



CITY OF HOBART

# SUPPORTING INFORMATION

## PLANNING AUTHORITY COMMITTEE MEETING

### OPEN PORTION OF THE MEETING

**WEDNESDAY, 24 SEPTEMBER 2025**

**AT 4.00 PM**

**VENUE: COUNCIL CHAMBER, TOWN HALL**

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## SECTION 57 DELEGATED REPORT DISCRETIONARY PLANNING PERMIT

This is a Discretionary Planning Permit issued in accordance with section 57 of the *Land Use Planning and Approvals Act 1993*.

<b>Type of Report:</b>	Committee <i>The development has been assessed under the Hobart Interim Planning Scheme 2015.</i>
<b>Delegated:</b>	24 September 2025
<b>Expiry Date:</b>	25 September 2025
<b>Application No:</b>	PLN-HOB-2021-0710 (PLN-21-710)
<b>Address:</b>	1 SANDY BAY ROAD, HOBART, 3 SANDY BAY ROAD, HOBART, 5-7 SANDY BAY ROAD, HOBART, AND 2A HEATHFIELD AVENUE, HOBART
<b>Proposal:</b>	Partial Demolition, Alterations, Extension, and Partial Change of Use to Visitor Accommodation and Hotel Industry
<b>Applicant:</b>	Richard Crawford 293 Macquarie Street, Hobart, 7000, Tas, Australia
<b>Advertisement:</b>	The application was advertised between 13 May 2025 and 27 May 2025.
<b>Representations:</b>	A total of twenty-nine (29) representations were received during the statutory advertising period.
<b>Zone:</b>	15.0 Urban Mixed Use
<b>Performance criteria:</b>	15.2 Use 15.3.1 Hours of operation 15.4.1 Building Height 15.4.2 Setback 15.4.8 Residential Amenity E6.6.1 Number of Car Parking Spaces E13.7.1 Demolition of a Heritage Place E13.7.2 Buildings and Works other than Demolition of a Heritage Place E13.10.1 Building, Works and Demolition in a Place of Archaeological Potential
<b>Recommendations:</b>	Pursuant to the <i>Hobart Interim Planning Scheme 2015</i> , the application should be approved for the reasons outlined in this report and a permit containing the conditions in Attachment A.
<b>Attachments:</b>	Attachment A: Conditions Attachment B: Urban Design Advisory Panel Report Attachment C: Application Referral Cultural Heritage Response by Gray Planning Obo Senior Cultural Heritage Officer

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Attachment C: TasWater Submission to Planning Authority

Attachment E: Development Engineering Assessment Report

Attachment F: Notice of Heritage Decision

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## REPORT

### 1. Executive Summary

- 1.1. Planning approval is sought for the Partial Demolition, Alterations, Extension, and Partial Change of Use to Visitor Accommodation and Hotel Industry at 1 SANDY BAY ROAD, HOBART, 3 SANDY BAY ROAD, HOBART, 5-7 SANDY BAY ROAD, HOBART, AND 2A HEATHFIELD AVENUE, HOBART.
- 1.2. More specifically, the proposal includes:
  - Proposed works on the Masonic Temple, which is permanently listed under the Tasmanian Heritage Register and listed as a Heritage Place per the Planning's Schemes Historic Heritage Codes;
  - Structural works and minor excavation for proposed structural columns that would encroach onto the adjoining properties at 1 Sandy Bay Road, Hobart, 5-7 Sandy Bay Road, Hobart, and 2A Heathfield Avenue, Hobart at ground level;
  - Partial demolition and layout changes to the Masonic Temple by converting the existing kitchen / servery, general storage areas and regalia rooms for the proposed hotel lift lobby and reception areas, north-western fire staircase and south-eastern fire staircase;
  - Partial demolition, structural works and alterations of the existing roof framing and building walls of the Masonic Temple for the proposed structural columns and trusses;
  - Removal of the existing roof structures of the Masonic Temple for the proposed fire staircases;
  - An additional seven-storey hotel accommodation, containing a total of 36 hotel units, immediately atop the two-storey Masonic Temple building, that comprise of 29 x one-bedroom hotel units, 5 x two-bedroom hotel units and 2 x three-bedroom hotel units with a rooftop bar, and includes storage areas, and plant and equipment zone;
  - Pedestrian access to the proposed hotel would be via an existing access ramp on the north-western side of the Masonic Temple;
  - Concept off-site guest pick-up / drop off area, measured at 8m long x 2.5m wide, adjacent 5-7 Sandy Bay Road on Wilmot Street;
  - External bin storage area at the north-eastern side of the Masonic Temple;
  - A maximum proposed floor area of 4,286.5m<sup>2</sup>;
  - A maximum building height of approximately 34.31m above the existing ground level; and
  - No vehicular access to the development or on-site car parking proposed.
- 1.3. The proposal relies on performance criteria to satisfy the following standards and codes:

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- 1.3.1. 15.2 Use
- 1.3.2. 15.3.1 Hours of operation
- 1.3.3. 15.4.1 Building Height
- 1.3.4. 15.4.2 Setback
- 1.3.5. 15.4.8 Residential Amenity
- 1.3.6. E6.6.1 Number of Car Parking Spaces
- 1.3.7. E13.7.1 Demolition of a Heritage Place
- 1.3.8. E13.7.2 Buildings and Works other than Demolition of a Heritage Place
- 1.3.9. E13.10.1 Building, Works and Demolition in a Place of Archaeological Potential

- 1.4. A total of twenty-nine (29) representations was received within the statutory advertising period between 13 May 2025 and 27 May 2025. It is noted that only one of the representations was in support of the proposed development.
- 1.5. The proposal is recommended for approval subject to conditions contained in Attachment A.
- 1.6. The final decision is delegated to the Planning Committee, because the recommendation is to approve and because the proposal constitutes a Major Planning Application.

## 2. Site Detail

- 2.1. The subject site is Lot 1 of Title 71169, or is commonly known as 3 Sandy Bay Road, Hobart, and is a rectangular allotment with an area of 675m<sup>2</sup>.
- 2.2. The site has a primary frontage, along the north-eastern boundary, and adjoins the following properties:
  - Mantra One Hotel at 1 Sandy Bay Road (northwest adjoining)
  - Two-storey dwelling at 2A Heathfield Avenue (southwest adjoining)
  - Conservatory building at 5-7 Sandy Bay Road (southeast adjoining)
- 2.3. Diagonally behind the site is the Telstra Exchange building at 2 Heathfield Avenue.
- 2.4. To the northeast of the site, on the opposite side of Sandy Bay Road, is St Davids Park and approximately 65m southeast of the site, along Sandy Bay Road, is Wilmot Street. It is noted that Wilmot Street is a one-way street with access via Hampden Road.
- 2.5. Forward of the site is one-hour timed parking on weekdays and no stopping at specified hours on the weekdays and Saturday. There is generally no stopping or parking zones at Sandy Bay Road within the immediate vicinity of the site. Longer timed parking is located on the opposite side of Sandy Bay Road.
- 2.6. The site and adjoining properties are within the Urban Mixed Use Zone of the Hobart Interim Planning Scheme 2015. St Davids Park is within the Open Space Zone and is

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a potentially contaminated site. To the northwest of the Site, opposite Davey Street is Central Business Zoning of the City of Hobart.

- 2.7. The site is occupied by the Masonic Temple, which is individually listed in the Tasmanian Heritage Register and as a heritage place in the Planning Scheme's Historic Heritage code. In the wider context of Hobart CBD, the site and adjoining properties are within an area of archaeological potential.
- 2.8. The west portion of the site (rear corner) is within the attenuation buffer of The Duke Hotel, which is a listed live music venue.



Figure 1: Aerial view of site (in orange).

Source: Council GIS Map

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Figure 2: Aerial view of surrounding land zoning – Central Business Zone (dark blue), Open Space Zone (green), Mixed Use Zone (shaded in grey).

Source: Council GIS Map



Figure 3: The west portion of the site is within the attenuation buffer (indicated in purple) of The Duke Hotel, which is listed as a late-night music venue, and therefore requires assessment against the Planning Scheme's Attenuation Code.

Source: Council GIS Map

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Figure 4: Potentially Contaminated Site (indicated in light diagonal-hatched purple) at St Davids Park adjacent the site across Sandy Bay Road.

Source: Council GIS Map

### 3. Proposal

3.1. Planning approval is sought for the Partial Demolition, Alterations, Extension, and Partial Change of Use to Visitor Accommodation and Hotel Industry at 1 SANDY BAY ROAD, HOBART, 3 SANDY BAY ROAD, HOBART, 5-7 SANDY BAY ROAD, HOBART, AND 2A HEATHFIELD AVENUE, HOBART.

3.2. More specifically, the proposal includes:

- Proposed works on the Masonic Temple, which is permanently listed under the Tasmanian Heritage Register and listed as a Heritage Place per the Planning's Schemes Historic Heritage Codes;
- Structural works and minor excavation for proposed structural columns that would encroach onto the adjoining properties at 1 Sandy Bay Road, Hobart, 5-7 Sandy Bay Road, Hobart, and 2A Heathfield Avenue, Hobart at ground level;
- Partial demolition and layout changes to the Masonic Temple by converting the existing kitchen / servery, general storage areas and regalia rooms for the proposed hotel lift lobby and reception areas, north-western fire staircase and south-eastern fire staircase;
- Partial demolition, structural works and alterations of the existing roof framing and building walls of the Masonic Temple for the proposed structural columns and trusses;
- Removal of the existing roof structures of the Masonic Temple for the proposed fire staircases;
- An additional seven-storey hotel accommodation, containing a total of 36 hotel units, immediately atop the two-storey Masonic Temple building, that comprise of 29 x one-bedroom hotel units, 5 x two-bedroom hotel units and 2 x three-

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bedroom hotel units with a rooftop bar, and includes storage areas, and plant and equipment zone;

- Pedestrian access to the proposed hotel would be via an existing access ramp on the north-western side of the Masonic Temple;
- Concept off-site guest pick-up / drop off area, measured at 8m long x 2.5m wide, adjacent 5-7 Sandy Bay Road on Wilmot Street;
- External bin storage area at the north-eastern side of the Masonic Temple;
- A maximum proposed floor area of 4,286.5m<sup>2</sup>;
- A maximum building height of approximately 34.31m above the existing ground level; and
- No vehicular access to the development or on-site car parking proposed.

3.3. The proposal is summarised in the table below:

Level	Proposed
Ground floor (Masonic Temple)	<ul style="list-style-type: none"> <li>• External on-site bin storage area at north-eastern side of Masonic Temple;</li> <li>• Demolition of:               <ul style="list-style-type: none"> <li>○ Internal walls and floors of the existing kitchen / servery and general store areas, and</li> <li>○ Masonry wall of the Masonic Banquet Hall;</li> </ul> </li> <li>• Layout and fitout changes for the proposed north-western fire staircase and hotel lift lobby and reception areas;</li> <li>• External excavation at the rear corners of the building for proposed column / footing structures that encroach onto the adjoining sites;</li> <li>• New stairs from general store to first floor storage room; and</li> <li>• Concept off-site visitor pick-up / drop off area on Wilmot Street.</li> </ul>
First floor (Masonic Temple)	<ul style="list-style-type: none"> <li>• Demolition of:               <ul style="list-style-type: none"> <li>○ Internal and external walls and floors of the existing store and regalia room areas, and</li> <li>○ Masonry wall of the Masonic Banquet Hall;</li> </ul> </li> <li>• Layout and fitout changes for the proposed north-western fire staircase and lift lobby; and</li> <li>• External column zones at the rear corners of the building.</li> </ul>
Existing roof (Masonic Temple)	<ul style="list-style-type: none"> <li>• Removal of existing roof structures above lift / north-western fire staircase and south-eastern fire staircase; and</li> <li>• Modification to the existing roof and framing for structural trusses.</li> </ul>
Level 2 (Hotel accommodation)	<ul style="list-style-type: none"> <li>• North-western fire staircase and lift lobby;</li> <li>• South-eastern fire staircase;</li> <li>• Bathrooms;</li> <li>• Storage room; and</li> <li>• Plant and equipment zone.</li> </ul>
Level 3 and Level 4	<ul style="list-style-type: none"> <li>• North-western fire staircase and lift lobby;</li> <li>• South-eastern fire staircase;</li> </ul>

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(Hotel accommodation)	<ul style="list-style-type: none"> <li>• Hotel accommodation: <ul style="list-style-type: none"> <li>○ 4 x one-bedroom units,</li> <li>○ 1 x two-bedroom unit, and</li> <li>○ 1 x three-bedroom unit.</li> </ul> </li> </ul>
Level 5, Level 6 and Level 7 (Hotel accommodation)	<ul style="list-style-type: none"> <li>• North-western fire staircase and lift lobby;</li> <li>• South-eastern fire staircase;</li> <li>• Hotel accommodation: <ul style="list-style-type: none"> <li>○ 7 x one-bedroom units, and</li> <li>○ 1 x two-bedroom unit.</li> </ul> </li> </ul>
Level 8 (Hotel accommodation)	<ul style="list-style-type: none"> <li>• North-western fire staircase and lift lobby;</li> <li>• South-eastern fire staircase;</li> <li>• Central rooftop bar area;</li> <li>• Northeast and southwest outdoor terraces; and</li> <li>• Toilets.</li> </ul>
Roof above Level 8 (Hotel accommodation)	<ul style="list-style-type: none"> <li>• Clear glazing dome roof structure; and</li> <li>• Metal roofing.</li> </ul>
External materials and finishes (but not limited to)	<ul style="list-style-type: none"> <li>• Zinc metal cladding;</li> <li>• Glass balustrades;</li> <li>• Perforated façade metal screening;</li> <li>• Tinted glazing; and</li> <li>• Existing brickwork, steel roofing with paint finish and metal framed glazing.</li> </ul>

#### 4. Background

- 4.1. Planning Application PLN-930130 was delegated in February 1993 for internal alterations to the exhaust and grease trap of the kitchen of the Masonic Temple.
- 4.2. Council's records (TRIM), indicates that the property on the Site has historically been used for commercial and food premises.
- 4.3. On 28 April 2021, an Urban Design Advisory Panel was held for a pre-Application referral of the proposed development. The panel provided recommendations, but not limited to, improving the design and materiality. The Panel also raised concerns regarding the incompatible building height and loss of transition with the nearby building, the bulk and massing of seven storey addition and townscape views, amenity impacts to the adjoining properties, car parking and the functionality of the rooftop bar with the proposed hotel.
- 4.4. On 20 October 2021, the planning permit application PLN-21-710 (subject application) was received and further information was requested by Council.

