined that the site requires 72 on-site parking spaces. This is a reduction from the 99 spaces required by the planning scheme, justified by factors like the two-bedroom apartments needing only one space each and the existing tenancies continuing to operate with their current 21 spaces. The proposed reduction is acceptable."

"Brisbane Street has sufficient width to support on-street parking. Kerbside restrictions are in place to ensure a high turnover of these spaces, which is beneficial for the retail component of the development."

"The site is in close proximity to high-frequency bus routes. The nearest bus stops are on Elizabeth Street and Argyle Street, both within 200 metres of the development, with services running every 10 minutes."

"Due to its proximity to the Hobart CBD, walking and cycling are considered suitable alternative transport modes. The TIA specifically mentions on-road cycle lanes along Argyle Street. A survey conducted for the report at a similar building indicated that 40% of occupants use alternative transport options like ridesharing, walking, and cycling."

Based on the documentation submitted to date and given the above assessment, the car parking numbers is accepted as meeting the *Performance Criteria P1.2:C2.5.1* of the Planning Scheme. This is particularly due to the actual parking demands that will be generated by the development.

C2.5.2 Bicycle parking numbers:

Objective: C2.5.2	That an appropriate level of bicycle parking spaces are provided to meet the needs of the use.
Clause C2.5.2: Bicycle parking numbers NOT APPLICABLE	<p>The bicycle parking number assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. <u>Documentation submitted to date does not invoke clause C2.5.2.</u></p> <p>Acceptable solution - A1: Bicycle parking spaces must:</p> <p>(a) be provided on the site or within 50m of the site; and (b) be no less than the number specified in <u>Table C2.1</u>.</p> <p>Documentation submitted to date does not indicate any changes proposed to the existing bicycle parking numbers as approved under PLN-HOB-2025-0147.</p> <p>- Development Engineering notes that the submitted Traffic Impact Assessment refers to clauses and demand tables from the previous planning scheme, as assessed under the previously approved planning application PLN HOB 2025 0147. - Development Engineering has been advised the proposal is to be assessed in conjunction with the original approval, and not in isolation. The multiple dwellings / residential component (eight previously approved and five proposed) demand rates remain consistent between the former and current scheme for the residential use.</p> <p>- Key elements already approved such as land use, overall development intent, and baseline impacts are generally accepted as established.</p>

C2.5.3 Motorcycle parking numbers:

Objective: C2.5.3	That the appropriate level of motorcycle parking is provided to meet the needs of the use.
Clause C2.5.3: Motorcycle parking numbers NOT APPLICABLE	<p>The motorcycle parking number assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. <u>Documentation submitted to date does not invoke clause C2.5.3.</u></p> <p>Clause C2.5.3 only applies to use and development in the following Use Classes:</p> <p>(a) Business and Professional Services; - Applicable (b) Community Meeting and Entertainment; - Not Applicable (c) Custodial Facility; - Not Applicable (d) Crematoria and Cemeteries; - Not Applicable (e) Educational and Occasional Care; - Not Applicable (f) Food Services; - Not Applicable (g) General Retail and Hire; - Applicable (h) Hospital Services; - Not Applicable</p>

	<p>(i) Hotel Industry; - Not Applicable (j) Pleasure Boat Facility; - Not Applicable (k) Residential if for a communal residence, multiple dwellings or hostel use; - Not Applicable (l) Sports and Recreation; and- Not Applicable (m) Tourist Operation. - Not Applicable</p> <p>Documentation submitted to date does not indicate any changes proposed to the existing motorcycle parking numbers as approved under PLN-HOB-2025-0147.</p> <p>- Development Engineering notes that the submitted Traffic Impact Assessment refers to clauses and demand tables from the previous planning scheme, as assessed under the previously approved planning application PLN HOB 2025 0147.</p> <p>- Development Engineering has been advised the proposal is to be assessed in conjunction with the original approval, and not in isolation. The multiple dwellings / residential component (eight previously approved and five proposed) demand rates remain consistent between the former and current scheme for the residential use.</p> <p>- Key elements already approved such as land use, overall development intent, and baseline impacts are generally accepted as established.</p>
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C2.5.4 Loading Bays:

Objective:	That adequate access for goods delivery and collection is provided, and to avoid unreasonable loss of amenity and adverse impacts on traffic flows.
C2.5.4	
Clause C2.5.4:	The Loading Bay assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> .
Loading Bays	Documentation submitted to date does not invoke clause C2.5.4.
NOT APPLICABLE	<p>Clause C2.5.4 only applies to use and development in the following Use Classes: (a) Bulky Goods Sales; - Not Applicable (b) General Retail and Hire; - Applicable (c) Manufacturing and Processing; and - Not Applicable (d) Storage. - Not Applicable</p> <p>Documentation submitted to date does not indicate any changes proposed to the existing loading bay considerations as approved under PLN-HOB-2025-0147.</p> <p>- Development Engineering notes that the submitted Traffic Impact Assessment refers to clauses and demand tables from the previous planning scheme, as assessed under the previously approved planning application PLN HOB 2025 0147.</p> <p>- Development Engineering has been advised the proposal is to be assessed in conjunction with the original approval, and not in isolation. The multiple dwellings / residential component (eight previously approved and five proposed) does not invoke additional loading bay considerations.</p> <p>- Key elements already approved such as land use, overall development intent, and baseline impacts are generally accepted as established.</p>

C2.5.5 Number of car parking spaces within the General Res. Zone and Inner Res. Zone:

Objective:	To:
C2.5.5	(a) facilitate the reuse of existing non-residential buildings within the General Residential Zone and Inner Residential Zone; and

	(b) to not cause an unreasonable impact on residential amenity by the car parking generated by that reuse.
<p>Clause C2.5.5: Number of car parking spaces within the General Residential Zone and Inner Residential Zone (Reuse of existing non-residential buildings) NOT APPLICABLE</p>	<p>The number of car parking spaces within the General Residential Zone and Inner Residential Zone must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. Documentation submitted to date does not invoke clause C2.5.5.</p> <p>Clause C2.5.5 only applies to use and development in the following Use Classes: (a) Business and Professional Services; - Not Applicable (b) Community Meeting and Entertainment; - Not Applicable (c) Educational and Occasional Care; - Not Applicable (d) Emergency Services; - Not Applicable (e) Food Services; - Not Applicable (f) General Retail and Hire; - Not Applicable (g) Sports and Recreation; and- Not Applicable (h) Utilities, if not for minor utilities. - Not Applicable</p> <p>Documentation submitted to date does not indicate any requirement for car parking spaces within the General Residential Zone and Inner Residential Zone for the respective Use Classes (C2.2.4)</p>

[C2.6](#) Development Standards for Buildings and Works:

C2.6	Development Standards for Buildings and Works This code applies to all use and development.
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[C2.6.1](#) Construction of parking areas:

Objective: C2.6.1	That parking areas are constructed to an appropriate standard.
Clause for Assessment	Comments / Discussion (In Bold)
<p>Clause C2.6.1: Construction of parking areas ACCEPTABLE SOLUTION</p>	<p>The construction of parking areas must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. Documentation submitted to date does comply with the Acceptable Solution for clause C2.6.1.</p> <p>Acceptable Solution A1; - DOES COMPLY All parking, access ways, manoeuvring and circulation spaces must:</p> <p>(a) be constructed with a durable all weather pavement;</p> <p>(b) be drained to the public stormwater system, or contain stormwater on the site; and</p> <p>(c) excluding all uses in the Rural Zone, Agriculture Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.</p> <p>Documentation submitted to date indicates a concrete surface treatment, able to be drained to an approved stormwater system. Condition on Planning Permit to ratify timing.</p>

[C2.6.2](#) Design and layout of parking areas:

Objective:	That parking areas are designed and laid out to provide convenient, safe and
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C2.6.2	efficient parking.
Clause C2.6.2:	The design and layout of parking areas must comply with the Acceptable Solutions or
Design and layout	meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian</i>
of parking areas	<i>Planning Scheme - State Planning Provisions.</i>
PERFORMANCE	Documentation submitted to date does not comply with the Acceptable
CRITERIA	Solution, therefore assessment against the Performance Criterion is relied on
	for clause C2.6.2.
(iv) 2.6m min.	Acceptable Solution - A1: - DOES NOT COMPLY: (ai), (aiv), (av), (avi)
width	Parking, access ways, manoeuvring and circulation spaces must either:
	(a) comply with the following:
(v) 3 or more car	(i) have a gradient in accordance with Australian Standard AS 2890- Parking
parking spaces	facilities, Parts 1-6 ; - NOT MET (Turning gradient > 5%)
	(ii) provide for vehicles to enter and exit the site in a forward direction where
	providing for more than 4 parking spaces; - MET (As approved under
	PLN-HOB-2025-0147)
	(iii) have an access width not less than the requirements in Table C2.2 ; -
	MET (As approved under PLN-HOB-2025-0147)
	(iv) have car parking space dimensions which satisfy the requirements in
	Table C2.3 ; - NOT MET (Parking spaces 2.4m in width and small car
	parking space)
	(v) have a combined access and manoeuvring width adjacent to parking
	spaces not less than the requirements in Table C2.3 where there are 3 or
	more car parking spaces ; - NOT MET (Parking spaces 2.4m in width)
	(vi) have a vertical clearance of not less than 2.1m above the parking surface
	level; - NOT MET (Due to vertical constraints imposed by the building
	structure, the parking bay located at the right end of the top row will
	be designated for small car use only)
	and
	(vii) excluding a single dwelling, be delineated by line marking or other clear
	physical means; - MET (As approved under PLN-HOB-2025-0147)
	or
	(b) comply with Australian Standard AS 2890- Parking facilities, Parts 1-6 .
	- Assessed under part (a)
	Acceptable Solution - A1.2: - DOES NOT COMPLY: (a)
	Parking spaces provided for use by persons with a disability must satisfy the
	following:
	(a) be located as close as practicable to the main entry point to the building; -
	NOT MET
	(b) be incorporated into the overall car park design; and - MET
	(c) be designed and constructed in accordance with Australian/New Zealand
	Standard AS/NZS 2890.6:2009 Parking facilities, Off-street parking for
	people with disabilities . ³⁵ - MET
	³⁵ Requirements for the number of accessible car parking spaces are specified in part
	D3 of the National Construction Code 2016.

Performance Criteria - P1: - **ACCEPTED AS MEETING THE PERFORMANCE CRITERIA**

All parking, access ways, manoeuvring and circulation spaces must be designed and readily identifiable to provide convenient, safe and efficient parking, having regard to:

(a) the characteristics of the site;

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

(b) the proposed slope, dimensions and layout;

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

(c) useability in all weather conditions;

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

(d) vehicle and pedestrian traffic safety;

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

(e) the nature and use of the development;

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

(f) the expected number and type of vehicles;

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

(g) the likely use of the parking areas by persons with a disability;

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

(h) the nature of traffic in the surrounding area;

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

(i) the proposed means of parking delineation;

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

and

(j) the provisions of [Australian Standard AS 2890.1:2004 - Parking facilities, Part 1: Off-street car parking](#) and [AS 2890.2 -2002 Parking facilities, Part 2: Off-street commercial vehicle facilities](#).

- **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

	<p>- Development Engineering notes that the submitted Traffic Impact Assessment refers to clauses and demand tables from the previous planning scheme, as assessed under the previously approved planning application PLN HOB 2025 0147.</p> <p>- Development Engineering has been advised the proposal is to be assessed in conjunction with the original approval, and not in isolation. The residential component remains consistent with previous considerations.</p> <p>- Key elements already approved such as land use, overall development intent, and baseline impacts are generally accepted as established.</p> <p>Based on the submitted documentation to date and given the above assessment, the design and layout of parking areas is accepted as meeting the Performance Criteria P1:C2.6.2 of the Planning Scheme.</p>
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C2.6.3 Number of accesses for vehicles:

Objective: C2.6.3	<p>That:</p> <p>(a) access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses;</p> <p>(b) accesses do not cause an unreasonable loss of amenity of adjoining uses; and</p> <p>(c) the number of accesses minimise impacts on the streetscape.</p>
Clause C2.6.3: Number of accesses for vehicles NOT APPLICABLE	<p>The number of accesses for vehicles must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p><u>Documentation submitted to date does not invoke clause C2.6.3.</u></p> <p><u>Acceptable Solution A1:</u> The number of accesses provided for each frontage must: (a) be no more than 1; or (b) no more than the existing number of accesses, whichever is the greater.</p> <p><u>Acceptable Solution A2:</u> Within the Central Business Zone or in a pedestrian priority street no new access is provided unless an existing access is removed.</p> <p><u>Documentation submitted to date does not indicate any changes proposed to the existing number of accesses for vehicles considerations as approved under PLN-HOB-2025-0147.</u></p> <p>- Development Engineering notes that the submitted Traffic Impact Assessment refers to clauses and demand tables from the previous planning scheme, as assessed under the previously approved planning application PLN HOB 2025 0147.</p> <p>- Development Engineering has been advised the proposal is to be assessed in conjunction with the original approval, and not in isolation. The multiple dwellings / residential component (eight previously approved and five proposed) does not invoke additional access considerations.</p> <p>- Key elements already approved such as land use, overall development intent, and baseline impacts are generally accepted as established.</p>

C2.6.4 Lighting of parking areas within the General Business Zone and Central Business Zone:

Objective: C2.6.4	That parking and vehicle circulation roads and pedestrian paths within the General Business Zone and Central Business Zone, which are used outside daylight hours, are provided with lighting to a standard which: (a) enables easy and efficient use; (b) promotes the safety of users; (c) minimises opportunities for crime or anti-social behaviour; and (d) prevents unreasonable light overspill impacts.
Clause C2.6.4: Lighting of parking areas within the General Business Zone and Central Business Zone (5 or more car parking spaces) NOT APPLICABLE	The lighting of parking areas within the General Business Zone and Central Business Zone must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> . <u>Documentation submitted to date does not invoke clause C2.6.4.</u> Documentation submitted to date indicates any lighting of parking areas within the General Business Zone and Central Business Zone proposed (within a Commercial Zone). Documentation submitted to date does not indicate any changes proposed to the existing lighting of parking areas considerations as approved under PLN-HOB-2025-0147.

C2.6.5 Pedestrian access:

Objective: C2.6.5	That pedestrian access within parking areas is provided in a safe and convenient manner.
Clause C2.6.5: Pedestrian access PERFORMANCE CRITERIA (Uses that require 10 or more car parking spaces)	The pedestrian access must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> . <u>Documentation submitted to date does not comply with the Acceptable Solution, therefore assessment against the Performance Criterion is relied on for clause C2.6.5.</u> Acceptable Solution A1.1: - DOES NOT COMPLY: (ai), (a), (b) Uses that require 10 or more car parking spaces must: (a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by: (i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or (ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and (b) be signed and line marked at points where pedestrians cross access ways or parking aisles. Acceptable Solution A1.2: - DOES NOT COMPLY In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building. Performance Criteria - P1: - ACCEPTED AS MEETING THE PERFORMANCE CRITERIA

	<p>Safe and convenient pedestrian access must be provided within parking areas, having regard to:</p> <ul style="list-style-type: none"> (a) the characteristics of the site; (b) the nature of the use; (c) the number of parking spaces; (d) the frequency of vehicle movements; (e) the needs of persons with a disability; (f) the location and number of footpath crossings; (g) vehicle and pedestrian traffic safety; (h) the location of any access ways or parking aisles; and (i) any protective devices proposed for pedestrian safety. <p>Documentation submitted to date does not indicate any significant changes proposed to the existing pedestrian access considerations as approved under PLN-HOB-2025-0147.</p> <ul style="list-style-type: none"> - Development Engineering notes that the submitted Traffic Impact Assessment refers to clauses and demand tables from the previous planning scheme, as assessed under the previously approved planning application PLN HOB 2025 0147. - Development Engineering has been advised the proposal is to be assessed in conjunction with the original approval, and not in isolation. - Key elements already approved such as land use, overall development intent, and baseline impacts are generally accepted as established. - Statement provided by the applicant's traffic engineer contained within the Traffic Impact Assessment; <p><i>"The two accessible parking spaces will be located adjacent to the enclosed residential parking module and supported by a compliant shared zone. A delineated pedestrian pathway will connect these spaces to the lift lobby on the second floor, facilitating accessible vertical circulation throughout the building and direct access to Brisbane Street. The design includes a dedicated doorway between the accessible parking area and the lift lobby, ensuring safe, convenient, and equitable access for all users."</i></p> <p><i>"From a traffic engineering and road safety perspective, the additional traffic generated from this development is not expected to create any adverse safety, amenity, or traffic efficiency problems"</i></p> <p>Based on the submitted documentation to date and given the above assessment, the pedestrian access is accepted as meeting the Performance Criteria P1: C2.6.5 of the Planning Scheme.</p>
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[C2.6.6](#) Loading bays:

Objective:	That the area and dimensions of loading bays are adequate to provide safe and efficient delivery and collection of goods.
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<p>Clause C2.6.6: Loading bays NOT APPLICABLE</p>	<p>The loading bays must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. <u>Documentation submitted to date does not invoke clause C2.6.6.</u></p> <p>Documentation submitted to date does not indicate any changes proposed to the existing loading bay considerations as approved under PLN-HOB-2025-0147.</p> <ul style="list-style-type: none"> - Development Engineering notes that the submitted Traffic Impact Assessment refers to clauses and demand tables from the previous planning scheme, as assessed under the previously approved planning application PLN HOB 2025 0147. - Development Engineering has been advised the proposal is to be assessed in conjunction with the original approval, and not in isolation. The multiple dwellings / residential component (eight previously approved and five proposed) does not invoke additional loading bay considerations. - Key elements already approved such as land use, overall development intent, and baseline impacts are generally accepted as established.
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C2.6.7 Bicycle parking and storage:

<p>Objective: C2.6.7</p>	<p>That parking for bicycles are safe, secure and convenient, within the General Business Zone and Central Business Zone.</p>
<p>Clause C2.6.7: Bicycle parking and storage facilities within the General Business Zone and Central Business Zone NOT APPLICABLE</p>	<p>The bicycle parking and storage facilities within the General Business Zone and Central Business Zone must satisfy the Acceptable Solutions of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. <u>Documentation submitted to date does not invoke clause C2.6.7.</u></p> <p>Documentation submitted to date does not indicate any proposed bicycle parking and storage facilities within the General Business Zone and Central Business Zone (within a Commercial Zone).</p> <p>Documentation submitted to date does not indicate any changes proposed to the existing bicycle parking and storage facilities considerations as approved under PLN-HOB-2025-0147.</p>

C2.6.8 Siting of parking and turning areas:

<p>Objective: C2.6.8</p>	<p>That the siting of vehicle parking and access facilities in an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone or Central Business Zone does not cause an unreasonable visual impact on streetscape character or loss of amenity to adjoining properties.</p>
<p>Clause C2.6.8: Siting of parking and turning areas NOT APPLICABLE</p>	<p>The siting of parking and turning areas within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone must satisfy the Acceptable Solutions of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. <u>Documentation submitted to date does not invoke clause C2.6.8.</u></p> <p>Documentation submitted to date does not indicate any proposed parking and turning areas within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone (within a Commercial Zone).</p> <p>Siting of parking and turning areas within an Inner Residential Zone, Village Zone, Urban Mixed Use</p>

Zone, Local Business Zone or General Business Zone	
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C2.7 Parking Precinct Plan:

C2.7	Parking Precinct Plan
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C2.7.1 On-site car parking spaces within an area defined by a parking precinct plan:

Objective: C2.7.1	To minimise the amount of on-site car parking spaces within an area defined by a parking precinct plan, and that parking does not detract from the streetscape of the area.
Clause for Assessment	Comments / Discussion (In Bold)
Clause C2.7.1: Parking precinct plan NOT APPLICABLE On-site car parking spaces within an area defined by a parking precinct plan	The on-site car parking spaces within an area defined by a parking precinct plan must satisfy the Acceptable Solutions of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> . <u>Documentation submitted to date does not invoke clause C2.7.1.</u> Documentation submitted to date does not indicate any on-site car parking spaces within an area defined by a parking precinct plan.

C3.0 Road and Railway Assets Code:

C3.1	Code Purpose
	The purpose of the Road and Railway Assets Code is: C3.1.1 To protect the safety and efficiency of the road and railway networks; and C3.1.2 To reduce conflicts between sensitive uses and major roads and the rail network.
C3.2 Application of this Code	Application of this Code:
C3.2.1	This code applies to a use or development that: (a) will increase the amount of vehicular traffic or the number of movements of vehicles longer than 5.5m using an existing vehicle crossing or private level crossing; (b) will require a new vehicle crossing, junction or level crossing; or (c) involves a subdivision or habitable building within a road or railway attenuation area if for a sensitive use.
C3.4.1	There are no exemptions from this code.

C3.5 Use Standards:

C3.5	Use Standards
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C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction:

Objective:	To minimise any adverse effects on the safety and efficiency of the road or rail network from vehicular traffic generated from the site at an existing or new vehicle crossing or level crossing or new junction.
Clause for Assessment	Comments / Discussion (In Bold)
Clause C3.5.1: Traffic generation at a vehicle crossing, level crossing or new junction ACCEPTABLE SOLUTION	<p>The traffic generation at a vehicle crossing, level crossing or new junction must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does comply with the Acceptable Solution for clause C3.5.1.</p> <p><u>Acceptable Solution A1.1:</u> - DOES COMPLY For a category 1 road or a limited access road, vehicular traffic to and from the site will not require:</p> <ul style="list-style-type: none"> (a) a new junction; (b) a new vehicle crossing; or (c) a new level crossing. <p><u>Acceptable Solution A1.2:</u> For a road, excluding a category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority. - N/A</p> <p><u>Acceptable Solution A1.3:</u> - NOT APPLICABLE For the rail network, written consent for a new private level crossing to serve the use and development has been issued by the rail authority.</p> <p><u>Acceptable Solution A1.4:</u> - DOES COMPLY Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than:</p> <ul style="list-style-type: none"> (a) the amounts in Table C3.1; or <p>- Acceptable, documentation submitted to date can meet this requirement</p> <p>Vehicle crossings on other roads: 20% or 40 vehicle movements per day, whichever is the greater</p> <ul style="list-style-type: none"> (b) allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road. <p><u>Acceptable Solution A1.5:</u> - DOES COMPLY Vehicular traffic must be able to enter and leave a major road in a forward direction. - Acceptable, documentation submitted to date can meet this requirement</p>

C3.6 Development Standards for Buildings or Works:

C3.6	Development Standards for Buildings or Works
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C3.6.1 Habitable buildings for sensible uses within a road or railway attenuation area:

Objective: C3.6.1	To minimise the effects of noise, vibration, light and air emissions on sensitive uses within a road or railway attenuation area, from existing and future major roads and the rail network.
Clause C3.6.1: Habitable buildings for sensitive uses within a road or railway attenuation area NOT APPLICABLE	The habitable buildings for sensitive uses within a road or railway attenuation area must satisfy the Acceptable Solutions of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> . Documentation submitted to date does not invoke clause C3.6.1. Documentation submitted to date does not indicate any habitable buildings for sensitive uses within a road or railway attenuation area.

C3.7 Development Standards for Subdivision:

C3.7	Development Standards for Subdivision
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C3.7.1 Subdivision for sensitive uses within a road or railway attenuation area:

Objective: C3.7.1	To minimise the effects of noise, vibration, light and air emissions on lots for sensitive uses within a road or railway attenuation area, from existing and future major roads and the rail network.
Clause C3.7.1: Subdivision for sensitive uses within a road or railway attenuation area NOT APPLICABLE	The subdivision for sensitive uses within a road or railway attenuation area must satisfy the Acceptable Solutions of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> . Documentation submitted to date does not invoke clause C3.7.1. Documentation submitted to date does not indicate any subdivision for sensitive uses within a road or railway attenuation area.

6.11.2 (g) Erosion, and stormwater volume and quality controls:

6.11	Conditions and Restrictions on a Permit
Clause 6.11.2 (g)	Erosion, and stormwater volume and quality controls.
ACCEPTABLE SOLUTION	The erosion, and stormwater volume and quality controls are to be considered under sub-clauses 6.11.2(g) of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> . Documentation submitted to date does require conditions and restrictions imposed by the planning authority on a permit under clause C6.11.2(g). - Stormwater from new impervious surfaces disposed of by gravity to public stormwater infrastructure. - Referred to the Waterways Group for determination and conditioning.

OVERVIEW:

Alterations, Extension to Previously Approved Development (PLN-HOB-2024-0147) and Thirteen Multiple Dwellings (Eight Approved and Five Proposed)

- Development Engineering notes that the submitted Traffic Impact Assessment refers to clauses and demand tables from the previous planning scheme, as assessed under the previously approved planning application PLN HOB 2025 0147.

- Development Engineering has been advised the proposal is to be assessed in conjunction with the original approval, and not in isolation. The multiple dwellings / residential component (eight previously approved and five proposed) is being assessed.

- Key elements already approved such as land use, overall development intent, and baseline impacts are generally accepted as established.

ADVICE:

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's [website](#) for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

Building Permit:

You may need building approval in accordance with the *Building Act 2016*, further details are available on the [Council's website](#), which may assist you in understanding the relevant requirements.

A checklist has also been developed by Consumer, Building and Occupational Services (CBOS) to help property owners understand their responsibilities before, during, and after building work. It outlines the key steps in the building work approval process for notifiable (medium risk) and permit (high risk) work under the *Building Act 2016*. This resource is designed to support owners in meeting their obligations and ensuring a smooth approvals process. You can access the checklist [here](#).

This is a Discretionary Planning Permit issued in accordance with section 57 of the *Land Use Planning and Approvals Act 1993*.

Plumbing Permit:

You may need plumbing approval in accordance with the *Building Act 2016*, *Building Regulations 2016* and the National Construction Code. Further details are available on the [Council's website](#), which may assist you in understanding the relevant requirements.

A checklist has also been developed by Consumer, Building and Occupational Services (CBOS) to help property owners understand their responsibilities before, during, and after building work. It outlines the key steps in the building work approval process for notifiable (medium risk) and permit (high risk) work under the *Building Act 2016*. This resource is designed to support owners in meeting their obligations and ensuring a smooth approvals process. You can access the checklist [here](#).

Occupation Of The Public Highway:

You may require a permit for the occupation of the public highway for construction (e.g. placement of skip bin, crane, scissor lift etc). Click [here](#) for more information.

General Exemption (Temporary) Parking Permits:

You may qualify for a General Exemption permit for construction vehicles i.e. residential or meter parking/loading zones. Click [here](#) for more information.

Waste Disposal:

It is recommended that the developer liaise with the Council's City Resilience Group regarding reducing, reusing and recycling materials associated with demolition on the site to minimise solid waste being directed to landfill.

Further information regarding waste disposal can also be found on the Council's [website](#).

Fees and Charges:

Click [here](#) for information on the Council's fees and charges.

Dial Before You Dig:

Click [here](#) for dial before you dig information.

Attachment A**PLN-HOB-2025-0658 – 65 Brisbane Street, Hobart**

In person:

Mark Gardner
Lara Maesele
Tim Watson

Proponent
Project Architect
Engineer

The Panel met to discuss the proposal in detail and the report below is a summary of the Panel's views and is provided for the consideration of the proponents and Council officers.

Description

The proposal is an amendment to the earlier approval for a five-storey mixed use building on the site. The amendment would include an additional storey which would be for another level of residential which would include another five (5) dwellings with the full proposal comprising six-storey building with three floors retail and office and three floors residential with thirteen (13) dwellings total.

The proposed building would be constructed within the south-eastern part of the property, close to the site frontage with Brisbane Street. This footprint remains unchanged from the earlier approval. The building would have six storeys, although the ground floor and first floor would not extend the full footprint given the gradient of the site. The ground floor would contain a retail outlet and associated services. Storage is also proposed on this level, including waste storage, bicycle storage, and storage associated with the residential use proposed on other levels. The waste storage and associated residential use storage areas have been increased under this proposal.

Level 1 of the proposed building would contain offices with a floor area of approximately 578m², an increase above the previous approval. A further 427m² of floor area for offices is proposed on level 2 as well as car parking for 18 vehicles, an increase of 5 additional spaces of which two (2) of these are now outside the building footprint. This car parking area would be accessed via the rear of the building. Residential use is proposed across the remaining three levels of the building. Five apartments are proposed on level 3 and level 4 while three penthouse apartments are proposed on the top level, level 5.

The revised proposal also includes a further refined pedestrian access along the western elevation, independent of the northern stairwell, allowing movement toward Brisbane Street without entering the building.

Vehicular access to the development would be via the existing entry to the site that currently provides access to a commercial car park and other uses.

Pedestrian access to the retail outlet proposed on the ground floor of the building would be directly from the site frontage. Pedestrian access to the

remainder of the development would be via a covered lobby on the south-western side of the building.

Proposed external materials are not specified on the submitted plans although the proposed elevations clearly show substantial areas of glazing. The application has been referred to Heritage Tasmania which has issued a request for additional information requiring the applicant to specify the proposed external materials.

Landscaping around the heritage listed Sydney Lodge building on the site, that would be to the rear of the proposed building, is also proposed.

Panel Report

Please Note: The Panel's views expressed below include those discussed with the Applicant and those discussed between Panel members in the absence of the Applicant.

The Panel appreciated the opportunity to comment but noted that the proposal is a submitted development application. The Panel also noted that a previous scheme was referred to them in April 2025 and a concise report was prepared that does not seem to be referred to at all in this amendment.

There were still significant concerns regarding setbacks. The Panel noted that some apartments were too close to the boundary of the neighbouring property. Any future development of the neighbouring site would lead to poor residential amenity including compromised light and privacy.

The Panel still believes in the importance of a site development plan to better inform the set of issues this type of scheme should address. The Panel did not think that the typography of the city has been understood. The city centre should be understood as a land form centre. The hill should also be recognised as a permeable neighbourhood, not just a hill.

Urban design needs to address how the building sits within the city from a townscape perspective. Detailed urban analysis of response to context. These were previously outlined and become more important as the project gets bigger.

The addition of extra height was seen by the Panel as exacerbating existing issues with amenity due to the limited set back, noting that any height increase should positively contribute back to the city's character. They felt

that while there was an opportunity to increase height further towards the back of the site, they expected that any extra height would create allowance for stepping back in terms of developing to the boundary, particularly to allow for residential amenity. The Panel does not support allowing developments that are the first to spread to the boundaries without some form of control.

The Panel questioned the quality of some apartments, particularly those with compromised access and light and thought the development failed to address residential amenity for those apartments.

Detailing of the lower three levels of the street wall, from undercroft to the top of the balustrade, needs to be carefully developed further as this establishes the urban interface and building character. Consideration of the impact of ceilings and interior elements on the reading of the façade needs to be explored in terms of the positive contribution to the city.

The Panel thought a landscape plan would have been of great benefit to the scheme, noting that thinking about landscaping early on can help inform a better response to the design. The Panel also emphasised that landscape is a thing, not just plants. Greater consideration needs to be given for deep soil planting, which is something that can be teased out by proper landscaping planning.

The Panel strongly supports more residential in the city, noting it is important to clearly consider the impact of the development of each site on the city and elaborate on overall principles of the site, including future development. However, they felt that this scheme fails to consider residential amenity, the typography of the site and the extra height exacerbates the issues that were raised previously. The Panel recommends the exploration of principles for the development for the site, which engage with the urban conditions, and to present strategies for various stages of the development that reinforce the principles of how the site is configured in relation to the broader landform of the city.



SUBMISSION TO PLANNING AUTHORITY NOTICE - SPAN

Reference

TASWASPAN-HOB-2025-0268

Application Reference

PLN-HOB-2025-0663

Address

310 ARGYLE ST NORTH HOBART TAS 7000

Titles

198694/2, 27209/1, 62966/26, 27174/2, 230163/1, 198694/1

Applicant

Name	Email	Phone	Address	Involvement
Mr John Etherington	john@sgroup.com.au	0405498065	6/100 Elizabeth Street, Hobart, Tasmania, Australia, 7000	Applicant

Owner

Name	Email Address	Address
Mr John Etherington	john@sgroup.com.au	6/100 Elizabeth Street, Hobart, Tasmania, Australia, 7000

Relevant Property Title/s

Selected Titles	Total Area: 0m ²
198694/2 27209/1 62966/26 27174/2 230163/1 198694/1	

Council Description of Use and/or Development

Council Proposed Use or Development Description

Partial Change of Use to Sports and Recreation

Response

Pursuant to the Water and Sewerage Industry Act 2008 (TAS)

Section 56P(1) - TasWater imposes the following conditions on the permit for this application:

Conditions

#	Condition	Origin
1	<p>Connections, Metering, Backflow - connections</p> <p>A suitably sized water supply with metered connections and sewerage system and connections to the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit.</p> <p>Advice: TasWater will not accept direct fire boosting from the network unless it can be demonstrated that the periodic testing of the system will not have a significant negative effect on our network and the minimum service requirements of other customers serviced by the network. To this end break tanks may be required with the rate of flow into the break tank controlled so that peak flows to fill the tank do not also cause negative effect on the network.</p>	Water and Sewerage Industry Act 2008
2	<p>Connections, Metering, Backflow - Removal/installation</p> <p>Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost.</p>	Water and Sewerage Industry Act 2008
3	<p>Connections, Metering, Backflow</p> <p>Prior to commencing construction of the subdivision/use of the development, any water connection utilised for construction/the development must have a backflow prevention device and water meter installed, to the satisfaction of TasWater.</p>	Water and Sewerage Industry Act 2008
4	<p>Asset creation Infrastructure works - Locate</p> <p>Prior to applying for a Certificate for Certifiable Works, the developer must physically locate all existing infrastructure to provide sufficient information for accurate design and physical works to be undertaken.</p>	Water and Sewerage Industry Act 2008

5	Asset creation Infrastructure works - Markers Prior to undertaking any works related to water and sewerage, physical markers must be in place that clearly identify where water and/or sewer connections are to be made in accordance with any approved plan to TasWater's satisfaction.	Water and Sewerage Industry Act 2008
6	Conditions - Trade Waste Food 1. Prior to the commencement of operation the developer/property owner must obtain Consent to discharge Trade Waste from TasWater. 2. The developer must install appropriately sized and suitable pre-treatment devices prior to gaining Consent to discharge. 3. The Developer/property owner must comply with all TasWater conditions prescribed in the Trade Waste Consent.	Water and Sewerage Industry Act 2008 Certificate for Certifiable Work (Plumbing)
7	Condition (Water) Prior to TasWater issuing a Consent to Register a Legal Document /Certificate(s) for Certifiable Work (Building) and/or (Plumbing), the applicant or landowner as the case may be, must pay a developer charge totalling \$2,147.05 to TasWater for water infrastructure for 1.222 additional Equivalent Tenements, indexed by the Consumer Price Index All groups (Hobart) from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater.	Water and Sewerage Industry Act 2008
8	Condition (Sewerage) Prior to TasWater issuing a Consent to Register a Legal Document /Certificate(s) for Certifiable Work (Building) and/or (Plumbing), the applicant or landowner as the case may be, must pay a developer charge totalling \$3,584.28 to TasWater for sewerage infrastructure for 2.04 additional Equivalent Tenements, indexed by the Consumer Price Index All groups (Hobart) from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater.	Water and Sewerage Industry Act 2008
9	Fees payment required The applicant or landowner as the case may be, must pay a development assessment fee of \$251.35 to TasWater, as approved by the Economic Regulator and the fees will be indexed, until the date paid to TasWater. The payment is required within 30 days of the issue of an invoice by TasWater.	Water and Sewerage Industry Act 2008
10	Declaration The drawings/documents and conditions stated above constitute TasWater's Submission to Planning Authority Notice.	Water and Sewerage Industry Act 2008
11	Advice - General For information on TasWater development standards, please visit https://www.taswater.com.au/building-and-development/technical-standards For application forms please visit https://www.taswater.com.au/building-and-development/development-application-form	
12	Advice - Service Locations Please note that the developer is responsible for arranging to locate the existing TasWater infrastructure and clearly showing it on the drawings. Existing TasWater infrastructure may be located by a surveyor and/or a private contractor engaged at the developers cost to locate the infrastructure. A copy of the GIS is included in email with this notice and should aid in updating of the documentation. The location of this infrastructure as shown on the GIS is indicative only. <ul style="list-style-type: none">A permit is required to work within TasWater's easements or in the vicinity of its infrastructure. Further information can be obtained from TasWaterTasWater has listed a number of service providers who can provide asset detection and location services should you require it. Visit www.taswater.com.au/Development/Service-location for a list of companiesTasWater will locate residential water stop taps free of chargeSewer drainage plans or Inspection Openings (IO) for residential properties are available from your local council.	Water and Sewerage Industry Act 2008
13	Advice - Boundary Trap Area The proposed development is within a boundary trap area and the developer will need to provide a boundary trap that prevents noxious gases or persistent odours back venting into the property's sanitary drain. The boundary trap is to be contained within the property boundaries and the property owner remains responsible for the ownership, operation and maintenance of the boundary trap.	Water and Sewerage Industry Act 2008
14	Water Submetering As of July 1 2022, TasWater's Sub-Metering Policy no longer permits TasWater sub-meters to be installed for new developments. Please ensure plans submitted with the application for Certificate(s) for Certifiable Work (Building and/or Plumbing) reflect this. For clarity, TasWater does not object to private sub-metering arrangements. Further information is available on our website (www.taswater.com.au) within our Sub-Metering Policy and Water Metering Guidelines.	Water and Sewerage Industry Act 2008
15	Advice - Developer Charges For information on Developer Charges please visit the following webpage - https://www.taswater.com.au/building-and-development/developer-charges	

Referral Documents

Version	Document Date	Document Type	Description	Prepared By
1	19 Dec 2025	General	Application Form - redacted	Jacqui Loring
1	18 Dec 2025	Property Title Document	FolioText-198694-2.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioPlan-198694-2.pdf	Mr John Etherington

1	18 Dec 2025	Property Title Document	FolioPlan-27209-1.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioText-27209-1.pdf	Mr John Etherington
1	18 Dec 2025	Architectural Plans	20251218 X Golf Concept	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioPlan-62966-26.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioText-62966-26.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioPlan-27174-2.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioText-27174-2.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioPlan-230163-1.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioText-230163-1.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioPlan-198694-1.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioText-198694-1.pdf	Mr John Etherington

Schedule of Drawings/Documents

Schedule of Drawings/Documents

Prepared By	Drawings / Document No.	Revision No.	Date of Issue
S Group	Floor Plan	-	22/12/2025

TasWater Details

Officer Name Al Cole	Officer Title Senior Assessment Officer	Officer Contact Number 0439605108
--------------------------------	---	---

Submitted on 13/01/2026

Form published: 08/10/2025 08:54



PLANNING APPLICATION

Status: Submitted

Reference
 PLN-HOB-2025-0663

Address
 310 ARGYLE ST NORTH HOBART TAS 7000

Titles
 198694/2, 27209/1, 62966/26, 27174/2, 230163/1, 198694/1

Application Checklist

As part of the preliminary review of the application, at a minimum, please check:

- The right title and schedule of easements have been provided.
- The right fee has been paid.
- The plans are legible.
- The information provided by the applicant is correct/sufficient.
- Does it adjoin Council owned land? If so, please refer to Parks.

Please also don't forget to check for:

- TasWater
- TasNetworks
- Archaeology
- Contaminated sites
- Is it a Major Project?
- Who has delegation to determine the application

Checked By

Pre-Application Advice

Have you spoken with anyone at Council about this application?

- Yes - enter details below
- No - continue to the next section

If yes, provide the name of the person you contacted

Applicant

Name	Email	Phone	Address	Involvement
Mr John Etherington				Applicant

Owners

Name	Email Address	Address

Relevant Property Title/s

Selected Titles						Total Area: 0m²
198694/2	27209/1	62966/26	27174/2	230163/1	198694/1	

Owner Notification

Are you the sole owner of the land?

- Yes - continue to the next section
 No - answer question below

If no, have you notified all owners, joint or part owners of your intention to submit this application?

- Yes - enter owner details below
 No - you must notify all owners before proceeding with this application

List all owners, joint or part owners as recorded on the Title documents notified:

UPTONS SALVAGE PTY LTD

Enter the date that the last owner, joint or part owner was notified

18/12/2025

Declaration

- I declare that all land owners, joint or part owners have been notified of this planning application.

Crown Land Consent

Is Crown Land involved in the proposed use or development?

- Yes - complete question below
 No - continue to the next section - see further information below
 Unsure

If yes, has written Crown Land consent been obtained?

- Yes - upload written consent
 No - application will not be progressed until consent has been provided

General Manager Consent

Is Council-owned or administered land involved in the proposed use or development?

- Yes - complete question below
 No - continue to the next section
 Unsure

If yes, has written consent been obtained from the Council General Manager?

- Yes - upload written consent
 No - application will not be progressed until consent has been provided

Proposed Use or Development

What is the reason for your planning application?

- I want to change how the property is used
 I want to use the property for visitor accommodation
 I want to subdivide
 I want to undertake a new development or alteration
 I want to do a minor boundary adjustment
 I want to put up a sign(s)
 I want to demolish
 I want to do works only
 Other

If your application is to subdivide, please enter the number of proposed lots.

0

If your application is for signage, please enter the number of signs.

0

Is the property a Tasmanian Heritage Listed Property?

- Yes
 No

Is the application for an EPA Activity under the Environmental Management and Pollution Control Act 1994?

- Yes

- No
- Unsure

Is the proposed use or development permitted or discretionary?

- Permitted
- Discretionary
- Unsure if permitted or discretionary

Provide a full description of the proposed use or development

Change of use to sports & recreation (X Golf)
 Internal Alterations.

Will the proposed use or development involve a road reserve?

- Yes - complete the section below
- No - continue to the next section
- Unsure

If yes, enter the address(es) or locations below:

If yes, how will the road reserve be affected?

Value of Work

What is the estimated value of the works?

350000

Assessment Documents

Version	Document Date	Document Type	Description	Prepared By
1	19 Dec 2025	General	Application Form - redacted	Jacqui Loring
1	18 Dec 2025	Property Title Document	FolioText-198694-2.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioPlan-198694-2.pdf	Mr John Etherington
1	18 Dec 2025	Property Title Document	FolioPlan-27209-1.pdf	Mr John Etherington
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1	18 Dec 2025	Property Title Document	FolioText-198694-1.pdf	Mr John Etherington

Council Description

Council Proposed Use or Development Description

Partial Change of Use to Sports and Recreation

Assessment Timeframe

Assessment Timeframe

42 council business days

Requests / Referrals

No Referrals

PLANNING REPORT

Change of Use – Sport & Recreation (Indoor Golf
Driving Range)

310 Argyle Street, North Hobart

April 2026



Job Number: H241112

Prepared by: Alex Bowles
(alex@novaland.com.au)
Town Planner

Reviewed by: James Stewart
Senior Town Planner

Rev. no	Description	Date
1	Draft	1 April 2026
2	Final	16 April 2026



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1. Introduction

This report has been prepared in support of a planning permit application under Section 57 of the *Land Use Planning and Approval Act 1993* (the 'Act') to develop land at 310 Argyle Street, North Hobart (the 'subject site').

The proposal seeks approval for the intended use or development for a Change of Use – Sport & Recreation (Indoor Golf Driving Range) at the subject site. This report provides a detailed assessment of the site's characteristics, the proposed development's alignment with planning controls of the municipal area and considers any potential impacts.

This application is to be read in conjunction with the following supporting documentation:

Document	Consultant
Proposal Plans	S. Group
Traffic Impact Assessment	Midson Traffic
Lighting Plan	Middy's

1.1 Summary

Subject Site	
Address(es)	310 Argyle Street, North Hobart
Property ID	7660028
Title(s)	62966/26, 198694/1 & 27209/1
Planning Authority	Hobart City Council
Access	Access is via the existing crossover on Argyle Street
Planning Controls	
Zone	Light Industrial Zone (18.0)
General Overlay	HOB-S5.0 - Hobart Light Industrial Zone Specific Area Plan HOB-S10.0 - Royal Hobart Hospital Helipad Specific Area Plan – Area 4 – 100m AHD
Overlays	Electricity Transmission Infrastructure Protection Code (C4.0)
Proposal	
Proposed Use/Development	Change of Use – Sport & Recreation (Indoor Golf Driving Range)
Use Class	Sports and Recreation
Use Class Status	Discretionary



Existing services and infrastructure	
Water	Within a TasWater serviced area
Sewer	Within a TasWater serviced area
Stormwater	Within a stormwater serviced area, managed by a stormwater authority

2. Site Characteristics and Surroundings

2.1 Subject Site

The subject site is located at 310 Argyle Street, North Hobart. While addressed as a single property, the land comprises three separate titles that collectively function as one site for the purposes of the proposal. These include:

- CT 62966/26 - containing the existing building proposed to accommodate the indoor golf simulator facility;
- CT 198694/1 & 27209/1 - containing the existing on-site car parking area;

The site is situated within an established urban locality on the fringe of the Hobart CBD. Although zoned for industrial purposes, the surrounding area exhibits a mixed urban character, with nearby residential development, commercial premises and service-based activities. The site benefits from direct frontage to Argyle Street, a major arterial road that provides strong accessibility and connectivity to the broader Hobart area.

The site was previously used as a building supplies store, with the existing building and associated parking areas utilised to support the commercial operation.

Under the Tasmanian Planning Scheme - Local Provisions Schedule - Hobart, the land is zoned Light Industrial Zone (18.0). The site is also affected by the following planning overlays:

- HOB-S5.0 - Hobart Light Industrial Zone Specific Area Plan, which alters the applicable use table for the zone and identifies the proposed use as discretionary;
- HOB-S10.0 - Royal Hobart Hospital Helipad Specific Area Plan - Area 4 (100m AHD). As the proposal does not involve new building works or changes to existing building heights, this overlay is not materially relevant to the application; and
- Electricity Transmission Infrastructure Protection Code (C4.0). This code is not applicable, as no works are proposed within any transmission corridor adjoining or in front of the site.

The site contains some moderate slope towards Argyle Street, with site levels generally ranging between approximately 44 and 47 metres Australian Height Datum (AHD), with an approximate 5° slope.

The site is fully serviced and benefits from connection to established reticulated infrastructure networks. Water and sewer services are available to the land through the TasWater mains. These services have historically supported commercial activities on the site and are capable of continuing to service the proposed indoor recreation use without the need for substantial upgrades or new infrastructure.

Given the proposal primarily involves adaptive reuse of an existing building, no abnormal servicing demand is anticipated beyond that typically associated with a commercial tenancy of this nature.

The site is located within a stormwater serviced area managed by the relevant stormwater authority. As no substantial external works or expansion of impervious surfaces are proposed, the development is not expected to materially alter stormwater flows or create any adverse drainage impacts.

Overall, the subject site is an established, accessible and well-serviced property containing an existing commercial building and associated parking area. The presence of existing infrastructure connections and servicing capacity supports the suitability of the land for adaptive reuse as the proposed indoor golf simulator facility.

2.2 Images



Figure 1 - Aerial view of subject site (highlighted in blue). Source: LISTMAP



Figure 2 - Zoning of the subject site (highlighted in blue). Source: LISTMAP

3. Proposed Use and Development

3.1 Proposal

The proposal seeks approval for the use and development of an existing building for the purposes of an indoor golf simulator facility. The development will repurpose the building to provide an indoor recreation venue centred around simulated golfing experiences.

The facility will comprise six golf simulator bays, each accommodating up to six players at a time. Each simulator is designed as an open cubicle measuring approximately 5 metres by 5 metres, fitted with artificial turf, projection screens, and camera-based tracking technology.

Players will strike golf balls into a safety net, with simulator software replicating ball flight, trajectory, and gameplay outcomes in a virtual environment. The facility will enable users to participate in a range of virtual golf courses and formats, supporting both casual play and organised competitions within a fully enclosed indoor setting.

Bookings are typically made per simulator bay on an hourly basis, allowing for structured patron flow and efficient use of the facility. By way of example, patronage data from a comparable facility is provided below:

Table: Example Daily Patronage (Friday 20 February 2026 – XGolf Launceston)

Time	Number of Users
10:00am	0
11:00am	5
12:00pm	7
1:00pm	0
2:00pm	0
3:00pm	0
4:00pm	2
5:00pm	6
6:00pm	27
7:00pm	27
8:00pm	25
9:00pm	20
10:00pm	0
11:00pm	0
Total	119

This patronage data demonstrates that peak usage occurs in the evening period, reinforcing that the primary activity period aligns with typical leisure times outside standard business hours. It is noted that patronage use as shown above, is relative to a Friday. Weekday usage of the facility is generally less than what is provided on Friday and throughout the weekend.