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**5. Representation**

5.1. The table below summarises all the concerns raised by representors during the statutory advertising period.

Concerns	Response
<p><b>Encroachment onto adjoining properties and Right of Way</b></p> <ul style="list-style-type: none"> <li>• Construction of columns on neighbouring land without written consent from adjoining property owners.</li> <li>• Use of the Right of Way laneway, on 1 Sandy Bay Road, for construction and operational purposes without agreements.</li> <li>• Potential damage to common property and unauthorized access during construction works.</li> </ul>	<p>The application has validity under s51 of the <i>Land Use Planning and Approvals Act 1993</i> with respect to notification of adjoining land. This concern is not a planning consideration under the <i>Hobart Interim Planning Scheme 2015</i>. This concern has been addressed in Section 8 of this report.</p> <p>Specifically, a referral was made to Council's Surveying Services Manager, who finds the proposal acceptable subject to a condition.</p>
<p><b>Building Height and Scale</b></p> <p>The proposed building is considered significantly taller than surrounding structures. Concerns include:</p> <ul style="list-style-type: none"> <li>• Lack of height transition between the development and adjacent properties.</li> <li>• Incompatibility with the low-rise character of the area.</li> <li>• Dominance over neighbouring buildings, particularly along Sandy Bay Road and Heathfield Avenue.</li> </ul>	<p>This concern has been addressed in Section 7 of this report, specifically in the height assessment against performance criteria P1 at clause 15.4.1.</p>
<p><b>Visual Impact and views</b></p> <ul style="list-style-type: none"> <li>• Does not reflect or enhance the architectural character of the area.</li> <li>• Is visually jarring and out of place, especially when viewed from Davey Street.</li> <li>• Fails to act as a heritage gateway to Battery Point.</li> <li>• The proposed setback detract from the context of the neighbourhood, especially with the approved façade on 5-7 Sandy Bay Road.</li> <li>• The proposed development will block views from the street levels of Heathfield Avenue and Sandy Bay</li> </ul>	<p>This concern has been addressed in Section 7 of this report, specifically in the height assessment against performance criteria P1 at clause 15.4.1.</p> <p>In addition, the application has been reviewed by Council's Urban Design Advisory Panel and by the Gray Planning Senior Cultural Heritage Officer.</p>

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	It is noted that blocking of views is not a planning consideration.
<p><b>Noise Impacts</b></p> <p>Concerns include:</p> <ul style="list-style-type: none"> <li>• Construction noise affecting residents and hotel guests.</li> <li>• Ongoing noise from increased traffic and operations, especially from a proposed rooftop bar.</li> <li>• Late-night rubbish disposal contributing to noise pollution.</li> <li>• Lack of noise mitigation measures and unnegotiated construction hours.</li> </ul>	<p>This concern has been addressed in Section 7 of this report, specifically in the noise assessment against performance criteria P1 at clause 15.3.1.</p> <p>In addition, this concern is discussed in Section 8 of this report.</p> <p>Furthermore, conditions have been included in Attachment A that relates to construction and operational noise.</p>
<p><b>Privacy Impacts and Direct Overlooking</b></p> <p>Some objectors noted:</p> <ul style="list-style-type: none"> <li>• Potential overlooking from upper levels of the development into nearby homes.</li> <li>• Loss of privacy for residents in adjacent buildings.</li> </ul>	<p>This concern has been addressed in Section 7 of this report, specifically in the amenity assessment against performance criteria P2 at clause 15.4.8.</p>
<p><b>Amenity Impacts</b></p> <p>The development will be detrimental to the peaceful environment of the area and result in:</p> <ul style="list-style-type: none"> <li>• Reduced guest experience at Mantra One hotel.</li> <li>• Loss of revenue and goodwill for property owners due to diminished amenity.</li> <li>• Increased dust, litter, and potential for antisocial behaviour.</li> </ul>	<p>This concern is not a planning consideration under the <i>Hobart Interim Planning Scheme 2015</i>. This concern is discussed in Section 8 of this report.</p>
<p><b>Overshadowing and Loss of Sunlight</b></p> <p>The increased height and bulk of the building will:</p> <ul style="list-style-type: none"> <li>• Block views and reduce natural light in northeast and east-facing apartments at 1 Sandy Bay Road.</li> <li>• Significantly affect the amenity of the adjoining apartments.</li> <li>• Overshadow the residential dwellings along Heathfield Avenue.</li> </ul>	<p>In the site context this concern is not a planning consideration under the Urban Mixed Use Zone development standards of the <i>Hobart Interim Planning Scheme 2015</i>.</p> <p>This concern is discussed in Section 8 of this report.</p> <p>It is noted that blocking of views is not a planning consideration.</p>

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<p><b>Traffic and Access Issues</b>                  The proposal will result in:</p> <ul style="list-style-type: none"> <li>• Increased congestion on Sandy Bay Road, Davey Street, Wilmot Street, and Heathfield Avenue.</li> <li>• Inadequate loading zones leading to delivery vehicles using the narrow laneway.</li> <li>• Safety risks due to increased traffic in a constrained area.</li> </ul>	<p>This concern has been addressed in Section 7 of this report, specifically in the parking assessment against performance criteria P1 at clause E6.6.1.</p> <p>In addition, the application has been reviewed by Council's Development Engineering and Traffic Engineering Officers.</p> <p>Furthermore, conditions have been included in Attachment A that relates to a guest communication plan regarding to parking arrangements and times.</p>
<p><b>Parking Issues</b>                  The proposal will have:</p> <ul style="list-style-type: none"> <li>• No provision for on-site parking in the development.</li> <li>• Increased demand on already limited parking for Mantra Hotel guests.</li> <li>• Spillover parking into surrounding residential streets.</li> </ul>	<p>This concern has been addressed in Section 7 of this report, specifically in the parking assessment against performance criteria P1 at clause E6.6.1.</p> <p>In addition, the application has been reviewed by Council's Development Engineering and Traffic Engineering Officers.</p> <p>Furthermore, conditions have been included in Attachment A that relates to a guest communication plan regarding to parking arrangements and times.</p>
<p><b>Waste Management</b>                  Concerns include:</p> <ul style="list-style-type: none"> <li>• Inadequate rubbish storage and collection zones.</li> <li>• Potential encroachment of waste facilities onto footpaths and the laneway.</li> <li>• Noise from late-night waste disposal.</li> </ul>	<p>This concern has been addressed in Section 7 of this report.</p> <p>Furthermore, conditions have been included in</p>

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	Attachment A that relates to waste management.
<p><b>Heritage Impacts</b></p> <p>The development is seen as:</p> <ul style="list-style-type: none"> <li>• Inconsistent with the heritage character of the area.</li> <li>• Detrimental to the historic streetscape and nearby buildings.</li> <li>• Is not in keeping with the architectural design of the Masonic Temple.</li> </ul>	<p>This concern has been addressed in Section 7 of this report.</p> <p>In addition, the application has been reviewed by Gray Planning Senior Cultural Heritage Officer. The application is found to be supportable subject to conditions.</p>
<p><b>Streetscape Compatibility</b></p> <p>The proposal:</p> <ul style="list-style-type: none"> <li>• Lacks harmony with the existing streetscape.</li> <li>• Introduces an abrupt and incompatible scale and design.</li> <li>• Disrupts the visual and architectural continuity of the area.</li> </ul>	<p>This concern has been addressed in Section 7 of this report.</p>
<p><b>Inconsistencies</b></p> <p>Some representations noted:</p> <ul style="list-style-type: none"> <li>• Incomplete or missing information in the application (e.g., owner notification).</li> <li>• Lack of clarity around construction impacts and mitigation strategies.</li> </ul>	<p>This concern is not a planning consideration under the <i>Hobart Interim Planning Scheme 2015</i>. This concern is discussed in Section 8 of this report.</p>

## 6. Assessment

- 6.1. The *Hobart Interim Planning Scheme 2015* is a performance based planning scheme. To meet an applicable standard, a proposal must demonstrate compliance with either an acceptable solution or a performance criterion. Where a proposal complies with a standard by relying on one or more performance criteria, the Council may approve or refuse the proposal on that basis. The ability to approve or refuse the proposal relates only to the performance criteria.
- 6.2. This site is located within the 15.0 Urban Mixed Use of the *Hobart Interim Planning Scheme 2015*.
- 6.3. The existing use is Community Meeting and Entertainment and is Permitted in the zone. The proposed partial change of use is Visitor Accommodation and is Permitted in the zone, and Hotel Industry, which is Discretionary in the zone.
- 6.4. The proposal has been assessed against:
- 6.4.1. 15.0 Urban Mixed Use

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- 6.4.2. E5.0 Road and Railway Assets Code,
  - 6.4.3. E6.0 Parking and Assets Code,
  - 6.4.4. E9.0 Attenuation Code,
  - 6.4.5. E7.0 Stormwater Management Code,
  - 6.4.6. E13.0 Historic Heritage Code
- 6.5. The proposal relies on the following criteria to comply with the acceptable standards:
- 6.5.1. Urban Mixed Use Zone Use
    - 15.2 – Use
  - 6.5.2. Urban Mixed Used Zone Use Standards
    - 15.3.1 Hours of operation – P1
  - 6.5.3. Urban Mixed Use Zone Development Standards
    - 15.4.1 Building Height – P1
    - 15.4.2 Setback – P1
    - 15.4.8 Residential Amenity – P2
  - 6.5.4. Parking and Access Code Use Standards
    - E6.6.1 Number of Car Parking Spaces – P1
  - 6.5.5. Historic Heritage Code Development Standards for Heritage Places
    - E13.7.1 Demolition of a Heritage Place – P1
    - E13.7.2 Buildings and Works other than Demolition of a Heritage Place – P1
  - 6.5.6. Historic Heritage Code Development Standards for Places of Archaeological Potential
    - E13.10.1 Building, Works and Demolition in a Place of Archaeological Potential – P1, P2 and P3
- 6.6. Each of the performance criteria is assessed in Section 7 of this report.

## 7. Performance Criteria

- 7.1. 15.2 Use

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- 7.2. The proposed bar is classed as hotel industry and is a discretionary use in the zone.
- 7.3. There is no acceptable solution with regard to use, rather clause 8.10.1 and 8.10.2 ('determining applications') states that when determining an application for a discretionary use, the Council must consider the following (in-so-far as each is relevant to the discretion being exercised):
- All applicable standards and codes;
  - Any representations received;
  - The purpose of the applicable zone;
  - Any relevant local area objective or desired future character statement for the applicable zone;
  - The purpose of any applicable code;
  - The purpose of any applicable specific area plan.
- 7.4. The zone purpose statement for the Urban Mixed Use says that the zone should:
- Provide for integration of residential, retail, community services and commercial activities in urban locations;
  - Encourage use and development at street level that generates activity and pedestrian movement through the area;
  - Provide for design that maximises the amenity at street level including considerations of microclimate, lighting, safety, and pedestrian connectivity;
  - Ensure that commercial use are consistent with the activity centre hierarchy;
  - Ensure development is accessible by public transport, walking and cycling;
  - Provide for a diversity of uses at densities responsive to the character of streetscapes, historic areas and buildings and which do not compromise the amenity of surrounding residential areas;
  - Encourage the retention of existing residential uses and the greater use of underutilised sites as well as the reuse and adaption of existing buildings for uses with a scale appropriate to the site and area;
  - Ensure that the proportions, materials, openings and decoration of building facades contribute positively to the streetscape and reinforce the built environment of the area in which the site is situated;
  - Maintain an appropriate level of amenity for residential uses without unreasonable restriction or constraint on the nature and hours of commercial activities; and
  - Ensure that retail shopping strips do not develop along major arterial roads within the zone.
- 7.4.2. There are no local area objectives or desired future character statements in the zone.

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- 7.4.3. The proposal is for a bar to be located on the rooftop of the proposed extension and development. The use will represent an integration of commercial activities within urban locations. Whilst the use will not have any direct street frontage, the larger visitor accommodation use which is also proposed will contribute towards the further improvement of the site at the street level.
- 7.4.4. The proposed hotel industry use will remain consistent with the activity centre hierarchy and will not be at a size or scale that would represent detrimental impacts with any surrounding commercial uses. The proposal will further contribute towards a diversity of uses in the area. Details as to its operation and compliance with noise standards have demonstrated that it will not compromise the amenity of surrounding residential areas.
- 7.4.5. As part of the wider proposed development the use will be accessible by public transport, walking and cycling given its central location.
- 7.4.6. The proposed use will not displace any residential uses on the site and represents a reinvigoration of an underutilised site with the opportunity for the adaptation of an existing building.
- 7.4.7. Representations were received objecting to the proposed hotel industry use with respect to noise and activity from its operation. As per the below zone use standard assessment, this use will not represent an unreasonable detriment.

7.5. 15.3 Use Standards

7.6. 15.3.1 Non-Residential Use

- 7.6.1. The acceptable solution at clause 15.3.1 A1 requires the hours of operation to be within:
- 7.00 am to 9.00 pm Mondays to Fridays inclusive;
  - 8.00 am to 6.00 pm Saturdays; and
  - 9.00 am to 5.00 pm Sundays and Public Holidays;
- 7.6.2. The proposed rooftop bar will operate between 6.00am to 12.00am seven days per week.
- 7.6.3. As such, the assessment relies upon assessment against performance criterion P1 at clause 15.3.1, which reads:

**P1**

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*Hours of operation must not have an unreasonable impact upon the residential amenity through commercial vehicle movements, noise or other emissions that are unreasonable in their timing, duration or extent.*

7.6.4. The rooftop bay has been designed with an enclosed central area 'rooftop bar area' and outdoor patios northeast and southwest of the central indoor area. Specifically, music speakers are proposed within the internal area for background ambience.

7.6.5. The submitted noise report titled 'Masonic Temple – DA Noise Assessment, dated 20 January 2021, reference: Doc 6295 and prepared by Noise Vibration Consulting, concludes that:

*Noise emissions from patrons and background music within the building are predicted to be below 20 dBA at all nearby residences. Noise emissions from patrons on the external patio areas are predicted to be between 28 and 38 dBA at the boundaries of site nearest the neighbouring buildings. These levels are below both the day and night time criteria and measured, and therefore comply with the Acceptable Solutions criteria under clause 15.3.1-A2 of the Scheme.*

7.6.6. Representations were made in relation to ongoing operational and traffic noise impacts.

7.6.7. It is considered that the noise emissions, as measured at the boundaries of the site, comply with the acceptable solution A2 at clause 15.3.1, and that the operations of the rooftop bar will not have unreasonable impact upon the residential amenity of the adjoining properties.

7.6.8. A condition will be included on the permit to ensure compliance with clause 15.3.1 (A2). The representation grounds are supported by this condition.

7.6.9. For the reasons as outlined above, it is considered the submissions made by the representors have been adequately addressed.

7.6.10. The proposal complies with the performance criteria, subject to conditions.

7.7. 15.4 Development Standards for Buildings and Works

7.8. 15.4.1 P1 - Building Height

7.8.1. The acceptable solution 15.4.1 A1 allows for a maximum building height of 10m.

7.8.2. The proposed seven storey addition results in a maximum building height of 34.3m, as measured from the ground level.

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- 7.8.3. As such, the assessment relies upon assessment against performance criterion P1 at clause 15.4.1, which reads:

**P1**

*Building height must satisfy all of the following:*

- (a) be consistent with any Desired Future Character Statements provided for the area;*
- (b) be compatible with the scale of nearby buildings;*
- (c) not unreasonably overshadow adjacent public space;*
- (d) allow for a transition in height between adjoining buildings, where appropriate;*

- 7.8.4. Subclause (a) is not applicable as the Planning Scheme does not nominate Desired Future Character Statements for the area.

- 7.8.5. With regards to subclause (b), the term “compatible” is not defined in the planning scheme. However, recent Tribunal decisions of *9 Sandy Bay Road Pty Ltd v Hobart City Council & Ors* [2017] TASRMPAT 19 – Appeal 100/16P define “compatibility” as follows:

*...To be compatible is to be consistent and congruous with that which comparison is required to be made. The Tribunal holds that to be “compatible” requires that the building height be capable of co-existing with the scale of nearby buildings.*

Furthermore, the Tribunal defined the term ‘compatible’ in two decisions: *Henry Design & Consulting v Clarence City Council* and *Flood v George Town Council*. In *Henry Design*, the Tribunal held at [50] that ‘compatible’ meant “not necessarily the same... but at least similar to, or in harmony or broad correspondence with the surrounding area”.

Additionally, the objective of the performance criterion reads as follows:

*To ensure that building height contributes positively to the streetscape and does not result in unreasonable impact on residential amenity of land in the General Residential Zone or Inner Residential Zone*

According to the planning scheme, “Streetscape”:

*means the visual quality of a street depicted by road width, street planting, characteristics and features, public utilities constructed within the road reserve, the setbacks of buildings and structures from the lot boundaries, the quality, scale, bulk and design of buildings and structures fronting the road reserve.*

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*For the purposes of determining streetscape with respect to a particular site, the above factors are relevant if within 100m of the site.*

- 7.8.6. In consideration of the above, the scale of the proposed development must demonstrate that the proposed heights contributes positively to the streetscape of the immediate locality and be in harmony with the scale of nearby buildings. The relevant finished levels of the proposed development and adjoining properties are as follows:

Development	Finished level
Proposed development	51.670 RL
Mantra One Hotel	45.710 RL
Telstra Exchange building	52.770 RL
Telstra Communication Tower (to be demolished per PLN-HOB-2024-0604)	88.757 RL
Approved alterations to conservatorium building at 5-7 Sand Bay Road (PLN-HOB-2024-0604)	39.938 RL

The proposed development would be approximately 6m taller than the Mantra One Hotel and 11.7m taller than the conservatorium building (once construction is completed) and will be 1.1m lower than the Telstra Exchange building. From a numerical point of view, the proposal represents a relative height transition between the existing and emerging developments adjoining the site.



Figure 5: Street view southwest of site at Sandy Bay Road  
 Source: Site visit photos

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Figure 6: Street view northeast of site at Davey Street  
Source: Site visit photos



Figure 7: Street view northwest of Site at Davey Street  
Source: Site visit photos

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Figure 8: Street view west of site at Heathfield Avenue  
Source: Site visit photos



Figure 9: Perspectives of proposal  
Source: Architectural Plans Revision PA1

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Figure 10: Perspectives of proposal

Source: Architectural Plans Revision PA1



Figure 11: Perspectives of proposal

Source: Architectural Plans Revision PA1

As shown in the figures above, the proposal, when viewed from the nearby streetscape, is wholly or partially obscured by the existing buildings. Specifically, the proposed height and setback of the addition allows the proposal to sit comfortably between the existing developments. While the proposal is slightly higher than the adjoining properties along Sandy Bay Road, the bulk of the proposal is adequately setback to the rear – in that the proposal is partially obscured. In addition, the design and architectural features of the addition respects the historic values of the Masonic Temple

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and would remain as an underwhelming presence relative to the Masonic Temple.

The proposed building would be most visible along Heathfield Avenue. While the development adjoins two-storey residential dwellings at the rear of the site the proposal would not be inconsistent with the established pattern of the development in the area, specifically with the adjoining Telstra Exchange Tower and Mantra One Hotel when viewed from various vantage points southwest of the site. It is noted that topography of the site, from Sandy Bay Road (location of the site) slopes to a higher elevation at Heathfield Avenue and beyond. The proposal is not considered to dominate any views of the central business area or would it block views to areas of significance. The proposed building height is acceptable in this instance.

For the reasons outlined above, the proposal is considered to be in harmony or broad correspondence with the height of buildings adjoining the site. Properties within 100m of the site, as stated in the definition of Streetscape of the scheme, are of diverse building heights and footprints. The proposal sits comfortably within the immediate context and does not negatively distinguish itself from the existing buildings in relation to bulk and scale or building height. It is considered that the proposal, in the broader context, can co-exist with the built form of the immediate locality.

While the Urban Mixed Use Zone allows for residential use and development as "permitted" use and development, as the title of this zone suggests, it is a mixed-use zone. Another recent Tribunal decision (*Clegg & Ors v Clarence City Council* [2018] TASRMPAT 21) found that a mixed use zone is not considered to be a residential zone. In addition, the site does not directly adjoin any residential zones. As such, the proposal is not inconsistent with the objective of the clause as the proposal cannot result in unreasonable impacts on residential amenity of any adjoining residential land.

- 7.8.7. Due to the orientation of the site and St David's Park, it is not considered that the proposed development will result in any overshadowing and therefore satisfies subclause (c).
- 7.8.8. Representations were made in relation to building height and scale, specifically with height transition, incompatibility with low-rise buildings and dominance over neighbouring buildings.
- 7.8.9. When compared to the adjoining properties along Sandy Bay Road, the Masonic Temple represents a gap in the existing and emerging building transition of the immediate locality. The Masonic Temple is significantly shorter than the adjoining Mantra One Hotel and the existing conservatorium building.
- 7.8.10. The proposed seven-storeys has been designed with an appropriate proposed setback, that would retain the heritage value of the Masonic Hall while providing an appropriate height transition along the streetscape of Sandy Bay Road. As such, the proposal provides adequate height transition, is compatible with the existing buildings within the immediate locality, and would not dominate the adjoining properties.

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7.8.11. For the reasons as outlined above, it is considered the submissions made by the representors have been adequately addressed.

7.8.12. The proposal complies with the performance criterion

7.9. 15.4.2 P1 - Setback

7.9.1. The acceptable solution A1 at clause 15.4.2 requires a building setback from frontage must be parallel to the frontage and must be no more than 1m from the median street setback of all existing buildings on the same side of the street within 100m of the site.

7.9.2. The following setbacks of the properties located within 100m of the site on Sandy Bay Road are as follows:

Properties	Setbacks
166-170 Macquarie Street, Hobart.	0m
58 Harrington Street, Hobart	0m
1 Sandy Bay Road, Hobart.	2m
5-7 Sandy Bay Road, Hobart.	0m
9 Sandy Bay Road, Hobart.	3.1m
12 Wilmot Street, Hobart.	3.9m
<b>Median:</b>	1m

7.9.3. The proposed extension and works are setback approximately 7m from the frontage.

7.9.4. As such, the proposed development relies on assessment against performance criteria P1 at clause 15.4.2, which reads:

**P1**

*Building setback from frontage must satisfy all of the following:*

- (a) *be consistent with any Desired Future Character Statements provided for the area;*
- (b) *be compatible with the setback of adjoining buildings, generally maintaining a continuous building line if evident in the streetscape;*
- (c) *enhance the characteristics of the site, adjoining lots and the streetscape;*
- (d) *provide for small variations in building alignment only where appropriate to break up long building facades, provided that no potential concealment or entrapment opportunity is created;*
- (e) *provide for large variations in building alignment only where appropriate to provide for a forecourt for space for public use, such as outdoor dining or landscaping, provided the that no potential*

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*concealment or entrapment opportunity is created and the forecourt is afforded very good passive surveillance.*

7.9.5. The objective of the clause reads as follows:

*To ensure that building setback contributes positively to the streetscape and does not result in unreasonable impact on residential amenity of land in a residential zone.*

7.9.6. The site does not adjoin any residential zones. As such, the proposal does not result in unreasonable impact on residential amenity on any residential land. Nevertheless, the site adjoins two-storey dwellings to the rear. The proposal should demonstrate unreasonable impacts to residential amenity.

7.9.7. Subclause (a) is not applicable as the Planning Scheme does not nominate Desired Future Character Statements for the area.

7.9.8. The proposed setback of the above extension has been designed to retain the heritage value and streetscape amenity of the Masonic Temple. It is considered that a continuous building line of the existing streetscape would be retained due to the prominence of the Masonic Temple, as such satisfying subclauses (b) and (c).

7.9.9. The large variation in building alignment is appropriate to allow for the streetscape prominence and heritage value of the Masonic Temple to be retained. It is not considered that the proposed works along the northwestern entrance would result in an area of concealment with poor surveillance, as such satisfying subclauses (d) and (e).

7.9.10. For the reasons outlined above, the proposal would not result in unreasonable passive surveillance impacts to the residential amenity of the adjoining properties. As such, the proposal is consistent with the objective of the clause.

7.9.11. Representations were made in relation to visual Impact and views due to the proposed setbacks.

7.9.12. It has been demonstrated proposed setback would not be visually jarring when viewed from the streetscape and surrounding area or would it detract from the immediate neighbourhood.

7.9.13. For the reasons as outlined above, it is considered the submissions made by the representors have been adequately addressed.

7.9.14. The proposal complies with the performance criteria.

7.10. 15.4.8 P2 - Residential Amenity

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- 7.10.1. The acceptable solution 15.4.8 A2 requires that the potential for direct overlooking from windows of habitable rooms with a finished surface or floor level more than 1m above natural ground level on one lot to the windows of habitable rooms, balconies, decks and roof gardens on adjacent lots must be avoided or minimised by:
- have a side boundary setback no less than 3 m;
  - be offset no less than 1.5 m from the windows of habitable rooms on adjacent lots where on the same horizontal lane;
  - have a window seal height no less than 1.5 m
- 7.10.2. The northwestern façade of the proposed hotel accommodation addition includes windows overlooking the Mantra One Hotel. As indicated on the architectural plans, the bedroom windows are fixed with metal mesh screening and include an unscreened opening on each of the hotel accommodation apartments on levels 4 to 7. Specifically, the unscreened opening on Level 7 achieves a seal height of approximately 14.4m above the finished floor level. The remaining openings on the lower levels are greater than 1.5m above the finished floor levels. The proposed windows are setback less than 3m from the side boundary.
- 7.10.3. The southwestern bedroom windows incorporate metal mesh screening and floor to ceiling clear windows that are setback less than 3m to the rear boundary.
- 7.10.4. As such, the proposal relies on assessment against P2 at clause 15.4.8, which reads:

**P2**

*The potential for direct overlooking from windows of habitable rooms with a finished surface or floor level more than 1m above natural ground level on one lot to the windows of habitable rooms, balconies, decks and roof gardens on adjacent lots must be avoided or minimised through their separation and off-set or by use of solid or translucent screening.*

- 7.10.5. Adjoining the northwest of the site is the Mantra One Hotel at 1 Sandy Bay Road, and to the rear of the site at 2A Heathfield Avenue and 5-7 Sandy Bay Road are two-storey residential dwellings, and the public domain of Heathfield Avenue. Opposite Heathfield Avenue are two-storey residential dwellings.
- 7.10.6. The unscreened bedroom windows on levels 6 and 7 do not directly overlook into any adjoining habitable room windows as they are situated well above the roof line of the Mantra One Hotel. As such, it is not required that the unscreened window on level 7 requires additional screening as there are no overlooking impacts, and that the 1.4m seal height is acceptable in this instance.
- 7.10.7. Furthermore, the unscreened windows on levels 3 to 5 are horizontally and vertically offset and do not directly overlook into the habitable windows of

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apartments 307, 308, 407, 408, 507 and 508 of the Mantra One Hotel. Specifically, the proposed windows are such that they do not align with the existing floor levels and window seal heights of the Mantra One Hotel and would only have direct views onto the external brick façade. It is not considered that there would be unreasonable overlooking into the habitable area windows of the adjoining northeastern property.

- 7.10.8. As seen in the figures below, to the rear of the site are adjoining two-storey dwellings at 2A Heathfield Avenue and 5-7 Sandy Bay Road. The proposed rear windows of the addition are located directly above the Masonic Temple and are situated well above the roof line of the adjoining two-storey dwellings. It is not considered that there will be direct overlooking into the habitable windows, nor would there be any privacy impacts onto to the rear car parking areas of the adjoining residential properties.
- 7.10.9. Furthermore, the public domain of Heathfield Avenue provides adequate separation where there will be no direct overlooking into the habitable and private open space areas of the properties opposite Heathfield Avenue.



Figure 12: Street view of adjoining rear two-storey dwellings.

Source: Site visit photos

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Figure 13: Street view of adjoining rear two-storey dwellings with view into the rear of the Masonic Temple.

Source: Site visit photos



Figure 14: Driveway and covered car parking spaces of the adjoining rear properties. Existing Masonic Hall (red brick).

Source: Realestate.com

7.10.10. In consideration of the approved development on 5-7 Sandy Bay Road, the proposed southeastern windows are fully screened and would not result in any unreasonable overlooking onto the habitable rooms, balconies and communal roof terrace of the adjoining property. It is noted that there are no approved windows or balconies on the northwestern façade of the adjoining conservatorium.

7.10.11. Representations were made in relation to privacy impacts onto the adjoining properties.

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- 7.10.12. It is considered that the proposal has been designed with adequate separation and screening that would not result in unreasonable overlooking impacts onto the adjoining properties.
- 7.10.13. For the reasons as outlined above, it is considered the submissions made by the representors have been adequately addressed.
- 7.10.14. The proposal complies with the performance criteria.

7.11. E6.6 Use Standards

7.12. E6.6.1 P1 - Number of Car Parking Spaces

- 7.12.1. The acceptable solution A1 of clause E6.6.1 requires the number of on-site car parking spaces must be no less and no greater than the number specified in Table E6.1. The total required on-site car parking spaces are calculated to be:

*Visitor accommodation – 1 car parking space for each bedroom; or alternatively*

*Serviced apartment – 1 car parking space for each serviced apartment unit; and*

*Hotel industry – 1 car parking space for each 3 m<sup>2</sup> of public bar room floor area.*

*Accordingly, the proposal has a statutory requirement to provide:*

*Visitor accommodation – 33 spaces*

*Bar – 46 spaces*

*Total required parking spaces: 79*

- 7.12.2. The proposal does not provide any on-site car parking arrangements.
- 7.12.3. As such, the proposal relies on assessment against P1 at clause E6.6.1, which reads:

**P1**

*The number of on-site car parking spaces must be sufficient to meet the reasonable needs of users, having regard to all of the following:*

- (a) car parking demand;*
- (b) the availability of on-street and public car parking in the locality;*
- (c) the availability and frequency of public transport within a 400m walking distance of the site;*
- (d) the availability and likely use of other modes of transport;*

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- (e) *the availability and suitability of alternative arrangements for car parking provision;*
- (f) *any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces;*
- (g) *any car parking deficiency or surplus associated with the existing use of the land;*
- (h) *any credit which should be allowed for a car parking demand deemed to have been provided in association with a use which existed before the change of parking requirement, except in the case of substantial redevelopment of a site;*
- (i) *the appropriateness of a financial contribution in lieu of parking towards the cost of parking facilities or other transport facilities, where such facilities exist or are planned in the vicinity;*
- (j) *any verified prior payment of a financial contribution in lieu of parking for the land;*
- (k) *any relevant parking plan for the area adopted by Council;*
- (l) *the impact on the historic cultural heritage significance of the site If subject to the Local Heritage Code;*
- (m) *whether the provision of the parking would result in the loss, directly or indirectly, of one or more significant trees listed in the Significant Trees Code.*

7.12.4. The application was referred and found to be supportable by Council's Development Engineer.

7.12.5. The following comments has been provided from Attachment E:

*The Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1 states the below in relation (a) in principal councils development engineering officer accepts the statement provided by the proponents traffic expert to meet P1(a).*

As such, the proposal satisfies subclause (a).

7.12.6. The following comments has been provided from Attachment E:

*There is a relatively large supply of on-street parking in the surrounding road network. Much of the available parking is in the form of time-restricted parking, with authorised residents excepted. Observations indicate that there is a large pool of parking that would be available to meet the potential demands of visitor and overflow parking, particularly after normal working hours.*

*The Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1 states the below in relation (b) in principal councils development engineering officer accepts the statement provided by the proponents traffic expert to meet P1(b).*

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As such, the proposal satisfies subclause (b).

7.12.7. The following comments has been provided from Attachment E:

*Metro Tasmania operate regular bus services along sandy pay road and Davey street which is within 400 metres of the subject site.*

*The Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1 states the below in relation (c) in principal councils development engineering officer accepts the statement provided by the proponents traffic expert to meet P1(c).*

As such, the proposal satisfies subclause (c).

7.12.8. The site is located a convenient walking distance from shops, and services. As such, the proposal satisfies subclause (d).

7.12.9. Subclause (e) is not applicable as no alternative parking provision is available or considered necessary.

7.12.10. Subclauses (f), (g), (h), (i) and (k) are not applicable.

7.12.11. Subclause (j) is not applicable as the City's current position is not to support a financial contribution in lieu of parking for developments.

7.12.12. Subclause (l) is not applicable the site is subject to the Local Heritage Code, but no on-site car park is proposed, meaning heritage significance is not impacted.