The facility provides one (1) staff member onsite during daytime trading, with up to three (3) staff during peak evening and weekend periods. While the majority of users are considered casual participants, the facility will provide "league nights", in which teams compete in an X-Golf run competition. League nights would be expected to run between Monday-Wednesday, between 7:00pm – 9:00pm.

In addition to the simulator areas, the facility will include ancillary seating spaces that allow for dining and spectating separate from the play areas. A bar and kitchen are proposed as part of the development. These components are designed to provide food and beverage services exclusively to patrons utilising the golf simulators, enhancing the overall recreational experience by allowing users to socialise and remain on-site during their booking.

The food and beverage offering is ancillary and subordinate to the primary sports and recreation use of the site. It is not intended to operate as a standalone hospitality venue or attract independent patronage. Rather, it functions as a subservient use, consistent with similar indoor recreation facilities. This approach aligns with planning principles regarding subservient uses as articulated in decisions such as the *Cai v Launceston City Council / Gorge Hotel* matter, where services that directly support and serve the primary use were correctly characterised as ancillary rather than standalone uses in their own right.

Table: Proposed Hours of Operation

Day	Hours of Operation
Monday	10:00am – 10:00pm
Tuesday	10:00am – 10:00pm
Wednesday	10:00am – 10:00pm
Thursday	10:00am – 10:00pm
Friday	10:00am – 11:00pm
Saturday	10:00am – 11:00pm
Sunday	10:00am – 6:00pm

The table above provides a summary of proposed operating hours. These hours reflect anticipated peak demand periods and are consistent with the nature of the use as an indoor recreational and social venue.

The development will retain the existing 14 on-site car parking spaces. A Traffic Impact Assessment (TIA) has been prepared, which demonstrates that the site and surrounding road network are capable of accommodating the anticipated use, having regard to the proposed operating hours and the expected timing of user visits to the facility.

Servicing of the site from commercial vehicles will be minimal, with approximately four to five deliveries per week anticipated. All deliveries will occur during standard business hours, between 7:00am and 6:00pm on weekdays, with no weekend servicing proposed. Noting that these are generally the 'off-peak' times for the use, this ensures current parking provision is not unreasonably impacted by commercial vehicle movements during deliveries. This level of activity is considered low intensity and appropriate for the scale and nature of the development.

Signage associated with the proposed use is not included as part of this application. The location, scale and design of any signage are currently being developed and will be subject to separate approval process, if required. Accordingly, signage does not form part of this assessment.

Lighting associated with the proposal will utilise the existing external wall-mounted and pole lighting already established on the site. No significant additional external lighting is proposed. Existing lighting will be managed to ensure operation does not occur between 11:00pm and 6:00am. A lighting plan has been prepared by Middy's, and is included as an annexure to this submission. The lighting plan demonstrates no unreasonable light intrusion outside of the subject site.

Noise generated by the use is expected to be low and contained within the building. The activity occurs wholly indoors, with no outdoor entertainment or congregation areas proposed. Any background music will be incidental and maintained at a level that allows normal conversation. Noise associated with patrons and simulator use is not expected to be audible beyond the site to any unreasonable extent, particularly when considered in the

context of existing background traffic noise from Argyle Street and surrounding commercial activity.

Overall, the proposal represents a low-impact indoor recreation use that will activate an existing building, provide a controlled and contained leisure environment, and offer a flexible recreational facility suitable for a wide range of users.

3.2 Proposal plans



Figure 3 - Proposal Plan. Source: S Group

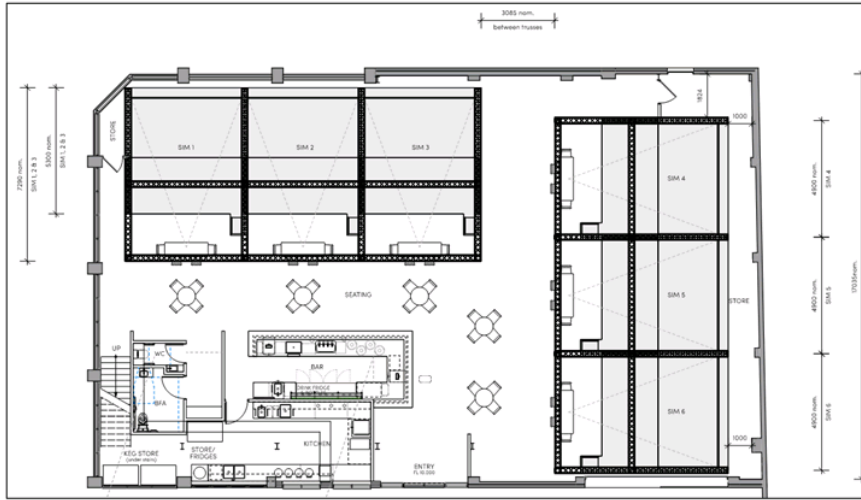


Figure 4 - Floor Plan. Source: S Group

A full set of plans is included with the application.

4. Planning Assessment

4.1 Planning Scheme Zone & SAP Assessment

18.0 Light Industrial Zone

18.1 Zone Purpose

The purpose of the Light Industrial Zone is:

- 18.1.1 To provide for manufacturing, processing, repair, storage and distribution of goods and materials where off site impacts are minimal or can be managed to minimise conflict with, or unreasonable loss of amenity to, any other uses.
- 18.1.2 To provide for use or development that supports and does not adversely impact on industrial activity.

Planners Response:

The proposed Sports and Recreation use is considered consistent with the purpose of the Light Industrial Zone.

The facility will operate entirely within an existing building and will not involve manufacturing, processing, or storage of goods or materials. All activity associated with the use is contained wholly within the building envelope, and the nature of the operation is such that off-site impacts will be minimal. There are no proposed activities that would generate unreasonable noise, emissions, odour or other amenity impacts on surrounding properties or uses.

The surrounding area along Argyle Street is characterised by a mixed pattern of land uses, including automotive services, office-based activities and commercial operations, in addition to traditional light industrial uses. The subject site has historically accommodated showroom and bulky goods retail activities, rather than intensive industrial operations, and the proposed use represents a similarly low-impact form of use within this context.

The proposed use will not adversely impact on surrounding industrial activity. The scale and operational characteristics of the facility are modest, and the use will not constrain or otherwise interfere with the ongoing operation or future development of adjoining or nearby industrial sites. The indoor and self-contained nature of the use, generally operating at peak times outside of typical business hours ensures that any potential for land use conflict is avoided.

Accordingly, the proposal is considered to support, and is compatible with, the purpose of the Light Industrial Zone.

HOB-S5.0 Hobart Light Industrial Zone-Specific Area Plan

HOB-S5.1 Plan Purpose

The purpose of the Hobart Light Industrial Zone Specific Area Plan is:

- HOB-S5.1.1 To provide for uses that are compatible with the unique, existing pattern of use in the area.
- HOB-S5.1.2 To provide for uses that minimise land use conflict in order to protect industrial viability and the safety and the amenity of sensitive land uses in adjacent zones.

Planners Response:

The proposal is considered to be consistent with the purpose of the *Hobart Light Industrial Zone Specific Area Plan*.

The proposed indoor golf simulator facility is a low-impact indoor recreation use that is capable of operating within the existing mixed-use context of the area without compromising

the viability of surrounding industrial and commercial activities. The use is contained wholly within an existing building and does not involve manufacturing, external storage, emissions, or operational characteristics that would interfere with the ongoing use or future development of nearby industrial land or the broader residential area.

The concept of a use being *compatible* is relevant to the purpose of the Specific Area Plan. Macquarie Dictionary considers 'compatible' to mean:

“being capable of existing together in harmony”; and

“capable of orderly, efficient integration with other elements of a system”

In this respect, the proposal is considered compatible with the surrounding area, as it can readily coexist alongside existing industrial, commercial and service-based activities without creating conflict with these areas or the surrounding residential areas.

The operating profile of the use further supports this outcome.

Peak patronage is expected to occur outside standard business hours, particularly during evenings and weekends, when demand from surrounding commercial and industrial uses is generally reduced. This temporal separation allows for a broader and more efficient utilisation of the precinct, including shared access to available parking and infrastructure, without adversely affecting established daytime business activity.

The proposal is also not expected to compromise the amenity of sensitive land uses in the adjoining Inner Residential Zone. Noise associated with the activity will be low and largely contained within the existing building. There are no outdoor congregation areas proposed and any background music will be incidental to the primary use, with operational noise considered minor in the context of existing elevated background noise from Argyle Street and surrounding urban activity.

Similarly, external lighting will remain confined to the site, with existing fittings retained and managed to prevent unreasonable spill beyond the property boundaries. As such, lighting impacts on nearby residential land are expected to be negligible.

The use also provides a transition or buffer between industrially zoned land and nearby sensitive uses by introducing a low-impact indoor activity that does not generate the same external impacts as more intensive industrial operations, such as Manufacturing and Processing or Service Industry uses that are 'Permitted' under the SAP.

Having regard to the above, the proposal is considered to integrate efficiently with the established pattern of development, minimise land use conflict, and satisfy the purpose of the Hobart Light Industrial Zone Specific Area Plan.

18.2 Use Table – Substituted with SAP - HOB-S5.5

Use Class	Qualification
No Permit Required	
Natural and Cultural Values Management	
Passive Recreation	
Utilities	If for minor utilities.
Permitted	
Emergency Services	
Equipment and Machinery Sales and Hire	If for hire of machinery and equipment.

Manufacturing and Processing	
Research and Development	
Service Industry	
Storage	
Discretionary	
Community Meeting and Entertainment	
Educational and Occasional Care	If for alterations or extensions to existing Educational and Occasional Care.
Food Services	
General Retail and Hire	If for alterations or extensions to existing General Retail and Hire.
Resource Processing	If for food and beverage production.
Sports and Recreation	
Transport Depot and Distribution	
Utilities	If not listed as No Permit Required.
Vehicle Fuel Sales and Service	
Prohibited	
All other uses	
<p>Planners Response:</p> <p>The proposed indoor golf simulator facility appropriately falls within the definition of ‘Sports and Recreation’ under the Tasmanian Planning Scheme – State Planning Provisions (See section 6.2 of the scheme), being the use of land for organised or competitive recreation or sporting purposes.</p> <p>The primary function of the premises is to provide a recreational golfing activity through the use of simulator technology. Patrons participate in a sport-based activity by striking golf balls into simulator screens, with the technology replicating ball flight, scoring outcomes and course play. This enables users to engage in golf in a structured and purpose-designed indoor environment. The activity is inherently recreational in nature and directly related to the sport of golf.</p> <p>The facility is intended to cater for both casual participation and organised competition, including social games, leagues, tournaments and other structured events. Accordingly, the use extends beyond passive entertainment and clearly involves organised and competitive sporting opportunities, consistent with the wording of the definition.</p> <p>The examples listed within the definition further support this classification. Uses such as a bowling alley, fitness centre, driving range and children’s play centre all involve purpose-built indoor or outdoor venues where patrons attend to participate in active recreation. The proposed golf simulator facility operates in a comparable manner, functioning as an indoor venue specifically designed for participation in a sporting activity.</p> <p>Any ancillary seating, food and beverage service, and patron amenities are subordinate to</p>	

the primary sporting use of the site and simply support the operation of the recreation facility. These elements do not alter the fundamental character of the premises as a sports and recreation use.

18.3 Use Standards

18.3.1 All uses

Objective: That uses do not cause an unreasonable loss of amenity to residential zones.

Acceptable Solutions	Performance Criteria
<p>A1</p> <p>Hours of operation of a use, excluding Emergency Services, Natural and Cultural Values Management, Passive Recreation or Utilities, on a site within 50m of a General Residential Zone, Inner Residential Zone, Low Density Residential Zone or Rural Living Zone, must be within the hours of:</p> <p>(a) 7.00am to 9.00pm Monday to Saturday; and</p> <p>(b) 8.00am to 9.00pm Sunday and public holidays.</p>	<p>P1</p> <p>Hours of operation of a use, excluding Emergency Services, Natural and Cultural Values Management, Passive Recreation or Utilities, on a site within 50m of a General Residential Zone, Inner Residential Zone, Low Density Residential Zone, or Rural Living Zone, must not cause an unreasonable loss of amenity to the residential zones, having regard to:</p> <p>(a) the timing, duration or extent of vehicle movements; and</p> <p>(b) noise, lighting or other emissions.</p>

Planners Response: Relies on performance criteria.

The application relies on the Performance Criteria, as the proposed operating hours extend beyond the Acceptable Solution and the site is located within 50 metres of an Inner Residential Zone.

Notwithstanding this, the proposed change of use is not considered to result in an unreasonable loss of amenity to nearby residential land.

Access to the site is via the existing crossover on the north-eastern boundary, connecting directly to Argyle Street. Argyle Street is a major urban arterial road forming part of Hobart's primary north-south movement network. The Traffic Impact Assessment prepared by Keith Midson notes that the road carries approximately 14,000 vehicles per day.

Given the established heavily trafficked environment, vehicle movements associated with the use between 9:00pm and 11:00pm are not expected to create unreasonable impacts. Peak departure activity is anticipated between 9:00pm and 10:00pm, only marginally outside the Acceptable Solution hours listed within A1, with movements after 10:00pm expected to be very limited. Vehicles will exit directly onto Argyle Street and are unlikely to idle or queue for extended periods.

The surrounding area already experiences regular traffic activity, lighting and urban movement, including a 24-hour fuel station located nearby.

Existing fencing, vegetation and parked vehicles further assist in screening headlights and vehicle activity from nearby dwellings. Accordingly, impacts from vehicle movements are considered negligible.

In terms of noise, the proposed use is a wholly indoor activity with no outdoor congregation or entertainment areas. Any background music within the premises will be low-level and incidental to the primary recreation use, allowing normal conversation and not intended to be audible externally.

The building has limited openings, being predominantly brick facing nearby residential properties, particularly toward the south and west, which assists in containing noise and reducing impacts to sensitive receivers further to these areas.

The existing background noise environment is already elevated due to the site's proximity to Argyle Street as a major arterial road, together with surrounding commercial and service-based activities like the 24-hour service station. In this context, operational noise generated by the proposed use will be negligibly impactful and not unreasonably affect residential amenity.

The proposal does not involve any significant changes to external lighting. Existing wall-mounted and pole lighting along the Argyle Street frontage will be retained. Lighting is intended to remain contained within the site, with no operation of external lighting between 11:00pm and 6:00am.

A lighting design for the existing light conditions is submitted with the application demonstrating negligible spill beyond the site boundaries, with lighting directed downward to prevent significant lighting throw toward any sensitive receptor, including those on the opposite side of Argyle Street. This existing lighting operating during these hours is not going to cause an unreasonable loss of amenity to near residential areas.

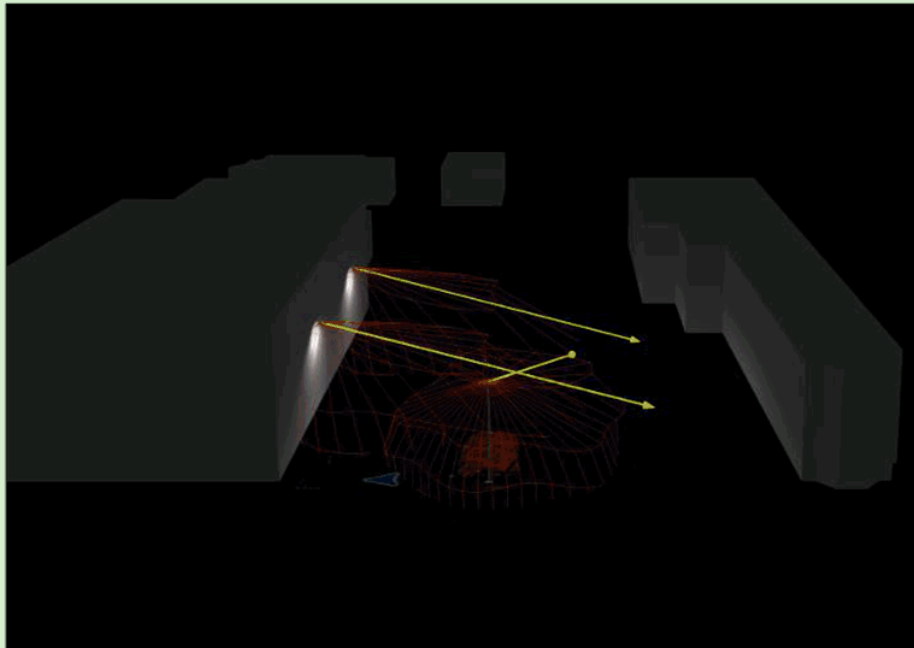


Figure 4 - Excerpt of Lighting Plan; Source: Middy's

In this context, and when considered alongside existing street lighting, adjacent commercial uses, and ongoing traffic activity along Argyle Street, the lighting associated with the proposal will integrate within the established urban environment without causing unacceptable impacts to adjoining residential properties.

Having regard to the nature of the use, the surrounding urban context, and the limited scale of emissions generated, the proposal is considered to satisfy the Performance Criteria P1.

A2

External lighting for a use, excluding Natural and Cultural Values Management or Passive Recreation, on a site within 50m of a General Residential Zone, Inner Residential Zone, Low Density Residential Zone or Rural Living Zone, must:

P2

External lighting for a use, excluding Natural and Cultural Values Management or Passive Recreation, on a site within 50m of a General Residential Zone, Inner Residential Zone, Low Density Residential Zone or Rural Living Zone, must not cause an unreasonable

<p>(a) not operate within the hours of 11.00pm to 6.00am, excluding any security lighting; and</p> <p>(b) if for security lighting, be baffled so that direct light does not extend into the adjoining property in those zones.</p>	<p>loss of amenity to the residential zones, having regard to:</p> <p>(a) the level of illumination and duration of lighting; and</p> <p>(b) the distance to habitable rooms of an adjacent dwelling.</p>
<p>Planners Response: Complies with acceptable solution.</p> <p>Existing external lighting on the site will not operate between the hours of 11:00pm and 6:00am.</p> <p>Accordingly, the proposal is considered to comply with the Acceptable Solution.</p>	
<p>A3</p> <p>Commercial vehicle movements and the unloading and loading of commercial vehicles for a use, excluding Emergency Services, on a site within 50m of a General Residential Zone, Inner Residential Zone, Low Density Residential Zone or Rural Living Zone, must be within the hours of:</p> <p>(a) 7.00am to 9.00pm Monday to Saturday; and</p> <p>(b) 8.00am to 9.00pm Sunday and public holidays.</p>	<p>P3</p> <p>Commercial vehicle movements and the unloading and loading of commercial vehicles for a use, excluding Emergency Services, on a site within 50m of a General Residential Zone, Inner Residential Zone, Low Density Residential Zone, or Rural Living Zone, must not cause an unreasonable loss of amenity to the residential zones, having regard to:</p> <p>(a) the time and duration of commercial vehicle movements;</p> <p>(b) the number and frequency of commercial vehicle movements;</p> <p>(c) the size of commercial vehicles involved;</p> <p>(d) manoeuvring required by the commercial vehicles, including the amount of reversing and associated warning noise;</p> <p>(e) any noise mitigation measures between the vehicle movement areas and the residential area; and</p> <p>(f) potential conflicts with other traffic.</p>
<p>Planners Response: Complies with acceptable solution.</p> <p>Servicing of the site from commercial vehicles will be minimal, with approximately four to five deliveries per week anticipated. All deliveries will occur during standard business hours, between 7:00am and 6:00pm on weekdays, with no weekend servicing proposed.</p> <p>These delivery times fall wholly within the prescribed hours outlined under A3 and therefore the proposal is considered to comply with the acceptable solution.</p>	



18.3.2 Discretionary uses

Objective:	That uses listed as Discretionary do not compromise the use or development of the land for industrial activities with minimal or managed off site impacts.
Acceptable Solutions	Performance Criteria
<p>A1</p> <p>No Acceptable Solution.</p>	<p>P1</p> <p>A use listed as Discretionary must not compromise the use or development of the surrounding properties for industrial activities with minimal or managed off site impacts, having regard to:</p> <ul style="list-style-type: none"> (a) the characteristics of the site; (b) the size and scale of the proposed use; and (c) the function of the industrial area.
<p>Planners Response: Relies on performance criteria.</p> <p>The proposed 'Sports and Recreation' use at 310 Argyle Street, North Hobart, will not compromise the use or development of surrounding properties for industrial activities, and will have minimal off-site impacts.</p> <p>The application relies on the performance criteria, as no acceptable solution is provided for. In assessing the proposal against P1, it is noted that the use is to occur wholly within an existing building and involves the conversion of a premises that has historically accommodated 'bulky good sales' activities, rather than traditional industrial operations.</p> <p>The site is located within a Light Industrial Zone; however, the surrounding area is characterised by a mixed pattern of land uses, including a service station and car yard to the north, business and professional services uses to the south-west, and vehicle servicing and automotive repair activities further along Argyle Street. The locality also interfaces with Inner Residential zoned land, which reflects its transitional and mixed-use character. As such, the area is not dominated by intensive industrial activity alone, but instead accommodates a broader range of commercial, service and light industrial uses.</p> <p>The proposed indoor golf simulator facility is a low-impact recreational use that will be contained entirely within the existing building envelope. The scale of the use is modest and will not distort the function of the industrial precinct. It will not involve external storage, manufacturing, emissions, significant noise, or other activities that would interfere with adjoining industrial premises or constrain their ongoing operation.</p> <p>Furthermore, the successful operation of comparable indoor golf and recreation facilities within light industrial areas of Launceston, including X-Golf Launceston (90 Gleadow Street) and WillFit Golf (81 Gleadow Street), demonstrates that sports and recreation uses of this nature can be effectively accommodated without adversely affecting the function or viability of the surrounding industrial estate.</p> <p>Accordingly, the proposal aligns with the evolving mixed-use character of this part of Argyle Street and will not compromise the ongoing use or future development potential of adjoining properties for industrial purposes.</p> <p>Accordingly, the proposal is considered to satisfy P1.</p>	



18.4 Development Standards for Buildings and Works

Planners Response: Not applicable to application.
 No buildings proposed. The change of use is proposed within an existing building. Therefore, this standard is not applicable.

18.5 Development Standards for Subdivision

Planners Response: Not applicable to application.
 The application does not propose subdivision of land. The proposal relates solely to a change of use within an existing building. Accordingly, this standard is not relevant to the assessment of the application.

4.2 Planning Scheme Code Assessment

C2.0 Car Parking and Sustainable Transport Code

C2.5 Use Standards

C2.5.1 Car parking numbers

Objective:	That an appropriate level of car parking spaces are provided to meet the needs of the use.
Acceptable Solutions	Performance Criteria
<p>A1</p> <p>The number of on-site car parking spaces must be no less than the number specified in Table 2.1, less the number of car parking spaces that cannot be provided due to the site including container refund scheme space, excluding if:</p> <ul style="list-style-type: none"> (a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan; (b) the site is contained within a parking precinct plan and subject to Clause C2.7; (c) the site is subject to Clause C2.5.5; or (d) it relates to an intensification of an existing use or development or a change of use where: <ul style="list-style-type: none"> (i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or 	<p>P1.1</p> <p>The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> (a) the availability of off-street public car parking spaces within reasonable walking distance of the site; (b) the ability of multiple users to share spaces because of: <ul style="list-style-type: none"> (i) variations in car parking demand over time; or (ii) efficiencies gained by consolidation of car parking spaces; (c) the availability and frequency of public transport within reasonable walking distance of the site; (d) the availability and frequency of other transport alternatives; (e) any site constraints such as existing buildings, slope, drainage, vegetation and landscaping; (f) the availability, accessibility and safety of on-street parking, having regard to the nature of the roads, traffic management

<p>(ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:</p> <p>$N = A + (C - B)$</p> <p>N = Number of on-site car parking spaces required</p> <p>A = Number of existing on site car parking spaces</p> <p>B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1</p> <p>C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1.</p>	<p>and other uses in the vicinity;</p> <p>(g) the effect on streetscape; and</p> <p>(h) any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development.</p> <p>P1.2</p> <p>The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:</p> <p>(a) the nature and intensity of the use and car parking required;</p> <p>(b) the size of the dwelling and the number of bedrooms; and</p> <p>(c) the pattern of parking in the surrounding area.</p>
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Planners Response: Relies on performance criteria.

The application relies on the Performance Criteria, as parking provision in accordance with Table C2.1 is not achieved. A Traffic Impact Assessment prepared by Midson Traffic supports the proposed parking provision and demonstrates that it is capable of meeting the reasonable needs of the use.

The proposal involves the change of use of an existing building from a former building supply/hardware store to a Sports and Recreation use (indoor golf simulator facility). The proposal provides 14 on-site car parking spaces located within the existing site layout, which previously supported the 'Bulky Goods Sales' use of the property.

There are no significant off-street public car parking facilities within convenient walking distance, however the site is supported by a combination of on-site parking and available on-street parking within the surrounding road network.

The use also exhibits strong temporal variation in parking demand. Weekday daytime demand is relatively low, while peak demand occurs during evening periods and weekends. This operating profile is complementary to surrounding land uses, which predominantly operate during standard business hours, allowing for efficient shared use of available on-street parking during peak periods of demand.

The site benefits from good access to public transport, with a bus stop located directly on Argyle Street adjacent to the site. Frequent services provide a viable alternative to private vehicle use, particularly for staff and patrons travelling during evening trading hours. In addition, the site is accessible via active transport options, including walking and cycling, supported by existing pedestrian infrastructure and bicycle lanes along Argyle Street. The nature of the use also supports ride-share and car-pooling arrangements, which are commonly associated with group recreational activities.

The site itself is constrained by the existing building footprint and established site layout, which limits the ability to increase on-site parking provision without significant redevelopment and loss of existing built form. As such, opportunities to expand parking on site are not practical.

Surveys of on-street parking in the surrounding area confirm that spaces are generally available, accessible and safe. During the evening period, when the proposed use generates its peak demand, there is typically significant capacity within the surrounding street

network to accommodate overflow parking demand.

The existing built form and site layout also contribute to maintaining the established streetscape character.

An assessment of parking demand indicates that typical weekday daytime demand is low, with higher demand occurring during peak evening trading periods. The 14 on-site spaces are sufficient to accommodate the majority of day-to-day demand, with only limited overflow occurring during peak times when surrounding on-street parking is available.

It is also relevant that the previous bulky goods use operated successfully with a similar level of parking provision without any identified traffic or parking issues. The proposed use is not expected to generate a higher level of traffic demand than the former use.

Having regard to the demonstrated demand profile, availability of surrounding on-street parking, access to public and active transport options, and the physical constraints of the site, the proposed parking provision is considered to be appropriate and sufficient to meet the reasonable needs of the use.

Accordingly, the proposal is consistent with the performance-based requirements for car parking.

C2.5.2 Bicycle parking numbers

Objective:	That an appropriate level of bicycle parking spaces are provided to meet the needs of the use.	
Acceptable Solutions	Performance Criteria	
<p>A1</p> <p>Bicycle parking spaces must:</p> <ul style="list-style-type: none"> (a) be provided on the site or within 50m of the site; and (b) be no less than the number specified in Table C2.1. 	<p>P1</p> <p>Bicycle parking spaces must be provided to meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> (a) the likely number of users of the site and their opportunities and likely need to travel by bicycle; and (b) the availability and accessibility of existing and any planned parking facilities for bicycles in the surrounding area. 	
<p>Planners Response: Complies with acceptable solution.</p> <p>There is no requirement for bicycle parking spaces under Table C2.1 for the proposed use.</p> <p>Accordingly, no dedicated bicycle parking is provided as part of the application. The proposal is therefore considered to comply with the Acceptable Solution.</p>		

C2.5.3 Motorcycle parking numbers

Objective:	That the appropriate level of motorcycle parking is provided to meet the needs of the use.	
Acceptable Solutions	Performance Criteria	

<p>A1</p> <p>The number of on-site motorcycle parking spaces for all uses must:</p> <ul style="list-style-type: none"> (a) be no less than the number specified in Table C2.4; and (b) if an existing use or development is extended or intensified, the number of on-site motorcycle parking spaces must be based on the proposed extension or intensification, provided the existing number of motorcycle parking spaces is maintained. 	<p>P1</p> <p>Motorcycle parking spaces for all uses must be provided to meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> (a) the nature of the proposed use and development; (b) the topography of the site; (c) the location of existing buildings on the site; (d) any constraints imposed by existing development; and (e) the availability and accessibility of motorcycle parking spaces on the street or in the surrounding area.
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Planners Response: Relies on performance criteria.

The application relies on the Performance Criteria, as no dedicated motorcycle parking is proposed.

The proposed use is a small-scale indoor recreation facility in which patrons predominantly attend in small groups and travel by private car, ride-share or public transport. Staff numbers are low, generally ranging between one and three persons. There is no evidence to suggest that a material proportion of patrons or staff will travel by motorcycle, and accordingly the reasonable demand for dedicated motorcycle parking is expected to be low.

The site is an established, level commercial tenancy with no topographical features that would generate a specific need for dedicated motorcycle parking infrastructure. The existing building footprint and compact car parking layout constrain the available external area, and there is limited opportunity to introduce formal motorcycle spaces without impacting the functionality of the existing 14 car parking spaces. No changes to the car parking layout are proposed as part of this application, and introducing a dedicated motorcycle bay would either reduce the existing car parking supply or create a non-standard arrangement within an already constrained site. Given the low expected motorcycle demand, such changes are not considered necessary for the functionality of the site.

The site is also located within an established corridor where on street parking is available in the surrounding road network. Motorcycles can generally be accommodated within on-street parking areas if required for overflow, providing a practical alternative to dedicated on-site provision.

Having regard to the small scale of the use, the low likelihood of significant motorcycle patronage, and the physical constraints of the existing site and parking layout, dedicated on-site motorcycle parking is not considered necessary to meet the reasonable needs of the use. The proposal is therefore considered to satisfy Performance Criteria P1.

C2.5.4 Loading Bays

<p>Objective:</p>	<p>That adequate access for goods delivery and collection is provided, and to avoid unreasonable loss of amenity and adverse impacts on traffic flows.</p>
<p>Acceptable Solutions</p>	<p>Performance Criteria</p>

<p>A1</p> <p>A loading bay must be provided for uses with a floor area of more than 1000m² in a single occupancy.</p>	<p>P1</p> <p>Adequate space for loading and unloading of vehicles must be provided, having regard to:</p> <ul style="list-style-type: none"> (a) the type of vehicles associated with the use; (b) the nature of the use; (c) the frequency of loading and unloading; (d) the location of the site; (e) the nature of traffic in the surrounding area; (f) the area and dimensions of the site; and (g) the topography of the site; (h) the location of existing buildings on the site; and (i) any constraints imposed by existing development.
<p>Planners Response: Not applicable to application.</p> <p>As per Clause C2.2.3, a dedicated loading bay is not required to be provided. Therefore proposal is not applicable to application.</p>	

C2.5.5 Number of car parking spaces within the General Residential Zone and Inner Residential Zone

<p>Objective:</p>	<p>To:</p> <ul style="list-style-type: none"> (a) facilitate the reuse of existing non-residential buildings within the General Residential Zone and Inner Residential Zone; and (b) to not cause an unreasonable impact on residential amenity by the car parking generated by that reuse.
Acceptable Solutions	Performance Criteria
<p>A1</p> <p>Within existing non-residential buildings in the General Residential Zone and Inner Residential Zone, on-site car parking is not required for:</p> <ul style="list-style-type: none"> (a) Food Services uses up to 100m² floor area or 30 seats, whichever is the greater; and (b) General Retail and Hire uses up to 100m² floor area, <p>provided the use complies with the hours of operation specified in the relevant Acceptable Solution for the relevant zone.</p>	<p>P1</p> <p>Within existing non-residential buildings in the General Residential Zone and Inner Residential Zone, the number of on-site car parking spaces must be sufficient to meet the reasonable needs of users and must not cause an unreasonable impact on residential amenity, having regard to:</p> <ul style="list-style-type: none"> (a) car parking demand generated by the proposed use during its proposed hours of operation; (b) the availability of on-street and public car parking in the surrounding area; (c) the availability and frequency of public transport within a 400m walking distance of the site; (d) the availability and likely use of other

	<p>modes of transport;</p> <ul style="list-style-type: none"> (e) the availability and suitability of alternative arrangements for car parking provision; (f) any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces; (g) any car parking deficiency or surplus associated with the existing use of the land; (h) any relevant parking plan for the area adopted by council; (i) any existing on-street car parking restrictions; and (j) the proportion of residential properties without off-street parking within a 100m radius of the subject site.
<p>Planners Response: Not applicable to application. Site is not within General Residential Zone and Inner Residential Zone. Therefore, not applicable to application.</p>	

C2.6 Development Standards for Buildings and Works

<p>Planners Response:</p> <p>The proposal involves a change of use only. No physical works or modifications to the existing car parking area, site access or internal circulation are proposed as part of this application.</p> <p>The site currently provides 14 on-site parking spaces, accessed via the established crossover to Argyle Street. This parking configuration has been in continuous operation for many years in association with the former building supply and bulky goods retail use of the site, and has functioned without identified issue during that time.</p> <p>As the proposal does not alter the parking layout, access arrangement, or number of spaces provided, the existing configuration will be retained in its current form. Given that no buildings or works are proposed in relation to car parking, no further assessment against the Development Standards for Buildings and Works is required.</p>

C2.7 Parking Precinct Plan

<p>Planners Response: Not applicable to application.</p> <p>Proposal is not located within a 'parking precinct plan' as identified within the a local provisions schedule .Therefore proposal is not applicable to application.</p>
--

C3.0 Road and Railway Assets Code

C3.5 Use Standards

C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction

Objective:	To minimise any adverse effects on the safety and efficiency of the road or rail network from vehicular traffic generated from the site at an existing or new vehicle crossing or level crossing or new junction.
Acceptable Solutions	Performance Criteria
<p>A1.1 For a category 1 road or a limited access road, vehicular traffic to and from the site will not require:</p> <ul style="list-style-type: none"> (a) a new junction; (b) a new vehicle crossing; or (c) a new level crossing. <p>A1.2 For a road, excluding a category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority.</p> <p>A1.3 For the rail network, written consent for a new private level crossing to serve the use and development has been issued by the rail authority.</p> <p>A1.4 Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than:</p> <ul style="list-style-type: none"> (a) the amounts in Table C3.1; or (b) allowed by a licence issued under Part IVA of the <i>Roads and Jetties Act 1935</i> in respect to a limited access road. <p>A1.5 Vehicular traffic must be able to enter and leave a major road in a forward direction.</p>	<p>P1 Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:</p> <ul style="list-style-type: none"> (a) any increase in traffic caused by the use; (b) the nature of the traffic generated by the use; (c) the nature of the road; (d) the speed limit and traffic flow of the road; (e) any alternative access to a road; (f) the need for the use; (g) any traffic impact assessment; and (h) any advice received from the rail or road authority.
<p>Planners Response: Complies with acceptable solution.</p> <p>As detailed in the Traffic Impact Assessment prepared by Midson Traffic, the proposed development is expected to generate approximately 66 to 68 two-way vehicle movements per day on average, with peak activity occurring during evening periods when up to approximately 25 vehicle arrivals may occur within the peak hour.</p> <p>This level of traffic generation is substantially lower than that associated with the former building supply and bulky goods use of the site, which is estimated to have generated approximately 234 vehicle movements per day. The proposal therefore represents a net</p>	

reduction in daily traffic volumes using the existing vehicle crossing.

The existing access to Argyle Street has operated without issue for many years, and sight distance at the crossing has been confirmed to comply with AS2890.1 requirements. No changes to the access arrangement are proposed.

As the proposed use results in a net reduction in vehicular traffic relative to the previous use, the proposal satisfies Acceptable Solution A1.4.

C3.6 Development Standards for Buildings or Works

C3.6.1 Habitable buildings for sensitive uses within a road or railway attenuation area

Planners Response: Not applicable to application.

Not applicable. The proposal does not involve sensitive use structures and is not located within a road or railway attenuation area. Not applicable.

C3.7 Development Standards for Subdivision

C3.7.1 Subdivision for sensitive uses within a road or railway attenuation area

Planners Response: Not applicable to application.

The proposal does not involve subdivision of land and is not located within a road or railway attenuation area. Not applicable.

C14.0 Potentially Contaminated Land Code

Planners Response:

The proposed development involves minimal earthworks, with the volume of excavation confirmed to be less than 3m³. Any earthworks will be limited to those necessary for the installation of hydraulic services, the exact location of which is yet to be determined. The extent of site disturbance is expected to be extremely minimal.

In relation to C14.5, the proposed use does not constitute a Sensitive Use as defined under the Code, and accordingly C14.5.1 is not applicable to this application.

With respect to C14.6.1, the proposal involves less than 250m³ of site disturbance and therefore complies with Acceptable Solution A1. No further assessment under this provision is required.

5. Conclusion

The proposed indoor golf simulator facility at 310 Argyle Street, North Hobart, represents a low-impact Sports and Recreation use that is considered appropriate within the Light Industrial Zone having regard to the mixed commercial character that has evolved along this section of Argyle Street.

The application has been assessed against the relevant provisions of the *Tasmanian Planning Scheme - Hobart*, including the zone purpose, applicable use standards, and relevant codes. The assessment demonstrates that the proposal either complies with the Acceptable Solution for each applicable standard, or satisfies the relevant Performance Criteria where reliance on those criteria is necessary.

The facility will operate entirely within an existing building, with no significant external works proposed. The use is self-contained and will not generate unreasonable off-site impacts in terms of noise, traffic, amenity or land use conflict. Daily traffic volumes associated with the proposed use are substantially lower than those generated by the former use of the site, and the existing parking provision has been demonstrated to meet the reasonable needs of the use having regard to actual operational demand, temporal variation in parking activity, and the availability of on-street parking during peak periods.

The food and beverage component of the facility is ancillary and subordinate to the primary Sports and Recreation use, and does not constitute an independent land use requiring separate assessment. The proposed hours of operation are appropriate for the nature of the use and will not give rise to unreasonable amenity impacts on surrounding properties.

Overall, the proposal represents an appropriate and compatible use of the site that will activate an existing building, contribute to the viability of the locality, and provide an indoor recreational facility that serves a broad community of users. On the basis of the assessment contained within this report, planning approval is recommended.

- Annexure 1 - Certificate of Title Plan and Folio Text**
- Annexure 2 - Proposal Plans**
- Annexure 3 - Traffic Impact Assessment**
- Annexure 4 - Lighting Plan**



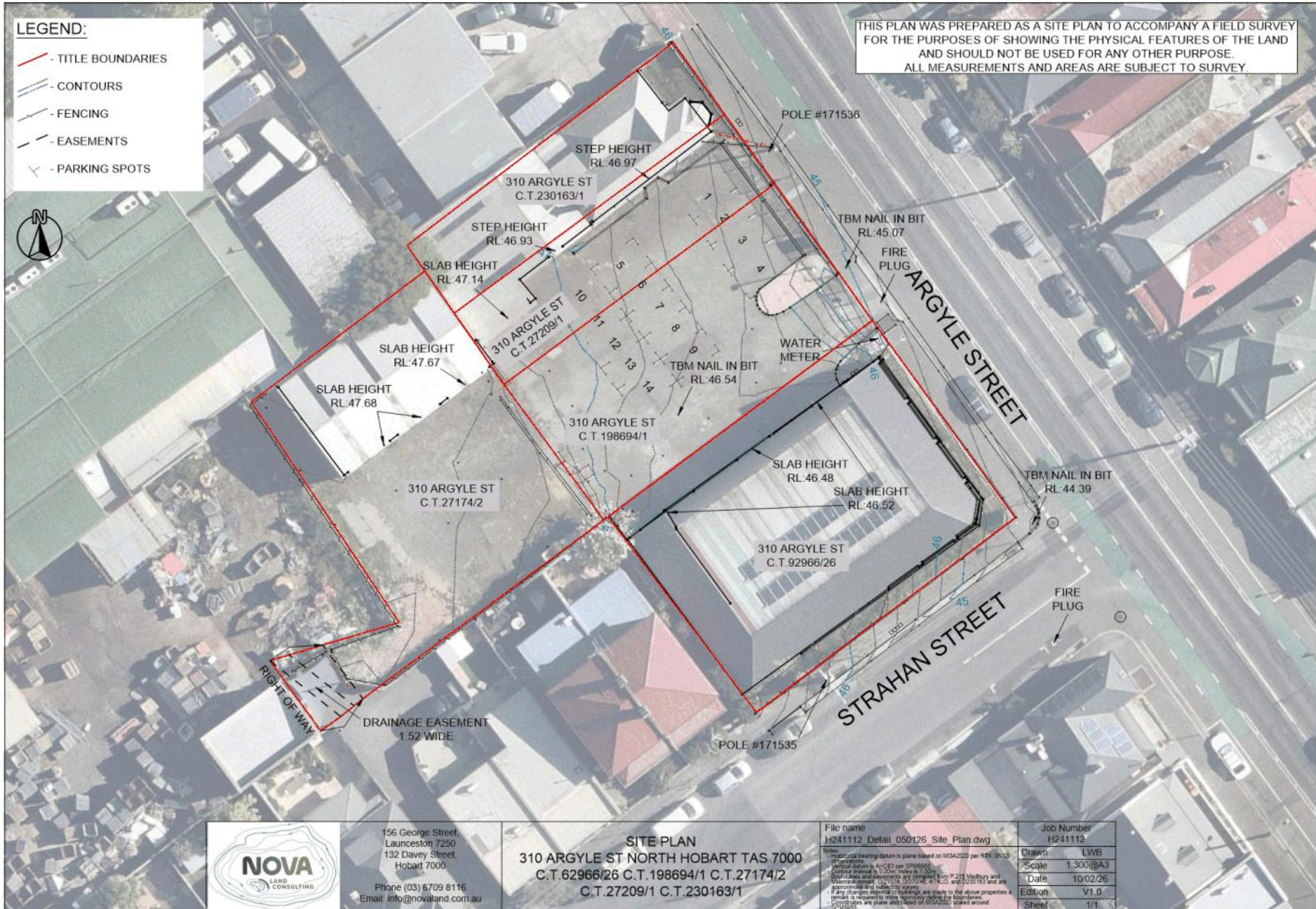
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SITE PLAN
 310 ARGYLE ST NORTH HOBART TAS 7000
 C.T. 62966/26 C.T. 198694/1 C.T. 27174/2
 C.T. 27209/1 C.T. 230163/1

File name
 H241112_Detail_050126_Site_Plan.dwg
Notes:
 Horizontal bearing datum is plane based on MGA2020 per RTK GNSS observations.
 Vertical datum is AGLD11 per SP48990.
 Contour interval is 0.2m, from 0 to 1.0m.
 Boundaries and easements are compliant with © 218 (Masonry and electrical) and © 219 (Water) of the Resource Management Act 1992.
 If any changes essential to planning, are made to the above properties a further site plan is required to be prepared and approved by the Council.
 Coordinates are plane and based on MGA2020 scaled around 1:100000.

Job Number	H241112
Drawn	LWB
Scale	1:300@A3
Date	10/02/26
Edition	V1.0
Sheet	1/1

LINCOLN GROUP PTY LTD

X GOLF HOBART
 312 ARGYLE STREET

DRAWING SCHEDULE:

Sheet No:	Drawing:	Rev:	Revision Date:
A001	Cover		
A101	Site Plan		
A201	Existing & Demolition Plan		
A202	Floor Plan		
A203	Roof Plan		
A204	Bar & Kitchen Plan		
A501	Elevations		
A502	Sections		
A503	3D Render		
A901	General Notes		

GENERAL INFORMATION:

Accredited Architect:	Sam Haberle	
Accreditation Number:	CC5618 U	
Land Title Reference Number:	C.T. 62966/26	(Certificate volume and folio)
Soil classification:	(Refer Eng.)	Site classification to AS 2870-2011 (Reference report author)
Wind Classification:	TBA	Site classification to AS 4055-2006 (Reference report author)
Climate Zone:	7	(www.abcb.gov.au/map)
Alpine Area:	N/A	<300m AHD (NCC section H7D3)
Bushfire-prone Area BAL Rating:	N/A	As determined by registered Bushfire Assessor
Corrosion environment:	N/A	For steel subject to the influence of salt water, breaking surf or heavy industrial areas, refer to NCC section H1D6 Framing, Cladding and fixings to manufacturer's recommendations
Other Known site hazards:	N/A	High wind, earthquake, flooding, landslip, dispersive soils, sand dunes, mine subsidence, landfill, snow & ice or other relevant factors
Site Area:	2220m²	
Total Floor Area:	467m²	



LOCATION PLAN

APPROVAL ISSUE



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 6/100 Elizabeth St, Hobart
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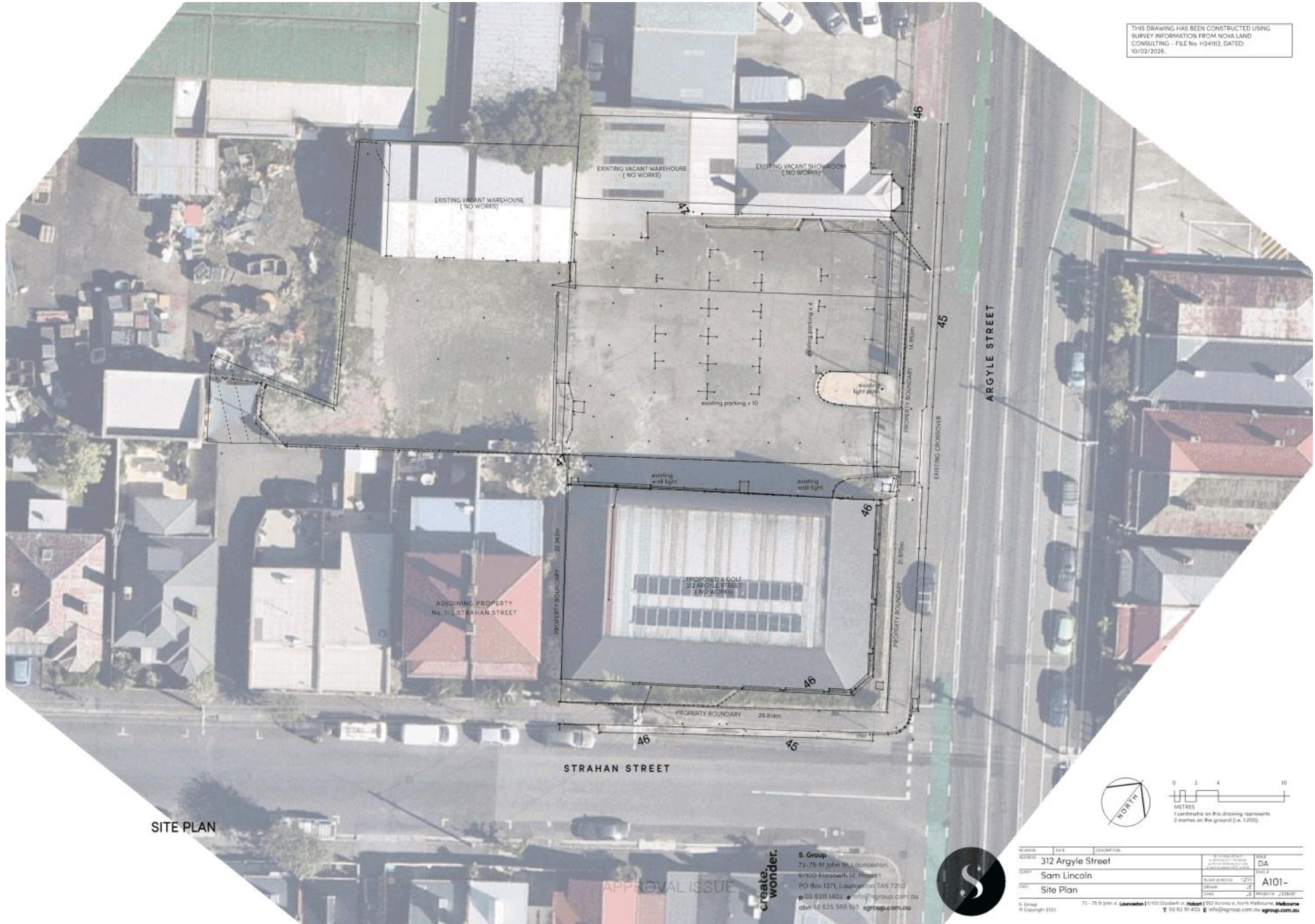


ADDRESS	DATE	DESCRIPTION	SCALE
312 Argyle Street		4 STOREY BUILDING	DA
Sam Lincoln		RECALL OF RECORD	1:1
Cover		RECALL OF RECORD	1:1

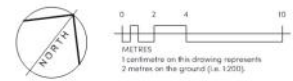
A001 -
 COVER

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10/03/2026.



SITE PLAN



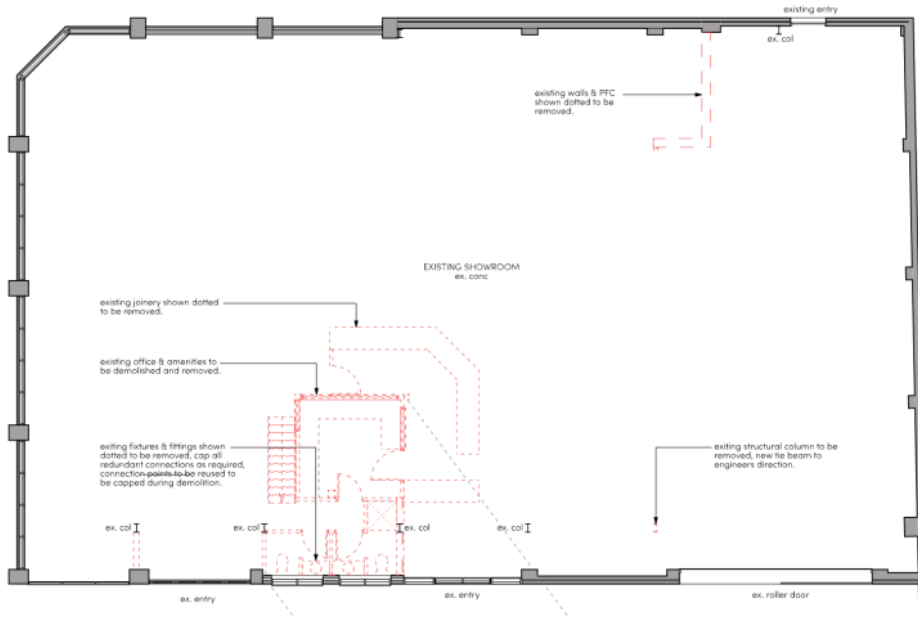
APPROVAL ISSUE

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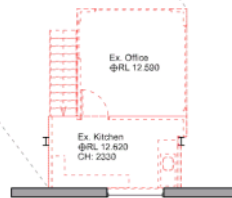
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STATUS	DATE	DESCRIPTION	BY	DATE
APPROVED		312 Argyle Street	DA	
OWNER		Sam Lincoln	A101-	
DATE		Site Plan		



EXISTING & DEMOLITION PLAN



EXISTING & DEMOLITION PLAN

WARNING
 BEWARE OF UNDERGROUND SERVICES. LOCATE ALL EXISTING SERVICES PRIOR TO CONSTRUCTION. THE LOCATION SHOWN ON PLANS ARE INDICATIVE ONLY AND PRECISE LOCATION SHOULD BE PROVEN ON SITE. NO GUARANTEE IS GIVEN THAT ALL SERVICES ARE SHOWN ON PLAN.



WALL LEGEND	
MARK	DESCRIPTION
—	EXISTING WALLS TO BE RETAINED.
- - - - -	EXISTING WALLS TO BE DEMOLISHED.
ex.FHR	EXISTING FIRE HOSE REEL (FHR).
ex.FE	EXISTING WALL MOUNTED FIRE EXTINGUISHER.
x 20.123	EXISTING SPOT LEVEL.

DEMOLITION NOTES

GENERALLY DEMOLITION WORKS MUST BE CARRIED OUT IN ACCORDANCE WITH AS 2501-2001: DEMOLITION OF STRUCTURES & REGULATIONS IS OF THE BUILDING REGULATIONS (Tas.) 2016.

PROCEDURES & METHODS OF DEMOLITION MUST BE ADEQUATE TO PREVENT INJURY TO PERSONS & AVOID DAMAGE TO NEIGHBORING PROPERTIES.

ALL DEMOLISHED MATERIALS, RUBBISH & DEBRIS SHALL BE REMOVED FROM THE SITE ON COMPLETION OF DEMOLITION.

BEFORE REMOVING EXISTING WALLS SHOWN TO BE DEMOLISHED, BUILDER SHALL CONFIRM ON-SITE WHETHER THEY ARE LOADBEARING / BRACING OR NOT.

IF IT IS FOUND THAT THEY ARE LOADBEARING / BRACING, A STRUCTURAL ENGINEER MUST BE ENGAGED TO DETERMINE ANY BEAMS REQUIRED TO SUPPORT THESE EXISTING LOADS.

ALL REDUNDANT STORMWATER, SEWER AND WATER CONNECTIONS ASSOCIATED WITH THE DEMOLITION SHALL BE CUT & SEALED TO THE SATISFACTION OF COUNCIL'S SENIOR PLUMBING INSPECTOR.

THE REMOVAL OF EXISTING PLUMBING FIXTURES SHALL INCLUDE ALL ASSOCIATED WASTE & VENT PIPES, FLOOR DRAINS, WATER SERVICE PIPEWORK BRACKETS, SUPPORTS etc. & SEAL OFF EXISTING SERVICES, SEAL OFF & MAKE GOOD ALL FLOOR, WALL & ROOF PENETRATIONS.

GENERALLY, MAKE GOOD TO EXISTING FLOORS, WALLS & CEILINGS WHERE ALL DEMOLITION WORK OCCURS TO MATCH EXISTING AS & WHERE REQUIRED.

ASBESTOS NOTES

IMPORTANT:

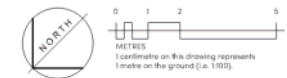
IDENTIFY ASBESTOS OR ASBESTOS CONTAINING MATERIALS (ACM'S) AT THE SITE.

IF IN PLACE, REFER TO THE ASBESTOS REGISTER FOR DETAILS.

IF NOT IN PLACE, A THOROUGH INSPECTION IS TO BE UNDERTAKEN BY A SUITABLY 'COMPETENT' PERSON & RECORDED IN DETAIL IN AN ASBESTOS REGISTER.

ASBESTOS REMOVAL SHALL BE CARRIED OUT BY QUALIFIED PERSONNEL ONLY IN ACCORDANCE WITH RELEVANT STANDARDS / CODES OF PRACTICE (PREFERABLY CLASS A ASBESTOS REMOVAL LICENCE HOLDERS ONLY), WITH A THOROUGH ASBESTOS MANAGEMENT PLAN IN PLACE.

FURTHERMORE, IF MORE THAN 10 sqm. OF ASBESTOS IS TO BE REMOVED A VISUAL CLEARANCE CERTIFICATE FORM No. AR4 IS TO BE COMPLETED & SUBMITTED TO THE BUILDING SURVEYOR. RECORDS OF CORRECT DISPOSAL ARE TO BE SUBMITTED TO THE BUILDING SURVEYOR FOR ANY ASBESTOS REMOVED.



APPROVAL ISSUE

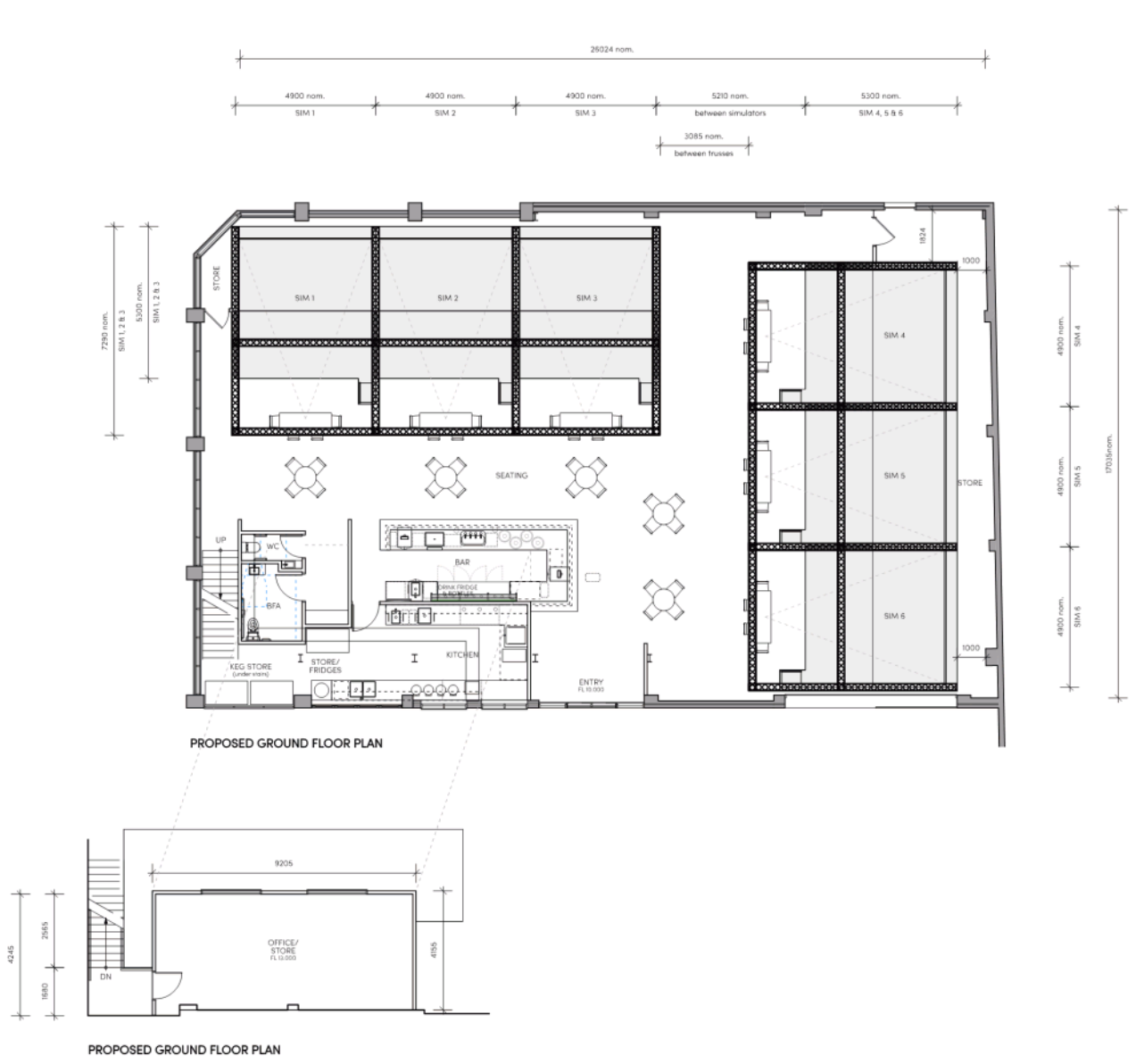
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PROJECT	DATE	DESCRIPTION	APPROVED BY	SCALE
ADDRESS			DA	
		312 Argyle Street		
OWNER		Sam Lincoln		
DRAWN		Existing & Demolition Plan		
CHECKED				
DATE				
				A201-

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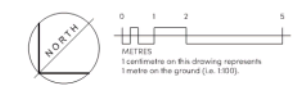


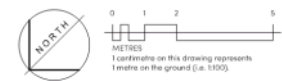
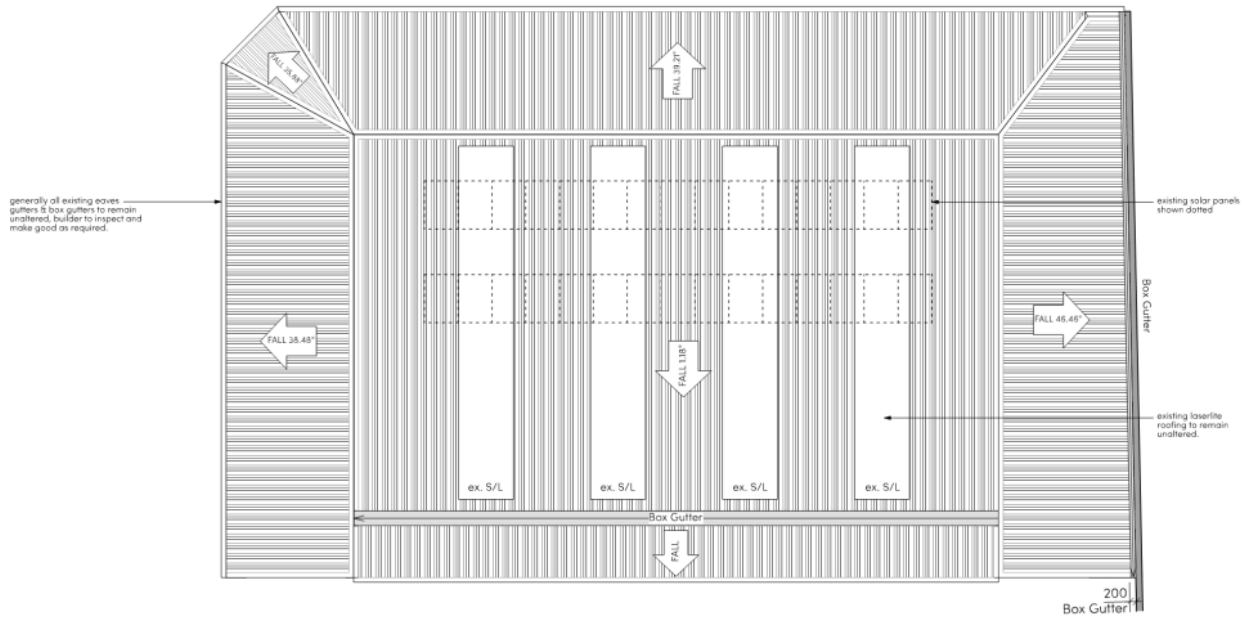
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PROJECT	DATE	DESCRIPTION	SCALE	BY
312 Argyle Street				DA
CLIENT				
Sam Lincoln			1:100	A202 -
DRAWN				

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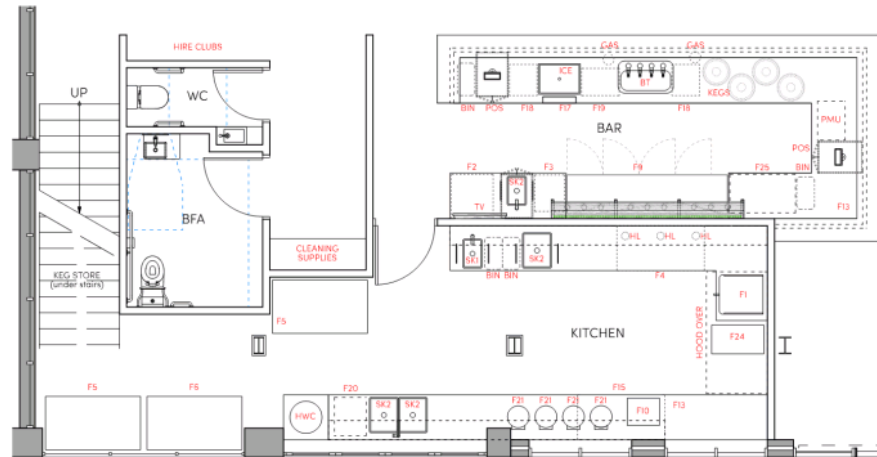
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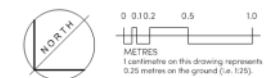


ADDRESS	DATE	DESCRIPTION	BY
312 Argyle Street		4/100 Elizabeth St, Hobart	DA
CLIENT			
Sam Lincoln		6/100 Elizabeth St, Hobart	A203-
DRAWN			
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LEGEND:

KEY	DESCRIPTION	POWER
F1	CONVECTION OVEN	6kW 25A
F2	ICE MACHINE	
F3	GLASS WASHER	
F4	2 DOOR U/C FRIDGE	10A 220-240V
F5	2 DOOR FREEZER	10kW 220-240V
F6	2 DOOR CHILLER	4.15kW 220-240V
F8	1 DOOR U/C FRIDGE	
F9	4 DOOR U/C FRIDGE	7.0kW 220-240V
F10	MICROWAVE	1000W 10A
F13	STEEL BENCHES	N/A
F14	GAS FRYER	N/A
F15	2 DOOR U/C FREEZER	7.8kW 220-240V
F17	8 BOTTLE SPEED RACK	N/A
F18	GLASS RACK	N/A
F19	ANDALE ICE BANK	N/A
F20	DISHWASHER	
F21	PIZZA OVEN	1200W 220-240V
F22	STAINLESS STEEL SHELF	N/A
F23	1 DOOR GLASS CHILLER	0.37A 220-240V
F24	ELECTRIC FRYER	
F25	GLASS CHILLER	
KEG	KEGS	
BIN	BIN	
BH	RANGEHOOD	
PMU	POST MIX UNIT	N/A
HL	HEAT LAMP	
PGC	POS MACHINE	
ICE	ICE WELL WITH POST MIX GUNS	N/A
BT	BEER TAP	
MX1	MIXER	
MX2	MIXER	
SK1	SINK: 450x300x250mm	N/A
SK2	SINK: 550x450x300mm	N/A
SU	SHELFING UNIT	
TV	SELECTED TV	TBC
GAS	SIZE D NITROGEN	



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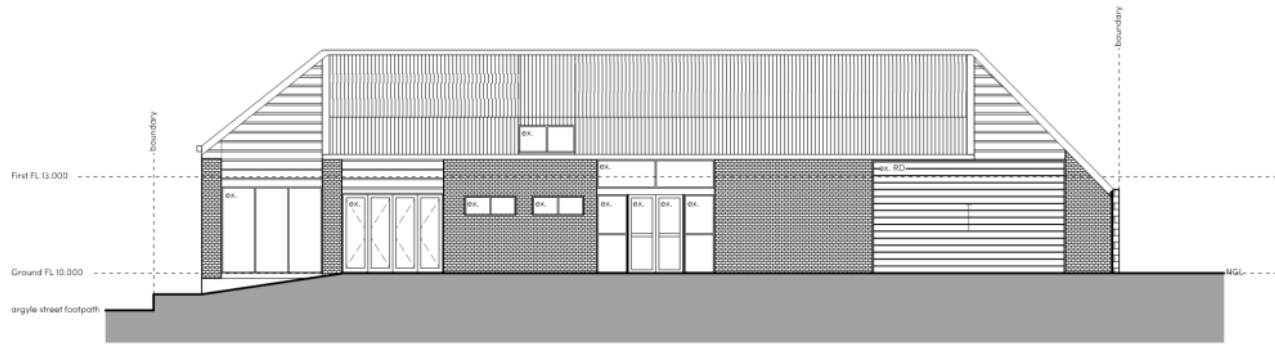
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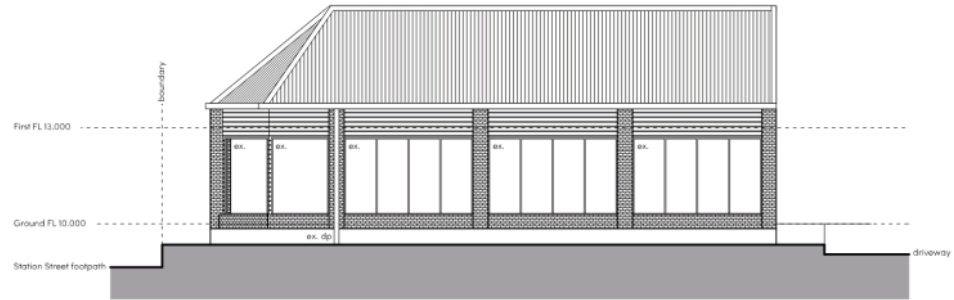
ADDRESS	DATE	DESCRIPTION	BY	DATE
312 Argyle Street			DA	
CLIENT			DA	
Sam Lincoln			DA	
DRAWN			DA	
Bar & Kitchen Plan			DA	
			DA	

A204-

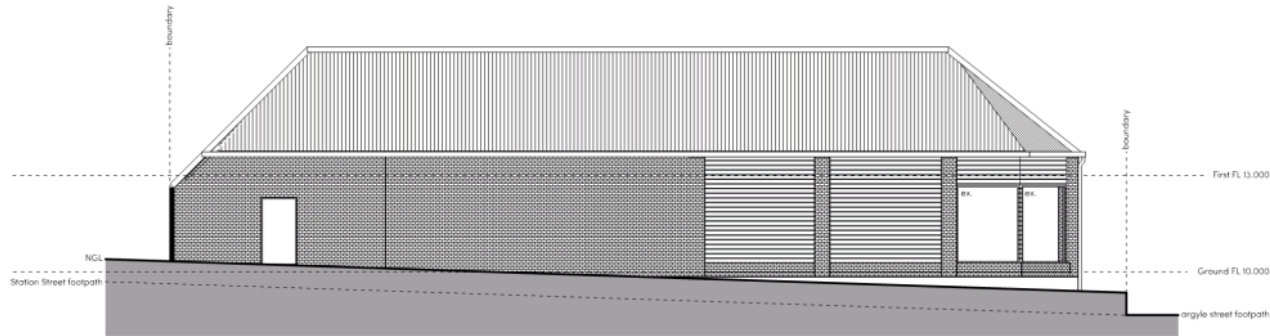
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NORTH-WEST ELEVATION



ARGYLE STREET ELEVATION



STATION STREET ELEVATION

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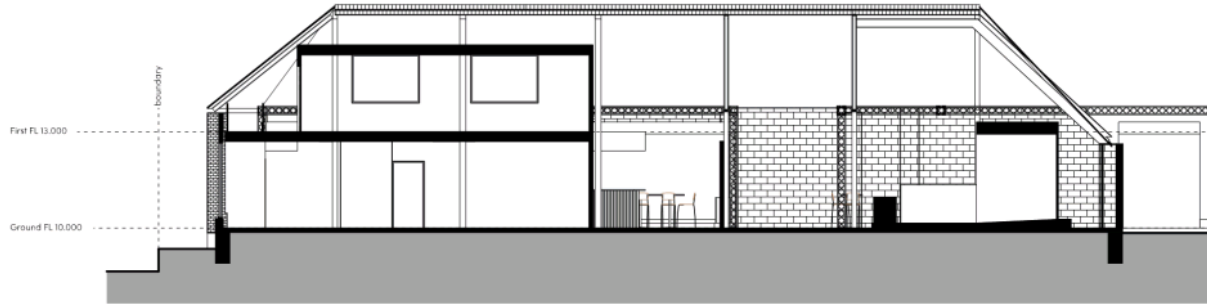
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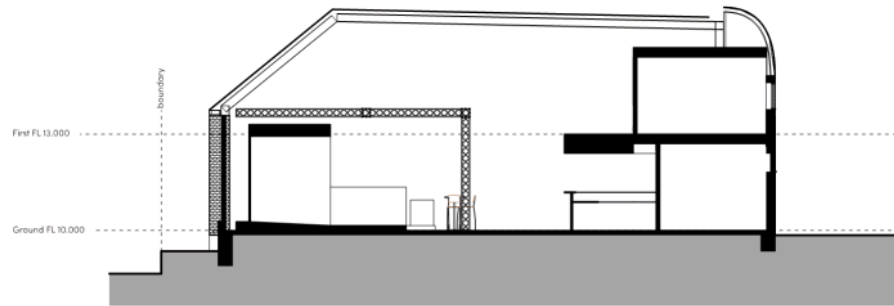


PROJECT	DATE	DESCRIPTION	SCALE
ADDRESS			
312 Argyle Street		4710222404	DA
OWNER		Sam Lincoln	1:100
DESIGNER		Sam Lincoln	DA
DATE		27/06/2023	

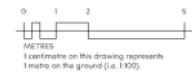
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SECTION A



SECTION B



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PROJECT	DATE	DESCRIPTION
ADDRESS	312 Argyle Street	4-11/2024/0101 DA
OWNER	Sam Lincoln	SCALE 0/50/100 1/100
DATE	2024	DRAWN JZ
		CHECKED JZ
		DATE 2024-06-27 10:00
A502 - Sections		
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PROJECT	DATE	DESCRIPTION	
312 Argyle Street		DA	
CLIENT			
Sam Lincoln		A503-	
DRAWN			
3D Render			

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<p>GLAZING NOTES</p> <p>PROVIDE CERTIFICATION FOR THE WINDOWS AND GLAZING AT THE COMPLETION OF THE BUILDING WORKS TO VERIFY COMPLIANCE WITH THE REQUIREMENTS OF AS 1288 (table 5).</p> <p>FULLY FRAMED GLAZING SHALL BE 'GRADE A SAFETY GLASS IN ACCORDANCE WITH TABLE 5.1 OF AS 1288-2006 (GLASS IN BUILDINGS - SELECTION & INSTALLATION).</p> <p>N.B. ALLOW FOR APPLIED WINDOW TINTING FILM AS DIRECTED BY 'TECHN'.</p>
<p>AS 1428.1-2009 - CLAUSE 13.1 LUMINANCE CONTRAST OF DOORWAYS</p> <p>ALL DOORWAYS SHALL HAVE A MINIMUM LUMINANCE CONTRAST OF 30% PROVIDED BETWEEN -</p> <p>(a) DOOR LEAF AND DOOR JAMB; (b) DOOR LEAF AND ADJACENT WALL; (c) ARCHITRAVE AND WALL; (d) DOOR LEAF AND ARCHITRAVE; or (e) DOOR JAMB AND ADJACENT WALL.</p> <p>THE MINIMUM WIDTH OF THE AREA OF LUMINANCE CONTRAST SHALL BE 500mm. REFER TO SPECIFICATIONS &/OR LUMINANCE CONTRAST CALCULATOR BY INTERIOR DESIGNER FOR FULL DETAILS.</p>
<p>PAINTING SCHEDULE</p> <p>NEW AND EXISTING WALLS: 1 COAT PLASTERBOARD SEALER 2 COATS OF 'DULUX' LD-SHEEN WASHABLE LATEX IN SELECTED COLOUR. ALLOW FOR 3 FEATURABLE WALL COLOURS. LOCATION TO BE CONFIRMED WITH CLIENTS.</p> <p>NEW AND EXISTING TIMBER DOORS & TIMBER WINDOWS: 1 COAT OF 3 IN 1 (PRIMER/SEALER/UNDERCOAT) 3 COATS OF SEMI-GLOSS ENAMEL IN SELECTED COLOUR. PAINTER TO ENSURE TOP & BOTTOM OF ALL DOORS ARE PAINTED.</p> <p>EXISTING CEILING GRID (WHERE APPLICABLE): 1 COAT OF SEMI-GLOSS ENAMEL.</p>
<p>FLOOR COVERING NOTES</p> <p>SUBMIT MANUFACTURERS DETAILS FOR ANY NEW CARPET &/OR FLOOR VINYL SHOWING THE FIRE HAZARD PROPERTIES TO ALLOW ASSESSMENT WITH SPECIFICATION C2019 OF THE NCC. PRIOR TO INSTALLATION.</p> <p>FLOOR COVERINGS / SURFACES SHALL ALSO COMPLY WITH PART 7 OF AS 1428.1-2009 (DESIGN FOR ACCESS & MOBILITY).</p> <p>WHERE APPLICABLE ALLOW FOR TREADSAFE DT204 NATURAL ANODISED TRANSITION TRIM AT ALL JUNCTIONS BETWEEN CARPET AND VINYL.</p> <p>ALLOW FOR CONTACT BACK FLOOR PROTECTION TO ALL FLOORING DURING CONSTRUCTION.</p>

NCC 2022 - CLASS 2 to CLASS 9 BUILDINGS (GENERAL NOTES)	
<p>GENERAL NOTES</p> <p>WHEN CARRYING OUT THE BUILDING WORK, A BUILDER (or owner-builder) SHOULD BE FAMILIAR WITH GENERAL CONSTRUCTION PRACTICES, THE REQUIREMENTS OF THE NATIONAL CONSTRUCTION CODE (NCC 2022), AS WELL AS LOCAL, COUNCIL, RULES / BYE-LAWS.</p> <p>A COPY OF ALL PLANNING, BUILDING & PLUMBING PERMITS, AND DRAWINGS STAMPED 'APPROVED' BY THE LOCAL AUTHORITY MUST BE KEPT ON SITE.</p>	<p>D3D26 - OPERATION OF LATCH</p> <p>ALL EXIST DOORS SHALL BE FITTED WITH LOCKWOOD 3072 VISIBLE BAR LOCKS AND LEVER LATCHES (OR EQUIVALENT TYPE) - TO COMPLY WITH CLAUSE D3D26 OF THE NCC 2022.</p> <p>E1D14 - PORTABLE FIRE EXTINGUISHERS</p> <p>FIT PORTABLE FIRE EXTINGUISHERS IN ACCORDANCE WITH PART E1D14 OF THE NCC 2022.</p> <p>PORTABLE FIRE EXTINGUISHERS ARE TO BE SELECTED & INSTALLED IN ACCORDANCE WITH AS 2444. REFER TO FLOOR PLAN FOR LOCATIONS.</p> <p>E4 - EMERGENCY LIGHTING / EXIT SIGNS</p> <p>LUMINATED EXIT SIGN FITTED DIRECTLY ABOVE DOORS OR WHERE SHOWN ON ELECTRICAL PLANS (BY OTHERS) - TO COMPLY WITH AS 2281.1 AND PART E4 OF THE NCC 2022 (eg. 'LEGRAND' TYPE ER EXIT SIGN or SIMILAR).</p> <p>F2 - WET AREAS & OVERFLOW PROTECTION</p> <p>WATERPROOFING OF WET AREAS TO COMPLY WITH AS 3740 & THE REQUIREMENTS OF CLAUSE F2D3 OF THE NCC 2022.</p> <p>BUILDING ELEMENTS WITHIN WET AREAS TO COMPLY WITH SPECIFICATION 26.</p> <p>WATERPROOFING OF INTERNAL WET AREAS (i.e. kitchen, bathrooms, toilet, laundry, etc.) SHALL BE CARRIED OUT IN ACCORDANCE WITH CLAUSE F2D3 (NCC 2022). PROVIDE SUITABLE WATERPROOF FIBRE CEMENT SHEET AS A SUBSTRATE TO WALLS IN WET AREA PLASTERBOARD (eg. 'SIPROCE AQUACHEF').</p> <p>BOARDS CONTAINING URINALS TO BE CONSTRUCTED TO COMPLY WITH CLAUSE F2D3 OF THE NCC 2022.</p> <p>FLOOR WASTES WHERE REQUIRED TO BE CONSTRUCTED TO COMPLY WITH CLAUSE F2D4 OF THE NCC 2022.</p> <p>F3D4 - GLAZED ASSEMBLIES</p> <p>PROVIDE CERTIFICATION FOR THE WINDOWS AND GLAZING AT THE COMPLETION OF THE BUILDING WORKS TO VERIFY COMPLIANCE WITH THE REQUIREMENTS OF AS 1288 AND NCC 2022 AND AS 2947 AND NCC 2022 PART F3D4 FOR THE APPROPRIATE WIND CLASSIFICATION.</p> <p>FULLY FRAMED GLAZING SHALL BE 'GRADE A SAFETY GLASS IN ACCORDANCE WITH TABLE 5.1 OF AS 1288-2006 (GLASS IN BUILDINGS - SELECTION & INSTALLATION).</p> <p>BUILDER SHALL CONFIRM WINDOW & DOOR FRAME SIZES ON SITE BEFORE INSTALLATION.</p> <p>F6D5 - ARTIFICIAL LIGHTING</p> <p>ARTIFICIAL LIGHTING SHALL BE INSTALLED TO COMPLY WITH CLAUSE F6D5 & 7 OF THE NCC 2022 & AS/NZS 3000. ALL WIRING SHALL BE CONCEALED.</p> <p>REFER TO LIGHTING PLAN (ENCL. J7) COMPLIANCE / CERTIFICATION BY OTHERS.</p> <p>F6D6 / F6D7 & F6D8 - VENTILATION</p> <p>VENTILATION REQUIREMENTS IN ACCORDANCE WITH PART F6 OF THE NCC 2022.</p> <p>VENTILATION TO ROOMS TO COMPLY WITH CLAUSE F6D6 OF THE NCC 2022.</p> <p>NATURAL VENTILATION TO COMPLY WITH CLAUSE F6D7 OF THE NCC 2022.</p> <p>BORROWED VENTILATION TO COMPLY WITH CLAUSE F6D8 OF THE NCC 2022.</p> <p>MECHANICAL VENTILATION OR AIR-CONDITIONING SYSTEMS ARE TO COMPLY WITH AS 3668.2 & AS/NZS 3668.1. REFER TO DWGS. BY OTHERS.</p>
<p>C2D11 - FIRE HAZARD PROPERTIES</p> <p>SUBMIT MANUFACTURERS DETAILS FOR THE NEW CARPET & FLOOR VINYL SHOWING THE FIRE HAZARD PROPERTIES TO ALLOW ASSESSMENT WITH SPECIFICATION 7 (C2019) OF THE NCC 2022. PRIOR TO INSTALLATION.</p> <p>FLOOR COVERINGS / SURFACES SHALL ALSO COMPLY WITH PART 7 OF AS 1428.1-2009.</p>	<p>PART J4 - ENERGY EFFICIENCY (BUILDING FABRIC)</p> <p>ALL RELEVANT BUILDING WORKS SHALL COMPLY WITH PART J4 (energy efficiency) OF THE NCC 2022.</p> <p>REQUIRED MINIMUM TOTAL R-VALUES (climate zone 7) SHALL BE AS FOLLOWS:-</p> <p>(a) ROOF AND/OR CEILING - R3.7 (SA = 0.45) R4.0 CEILING INSULATION BATTS.</p> <p>(b) WALLS - R2.4 R2.5 H.D. WALL INSULATION BATTS & SEALATION.</p> <p>(c) FLOORS (suspended and enclosed) - R2.0 Item: 'YELLOWGANGUE R-Floor (R1.75) SHEET FLOORING + 'INSULBOARD' (0.25) FIXED TO UNDERSIDE OF JOISTS.</p> <p>ALL BULK & REFLECTIVE THERMAL INSULATION MUST COMPLY WITH AS/NZS 4983.1 TO FORM A CONTINUOUS BARRIER WITH THE ROOF, CEILING, WALLS & FLOORS.</p> <p>PART J4D6 - GLAZING</p> <p>REFER TO ATTACHED GLAZING CALCULATOR.</p> <p>PART J5 - BUILDING SEALING</p> <p>BUILDING SEALING MUST COMPLY WITH PART J5 OF THE NCC 2022.</p> <p>ALL OPENABLE WINDOW BAGHES AND AROUND EDGES OF EXTERNAL DOORS SHALL BE FITTED WITH SELF-ADHESIVE FOAM OR RUBBER COMPRESSIBLE SEALS.</p> <p>ROOFS, EXTERNAL WALLS, EXTERNAL FLOORS AND ANY OPENING SUCH AS A WINDOW OR DOOR IN AN EXTERNAL WALL MUST BE CONSTRUCTED TO MINIMISE AIR LEAKAGE BY ENSURING INTERNAL SURFACES AT CEILING, WALL AND FLOOR JUNCTIONS ARE CLOSE FITTING OR SEALED BY GALKING, SKIRTING, ARCHTRAVES, CORNICES OR THE LIKE.</p> <p>EXHAUST FANS MUST BE FITTED WITH A SEALING DEVICE SUCH AS A SELF-CLOSING DAMPER, FILTER OR THE LIKE.</p> <p>PART J6 - AIR CONDITIONING & VENTILATION</p> <p>REFER TO DWGS BY OTHERS FOR ALL MECHANICAL VENTILATION DETAILS / CERTIFICATION & NCC 2022 PART J6 CALCULATIONS / COMPLIANCE.</p> <p>PART J7 - ARTIFICIAL LIGHTING & POWER</p> <p>REFER TO DRAWINGS BY OTHERS FOR ALL ELECTRICAL & LIGHTING DETAILS / CERTIFICATION & NCC 2022 PART J7 CALCULATIONS / COMPLIANCE.</p> <p>PART J8 - HOT WATER SUPPLY</p> <p>HOT WATER SUPPLY SYSTEM SHALL BE DESIGNATED & INSTALLED IN ACCORDANCE WITH SECTION 8 OF AS/NZS 3500.4 OR CLAUSE 3.38 OF AS/NZS 3500.5 - COMPLY WITH PART B2 OF NCC 2022 VOLUME 3 - PLUMBING CODE.</p> <p>THE FIRST 500mm OF PIPE FROM ANY HOT WATER STORAGE UNIT MUST BE INSULATED WITH Min. Item: 'CLOSED CELL POLYMER (wide building)' OR 25mm OF CLOSED CELL POLYMER (narrow building).</p>

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ADDRESS	DATE	DESCRIPTION	SCALE	BY
312 Argyle Street				DA
CLIENT				DA
Sam Lincoln				
DWG				A901-

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CELEBRATING
15 YEARS
2008 - 2023



MIDSON
traffic
pty ltd

Keith Midson
Midson Traffic Pty Ltd
28 Seaview Avenue
Taroona TAS 7053
0437 366 040

18th March 2026

John Etherington
S-Group Architects
100 Elizabeth Street
Hobart TAS 7000

Dear John,

310 ARGYLE STREET – RESPONSE TO COUNCIL RFI

I refer to Council's Request for Further Information (RFI) dated in relation to the proposed indoor golf simulator facility (X-Golf) at 310 Argyle Street, North Hobart.

An updated Traffic Impact Assessment (TIA) has been prepared and is submitted with this response. The TIA has been revised to address the matters raised in the RFI, with particular focus on parking demand, survey methodology and supporting evidence.

A summary of responses to the relevant traffic and parking matters contained in Council's RFI is provided in the following sections.

1. On-street parking surveys (Item 7(a))

Updated on-street parking surveys have been undertaken on Thursday 12 March, Friday 13 March and Saturday 7 March 2026. These surveys were completed outside school holiday periods and are considered representative of typical weekday and weekend conditions.

The surveys were undertaken within the same study area as the original assessment and confirm that measurable spare capacity exists within the surrounding road network. Importantly, the surveys demonstrate that parking availability is greatest during evening periods, which coincide with the peak demand of the proposed development.

2. Patronage assumptions and justification (Item 7(b))

The TIA has been updated to further clarify the derivation of patron numbers and traffic generation using first-principles assessment. This includes:

- Weekly patronage derived from projected turnover and average spend per patron;
- Observed patron behaviour at the operator's Launceston venue; and
- National franchise data indicating distribution of patron activity throughout the day.

The Launceston venue provides a relevant empirical benchmark, operating under comparable conditions with significantly lower on-site parking provision and reliance on surrounding on-street parking.

3. Distribution of demand (Item 7(c))

The updated TIA clarifies that parking and traffic demand is not evenly distributed throughout the week. Peak demand occurs during:

- Weekday evenings (particularly organised league nights); and
- Friday and Saturday evenings.

Daytime demand is comparatively low, with parking demand typically in the order of 5–8 vehicles at any one time.

4. Parking demand and Planning Scheme assessment (Item 7(d) and 7(e))

The updated TIA includes a comprehensive assessment of parking demand based on first principles, supported by updated survey data.

While the Acceptable Solution calculation under Clause C2.5.1 results in a theoretical requirement of 49 spaces, this rate is derived from a generic Planning Scheme provision and does not reflect the operational characteristics of the proposed use.

The assessment demonstrates that:

- Peak parking demand is in the order of 20–25 vehicles;
- The existing 14 on-site spaces accommodate the majority of demand; and
- Available on-street parking capacity during peak evening periods (typically 8–17 spaces) is sufficient to accommodate any overflow demand.

Importantly, there is a strong temporal alignment between peak parking demand and peak on-street parking availability.

5. Servicing and heavy vehicles (Item 4)

The TIA has been updated to include consideration of servicing activities.

The previous building supply use generated regular heavy vehicle movements associated with delivery of construction materials. In contrast, the proposed development will generate minimal servicing demand, limited to small commercial vehicles on an infrequent basis.

Accordingly, the proposal represents a significant reduction in heavy vehicle activity and associated impacts on the surrounding road network.

6. Conclusion

The updated TIA demonstrates that the proposed development will generate a modest level of traffic, substantially lower than the previous use of the site, and that parking demand can be appropriately accommodated through a combination of on-site provision and available on-street parking.

The proposal is therefore considered to satisfy the relevant Performance Criteria of the Planning Scheme and is acceptable from a traffic and parking perspective.

Please contact me on 0437 366 040 if you require any further information.

Yours sincerely,



Keith Midson BE MTraffic MTransport FIEAust CPEng EngExec NER

DIRECTOR

Midson Traffic Pty Ltd



X-Golf

**310 Argyle Street, North Hobart
Traffic Impact Assessment**

March 2026



CELEBRATING 18 YEARS
2008 - 2026



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1. Introduction

1.1 Background

Midson Traffic were engaged by X-Golf to prepare a traffic impact assessment for a proposed indoor golf simulator development at 310 Argyle Street, North Hobart.

1.2 Traffic Impact Assessment (TIA)

A traffic impact assessment (TIA) is a process of compiling and analysing information on the impacts that a specific development proposal is likely to have on the operation of roads and transport networks. A TIA should not only include general impacts relating to traffic management, but should also consider specific impacts on all road users, including on-road public transport, pedestrians, cyclists and heavy vehicles.

This TIA has been prepared in accordance with the Department of State Growth (DSG) publication, *Traffic Impact Assessment Guidelines*, August 2020. This TIA has also been prepared with reference to the Austroads publication, *Guide to Traffic Management*, Part 12: *Integrated Transport Assessments for Developments*, 2020.

Land use developments generate traffic movements as people move to, from and within a development. Without a clear understanding of the type of traffic movements (including cars, pedestrians, trucks, etc), the scale of their movements, timing, duration and location, there is a risk that this traffic movement may contribute to safety issues, unforeseen congestion or other problems where the development connects to the road system or elsewhere on the road network. A TIA attempts to forecast these movements and their impact on the surrounding transport network.

A TIA is not a promotional exercise undertaken on behalf of a developer; a TIA must provide an impartial and objective description of the impacts and traffic effects of a proposed development. A full and detailed assessment of how vehicle and person movements to and from a development site might affect existing road and pedestrian networks is required. An objective consideration of the traffic impact of a proposal is vital to enable planning decisions to be based upon the principles of sustainable development.

This TIA also addresses the relevant clauses of C2.0, *Parking and Sustainable Parking Code*, and C3.0, *Road and Railway Assets Code*, of the Tasmanian Planning Scheme – Hobart, 2025.

1.3 Statement of Qualification and Experience

This TIA has been prepared by an experienced and qualified traffic engineer in accordance with the requirements of Council's Planning Scheme and The Department of State Growth's, *Traffic Impact Assessment Guidelines*, August 2020, as well as Council's requirements.

The TIA was prepared by Keith Midson. Keith's experience and qualifications are briefly outlined as follows:

- 30 years professional experience in traffic engineering and transport planning.
- Master of Transport, Monash University, 2006
- Master of Traffic, Monash University, 2004



- Bachelor of Civil Engineering, University of Tasmania, 1995
- Engineers Australia: Fellow (FIEAust); Engineering Executive (EngExec)

1.4 Project Scope

The project scope of this TIA is outlined as follows:

- Review of the existing road environment in the vicinity of the site and the traffic conditions on the road network.
- Provision of information on the proposed development with regards to traffic movements and activity.
- Identification of the traffic generation potential of the proposal with respect to the surrounding road network in terms of road network capacity.
- Review of the parking requirements of the proposed development. Assessment of this parking supply with Planning Scheme requirements.
- Traffic implications of the proposal with respect to the external road network in terms of traffic efficiency and road safety.

1.5 Subject Site

The subject site is located at 310 Argyle Street, North Hobart. The site was previously used as a building supply wholesale store.

The subject site and surrounding road network is shown in Figure 1.



Figure 1 Subject Site & Surrounding Road Network



Image Source: LIST Map, DNRE

1.6 Reference Resources

The following references were used in the preparation of this TIA:

- Tasmanian Planning Scheme – Hobart, 2025 (Planning Scheme)
- Austroads, *Guide to Traffic Management, Part 12: Integrated Transport Assessments for Developments*, 2020
- Austroads, *Guide to Road Design, Part 4A: Unsignalised and Signalised Intersections*, 2021
- Department of State Growth, *Traffic Impact Assessment Guidelines*, 2020
- Transport NSW, *Guide to Traffic Impact Assessment, 2024* (TfNSW Guide)
- Australian Standards, AS2890.1, *Off-Street Parking*, 2004 (AS2890.1)



2. Existing Conditions

2.1 Transport Network

For the purposes of this report, the transport network consists of Argyle Street only.

Argyle Street is a major arterial road that provides one-way flow between Davey Street and Burnett Street. Adjacent to the subject site, Argyle Street has two-lanes/ two-way flow. On-street bicycle lanes are provided on both sides of Argyle Street near the subject site. Time restricted on-street car parking is available on both sides of Argyle Street.

Argyle Street carries approximately 14,000 vehicles per day. The intersection of Federal Street to the north of the site is signalised. The Strahan Street intersection to the south of the site is a give-way controlled T-junction.

Argyle Street adjacent to the subject site is shown in Figure 2.

Figure 2 Argyle Street



2.2 Public Transport

Metro Tasmania operates bus services along Argyle Street (routes 560, 561 and 562) and Elizabeth Street (routes 500, 502, 503, 504, 511, 512, 513, 520, 522, 722) on a frequent basis (generally at less than 10-minute intervals during weekday business hours).

A bus stop is located immediately adjacent to the subject site.



2.3 Road Safety Performance

Crash data can provide valuable information on the road safety performance of a road network. Existing road safety deficiencies can be highlighted through the examination of crash data, which can assist in determining whether traffic generation from the proposed development may exacerbate any identified issues.

Crash data was obtained from the Department of State Growth for a 5+ year period between 1st January 2021 and 31st January 2026 for Argyle Street between Strahan Street to Federal Street.

The findings of the crash data is summarised as follows:

- A total of 14 crashes were reported during this period.
- Severity. 1 crash involved minor injury; 2 crashes involved first aid at the scene; 11 crashes involved property damage only.
- Time of day. 7 crashes were reported between 8:30am and 3:30pm. 5 crashes were reported prior to 8:00am. 2 crashes were reported after 8:30pm.
- Day of week. No crash trends were noted by day of week. 5 crashes were reported on Tuesdays; 2 crashes were reported on Wednesdays, Thursdays, Fridays and Sundays; 1 crash was reported on a Saturday; no crashes were reported on Mondays.
- Crash types. 3 crashes involved 'cross-traffic' collisions; 2 crashes involved 'right-through' collisions; 2 crashes involved 'rear-end' collisions; 2 crashes involved 'other-manoeuving' collisions; and various other crash types with no clear trends noted.
- Crash locations. 9 crashes were reported at the Argyle Street/ Federal Street junction; 3 crashes were reported at midblock locations; 1 crash was reported in an off-road location. The crash locations are shown in Figure 3.
- Vulnerable road users. 1 crash involved a pedestrian (property damage only at Federal Street junction); and 1 crash involved a cyclist (property damage only at Federal Street junction).

The crash data is considered reasonably typical of an urban multi-lane arterial road. The crash rates and crash types at the Federal Street intersection is consistent with what would be expected at high-volume signalised urban junction. Midblock crashes were relatively low, with dominant crash types comprising of intersection related crashes.



Figure 3 Crash Locations



Source: Department of State Growth



3. Proposed Development

3.1 Development Proposal

The proposal relates to the establishment of an indoor golf simulator facility (X-Golf) within the existing commercial tenancy at 310 Argyle Street, Hobart.

The facility will operate as a recreation and entertainment venue incorporating multiple golf simulator bays and ancillary bar/hospitality services. The proposed operating hours are 10:00am–10:00pm Monday to Thursday, 10:00am–11:00pm Friday and Saturday, and 10:00am–6:00pm Sunday.

Based on the operator's experience at the Launceston venue and projected Hobart turnover, the business is expected to attract approximately 445 patrons per week, equating to an average of around 60–65 patrons per day. Peak patronage is anticipated to occur during weekday evenings (particularly organised league nights between 7:00pm and 9:00pm) and on Friday and Saturday evenings. Under normal operating conditions, the number of patrons present at any one time is typically up to approximately 36 persons, with occasional larger functions potentially accommodating up to 50 patrons.