7.12.13. Subclause (m) is not applicable as there are no impacts to any significant trees.

7.12.14. The application was referred and found to be supportable by Council's Traffic Engineering Officer. The following comments has been provided from Attachment E:

*Under the current planning scheme, the proposed short-term accommodation requires 33 parking spaces and the rooftop bar requires 46 parking spaces. It is important to note that the requirement of 46 parking spaces for the rooftop bar appears excessive given the nature of the development and its proximity to the Central Business Zone. Despite the apparent lack of on-site car parking, this is not identified as the primary concern for this development due to the proximity of the development to the Central Business zone (approximately 60m from the zone).*

*The assessment provided by SALT – Traffic Impact Assessment – demonstrates capacity in the availability of the on-street parking network to cater for the traffic generated by the development and for the pick up and drop*

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*off activity for the hotel development. Based on the above summary, there is no grounds to refuse the development applications, however, there are a number of concerns traffic engineering officers would like to raise in respect to this development.*

*During Salamanca Market days on Saturdays the parking demand in the Battery Point area is significantly high and this may make it difficult for guests to park and check in at those particular days.*

*Officers have raised concerns around compliance with the clearway zone on Sandy Bay Road fronting the proposed development which may present a significant potential issue when the short-term parking on Wilmot Street is utilised or drivers have turned into Sandy Bay Road to access the hotel and find that they are not able to turn right into Wilmot Street.*

*To mitigate conflicts arising from uninformed guests it is suggested that a communication strategy is developed that include the following:*

- (1) specific conditions regarding the communication of clearway zone times by the hotel should be stipulated. The clearway operates from 6:30 am to 9:30 am and 1:00 pm to 6:00 pm on weekdays, and from 9:00 am to 3:00 pm on Saturdays and compliance with the clearway is essential for the AM and PM traffic flow for Hobart's traffic network.*
- (2) Communicate to guests that there is no parking available off-street and provide information on parking available that is suitable for guests (i.e. nearby multi-storey car parks).*
- (3) Provide information on the route to access the short-term parking on Wilmot Street.*

*Furthermore, the existing loading zone times remain unaltered to ensure that the clearway is in operation during the peak period of traffic during the weekdays. A such, deliveries and rubbish collection for the development must be scheduled to occur between 9:30 am and 1:00 pm, Monday to Friday, aligning with the existing loading zone operational times.*

*Finally, the on-street parking is a public asset and therefore the proposed dedicated pick-up and drop-off zone cannot be exclusively for the hotels use; it must remain available for general public use and it will be sign posted as a 10 minute parking zone. With the development of 5-7 Sandy Bay Rd, it is possible to assume that the new short-term parking will be well highly utilised, especially for delivery drivers who want to pick up and drop off outside the period when the loading zone in Sandy Bay Road is in operation (during the clearway times).*

7.12.15. Further comments have been provided by Council's Development Engineering Officer:

*Based on the documentation submitted to date and given the above assessment, the parking provision is accepted as meeting the Performance*

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*Criteria P1:E6.6.1 of the Planning Scheme. This is particularly due to the actual parking demands that will be generated by the development.*

- 7.12.16. Representations were made in relation to lack of on-site parking, inadequate loading zone, risk of encroachment; insufficient rubbish storage and collection areas, and increased traffic impacts.
- 7.12.17. Council's Development Engineering Officer has provided responses to the representation concerns.

*Inadequate loading zone – too small and too far to meet operational needs.*

*Response:*

- *Waste collection will be handled by a private service provider, with bins transferred to the existing loading zone on Sandy Bay Road for kerbside pickup. (Condition ENG 13)*
- *The proposal includes time restricted pick-up/drop-off area on Wilmot Street, approximately 100m from the site, designed to accommodate up to two vehicles*
- *Whilst the proposal is not reliant on the outward delivery of goods, inward bound goods, such as linen and Food and beverage will be required. It is proposed that deliveries will occur via the loading zone situated within 50m of the site, along Sandy Bay Road. Therefore, Commercial movements associated with the proposal can comply with A1 of the HIPS*

*Risk of encroachment on the laneway and Right of Way due to deliveries.*

*Response:*

- *The proposal and Salt traffic impact assessment does not propose any use of the laneway for deliveries or access.*
- *All drop-offs and waste collection are planned via Wilmot Street and Sandy Bay Road, minimizing risk of encroachment.*

*Lack of parking will increase congestion and traffic safety issues on nearby streets.*

*Response:*

- *Guests will be informed to use public transport, taxis, Uber, or e-scooters, reducing reliance on private vehicles.*
- *Parking surveys (SALT Traffic Impact assessment) show sufficient on-street availability, especially during weekday evenings*
- *The development provides no on-site parking, but this is consistent with some other Hobart hotels located within the nearby CBZ*
- *Under the HIPS, the proposed short-term accommodation requires 33 parking spaces and the rooftop bar requires 46 parking spaces. It is important to note that the requirement of 46 parking spaces for the rooftop bar has been determined to be excessive given the nature of the development and its proximity to the Central Business Zone. Despite the lack of on-site car parking, this is not identified as the primary concern for this development due to the proximity of the development to the Central Business zone (approximately 60m from the zone).*

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- *The report provided by SALT – Traffic Impact Assessment – demonstrates capacity in the availability of the on-street parking network to cater for the traffic generated by the development and for the pick up and drop off activity for the hotel development.*

*Rubbish storage and collection zones are inadequate and may encroach on footpaths and laneway.*

*Response:*

- *Waste will be collected by a private contractor using the existing loading zone on Sandy Bay Road.*
- *Bins will be transferred only on collection days, and no permanent encroachment is planned.*
- *No use of the laneway or footpath for storage is indicated in the proposal.*

7.12.18. For the reasons as outlined above, it is considered the submissions made by the representors have been adequately addressed.

7.12.19. The proposal complies with the performance criteria.

7.13. E13.7 Development Standards for Heritage Places

7.14. E13.7.1 P1 - Demolition

7.14.1. Demolition of a Heritage Place is proposed and there is no acceptable solution at clause E13.7.1 A1.

7.14.2. As such, the proposal relies upon assessment against the performance criteria at clause E13.7.1 P1, which reads:

**P1**

*Demolition must not result in the loss of significant fabric, form, items, outbuildings or landscape elements that contribute to the historic cultural heritage significance of the place unless all of the following are satisfied;*

- there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;*
- there are no prudent and feasible alternatives;*
- important structural or façade elements that can feasibly be retained and reused in a new structure, are to be retained;*
- significant fabric is documented before demolition.*

7.14.3. The application was referred and found to be supportable by Gray Planning obo Senior Cultural Heritage Officer. The following comments has been provided from Attachment B:

*The form of the Temple building including its front façade will not be impacted by the proposed development. While some of the roof form will be lost, the*

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*roof form to be affected is located behind the front facing section of roof directly behind the parapet.*

*Any items such as regalia or internal detailing or original furniture required to be removed as a result of demolition works need to be documented in a Conservation Management Plan, with details provided on the fabric to be removed and its ability to be reused, relocated within the building or alternatively stored.*

7.14.4. The following comments has been provided from Attachment B:

*It is considered that the degree of demolition is relatively minor and has been restricted to areas of lower value within the Temple building.*

As such, the proposal satisfies subclause (a).

7.14.5. The following comments has been provided from Attachment B:

*It is considered that the extent of demolition is relatively minimal and will not impact the overall integrity of the Temple building or result in an unreasonable degree of impact of any fabric of moderate or high value.*

As such, the proposal satisfies subclause (b).

7.14.6. The following comments has been provided from Attachment B:

*The façade of the Masonic Temple building will remain intact as part of the proposed development. No façade elements are proposed to be impacted by the proposed development which has set back the new building 7m from the façade. Where works will occur to side walls, these are considered unavoidable due to site constraints and the location of adjacent development and are required to enable access and egress into the new building. These are considered to have been designed and located to minimise impact to the Temple building.*

*Side and rear access into the development is considered highly beneficial to avoid the façade and its central entrance being affected.*

As such, the proposal satisfies subclause (c).

7.14.7. *Some original detailing will be required to be unavoidably removed as part of the demolition works. The extent of this detailing to be impacted is minor and occurs in rooms of lower value within the building.*

*Nonetheless, any items such as regalia or internal detailing or original furniture required to be removed as a result of demolition works need to be documented in a Conservation Management Plan, with details provided on the fabric to be*

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*removed and its ability to be reused, relocated within the building or alternatively stored*

As such, the proposal satisfies subclause (d).

7.14.8. The proposal complies with the performance criteria, subject to conditions.

7.15. E13.7.2 P1 - Buildings and Works other than Demolition

7.15.1. Buildings and works other than demolition of a Heritage Place are proposed and there are no acceptable solutions at clause E13.7.2 A1.

7.15.2. As such, the proposal relies upon assessment against the performance criteria at clause E13.7.2 P1, which reads:

**P1**

*Development must not result in any of the following:*

- (a) loss of historic cultural heritage significance to the place through incompatible design, including in height, scale, bulk, form, fenestration, siting, materials, colours and finishes;*
- (b) substantial diminution of the historic cultural heritage significance of the place through loss of significant streetscape elements including plants, trees, fences, walls, paths, outbuildings and other items that contribute to the significance of the place.*

7.15.3. The application was referred and found to be supportable by Gray Planning obo Senior Cultural Heritage Officer. The following comments has been provided from Attachment B:

*'Streetscape' is defined in the Scheme as:*

*"the visual quality of a street depicted by road width, street planting, characteristics and features, public utilities constructed within the road reserve, the setbacks of buildings and structures from the lot boundaries, the quality, scale, bulk and design of buildings and structures fronting the road reserve. For the purposes of determining streetscape with respect to a particular site, the above factors are relevant if within 100 m of the site."*

*'Incompatible' is not defined in the Planning Scheme but is generally defined as follows"*

*'two things being so different in nature as to be incapable of coexisting'.*

*In terms of P1(a) the cultural significance of the Temple building has been summarised in this assessment as well as the Praxis report. Those identified values are broadly consistent with each other between the proponent's position and the one summarised in this assessment. It is considered that the*

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*proposed development will not result in a loss of those values through incompatible design.*

*In terms of scale, bulk and form, the proposed new building will sit within a cluster of comparable scale, height and massed buildings and behind the Masonic Temple building which by virtue of its intended purpose, is considered a substantial building itself in terms of bulk and mass, despite being only two storeys.*

*Consideration has been given to the external cladding of the proposed building which will be largely glass clad broken up by brass mesh screening across the street facing façade.*

*While it is not agreed that the proposed building will 'float' above the Temple building, its proposed cladding, colours and materials will reduce the visual appearance of the building and soften its appearance providing a more transparent cladding as opposed to solid opaque materials or finishes that would increase the visible presence of the new development.*

*In terms of height, the proposed 4.5 storey increase behind the Temple building is considered to be at the limit of what would be considered appropriate in terms of proposed height increase of a new building. This proposed height will sit comfortably against immediately adjacent buildings and owing to its set back from the Temple building, will result in the building being a background element rather than appearing as an extension to the Temple building that overwhelms the original street facing portion of the Temple. The Temple building will remain the dominant built form within the subject site when viewed from within the streetscape surrounding the subject site.*

*The 4.5 storey increase is not considered substantial and is comparable to a doubling of the height of the temple building itself.*

*The Temple building currently has a minimal streetscape presence that further reduces upon moving away from the subject site in either direction as a result of neighbouring development. Its form is primarily two dimensional in the streetscape owing to neighbouring development and its roof form is effectively hidden behind a parapet.*

*The setback behind the façade of the Temple building is considered effective is providing a visual separation that based on photo montages provided by the project architect, will not present as being visually prominent in the streetscape but rather, sit within an existing cluster of taller buildings without rising above any neighbour.*

*In terms of streetscape presence, the proposed building will only become really visible within the streetscape as a backdrop element to the Masonic Temple building when viewing the Temple building directly opposite or in close proximity.*

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*The proposal will neither screen or obscure the limited streetscape views of the Temple. The Temple building will retain its current extent of visible presence within the streetscape. Furthermore, the proposed building has importantly not sought to copy the architectural design of the temple or mimic the lighter sandstone hued bricks that accentuates the Temple building within the streetscape.*

*The relatively simple façade treatment and limited materials of the proposed building will not provide a visual distraction from the Temple building.*

*On this basis, the proposal is considered acceptable against P1(a).*

*In terms of P1(b), no streetscape elements will be impacted by the proposed development.*

7.15.4. The proposal complies with the performance criteria.

7.16. E13.7.2 P2 - Buildings and Works other than Demolition

7.16.1. Buildings and works other than demolition of a Heritage Place are proposed and there are no acceptable solutions at clause E13.7.2 A2.

7.16.2. As such, the proposal relies upon assessment against the performance criteria at clause E13.7.2 P2, which reads:

**P2**

*Development must be designed to be subservient and complementary to the place through characteristics including:*

- (a) scale and bulk, materials, built form and fenestration;*
- (b) setback from frontage;*
- (c) siting with respect to buildings, structures and listed elements;*
- (d) using less dominant materials and colours.*

7.16.3. The application was referred and found to be supportable by Gray Planning obo Senior Cultural Heritage Officer. The following comments has been provided from Attachment B:

*Neither 'subservient' or 'complementary' are defined in the Planning Scheme.*

*Subservient is defined as: "serving or acting in a subordinate capacity"*

*Complementary is defined as: "something which completes or makes perfect"*

*The assessment of the development requires an assessment of heritage values of the Place and a comparison exercise of building forms.*

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*The bulk and scale of the proposed building is considered complementary to the Masonic Temple form which also exhibits a substantial bulk and form for a two storey building.*

*The scale and accentuated height of the Heritage Place is part of its value and relates to its function as a meeting place for the Freemasonry movement. Such buildings typically were neither diminutive in scale or located so as to be inconspicuous in the streetscape or their setting.*

*The proposal seeks to place a larger (in height) building to the rear of a building lower in height.*

*In terms of the height, the height of the proposed building will not result in a loss or substantial reduction of any of the identified heritage values of the Temple building.*

*The difference in height is calculated by the project architect as being 4.5 floors that is set back a further 7m from the Sandy Bay Road frontage and building line of the Temple building.*

*It is considered that the proposed building has been designed and located to be responsive to the dominant characteristics of the Temple. The quite substantial height of the Temple building itself, which is a two storey building internally, but has a much higher form than normally expected for two storey buildings, is one of its values. On that basis, it is considered that the height of the proposed building to be located behind the main Temple form is complementary to the Temple building.*

*In terms of subservience, multiple factors have been considered in making an assessment of subservience. These relate to the scale, height, bulk and form of the Temple building itself, the scale, height and form of surrounding development and the presence of the Temple building within the streetscape, both before and after development.*

*The proposed building would not be considered appropriate for a diminutive one or two storey cottage exhibiting pitched roof forms or domestic scale when an assessment of comparison is undertaken against existing and proposed height and form.*

*However, the relatively substantial height, form and bulk of the Temple building itself affords a greater ability to consider a taller form adjacent which is further addressed by the proposed building being recessed into the subject site behind the main Temple form.*

*On the basis of a consideration of all values of the Temple building and a comparison exercise in terms of height when coupled with the location of the proposed building which will sit well behind the Temple building and not be visually prominent in the streetscape, it is considered that the proposed building is able to be considered both complimentary and subservient.*

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- 7.16.4. The proposal complies with the performance criteria.
- 7.17. *E13.7.2 P3 - Buildings and Works other than Demolition*
- 7.17.1. Buildings and works other than demolition of a Heritage Place are proposed and there are no acceptable solutions at clause E13.7.2 A3.
- 7.17.2. Representations were made in relation to bulk and scale, and setback of the proposed addition.
- 7.17.3. As detailed in the Heritage Officers assessment, the proposed building form is considered to be subservient to the Masonic Temple and would not be inconsistent with the existing and emerging form of the immediate locality.
- 7.17.4. For the reasons as outlined above, it is considered the submissions made by the representors have been adequately addressed.
- 7.17.5. As such, the proposal relies upon assessment against the performance criteria at clause E13.7.2 P3, which reads:

**P3**

*Materials, built form and fenestration must respond to the dominant heritage characteristics of the place, but any new fabric should be readily identifiable as such.*

- 7.17.6. The application was referred and found to be supportable by Gray Planning obo Senior Cultural Heritage Officer. The following comments has been provided from Attachment B:

*The proposed new building is to be constructed with glazing and brass mesh screening on the front street facing façade and zinc on side walls.*

*The proponent's heritage consultant Praxis describes the proposed materials as follows:*

*"The materials palette has been chosen to provide a more transparent and ephemeral contrast to the solid masonry and minimally fenestrated Masonic temple to promote a materiality hierarchy which emphasises the dominance of the earlier building and promotes subservience of the addition."*

*It is agreed that the predominantly glazed materials with brass metal mesh screening along with zinc will provide for a new fabric which does not visually dominate or compete with the visual hierarchy of the cream hued bricks which will remain as the primary visual focus in the streetscape.*

*The proposed new building provides vertical linear elements that reflect the strongly linear elements of the front façade and side walls of the temple building. The new building does not place strong emphasis on new façade elements but this approach is preferable to ensure the prominence and*

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*dominance of the architectural details of the Temple building which express strong vertical elements.*

- 7.17.7. Representations were made in relation to visual Impact due to the proposed design.
- 7.17.8. As detailed in the Heritage Officers assessment, the proposed façade and design of the addition would not result in disharmony within the existing streetscape. The proposed materials and architectural elements would not dominate the Masonic Temple façade.
- 7.17.9. For the reasons as outlined above, it is considered the submissions made by the representors have been adequately addressed.
- 7.17.10. The proposal complies with the performance criteria.
- 7.18. *E13.7.2 P4 - Buildings and Works other than Demolition*
- 7.18.1. Buildings and works other than demolition of a Heritage Place are proposed and there are no acceptable solutions at clause E13.7.2 A4.
- 7.18.2. As such, the proposal relies upon assessment against the performance criteria at clause P4, which reads:
- P4**  
*Extensions to existing buildings must not detract from the historic cultural heritage significance of the place.*
- 7.18.3. The application was referred and found to be supportable by Gray Planning obo Senior Cultural Heritage Officer. The following comments has been provided from Attachment B:
- As previously noted, the proposed new building which is technically an extension of the Temple building that for the most part, lightly touches the Temple as far as possible, and will not result in a loss or detracting of the identified cultural or historic heritage values of the Temple, its presence in the streetscape or its existing use.*
- 7.18.4. Representations were made in relation to visual Impact and streetscape compatibility due to the proposed bulk and scale.
- 7.18.5. Within the heritage context, the proposed design of the building would not be inconsistent with the heritage character of the area or detract from the architectural design of the Masonic Temple. The proposed setback of the addition would not disrupt the visual and architectural continuity of the area.
- 7.18.6. For the reasons as outlined above, it is considered the submissions made by the representors have been adequately addressed.
- 7.18.7. The proposal complies with the performance criteria.