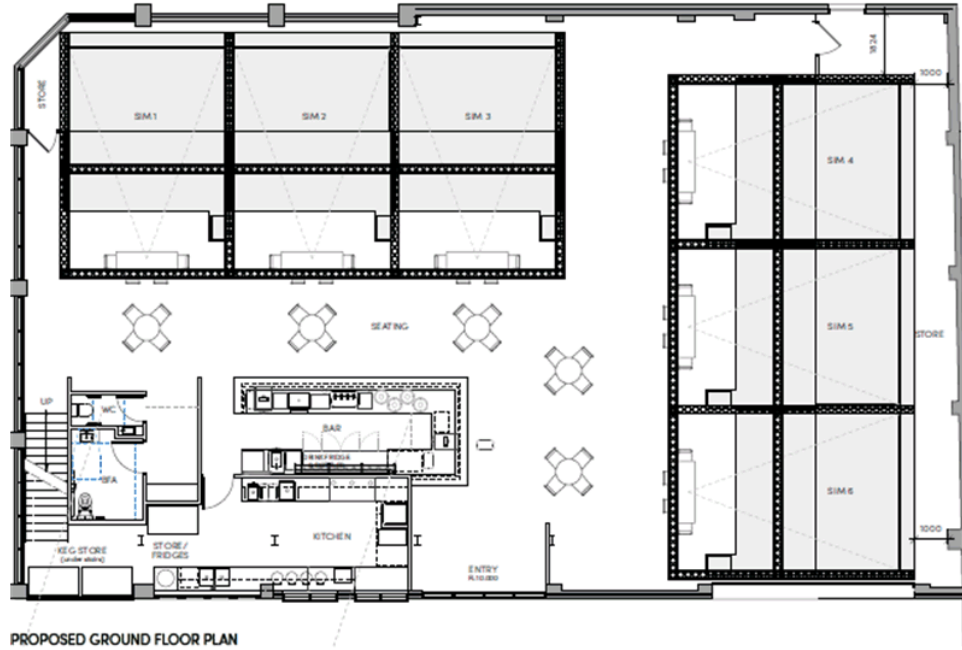
Staffing levels will generally comprise 1 staff member during daytime trading, increasing to up to 3 staff during peak evening and weekend periods.

The existing on-site car parking, consisting of 14 spaces, will be retained.

The proposed development is shown in Figure 4.



Figure 4 Proposed Development – Interior Design





4. Traffic Impacts

4.1 Trip Generation

The proposed development is an indoor golf simulator facility to operate generally between 10:00am and late evening, with peak customer activity occurring after 5:00pm on weekdays and across Friday/ Saturday evenings and weekends.

There is little empirical data for this type of facility available in typical traffic engineering resources. Trip generation has therefore been derived from first principles using the operator's expected trading turnover and observed patron behaviour at the existing Launceston venue (i.e. average spend per patron, group size, dwell time, and typical vehicle occupancy).

4.1.1 Daily Traffic Generation

The Hobart venue is expected to average approximately 445 patrons per week based on operations at its Launceston venue. This equates to approximately 64 patrons per day (average).

Based on the operator's experience, patrons typically attend in groups of 2–4 people, and often travel together. Adopting a conservative average vehicle occupancy of 2 patrons per car, the average customer vehicles arriving are as follows:

- Average customer vehicles per day (arrivals): $64 / 2 = 32$ vehicle arrivals per day
- Two-way daily vehicle movements (arrivals + departures): $32 \times 2 = 64$ vehicle movements per day

Staffing during daytime trading is typically 1 staff member (up to 2 at certain times). During peak evening/ weekend periods, staffing may be up to 3. Assuming staff generally drive:

- Average staff vehicles: 1–2 during daytime, up to 3 during peak periods.

Accordingly, the average total two-way daily vehicle movements associated with the business (customers + staff) would typically be in the order of:

- 64 customer movements per day + 2 to 4 staff movements per day = 66 to 68 movements per day (average)

National franchise data (March to May 2025) indicates weekday 10:00am to 5:00pm sales represent 32% of total weekly sales. Applying this to the Hobart forecast turnover:

- 142 patrons during weekday business hours (weekly total)
- $142 / 5 \approx 28$ patrons per weekday (10:00am–5:00pm)
- With 2 patrons per car: $28 / 2 \approx 14$ vehicle arrivals per weekday (business hours)



- Two-way weekday business-hours movements: $14 \times 2 = 28$ vehicle movements per weekday (10:00am–5:00pm)

On an hourly basis over the 7-hour business day, this equates to approximately:

- ~2 vehicle arrivals per hour (average) (and a similar level of departures).

4.1.2 Peak period activity and design-hour trip generation

Based on Launceston patronage data, the highest vehicle activity is occurring during league nights (Mon–Wed 7:00pm–9:00pm), involving 8 teams of 3 players (24 players total) across 4 simulators. The operator advises these nights can generate up to ~25 cars at the venue.

For assessment purposes, a reasonable (conservative) design-hour approximation is:

- Arrivals: up to ~25 vehicles in the hour leading into league commencement (i.e. around 6:00–7:00pm), and
- Departures: up to ~25 vehicles in the hour following league completion (i.e. around 9:00–10:00pm).

This represents a peak-hour generation of approximately 25 vehicle trips (arrivals) and 25 vehicle trips (departures) in the respective peak hours (i.e. ~50 vehicle movements across the two peak hours, but not concentrated in a single hour in one direction).

For weekend and public holiday peak conditions, the venue will typically accommodate up to approximately 36 patrons at any one time. Assuming an average vehicle occupancy of 2 patrons per vehicle, this equates to approximately 18 customer vehicles arriving and departing during the relevant peak period.

With up to 3 staff members present during busier trading periods, total peak vehicle arrivals would be in the order of 21 vehicles. These movements would not occur simultaneously, but would typically be spread across the pre-event arrival period (generally the hour prior to peak trading) and the corresponding departure period following the peak session.

Accordingly, a conservative design scenario for weekend/public holiday peak conditions would be in the order of 20 to 25 vehicle arrivals in the peak arrival hour, with a similar number of departures in the subsequent peak departure hour. This represents a modest peak-hour traffic generation for a commercial premises of this nature.



4.1.3 Traffic Generation Summary

Based on first principles:

- Average daily traffic generation: approximately 66 to 68 two-way vehicle movements per day (customers + staff, average conditions).
- Weekday business-hours traffic generation: approximately 28 two-way vehicle movements across 10:00am to 5:00pm (plus minor staff movements).
- Peak-hour traffic generation (league nights): up to 25 arrivals in the peak arrival hour and 25 departures in the peak departure hour.

4.2 Trip Assignment

At the site's access, it is likely that there will be a relatively even distribution of traffic entering and exiting the site via left-in/ right-in and left-out/ right-out movements.

4.3 Access Impacts

The Acceptable Solution A1.4 of Clause C3.5.1 of the Planning Scheme states "*Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than the amounts in Table C3.1*".

Table C3.1 specifies a maximum increase in daily traffic volume at an access to be 20% or 40 vehicle movements per day whichever is greater. In this case the traffic generation associated with the previous use is unknown, but can be estimated.

The previous use was a housing/ building supply store. In the absence of site-specific counts, the TfNSW Guide rates for "hardware stores" provide the closest proxy. Applying the weekday daily vehicle trip rate of 32.46 trips/ 100 m² GLFA to an estimated GLFA of ~720 m² indicates the previous use would have generated in the order of 234 two-way vehicle movements per day (and a site peak hour in the order of 30 vehicle movements/ hour).

As outlined in Section 4.1, the proposed indoor golf facility is expected to generate approximately 66 to 68 two-way vehicle movements per day on average, with peak activity occurring during evening periods. Even allowing for occasional peak trading scenarios, daily traffic generation associated with the proposed use remains well below that of the former hardware/building supply use.

Accordingly, the proposal does not represent an intensification of traffic at the existing access. Rather, it represents a substantial reduction in daily vehicle movements relative to the established lawful use of the site.

In terms of Acceptable Solution A1.4 of Clause C3.5.1, Table C3.1 specifies a maximum allowable increase in daily traffic volume of 20% or 40 vehicle movements per day, whichever is greater. Twenty percent of the estimated previous use traffic generation (234 vpd) equates to approximately 47 vehicle movements per day. Therefore, the relevant threshold in this case is 47 vehicle movements per day.

The proposed development results in a net reduction in daily traffic relative to the previous use and therefore satisfies Acceptable Solution A1.4 of Clause C3.5.1.



4.4 Sight Distance

Australian Standards, AS2890.1, provide the sight distance requirements for commercial driveways. Sight distance requirements are lower for driveways compared to road junctions.

The minimum sight distance requirements for a commercial driveway access in a 50-km/h frontage road is 45 metres. The available sight distance in both directions along Argyle Street exceeds 45 metres and therefore the AS2890.1 sight distance requirements are satisfied.

4.5 Pedestrian Impacts

Argyle Street has pedestrian footpaths and on-road cycle paths on both sides of the road. Pedestrian phasing has been included in the signalised intersection of Argyle Street and Federal Street to the northwest of the subject site.

The proposed development is likely to generate a small amount of pedestrian activity in the surrounding network. The existing pedestrian infrastructure in the surrounding road network is considered adequate to cater for the likely pedestrian demands associated with the proposed development.

4.6 Road Safety Impacts

An assessment of road safety impacts has been undertaken having regard to the existing crash history of Argyle Street, the nature and scale of traffic generation associated with the proposal, and the characteristics of the site access.

As outlined in Section 2.2, a total of 14 crashes were recorded on Argyle Street between Strahan Street and Federal Street over the 5+ year period from January 2021 to January 2026. The majority of crashes were property damage only, with only 1 minor injury crash recorded during the period. Crash patterns were typical of an urban arterial environment, with the majority of crashes occurring at the signalised Federal Street intersection. Midblock crash occurrence was low and no specific trend or systemic safety deficiency was identified in the vicinity of the site access.

The proposed development is expected to generate approximately 66 to 68 two-way vehicle movements per day on average, with peak-hour arrivals or departures in the order of 20 to 25 vehicles during evening trading periods. This represents a modest level of traffic generation and, importantly, is substantially lower than the estimated traffic generation associated with the former building supply use of the site (approximately 234 vehicle movements per day).

Given that:

- The access is existing and has been in continuous operation for many years without issue;
- Sight distance requirements in accordance with AS2890.1 are satisfied at the access on Argyle Street;
- The proposal results in a net reduction in daily traffic relative to the previous use;
- Argyle Street carries approximately 14,000 vehicles per day; and



-
- The reduction in heavy vehicle activity relative to the former use further contributes to an overall improvement in road safety conditions.

The additional traffic associated with the proposed use is considered negligible in the context of the surrounding road network. The proposal is not expected to adversely affect the road safety performance of Argyle Street or the surrounding network. The development is considered acceptable from a road safety perspective.



5.2 Theoretical Parking Demand

There is limited empirical parking generation data available for indoor golf simulator facilities in conventional traffic engineering resources. Accordingly, theoretical parking demand has been derived from first principles using the same operational parameters adopted in Section 4.1 (patronage, group size, dwell time and staff numbers).

The above assessment is consistent with the operation of the operator's existing Launceston venue, located at Gleadow Street. That facility comprises a tenancy of approximately 600 m² and is provided with only 6 on-site car parking spaces. The Launceston venue operates under comparable trading conditions and provides a reliable empirical benchmark for patron behaviour and parking demand.

Despite the limited on-site provision, the venue operates successfully by relying on minimal surrounding on-street parking. This demonstrates that indoor golf simulator facilities of this nature do not generate parking demands consistent with standard rate-based provisions, and instead operate effectively with a combination of on-site and on-street parking.

5.2.1 Average Operating Conditions

The venue is expected to attract approximately 445 patrons per week, equating to an average of approximately 64 patrons per day.

During weekday daytime trading (10:00am–5:00pm), national franchise data indicates approximately 28 patrons per day are likely to attend. Adopting a conservative average vehicle occupancy of 2 patrons per vehicle, this equates to approximately:

- 14 customer vehicles across the 7-hour business period.

Given an average dwell time of approximately 1.5 hours, arrivals and departures are staggered throughout the day. On this basis, the number of customer vehicles present at any one time during typical weekday daytime trading would be in the order of 4 to 6 vehicles.

With 1–2 staff members present during daytime trading, total parking demand during business hours would typically be in the order of:

- 5–8 vehicles (customers + staff)

5.2.2 Evening League Nights (Design Scenario)

The highest regular parking demand is expected during organised league nights (Monday to Wednesday, 7:00pm – 9:00pm).

These events involve 8 teams of 3 players (24 patrons). While this equates to 24 persons, the operator advises that patrons commonly travel together. Based on observed operations at the Launceston venue, up to approximately 25 vehicles may attend during these peak evenings.



For assessment purposes, a conservative design scenario of up to 25 vehicles on site (including staff) has been adopted.

5.2.3 Weekend / Public Holiday Peak Conditions

During busy weekend or public holiday periods, the venue typically accommodates up to approximately 36 patrons at any one time, with occasional larger functions of up to 50 patrons.

Assuming an average vehicle occupancy of 2 patrons per vehicle, 36 patrons equates to approximately 18 customer vehicles. With up to 3 staff members present, total theoretical on-site parking demand would be in the order of 20–21 vehicles.

For occasional larger functions (up to 50 patrons), assuming a slightly higher vehicle occupancy due to group attendance and ride-share use, total demand is expected to remain in the order of 20–25 vehicles.

5.2.4 Summary of Theoretical Parking Demand

Based on first principles assessment:

- Weekday daytime demand: typically 5–8 vehicles
- Regular peak demand (league nights / busy evenings): up to ~25 vehicles
- Occasional larger functions: up to 25 vehicles

Importantly, these peak parking demands occur outside standard weekday business hours, when surrounding on-street parking supply is less constrained.

The above provides a realistic estimate of parking demand for the proposed development based on observed operational characteristics.

5.3 On-Street Parking Demands

On-street car parking surveys were undertaken on Saturday 7th March, Thursday 12th March and Friday 13th March 2026 to determine the availability of on-street parking near the subject site under typical weekday and weekend conditions. These surveys were undertaken outside school holiday periods and are therefore considered representative of typical operating conditions.

Surveys were conducted in Argyle Street between Lefroy Street and Yardley Street, as well as Strahan Street between Argyle Street and No.17 Strahan Street, between 10:00am and 8:00pm. The survey results are summarised in Tables 1 to 3.

The survey results indicate that on-street parking within the immediate vicinity of the site experiences moderate to high utilisation during weekday daytime periods, particularly within sections of Argyle Street and along Strahan Street, which functions as a convenient parking area for surrounding commercial and residential uses.



Across the weekday surveys (Thursday and Friday), spare capacity during daytime periods (10:00am to 4:00pm) generally ranged between approximately 11 and 19 spaces, with typical availability in the order of 12–17 spaces. The lowest observed spare capacity during weekday conditions was approximately 11 spaces, occurring during the late morning and early afternoon periods. These results indicate that while parking demand is elevated during business hours, there remains a consistent level of available capacity within the immediate survey area.

Weekend conditions (Saturday) demonstrated a different demand profile, with higher utilisation observed during the middle of the day, reflecting peak activity within the North Hobart precinct. During this period, spare capacity reduced to a minimum of approximately 8 spaces at 2:00pm, indicating that parking operates closer to practical capacity during peak weekend daytime conditions.

A key outcome of the surveys is the clear temporal variation in parking demand. During the evening period (6:00pm to 8:00pm), which coincides with the expected peak demand of the proposed development, parking availability increases across all survey days.

- Thursday spare capacity: approximately 15–17 spaces
- Friday spare capacity: approximately 8–11 spaces
- Saturday spare capacity: approximately 3–4 spaces within the immediate core area, with additional capacity evident in adjoining streets

This increase in availability reflects the reduction in demand from surrounding commercial uses during evening periods.

Overall, the surveys indicate:

- Minimum observed spare capacity: approximately 8 spaces (Saturday afternoon peak)
- Typical weekday spare capacity: approximately 12–17 spaces
- Evening spare capacity (all days): generally 8–17 spaces, depending on location and day

Strahan Street was consistently observed to operate at or near capacity during parts of the day; however, this is offset by available capacity along Argyle Street and adjoining streets within short walking distance of the site.

It is also important to note that the survey area focuses on the streets immediately surrounding the site. Additional on-street parking is available beyond the surveyed extents along Argyle Street, as well as within nearby connecting streets including Federal Street and the broader North Hobart road network. Accordingly, the effective parking catchment available to the site extends beyond that captured in Tables 1 to 3.



In summary, while on-street parking is relatively well utilised during peak daytime periods, the surveys demonstrate that measurable spare capacity exists across all surveyed periods, and importantly, that parking availability is greatest during the evening periods when the proposed development generates its highest parking demand.

**Table 1 On-Street Parking Surveys - Saturday**

Location	Capacity	10:00	12:00	14:00	16:00	18:00	20:00
Argyle St – Lefroy-Strahan east	6 x ½P	2	2	1	0	3	2
Argyle St – Lefroy- Wellington west	3 x 2P	2	1	2	1	3	3
Argyle St – Strahan-Federal east	4 x unrestricted	2	2	4	2	3	2
Argyle St – Wellington-Federal west	10 x 2P	5	6	4	8	8	8
Argyle St – Federal-Yardley east	10 x 2P	6	8	8	6	6	7
Argyle St – Federal-Yardley west	8 x unrestricted	5	4	7	3	3	2
Strahan St – Argyle-17 Strahan north	6 x 2P + 9 x unrestricted	9	11	15	12	14	15
Strahan St – Argyle-17 Strahan south	5 x 1P + 5 unrestricted	6	6	10	9	12	12
Federal St – Argyle-Thomas north	4 x 1P + 4 unrestricted	2	1	2	2	8	8
Federal St – Argyle-Thomas south	4 x ½P + 4 x ¼P + 1 unrestricted	6	7	8	7	6	6
TOTAL		45	48	61	50	66	65
SPARE CAPACITY		24	21	8	19	3	4

**Table 2 On-Street Parking Surveys – Thursday**

Location	Capacity	10:00	12:00	14:00	16:00	18:00	20:00
Argyle St – Lefroy-Strahan east	6 x ½P	2	3	4	4	2	3
Argyle St – Lefroy- Wellington west	3 x 2P	3	3	3	2	3	3
Argyle St – Strahan-Federal east	4 x unrestricted	4	3	3	4	3	4
Argyle St – Wellington-Federal west	10 x 2P	5	5	9	8	5	5
Argyle St – Federal-Yardley east	10 x 2P	6	7	10	8	7	5
Argyle St – Federal-Yardley west	8 x unrestricted	7	7	6	5	5	4
Strahan St – Argyle-17 Strahan north	6 x 2P + 9 x unrestricted	10	11	9	8	11	10
Strahan St – Argyle-17 Strahan south	5 x 1P + 5 unrestricted	7	8	7	4	8	7
Federal St – Argyle-Thomas north	4 x 1P + 4 unrestricted	6	5	3	4	5	5
Federal St – Argyle-Thomas south	4 x ½P + 4 x ¼P + 1 unrestricted	2	3	3	3	5	6
TOTAL		52	55	57	50	54	52
SPARE CAPACITY		17	14	12	19	15	17

**Table 3 On-Street Parking Survey – Friday**

Location	Capacity	10:00	12:00	14:00	16:00	18:00	20:00
Argyle St – Lefroy-Strahan east	6 x ½P	3	3	4	4	3	2
Argyle St – Lefroy- Wellington west	3 x 2P	2	2	3	1	3	2
Argyle St – Strahan-Federal east	4 x unrestricted	3	2	3	3	3	4
Argyle St – Wellington-Federal west	10 x 2P	8	5	10	8	7	7
Argyle St – Federal-Yardley east	10 x 2P	8	8	10	6	7	6
Argyle St – Federal-Yardley west	8 x unrestricted	6	7	6	5	6	5
Strahan St – Argyle-17 Strahan north	6 x 2P + 9 x unrestricted	13	12	10	8	12	13
Strahan St – Argyle-17 Strahan south	5 x 1P + 5 unrestricted	8	8	6	4	9	9
Federal St – Argyle-Thomas north	4 x 1P + 4 unrestricted	6	4	3	3	6	5
Federal St – Argyle-Thomas south	4 x ½P + 4 x ¼P + 1 unrestricted	1	3	3	5	5	5
TOTAL		58	54	58	47	61	58
SPARE CAPACITY		11	15	11	22	8	11

5.4 Planning Scheme Parking Requirements

The Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme states:

"The number of on-site car parking spaces must be no less than the number specified in Table C2.1, excluding if:

- (a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;*
- (b) the site is contained within a parking precinct plan and subject to Clause C2.7;*
- (c) the site is subject to Clause C2.5.5; or*
- (d) it relates to an intensification of an existing use or development or a change of use where:*



(i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or

(ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:

$$N = A + (C - B)$$

N = Number of on-site car parking spaces required

A = Number of existing on site car parking spaces

B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1

C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1”.

The subject proposal represents a change of use from a former building supply / hardware store to a Sports and Recreation use (indoor golf simulator facility). Accordingly, Clause C2.5.1 A1(d) applies.

Under Table C2.1, the proposed land use is classified as:

- 'Sports and Recreation, excluding as otherwise specified in this Table'. This is a requirement for 50 spaces.

The previous use of the site was a building supply/ hardware store, which falls within 'Bulky Goods Sales (excluding as otherwise specified)', which requires 1 space per 50m² of floor area.

Applying this rate to the existing gross floor area of approximately 720 m²:

- $720 \div 50 = 15$ spaces (rounded up from 14.4 spaces)

Accordingly, the previous use required approximately 15 spaces under Table C2.1.

5.4.1 Application of Clause C2.5.1 A1(d)

As the number of spaces required for the proposed use (50) is greater than the number required for the previous use (14–15), Clause C2.5.1 A1(d)(ii) applies.

The formula is:

$$N = A + (C - B)$$



Where:

A = Number of existing on-site spaces (14)

B = Number of spaces required for previous use (15)

C = Number of spaces required for proposed use (50)

Using B = 15:

$N = 14 + (50 - 15)$

$N = 14 + 35$

$N = 49$ spaces

Accordingly, under the Acceptable Solution pathway, the site would require approximately 49 on-site car parking spaces.

As only 14 spaces are provided on site, the proposal does not satisfy Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme.

The Performance Criteria P1 of Clause C2.5.1 of the Planning Scheme states:

"The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use, having regard to:

- (a) the availability of off-street public car parking spaces within reasonable walking distance of the site;*
- (b) the ability of multiple users to share spaces because of:
 - (i) variations in car parking demand over time; or*
 - (ii) efficiencies gained by consolidation of car parking spaces;**
- (c) the availability and frequency of public transport within reasonable walking distance of the site;*
- (d) the availability and frequency of other transport alternatives;*
- (e) any site constraints such as existing buildings, slope, drainage, vegetation and landscaping;*
- (f) the availability, accessibility and safety of on-street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity;*
- (g) the effect on streetscape; and*
- (h) any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development".*



The following is relevant with respect to the development proposal:

- a. Availability of off-street public parking. There are no significant public off-street car parking facilities within convenient walking distance of the site. The proposal does not rely on formal public parking infrastructure, and parking demand is instead accommodated through a combination of on-site provision and available on-street parking within the surrounding road network.
- b. Shared parking principles. The proposed use exhibits strong temporal variation in parking demand.
 - Weekday daytime demand is low (typically 5–8 vehicles in total).
 - Peak parking demand occurs predominantly during evening periods (particularly 6:00pm–9:00pm) and on weekends.

During weekday business hours, surrounding commercial premises generate higher parking demand, however the proposed use is largely self-sufficient during this period.

During evenings, when the proposed use experiences its highest parking demand, surrounding commercial uses are generally closed or operating at reduced intensity. This creates a complementary demand profile, allowing efficient shared use of on-street parking in the vicinity of the site.

- c. Public transport. The site is located on Argyle Street, a major arterial corridor serviced by frequent public bus services. A bus stop is located directly adjacent to the site frontage. The availability and frequency of public transport provides a realistic alternative to private vehicle travel, particularly for evening and weekend patronage.
- d. Alternative transport. The site is located within the established urban area of North Hobart and is accessible by walking and cycling. Argyle Street provides on-road bicycle lanes in both directions and continuous pedestrian footpaths. The surrounding residential catchment allows for short-distance access without reliance on private vehicles. The nature of the use (group bookings, social events, evening trading) also lends itself to car-pooling and ride-share usage, which has been observed at the operator's Launceston venue.
- e. Site constraints. The site is an existing developed commercial tenancy with a fixed building footprint and established access arrangement. There is no practical capacity to expand on-site parking provision without significant building alteration or demolition. The frontage to Argyle Street is constrained, and the site configuration does not allow for additional compliant parking to be provided without unreasonable cost and loss of built form. Accordingly, physical site constraints materially limit the ability to increase on-site parking supply.
- f. On street public parking. Updated on-street parking surveys were undertaken on Thursday, Friday and Saturday in March 2026 to reflect typical weekday and weekend conditions (refer Section 5.3). The surveys indicate that:



- During weekday daytime periods, spare capacity within the immediate survey area is generally in the order of 12–17 spaces, with a minimum observed capacity of approximately 11 spaces.
- During peak weekend daytime periods, spare capacity reduces to approximately 8 spaces, reflecting higher background demand within the North Hobart precinct.
- During evening periods (6:00pm–8:00pm), spare capacity increases across all survey days, generally ranging between 8 and 17 spaces.

Importantly, the proposed development's peak parking demand occurs during the evening period, when on-street parking availability is demonstrably higher than during daytime conditions.

The surrounding road network comprises a mix of arterial and local streets with formalised on-street parking arrangements. The crash history review (Section 2.2) does not identify any safety issues associated with on-street parking in the vicinity of the site.

Accordingly, on-street parking in the surrounding area is considered to be available, accessible and safe, and capable of accommodating peak overflow demand beyond the 14 on-site spaces.

- g. Streetscape. Requiring compliance with the Acceptable Solution (49 spaces) would necessitate substantial redevelopment of the site and removal of built form to accommodate surface parking. This would be inconsistent with the established urban character of Argyle Street and would negatively affect the streetscape. Retention of the existing built form and parking arrangement maintains the established urban interface and is considered a superior planning outcome.
- h. Parking demands. A first-principles parking demand assessment has been undertaken in Section 5.2, having regard to patronage, vehicle occupancy, dwell time and staffing levels. The assessment indicates:
 - Weekday daytime demand: typically 5–8 vehicles
 - Regular peak demand (evenings / league nights): up to approximately 25 vehicles
 - Occasional larger functions: up to approximately 25 vehicles

The 14 on-site spaces accommodate the majority of demand under average operating conditions. During peak evening periods, an overflow demand of approximately 10–13 vehicles may occur. The updated on-street parking surveys confirm that available spare capacity within the surrounding road network during these periods (typically 8–17 spaces) is sufficient to accommodate this demand.

Importantly, there is a strong temporal alignment between peak parking demand and peak on-street parking availability, with the highest demand from the proposed use occurring at times when surrounding commercial parking demand is reduced.

The Acceptable Solution requirement of 49 spaces is derived from a generic rate and does not reflect the actual operational characteristics of the use, including group travel behaviour,



carpooling, ride-share usage, and evening-focused activity patterns. Accordingly, the proposed parking provision is consistent with the demonstrated and realistic parking demand of the use.

This conclusion is supported by the operation of the Launceston X-Golf venue, which accommodates a comparable use with substantially lower on-site parking provision. The successful operation of that facility confirms that actual parking demand for this land use is materially lower than that implied by generic Planning Scheme rates.

Having regard to the demonstrated parking demand, the temporal variation in demand, the availability of on-street parking during peak periods, the excellent public transport accessibility, and the physical constraints of the site, the proposed provision of 14 on-site parking spaces is considered to meet the reasonable needs of the use.

It is also relevant that the former bulky goods use of the site operated with 14 on-site spaces for many years without identified parking-related safety or network issues. The proposed use generates lower daily traffic volumes than the former use and does not represent an intensification of access activity. Accordingly, there is no evidence to suggest that retention of the existing 14 spaces would give rise to unacceptable parking or traffic impacts.

Accordingly, the proposal satisfies the Performance Criteria P1 of Clause C2.5.1 of the Planning Scheme. The parking provision is considered to meet the reasonable needs of the use and represents an appropriate outcome having regard to the demonstrated demand, temporal variation in parking activity and the availability of on-street parking during peak periods.

5.5 Motorcycle Parking

The Acceptable Solution A1 of Clause C2.5.3 of the Planning Scheme states:

"The number of on-site motorcycle parking spaces for all uses must:

- (a) be no less than the number specified in Table C2.4; and*
- (b) if an existing use or development is extended or intensified, the number of on-site motorcycle parking spaces must be based on the proposed extension or intensification, provided the existing number of motorcycle parking spaces is maintained".*

Table C2.4 specifies the number of motorcycle parking spaces required based on the number of car parking spaces required for the use.

As outlined in Section 5.4, the number of car parking spaces required for the proposed use under Clause C2.5.1 is 49 spaces (calculated under the change-of-use provisions).

In accordance with Table C2.4:

- 21–40 car spaces required → 1 motorcycle space



- 41 or more car spaces required → 1 additional motorcycle space for every additional 20 car spaces required

For 49 required car parking spaces, the motorcycle parking requirement equates to 1 motorcycle parking space. No dedicated motorcycle parking spaces are proposed as part of the development. Accordingly, the proposal does not satisfy Acceptable Solution A1 of Clause C2.5.3.

The Performance Criteria P1 of Clause C2.5.3 of the Planning Scheme states:

"Motorcycle parking spaces for all uses must be provided to meet the reasonable needs of the use, having regard to:

- (a) the nature of the proposed use and development;*
- (b) the topography of the site;*
- (c) the location of existing buildings on the site;*
- (d) any constraints imposed by existing development; and*
- (e) the availability and accessibility of motorcycle parking spaces on the street or in the surrounding area".*

The following is relevant with respect to the development proposal:

- a. Nature of use. The proposed use is a small-scale indoor recreation facility located within an existing commercial tenancy. The assessment of patron behaviour indicates that the majority of users attend in small groups (2–4 persons), and travel is predominantly by private car, ride-share and public transport. There is no evidence to suggest a material proportion of patrons will travel by motorcycle. Staff numbers are low (generally 1–3 persons), further limiting the likelihood of routine motorcycle commuting. On this basis, the reasonable demand for dedicated motorcycle parking is expected to be low.
- b. Topography of site. The development is contained within an existing built commercial site with established levels and access arrangements. There are no topographical features that would create a specific need for dedicated motorcycle parking (such as large gradients or separated areas requiring protected motorcycle parking). The topography does not drive the need for additional motorcycle facilities.
- c. Location of existing buildings. The existing building footprint and site configuration constrain available external area. The car parking area is an established, compact layout serving the tenancy. There is limited opportunity to introduce additional formal motorcycle parking spaces without reconfiguring the existing parking layout or reducing the number/ functionality of existing car spaces.
- d. Site constraints. The site is constrained by its existing development form, access arrangement and established car parking supply. No changes to the car parking layout are proposed and the existing 14 spaces will be retained. Creating a dedicated motorcycle bay would either reduce the



number of existing car spaces or introduce a non-standard arrangement within an already constrained layout. Given the low expected motorcycle demand, such changes are not considered reasonable or necessary.

- e. Availability and accessibility of motorcycle spaces in surrounding area. The site is located within an established commercial corridor where on-street parking is available in the surrounding road network. A motorcycle can generally be accommodated within on-street parking areas (subject to signage and demand) and within available kerbside spaces in the broader locality. Given the low expected motorcycle mode share for the use, the availability of surrounding kerbside parking provides a practical alternative to dedicated on-site motorcycle bays.

Having regard to the small scale of the use, the low likelihood of significant motorcycle patronage, and the physical constraints of the existing site and parking layout, the provision of a dedicated on-site motorcycle parking space is not considered necessary to meet the reasonable needs of the use. The proposal is therefore considered to satisfy Performance Criteria P1 of Clause C2.5.3.

5.6 Bicycle Parking

The Acceptable Solution A1 of Clause C2.5.2 of the Planning Scheme states that "*bicycle parking must (a) be provided on the site or within 50m of the site; and (b) be no less than the number specified in Table C2.1*".

Table C2.1 specifies 'No Requirement' for 'Sports and Recreation excluding as otherwise specified in this Table'. No bicycle parking is proposed, thereby satisfying the requirements of Acceptable Solution A1 of Clause C2.5.2 of the Planning Scheme.

5.7 Servicing and Commercial Vehicles

The previous use of the site as a building supply / hardware store would have required regular servicing by heavy vehicles, including deliveries of construction materials such as timber, doors and window frames. These activities typically involve larger vehicles, longer dwell times and greater interaction with the road network.

In contrast, the proposed indoor golf facility will generate minimal servicing demand. Deliveries will be infrequent and limited to small commercial vehicles (e.g. vans) associated with food/beverage supply and general maintenance. These activities are expected to occur during daytime periods when site parking demand is low (typically 5–8 vehicles on site at any one time).

Accordingly, the proposed development represents a substantial reduction in heavy vehicle activity relative to the former use and will reduce potential conflicts with general traffic, pedestrians and on-street parking.

Servicing can be accommodated within the existing site, consistent with surrounding commercial uses, and will not adversely impact traffic operations or parking availability.



5.8 Car Parking Layout

No changes to the existing on-site car parking layout are proposed as part of this development. The as-surveyed parking layout is shown in Figure 5.

The site currently contains 14 on-site parking spaces accessed via the existing Argyle Street crossover. These spaces have been in continuous operation for many years in association with the former building supply / bulky goods retail use of the site.

The proposal does not alter the site access arrangement, parking layout, internal circulation or the number of parking spaces provided. The existing parking configuration will be retained in its current form.

As no physical works or modifications to the car parking area are proposed, and the layout has been operating without issue for many years, no further assessment of car parking design compliance is required as part of this application.



6. Conclusions

This Traffic Impact Assessment (TIA) has investigated the traffic and parking impacts associated with the proposed indoor golf simulator facility (X-Golf) at 310 Argyle Street, North Hobart.

The key findings of this assessment are summarised as follows:

Traffic Generation

- The proposed development is expected to generate approximately 66 to 68 two-way vehicle movements per day on average.
- Peak activity is anticipated during evening periods, with up to approximately 25 vehicle arrivals in the peak arrival hour and a similar number of departures following organised league sessions.
- Daily traffic generation associated with the proposed use is substantially lower than the estimated traffic generation of the former building supply / bulky goods use (approximately 234 vehicle movements per day).

Access and Network Impacts

- The proposal utilises an existing access that has operated for many years without issue.
- Sight distance at the access complies with AS2890.1 requirements.
- The proposal results in a net reduction in daily traffic relative to the previous use and therefore satisfies Acceptable Solution A1.4 of Clause C3.5.1 of the Planning Scheme.

Road Safety

- Crash data indicates no systemic safety deficiencies in the vicinity of the site access.
- The proposed development generates materially less traffic than the previous use.
- The proposal is not expected to adversely affect the safety performance of Argyle Street or the surrounding road network.

Parking

The proposed development retains the existing 14 on-site car parking spaces.

Under the Acceptable Solution A1 of Clause C2.5.1, the change of use results in a calculated requirement of approximately 49 spaces. This requirement is derived from a generic rate and does not reflect the actual operational characteristics of the proposed use.

A first-principles assessment indicates that:

- Typical weekday daytime demand is low (5–8 vehicles); and
- Peak parking demand occurs during evening periods and is in the order of 20–25 vehicles.



Accordingly, the majority of parking demand is accommodated on-site, with a modest overflow demand of approximately 10–13 vehicles during peak periods.

Updated on-street parking surveys undertaken under typical weekday and weekend conditions demonstrate that:

- Measurable spare capacity exists within the surrounding road network at all times; and
- Parking availability is greatest during evening periods, when the proposed development experiences its highest demand.

This establishes a strong alignment between peak demand and peak parking availability, confirming that overflow parking can be accommodated within the surrounding network without adverse impact.

The site is also highly accessible by public transport and alternative transport modes, and the nature of the use supports carpooling and ride-share activity, further reducing parking demand. In addition, the site is constrained by its existing built form and there is no practical opportunity to increase on-site parking provision without significant and unreasonable alteration to the development.

Having regard to the demonstrated parking demand, the temporal variation in demand, the availability of on-street parking during peak periods, and the physical constraints of the site, the provision of 14 on-site spaces is considered to meet the reasonable needs of the use.

Overall Conclusion

Based on the findings of this assessment, the proposed development will not adversely impact the operation, safety or efficiency of the surrounding road network and is supported on traffic and parking grounds.



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Document Status

Revision	Author	Review	Date
0	Keith Midson	Zara Kacic-Midson	15 February 2026
1	Keith Midson	Zara Kacic-Midson	18 March 2026

**Installation :**

Project number : 312 Argyle Street
Customer : 312 Argyle Street
Processed by :
Date : 14.04.2026

Project description:

Tony Turnbull Middys TechEnergy Lighting Design Ph : 0408 579 780
Illuminating Engineering Society registered #4575 TIES

The attached lighting design is provided free of charge.
Compliance to Australian Lighting Standards is based on our interpretation of the advised layout, plans and information provided for the task application of the space.
This design assumes standard reflectance's and a typical layout type.
Whilst every effort is made to correctly interpret project requirements, no responsibility will be accepted for errors or omissions.
Please note that this lighting design incorporates the lifetime lumen depreciation which occurs with all luminaires through dirt buildup and equipment degradation

Compliant to AS1680.1 Interior Lighting
Compliant to AS2293.1.2018 Emergency Lighting
Compliant to AS1158.3.1.2020 PC3
Compliant to AS4282 Pre curfew

The following values are based on precise calculations performed on calibrated lamps and luminaires, and their configurations, whereby gradual, unavoidable deviations can occur in practice. All guarantee claims are excluded for the specified data.

This exclusion of liability applies irrespective of the legal grounds for both damages and consequential damages suffered by users and third parties.

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



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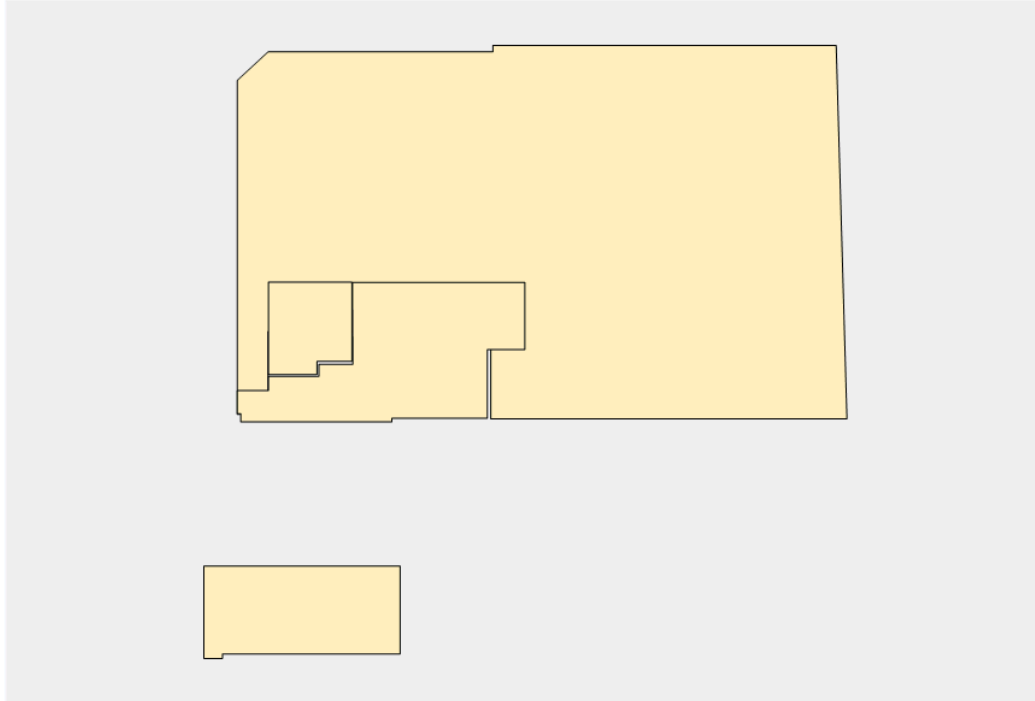
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Summary, Floor 1

.1 Floor overview



Number of rooms	4	
Total area	534.0 m ²	Calculated
Total volume	3184.4 m ³	
Number of luminaires	52	
Total luminous flux	279179 lm	
Total power	1934.4 W	
Total power per area	3.62 W/m ²	

Parts list

Type No.\Make

- HANECO**

2 7 x  Order No. :
 Luminaire name : SKYLUX200W-G4-150W-40K
 Equipment : 1 x SMD 2835/LED 147.6 W / 25323 lm

- 9 5 x  Order No. : DULAR-EV-TRI-4000K
 Luminaire name :
 Equipment : 1 x 7.1 W / 800 lm

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



Summary, Floor 1

.1 Floor overview

- 3 6 x

PIERLITE

Order No. : ECOBAT404ECSLSSG4 5000K 36W 0.35A

Luminaire name : ECO LED BATTEN GEN4 36W 4FT SENSOR COL SELECT & LUMEN SELECT N ON DIM

Equipment : 1 x 36 W / 4635 lm
- 10 14 x

Order No. : ECOBAT404ECSLSG4 4000K 35W 0.35A

Luminaire name : ECO LED BATTEN GEN4 36W 4FT COL SELECT & LUMEN SELECT NON DIM

Equipment : 1 x 36 W / 4300 lm
- 4 1 x

Clevertronics Pty. Ltd.,

Order No. : CFLLED LH Spot

Luminaire name : Clevertronics LP Supalite LED Emergency Luminaire.

Equipment : 2 x / 92.5 lm
- 5 1 x

Order No. : CFLLED RH Spot

Luminaire name : Clevertronics LP Supalite LED Emergency Luminaire.

Equipment : 2 x / 92.5 lm
- 6 3 x

Clevertronics Pty. Ltd., 1 Caribbean Drive, Scoresby, VIC 3179.

Order No. : CLIFE-PRO-SM

Luminaire name : Clevertronics LP LifeLight Pro Surface EM D80

Equipment : 2 x / 226.5 lm
- 7 5 x

Clevertronics Pty. Ltd., Moorabbin, VIC 3189.

Order No. : CCFLED

Luminaire name : Clevertronics LP Cleverfit Emergency LED Exit GEN2 Luminaire

Equipment : 6 x / 5.5 lm
- 8 2 x

Clevertronics,

Order No. : CLIFE

Luminaire name : Clevertronics LP Lifelight Recessed EM Disc D40

Equipment : 1 x 2.5 W / 215 lm
- 14 8 x

SAL

Order No. : I

Luminaire name : SMT9017/15TC/WH/3000K

Equipment : 1 x 14.5 W / 948 lm

Object :
 Installation :
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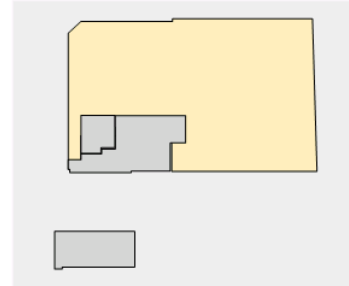


Summary, Floor 1

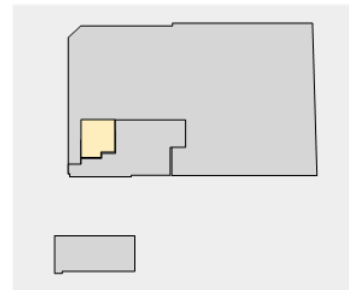
.1 Floor overview

Rooms

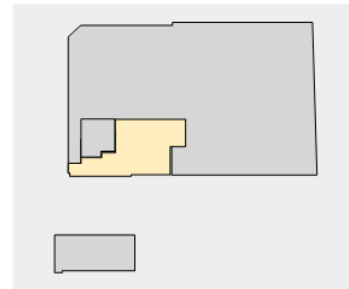
Seating Golf Sim Stores 22 x Luminaires
 Floor area 427.7 m²
 Wall area (without windows) 659.0 m²
 Window area 0.0 m²
 Volume 2908.2 m³
 Total luminous flux 206932 lm
 Total power 1272.9 W
 Total power per area (428 m²) 2.98 W/m²
 E_m 229 lx
 E_{min} 32 lx
 E_{min}/E_m (U_o) 0.14
 RUG 30.0



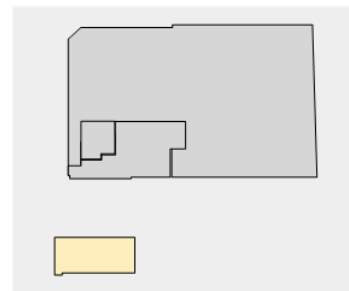
Amenities 6 x Luminaires
 Floor area 15.2 m²
 Wall area (without windows) 43.5 m²
 Window area 0.0 m²
 Volume 41.1 m³
 Total luminous flux 4215 lm
 Total power 38 W
 Total power per area (15 m²) 2.49 W/m²
 E_m 128 lx
 E_{min} 59 lx
 E_{min}/E_m (U_o) 0.46
 RUG ---



Bar Kitchen 16 x Luminaires
 Floor area 54.8 m²
 Wall area (without windows) 105.6 m²
 Window area 0.0 m²
 Volume 147.9 m³
 Total luminous flux 41984 lm
 Total power 404 W
 Total power per area (55 m²) 7.37 W/m²
 E_m 318 lx
 E_{min} 73 lx
 E_{min}/E_m (U_o) 0.23
 RUG ---



Office 8 x Luminaires
 Floor area 36.3 m²
 Wall area (without windows) 63.4 m²
 Window area 0.0 m²
 Volume 87.2 m³
 Total luminous flux 26048 lm
 Total power 219.5 W
 Total power per area (36 m²) 6.04 W/m²
 E_m 354 lx
 E_{min} 276 lx
 E_{min}/E_m (U_o) 0.78
 RUG <=27.3



Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026




1 Seating Golf Sim Stores


1.1 Description, Seating Golf Sim Stores

1.1.1 Luminaire data/Room elements

Product data:


Type No.\Make


HANECO
 2 7 x  Order No. :
 Luminaire name : SKYLUX200W-G4-150W-40K
 Equipment : 1 x SMD 2835/LED 147.6 W / 25323 lm

PIERLITE
 3 6 x  Order No. : ECOBAT404ECSLSSG4 5000K 36W 0.35A
 Luminaire name : ECO LED BATTEN GEN4 36W 4FT SENSOR COL SELECT & LUMEN SELECT N ON DIM
 Equipment : 1 x 36 W / 4635 lm

Clevertronics Pty. Ltd.,
 4 1 x  Order No. : CFLLED LH Spot
 Luminaire name : Clevertronics LP Supalite LED Emergency Luminaire.
 Equipment : 2 x / 92.5 lm

5 1 x  Order No. : CFLLED RH Spot
 Luminaire name : Clevertronics LP Supalite LED Emergency Luminaire.
 Equipment : 2 x / 92.5 lm

Clevertronics Pty. Ltd., 1 Caribbean Drive, Scoresby, VIC 3179.
 6 3 x  Order No. : CLIFE-PRO-SM
 Luminaire name : Clevertronics LP LifeLight Pro Surface EM D80
 Equipment : 2 x / 226.5 lm

Clevertronics Pty. Ltd., Moorabbin, VIC 3189.
 7 4 x  Order No. : CCFLED
 Luminaire name : Clevertronics LP Cleverfit Emergency LED Exit GEN2 Luminaire
 Equipment : 6 x / 5.5 lm

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



1 Seating Golf Sim Stores

1.1 Description, Seating Golf Sim Stores

1.1.1 Luminaire data/Room elements

Floor with luminaire and sensor positions:



Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



1 Seating Golf Sim Stores

1.1 Description, Seating Golf Sim Stores

1.1.1 Luminaire data/Room elements

No.	Centre point			Rotation angle			Target coordinates		
	X [m]	Y [m]	Z [m]	Z [°]	C0 [°]	C90 [°]	Xa [m]	Ya [m]	Za [m]
HANECO SKYLUX200W-G4-150W-40K									
1.1	0.89	3.93	6.80	0.00	0.00	0.00	0.89	3.93	0.00
1.2	7.07	3.93	6.80	0.00	0.00	0.00	7.07	3.93	0.00
1.3	13.25	3.93	6.80	0.00	0.00	0.00	13.25	3.93	0.00
1	12.28	-1.67	6.80	0.00	0.00	0.00	12.28	-1.67	0.00
2	16.20	11.70	6.80	0.00	0.00	0.00	16.20	11.70	0.00
3	16.20	1.20	6.80	0.00	0.00	0.00	16.20	1.20	0.00
19	16.20	6.40	6.80	0.00	0.00	0.00	16.20	6.40	0.00
PIERLITE ECO LED BATTEN GEN4 36W 4FT SENSOR COL SELECT & LUMEN SELECT NON DIM ECOBAT404ECSLSSG4 5000K 36W 0.35A									
4	24.40	12.20	3.00	0.00	0.00	0.00	24.40	12.20	0.00
5	25.60	7.70	3.00	270.00	0.00	0.00	25.60	7.70	0.00
6	25.80	-1.80	3.00	270.00	0.00	0.00	25.80	-1.80	0.00
7	25.70	2.90	3.00	270.00	0.00	0.00	25.70	2.90	0.00
8	-0.80	10.80	3.00	270.00	0.00	0.00	-0.80	10.80	0.00
18	-0.70	-0.80	6.80	0.00	0.00	0.00	-0.70	-0.80	1.80
Clevertronics Pty. Ltd., Clevertronics LP Supalite LED Emergency Luminaire. CFLLED LH Spot									
9	-1.30	-0.60	6.30	1.60	14.92	-6.21	-0.68	0.99	0.40
Clevertronics Pty. Ltd., Clevertronics LP Supalite LED Emergency Luminaire. CFLLED RH Spot									
10	-1.30	-1.00	6.30	357.60	-14.81	-9.31	-0.42	-2.50	0.77
Clevertronics Pty. Ltd., 1 Caribbean Drive, Scoresby, VIC 3179. Clevertronics LP LifeLight Pro Surface EM D80 CLIFE-PRO-SM									
11	17.10	11.80	6.80	0.00	0.00	0.00	17.10	11.80	0.00
12	10.10	3.90	6.80	0.00	0.00	0.00	10.10	3.90	0.00
13	13.50	-1.40	6.80	0.00	0.00	0.00	13.50	-1.40	0.00
Clevertronics Pty. Ltd., Moorabbin, VIC 3189. Clevertronics LP Cleverfit Emergency LED Exit GEN2 Luminaire CCFLED									
14	-1.10	3.90	2.62	270.00	0.00	0.00	-1.10	3.90	0.00
15	12.30	-3.90	2.62	90.00	0.00	0.00	12.30	-3.90	0.00
16	14.80	11.80	2.62	0.00	0.00	0.00	14.80	11.80	0.00
17	15.00	4.30	2.62	0.00	0.00	0.00	15.00	4.30	0.00

Structural elements

Measuring surface

No.	xm[m]	ym[m]	zm[m]	Length	Width	Height	Rotation angle	
							z axis	L axis
Ref. plane 1.1								
	-0.90	-3.30	0.10	26.89	16.20	0.00	0.00	0.00
M 1.1	-0.12	1.20	0.21	1.15	0.16	0.00	180.00	0.00
M 2.1	-0.12	0.95	0.41	1.15	0.16	0.00	180.00	0.00
M 3.1	-0.12	0.70	0.61	1.15	0.16	0.00	180.00	0.00
M 4.1	-0.12	0.45	0.81	1.15	0.16	0.00	180.00	0.00
M 5.1	-0.12	0.20	1.01	1.15	0.16	0.00	180.00	0.00
M 6.1	-0.12	-0.05	1.21	1.15	0.16	0.00	180.00	0.00
M 7.1	-0.12	-0.30	1.41	1.15	0.16	0.00	180.00	0.00
M 8.1	-0.12	-0.55	1.61	1.15	0.16	0.00	180.00	0.00
M 9.1	-0.12	-0.80	1.81	1.15	0.16	0.00	180.00	0.00
M 10.1	-0.12	-1.05	2.01	1.15	0.16	0.00	180.00	0.00

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



1 Seating Golf Sim Stores

1.1 Description, Seating Golf Sim Stores

1.1.1 Luminaire data/Room elements

M 11.1	-0.12	-1.30	2.21	1.15	0.16	0.00	180.00	0.00
M 12.1	-0.12	-1.55	2.41	1.15	0.16	0.00	180.00	0.00
M 13.1	-0.12	-1.80	2.61	1.15	0.16	0.00	180.00	0.00
M 14.1	-0.12	-2.05	2.81	1.15	0.16	0.00	180.00	0.00
M 15.1	-0.12	-2.30	3.01	1.15	0.16	0.00	180.00	0.00
M 1	-1.37	-2.52	3.10	1.34	1.55	0.00	0.00	0.00

Others

No.	xm[m]	ym[m]	zm[m]	Length	Width	Height	Rotation angle		
							z axis	L axis	Q axis
Tr 1	0.01	12.54	0.00	0.27	2.94	3.00	270.00	0.00	0.00
Tr 2	4.84	12.80	0.00	0.21	3.16	3.00	270.00	0.00	0.00
Tr 3	9.79	12.79	0.00	0.29	3.15	3.00	270.00	0.00	0.00
Tr 4	14.69	13.06	0.00	0.29	3.43	3.00	270.00	0.00	0.00
Tr 5	22.17	11.29	0.00	2.92	14.93	3.00	0.00	0.00	0.00
Tr 6	24.74	1.50	0.00	2.61	0.25	3.00	0.00	0.00	0.00
Tr 7	24.77	6.43	0.00	2.62	0.25	3.00	0.00	0.00	0.00
A 1	0.03	12.52	3.00	14.37	2.88	0.10	0.00	0.00	0.00
A 2	22.17	11.04	3.00	2.62	14.39	0.10	0.00	0.00	0.00
A 1.1	-0.07	1.24	0.00	1.24	0.23	0.20	180.00	0.00	0.00
A 2.1	-0.07	0.99	0.20	1.24	0.23	0.20	180.00	0.00	0.00
A 3.1	-0.07	0.74	0.40	1.24	0.23	0.20	180.00	0.00	0.00
A 4.1	-0.07	0.49	0.60	1.24	0.23	0.20	180.00	0.00	0.00
A 5.1	-0.07	0.24	0.80	1.24	0.23	0.20	180.00	0.00	0.00
A 6.1	-0.07	-0.01	1.00	1.24	0.23	0.20	180.00	0.00	0.00
A 7.1	-0.07	-0.26	1.20	1.24	0.23	0.20	180.00	0.00	0.00
A 8.1	-0.07	-0.51	1.40	1.24	0.23	0.20	180.00	0.00	0.00
A 9.1	-0.07	-0.76	1.60	1.24	0.23	0.20	180.00	0.00	0.00
A 10.1	-0.07	-1.01	1.80	1.24	0.23	0.20	180.00	0.00	0.00
A 11.1	-0.07	-1.26	2.00	1.24	0.23	0.20	180.00	0.00	0.00
A 12.1	-0.07	-1.51	2.20	1.24	0.23	0.20	180.00	0.00	0.00
A 13.1	-0.07	-1.76	2.40	1.24	0.23	0.20	180.00	0.00	0.00
A 14.1	-0.07	-2.01	2.60	1.24	0.23	0.20	180.00	0.00	0.00
A 15.1	-0.07	-2.26	2.80	1.24	0.23	0.20	180.00	0.00	0.00
A 3	-1.40	-2.50	0.00	1.39	1.59	3.00	0.00	0.00	0.00
A 4	-0.01	0.51	0.00	11.81	3.23	3.00	90.00	0.00	0.00
Tr 8	14.12	-4.01	0.00	0.11	2.00	3.00	90.00	0.00	0.00
Tr 9	-1.40	9.60	0.00	1.10	0.11	3.00	0.00	0.00	0.00

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



1.1 Description, Seating Golf Sim Stores, Light scene 1(C)

1.1.2 Floor plan



Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



1.1 Description, Seating Golf Sim Stores, Light scene 1(C)

1.1.2 Floor plan

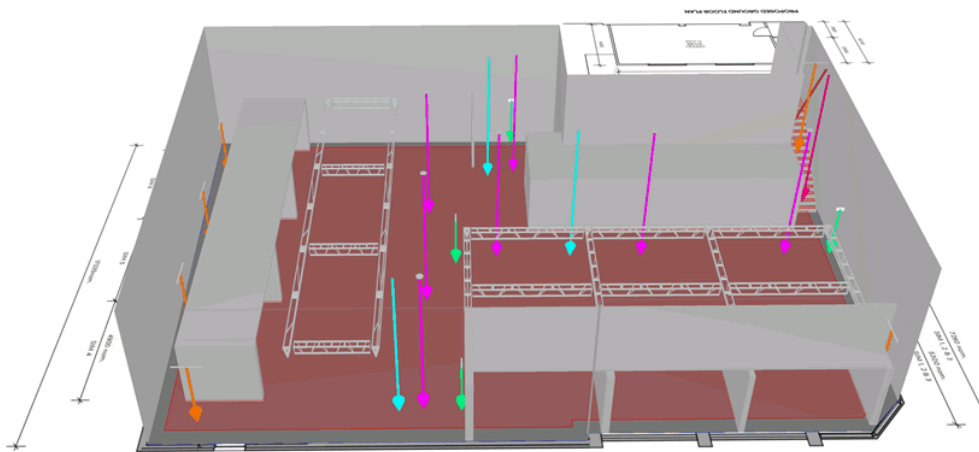
Wall	x	y	Length	Area[m ²]	Reflectance
1	10.20 m	0.00 m	10.20 m	69.36	50.0 %
2	10.20 m	-4.00 m	4.00 m	27.20	50.0 %
3	26.50 m	-4.00 m	16.30 m	110.84	50.0 %
4	26.00 m	13.10 m	17.11 m	116.33	50.0 %
5	10.30 m	13.10 m	15.70 m	106.76	50.0 %
6	10.30 m	12.80 m	0.30 m	2.04	50.0 %
7	0.00 m	12.80 m	10.30 m	70.04	50.0 %
8	-1.40 m	11.50 m	1.91 m	12.99	50.0 %
9	-1.40 m	-3.80 m	15.30 m	104.04	50.0 %
10	-1.20 m	-3.80 m	0.20 m	1.36	50.0 %
11	-1.20 m	-4.10 m	0.30 m	2.04	50.0 %
12	0.00 m	-4.10 m	1.20 m	8.16	50.0 %
13	0.00 m	0.00 m	4.10 m	27.88	50.0 %
Floor:				427.7	20.0 %
Ceiling:				427.7	70.0 %
Room height [m]:		6.80 m			
Height of ref. plane [m]:		0.10 m			
Volume					2908.2 m ³

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



1.1 Description, Seating Golf Sim Stores

1.1.3 3D view, View 1



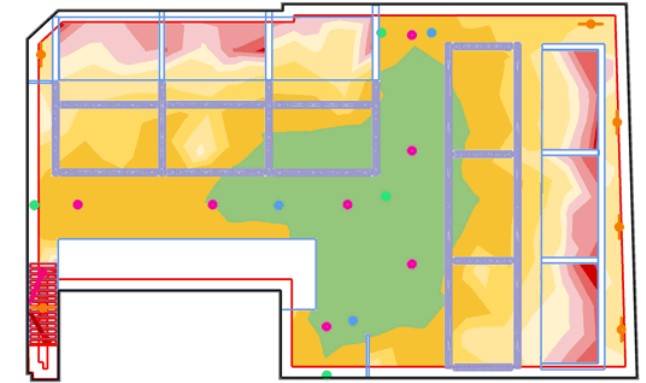
Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



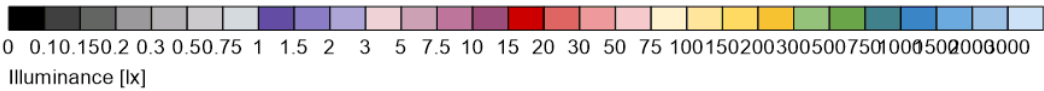
1 Seating Golf Sim Stores

1.2 Summary, Seating Golf Sim Stores, Light scene 1(C)

1.2.17 Result overview, Evaluation area 1



-1.4 1.1 3.6 6.1 8.6 11.1 13.6 16.1 18.6 21.1 23.6 x [m]



General

Calculation algorithm used	Average indirect fraction
Maintenance factor	0.70
Total lamp luminous flux	206932.00 lm
Luminaire luminous flux	206930.78 lm
Total power	1272.9 W
Total power per area (427.67 m ²)	2.98 W/m ² (1.30 W/m ² /100lx)

Evaluation area 1

Reference plane 1.1

\bar{E}_m	Horizontal	cylindrical
E_{min}	229 lx	89 lx
$E_{min}/\bar{E}_m (U_o)$	32 lx	24 lx
$E_{min}/E_{max} (U_d)$	0.14	0.27
E_z/E_h	0.07	0.37
Position	0.10 m	0.10 m
RUG (3.1H 5.0H)	30.0	

Luminaire :
 (SKYLUX200W-G4-150W-40K, SMD 2835/LED, , 147.6 W)
 Hints:
 - The RUG value has been limited to the upper bound of 30.

Type No.\Make







Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



1 Seating Golf Sim Stores

1.2 Summary, Seating Golf Sim Stores, Light scene 1(C)

1.2.17 Result overview, Evaluation area 1

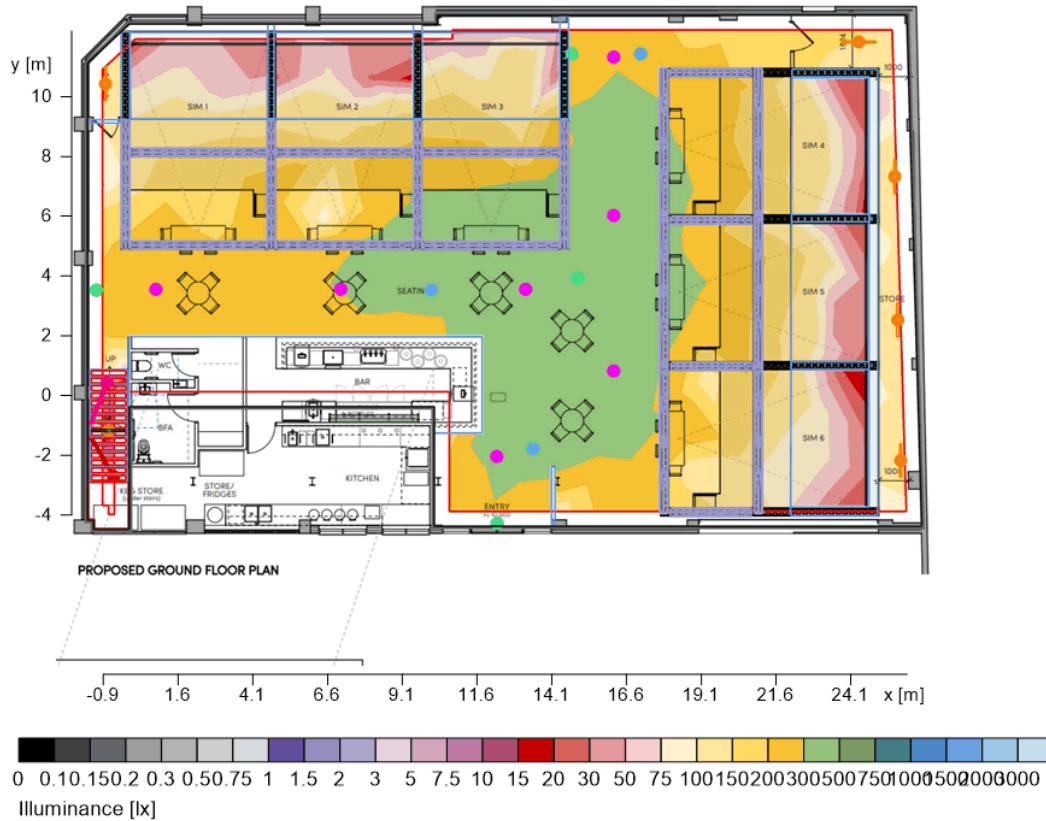
- 2 7 x  **HANECO**
Order No. :
Luminaire name : SKYLUX200W-G4-150W-40K
Equipment : 1 x SMD 2835/LED 147.6 W / 25323 lm
- 3 6 x  **PIERLITE**
Order No. : ECOBAT404ECSLSSG4 5000K 36W 0.35A
Luminaire name : ECO LED BATTEN GEN4 36W 4FT SENSOR COL SELECT & LUMEN SELECT N ON DIM
Equipment : 1 x 36 W / 4635 lm
- 4 1 x  **Clevertronics Pty. Ltd.,**
Order No. : CFLLED LH Spot
Luminaire name : Clevertronics LP Supalite LED Emergency Luminaire.
Equipment : 2 x / 92.5 lm
- 5 1 x  Order No. : CFLLED RH Spot
Luminaire name : Clevertronics LP Supalite LED Emergency Luminaire.
Equipment : 2 x / 92.5 lm
- 6 3 x  **Clevertronics Pty. Ltd., 1 Caribbean Drive, Scoresby, VIC 3179.**
Order No. : CLIFE-PRO-SM
Luminaire name : Clevertronics LP LifeLight Pro Surface EM D80
Equipment : 2 x / 226.5 lm
- 7 4 x  **Clevertronics Pty. Ltd., Moorabbin, VIC 3189.**
Order No. : CCFLED
Luminaire name : Clevertronics LP Cleverfit Emergency LED Exit GEN2 Luminaire
Equipment : 6 x / 5.5 lm

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



1.3 Calculation results, Seating Golf Sim Stores, Light scene 1(C)

1.3.20 Pseudo colours, Reference plane 1.1 (E)



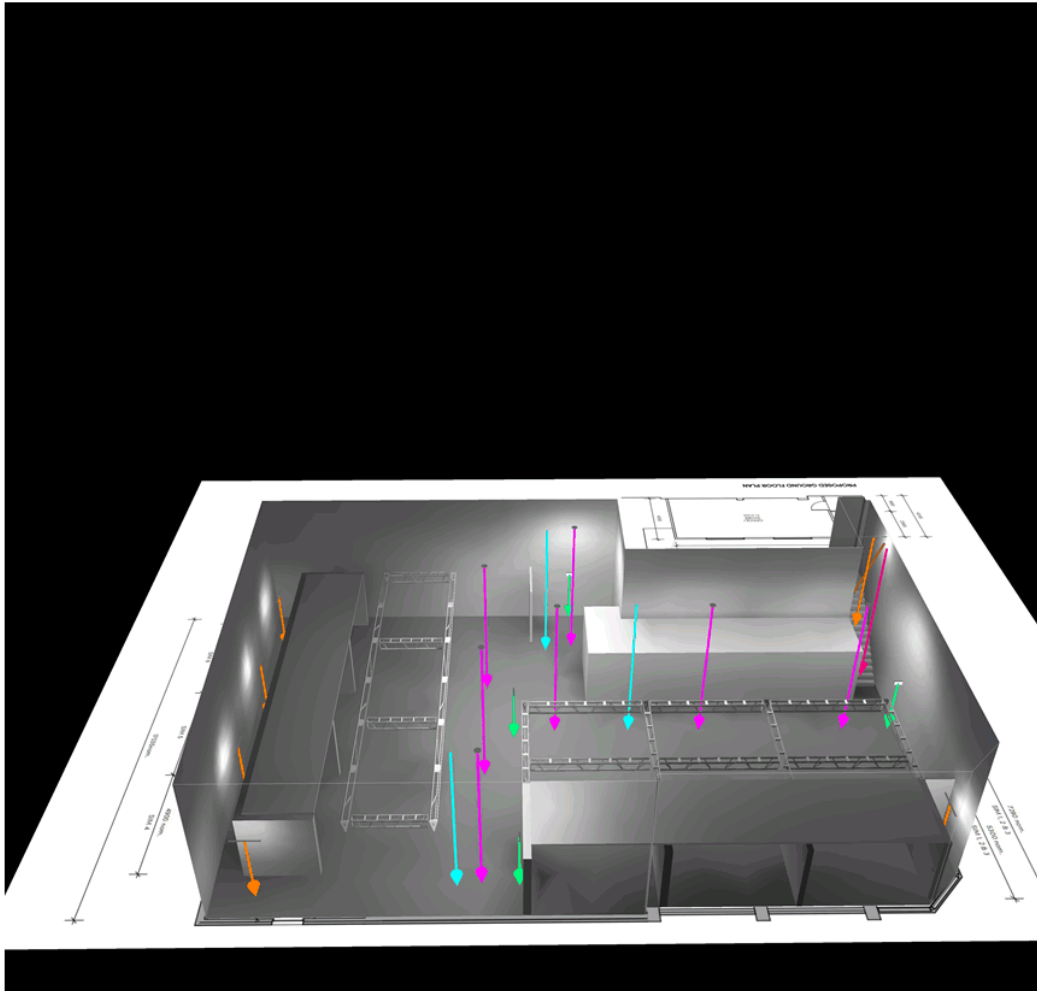
Height reference plane		: 0.10 m
Average illuminance	\bar{E}_m	: 229 lx
Minimum illuminance	E_{min}	: 32 lx
Maximum illuminance	E_{max}	: 464 lx
Uniformity U_0	E_{min}/\bar{E}_m	: 1 : 7.04 (0.14)

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



1.3 Calculation results, Seating Golf Sim Stores, Light scene 1(C)

1.3.38 3D luminance, View 1



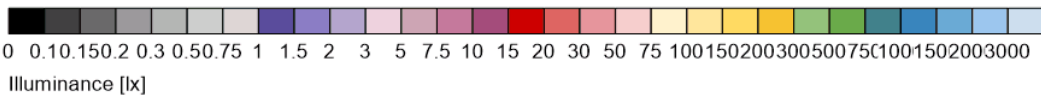
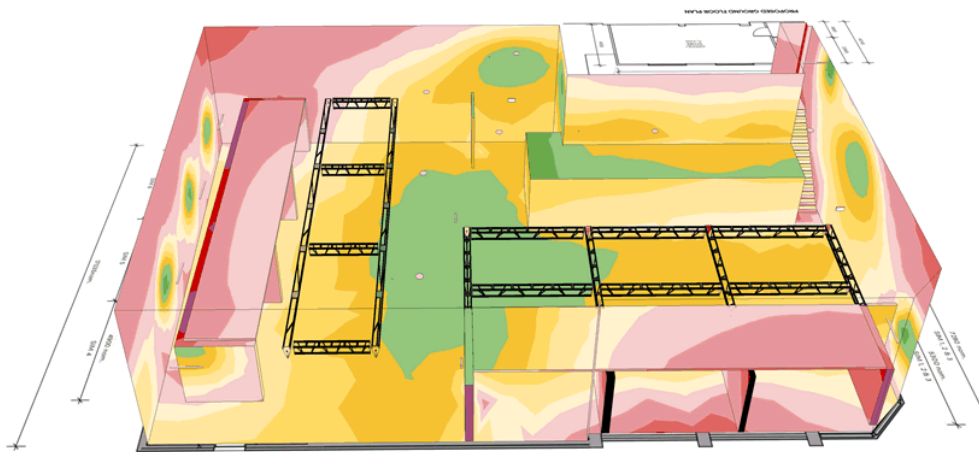
Luminance in the scene
Minimum: : 0 cd/m²
Maximum: : 138 cd/m²

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



1.3 Calculation results, Seating Golf Sim Stores, Light scene 1(C)

1.3.39 3D pseudo colours, View 1 (E)



Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026




2 Amenities

2.1 Description, Amenities

2.1.1 Luminaire data/Room elements

Product data:

Type No.\Make

9	5 x		HANECO Order No. : DULAR-EV-TRI-4000K Luminaire name : Equipment : 1 x 7.1 W / 800 lm
---	-----	---	---

8	1 x		Clevertronics, Order No. : CLIFE Luminaire name : Clevertronics LP Lifelight Recessed EM Disc D40 Equipment : 1 x 2.5 W / 215 lm
---	-----	---	--

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



2 Amenities

2.1 Description, Amenities

2.1.1 Luminaire data/Room elements

Floor with luminaire and sensor positions:



No.	Centre point			Rotation angle			Target coordinates		
	X [m]	Y [m]	Z [m]	Z [°]	C0 [°]	C90 [°]	Xa [m]	Ya [m]	Za [m]
HANECO DULAR-EV-TRI-4000K									
2	1.01	3.35	2.70	0.00	0.00	0.00	1.01	3.35	0.00
3	0.89	2.20	2.70	0.00	0.00	0.00	0.89	2.20	0.00
4	0.93	0.55	2.70	0.00	0.00	0.00	0.93	0.55	0.00
5	3.08	3.36	2.70	0.00	0.00	0.00	3.08	3.36	0.00
6	3.08	1.36	2.70	0.00	0.00	0.00	3.08	1.36	0.00
Clevertronics, Clevertronics LP Lifelight Recessed EM Disc D40 CLIFE									
1	1.09	1.48	2.70	0.00	0.00	0.00	1.09	1.48	0.00

Structural elements

Measuring surface

No.	xm[m]	ym[m]	zm[m]	Length	Width	Height	Rotation angle	
							z axis	L axis
Ref. plane 1.1	0.50	0.50	0.10	2.82	3.24	0.00	0.00	0.00

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



2 Amenities

2.1 Description, Amenities

2.1.1 Luminaire data/Room elements

Others

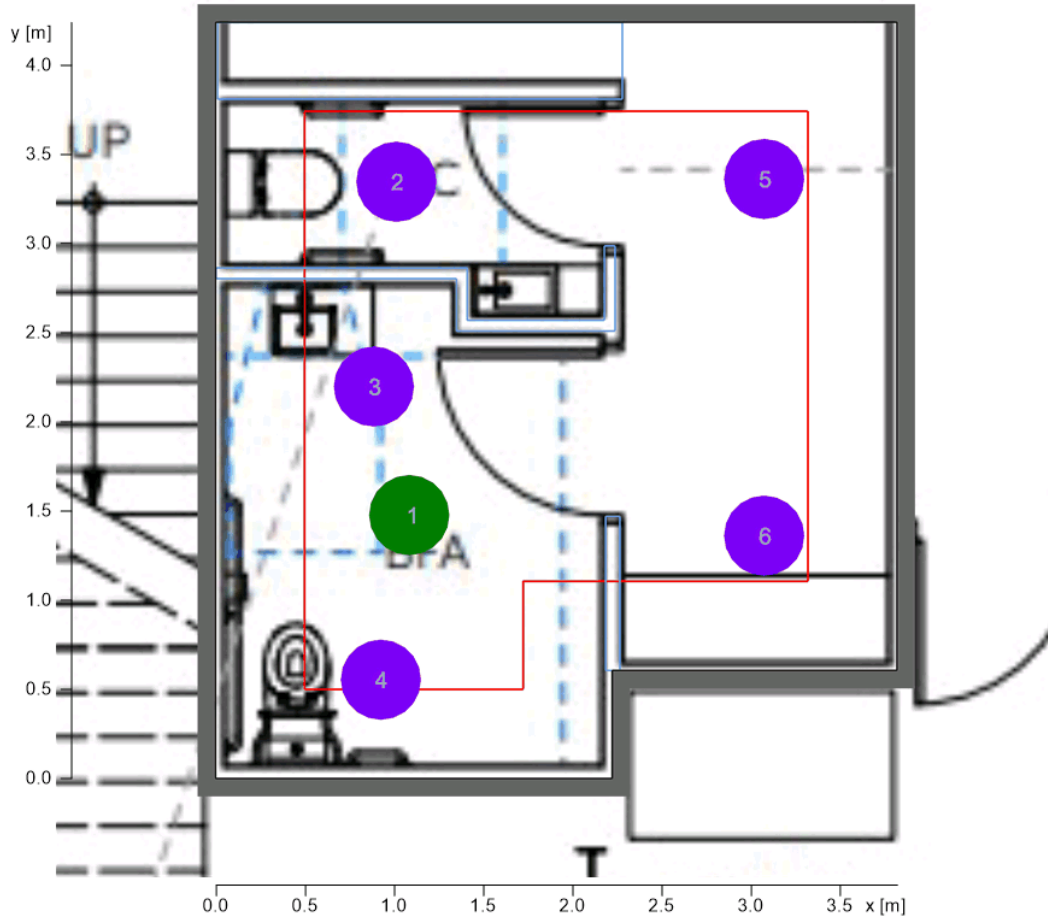
No.	xm[m]	ym[m]	zm[m]	Length	Width	Height	Rotation angle		
							z axis	L axis	Q axis
Tr 1	0.00	2.86	0.00	2.24	0.48	2.70	0.00	0.00	0.00
Tr 2	2.19	1.47	0.00	0.08	0.86	2.70	270.00	0.00	0.00
Tr 3	0.01	3.81	0.00	2.27	0.43	2.70	0.00	0.00	0.00

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



2.1 Description, Amenities, Light scene 1(C)

2.1.2 Floor plan



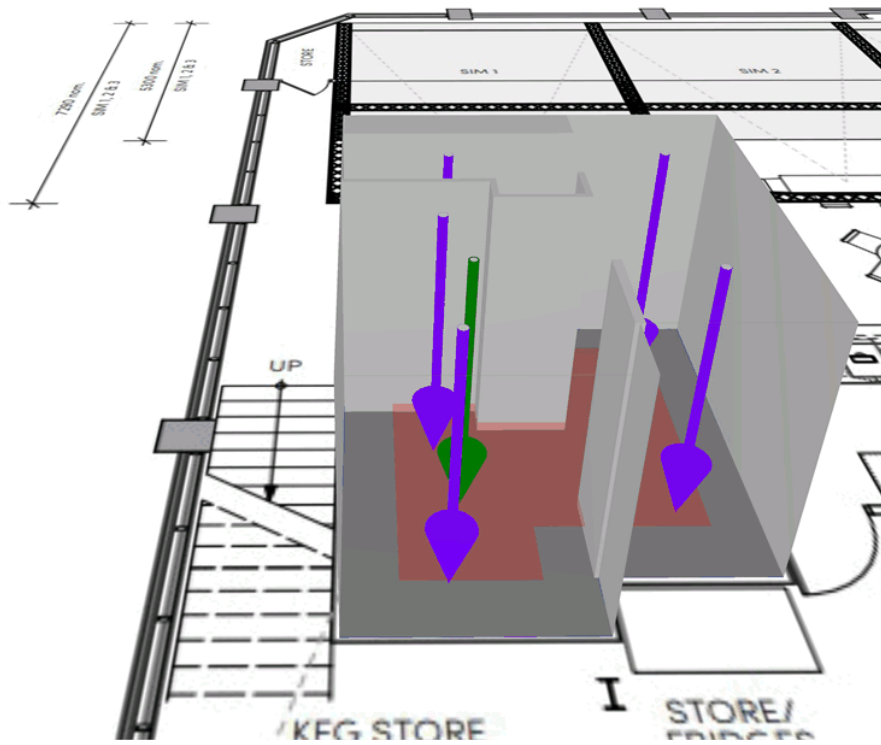
Wall	x	y	Length	Area[m ²]	Reflectance
1	2.25 m	-1.97 m	2.22 m	6.00	50.0 %
2	2.25 m	-1.37 m	0.61 m	1.64	50.0 %
3	3.84 m	-1.37 m	1.60 m	4.31	50.0 %
4	3.84 m	2.27 m	3.64 m	9.81	50.0 %
5	0.02 m	2.27 m	3.82 m	10.32	50.0 %
6	0.02 m	-1.97 m	4.24 m	11.45	50.0 %
Floor:				15.2	20.0 %
Ceiling:				15.2	70.0 %
Room height [m]:		2.70 m			
Height of ref. plane [m]:		0.10 m			
Volume					41.1 m ³

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



2.1 Description, Amenities

2.1.3 3D view, View 1



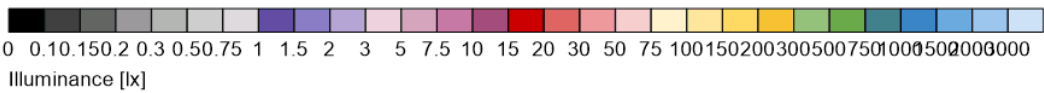
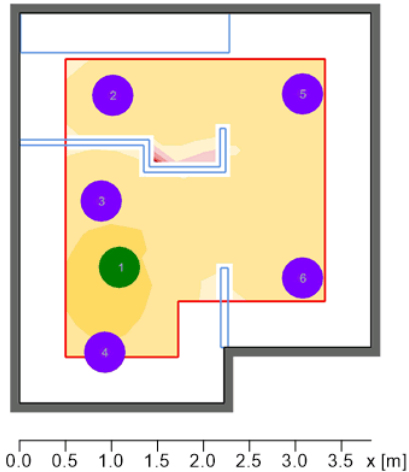
Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



2 Amenities

2.2 Summary, Amenities, Light scene 1(C)

2.2.1 Result overview, Evaluation area 1



General

Calculation algorithm used	Average indirect fraction
Height of luminaire plane	2.70 m
Maintenance factor	0.70
Total lamp luminous flux	4215.00 lm
Luminaire luminous flux	4213.41 lm
Total power	38.0 W
Total power per area (15.24 m ²)	2.49 W/m ² (1.96 W/m ² /100lx)

Evaluation area 1

Reference plane 1.1

	Horizontal	cylindrical
\bar{E}_m	128 lx	35 lx
E_{min}	59 lx	18 lx
$E_{min}/\bar{E}_m (U_0)$	0.46	0.51
$E_{min}/E_{max} (U_d)$	0.37	
E_z/E_h		0.24
Position	0.10 m	0.10 m
RUG (--- ---)	---	

Hints:
 - The scene contains luminaires that are unsuitable for the RUG tabular method.

Type No.\Make

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



2 Amenities

2.2 Summary, Amenities, Light scene 1(C)

2.2.1 Result overview, Evaluation area 1

9 5 x **HANECO**
Order No. : DULAR-EV-TRI-4000K
Luminaire name :
Equipment : 1 x 7.1 W / 800 lm

8 1 x **Clevertronics,**
Order No. : CLIFE
Luminaire name : Clevertronics LP Lifelight Recessed EM Disc D40
Equipment : 1 x 2.5 W / 215 lm

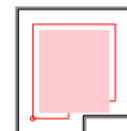
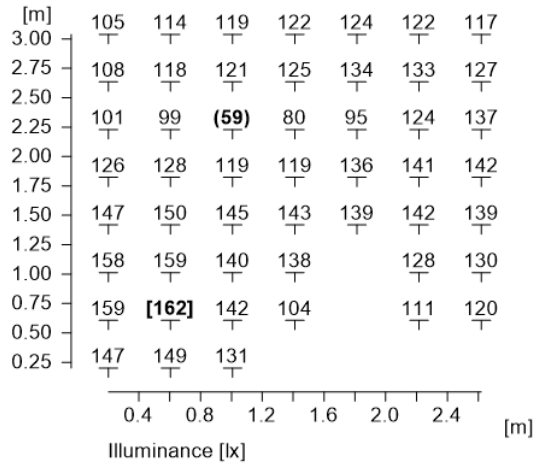
Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



2 Amenities

2.3 Calculation results, Amenities, Light scene 1(C)

2.3.1 Table, Reference plane 1.1 (E)



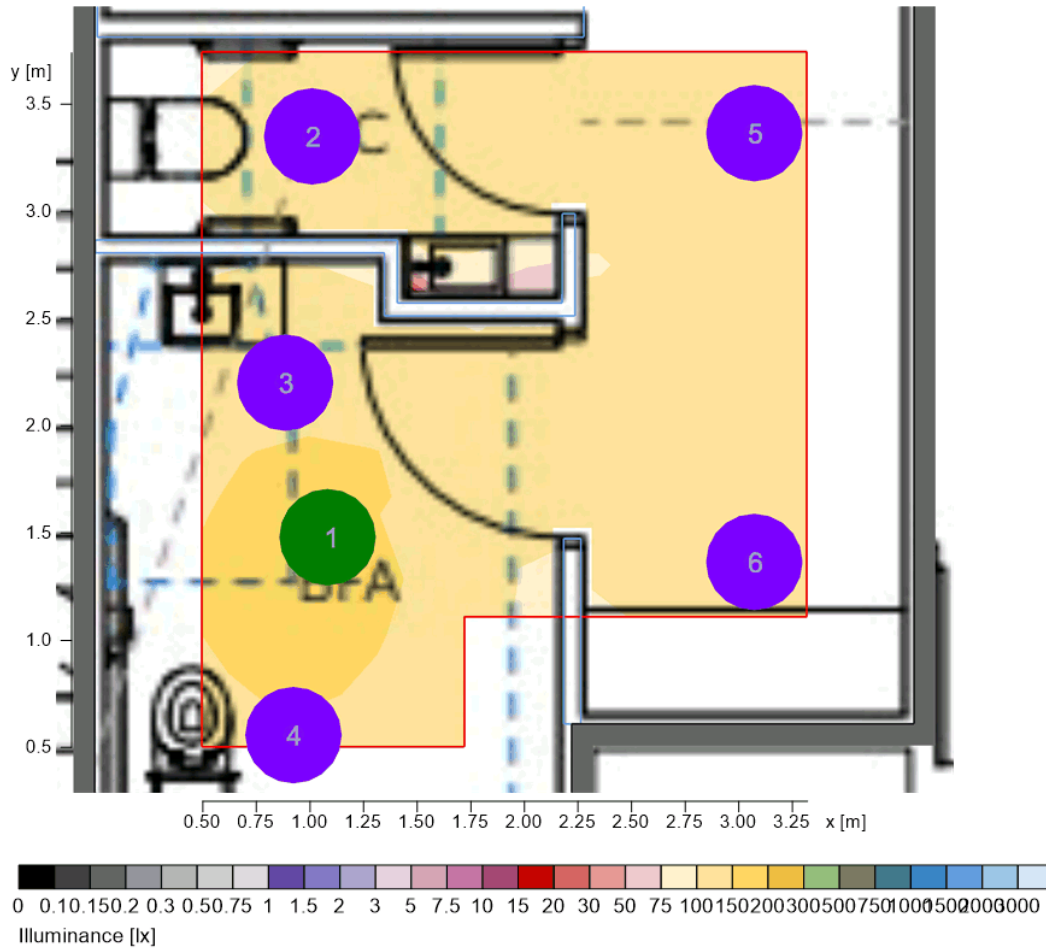
Height reference plane		: 0.10 m
Average illuminance	\bar{E}_m	: 128 lx
Minimum illuminance	E_{min}	: 59 lx
Maximum illuminance	E_{max}	: 162 lx
Uniformity U_0	E_{min}/\bar{E}_m	: 1 : 2.15 (0.46)

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



2.3 Calculation results, Amenities, Light scene 1(C)

2.3.4 Pseudo colours, Reference plane 1.1 (E)



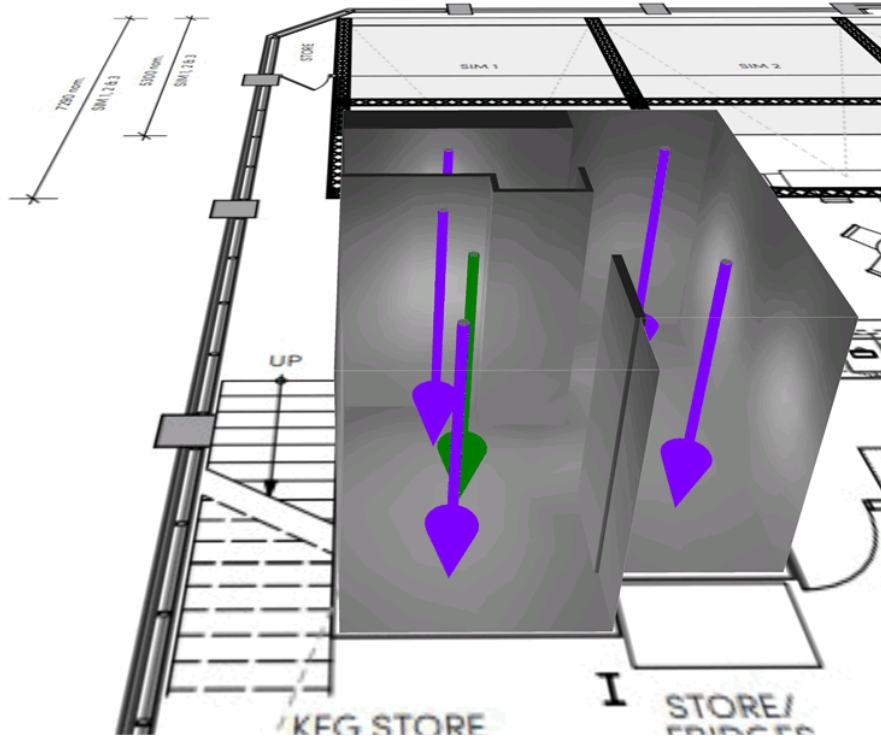
Height reference plane	:	0.10 m
Average illuminance	\bar{E}_m	: 128 lx
Minimum illuminance	E_{min}	: 59 lx
Maximum illuminance	E_{max}	: 162 lx
Uniformity U_0	E_{min}/\bar{E}_m	: 1 : 2.15 (0.46)

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



2.3 Calculation results, Amenities, Light scene 1(C)

2.3.6 3D luminance, View 1



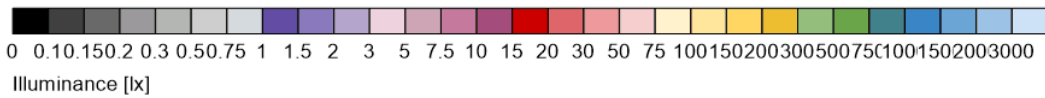
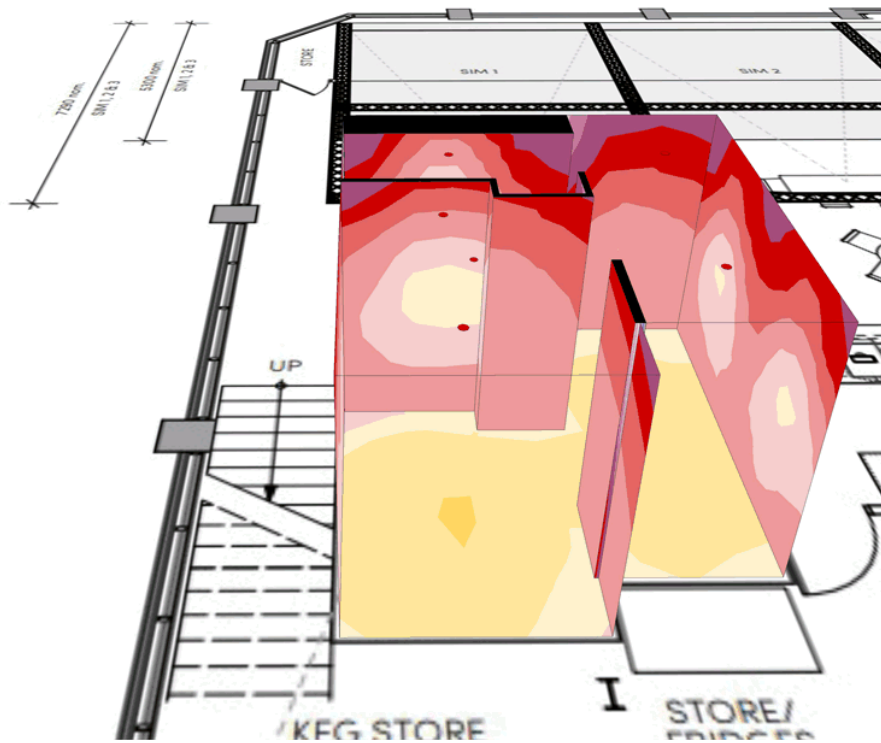
Luminance in the scene
Minimum: : 0 cd/m²
Maximum: : 30.9 cd/m²

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



2.3 Calculation results, Amenities, Light scene 1(C)

2.3.7 3D pseudo colours, View 1 (E)



Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026




3 Bar Kitchen


3.1 Description, Bar Kitchen

3.1.1 Luminaire data/Room elements

Product data:

Type No.\Make

10	8 x	PIERLITE	Order No. : ECOBAT404ECSLSG4 4000K 35W 0.35A
			Luminaire name : ECO LED BATTEN GEN4 36W 4FT COL SELECT & LUMEN SELECT NON DIM
			Equipment : 1 x 36 W / 4300 lm

14	8 x	SAL	Order No. : I
			Luminaire name : SMT9017/15TC/WH/3000K
			Equipment : 1 x 14.5 W / 948 lm

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026

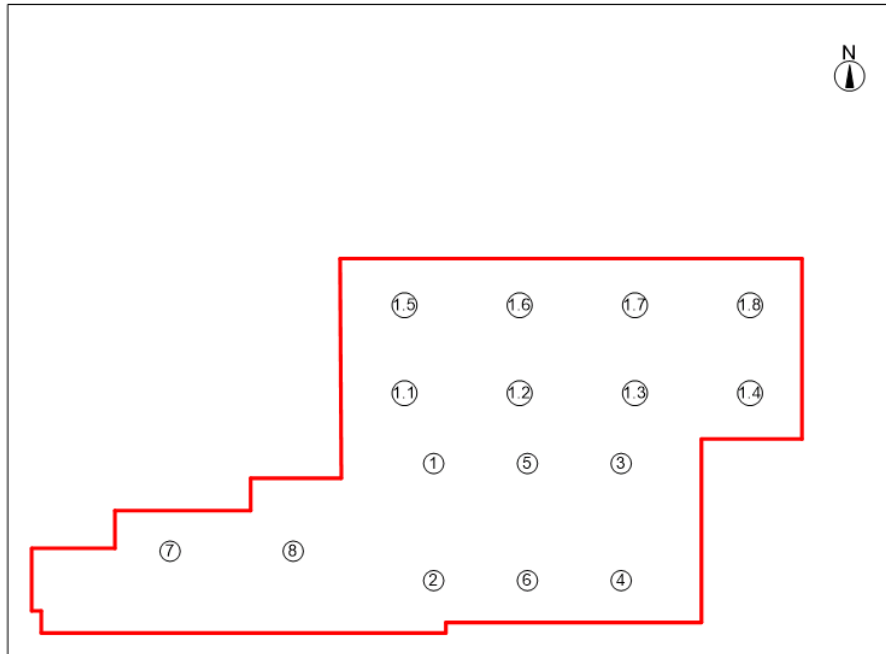


3 Bar Kitchen

3.1 Description, Bar Kitchen

3.1.1 Luminaire data/Room elements

Floor with luminaire and sensor positions:



No.	Centre point			Rotation angle			Target coordinates		
	X [m]	Y [m]	Z [m]	Z [°]	C0 [°]	C90 [°]	Xa [m]	Ya [m]	Za [m]
PIERLITE ECO LED BATTEN GEN4 36W 4FT COL SELECT & LUMEN SELECT NON DIM									
ECOBAT404ECSLSG4 4000K 35W 0.35A									
1	1.60	-3.50	2.70	0.00	0.00	0.00	1.60	-3.50	0.00
2	1.60	-5.50	2.70	0.00	0.00	0.00	1.60	-5.50	0.00
3	4.80	-3.50	2.70	0.00	0.00	0.00	4.80	-3.50	0.00
4	4.80	-5.50	2.70	0.00	0.00	0.00	4.80	-5.50	0.00
5	3.20	-3.50	2.70	0.00	0.00	0.00	3.20	-3.50	0.00
6	3.20	-5.50	2.70	0.00	0.00	0.00	3.20	-5.50	0.00
7	-2.90	-5.00	2.70	0.00	0.00	0.00	-2.90	-5.00	0.00
8	-0.80	-5.00	2.70	0.00	0.00	0.00	-0.80	-5.00	0.00
SAL SMT9017/15TC/WH/3000K !									
1.1	1.10	-2.30	2.70	0.00	0.00	0.00	1.10	-2.30	0.00
1.2	3.07	-2.30	2.70	0.00	0.00	0.00	3.07	-2.30	0.00
1.3	5.03	-2.30	2.70	0.00	0.00	0.00	5.03	-2.30	0.00
1.4	7.00	-2.30	2.70	0.00	0.00	0.00	7.00	-2.30	0.00
1.5	1.10	-0.80	2.70	0.00	0.00	0.00	1.10	-0.80	0.00
1.6	3.07	-0.80	2.70	0.00	0.00	0.00	3.07	-0.80	0.00
1.7	5.03	-0.80	2.70	0.00	0.00	0.00	5.03	-0.80	0.00
1.8	7.00	-0.80	2.70	0.00	0.00	0.00	7.00	-0.80	0.00