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- 7.19. E13.10 Development Standards for Places of Archaeological Potential  
7.20. E13.10.1 P1 - Building, Works and Demolition

- 7.20.1. The acceptable solution at clause E13.10.1 A1 requires building and works to not involve excavation or ground disturbance.
- 7.20.2. The proposal will involve excavation and ground disturbance for the foundations of the proposed building to the rear of the site.
- 7.20.3. As such, the assessment relies upon assessment against performance criterion P1 at clause E13.10.1, which reads:

**P1**

*Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:*

- (a) the nature of the archaeological evidence, either known or predicted;*
- (b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;*
- (c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition;*
- (d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;*
- (e) measures proposed to preserve significant archaeological evidence 'in situ'.*

- 7.20.4. The application was referred and found to be supportable by Gray Planning obo Senior Cultural Heritage Officer. The following comments has been provided from Attachment B:

*The Conservation Management Policy, Heritage Impact Assessment & Statement of Compliance report prepared by Praxis Environment (author Mr Brad Williams) provides a detailed history of the subject site.*

*The subject site was formerly part of the gardens of the Highfield Estate with Frankland's 1839 map of Hobart and surrounds showing the subject site as being laid out with formal gardens below the Highfield dwelling.*

*The 1841 Census map of Hobart and surrounds likewise shows the subject site laid out as gardens.*

*Photographic images as early as 1857 show the subject site accordingly landscaped:*

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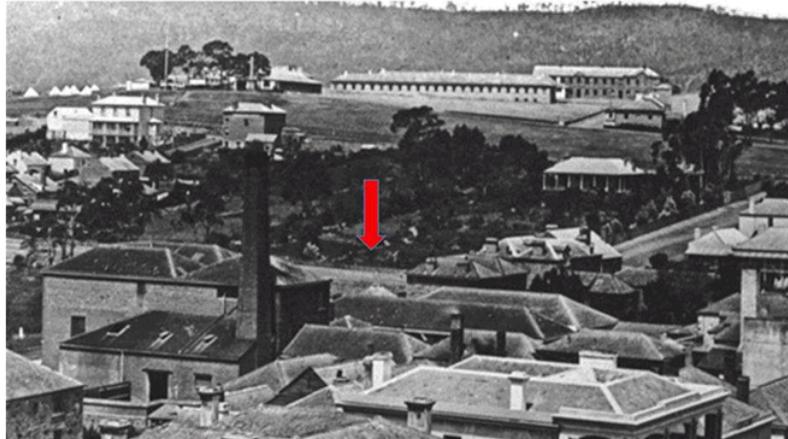


Figure 15: Excerpt from an 1857 Alfred Abbott panorama of Hobart, the red arrow depicting the approximate subject site. Libraries Tasmania.

Source: Praxis HIT, page 7

*Metropolitan Drainage Board plans originally held by Council show the subject site still undeveloped as of 1907.*

*No buildings are definitely known to have occupied the subject site prior to the construction of the Temple. Aside from being part of the Highfield Estate gardens, the subject site may (unconfirmed) have been part of a small nursery run by a Mr Latham with valuation rolls between 1880 and 1890 suggesting this use may have been partially located within the subject site.*

*However, there is no firm evidence that this use ever included any buildings. The submitted Praxis report provides detailed information about the history of the subject site and the subsequent development of the subject site as a Masonic Temple.*

*In light of the lack of evidence of any buildings or development known to have occurred on the subject site at any point prior to the construction of the Temple building in the late 1930's, it is considered that the subject site is almost certain to not include any archaeological potential.*

*On that basis, the proposal is not considered to be problematic against the P1 Performance Criteria of clause E13.10.1 and is appropriate for an approval against the standard without the requirement for any conditional approval.*

7.20.5. The proposal complies with the performance criteria.

## 8. Discussion

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- 8.1. As discussed in Section 5 and Section 6 of this report, the proposal relies upon performance criteria to satisfy the standards and codes of the relevant planning scheme. The proposal has been assessed against the provisions of section 57 of the *Land Use Planning and Approvals Act 1993* and is considered to perform well.
- 8.2. Matters, relating to building height and scale, noise impacts, privacy, traffic and access, on-site parking and waste, raised in the representations, received during the statutory advertisement period, has been adequately addressed in Section 6 – Assessment of this report.
- 8.3. The table below summarises the outstanding representation concerns:

Encroachment onto adjoining properties and Right of Way	<p>The application was referred and found to be supportable by Council's Surveying Services Manager, subject to a condition. The following comments were provided:</p> <p><i>The architect's Floor Plan of the proposed building (Job No. 1802 drawing DA 05 revision PA1) shows that a supporting column, the fire stair and lift while they are contained within the boundaries of 3 Sandy Bay Road encroach into the existing right of way 0.62 wide (private) that burdens the title for 3 Sandy Bay Road. The floor plan shows that the fire stair and lift will be offset 195 mm from the boundary while the right of way is 620mm wide. This right of way is favour of the property at 1 Sandy Bay Road that is strata-titled by strata plan 151831 and the property at 2 Heathfield Avenue (CT 144793/2). My view is that the encroaching column, fire stair and lift will mean that 1 Sandy Bay Road and 2 Heathfield Avenue will be unable to exercise the right of carriageway that they have the benefit of.</i></p> <p><i>My view is that it will be necessary to extinguish the right of way 0.62 wide (private) that burdens the title for 3 Sandy Bay Road. This will require the agreement of the owners of the strata lots at 1 Sandy Bay Road and the owner of 2 Heathfield Avenue. I have discussed this with Council's Legal Officer, who advised that this is a private matter for the applicant to resolve and not a planning matter. We cannot condition for the right of way to be extinguished; however, we can include advice regarding the encroachment to bring it to the applicant's attention.</i></p>
Amenity Impacts	<p>Concerns regarding guest experience, antisocial behaviour and reduced guest experience are not considered in the Planning Scheme.</p> <p>There is no evidence that suggests that the proposal would result in increased dust, litter, and potential for antisocial behaviour. Notwithstanding, these concerns are not considered in the Planning Scheme.</p>
Overshadowing and Loss of Sunlight	<p>A number of representations were raised regarding the overshadowing impacts onto the adjoining residential developments.</p> <p>With respect to the development standards for building works in the Mixed Urban Use Zone, the only relevant performance criteria at clause 15.4.1 P1 that relate to overshadowing</p>

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	<p>impacts are onto adjoining public spaces and clause 15.4.2 P2 which relates to overshadowing of residential zones. As discussed in this report, it is not considered that the proposal would have unreasonable impacts on St Davids Park or the public domain of Heathfield Avenue. The site does not adjoin and is not in close proximity to any residential zones.</p> <p>Notwithstanding, the submitted shadow diagrams show that the proposal would result in additional overshadowing during the mornings. However, the existing Mantra One Hotel and Telstra Exchange buildings predominantly overshadow these properties throughout the day.</p> <p>Due to the orientation of the adjoining northwestern property. It is not considered that the proposed development would result in any unreasonable overshadowing throughout the day. The submitted shadow diagrams shows that there is slight overshadowing impacts during the mornings on the residential dwellings opposite Heathfield Avenue. However, the properties are unaffected from 12:00pm onwards due to the orientation of the site in relation with the sun. In addition, it is considered that the Telstra Exchange building contributes more to the overshadowing impacts due to the existing building height and proximity to the residential dwellings. The proposed development is topographically on a lower ground level, will have a lower building height and does not directly adjoin Heathfield Avenue.</p> <p>In consideration of approved development on 5-7 Sandy Bay Road, the northeastern visitor accommodation apartments (Conservatorium) would retain unobstructed solar access due to the setback and position of the proposal. Similarly, the communal roof terrace retains unobstructed for most of the day, except in the afternoons. It is not considered that the overshadowing impacts from 3pm onwards are unreasonable due to the orientation of the site. Furthermore, the lower southwestern apartments do not receive adequate sunlight due to the existing shadows casted by the Masonic Temple and that these windows are predominantly southwest facing. It is considered that the adjoining properties retain adequate levels of solar access all-year round and that the proposal would not cause unreasonable loss in solar access amenity.</p>
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8.4. The application was referred to the City of Hobart's Urban Design Advisory Panel. Their minutes are included in full at Attachment A. The panel concluded the following:

- The proposed design has more vertical articulation in the new tower form when compared to the previous design, and is recognised as an improvement.
- The public realm at the entry along Sandy Bay Riad lack of design and detail when considering that the space has to fulfil a lot of functions; front entrance, waste and laundry servicing.

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- Further consideration of the laneway design could incorporate the use of public art, lighting, seating, high quality material and finishes to improve the safety, whilst maintaining the heritage context.
  - There are concerns regarding the operational viability of the commercial collection waste area on the adjoining property at 5-7 Sandy Bay Road. It is considered that a precinct plan would have been beneficial.
  - The selected base material palette is considered acceptable and adequate, however further deviation from the proposed would require future consideration by the Panel, specifically with relations to engineering and architectural resolutions.
  - The blank wall along the south-eastern elevation of the tower will be imposing and visible at a distance. Further articulation for the wall is recommended.
  - The plant level, just above the Masonic Temple's roofline, could be improved with a recessive/darker colour.
  - Heights and details should be included in the drawing set with regards to the lift overrun and possible service infrastructure.
  - Additional detail regarding the building height should be provided.
  - Inconsistencies in the drawings will need to be resolved, such as incorrect codes and street names, and lack of detail.
- 8.5. The proposal has been assessed and found to be supportable by Council's Environmental Development Planner and Traffic Engineering Officer, subject to conditions.
- 8.6. The proposal has been referred, and no objections were raised by TasWater, subject to conditions.

## 9. Conclusion

- 9.1. The proposed Partial Demolition, Alterations, Extension, and Partial Change of Use to Visitor Accommodation and Hotel Industry at 1 SANDY BAY ROAD, HOBART, 3 SANDY BAY ROAD, HOBART, 5-7 SANDY BAY ROAD, HOBART, AND 2A HEATHFIELD AVENUE, HOBART for the reasons outlined in the officer's report is recommended for approval, subject to the conditions contained within Attachment A.

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**ATTACHMENT A: CONDITIONS****GEN - General**

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-HOB-2021-0710 3 SANDY BAY RD HOBART TAS 7000 Final Planning Documents except where modified below.

**THC - General**

The use and/or development must comply with the requirements of the Tasmanian Heritage Council as detailed in the Notice of Heritage Decision, THC Works Ref: 8593 dated 4 June 2025, as attached to the permit.

**TW - General**

The use and/or development must comply with the requirements of TasWater as detailed in the form Submission to Planning Authority Notice, Reference No. TWDA 2021/01834-HCC dated 05/11/2021 as attached to the permit.

**GUEST COMMUNICATION MANAGEMENT PLAN**

Prior to the commencement of the approved use, a management plan for the operation of the development must be submitted and approved as a condition endorsement, to the satisfaction of the Senior Roads & Traffic Engineer. The management plan must include:

- Communication package to guests that there is no parking available off-street and provide information on parking available that is suitable for guests (i.e. nearby multi-storey car parks), and
- Information on the route to access the short-term parking on Wilmot Street.

Once approved, the management plan must be implemented prior to the commencement of the approved use and must be maintained for as long as the development is in operation. The management plan must be provided to guests prior to arrival.

**CONSERVATION MANAGEMENT PLAN**

1. A Conservation Management Plan must be prepared to the satisfaction of the Senior Cultural Heritage Officer, and be submitted with plans and documents lodged to Council for building approval.

This CMP should include the following:

- Consideration of all proposed plumbing, electrical, fire detection and protection requirements, security, acoustics and environmental requirements to ensure that installation is reversible and is undertaken in as unobtrusive a manner as possible.
- Documentation of any relocated or removed detailing, regalia or Masonic furnishings, confirming the extent removed and details of reuse as part of the development or details of cataloguing and storage within the Temple building for possible future use.
- Where it is determined by the project heritage consultant that NCC compliance will result in substantial heritage impact, alternatives are considered and documented to reduce impact accordingly to an acceptable limit.

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- Details of proposed interpretation panels or displays to be incorporated within the proposed Hotel building.
- A program of curatorial input for moveable heritage items to be effectively managed and protected.
- Details on how the removal of any significant detailing (e.g. in the current storage areas and upstairs assembly area) to facilitate the installation of the lift and stairs will be either retained for reuse on the site (e.g. in the refit of the kitchen as the hotel lobby) or alternatively stored for possible future use.

*Reason for condition:*

*To ensure the ongoing protection and appropriate management of identified historic and cultural heritage values of 3 Sandy Bay Road.*

2. Details of finalised colours, materials and finishes must be prepared to the satisfaction of the Senior Cultural Heritage Officer, and be submitted with plans and documents lodged to Council for building approval. No colour tinted or mirror style reflective glazing finishes are permitted for any external glazing.

*Reason for condition:*

*To clarify the extent and nature of the proposed approved works.*

#### **CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN**

A Construction Environmental Management Plan must be submitted and approved as a condition endorsement prior to the commencement of works and prior to the issue of any approval under the *Building Act 2016*, whichever occurs first.

The plan must include, but is not limited to, the following:

An overview of proposed demolition and construction methodologies and expected likely timeframes.

1. The proposed days and hours of work and proposed hours of activities likely to generate significant noise emissions (including volume and timing of any heavy vehicles entering and leaving the site, any rock breaking and any concrete pours).
2. Details of potential environmental impacts associated with the demolition and construction works including noise, erosion and pollution (air, land and water).
3. Details of proposed measures to avoid or mitigate all identified potential environmental impacts during demolition and construction works including, but not limited to:

A construction noise management plan endorsed by a suitably qualified person as being generally consistent with *AS 2436-2010 - Guide to Noise and Vibration Control on Construction, Demolition and Maintenance Sites* and the *Interim Construction Noise Guidelines* (New South Wales Department of Environment and Climate Change, July 2009), and with any relevant guidelines or standards referenced by those documents. Evidence of endorsement must be submitted with the CEMP.

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- a. Soil and water management measures to minimise sediment, dust or debris leaving the site or entering the stormwater system.
4. Details of proposed responsible persons, public communication protocols, compliance, recording and auditing procedures and complaint handling and response procedures.

Once approved the Construction Environmental Management Plan forms part of this permit and must be implemented and complied with.

A copy of the approved Construction Environmental Management Plan must be kept on site for the duration of the works and be available for inspection on request.

#### **SW 01 - Stormwater - Protection of Hobart City Council Assets**

Prior to the issue of any approval under the *Building Act 2016* or the commencement of work on the site (whichever occurs first), a pre-construction structural condition assessment and visual record (eg video and photos) of the Hobart City Council's stormwater infrastructure within/adjacent to the proposed development must be submitted to the City of Hobart.