Structural elements

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



3 Bar Kitchen

3.1 Description, Bar Kitchen

3.1.1 Luminaire data/Room elements

Measuring surface

No.	xm[m]	ym[m]	zm[m]	Length	Width	Height	Rotation angle z axis	L axis
Ref. plane 1.1	-5.18	-5.03	0.75	13.04	6.30	0.00	270.29	0.00

Others

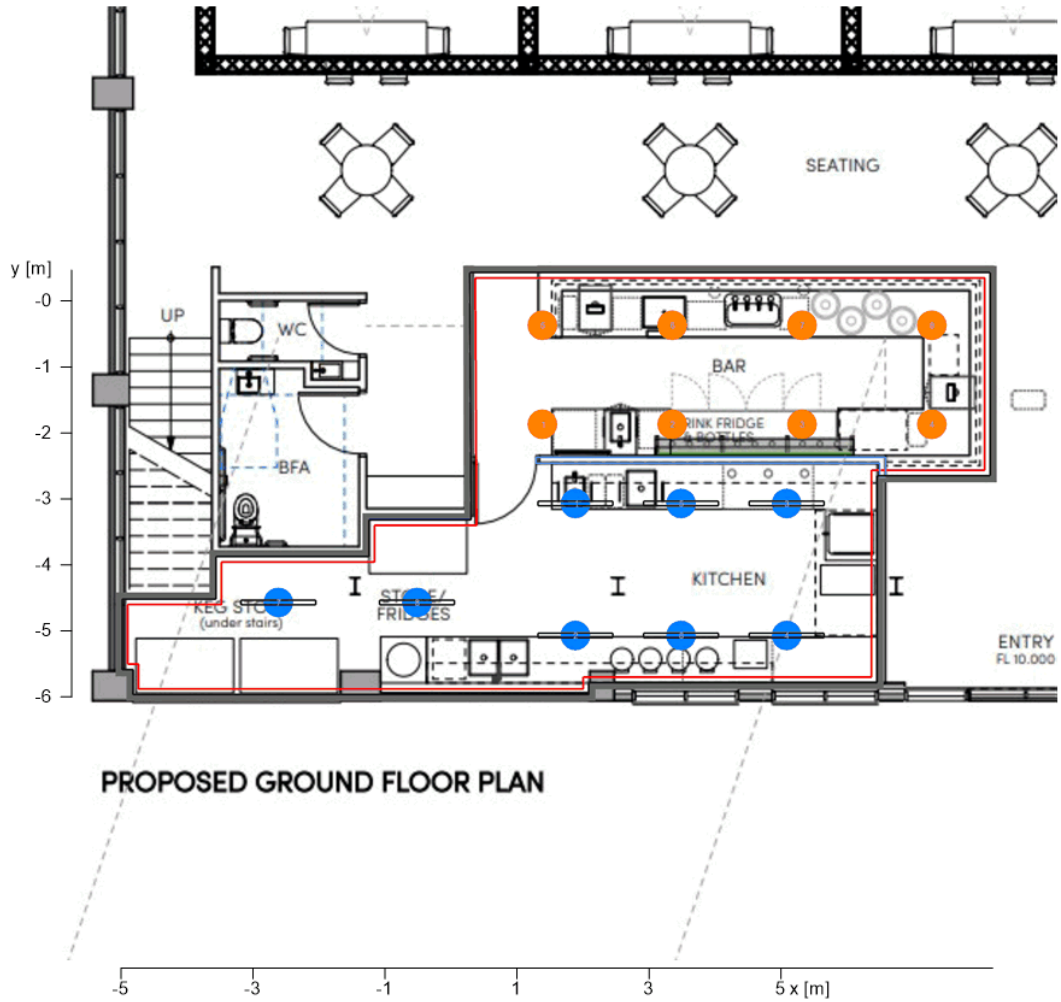
No.	xm[m]	ym[m]	zm[m]	Length	Width	Height	Rotation angle z axis	L axis	Q axis
Tr 1	1.00	-2.80	0.00	5.30	0.30	2.70	0.00	0.00	0.00

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



3.1 Description, Bar Kitchen, Light scene 1(C)

3.1.2 Floor plan



Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



3.1 Description, Bar Kitchen, Light scene 1(C)

3.1.2 Floor plan

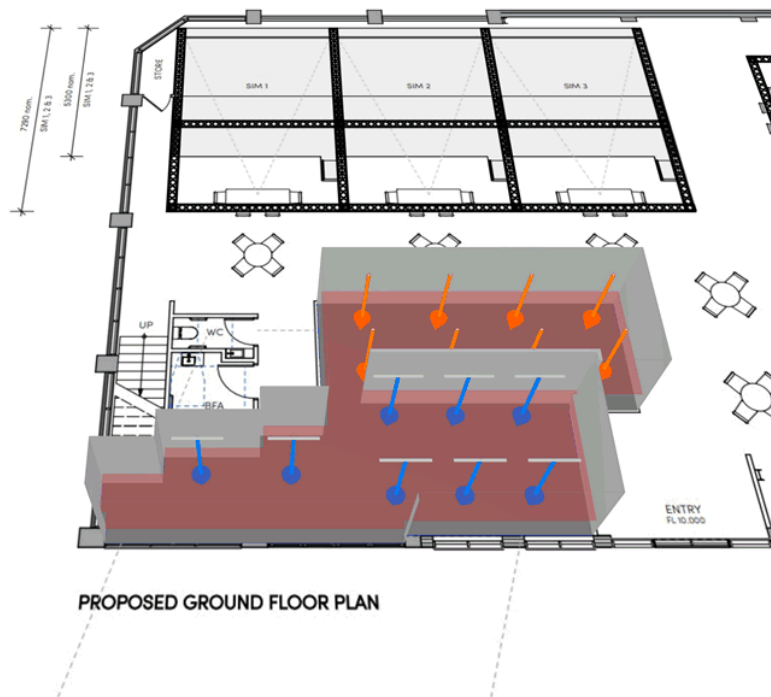
Wall	x	y	Length	Area[m ²]	Reflectance
1	3.89 m	-1.50 m	3.75 m	10.12	50.0 %
2	2.34 m	-1.50 m	1.55 m	4.17	50.0 %
3	2.34 m	-2.05 m	0.56 m	1.50	50.0 %
4	0.02 m	-2.05 m	2.32 m	6.26	50.0 %
5	0.02 m	-2.70 m	0.64 m	1.74	50.0 %
6	-1.40 m	-2.70 m	1.42 m	3.83	50.0 %
7	-1.40 m	-3.77 m	1.07 m	2.89	50.0 %
8	-1.23 m	-3.77 m	0.16 m	0.44	50.0 %
9	-1.23 m	-4.14 m	0.38 m	1.02	50.0 %
10	5.67 m	-4.14 m	6.90 m	18.64	50.0 %
11	5.67 m	-3.96 m	0.18 m	0.49	50.0 %
12	10.04 m	-3.96 m	4.37 m	11.79	50.0 %
13	10.04 m	-0.83 m	3.13 m	8.46	50.0 %
14	11.75 m	-0.83 m	1.71 m	4.63	50.0 %
15	11.75 m	2.25 m	3.08 m	8.32	50.0 %
16	3.87 m	2.25 m	7.88 m	21.29	50.0 %
Floor:				54.8	20.0 %
Ceiling:				54.8	70.0 %
Room height [m]:		2.70 m			
Height of ref. plane [m]:		0.75 m			
Volume					147.9 m ³

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



3.1 Description, Bar Kitchen

3.1.3 3D view, View 1



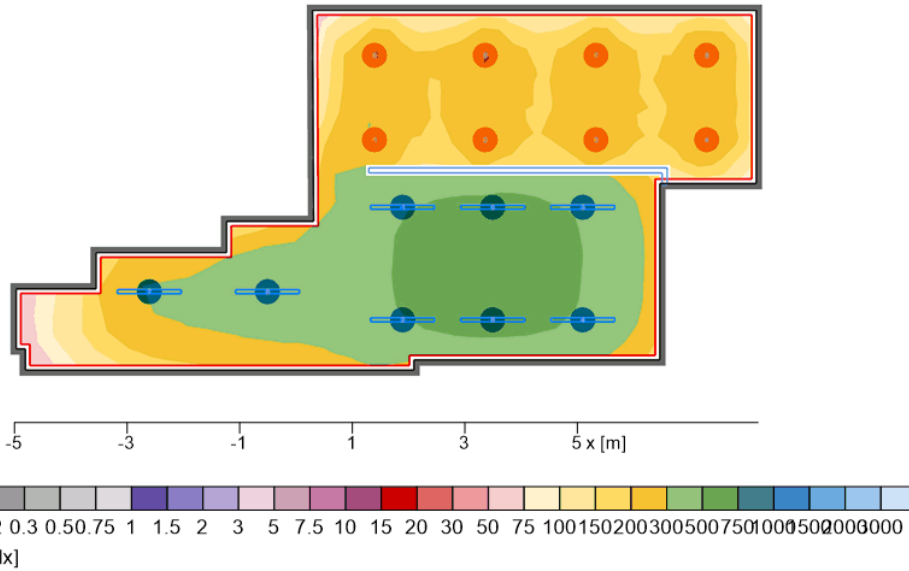
Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



3 Bar Kitchen

3.2 Summary, Bar Kitchen, Light scene 1(C)

3.2.1 Result overview, Evaluation area 1



General

Calculation algorithm used	Average indirect fraction
Height of luminaire plane	2.70 m
Maintenance factor	0.70
Total lamp luminous flux	41984.00 lm
Luminaire luminous flux	41976.57 lm
Total power	404.0 W
Total power per area (54.78 m ²)	7.37 W/m ² (2.32 W/m ² /100lx)

Evaluation area 1

Reference plane 1.1

	Horizontal	cylindrical
\bar{E}_m	318 lx	140 lx
E_{min}	73 lx	45 lx
$E_{min}/\bar{E}_m (U_0)$	0.23	0.32
$E_{min}/E_{max} (U_d)$	0.12	
E_z/E_h		0.38
Position	0.75 m	0.75 m
RUG (--- ---)	---	

Hints:
 - Room dimensions deviate too much from a rectangular room.

Type No.\Make

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



3 Bar Kitchen

3.2 Summary, Bar Kitchen, Light scene 1(C)

3.2.1 Result overview, Evaluation area 1

10	8 x	PIERLITE	Order No. : ECOBAT404ECSLSG4 4000K 35W 0.35A
		Luminaire name : ECO LED BATTEN GEN4 36W 4FT COL SELECT & LUMEN SELECT NON DIM	Equipment : 1 x 36 W / 4300 lm

14	8 x	SAL	Order No. : I
		Luminaire name : SMT9017/15TC/WH/3000K	Equipment : 1 x 14.5 W / 948 lm

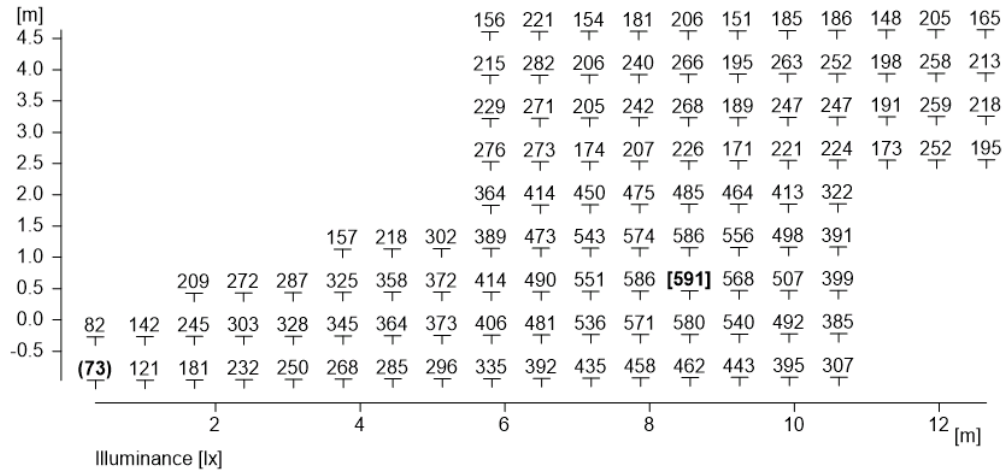
Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



3 Bar Kitchen

3.3 Calculation results, Bar Kitchen, Light scene 1(C)

3.3.1 Table, Reference plane 1.1 (E)



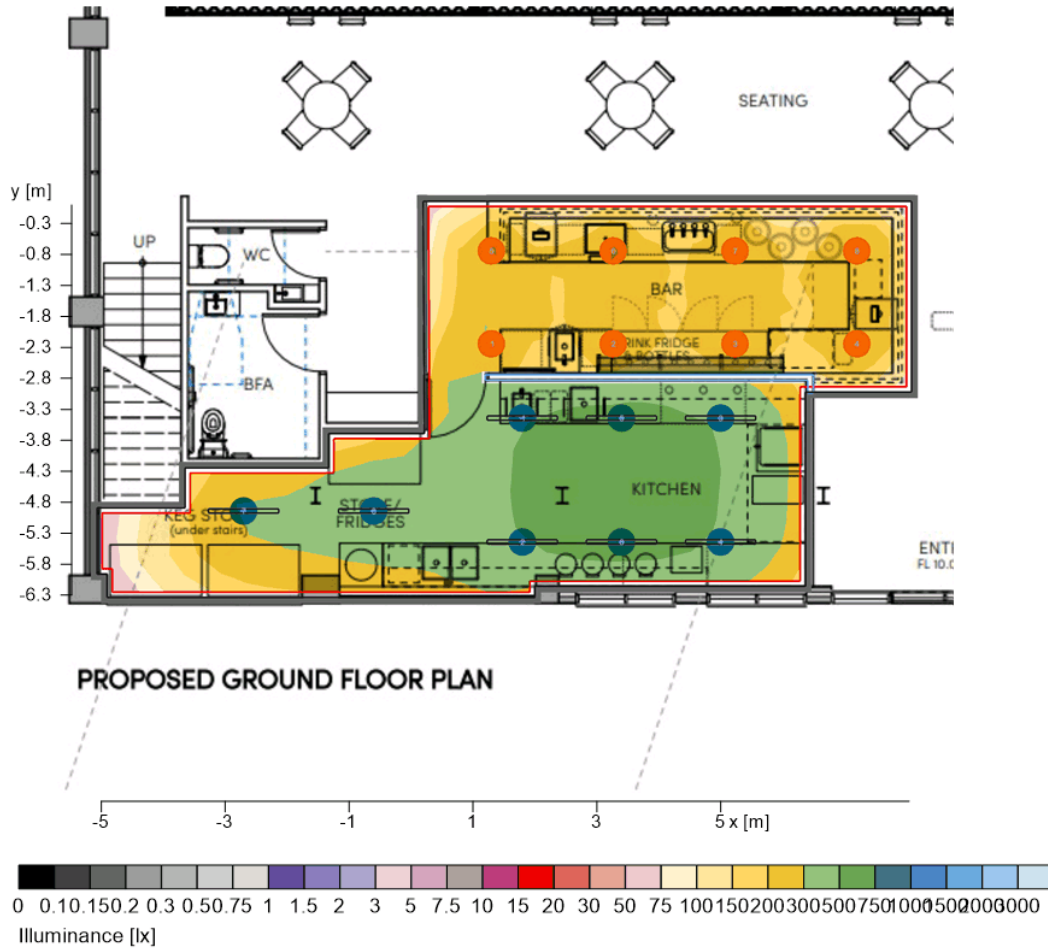
Height reference plane		: 0.75 m
Average illuminance	\bar{E}_m	: 318 lx
Minimum illuminance	E_{min}	: 73 lx
Maximum illuminance	E_{max}	: 591 lx
Uniformity U_o	E_{min}/\bar{E}_m	: 1 : 4.35 (0.23)

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



3.3 Calculation results, Bar Kitchen, Light scene 1(C)

3.3.4 Pseudo colours, Reference plane 1.1 (E)



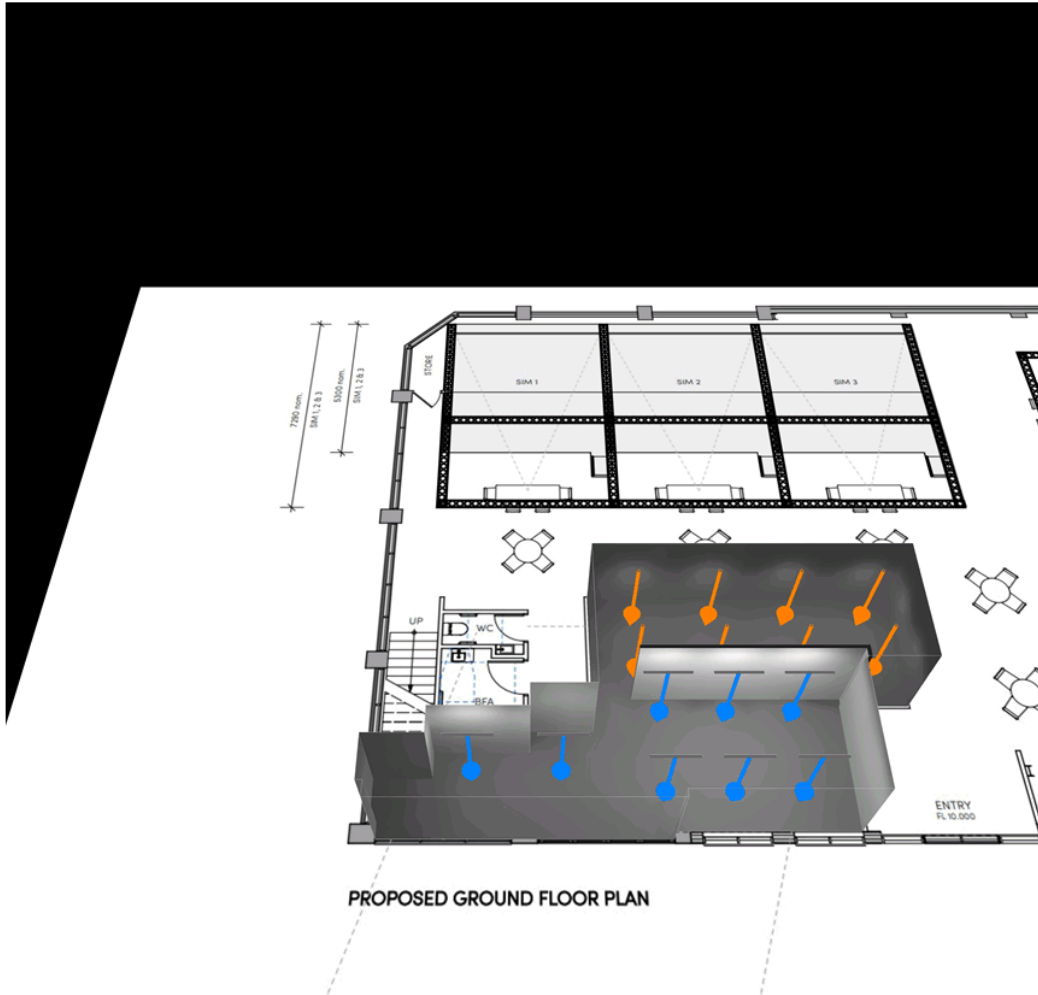
Height reference plane	:	0.75 m
Average illuminance	\bar{E}_m	: 318 lx
Minimum illuminance	E_{min}	: 73 lx
Maximum illuminance	E_{max}	: 591 lx
Uniformity U_0	E_{min}/\bar{E}_m	: 1 : 4.35 (0.23)

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



3.3 Calculation results, Bar Kitchen, Light scene 1(C)

3.3.6 3D luminance, View 1



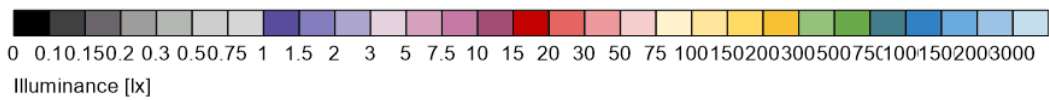
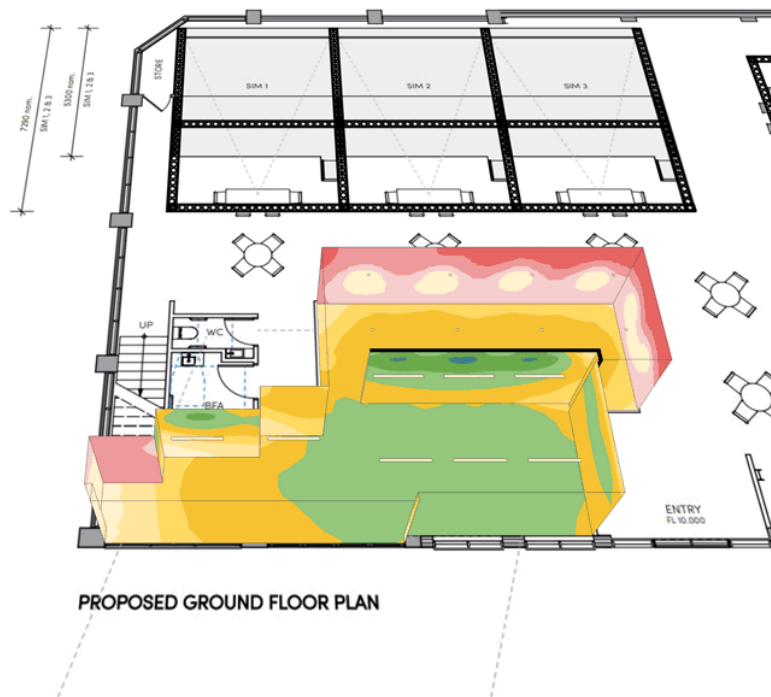
Luminance in the scene
Minimum: : 0 cd/m²
Maximum: : 129 cd/m²

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



3.3 Calculation results, Bar Kitchen, Light scene 1(C)

3.3.7 3D pseudo colours, View 1 (E)



Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026




4 Office


4.1 Description, Office

4.1.1 Luminaire data/Room elements

Product data:

Type No.\Make

PIERLITE
10 6 x Order No. : ECOBAT404ECSLSG4 4000K 35W 0.35A
 Luminaire name : ECO LED BATTEN GEN4 36W 4FT COL SELECT & LUMEN SELECT NON DIM
Equipment : 1 x 36 W / 4300 lm

Clevertronics Pty. Ltd., Moorabbin, VIC 3189.
7 1 x Order No. : CCFLED
 Luminaire name : Clevertronics LP Cleverfit Emergency LED Exit GEN2 Luminaire
Equipment : 6 x / 5.5 lm

Clevertronics,
8 1 x Order No. : CLIFE
 Luminaire name : Clevertronics LP Lifelight Recessed EM Disc D40
Equipment : 1 x 2.5 W / 215 lm

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



4 Office

4.1 Description, Office

4.1.1 Luminaire data/Room elements

Floor with luminaire and sensor positions:



No.	Centre point			Rotation angle			Target coordinates		
	X [m]	Y [m]	Z [m]	Z [°]	C0 [°]	C90 [°]	Xa [m]	Ya [m]	Za [m]
PIERLITE ECO LED BATTEN GEN4 36W 4FT COL SELECT & LUMEN SELECT NON DIM									
ECOBAT404ECSLSG4 4000K 35W 0.35A									
1.1	1.50	1.06	2.40	270.00	0.00	0.00	1.50	1.06	0.00
1.2	4.49	1.06	2.40	270.00	0.00	0.00	4.49	1.06	0.00
1.3	7.49	1.06	2.40	270.00	0.00	0.00	7.49	1.06	0.00
1.4	1.50	3.17	2.40	270.00	0.00	0.00	1.50	3.18	0.00
1.5	4.49	3.17	2.40	270.00	0.00	0.00	4.49	3.18	0.00
1.6	7.49	3.17	2.40	270.00	0.00	0.00	7.49	3.18	0.00
Clevertronics Pty. Ltd., Moorabbin, VIC 3189. Clevertronics LP									
Cleverfit Emergency LED Exit GEN2 Luminaire CCFLED									
1	0.10	1.00	1.82	0.00	0.00	0.00	0.10	1.00	0.00
Clevertronics, Clevertronics LP Lifelight Recessed EM Disc D40									
CLIFE									
2	4.50	2.00	2.40	0.00	0.00	0.00	4.50	2.00	0.00

Structural elements

Measuring surface

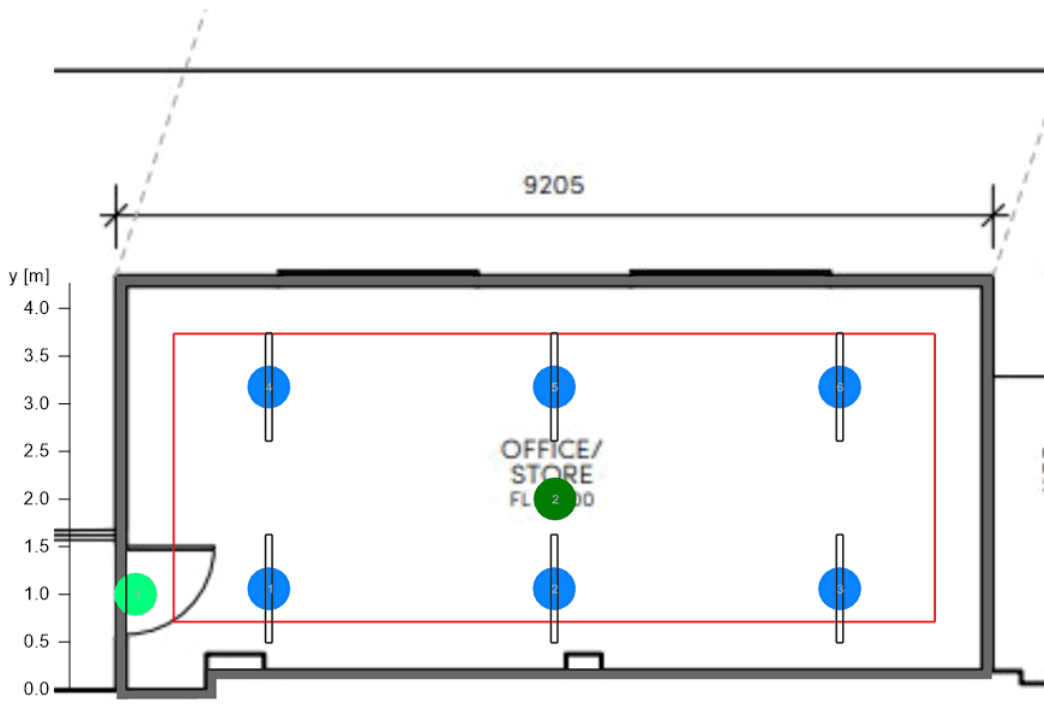
No.	xm[m]	ym[m]	zm[m]	Length	Width	Height	Rotation angle	
							z axis	L axis
Ref. plane 1.1								
	0.50	0.71	0.75	8.01	3.08	0.00	0.20	0.00

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



4.1 Description, Office, Light scene 1(C)

4.1.2 Floor plan



3D GROUND FLOOR PLAN



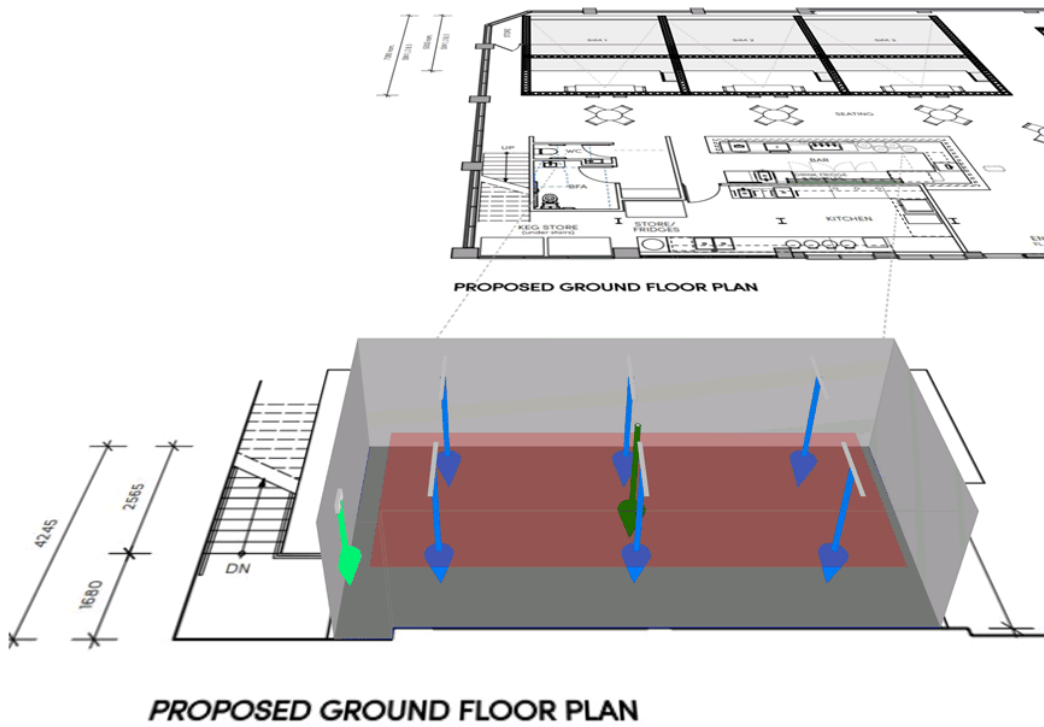
Wall	x	y	Length	Area[m ²]	Reflectance
1	-2.08 m	-14.96 m	0.85 m	2.04	50.0 %
2	-2.08 m	-14.75 m	0.21 m	0.50	50.0 %
3	6.05 m	-14.75 m	8.14 m	19.53	50.0 %
4	6.05 m	-10.73 m	4.02 m	9.66	50.0 %
5	-2.93 m	-10.73 m	8.98 m	21.56	50.0 %
6	-2.93 m	-14.96 m	4.23 m	10.16	50.0 %
Floor:				36.3	20.0 %
Ceiling:				36.3	70.0 %
Room height [m]:		2.40 m			
Height of ref. plane [m]:		0.75 m			
Volume					87.2 m ³

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



4.1 Description, Office

4.1.3 3D view, View 1



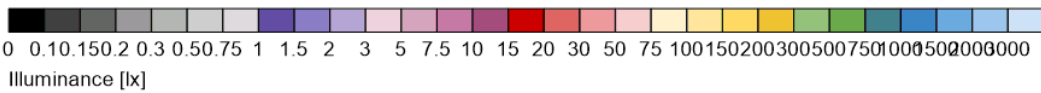
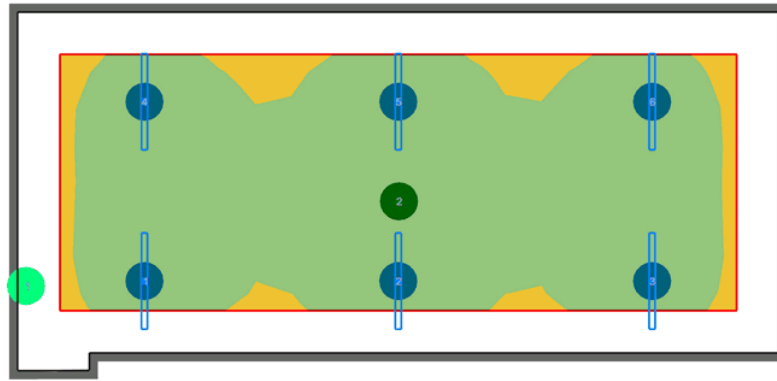
Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



4 Office

4.2 Summary, Office, Light scene 1(C)

4.2.1 Result overview, Evaluation area 1



General

Calculation algorithm used	Average indirect fraction
Maintenance factor	0.70
Total lamp luminous flux	26048.00 lm
Luminaire luminous flux	26045.65 lm
Total power	219.5 W
Total power per area (36.32 m²)	6.04 W/m² (1.71 W/m²/100lx)

Evaluation area 1

Reference plane 1.1

\bar{E}_m	Horizontal	cylindrical
E_{min}	354 lx	186 lx
$E_{min}/\bar{E}_m (U_o)$	276 lx	156 lx
$E_{min}/E_{max} (U_d)$	0.78	0.84
E_z/E_h	0.62	0.46
Position	0.75 m	0.75 m
$R_{UG} (3.5H \ 7.5H)$	≤ 27.3	

Luminaire :
 (ECO LED BATTEN GEN4 36W 4FT COL SELECT & LUMEN SELECT NON DIM , ECOBAT404ECSLSG4 4000K 35W 0.35A , 36.0 W)

Type No.\Make

10	6 x	PIERLITE	
		Order No.	: ECOBAT404ECSLSG4 4000K 35W 0.35A
		Luminaire name	: ECO LED BATTEN GEN4 36W 4FT COL SELECT & LUMEN SELECT NON DIM
		Equipment	: 1 x 36 W / 4300 lm


Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



4 Office

4.2 Summary, Office, Light scene 1(C)

4.2.1 Result overview, Evaluation area 1

Clevertronics Pty. Ltd., Moorabbin, VIC 3189.
7 1 x Order No. : CCFLED
 Luminaire name : Clevertronics LP Cleverfit Emergency LED Exit GEN2 Luminaire
Equipment : 6 x / 5.5 lm

Clevertronics,
8 1 x Order No. : CLIFE
 Luminaire name : Clevertronics LP Lifelight Recessed EM Disc D40
Equipment : 1 x 2.5 W / 215 lm

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026

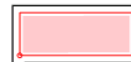


4 Office

4.3 Calculation results, Office, Light scene 1(C)

4.3.1 Table, Reference plane 1.1 (E)

[m]	(276)	341	368	349	305	277	282	319	369	391	368	319	282	278	306	349	368	340	(276)
2.75	303	374	404	382	335	304	310	352	406	430	406	352	310	305	337	384	406	375	304
2.25	306	374	402	384	342	314	321	360	411	433	411	360	321	316	344	387	404	375	308
1.75	304	365	391	377	341	317	323	360	405	425	405	360	324	319	344	380	393	366	305
1.25	307	371	397	382	343	317	325	363	411	431	410	363	325	320	346	384	399	371	307
0.75	310	381	411	390	343	313	320	363	418	441	416	362	321	316	347	393	413	381	311
0.25	294	366	396	372	324	293	300	343	398	422	397	342	301	296	329	377	398	367	296
		1		2		3		4		5		6		7					
		Illuminance [lx]																	



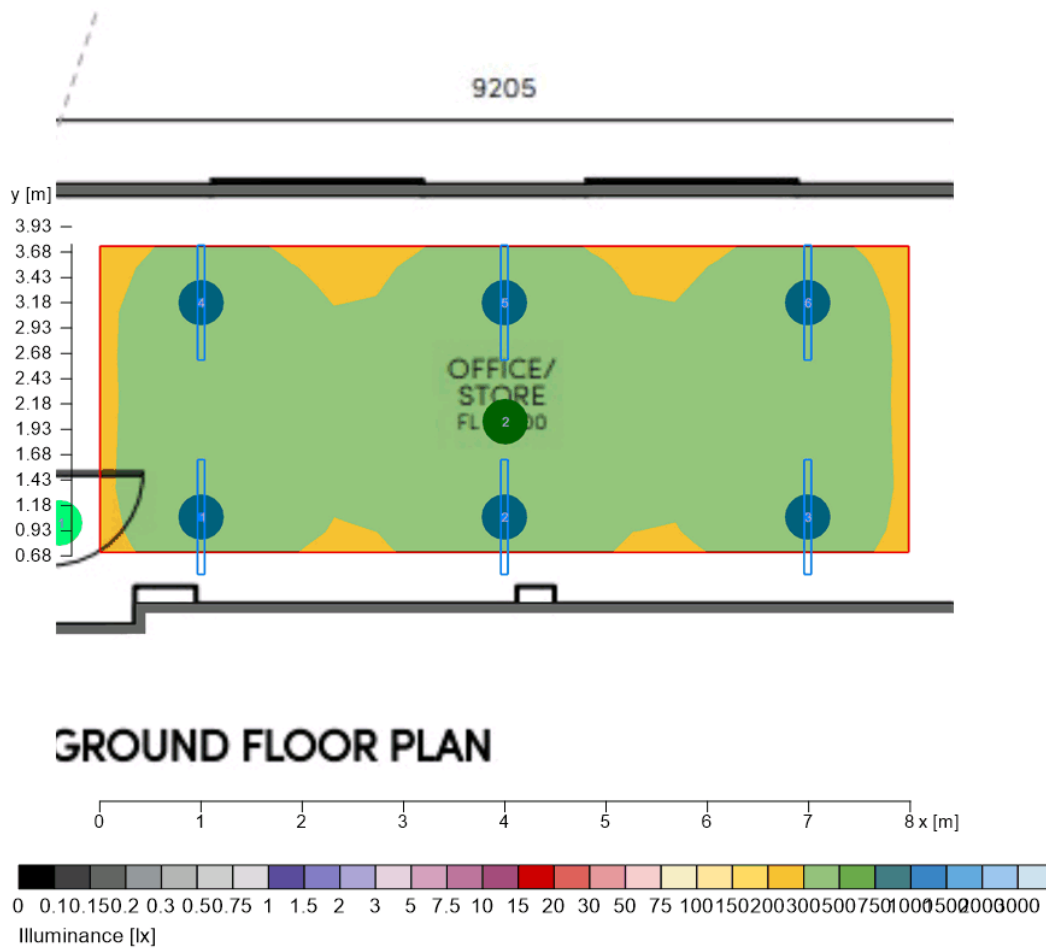
Height reference plane		: 0.75 m
Average illuminance	\bar{E}_m	: 354 lx
Minimum illuminance	E_{min}	: 276 lx
Maximum illuminance	E_{max}	: 441 lx
Uniformity U_o	E_{min}/\bar{E}_m	: 1 : 1.28 (0.78)

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



4.3 Calculation results, Office, Light scene 1(C)

4.3.4 Pseudo colours, Reference plane 1.1 (E)



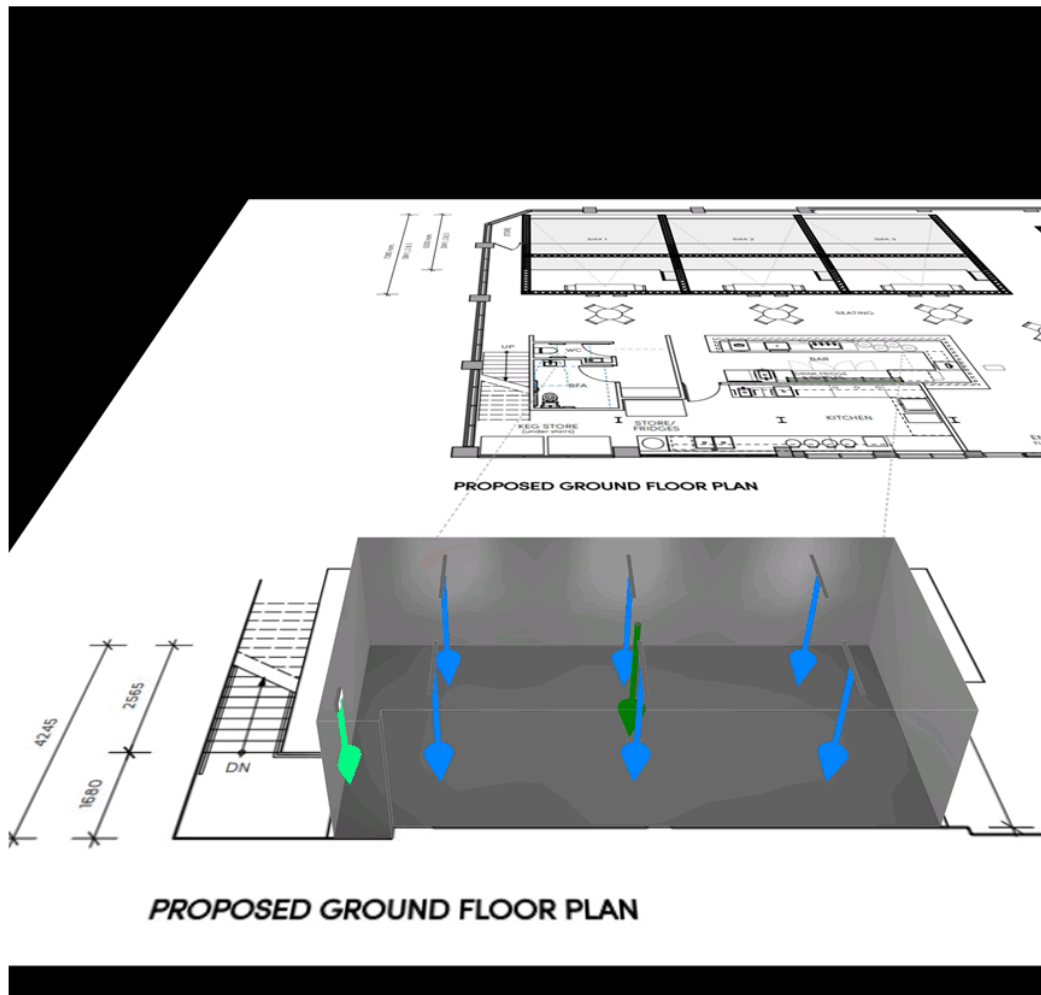
Height reference plane	:	0.75 m
Average illuminance	\bar{E}_m	: 354 lx
Minimum illuminance	E_{min}	: 276 lx
Maximum illuminance	E_{max}	: 441 lx
Uniformity U_0	E_{min}/\bar{E}_m	: 1 : 1.28 (0.78)

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



4.3 Calculation results, Office, Light scene 1(C)

4.3.6 3D luminance, View 1



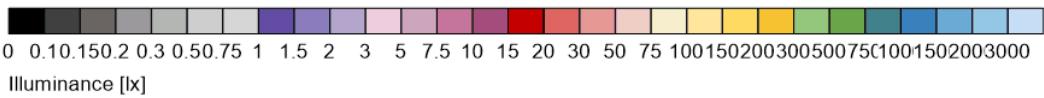
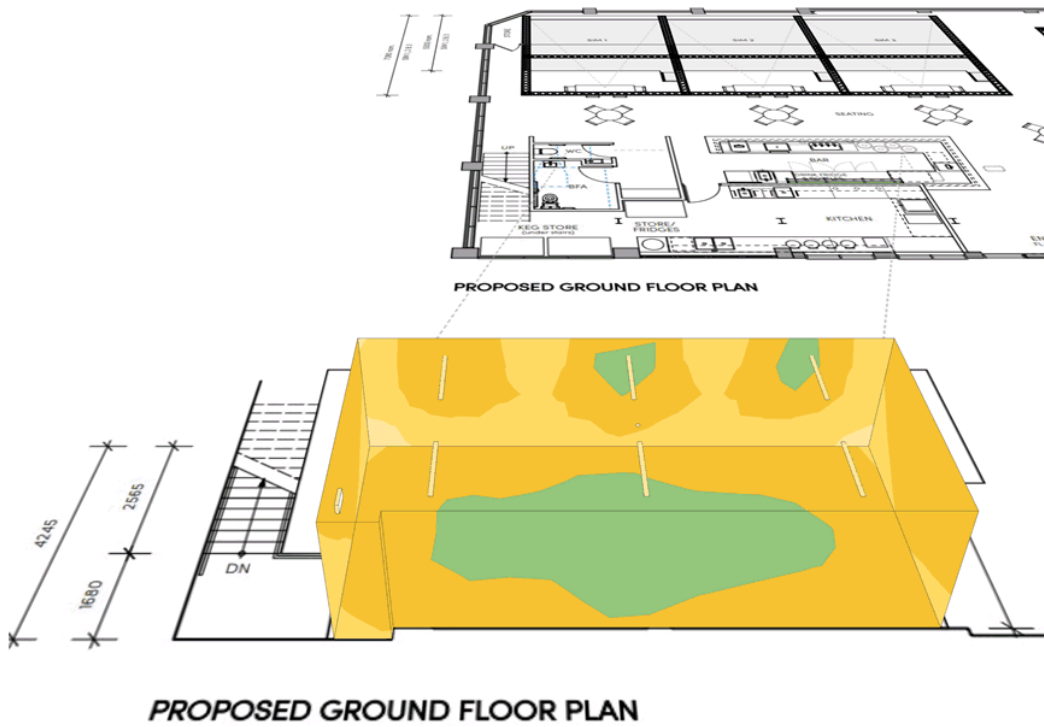
Luminance in the scene
Minimum: : 10.8 cd/m²
Maximum: : 90.9 cd/m²

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



4.3 Calculation results, Office, Light scene 1(C)

4.3.7 3D pseudo colours, View 1 (E)



Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026




2 Exterior

2.1 Description, Exterior

2.1.1 Luminaire data/Room elements

Product data:

Type No.\Make

1	3 x		enLighten Australia
			Order No. : ZAL40S-T4-4K
			Luminaire name : 40W TYPE 4 ZORRO 2 STREET/AREA LIGHT
			Equipment : 1 x 40 W / 6431 lm

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026

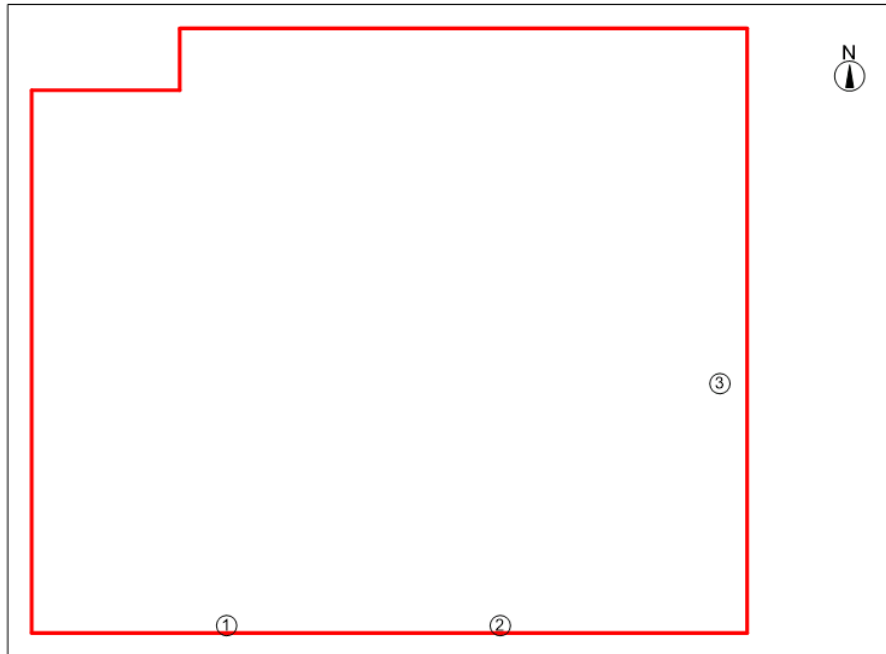


2 Exterior

2.1 Description, Exterior

2.1.1 Luminaire data/Room elements

Floor with luminaire and sensor positions:



No.	Centre point			Rotation angle			Target coordinates		
	X [m]	Y [m]	Z [m]	Z [°]	C0 [°]	C90 [°]	Xa [m]	Ya [m]	Za [m]
enLighten Australia 40W TYPE 4 ZORRO 2 STREET/AREA LIGHT ZAL40S-T4-4K									
1	69.90	51.80	5.00	90.00	0.00	-10.00	69.90	70.46	0.00
2	81.00	51.80	5.00	90.00	0.00	-10.00	81.00	70.46	0.00
3	89.90	61.60	5.00	165.22	2.58	-9.67	72.20	66.04	0.00

Structural elements

Measuring surface

No.	xm[m]	ym[m]	zm[m]	Length	Width	Height	Rotation angle	
							z axis	L axis
M 1	89.50	53.60	0.00	31.86	26.96	0.00	6.01	0.00

Others

No.	xm[m]	ym[m]	zm[m]	Length	Width	Height	Rotation angle		
							z axis	L axis	Q axis
A 1	48.50	49.50	0.00	12.55	14.81	4.00	0.98	0.00	0.00
A 2	62.70	51.40	0.00	27.60	17.30	8.00	0.00	0.00	0.00
A 3	42.40	46.00	0.00	12.20	14.40	4.00	0.00	0.00	0.00
A 4	22.40	44.80	0.00	8.90	12.40	4.00	0.00	0.00	0.00
A 5	23.70	44.70	0.00	7.00	3.40	4.00	90.00	0.00	0.00

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



2 Exterior

2.1 Description, Exterior

2.1.1 Luminaire data/Room elements

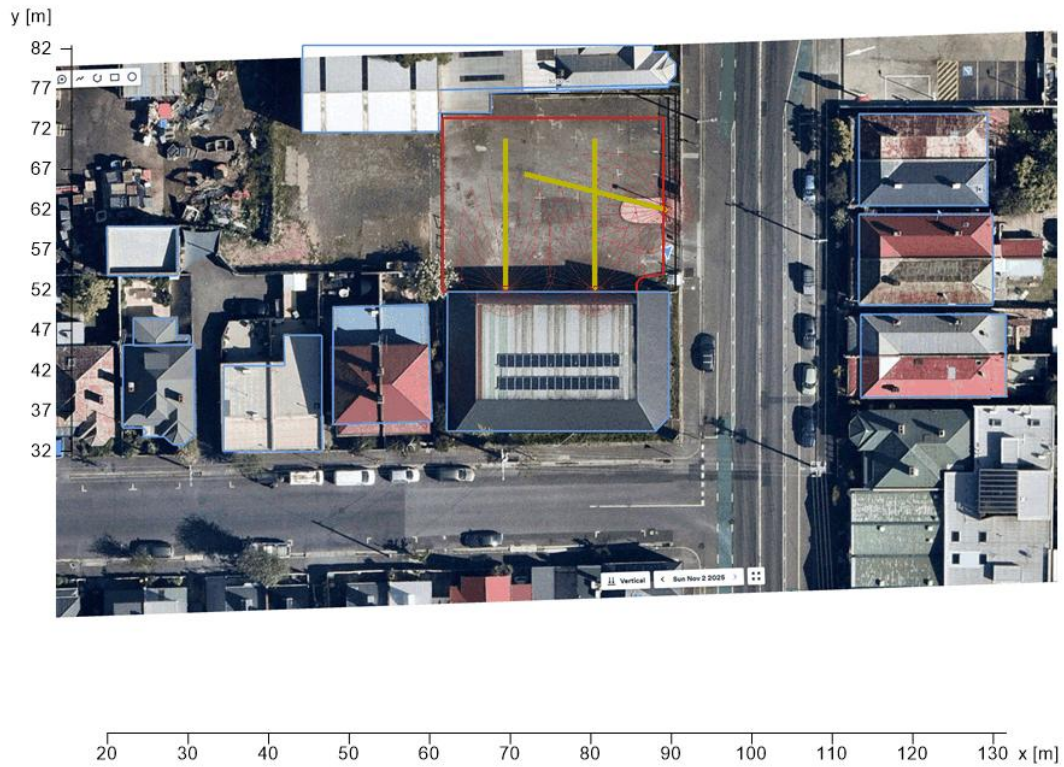
A 6	20.40	59.50	0.00	8.80	6.10	4.00	0.00	0.00	0.00
A 7	44.70	80.30	0.00	46.30	10.80	6.00	270.00	0.00	0.00
A 8	113.80	73.50	0.00	15.90	11.40	4.00	0.00	0.00	0.00
A 9	113.80	49.70	0.00	16.60	11.30	4.00	90.00	0.00	0.00
A 10	114.00	38.30	0.00	18.00	10.30	4.00	90.00	0.00	0.00
A 1	90.20	61.60	0.00	0.10	0.10	5.00	0.00	0.00	0.00

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



2.1 Description, Exterior , Light scene 1(C)

2.1.2 Floor plan

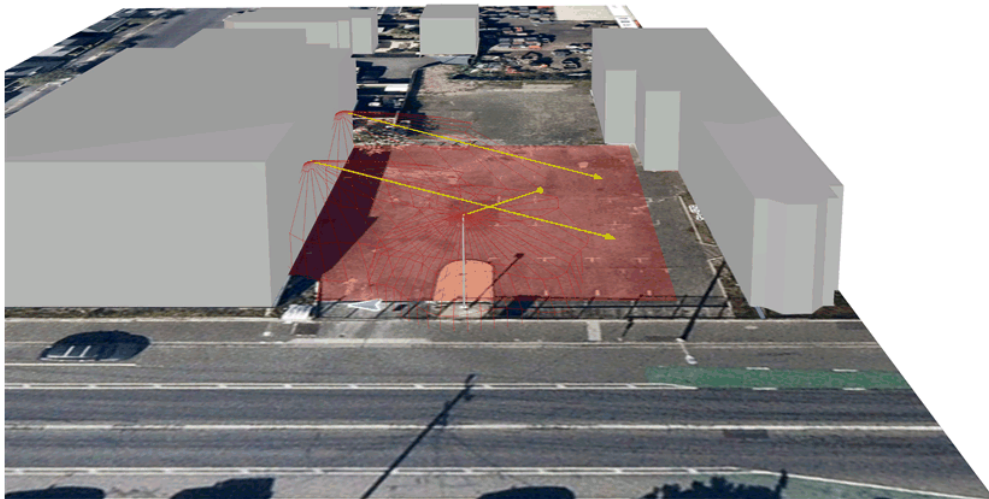


Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



2.1 Description, Exterior

2.1.3 3D view, View 1



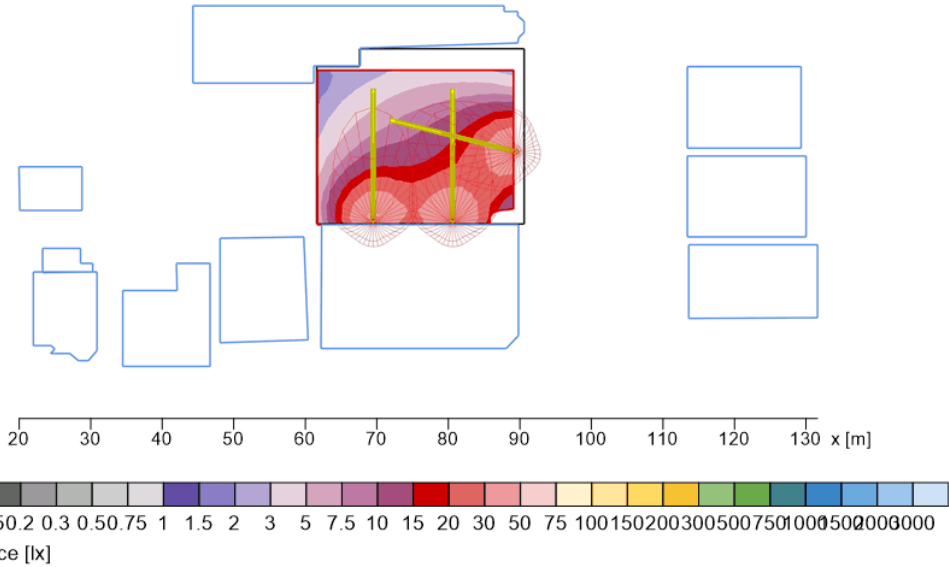
Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



2 Exterior

2.2 Summary, Exterior , Light scene 1(C)

2.2.1 Result overview, Measuring area 1




General

Calculation algorithm used	Average indirect fraction
Height of evaluation surface	0.00 m
Height (phot. centre) [m]:	5.00 m
Maintenance factor	0.70
Total luminous flux	19293 lm
Total power	120 W
Total power per area (695.50 m²)	0.17 W/m²

Illuminance

Average illuminance	\bar{E}_m	15.8 lx
Minimum illuminance	E_{min}	1.8 lx
Maximum illuminance	E_{max}	52.7 lx
Uniformity U_0	E_{min}/\bar{E}_m	1:8.81 (0.11)
Diversity U_d	E_{min}/E_{max}	1:29.4 (0.03)

Type No.\Make

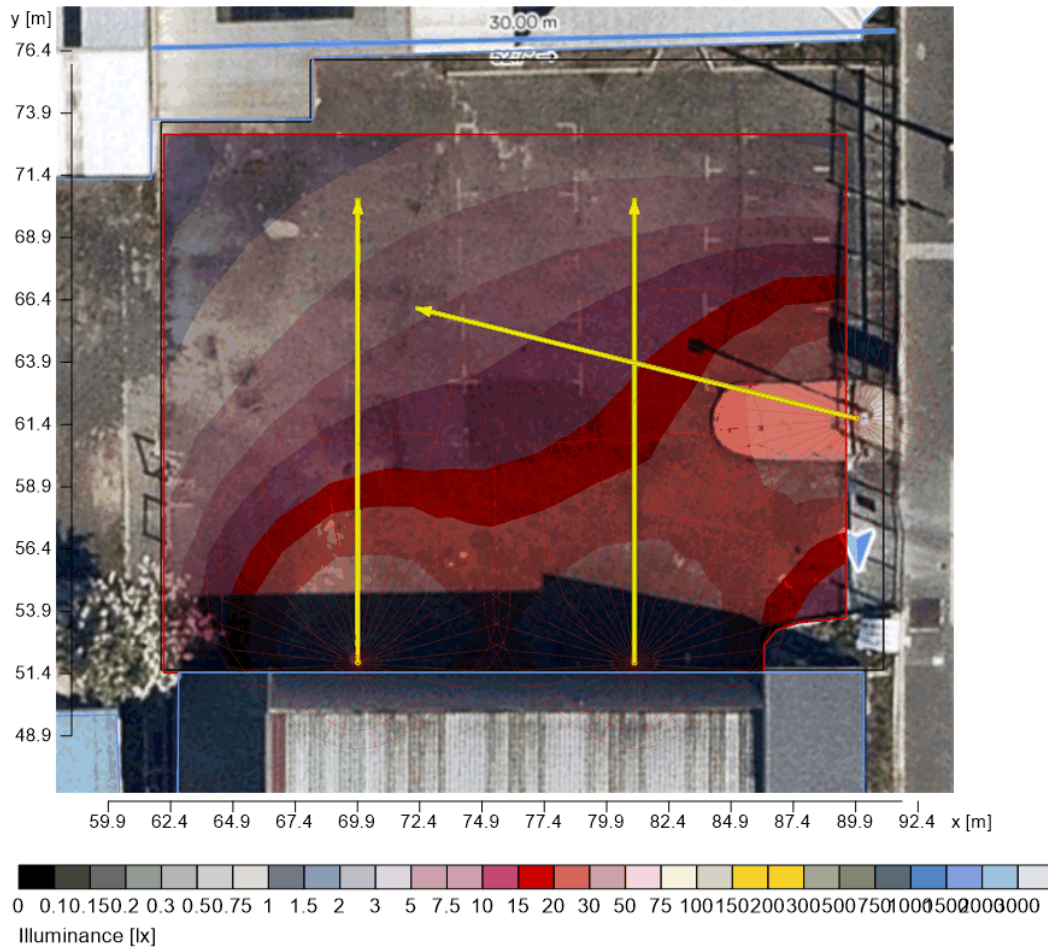
enLighten Australia	
1 3 x	Order No. : ZAL40S-T4-4K
	Luminaire name : 40W TYPE 4 ZORRO 2 STREET/AREA LIGHT
	Equipment : 1 x 40 W / 6431 lm

Object :
 Installation :
 Project number : 312 Argyle Street
 Date : 14.04.2026



2.3 Calculation results, Exterior , Light scene 1(C)

2.3.2 Pseudo colours, Measuring area 1 (E)



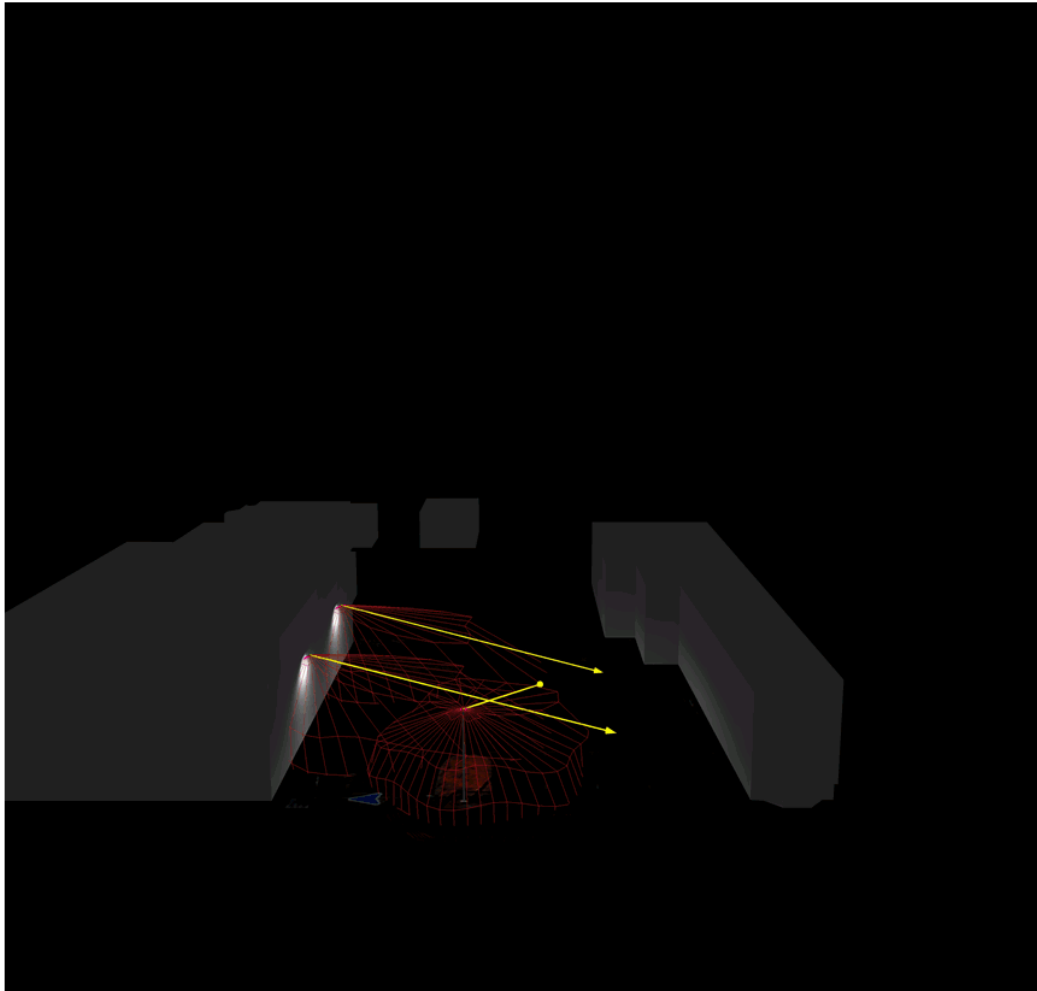
Height reference plane		: 0.00 m
Average illuminance	\bar{E}_m	: 15.8 lx
Minimum illuminance	E_{min}	: 1.79 lx
Maximum illuminance	E_{max}	: 53 lx
Uniformity U_0	E_{min}/\bar{E}_m	: 1 : 8.81 (0.11)

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



2.3 Calculation results, Exterior , Light scene 1(C)

2.3.3 3D luminance, View 1



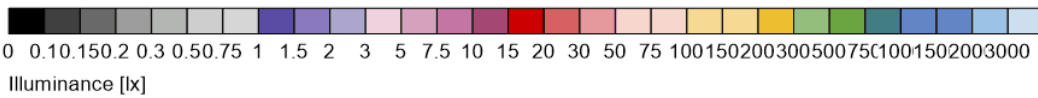
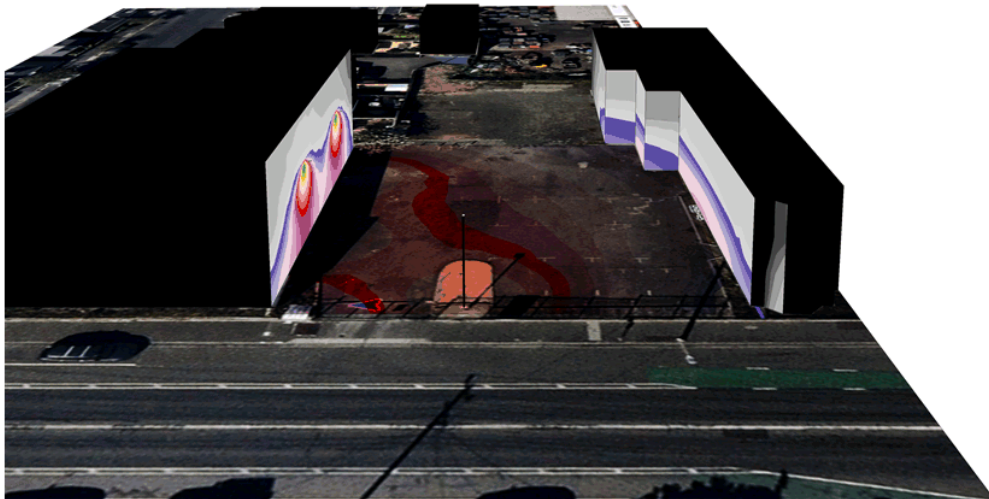
Luminance in the scene
Minimum: : 0 cd/m²
Maximum: : 187 cd/m²

Object :
Installation :
Project number : 312 Argyle Street
Date : 14.04.2026



2.3 Calculation results, Exterior , Light scene 1(C)

2.3.4 3D pseudo colours, View 1 (E)



**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



SEARCH OF TORRENS TITLE

VOLUME	FOLIO
62966	26
EDITION	DATE OF ISSUE
1	18-Oct-1994

SEARCH DATE : 18-Dec-2025

SEARCH TIME : 01.08 pm

DESCRIPTION OF LAND

City of HOBART

Lot 26 on Plan [62966](#) (formerly being P215)

Derivation : Part of 4A-0R-15Ps. Gtd. to J. Walton

Prior CT [2880/46](#)SCHEDULE 1[A997959](#) TRANSFER to UPTONS SALVAGE PTY. LTD.SCHEDULE 2

Reservations and conditions in the Crown Grant if any

[A997960](#) MORTGAGE to Commonwealth Bank of Australia

Registered 25-Sept-1985 at 12.02 pm

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



SEARCH OF TORRENS TITLE

VOLUME	FOLIO
198694	1
EDITION	DATE OF ISSUE
1	07-Aug-1995

SEARCH DATE : 18-Dec-2025

SEARCH TIME : 01.09 pm

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Plan [198694](#)

Derivation : Whole of 0A-0R-18.1/2Ps and Part of 4A-0R-15Ps

Gtd to J W Arnold & J Walton respectively

Prior CT [2318/23](#)SCHEDULE 1[A997959](#) TRANSFER to UPTONS SALVAGE PTY. LTD.SCHEDULE 2Reservations and conditions in the Crown Grant if any
9694 CAVEAT by The Lord Mayor Aldermen and Citizens of the
City of Hobart. Lodged 16-Jun-1961 at 11.35 am[A997960](#) MORTGAGE to Commonwealth Bank of Australia
Registered 25-Sept-1985 at 12.02 pmUNREGISTERED DEALINGS AND NOTATIONS

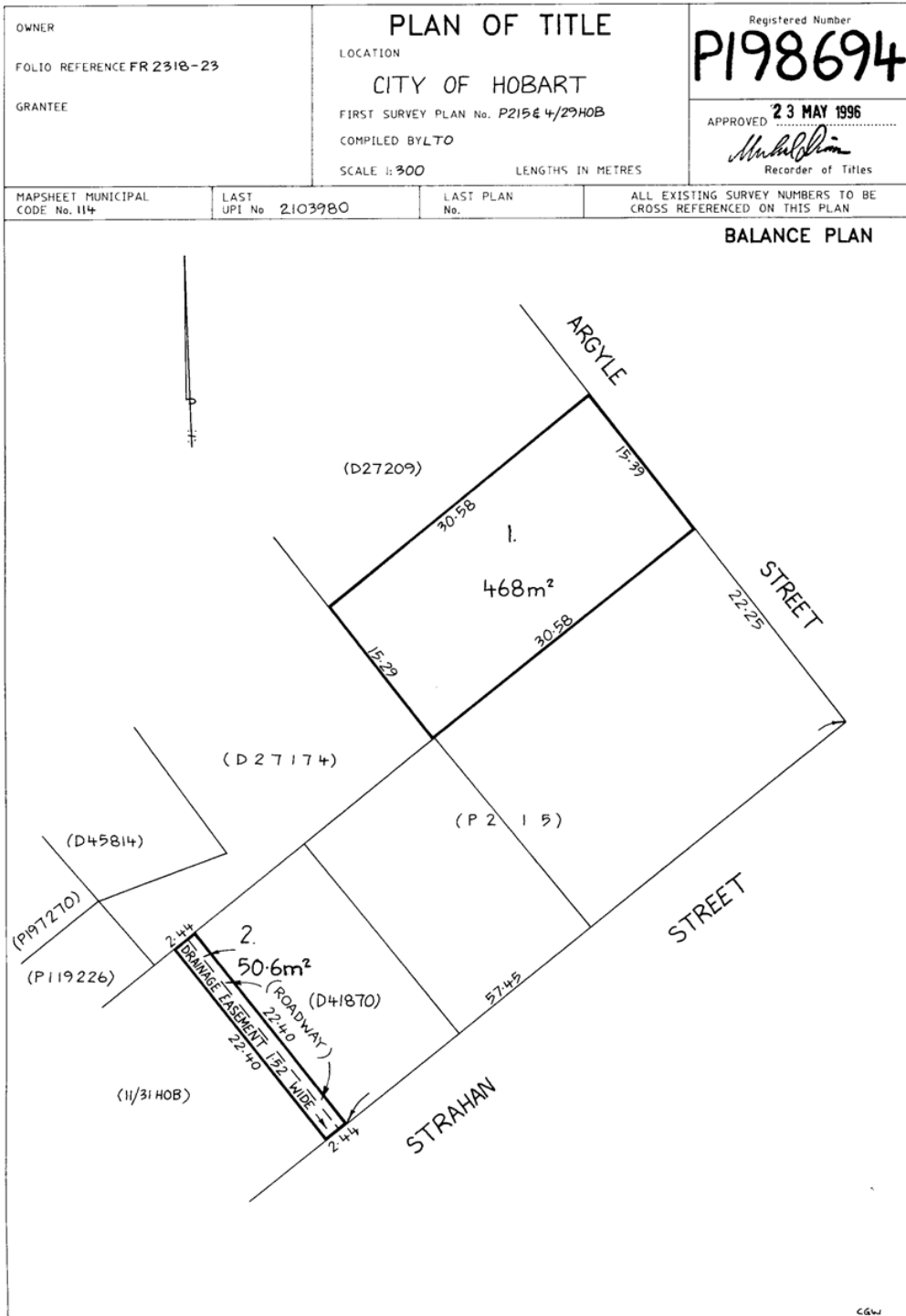
No unregistered dealings or other notations



FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

SEARCH OF TORRENS TITLE

VOLUME 27209	FOLIO 1
EDITION 1	DATE OF ISSUE 10-May-1994

SEARCH DATE : 18-Dec-2025

SEARCH TIME : 01.08 pm

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Diagram [27209](#)

Derivation : Whole of 0A-0R-9/12 Gtd. to J. Kettle.

Prior CT [4210/3](#)SCHEDULE 1

UPTONS SALVAGE PTY. LTD.

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

B8897 MORTGAGE to Commonwealth Bank of Australia

Registered 21-Oct-1985 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



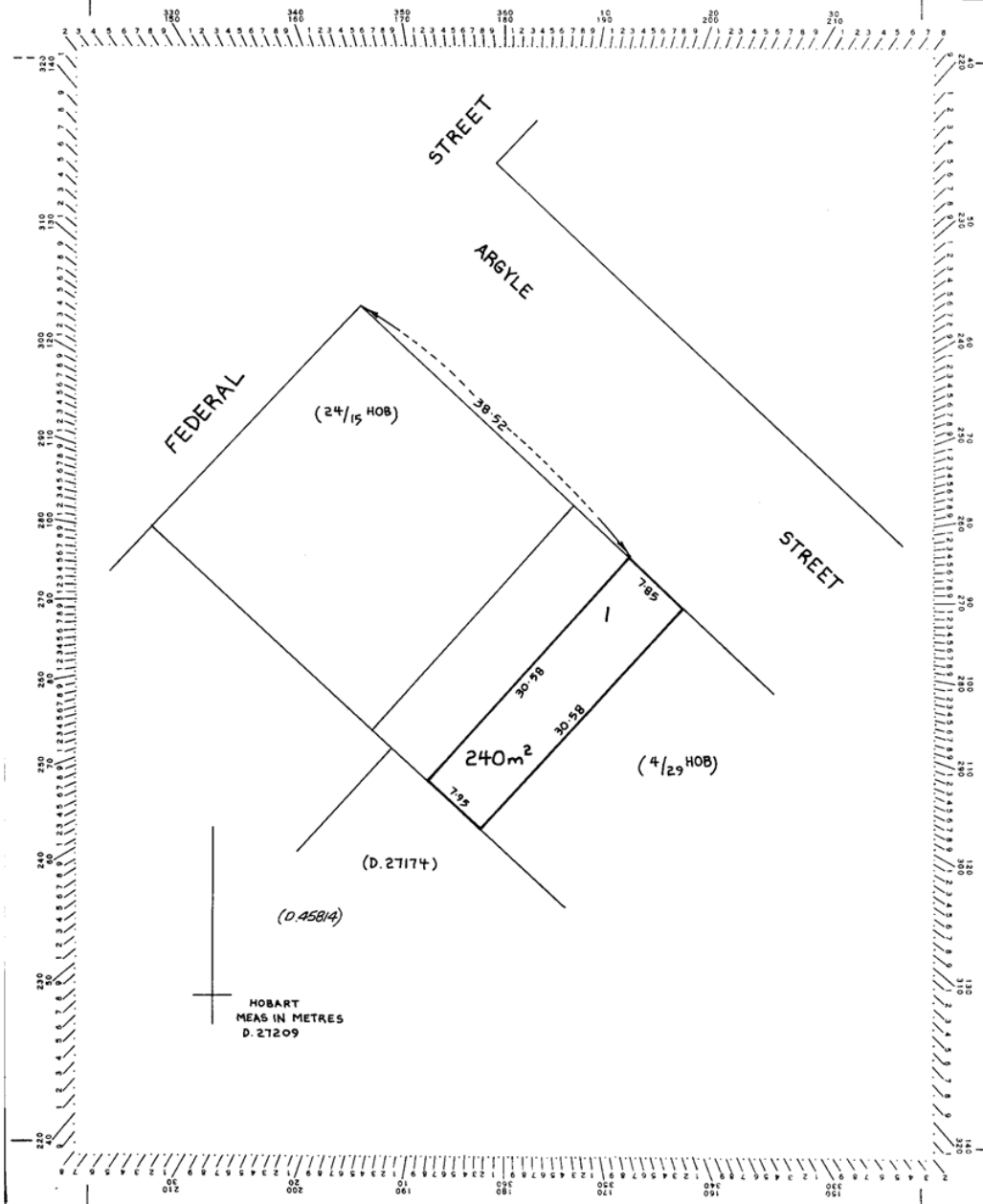
FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



Owner: L.T. ACT 1980	PLAN OF SURVEY of land situated in the CITY OF HOBART	Registered Number: D.27209
Title Reference: Y 4902		Approved: 10 OCT 1985
Grantee: WHOLE OF 0-0-9½ JAMES KETTLE	COMPILED FROM: 4/74 L.O.	<i>E.R. Thang</i> Recorder of Titles
SCALE 1: 400 MEASUREMENTS IN METRES		



**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

SEARCH OF TORRENS TITLE

VOLUME 230163	FOLIO 1
EDITION 1	DATE OF ISSUE 17-Feb-1995

SEARCH DATE : 18-Dec-2025

SEARCH TIME : 01.09 pm

DESCRIPTION OF LAND

City of HOBART
 Lot 1 on Plan [230163](#)
 Derivation : The Allotment Gtd. to J. Kettle.
 Prior CT [2988/52](#)

SCHEDULE 1

[A997959](#) TRANSFER to UPTONS SALVAGE PTY. LTD.

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
[A977960](#) MORTGAGE to Commonwealth Bank of Australia
 Registered 25-Sept-1985 at 12.02 pm

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



ORIGINAL - NOT TO BE REMOVED FROM TITLES OFFICE

R.P. 1469
TASMANIA
REAL PROPERTY ACT, 1862, as amended

CERTIFICATE OF TITLE



Register Book
Vol. Fol.

NOTE - REGISTERED FOR OFFICE
CONVENIENCE TO REPLACE

2988 52

Cert. of Title Vol. 356. Fol. 51.

I certify that the person described in the First Schedule is the registered proprietor of an estate in fee simple in the land within described together with such interests and subject to such encumbrances and interests as are shown in the Second Schedule. In witness whereof I have hereunto signed my name and affixed my seal.

M. H. Lincoln
Recorder of Titles.



DESCRIPTION OF LAND

CITY OF HOBART
NINE PERCHES AND ONE HALF OF A PERCH on the Plan hereon

FIRST SCHEDULE (Continued overleaf)

BERNARD KEITH KREMMER of Hobart, Gentleman *G.V. Kremmer*
ACTING

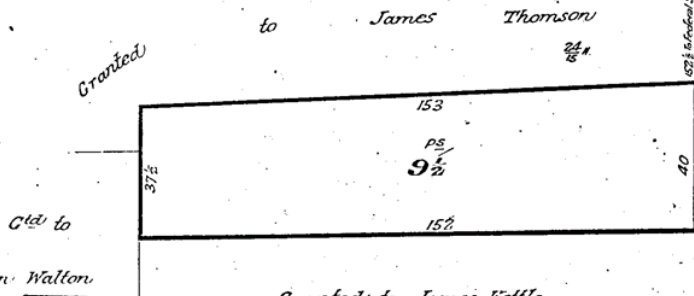
SECOND SCHEDULE (Continued overleaf)
NIL.

TITLES ARE NO LONGER SUBSISTING.

Lot 1 of this plan consists of all the land comprised in the above-mentioned cancelled folio of the Register.

REGISTERED NUMBER
230163

NOTE - ENTIRE'S CANCELLED



CANCELLED
17 FEB 1995
M. H. Lincoln
RECORDER OF TITLES
TASMANIA

The Allotment Gtd. to J. Kettle Meas. in Links
FIRST Edition. Registered **24 MAR 1971**
Derived from C.T. Vol. 356. Fol. 51. Transfer A331595 G.V. Kremmer *pc.*

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



SEARCH OF TORRENS TITLE

VOLUME 27174	FOLIO 2
EDITION 1	DATE OF ISSUE 02-June-1994

SEARCH DATE : 18-Dec-2025

SEARCH TIME : 01.08 pm

DESCRIPTION OF LAND

City of HOBART

Lot 2 on Diagram [27174](#)

Being the land described in Conveyance No. 59/7189

Derivation : Part of 4A-0R-15Ps. Gtd. to J. Walton

Prior CT [4210/4](#)SCHEDULE 1

UPTONS SALVAGE PTY LTD

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 BURDENING EASEMENT a right of carriageway for the Hydro
 Electric Commission over the piece of land marked A.B.
 C.D. on Diagram No. [27174](#)

BURDENING EASEMENT a right of drainage, as appurtenant to 50.6
 square metres being the land secondly described in
 Certificate of Title Volum 2318 Folio 23 owned by the
 Queens Bridge Investments Proprietary Limited over
 the strip of land 1.52 metres wide marked Drainage
 Easement on Diagram No. [27174](#)

B8897 MORTGAGE to Commonwealth Bank of Australia
 Registered 21-Oct-1985 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



FOLIO PLAN

RECORDER OF TITLES

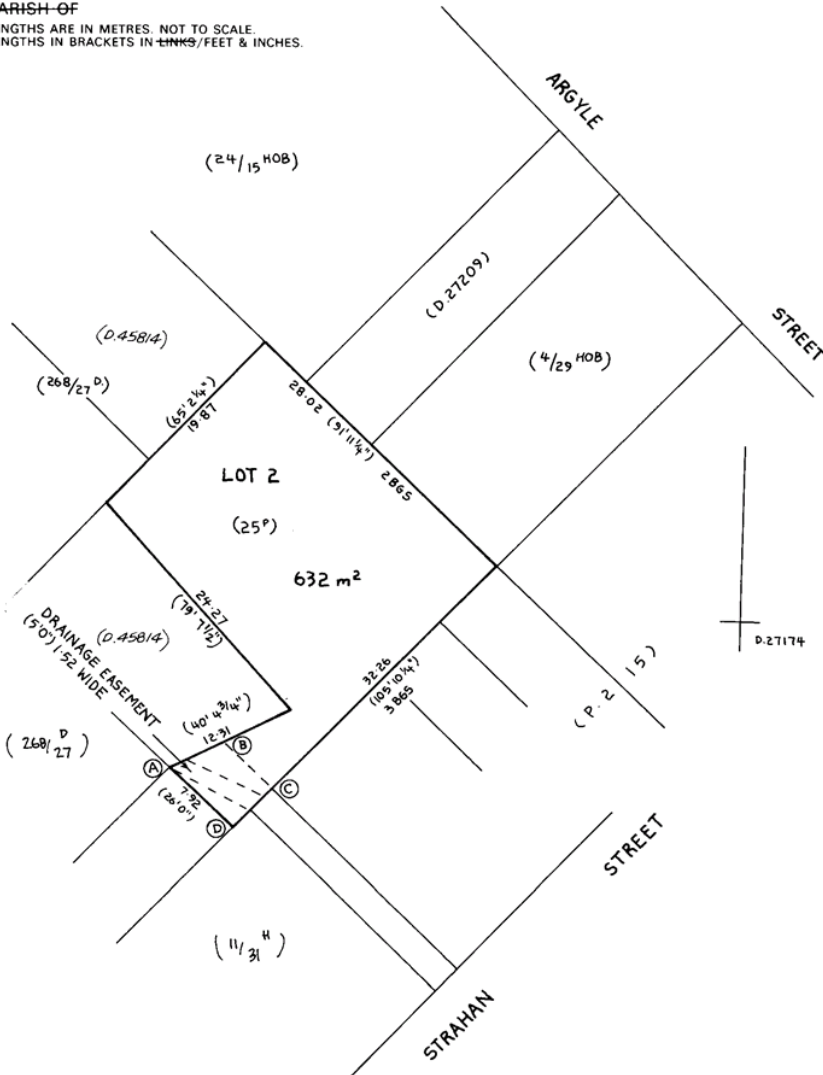
Issued Pursuant to the Land Titles Act 1980



APPROVED 10 OCT 1985 <i>E.R. Thong</i> RECORDER OF TITLES	CONVERSION PLAN CONVERTED FROM 70/46 P.O.	REGISTERED NUMBER D. 27174
FILE NUMBER Y4902	GRANTEE PART OF 4-0-15 JOHN WALTON	DRAWN O.J. WILLSON 2-10-85

SKETCH BY WAY OF ILLUSTRATION ONLY

CITY/TOWN OF HOBART
 LAND DISTRICT OF
 PARISH OF
 LENGTHS ARE IN METRES, NOT TO SCALE.
 LENGTHS IN BRACKETS IN METRES/FEET & INCHES.



**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



SEARCH OF TORRENS TITLE

VOLUME 198694	FOLIO 2
EDITION 1	DATE OF ISSUE 07-Aug-1995

SEARCH DATE : 18-Dec-2025

SEARCH TIME : 01.07 pm

DESCRIPTION OF LAND

City of HOBART

Lot 2 on Plan [198694](#)

Derivation : Whole of 0A-0R-18.1/2Ps and Part of 4A-0R-15Ps

Gtd to J W Arnold & J Walton respectively

Prior CT [2318/23](#)SCHEDULE 1[A997959](#) TRANSFER to UPTONS SALVAGE PTY. LTD.SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 BURDENING EASEMENT: a right of carriageway (appurtenant to the lands comprised in Certificate of Title Volume 166 Folio 181 and mentioned in Notification No. 1698) over the said land within described

BURDENING EASEMENT: a right of drainage (appurtenant to the land comprised in Certificate of Title Volume 1012 Folio 38) over the "Drainage Easement 1.52 wide" on Plan No. [198694](#)

[A997960](#) MORTGAGE to Commonwealth Bank of Australia
 Registered 25-Sept-1985 at 12.02 pm

UNREGISTERED DEALINGS AND NOTATIONS

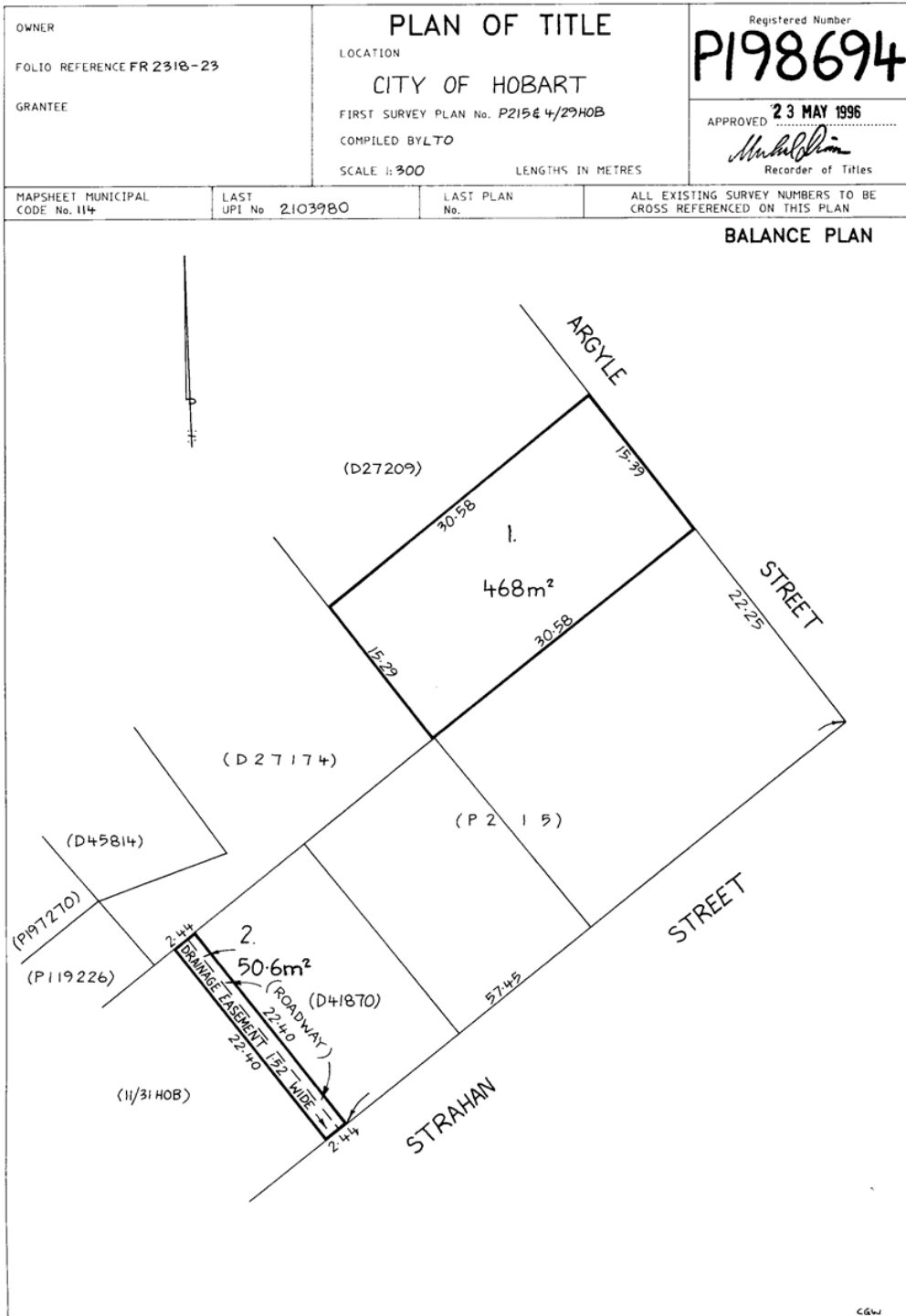
No unregistered dealings or other notations



FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980





INTERNAL REFERRAL

Reference

PLANNA-HOB-2025-5115

Application Reference

PLN-HOB-2025-0663

Address

310 ARGYLE ST NORTH HOBART TAS 7000

Titles

198694/2, 27209/1, 62966/26, 27174/2, 230163/1, 198694/1

Referral Report Type

Referral Report

Referral Report - Development Engineering

Referral Recommendation

Recommendation

Acceptable Subject to Conditions

Referral Officer Name

Stefan Gebka

Referral Officer Role

Development Engineering

Date Completed

10/06/2026

Applicable Codes

Select Applicable Codes/Provisions

-

Overview

Overview

Refer Detailed Assessment

Summary

Assessment Summary

Conditions

#	Condition	Origin
1	<p>ENG 3C - Development Engineering - Parking and Access Design</p> <p>Prior to the first occupation, a suitably qualified engineer must certify that the access driveway and parking area has been constructed in accordance with design drawings approved by condition ENG 3B.</p> <p><i>Advice:</i></p> <p><i>We strongly encourage you to speak to your engineer before works begin so that you can discuss the number and nature of the inspections they will need to do during the works in order to provide this certification. It may be necessary for a surveyor to also be engaged to ensure that the driveway will be constructed as approved.</i></p> <p><i>The reason this condition has been imposed as part of your planning permit is that the driveway is outside the Australian Standard gradients or design parameters. If the driveway is not constructed as it has been approved then this may mean that the driveway will either be unsafe or will not function properly.</i></p> <p><i>An example certificate is available on our website.</i></p>	LUPAA 1993

2	<p>ENG 3B - Development Engineering - Parking and Access Design</p> <p>The following aspects of the access driveway and parking area require further detailed designs:</p> <ol style="list-style-type: none"> 1. Car parking layout 2. Parking spaces provided for use by persons with a disability (where required) <p>This documentation must be submitted and approved as a condition endorsement, prior to the issuing of any approval under the <i>Building Act 2016</i>.</p> <p>The detailed designs must:</p> <ol style="list-style-type: none"> 1. be prepared and certified by a suitably qualified person; 2. be in accordance with the Australian Standard AS/NZS 2890.1:2004, if possible; 3. where the design deviates from AS/NZS 2890.1:2004 the designer must demonstrate that the design will provide a safe and efficient access, and enable safe, easy and efficient use; 4. be in accordance with the Australian Standard AS/NZS 2890.6:2009, where required, and 5. show dimensions and gradients, and other details as Council deem necessary to satisfy the above requirement. <p>The access driveway and parking area must be constructed in accordance with the approved detailed designs prior to first occupation.</p> <p><i>Advice:</i> The detailed design of the access, driveway, and manoeuvring area should be considered prior to finalising the finished floor level of the parking spaces (particularly if located within a garage intrinsic to a dwelling); failure to do so may result in difficulty complying with this condition.</p>	LUPAA 1993
3	<p>ENG 4 - Development Engineering - Parking and Access Design</p> <p>The access driveway and parking module (car parking spaces, aisles and manoeuvring area) approved by this permit must be constructed to a sealed standard (spray seal, asphalt, concrete, pavers or equivalent Council approved) and surface drained to the Council's stormwater infrastructure prior to the commencement of use.</p>	LUPAA 1993
4	<p>ENG 5 - Development Engineering - Number of Car Parking Spaces and Line Marking</p> <p>The approved car parking spaces on the site must comply with the following requirements:</p> <ol style="list-style-type: none"> 1. All parking spaces are to be allocated for customer use only, with no spaces designated for staff parking; 2. any parking spaces designated for people with disabilities must be designed and constructed in accordance with AS2890.6:2009; and 3. all parking spaces on the site must be used in accordance with the use approved by this planning permit. <p>All parking spaces must be delineated by means of white or yellow lines 80mm to 100mm wide, or white or yellow pavement markers in accordance with Australian Standards AS/NZS 2890.1 2004 and AS/NZS 2890.6: 2009, prior to the commencement of use.</p>	
5	<p>ENG 6 - Development Engineering - Parking Spaces For People With Disabilities</p> <p>Any car parking spaces for people with disabilities must be delineated to Australian/NZS Standard, Parking facilities Part 6: Off-street parking for people with disabilities AS/NZS 2890.6: 2009, prior to the commencement of use (whichever occurs first).</p>	LUPAA 1993
6	<p>ENG 1A - Development Engineering - Protection of Council Assets</p> <p>Any damage to council infrastructure resulting from the implementation of this permit, must, at the discretion of the Council:</p> <ol style="list-style-type: none"> 1. Be met by the owner by way of reimbursement (cost of repair and reinstatement to be paid by the owner to the Council); or 2. Be repaired and reinstated by the owner to the satisfaction of the Council. <p>A photographic record of the Council's infrastructure adjacent to the subject site should be provided to the Council prior to any commencement of works.</p> <p>A photographic record of the Council's infrastructure (e.g. existing property service connection points, roads, buildings, stormwater, footpaths, driveway crossovers and nature strips, including if any, pre-existing damage) will be relied upon to establish the extent of damage caused to the Council's infrastructure during construction.</p> <p>In the event that the owner/developer fails to provide to the Council a photographic record of the Council's infrastructure, then any damage to the Council's infrastructure found on completion of works will be deemed to be the responsibility of the owner.</p>	LUPAA 1993
7	<p>ADVICE</p> <p>The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, bylaws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's website for further information.</p> <p>Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.</p>	
8	<p>CONDITION ENDORSEMENT</p> <p>If any condition requires that further documents are submitted and approved, you will need to submit the relevant documentation to satisfy the condition via the Condition Endorsement Submission through PlanBuild. Detailed instructions can be found here.</p> <p>Once approved, the Council will respond to you via PlanBuild that the condition has been endorsed (satisfied).</p> <p>Where building approval is also required, it is recommended that documentation for condition endorsement be submitted well before submitting documentation for building approval. Failure to address condition endorsement requirements prior to submitting for building approval may result in unexpected delays.</p> <p>Fees for Condition Endorsement are set out in Council's Fees and Charges.</p>	

9	<p>BUILDING PERMIT You may need building approval in accordance with the <i>Building Act 2016</i>, further details are available on the Council's website, which may assist you in understanding the relevant requirements. A checklist has also been developed by Consumer, Building and Occupational Services (CBOS) to help property owners understand their responsibilities before, during, and after building work. It outlines the key steps in the building work approval process for notifiable (medium risk) and permit (high risk) work under the <i>Building Act 2016</i>. This resource is designed to support owners in meeting their obligations and ensuring a smooth approvals process. You can access the checklist here.</p> <p>This is a Discretionary Planning Permit issued in accordance with section 57 of the <i>Land Use Planning and Approvals Act 1993</i>.</p> <p>PLUMBING PERMIT You may need plumbing approval in accordance with the <i>Building Act 2016</i>, <i>Building Regulations 2016</i> and the National Construction Code. Further details are available on the Council's website, which may assist you in understanding the relevant requirements. A checklist has also been developed by Consumer, Building and Occupational Services (CBOS) to help property owners understand their responsibilities before, during, and after building work. It outlines the key steps in the building work approval process for notifiable (medium risk) and permit (high risk) work under the <i>Building Act 2016</i>. This resource is designed to support owners in meeting their obligations and ensuring a smooth approvals process. You can access the checklist here.</p>	
10	<p>OCCUPATION OF THE PUBLIC HIGHWAY You may require a permit for the occupation of the public highway for construction (e.g. placement of skip bin, crane, scissor lift etc.). Click here for more information.</p> <p>GENERAL EXEMPTION (TEMPORARY) PARKING PERMITS You may qualify for a General Exemption permit for construction vehicles i.e. residential or meter parking/loading zones. Click here for more information.</p>	
11	<p>FEES AND CHARGES Click here for information on the Council's fees and charges.</p> <p>BEFORE YOU DIG Click here for before you dig information.</p>	
12	<p>ENG SW1 - Stormwater - Design All stormwater from the proposed development must be drained to a lawful point of discharge to the public stormwater system prior to occupancy or commencement of use (whichever occurs first). <i>Advice:</i> Under section 23 of the Urban Drainage Act 2013 it is an offence for a property owner to direct stormwater onto a neighbouring property.</p>	LUPAA 1993

Detailed Assessment

Detailed Assessment

DEVELOPMENT ENGINEERING

ASSESSMENT SUMMARY**:**

Tasmanian Planning Scheme - State Planning Provisions		
CLAUSE	DESCRIPTION	ASSESSMENT DECISION
C2.0 - Parking and Sustainable Transport Code	DOES APPLY	
C2.5 - Use Standards		
C2.5.1	Car parking numbers	PERFORMANCE CRITERIA
C2.5.2	Bicycle parking numbers	NOT APPLICABLE
C2.5.3	Motorcycle parking numbers (>20 car parking spaces)	PERFORMANCE CRITERIA
C2.5.4	Loading Bays (>1000m2 floor area in a single occupancy)	NOT APPLICABLE
C2.5.5	Number of car parking spaces within the General Residential Zone and Inner Residential Zone (Reuse/COU)	NOT APPLICABLE
C2.6 - Development Standards for Buildings and Works		
C2.6.1	Construction of parking areas	NOT APPLICABLE
C2.6.2	Design and layout of parking areas	ACCEPTABLE SOLUTION
C2.6.3	Number of accesses for vehicles	NOT APPLICABLE
C2.6.4	Lighting of parking areas within the General Business Zone and Central Business Zone (5 or more car parking spaces)	NOT APPLICABLE

C2.6.5	Pedestrian access (<i>10 or more car parking spaces</i>)	PERFORMANCE CRITERIA
C2.6.6	Loading bays (<i>Based on C2.5.4</i>)	NOT APPLICABLE
C2.6.7	Bicycle parking and storage facilities within the General Business Zone and Central Business Zone	NOT APPLICABLE
C2.6.8	Siting of parking and turning areas (<i>Specific Zones</i>)	NOT APPLICABLE
C2.7 - Parking Precinct Plan		
C2.7.1	Parking precinct plan	NOT APPLICABLE
C3.0 - Road and Railway Assets Code		
DOES NOT APPLY		
C3.5 - Use Standards		
C3.5.1	Traffic generation at a vehicle crossing, level crossing or new junction	NOT APPLICABLE
C3.6 - Development Standards for Buildings or Works		
C3.6.1	Habitable buildings for sensitive uses within a road or railway attenuation area	NOT APPLICABLE
C3.7 - Development Standards for Subdivision		
C3.7.1	Subdivision for sensitive uses within a road or railway attenuation area	NOT APPLICABLE
6.11 Conditions and Restrictions on a Permit		
6.11.2 (g) / C11 / C12 Erosion, and stormwater volume and quality controls		
DO NOT APPLY		
6.0 - Assessment of an Application for Use or Development		
6.11.2 (g)	Erosion, and stormwater volume and quality controls Protection of Hobart City Council Assets	NOT APPLICABLE
C11.0 - Coastal Inundation Hazard Code		
C11.6.1	Buildings and works, excluding coastal protection works, within a coastal inundation hazard area	NOT APPLICABLE
C11.7.1	Subdivision within a coastal inundation hazard area	NOT APPLICABLE
C12.0 - Flood-Prone Areas Hazard Code		
C12.6.1	Buildings and works within a flood-prone hazard area	NOT APPLICABLE
C12.7.1	Subdivision within a flood-prone hazard area	NOT APPLICABLE
HOB – Local Provisions Schedule		

DISCRETION SUMMARY:[C2.5.1](#) Car parking numbers

Documentation submitted to date does not comply with the Acceptable Solution, therefore assessment against the Performance Criterion is relied on for clause C2.5.1.