The condition assessment must include at least:

1. a site plan clearly showing the location of the investigation, with access points and all segments and nodes shown and labelled, with assets found to have a different alignment from that shown on the City of Hobart's plans to be marked on the ground and on the plan,
2. a digital recording of a CCTV inspection and written condition assessment report in accordance with WSA 05-2013 Conduit Inspection Reporting Code of Australia, in a 'Wincan' compatible format; and
3. photos of any existing drainage structures connected to or modified as part of the development.

The preconstruction condition assessment will be relied upon to establish the extent of any damage caused to Hobart City Council's stormwater infrastructure during construction. If the owner/developer fails to provide the City of Hobart with an adequate pre-construction condition assessment then any damage to the City of Hobart's infrastructure identified in the postconstruction condition assessment will be the responsibility of the owner/developer.

#### **SW 02 - Stormwater - Protection of Hobart City Council Assets**

Prior to occupancy or the commencement of the approved use (whichever occurs first), a post-construction structural condition assessment and visual record (eg video and photos) of the Hobart City Council's stormwater infrastructure within/adjacent to the proposed development, along with photos of any existing drainage structures connected to or modified as part of the development, must be submitted to the City of Hobart.

The condition assessment must include at least:

1. a site plan clearly showing the location of the investigation, with access points and all segments and nodes shown and labelled, with assets found to have a different alignment from that shown on the City of Hobart's plans shall be marked on the ground and on the plan,

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2. a digital recording of a CCTV inspection and written condition assessment report in accordance with WSA 05-2013 Conduit Inspection Reporting Code of Australia, in a 'Wincan' compatible format; and
3. photos of any existing drainage structures connected to or modified as part of the development.

The postconstruction condition assessment will be relied upon to establish the extent of any damage caused to the Hobart City Council's stormwater infrastructure during construction. If the owner/developer fails to provide the City of Hobart with an adequate post-construction assessment then any damage to the Hobart City Council's infrastructure identified in the postconstruction CCTV will be deemed to be the responsibility of the owner/developer.

#### **SW 10A - Stormwater - Design**

All stormwater from the site (including hardstand runoff) must be disposed of to the public stormwater system prior to occupancy or the commencement of the use (whichever occurs first).

Prior to the issuing of any approval under the *Building Act 2016* or commencement of works (whichever occurs first), a stormwater management report and design must be submitted and approved as a condition endorsement. The stormwater management report and design must be prepared by a suitably qualified engineer and must:

1. Detail changes in the pre and post development impervious surface amounts and the relevant runoff volumes;
2. Detail any water quality and quantity control measures required to achieve the quality and quantity targets of the State Stormwater Strategy and council's Stormwater Guidelines for Development.
3. Clearly delineate the separation of private and public stormwater systems, ensuring all private systems are contained within the lot boundary and including a boundary inspection point for the new stormwater connection.
4. Provide full detail of the connection to the public stormwater main, with particular attention to the City of Hobart's standard exclusions from the state standard drawings which prohibit the use of saddle connections for this purpose.

All work required by this condition must be undertaken and maintained in accordance with the approved stormwater management report and design.

#### **SW 10B - Stormwater - Design**

The installation of the onsite stormwater disposal system must be inspected by the designer or the designer's agent. The designer, on completion of the installation, must certify that the installation has been constructed in accordance with the approved certified design.

A signed copy of the certification is to be forwarded to City of Hobart prior to occupancy or the commencement of the use (whichever occurs first).

#### **Screening for External Mechanical Plant**

External mechanical plant and associated screening must be sited, designed and constructed so that noise emissions from the plant do not exceed 40dB(A) at the boundaries of the site.

PLN-HOB-2021-0710 (PLN-21-710) Committee Report

**MUSIC SPEAKERS OPERATION**

Music speakers used in bar operations must only be sited within the internal area of the bar and speaker output must not exceed 75dB(A).

**BAR HOURS OF OPERATION**

Operation of the bar must be within the hours of 6am until midnight, 7 days per week.

**COMMERCIAL VEHICLE MOVEMENTS**

Commercial vehicle movements (including loading and unloading and garbage removal) to or from the site must be limited to within the hours of:

- (a) 7.00 am to 5.00 pm Mondays to Fridays inclusive;
- (b) 8.00 am to 5.00 pm Saturdays; and
- (c) 9.00 am to 12 noon Sundays and Public Holidays.

**ENG s1 - Development Engineering - On-street Parking**

Approval from Council's Strategic and Regulatory Services Network must be obtained prior to the issue of any consent under the Building Act 2016 (excluding demolition or excavation) for any changes to the existing on-street parking arrangements in Wilmont Street.

Advice: All works will be at the developer's expense. Please contact Council's City Transport Group Manager with regard to the application process for any changes to the on-street parking arrangements in Wilmont Street.

<https://www.hobartcity.com.au/City-services/Parking/Integrated-Parking-System/Dial-Before-You-DigParking-Sensors>

<https://www.hobartcity.com.au/City-services/Parking/Integrated-Parking-System/DialBefore-You-Dig-Parking-Sensors>

**ENG 1A - Development Engineering - Protection of Council Assets**

Any damage to council infrastructure resulting from the implementation of this permit, must, at the discretion of the Council:

1. Be met by the owner by way of reimbursement (cost of repair and reinstatement to be paid by the owner to the Council); or
2. Be repaired and reinstated by the owner to the satisfaction of the Council.

A photographic record of the Council's infrastructure adjacent to the subject site should be provided to the Council prior to any commencement of works.

A photographic record of the Council's infrastructure (e.g. existing property service connection points, roads, buildings, stormwater, footpaths, driveway crossovers and nature strips, including if any, pre-

PLN-HOB-2021-0710 (PLN-21-710) Committee Report

existing damage) will be relied upon to establish the extent of damage caused to the Council's infrastructure during construction.

In the event that the owner/developer fails to provide to the Council a photographic record of the Council's infrastructure, then any damage to the Council's infrastructure found on completion of works will be deemed to be the responsibility of the owner.

#### **ENG 12 - Development Engineering - Construction waste management**

A construction waste management plan must be implemented throughout construction.

A construction waste management plan must be submitted and approved as a Condition Endorsement, prior to commencement of work on the site. The construction waste management plan must include:

- Provisions for commercial waste services for the handling, storage, transport and disposal of post-construction solid waste and recycle bins from the development; and
- Provisions for the handling, transport and disposal of demolition material, including any contaminated waste and recycling opportunities, to satisfy the above requirement.

All work required by this condition must be undertaken in accordance with the approved construction waste management plan.

*Advice:*

*This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.*

*It is recommended that the developer liaise with the Council's City Resilience Unit regarding reducing, reusing and recycling materials associated with demolition on the site to minimise solid waste being directed to landfill. Further information can also be found on the Council's.*

Reason for condition

To ensure that solid waste management from the site meets the Council's requirements and standards.

#### **ENG Tr2 - Development Engineering - Construction Traffic and Parking Management**

A construction traffic and parking management plan must be implemented prior to the commencement of work on the site (including demolition).

The construction traffic (including cars, public transport vehicles, service vehicles, pedestrians and cyclists) and parking management plan must be submitted and approved as a Condition Endorsement, prior to commencement work (including demolition). The construction traffic and parking management plan must:

1. Be prepared by a suitably qualified person.
2. Develop a communications plan to advise the wider community of the traffic and parking impacts during construction.
3. Include a start date and finish dates of various stages of works.

PLN-HOB-2021-0710 (PLN-21-710) Committee Report

4. Include times that trucks and other traffic associated with the works will be allowed to operate.
5. Nominate a superintendent, or the like, to be responsible for the implementation of the approved traffic management plan and available as a direct contact to Council and/or members of the community regarding day to day construction traffic operations at the site, including any immediate traffic issues or hazards that may arise.

All work required by this condition must be undertaken in accordance with the approved construction traffic and parking management plan.

*Advice:*

*This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.*

Reason for condition

To ensure the safety of vehicles entering and leaving the development and the safety and access around the development site for the general public and adjacent businesses.

#### **ADVICE**

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, bylaws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's website for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

#### **CONDITION ENDORSEMENT**

If any condition requires that further documents are submitted and approved, you will need to submit the relevant documentation to satisfy the condition via the Condition Endorsement Submission through PlanBuild. Detailed instructions can be found [here](#).

Once approved, the Council will respond to you via PlanBuild that the condition has been endorsed (satisfied).

Where building approval is also required, it is recommended that documentation for condition endorsement be submitted well before submitting documentation for building approval. Failure to address condition endorsement requirements prior to submitting for building approval may result in unexpected delays.

Fees for Condition Endorsement are set out in Council's [Fees and Charges](#).

#### **BUILDING PERMIT**

You may need building approval in accordance with the *Building Act 2016*, further details are available on the [Council's website](#), which may assist you in understanding the relevant requirements.

A checklist has also been developed by Consumer, Building and Occupational Services (CBOS) to help property owners understand their responsibilities before, during, and after building work. It outlines the key steps in the building work approval process for notifiable (medium risk) and permit (high risk) work under

PLN-HOB-2021-0710 (PLN-21-710) Committee Report

the *Building Act 2016*. This resource is designed to support owners in meeting their obligations and ensuring a smooth approvals process. You can access the checklist [here](#).

**This is a Permitted Planning Permit issued in accordance with section 58 of the *Land Use Planning and Approvals Act 1993*.**

#### **PLUMBING PERMIT**

You may need plumbing approval in accordance with the *Building Act 2016*, *Building Regulations 2016* and the National Construction Code. Further details are available on the [Council's website](#), which may assist you in understanding the relevant requirements.

A checklist has also been developed by Consumer, Building and Occupational Services (CBOS) to help property owners understand their responsibilities before, during, and after building work. It outlines the key steps in the building work approval process for notifiable (medium risk) and permit (high risk) work under the *Building Act 2016*. This resource is designed to support owners in meeting their obligations and ensuring a smooth approvals process. You can access the checklist [here](#).

#### **SPECIAL CONNECTION PERMIT**

You may need a Special Connection Permit (Trade Waste) in accordance with the *Plumbing Regulations 2014* and the Tasmanian Plumbing Code. Click [here](#) for more information.

#### **RIGHT OF WAY**

The proposal plans show that a supporting column, the fire stair and lift for the proposed hotel while they are contained within the boundaries of 3 Sandy Bay Road encroach into the existing right of way 0.62 wide (private) that burdens the title for 3 Sandy Bay Road.

This right of way is favour of the property at 1 Sandy Bay Road that is strata-titled by strata plan 151831 and the property at 2 Heathfield Avenue (CT 144793/2).

You should inform yourself as to your rights and responsibilities in respect to the private right of way particularly reducing, restricting or impeding the right during and after construction.

#### **FEES AND CHARGES**

Click [here](#) for information on the Council's fees and charges.

#### **BEFORE YOU DIG**

Click [here](#) for before you dig information.

PLN-HOB-2021-0710 (PLN-21-710) Committee Report

**ATTACHMENT B: URBAN DESIGN ADVISORY PANEL REPORT – PLN-HOB-2024-0710 - 3 SANDY BAY ROAD, 15 APRIL 2025**

PLN-HOB-2021-0710 3 SANDY BAY RD HOBART TAS 7000 Attachment B - Urban Design Advisory Panel Report

PLN-HOB-2021-0710 (PLN-21-710) Committee Report

**ATTACHMENT C: APPLICATION REFERRAL CULTURAL HERITAGE RESPONSE BY GRAY  
PLANNING OBO SENIOR CULTURAL HERITAGE OFFICER**

PLN-HOB-2021-0710 3 SANDY BAY RD HOBART TAS 7000 Attachment C - Heritage Referral  
Assessment Report

PLN-HOB-2021-0710 (PLN-21-710) Committee Report

**ATTACHMENT C: TASWATER SUBMISSION TO PLANNING AUTHORITY**

PLN-HOB-2021-0710 3 SANDY BAY RD HOBART TAS 7000 Attachment D - TasWater Submission to Planning Authority

PLN-HOB-2021-0710 (PLN-21-710) Committee Report

**ATTACHMENT E: DEVELOPMENT ENGINEERING ASSESSMENT REPORT**

PLN-HOB-2021-0710 3 SANDY BAY RD HOBART TAS 7000 Attachment E - Development Engineering Assessment Report

PLN-HOB-2021-0710 (PLN-21-710) Committee Report

**ATTACHMENT F: NOTICE OF HERITAGE DECISION**

PLN-HOB-2021-0710 3 SANDY BAY RD HOBART TAS 7000 Attachment F – THC Notice of Heritage Decision



## PLANNING APPLICATION

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**Status:** **Assessment**

**Reference**  
 PLN-HOB-2021-0710

**External Reference**  
 PLN-21-710

**Address**  
 3 SANDY BAY RD HOBART TAS 7000

**Titles**  
 71169/1

### Assessment Clock Activity

Start	End	Reason
25 Oct 2021, 12:00:00 am	6 May 2025, 12:00:00 am	Referred for more information
15 Jan 2025, 11:44:53 am	17 Jan 2025, 12:00:00 am	PLANNA-HOB-2025-0136 - Request for Information
13 Feb 2025, 8:07:28 am	5 May 2025, 12:00:00 am	PLANNA-HOB-2025-0492 - Request for Information

### Application Checklist

As part of the preliminary review of the application, at a minimum, please check:

- The right title and schedule of easements have been provided.
- The right fee has been paid.
- The plans are legible.
- The information provided by the applicant is correct/sufficient.
- Does it adjoin Council owned land? If so, please refer to Parks.

Please also don't forget to check for:

- TasWater
- TasNetworks
- Archaeology
- Contaminated sites
- Is it a Major Project?
- Who has delegation to determine the application

**Checked By**  
 -

### Applicant

Name	Email	Phone	Address	Involvement

### Owners

Name	Email Address	Address

## Relevant Property Title/s

---

Selected Titles

Total Area: 0m<sup>2</sup>

71169/1

## Owner Notification

---

**Are you the sole owner of the land?**

- Yes - continue to the next section
- No - answer question below

**If no, have you notified all owners, joint or part owners of your intention to submit this application?**

- Yes - enter owner details below
- No - you must notify all owners before proceeding with this application

**List all owners, joint or part owners as recorded on the Title documents notified:****Enter the date that the last owner, joint or part owner was notified****Declaration**

- I declare that all land owners, joint or part owners have been notified of this planning application.

## Crown Land Consent

---

**Is Crown Land involved in the proposed use or development?**

- Yes - complete question below
- No - continue to the next section - see further information below
- Unsure

**If yes, has written Crown Land consent been obtained?**

- Yes - upload written consent
- No - application will not be progressed until consent has been provided

## General Manager Consent

---

**Is Council-owned or administered land involved in the proposed use or development?**

- Yes - complete question below
- No - continue to the next section
- Unsure

**If yes, has written consent been obtained from the Council General Manager?**

- Yes - upload written consent
- No - application will not be progressed until consent has been provided

## Proposed Use or Development

---

**What is the reason for your planning application?**

- I want to change how the property is used
- I want to use the property for visitor accommodation
- I want to subdivide
- I want to undertake a new development or alteration
- I want to do a minor boundary adjustment
- I want to put up a sign(s)
- I want to demolish
- I want to do works only
- Other

**If your application is to subdivide, please enter the number of proposed lots.**

0

**If your application is for signage, please enter the number of signs.****Is the property a Tasmanian Heritage Listed Property?**

- Yes
- No

Is the application for an EPA Activity under the Environmental Management and Pollution Control Act 1994?

- Yes
- No
- Unsure

Is the proposed use or development permitted or discretionary?

- Permitted
- Discretionary
- Unsure if permitted or discretionary

Provide a full description of the proposed use or development

Atop of the existing 2 level building the construction of new accommodation to Levels 3-7 and Level 8 a roof top bar. Limited demolition internally to low heritage value areas

Will the proposed use or development involve a road reserve?

- Yes - complete the section below
- No - continue to the next section
- Unsure

If yes, enter the address(es) or locations below:

If yes, how will the road reserve be affected?

### Value of Works

---

What is the estimated value of the works?

17000000

### Assessment Documents

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No Documents.

### Council Description of Use and/or Development

---

Council Proposed Use or Development Description

Partial Demolition, Alterations, Extension, and Partial Change of Use to Visitor Accommodation and Hotel Industry

### Assessment

---

Application Type

-

Planning Scheme Name

-

Applicable Zone

-

Applicable Code(s)

-

Applicable Overlay(s)

-

Proposed Category of Use and/or Development (Use Class)

-

Existing Use Class Description

-

Use Class Details

-

Use Standards Details

-

Development Standards

-

Site Background and History

-

**Requests and/or Referrals**

Created On	Reference	Referral	Recipient	Current Due Date	Completed Date	Status
27 Sept 2023, 10:07:00 am	DA-HCC-202023-63829	Request for Information			27 Sept 2023, 10:07:00 am	Completed
22 Oct 2021, 6:46:00 pm	DA-HCC-202023-39357	Request for Information		22 Oct 2022, 6:46:00 pm		Draft
15 Jan 2025, 11:43:44 am	PLANNA-HOB-2025-0136	Request for Information	Phil Gartrell		7 May 2025, 7:54:03 am	Completed
20 Jan 2025, 12:14:20 pm	PLANNA-HOB-2025-0180	General Information Request	Phil Gartrell		30 Jan 2025, 8:09:43 am	Completed
11 Feb 2025, 10:06:51 am	PLANNA-HOB-2025-0446	Internal Referral	Stormwater Referrals		24 Feb 2025, 8:59:17 am	Completed
3 Feb 2025, 10:17:50 am	PLANNA-HOB-2025-0352	General Information Request	Phil Gartrell		11 Feb 2025, 9:27:02 am	Completed
11 Feb 2025, 10:07:30 am	PLANNA-HOB-2025-0447	Internal Referral	Mobility Referrals	13 Feb 2025, 12:00:00 am		Assessment
11 Feb 2025, 10:05:45 am	PLANNA-HOB-2025-0445	Internal Referral	Development Engineer Referrals	13 Feb 2025, 12:00:00 am		Assessment
11 Feb 2025, 10:02:29 am	PLANNA-HOB-2025-0443	Internal Referral	Heritage Referrals	13 Feb 2025, 12:00:00 am		Assessment
11 Feb 2025, 10:16:01 am	PLANNA-HOB-2025-0448	Internal Referral	Rowan Moore	13 Feb 2025, 12:00:00 am		Assessment
11 Feb 2025, 2:14:22 pm	RHT-HOB-2025-0012	Heritage Tasmania - Interest Registered			12 Feb 2025, 5:09:50 pm	Completed
13 Feb 2025, 8:02:33 am	PLANNA-HOB-2025-0492	Request for Information	Phil Gartrell		7 May 2025, 7:45:33 am	Completed
11 Mar 2025, 8:31:28 am	PLANNA-HOB-2025-0870	Submit Request	Adam Smee			Rejected
6 May 2025, 9:45:33 am	PLANNA-HOB-2025-1630	Referral of RFI Response to Authority			6 May 2025, 3:06:01 pm	Completed

**Advertising**

No Advertisements

**Assessor Recommendation****Assessor Recommendation**

-

**Assessor Name**

-

**Assessor Role**

-

**Date Completed**

-

**Determination****Determination**

That <Council Name> has <approved/refused> a planning application pursuant to <S57/S58> of the *Land Use Planning and Approvals Act 1993* under authority delegated by the Council subject to the <Conditions/Reasons> tabled below.

**Conditions**

No entries.

**Determination Details****Determination**

-

Determination Date

-

Authorised By

-

Delegated Authorisation Name

-

Delegated Authorisation Role

-

**Permit Dates**

---

**Signatory**

---

Form published: 05/07/2024 14:13



## RESULT OF SEARCH

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



## SEARCH OF TORRENS TITLE

VOLUME 71169	FOLIO 1
EDITION 1	DATE OF ISSUE 27-Jun-1995

SEARCH DATE : 19-Oct-2021

SEARCH TIME : 12.52 PM

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Diagram 71169 (formerly being 111-39NS)

Derivation : Part of 3A-OR-6Ps Section R 3 Gtd to A Moodie

Prior CT 2863/89

SCHEDULE 1

88074 THE TASMANIAN FREEMASONS' HALL COMPANY LIMITED

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

BURDENING EASEMENT: a free and full right of drainage for Arthur John Renney and the owner and occupier of land comprised in Certificate of Title Volume 390 Folio 142 and Volume 394 Folio 101 in common with Elinor Wayne Lake and all other persons having the like right as owners or occupiers of the balance of the land comprised in Certificate of Title Volume 338 Fol 136 in and over the strip of land marked "Drain" on D 711169 and from time to time on giving previous reasonable notice in that behalf to enter upon the said strip of land with servants workmen and others by his and their permission for the purposes of laying making repairing cleansing and maintaining any drains or pipes but making good all damage thereby done to the surface of the ground

BURDENING EASEMENT: a right of carriage way (appurtenant to the land comprised in Certificate of Title Volume 2948 Folio 40) over the Right of Way on D 11362

M418898 CAVEAT by Kerry Donald Bowerman, John James Caulfield and John William Woods Registered 08-May-2013 at noon

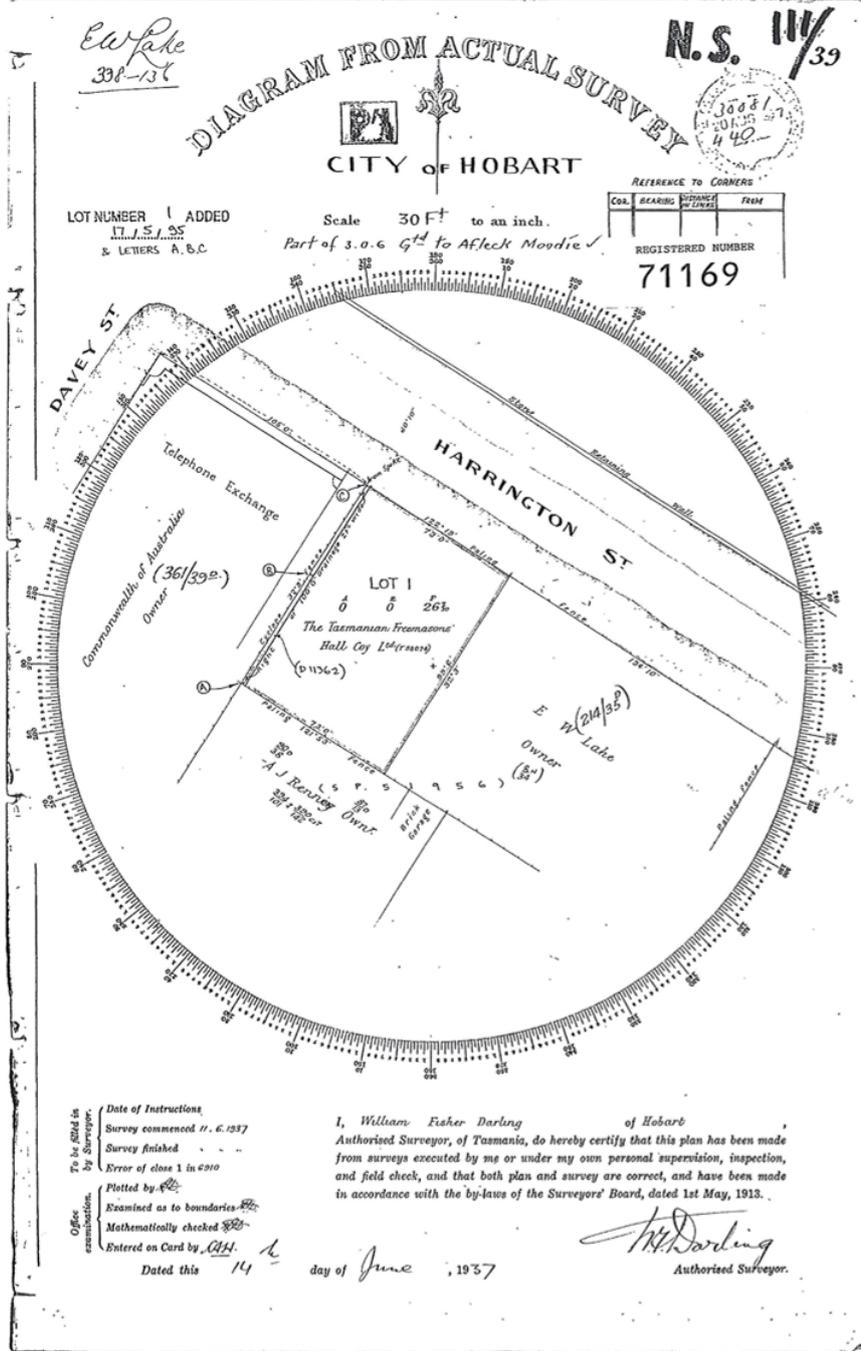
UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



FOLIO PLAN  
RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



**RESULT OF SEARCH**

RECORDER OF TITLES

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## SEARCH OF TORRENS TITLE

VOLUME	FOLIO
51956	8
EDITION	DATE OF ISSUE
3	17-Aug-2009

SEARCH DATE : 18-Nov-2021

SEARCH TIME : 08.33 AM

DESCRIPTION OF LAND

City of HOBART

Lot 8 on Sealed Plan 51956

Derivation : Part of 3A-0R-6Ps. Gtd. to A. Moodie and Part of  
2 Acres Gtd. to W. Murray

Prior CT 4823/81

SCHEDULE 1C928191 TRANSFER to YIANNIS TELLYROS and NEOPHYTOS TELLYROS  
as tenants in common in equal shares Registered  
17-Aug-2009 at noonSCHEDULE 2Reservations and conditions in the Crown Grant if any  
SP 51956 EASEMENTS in Schedule of Easements  
C928192 MORTGAGE to Commonwealth Bank of Australia  
Registered 17-Aug-2009 at 12.01 PMUNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



**FOLIO PLAN**

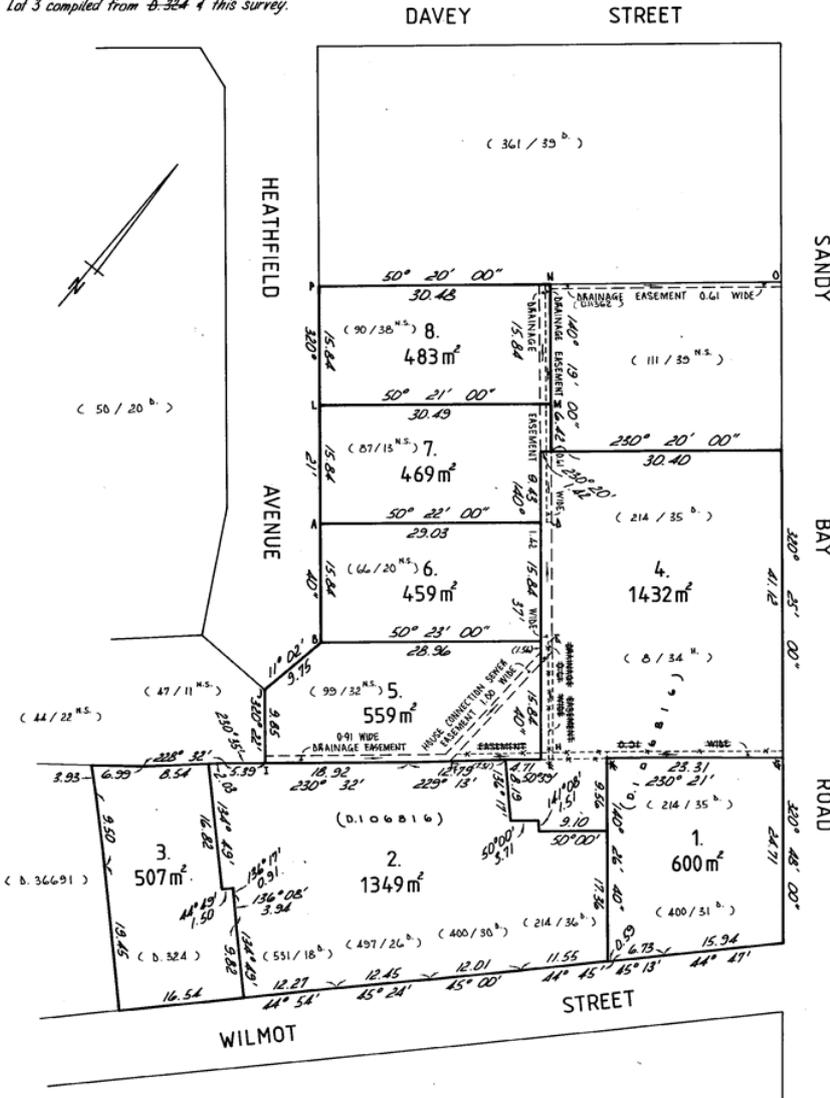
RECORDER OF TITLES

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Owner: <i>Australian Broadcasting Corporation</i>	PLAN OF SURVEY by Surveyor: <i>B. A. Connor</i> of land situated in the <i>J. B. MEDBURY P.L., SURVEYORS OF 264 CAMPBELL ST., HOBART.</i>	Registered Number: <b>SP51956</b>
Title Reference: <i>C.T. 2489-55 &amp; C.T. 3316/188</i>	CITY OF HOBART SEC. R3 SCALE 1: 400 MEASUREMENTS IN METRES	Approved: Effective from: <b>28 OCT 1991</b> <i>Michael O'Brien</i> Recorder of Titles
Grantee: <i>Part of 3<sup>d</sup>-0<sup>a</sup>-6<sup>o</sup> gtd. to Afleck Moodie Part of 2<sup>d</sup>-0<sup>a</sup>-0<sup>o</sup> gtd. to William Murray.</i>		

*Lot 3 compiled from B-334 & this survey.*



THE DRAINAGE EASEMENT MARKED FEK, 0-91 WIDE AND THE DRAINAGE EASEMENT MARKED CE 0-61 WIDE DELETED BY ME THIS TWENTY FIRST DAY OF JANUARY 1993 PURSUANT TO A REQUEST TO AMEND B.024521 MADE UNDER SECTION 481 OF THE LOCAL GOVERNMENT ACT 1962

*Michael O'Brien*  
 RECORDER OF TITLES



**SCHEDULE OF EASEMENTS**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



**SCHEDULE OF EASEMENTS**

**PLAN NO.**

**SP51956**

Note:—The Town Clerk or Council Clerk must sign the certificate on the back page for the purpose of identification.

The Schedule must be signed by the owners and mortgagees of the land affected. Signatures should be attested. Statements 3,4,5,6,7,8 & 9 were deleted and statement 1 added by me pursuant to a request to amend No. B624521 made under Section 481 of the Land Titles Act 1962.

*Michael D. [Signature]*  
 21-1-1993  
 Recorder of Titles

**EASEMENTS AND PROFITS**

Each lot on the plan is together with:—

- (1) such rights of drainage over the drainage easements shewn on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits à prendre described hereunder.

Each lot on the plan is subject to:—

- (1) such rights of drainage over the drainage easements shewn on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits à prendre described hereunder.

The direction of the flow of water through the drainage easements shewn on the plan is indicated by arrows. Lot 5 is subject to rights of drainage for Hilda Harriet Maning, Cecil William Baldwin and Frances Emily Ruth Baldwin over the Drainage Easement 0.91 wide marked IF hereon and the House Connection Sewer Easement 1.00 wide marked hereon.