Acceptable solution - A1: - **DOES NOT COMPLY: (d/ii).**

The proposal includes fourteen (14x) on-site car parking spaces, which is thirty-five (35x) less than required by Table C2.1.

Performance Criteria - P1.1: - **ACCEPTED AS MEETING THE PERFORMANCE CRITERIA** The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use.

Based on the documentation submitted to date and given the assessment, the car parking numbers is accepted as meeting the Performance Criteria P1.1:C2.5.1 of the Planning Scheme.

C2.5.3 Motorcycle parking numbers (>20 car parking spaces for the use)

The motorcycle parking number assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the *Tasmanian Planning Scheme - State Planning Provisions*.

Documentation submitted to date does comply with the Acceptable Solution for clause C2.5.3.

Acceptable Solution - A1: - **DOES NOT COMPLY: (a)**

The number of on-site motorcycle parking spaces for all uses must:

(a) be no less than the number specified in [Table C2.4](#);

*Based on the documentation submitted to date and given the assessment, the motorcycle parking numbers is accepted as meeting the Performance Criteria P1:C.2.5.3 of the Planning Scheme.***

C2.6.5 Pedestrian access (10 or more car parking spaces)

The pedestrian access must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the *Tasmanian Planning Scheme - State Planning Provisions*.

Documentation submitted to date does not comply with the Acceptable Solution, therefore assessment against the Performance Criterion is relied on for clause C2.6.5.

Acceptable Solution A1.1: - **DOES NOT COMPLY: (ai), (aii), (b)**

Acceptable Solution A1.2: - **DOES NOT COMPLY**

Performance Criteria - P1: - **ACCEPTED AS MEETING THE PERFORMANCE CRITERIA**

Based on the submitted documentation to date and given the above assessment, the pedestrian access is accepted as meeting the Performance Criteria P1: C2.6.5 of the Planning Scheme.

REFERRALS:

TasWater:	REFERRAL NOT REQUIRED					
Check Applicable (X)	Sewer Main Proximity	New Sewer Connection	New Water Connection	Additional loading	Subdivision	Other

Waterways:	REFERRAL NOT REQUIRED					
Check Applicable (X)	New Connection	Detention or >100m2 Hardstand	Treatment	Pump / Onsite	S/W Main Proximity	Subdivision

Roads:	REFERRAL NOT REQUIRED					
Check Applicable (X)	New Crossover	Structures in Road Reservation	Excavation within Road Reservation	Adjusting Footpath Levels	Subdivision	Other

Traffic:	REFERRAL REQUIRED					
Check Applicable (X)	On-street Parking Demand X	On-street Parking Controls	New Access	Parking Precinct Plan	Subdivision	Other

Survey:	REFERRAL REQUIRED					
Check Applicable (X)	Title Interest	Boundary Interest X	Easement Interest	Strata Interest	Subdivision	Other

Other:	REFERRAL NOT REQUIRED					
Check Applicable (X)	TasNetworks	Parks	Sport & Rec.	Waste	Parking	Other

COMMENTS:

Due to the scope of the proposal, the application has been referred to the Council's Manager City Mobility and Surveying Services Manager. The delegated officers' responses, including recommendations are inserted in the respective referral reports.

RECOMMENDATION:

Approval with conditions

REPRESENTATIONS:

Any representations to be directed to the responsible referral Group / officer for a detailed response.

CONDITIONS:

- ENG 1A: Protection of Council assets
- ENG 3B / ENG 3C: Driveways

- ENG 4: Surface treatment
- ENG 5: The approved car parking spaces on the site must comply with the following requirements:
 1. All parking spaces are to be allocated for customer use only, with no spaces designated for staff parking;
 2. any parking spaces designated for people with disabilities must be designed and constructed in accordance with AS2890.6:2009; and
 3. all parking spaces on the site must be used in accordance with the use approved by this planning permit.
- ENG 6: All car parking spaces for people with disabilities must be delineated to Australian/NZS Standard, Parking facilities Part 6: Off-street parking for people with disabilities AS/NZS 2890.6: 2009

- ENG SW1: Stormwater disposal
- SURV s1: As the approved car parking for the proposed golf simulation activity and associated bar / kitchen is located on land comprised in certificates of title 198694/1 and CT 27209/1, should these titles be transferred to different ownership from the land comprised in certificate of title 62966/26, car parking easements in favour of CT 62966/26 are to be registered over the approved car parking located on land comprised in certificates of title 198694/1 and CT 27209/1 to allow the golf simulation activity and associated bar / kitchen to continue. (Surveying Services Group Imposed)

ADVICE:

- Dial before you dig
- Fees and charges
- Building Permit
- Plumbing Permit
- Occupation of the Public Highway
- Condition endorsement engineering

DETAILED ASSESSMENT***

C2.0 Parking and Sustainable Transport Code:

C2.1	Code Purpose:
	The purpose of the Parking and Sustainable Transport Code is: C2.1.1 To ensure that an appropriate level of parking facilities is provided to service use and development. C2.1.2 To ensure that cycling, walking and public transport are encouraged as a means of transport in urban areas. C2.1.3 To ensure that access for pedestrians, vehicles and cyclists is safe and adequate. C2.1.4 To ensure that parking does not cause an unreasonable loss of amenity to the surrounding area. C2.1.5 To ensure that parking spaces and accesses meet appropriate standards. C2.1.6 To provide for parking precincts and pedestrian priority streets.
C2.2 Application of this Code	This Code applies to use or development of land:
C2.2.1	Unless stated otherwise in a particular purpose zone, or sub-clause C2.2.2, C2.2.3 or C2.2.4, this code applies to all use and development

C2.2.2	Clause C2.5.3 only applies to use and development in the following Use Classes: (a) Business and Professional Services; (b) Community Meeting and Entertainment; (c) Custodial Facility; (d) Crematoria and Cemeteries; (e) Educational and Occasional Care; (f) Food Services; (g) General Retail and Hire; (h) Hospital Services; (i) Hotel Industry; (j) Pleasure Boat Facility; (k) Residential if for a communal residence, multiple dwellings or hostel use; (l) Sports and Recreation; and (m) Tourist Operation.
C2.2.3	Clause C2.5.4 only applies to use and development in the following Use Classes: (a) Bulky Goods Sales; (b) General Retail and Hire; (c) Manufacturing and Processing; and (d) Storage.
C2.2.4	Clause C2.5.5 only applies to use and development in the following Use Classes: (a) Business and Professional Services; (b) Community Meeting and Entertainment; (c) Educational and Occasional Care; (d) Emergency Services; (e) Food Services; (f) General Retail and Hire; (g) Sports and Recreation; and (h) Utilities, if not for minor utilities.

C2.5 Use Standards:

C2.5	Use Standards This code applies to all use and development.

C2.5.1 Car parking numbers:

Objective: C2.5.1	That an appropriate level of car parking spaces are provided to meet the needs of the use.
Clause for Assessment	Comments / Discussion (In Bold)
	<p>The car parking number assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does not comply with the Acceptable Solution, therefore assessment against the Performance Criterion is relied on for clause C2.5.1.</p> <p>Acceptable solution - A1: - DOES NOT COMPLY: (d/ii). The proposal includes fourteen (14x) on-site car parking spaces, which is thirty-five (35x) less than required by Table C2.1.</p> <p>The number of on-site car parking spaces must be no less than the number specified in Table C2.1, less the number of car parking spaces that cannot be provided due to the site including container refund scheme space, excluding if:</p> <p>(a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan; - Not Applicable, no area plan has been adopted by council.</p> <p>(b) the site is contained within a parking precinct plan and subject to Clause C2.7; - Not Applicable, no parking precinct plan has been adopted by council.</p>

(c) the site is subject to Clause [C2.5.5](#); or

(d) it relates to an intensification of an existing use or development or a change of use where:

(i) the number of on-site car parking spaces for the existing use or development specified in [Table C2.1](#) is greater than the number of car parking spaces specified in [Table C2.1](#) for the proposed use or development, in which case no additional on-site car parking is required; or

(ii) the number of on-site car parking spaces for the existing use or development specified in [Table C2.1](#) is less than the number of car parking spaces specified in [Table C2.1](#) for the proposed use or development, in which case on-site car parking must be calculated as follows:

$$N = A + (C - B)$$

N = Number of on-site car parking spaces required

A = Number of existing on site car parking spaces

B = Number of on-site car parking spaces required for the existing use or development specified in [Table C2.1](#)

C = Number of on-site car parking spaces required for the proposed use or development specified in [Table C2.1](#).

The analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment:

"The subject proposal represents a change of use from a former building supply / hardware store to a Sports and Recreation use (indoor golf simulator facility). Accordingly, Clause C2.5.1 A1(d) applies.

Under Table C2.1, the proposed land use is classified as:

- 'Sports and Recreation, excluding as otherwise specified in this Table'. This is a requirement for 50 spaces.

The previous use of the site was a building supply/ hardware store, which falls within 'Bulky Goods Sales (excluding as otherwise specified)', which requires 1 space per 50m² of floor area. Applying this rate to the existing gross floor area of approximately 720 m²:

- $720 \div 50 = 15$ spaces (rounded up from 14.4 spaces)

Accordingly, the previous use required approximately 15 spaces under Table C2.1."

"As the number of spaces required for the proposed use (50) is greater than the number required for the previous use (14-15), Clause C2.5.1 A1(d)(ii) applies.

The formula is:

$$N = A + (C - B)$$

Where:

A = Number of existing on-site spaces (14)

B = Number of spaces required for previous use (15)

C = Number of spaces required for proposed use (50)

Using B = 15:

$$N = 14 + (50 - 15)$$

$$N = 14 + 35$$

$$N = 49 \text{ spaces}$$

Accordingly, under the Acceptable Solution pathway, the site would require approximately 49 on-site car parking spaces.

As only 14 spaces are provided on site, the proposal does not satisfy Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme."

Performance Criteria - P1.1: - ACCEPTED AS MEETING THE PERFORMANCE CRITERIA The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use, having regard to:

(a) the availability of off-street public car parking spaces within

Clause C2.5.1

Car parking numbers

reasonable walking distance of the site;
- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment

"There are no significant public off-street car parking facilities within convenient walking distance of the site. The proposal does not rely on formal public parking infrastructure, and parking demand is instead accommodated through a combination of on-site provision and available on-street parking within the surrounding road network."

(b) the ability of multiple users to share spaces because of:
 (i) variations in car parking demand over time; or
 (ii) efficiencies gained by consolidation of car parking spaces;
- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment

"The proposed use exhibits strong temporal variation in parking demand.

*→ Weekday daytime demand is low (typically 5–8 vehicles in total).
 → Peak parking demand occurs predominantly during evening periods (particularly 6:00pm–9:00pm) and on weekends.*

During weekday business hours, surrounding commercial premises generate higher parking demand, however the proposed use is largely self-sufficient during this period.

During evenings, when the proposed use experiences its highest parking demand, surrounding commercial uses are generally closed or operating at reduced intensity. This creates a complementary demand profile, allowing efficient shared use of on-street parking in the vicinity of the site."

(c) the availability and frequency of public transport within reasonable walking distance of the site;
- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment

"The site is located on Argyle Street, a major arterial corridor serviced by frequent public bus services. A bus stop is located directly adjacent to the site frontage. The availability and frequency of public transport provides a realistic alternative to private vehicle travel, particularly for evening and weekend patronage."

(d) the availability and frequency of other transport alternatives;
- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment

"The site is located within the established urban area of North Hobart and is accessible by walking and cycling. Argyle Street provides on-road bicycle lanes in both directions and continuous pedestrian footpaths. The surrounding residential catchment allows for short-distance access without reliance on private vehicles. The nature of the use (group bookings, social events, evening trading) also lends itself to car-pooling and ride-share usage, which has been observed at the operator's Launceston venue."

(e) any site constraints such as existing buildings, slope, drainage, vegetation and landscaping;
- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment

"The site is an existing developed commercial tenancy with a fixed building footprint and established access arrangement. There is no practical capacity to expand on-site parking provision without significant building alteration or demolition. The frontage to Argyle Street is constrained, and the site configuration does not allow for additional compliant parking to be provided without unreasonable cost and loss of built form. Accordingly, physical site constraints materially limit the

PERFORMANCE CRITERIA

ability to increase on-site parking supply.”

(f) the availability, accessibility and safety of on-street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity;

- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment

“Updated on-street parking surveys were undertaken on Thursday, Friday and Saturday in March 2026 to reflect typical weekday and weekend conditions (refer Section 5.3).

The surveys indicate that:

→ During weekday daytime periods, spare capacity within the immediate survey area is generally in the order of 12–17 spaces, with a minimum observed capacity of approximately 11 spaces.

→ During peak weekend daytime periods, spare capacity reduces to approximately 8 spaces, reflecting higher background demand within the North Hobart precinct.

→ During evening periods (6:00pm–8:00pm), spare capacity increases across all survey days, generally ranging between 8 and 17 spaces.

Importantly, the proposed development's peak parking demand occurs during the evening period, when on-street parking availability is demonstrably higher than during daytime conditions.

The surrounding road network comprises a mix of arterial and local streets with formalised on-street parking arrangements. The crash history review (Section 2.2) does not identify any safety issues associated with on-street parking in the vicinity of the site.

Accordingly, on-street parking in the surrounding area is considered to be available, accessible and safe, and capable of accommodating peak overflow demand beyond the 14 on-site spaces.”

(g) the effect on streetscape; and

- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment

“Requiring compliance with the Acceptable Solution (49 spaces) would necessitate

substantial redevelopment of the site and removal of built form to accommodate surface parking. This would be inconsistent with the established urban character of Argyle Street and would negatively affect the streetscape. Retention of the existing built form and parking arrangement maintains the established urban interface and is considered a superior planning outcome.”

(h) any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development.

- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment

“A first-principles parking demand assessment has been undertaken in Section

5.2, having regard to patronage, vehicle occupancy, dwell time and staffing levels. The assessment indicates:

→ Weekday daytime demand: typically 5–8 vehicles

→ Regular peak demand (evenings / league nights): up to approximately 25 vehicles

→ Occasional larger functions: up to approximately 25 vehicles

The 14 on-site spaces accommodate the majority of demand under average operating conditions.

During peak evening periods, an overflow demand of approximately 10–13 vehicles may occur.

The updated on-street parking surveys confirm that available spare capacity within the surrounding road network during these periods (typically 8–17

spaces) is sufficient to accommodate this demand.

Importantly, there is a strong temporal alignment between peak parking demand and peak on-street parking availability, with the highest demand from the proposed use occurring at times when surrounding commercial parking demand is reduced.

The Acceptable Solution requirement of 49 spaces is derived from a generic rate and does not reflect the actual operational characteristics of the use, including group travel behaviour, carpooling, ride-share usage, and evening-focused activity patterns.

Accordingly, the proposed parking provision is consistent with the demonstrated and realistic parking demand of the use.

This conclusion is supported by the operation of the Launceston X-Golf venue, which accommodates a comparable use with substantially lower on-site parking provision. The successful operation of that facility confirms that actual parking demand for this land use is materially lower than that implied by generic Planning Scheme rates.

Having regard to the demonstrated parking demand, the temporal variation in demand, the availability of on-street parking during peak periods, the excellent public transport accessibility, and the physical constraints of the site, the proposed provision of 14 on-site parking spaces is considered to meet the reasonable needs of the use. It is also relevant that the former bulky goods use of the site operated with 14 on-site spaces for many years without identified parking-related safety or network issues. The proposed use generates lower daily traffic volumes than the former use and does not represent an intensification of access activity.

Accordingly, there is no evidence to suggest that retention of the existing 14 spaces would give rise to unacceptable parking or traffic impacts.

Accordingly, the proposal satisfies the Performance Criteria P1 of Clause C2.5.1 of the Planning Scheme.

The parking provision is considered to meet the reasonable needs of the use and represents an appropriate outcome having regard to the demonstrated demand, temporal variation in parking activity and the availability of on-street parking during peak periods."

Performance Criteria - P1.2: - NOT APPLICABLE

The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:

- (a) the nature and intensity of the use and car parking required;
- (b) the size of the dwelling and the number of bedrooms; and
- (c) the pattern of parking in the surrounding area.

- Council's City Transport Group did not express concerns with the statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment.

"Based on the analysis of the TIA, the proposed X-Golf facility at 310 Argyle Street, North Hobart, has a theoretical parking shortfall when assessed against generic Planning Scheme requirements. However, the TIA provides a full explanation for this shortfall and demonstrates the capacity to absorb parking spillage through:

- ü A realistic assessment of actual parking demand, which is significantly lower than theoretical rates.
- ü The availability of public transport and encouragement of alternative transport methods.
- ü Physical site constraints that make increasing on-site parking impractical.
- ü Empirical evidence from a successfully operating comparable venue in Launceston.

The maximum number of patrons simultaneously is typically 36, with occasional functions accommodating up to 50. The TIA concludes that the proposed development will not adversely impact the operation, safety, or efficiency of the surrounding road network and is supported on traffic and parking grounds [1, p. 33].

Outside the scope of the Traffic Impact Assessment (TIA), surrounding streets—including Federal Street (towards Brooker Avenue), Yardley Street, and Wignall Street—present opportunities for spillage parking. A total of 20 residential parking permits were recorded across the surveyed streets (4 on Lefroy St, 12 on Strahan St, and 4 on Argyle St), indicating that some residents may be relying

	<p>on-street parking for additional vehicles not covered by a permit.</p> <p>Residents have raised their concerns regarding the impact of on-street parking during late night periods. The events are expected to have a lower number of patrons during the evening. The demand in North Hobart are reasonably high meaning that residents would have to already compete with parking pressures on the street during that period. The shortfall from the development is unlikely to significantly exacerbate the current peak demand. The on-street car park is a public asset and available to all businesses and residents on the street.</p> <p>While weekend availability is expected to be constrained, the TIA demonstrates that the surrounding network has the capacity to absorb a spillage of 11 to 12 vehicles during peak periods. From City Transport's perspective, the parking demand generated from the development will not significantly impact the current demand on the surrounding streets. Patrons also have access to nearby public transport services and active travel modes."</p> <p>Based on the documentation submitted to date and given the above assessment, the car parking numbers is accepted as meeting the <i>Performance Criteria P1.1:C2.5.1</i> of the Planning Scheme.</p>
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C2.5.2 Bicycle parking numbers:

Objective: C2.5.2	That an appropriate level of bicycle parking spaces are provided to meet the needs of the use.
<p>Clause C2.5.2: Bicycle parking numbers ACCEPTABLE SOLUTION</p>	<p>The bicycle parking number assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does comply with the Acceptable Solution for clause C2.5.2.</p> <p>Acceptable solution - A1: - DOES COMPLY Bicycle parking spaces must: (a) be provided on the site or within 50m of the site; - N/A</p> <p>and</p> <p>(b) be no less than the number specified in Table C2.1.</p> <p>- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment</p> <p>"The Acceptable Solution A1 of Clause C2.5.2 of the Planning Scheme states that "bicycle parking must (a) be provided on the site or within 50m of the site; and (b) be no less than the number specified in Table C2.1".</p> <p>Table C2.1 specifies 'No Requirement' for 'Sports and Recreation excluding as otherwise specified in this Table'.</p> <p>No bicycle parking is proposed, thereby satisfying the requirements of Acceptable Solution A1 of Clause C2.5.2 of the Planning Scheme."</p>

C2.5.3 Motorcycle parking numbers:

Objective: C2.5.3	That the appropriate level of motorcycle parking is provided to meet the needs of the use.
	<p>The motorcycle parking number assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does comply with the Acceptable Solution for clause C2.5.3.</p> <p>Acceptable Solution - A1: - DOES NOT COMPLY: (a) The number of on-site motorcycle parking spaces for all uses must: (a) be no less than the number specified in Table C2.4;</p>

Clause C2.5.3:

Motorcycle parking numbers

PERFORMANCE CRITERIA

and
 (b) if an existing use or development is extended or intensified, the number of on-site motorcycle parking spaces must be based on the proposed extension or intensification, provided the existing number of motorcycle parking spaces is maintained.

Performance Criteria - P1: - **ACCEPTED AS MEETING THE PERFORMANCE CRITERIA** Motorcycle parking spaces for all uses must be provided to meet the reasonable needs of the use, having regard to:

The analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment;

"Table C2.4 specifies the number of motorcycle parking spaces required based on the number of car parking spaces required for the use.

As outlined in Section 5.4, the number of car parking spaces required for the proposed use under Clause C2.5.1 is 49 spaces (calculated under the change-of-use provisions).

In accordance with Table C2.4:

- 21–40 car spaces required → 1 motorcycle space
- 41 or more car spaces required → 1 additional motorcycle space for every additional 20 car spaces required

For 49 required car parking spaces, the motorcycle parking requirement equates to 1 motorcycle parking space. No dedicated motorcycle parking spaces are proposed as part of the development. Accordingly, the proposal does not satisfy Acceptable Solution A1 of Clause C2.5.3."

(a) the nature of the proposed use and development;
 - **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

"The proposed use is a small-scale indoor recreation facility located within an existing commercial tenancy. The assessment of patron behaviour indicates that the majority of users attend in small groups (2–4 persons), and travel is predominantly by private car, ride-share and public transport. There is no evidence to suggest a material proportion of patrons will travel by motorcycle. Staff numbers are low (generally 1–3 persons), further limiting the likelihood of routine motorcycle commuting. On this basis, the reasonable demand for dedicated motorcycle parking is expected to be low."

(b) the topography of the site;
 - **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

"The development is contained within an existing built commercial site with established levels and access arrangements. There are no topographical features that would create a specific need for dedicated motorcycle parking (such as large gradients or separated areas requiring protected motorcycle parking). The topography does not drive the need for additional motorcycle facilities."

(c) the location of existing buildings on the site;
 - **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

"The existing building footprint and site configuration constrain available external area. The car parking area is an established, compact layout serving the tenancy. There is limited opportunity to introduce additional formal motorcycle parking spaces without reconfiguring the existing parking layout or reducing the number/ functionality of existing car spaces."

(d) any constraints imposed by existing development; and
 - **Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment**

	<p><i>"The site is constrained by its existing development form, access arrangement and established car parking supply. No changes to the car parking layout are proposed and the existing 14 spaces will be retained. Creating a dedicated motorcycle bay would either reduce the number of existing car spaces or introduce a non-standard arrangement within an already constrained layout. Given the low expected motorcycle demand, such changes are not considered reasonable or necessary."</i></p> <p>(e) the availability and accessibility of motorcycle parking spaces on the street or in the surrounding area. - Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment</p> <p><i>"The site is located within an established commercial corridor where on-street parking is available in the surrounding road network. A motorcycle can generally be accommodated within on-street parking areas (subject to signage and demand) and within available kerbside spaces in the broader locality. Given the low expected motorcycle mode share for the use, the availability of surrounding kerbside parking provides a practical alternative to dedicated on-site motorcycle bays."</i></p> <p><i>"Having regard to the small scale of the use, the low likelihood of significant motorcycle patronage, and the physical constraints of the existing site and parking layout, the provision of a dedicated on-site motorcycle parking space is not considered necessary to meet the reasonable needs of the use. The proposal is therefore considered to satisfy Performance Criteria P1 of Clause C2.5.3."</i></p> <p><i>Based on the documentation submitted to date and given the above assessment, the motorcycle parking numbers is accepted as meeting the Performance Criteria P1:C.2.5.3 of the Planning Scheme.**</i></p>
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C2.5.4 Loading Bays:

Objective: C2.5.4	That adequate access for goods delivery and collection is provided, and to avoid unreasonable loss of amenity and adverse impacts on traffic flows.
<p>Clause C2.5.4:</p> <p>Loading Bays</p> <p>NOT APPLICABLE</p>	<p>The Loading Bay assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. Documentation submitted to date does not invoke clause C2.5.4.</p> <p>Clause C2.5.4 only applies to use and development in the following Use Classes: (a) Bulky Goods Sales; - Not Applicable (b) General Retail and Hire; - Not Applicable (c) Manufacturing and Processing; and - Not Applicable (d) Storage. - Not Applicable</p> <p>Documentation submitted to date does not indicate any requirement for loading bays for the respective Use Classes (C2.2.3)</p>

C2.5.5 Number of car parking spaces within the General Res. Zone and Inner Res. Zone:

Objective: C2.5.5	<p>To: (a) facilitate the reuse of existing non-residential buildings within the General Residential Zone and Inner Residential Zone; and (b) to not cause an unreasonable impact on residential amenity by the car parking generated by that reuse.</p>
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<p>Clause C2.5.5:</p> <p>Number of car parking spaces within the General Residential Zone and Inner Residential Zone</p> <p><i>(Reuse of existing non-residential buildings)</i></p> <p>NOT APPLICABLE</p>	<p>The number of car parking spaces within the General Residential Zone and Inner Residential Zone must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does not invoke clause C2.5.5.</p> <p>Clause C2.5.5 only applies to use and development in the following Use Classes:</p> <p>(a) Business and Professional Services; - Not Applicable (b) Community Meeting and Entertainment; - Not Applicable (c) Educational and Occasional Care; - Not Applicable (d) Emergency Services; - Not Applicable (e) Food Services; - Not Applicable (f) General Retail and Hire; - Not Applicable (g) Sports and Recreation; and** - Applicable** (h) Utilities, if not for minor utilities. - Not Applicable</p> <p>Documentation submitted to date does not indicate any requirement for car parking spaces within the General Residential Zone and Inner Residential Zone for the respective Use Classes (C2.2.4)</p>
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C2.6 Development Standards for Buildings and Works:

<p>C2.6</p>	<p>Development Standards for Buildings and Works This code applies to all use and development.</p>
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C2.6.1 Construction of parking areas:

<p>Objective: C2.6.1</p>	<p>That parking areas are constructed to an appropriate standard.</p>
<p>Clause for Assessment</p>	<p>Comments / Discussion (In Bold)</p>
<p>Clause C2.6.1:</p> <p>Construction of parking areas</p> <p>NOT APPLICABLE</p>	<p>The construction of parking areas must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does not invoke clause C2.6.1.</p> <p>Documentation submitted to date does not indicate any changes proposed to existing parking area(s).</p>

C2.6.2 Design and layout of parking areas:

<p>Objective: C2.6.2</p>	<p>That parking areas are designed and laid out to provide convenient, safe and efficient parking.</p>
	<p>The design and layout of parking areas must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does comply with the Acceptable Solution for clause C2.6.2. Acceptable Solution - A1: - DOES COMPLY Parking, access ways, manoeuvring and circulation spaces must either:</p> <p>(a) comply with the following:</p> <p>(i) have a gradient in accordance with Australian Standard AS 2890 - Parking facilities, Parts 1-6; - Acceptable, no change to existing</p> <p>(ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces; - Acceptable, no change to existing</p> <p>(iii) have an access width not less than the requirements in Table C2.2; - Acceptable, no change to existing</p>

<p>Clause C2.6.2:</p> <p>Design and layout of parking areas</p> <p>ACCEPTABLE SOLUTION</p> <p>(iv) 2.6m min. width</p> <p>(v) 3 or more car parking spaces</p>	<p>(iv) have car parking space dimensions which satisfy the requirements in Table C2.3; - N/A as no change to existing</p> <p>(v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces; - N/A as no change to existing</p> <p>(vi) have a vertical clearance of not less than 2.1m above the parking surface level; - Acceptable, documentation submitted to date can meet this requirement</p> <p>and</p> <p>(vii) excluding a single dwelling, be delineated by line marking or other clear physical means; - Acceptable, documentation submitted to date can meet this requirement</p> <p>or</p> <p>(b) comply with Australian Standard AS 2890- Parking facilities, Parts 1-6. - Complies with part (a)</p> <p><i>If a proposal fully complies with the Acceptable Solution for Clause C2.6.2 (Design and layout of parking areas), a planning authority cannot require additional assessment against the performance criteria of that clause, including importing extra considerations such as sightlines that are not part of C2.6.2 itself.</i></p> <p>The analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment;</p> <p><i>"No changes to the existing on-site car parking layout are proposed as part of this development. The as-surveyed parking layout is shown in Figure 5.</i></p> <p><i>The site currently contains 14 on-site parking spaces accessed via the existing Argyle Street crossover.</i></p> <p><i>These spaces have been in continuous operation for many years in association with the former building supply / bulky goods retail use of the site.</i></p> <p><i>The proposal does not alter the site access arrangement, parking layout, internal circulation or the number of parking spaces provided. The existing parking configuration will be retained in its current form.</i></p> <p><i>As no physical works or modifications to the car parking area are proposed, and the layout has been operating without issue for many years, no further assessment of car parking design compliance is required as part of this application."</i></p> <p>Acceptable Solution - A1.2: - DOES COMPLY</p> <p>Parking spaces provided for use by persons with a disability must satisfy the following:</p> <p>(a) be located as close as practicable to the main entry point to the building; - Acceptable, documentation submitted to date can meet this requirement verified via condition on the planning permit</p> <p>(b) be incorporated into the overall car park design; - Acceptable, documentation submitted to date can meet this requirement verified via condition on the planning permit</p> <p>and</p> <p>(c) be designed and constructed in accordance with Australian/New Zealand Standard AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities.³⁵</p> <p><i>35 Requirements for the number of accessible car parking spaces are specified in part D3 of the National Construction Code 2016.</i></p>
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C2.6.3 Number of accesses for vehicles:

<p>Objective: C2.6.3</p>	<p>That: (a) access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses; (b) accesses do not cause an unreasonable loss of amenity of adjoining uses; and (c) the number of accesses minimise impacts on the streetscape.</p>
<p>Clause C2.6.3: Number of accesses for vehicles NOT APPLICABLE</p>	<p>The number of accesses for vehicles must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. Documentation submitted to date does not invoke clause C2.6.3.</p> <p>Acceptable Solution A1: The number of accesses provided for each frontage must: (a) be no more than 1; or (b) no more than the existing number of accesses, whichever is the greater.</p> <p>Acceptable Solution A2: Within the Central Business Zone or in a pedestrian priority street no new access is provided unless an existing access is removed.</p> <p>Documentation submitted to date does not indicate any changes proposed to the existing number of accesses for vehicles.</p>

C2.6.4 Lighting of parking areas within the General Business Zone and Central Business Zone:

<p>Objective: C2.6.4</p>	<p>That parking and vehicle circulation roads and pedestrian paths within the General Business Zone and Central Business Zone, which are used outside daylight hours, are provided with lighting to a standard which: (a) enables easy and efficient use; (b) promotes the safety of users; (c) minimises opportunities for crime or anti-social behaviour; and (d) prevents unreasonable light overspill impacts.</p>
<p>Clause C2.6.4: Lighting of parking areas within the General Business Zone and Central Business Zone (5 or more car parking spaces) NOT APPLICABLE</p>	<p>The lighting of parking areas within the General Business Zone and Central Business Zone must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. Documentation submitted to date does not invoke clause C2.6.4. Documentation submitted to date does not indicate any lighting of parking areas within the General Business Zone and Central Business Zone proposed.</p>

C2.6.5 Pedestrian access:

<p>Objective: C2.6.5</p>	<p>That pedestrian access within parking areas is provided in a safe and convenient manner.</p>
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<p>Clause C2.6.5: Pedestrian access</p> <p>PERFORMANCE CRITERIA (Uses that require 10 or more car parking spaces)</p>	<p>The pedestrian access must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>. Documentation submitted to date does not comply with the Acceptable Solution, therefore assessment against the Performance Criterion is relied on for clause C2.6.5.</p> <p>Acceptable Solution A1.1: - DOES NOT COMPLY: (ai), (aia), (b) Uses that require 10 or more car parking spaces must: (a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:</p> <p>(i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or</p> <p>(ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and</p> <p>(b) be signed and line marked at points where pedestrians cross access ways or parking aisles.</p> <p>Acceptable Solution A1.2: - DOES NOT COMPLY In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building.</p> <p>Performance Criteria - P1: - ACCEPTED AS MEETING THE PERFORMANCE CRITERIA</p> <p>Safe and convenient pedestrian access must be provided within parking areas, having regard to:</p> <p>(a) the characteristics of the site; (b) the nature of the use; (c) the number of parking spaces; (d) the frequency of vehicle movements; (e) the needs of persons with a disability; (f) the location and number of footpath crossings; (g) vehicle and pedestrian traffic safety; (h) the location of any access ways or parking aisles; and (i) any protective devices proposed for pedestrian safety.</p> <p>- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment</p> <p><i>"Argyle Street has pedestrian footpaths and on-road cycle paths on both sides of the road. Pedestrian phasing has been included in the signalised intersection of Argyle Street and Federal Street to the northwest of the subject site.</i></p> <p><i>The proposed development is likely to generate a small amount of pedestrian activity in the surrounding network. The existing pedestrian infrastructure in the surrounding road network is considered adequate to cater for the likely pedestrian demands associated with the proposed development."</i></p> <p>Based on the submitted documentation to date and given the above assessment, the pedestrian access is accepted as meeting the Performance Criteria P1: C2.6.5 of the Planning Scheme.</p>
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C2.6.6 Loading bays:

<p>Objective: C2.6.6</p>	<p>That the area and dimensions of loading bays are adequate to provide safe and efficient delivery and collection of goods.</p>
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<p>Clause C2.6.6:</p> <p>Loading bays</p> <p>NOT APPLICABLE</p>	<p>The loading bays must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does not invoke clause C2.6.6.</p> <p>Documentation submitted to date does not indicate any loading bays proposed.</p>
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C2.6.7 Bicycle parking and storage:

<p>Objective: C2.6.7</p>	<p>That parking for bicycles are safe, secure and convenient, within the General Business Zone and Central Business Zone.</p>
<p>Clause C2.6.7:</p> <p>Bicycle parking and storage facilities within the General Business Zone and Central Business Zone</p> <p>NOT APPLICABLE</p>	<p>The bicycle parking and storage facilities within the General Business Zone and Central Business Zone must satisfy the Acceptable Solutions of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does not invoke clause C2.6.7.</p> <p>Documentation submitted to date does not indicate any proposed bicycle parking and storage facilities within the General Business Zone and Central Business Zone.</p>

C2.6.8 Siting of parking and turning areas:

<p>Objective: C2.6.8</p>	<p>That the siting of vehicle parking and access facilities in an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone or Central Business Zone does not cause an unreasonable visual impact on streetscape character or loss of amenity to adjoining properties.</p>
<p>Clause C2.6.8:</p> <p>Siting of parking and turning areas</p> <p>NOT APPLICABLE</p> <p>Siting of parking and turning areas</p> <p>within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone</p>	<p>The siting of parking and turning areas within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone must satisfy the Acceptable Solutions of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does not invoke clause C2.6.8.</p> <p>Documentation submitted to date does not indicate any proposed parking and turning areas within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone.</p>

C2.7 Parking Precinct Plan:

C2.7	Parking Precinct Plan
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C2.7.1 On-site car parking spaces within an area defined by a parking precinct plan:

<p>Objective: C2.7.1</p>	<p>To minimise the amount of on-site car parking spaces within an area defined by a parking precinct plan, and that parking does not detract from the streetscape of the area.</p>
<p>Clause for Assessment</p>	<p>Comments / Discussion (In Bold)</p>

<p>Clause C2.7.1:</p> <p>Parking precinct plan</p> <p>NOT APPLICABLE</p> <p>On-site car parking spaces within an area defined by a parking precinct plan</p>	<p>The on-site car parking spaces within an area defined by a parking precinct plan must satisfy the Acceptable Solutions of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does not invoke clause C2.7.1.</p> <p>Documentation submitted to date does not indicate any on-site car parking spaces within an area defined by a parking precinct plan.</p>
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C3.0 Road and Railway Assets Code:

C3.1	Code Purpose
	<p>The purpose of the Road and Railway Assets Code is:</p> <p>C3.1.1 To protect the safety and efficiency of the road and railway networks; and</p> <p>C3.1.2 To reduce conflicts between sensitive uses and major roads and the rail network.</p>
C3.2 Application of this Code	Application of this Code:
C3.2.1	<p>This code applies to a use or development that:</p> <p>(a) will increase the amount of vehicular traffic or the number of movements of vehicles longer than 5.5m using an existing vehicle crossing or private level crossing;</p> <p>(b) will require a new vehicle crossing, junction or level crossing; or</p> <p>(c) involves a subdivision or habitable building within a road or railway attenuation area if for a sensitive use.</p>
C3.4.1	There are no exemptions from this code.

C3.5 Use Standards:

C3.5	Use Standards

C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction:

Objective:	To minimise any adverse effects on the safety and efficiency of the road or rail network from vehicular traffic generated from the site at an existing or new vehicle crossing or level crossing or new junction.
Clause for Assessment	Comments / Discussion (In Bold)
	<p>The traffic generation at a vehicle crossing, level crossing or new junction must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Tasmanian Planning Scheme - State Planning Provisions</i>.</p> <p>Documentation submitted to date does comply with the Acceptable Solution for clause C3.5.1.</p> <p>Acceptable Solution A1: - NOT APPLICABLE For a category 1 road or a limited access road, vehicular traffic to and from the site will not require:</p> <p>(a) a new junction; (b) a new vehicle crossing; or (c) a new level crossing.</p> <p>Acceptable Solution A1.2: For a road, excluding a category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road</p>

<p>Clause C3.5.1:</p> <p>Traffic generation at a vehicle crossing, level crossing or new junction</p> <p>ACCEPTABLE SOLUTION</p>	<p>authority. - N/A</p> <p>Acceptable Solution A1.3: - NOT APPLICABLE For the rail network, written consent for a new private level crossing to serve the use and development has been issued by the rail authority.</p> <p>Acceptable Solution A1.4: - DOES COMPLY Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than:</p> <p>(a) the amounts in Table C3.1; or</p> <p>Vehicle crossings on other roads: 20% or 40 vehicle movements per day, whichever is the greater</p> <p>(b) allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road.</p> <p>- Acceptable, documentation submitted to date can meet this requirement by accepting the analysis / statements provided by the applicant's traffic engineer contained within the Traffic Impact Assessment</p> <p><i>"Table C3.1 specifies a maximum increase in daily traffic volume at an access to be 20% or 40 vehicle movements per day whichever is greater. In this case the traffic generation associated with the previous use is unknown, but can be estimated.</i></p> <p><i>The previous use was a housing/ building supply store. In the absence of site-specific counts, the TfNSW Guide rates for "hardware stores" provide the closest proxy. Applying the weekday daily vehicle trip rate of 32.46 trips/ 100 m² GLFA to an estimated GLFA of ~720 m² indicates the previous use would have generated in the order of 234 two-way vehicle movements per day (and a site peak hour in the order of 30 vehicle movements/ hour).</i></p> <p><i>As outlined in Section 4.1, the proposed indoor golf facility is expected to generate approximately 66 to 68 two-way vehicle movements per day on average, with peak activity occurring during evening periods.</i></p> <p><i>Even allowing for occasional peak trading scenarios, daily traffic generation associated with the proposed use remains well below that of the former hardware/building supply use.</i></p> <p><i>Accordingly, the proposal does not represent an intensification of traffic at the existing access. Rather, it represents a substantial reduction in daily vehicle movements relative to the established lawful use of the site.</i></p> <p><i>In terms of Acceptable Solution A1.4 of Clause C3.5.1, Table C3.1 specifies a maximum allowable increase in daily traffic volume of 20% or 40 vehicle movements per day, whichever is greater. Twenty percent of the estimated previous use traffic generation (234 vpd) equates to approximately 47 vehicle movements per day. Therefore, the relevant threshold in this case is 47 vehicle movements per day.</i></p> <p><i>The proposed development results in a net reduction in daily traffic relative to the previous use and therefore satisfies Acceptable Solution A1.4 of Clause C3.5.1."</i></p> <p>Acceptable Solution A1.5: - NOT APPLICABLE Vehicular traffic must be able to enter and leave a major road in a forward direction.</p>
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C3.6 Development Standards for Buildings or Works:

C3.6	Development Standards for Buildings or Works
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C3.6.1 Habitable buildings for sensible uses within a road or railway attenuation area:

Objective: C3.6.1	To minimise the effects of noise, vibration, light and air emissions on sensitive uses within a road or railway attenuation area, from existing and future major roads and the rail network.
Clause C3.6.1: Habitable buildings for sensitive uses within a road or railway attenuation area NOT APPLICABLE	The habitable buildings for sensitive uses within a road or railway attenuation area must satisfy the Acceptable Solutions of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> . Documentation submitted to date does not invoke clause C3.6.1. Documentation submitted to date does not indicate any habitable buildings (proposed) for sensitive uses within a road or railway attenuation area.

C3.7 Development Standards for Subdivision:

C3.7	Development Standards for Subdivision
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C3.7.1 Subdivision for sensitive uses within a road or railway attenuation area:

Objective: C3.7.1	To minimise the effects of noise, vibration, light and air emissions on lots for sensitive uses within a road or railway attenuation area, from existing and future major roads and the rail network.
Clause C3.7.1: Subdivision for sensitive uses within a road or railway attenuation area NOT APPLICABLE	The subdivision for sensitive uses within a road or railway attenuation area must satisfy the Acceptable Solutions of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> . Documentation submitted to date does not invoke clause C3.7.1. Documentation submitted to date does not indicate any subdivision for sensitive uses within a road or railway attenuation area.

6.11.2 (g) Erosion, and stormwater volume and quality controls:

6.11	Conditions and Restrictions on a Permit
Clause 6.11.2 (g)	Erosion, and stormwater volume and quality controls.
NOT APPLICABLE	The erosion, and stormwater volume and quality controls are to be considered under sub-clauses 6.11.2(g) of the <i>Tasmanian Planning Scheme - State Planning Provisions</i> . Documentation submitted to date does not invoke clause 6.11.2(g). Documentation submitted to date does not indicate any changes proposed to existing erosion, and stormwater volume and quality controls.

OVERVIEW:

Partial Demolition and Alterations for Partial Change of Use to Sports and Recreation

Surveying Services Manager's comments:

"The proposal involves internal demolition and alterations to the existing building on land comprised in CT 62966/26. The existing 12 car parking spaces are located on the adjoining two titles CT 198694/1 and CT 27209/1. All 3 titles are owned by Uptons Salvage Pty. Ltd. There are no changes proposed to the existing parking. As they are shown on the proposal plan presumably they will be used by patrons for the proposed bar / kitchen and golf simulation activity within the building. The existing parking spaces straddle the boundaries between CT 198694/1 and CT 27209/1.

The existing building on CT 230163/1 is partly over the boundary onto CT 27209/1. For this reason it would not be possible to adhere Cs of T 198694/1 and 27209/1 without also adhering CT 230163/1. For this reason I do not propose adhering the titles. While the

titles are all in common ownership this does not pose an issue.

Instead I propose including a condition in the permit: "As the approved car parking for the proposed golf simulation activity and associated bar / kitchen is located on land comprised in certificates of title 198694/1 and CT 27209/1, should these titles be transferred to different ownership from the land comprised in certificate of title 62966/26, car parking easements in favour of CT 62966/26 are to be registered over the approved car parking located on land comprised in certificates of title 198694/1 and CT 27209/1."

ADVICE:

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's [website](#) for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

Condition endorsement:

If any condition requires that further documents are submitted and approved, you will need to submit to the City the drawing, design or detail required by the condition through the condition endorsement submission function in PlanBuild. Detailed instructions can be found [here](#).

Once approved, the Council will respond to you via email that the condition has been endorsed (satisfied).

Where building approval is also required, it is recommended that documentation for condition endorsement be submitted well before submitting documentation for building approval. Failure to address condition endorsement requirements prior to submitting for building approval may result in unexpected delays.

Building Permit:

You may need building approval in accordance with the *Building Act 2016*, further details are available on the [Council's website](#), which may assist you in understanding the relevant requirements.

A checklist has also been developed by Consumer, Building and Occupational Services (CBOS) to help property owners understand their responsibilities before, during, and after building work. It outlines the key steps in the building work approval process for notifiable (medium risk) and permit (high risk) work under the *Building Act 2016*. This resource is designed to support owners in meeting their obligations and ensuring a smooth approvals process. You can access the checklist [here](#).

This is a Discretionary Planning Permit issued in accordance with section 57 of the *Land Use Planning and Approvals Act 1993*.

Plumbing Permit:

You may need plumbing approval in accordance with the *Building Act 2016*, *Building Regulations 2016* and the National Construction Code. Further details are available on the [Council's website](#), which may assist you in understanding the relevant requirements.

A checklist has also been developed by Consumer, Building and Occupational Services (CBOS) to help property owners understand their responsibilities before, during, and after building work. It outlines the key steps in the building work approval process for notifiable (medium risk) and permit (high risk) work under the *Building Act 2016*. This resource is designed to support owners in meeting their obligations and ensuring a smooth approvals process. You can access the checklist [here](#).

Fees and Charges:

Click [here](#) for information on the Council's fees and charges.

Dial Before You Dig:

Click [here](#) for dial before you dig information.

Additional Documents

Version	Document Date	Document Type	Description	Prepared By
1	10 June 2026	Planning Assessment Report	DE Report	Stefan Gebka

Submitted on 10/06/2026