~~Lot 4 is TOGETHER WITH a right of carriage way (appurtenant to the land marked A.B.C.D. hereon) over Heathfield Avenue delineated hereon.~~

~~TOGETHER WITH a right of drainage (appurtenant as aforesaid) over the strip of land 2 feet wide marked C.E. hereon together with a right to connect any drain which may be constructed over the said strip of land with any drain which may be constructed over the land 3 feet wide marked F.G. hereon.~~

~~Lot 4 is SUBJECT TO a right of drainage (appurtenant to the lands comprised in Certificates of Title Volume 335 Folio 73, Volume 338 Folio 133 and Volume 416 Folio 12) over the land 3 feet wide marked E.G. hereon.~~

~~Lot 5 is TOGETHER WITH a right of drainage (appurtenant to the land marked B.C.H.I.J. hereon) over the land 3 feet wide marked H.K. hereon.~~

~~Lot 4 is SUBJECT TO a right of drainage (appurtenant to the land marked B.C.H.I.J. hereon) over the land 3 feet wide marked H.K. hereon.~~

~~Lot 5 is SUBJECT TO rights of drainage for Hilda Harriet Maning, Cecil William Baldwin and Frances Emily Ruth Baldwin over the land 3 feet wide marked I.H. hereon.~~

~~Lot 4 is SUBJECT TO a right of drainage (appurtenant to the land marked A.B.C.D. hereon) over the land 2 feet wide marked C.E. hereon.~~

~~Lot 4 is SUBJECT TO the right of Frederick Lord his heirs and assigns to connect any drain which may be constructed over the land 2 feet wide marked C.F. hereon with any drain which may be constructed over the land 3 feet wide marked I.K. hereon such right to be appurtenant to the land marked A.B.C.D. hereon.~~

Lot 7 is TOGETHER WITH (appurtenant to the land marked A.D.M.L. hereon) a full and free right of drainage for the owner and occupier of the said land marked A.D.M.L. in common with Elinor Wayne Lake and the owner for the time being of the balance of the land comprised in Certificate of Title Volume 338 Folio 136 in or over the strip of land 2 feet wide marked M.N.O. hereon with power at any time on giving previous reasonable notice to enter upon the said strip of land 2 feet wide and to make lay repair cleanse and maintain any pipes or drains the

*[Handwritten mark]*



SCHEDULE OF EASEMENTS

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



person or persons entering to make good all damage to the surface occasioned thereby.

TOGETHER WITH (appurtenant to the land marked L.M.N.P. hereon) a full and free right of drainage for the owner and occupier of the said land marked L.M.N.P. in common with Elinor Wayne Lake and all other persons having the like right as owners or occupiers of the balance of the land comprised in Certificate of Title Volume 338 Folio 136 in and over the strip of land 2 feet wide marked N.O. hereon and from time to time on giving previous reasonable notice in that behalf to enter upon the said strip of land with servants workmen and others by his and their permission for the purpose of laying making repairing cleansing and maintaining any drains or pipes but making good all damage thereby done to the surface of the ground.

Lot 8

Lot 7

SUBJECT TO the like right for the owner and occupier of the land described in Certificate of Title Volume 390 Folio 142 in and over the strip of land 2 feet wide marked M.N. hereon.

Lot 2 is together with a right of sewerage over the land marked House Connection Sewer Easement 1.00 Wide for the owner or owners for the time being to discharge sewerage and sullage therefrom through any sewer or sewers now existing or to be constructed in the strip of land over which such right is expressed to be granted and the right for the owner or owners and surveyors and workmen from time to time and at all times hereafter if he or they shall think fit to enter upon the said strip of land and to construct therein a new sewer or sewers in substitution for the existing sewer or sewers and to inspect maintain and amend any such existing or substituted sewer or sewers making good any damage done to the said strip of land.

Lot 5 is subject to a right of sewerage appurtenant to Lot 2 over the land marked House Connection Sewer Easement 1.00 Wide for the owner or owners for the time being to discharge sewerage and sullage therefrom through any sewer or sewers now existing or to be constructed in the strip of land over which such right is expressed to be granted and the right for the owner or owners and surveyors and workmen from time to time and at all times hereafter if he or they shall think fit to enter upon the said strip of land and to construct therein a new sewer or sewers in substitution for the existing sewer or sewers and to inspect maintain and amend any such existing or substituted sewer or sewers making good any damage done to the said strip of land.

Diagrams 214/35, 214/36, 90/38NS, 66/20NS, 99/32NS, 400/30, 497/26, 531/18, and 400/31

IN WITNESS WHEREOF this Schedule has been executed on the 2nd day of October 1991.

SEAL OF THE AUSTRALIAN BROADCASTING CORPORATION was hereunto affixed by) authority of its Board of Directors) in the presence of: )



*Paddy Conroy*  
General Manager Legal & Administrative Services PADDY CONROY ACTING M.D.

*Elaine D. Cook*  
A permanent officer of the Corporation



**SCHEDULE OF EASEMENTS**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



This is the schedule of easements attached to the plan of AUSTRALIAN BROADCASTING.....  
*(Insert Subdivider's Full Name)*

CORPORATION .....affecting land in

VOLUME 2489 FOLIO 55 VOLUME 3316 FOLIO 88  
*(Insert Title Reference)*

Sealed by HOBART CITY COUNCIL on 14<sup>th</sup> October 1991

Solicitor's Reference .....  
60905 *[Signature]*  
Council Clerk/Town Clerk

**RESULT OF SEARCH**

RECORDER OF TITLES

*Issued Pursuant to the Land Titles Act 1980*

## SEARCH OF TORRENS TITLE

VOLUME 51956	FOLIO 7
EDITION 2	DATE OF ISSUE 03-Jul-2017

SEARCH DATE : 18-Nov-2021

SEARCH TIME : 08.40 AM

DESCRIPTION OF LAND

City of HOBART  
 Lot 7 on Sealed Plan 51956  
 Derivation : Part of 3A-0R-6Ps. Gtd. to A. Moodie and Part of  
 2 Acres Gtd. to W. Murray  
 Prior CT 4823/80

SCHEDULE 1

E51535 TRANSFER to FRAGRANCE TAS-HOBART (SANDY BAY) PTY LTD  
 Registered 03-Jul-2017 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any  
 SP 51956 EASEMENTS in Schedule of Easements

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



**FOLIO PLAN**

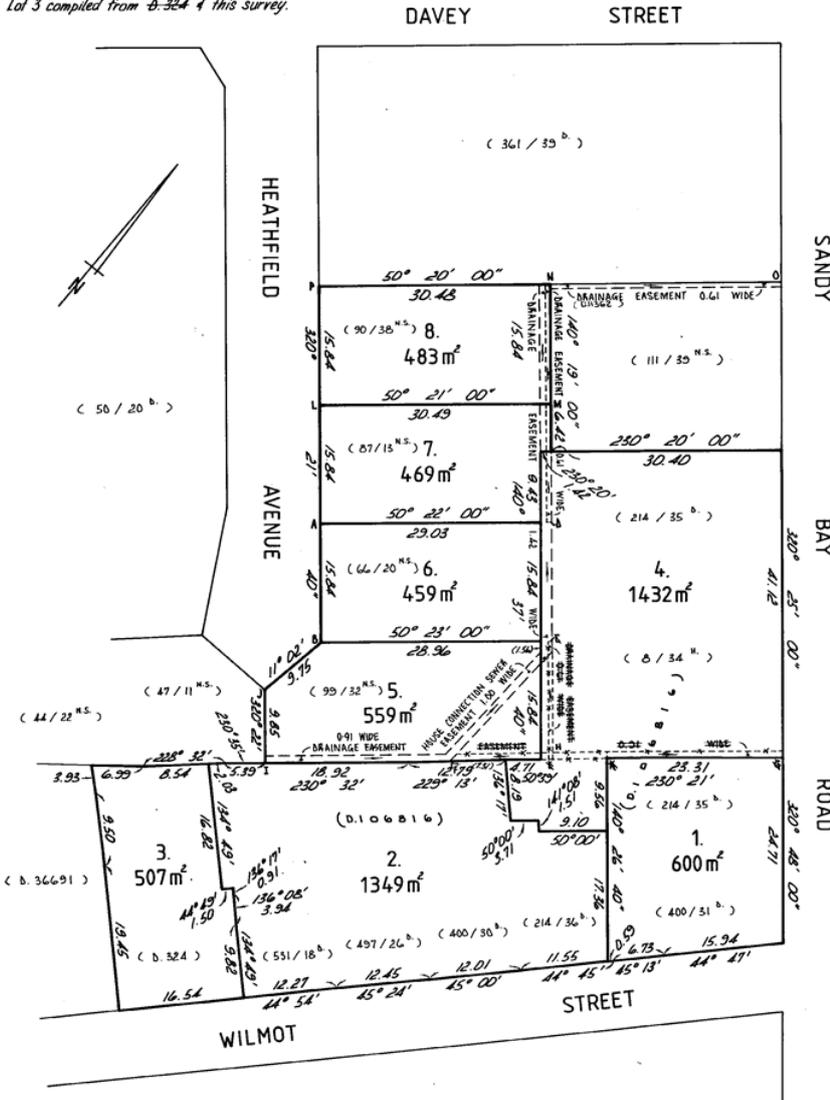
RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



Owner: <i>Australian Broadcasting Corporation</i>	PLAN OF SURVEY by Surveyor: <i>B. A. Connor</i> of land situated in the <i>J. B. MEDBURY P.L., SURVEYORS OF 264 CAMPBELL ST., HOBART.</i>	Registered Number: <b>SP51956</b>
Title Reference: <i>C.T. 2489-55 &amp; C.T. 3316/188</i>	CITY OF HOBART SEC. R3 SCALE 1: 400 MEASUREMENTS IN METRES	Approved: Effective from: <b>28 OCT 1991</b> <i>Michael O'Brien</i> Recorder of Titles
Grantee: <i>Part of 3<sup>d</sup>-0<sup>a</sup>-6<sup>o</sup> gtd. to Afleck Moodie Part of 2<sup>d</sup>-0<sup>a</sup>-0<sup>o</sup> gtd. to William Murray.</i>		

*Lot 3 compiled from B-334 & this survey.*



THE DRAINAGE EASEMENT MARKED FEK, 0-91 WIDE AND THE DRAINAGE EASEMENT MARKED C.E 0-61 WIDE DELETED BY ME THIS TWENTY FIRST DAY OF JANUARY 1993 PURSUANT TO A REQUEST TO AMEND B.024521 MADE UNDER SECTION 481 OF THE LOCAL GOVERNMENT ACT 1962

*Michael O'Brien*  
 RECORDER OF TITLES



SCHEDULE OF EASEMENTS

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



SCHEDULE OF EASEMENTS

PLAN NO.

SP51956

NOTE:—The Town Clerk or Council Clerk must sign the certificate on the back page for the purpose of identification.

The Schedule must be signed by the owners and mortgagees of the land affected. Signatures should be attested. Statements 3,4,5,6,7,8 & 9 were deleted and statement 1 added by me pursuant to a request to amend No. B624521 made under Section 481 of the Land Titles Act 1962.

*Michael D. ...*  
21-1-1993  
Recorder of Titles

EASEMENTS AND PROFITS

Each lot on the plan is together with:—

- (1) such rights of drainage over the drainage easements shewn on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits à prendre described hereunder.

Each lot on the plan is subject to:—

- (1) such rights of drainage over the drainage easements shewn on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits à prendre described hereunder.

The direction of the flow of water through the drainage easements shewn on the plan is indicated by arrows. Lot 5 is subject to rights of drainage for Hilda Harriet Maning, Cecil William Baldwin and Frances Emily Ruth Baldwin over the Drainage Easement 0.91 wide marked IF hereon and the House Connection Sewer Easement 1.00 wide marked hereon. TOGETHER WITH a right of carriage way (appurtenant to the land marked A.B.C.D. hereon) over Heathfield Avenue delineated hereon.

~~Lot 6 is TOGETHER WITH a right of drainage (appurtenant as aforesaid) over the strip of land 2 feet wide marked C.E. hereon together with a right to connect any drain which may be constructed over the said strip of land with any drain which may be constructed over the land 3 feet wide marked F.G. hereon.~~

~~Lot 4 is SUBJECT TO a right of drainage (appurtenant to the lands comprised in Certificates of Title Volume 335 Folio 73, Volume 338 Folio 133 and Volume 416 Folio 12) over the land 3 feet wide marked E.G. hereon.~~

~~Lot 5 is TOGETHER WITH a right of drainage (appurtenant to the land marked B.C.H.I.J. hereon) over the land 3 feet wide marked H.K. hereon.~~

~~Lot 4 is SUBJECT TO a right of drainage (appurtenant to the land marked B.C.H.I.J. hereon) over the land 3 feet wide marked H.K. hereon.~~

~~Lot 5 is SUBJECT TO rights of drainage for Hilda Harriet Maning, Cecil William Baldwin and Frances Emily Ruth Baldwin over the land 3 feet wide marked I.H. hereon.~~

~~Lot 4 is SUBJECT TO a right of drainage (appurtenant to the land marked A.B.C.D. hereon) over the land 2 feet wide marked C.E. hereon.~~

~~Lot 6 is SUBJECT TO the right of Frederick Lord his heirs and assigns to connect any drain which may be constructed over the land 2 feet wide marked C.E. hereon with any drain which may be constructed over the land 3 feet wide marked I.K. hereon such right to be appurtenant to the land marked A.B.C.D. hereon.~~

Lot 7 is TOGETHER WITH (appurtenant to the land marked A.D.M.L. hereon) a full and free right of drainage for the owner and occupier of the said land marked A.D.M.L. in common with Elinor Wayne Lake and the owner for the time being of the balance of the land comprised in Certificate of Title Volume 338 Folio 136 in or over the strip of land 2 feet wide marked M.N.O. hereon with power at any time on giving previous reasonable notice to enter upon the said strip of land 2 feet wide and to make lay repair cleanse and maintain any pipes or drains the

*Handwritten mark*



SCHEDULE OF EASEMENTS

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



person or persons entering to make good all damage to the surface occasioned thereby.

TOGETHER WITH (appurtenant to the land marked L.M.N.P. hereon) a full and free right of drainage for the owner and occupier of the said land marked L.M.N.P. in common with Elinor Wayne Lake and all other persons having the like right as owners or occupiers of the balance of the land comprised in Certificate of Title Volume 338 Folio 136 in and over the strip of land 2 feet wide marked N.O. hereon and from time to time on giving previous reasonable notice in that behalf to enter upon the said strip of land with servants workmen and others by his and their permission for the purpose of laying making repairing cleansing and maintaining any drains or pipes but making good all damage thereby done to the surface of the ground.

Lot 8

Lot 7

SUBJECT TO the like right for the owner and occupier of the land described in Certificate of Title Volume 390 Folio 142 in and over the strip of land 2 feet wide marked M.N. hereon.

Lot 2 is together with a right of sewerage over the land marked House Connection Sewer Easement 1.00 Wide for the owner or owners for the time being to discharge sewerage and sullage therefrom through any sewer or sewers now existing or to be constructed in the strip of land over which such right is expressed to be granted and the right for the owner or owners and surveyors and workmen from time to time and at all times hereafter if he or they shall think fit to enter upon the said strip of land and to construct therein a new sewer or sewers in substitution for the existing sewer or sewers and to inspect maintain and amend any such existing or substituted sewer or sewers making good any damage done to the said strip of land.

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Diagrams 214/35, 214/36, 90/38NS, 66/20NS, 99/32NS, 400/30, 497/26, 531/18, and 400/31

IN WITNESS WHEREOF this Schedule has been executed on the 2nd day of October 1991.

SEAL OF THE AUSTRALIAN BROADCASTING CORPORATION was hereunto affixed by) authority of its Board of Directors) in the presence of: )



*Paddy Conroy*  
General Manager - Legal & Administrative Services PADDY CONROY ACTING M.D.

*Elaine D. Cook*  
A permanent officer of the Corporation



### SCHEDULE OF EASEMENTS

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



This is the schedule of easements attached to the plan of AUSTRALIAN BROADCASTING.....  
*(Insert Subdivider's Full Name)*

CORPORATION .....affecting land in

VOLUME 2489 FOLIO 55 VOLUME 3316 FOLIO 88  
*(Insert Title Reference)*

Sealed by HOBART CITY COUNCIL on 14<sup>th</sup> October 1991

Solicitor's Reference .....  
60905 *[Signature]*  
Council Clerk/Town Clerk

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



## SEARCH OF TORRENS TITLE

VOLUME 106816	FOLIO 1
EDITION 3	DATE OF ISSUE 03-Jul-2017

SEARCH DATE : 18-Nov-2021

SEARCH TIME : 08.40 AM

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Diagram 106816

Derivation : Part of 3a-0r-6ps,gtd. to Afleck Moodie & Part of  
2a-0r-0ps,gtd. to William Murray, Part of 3A-0R-6Ps. Gtd. to A.  
Moodie and Part of 2 Acres Gtd. to W. Murray  
Prior CTs 51956/1, 51956/2 and 4823/77

SCHEDULE 1

E51535 TRANSFER to FRAGRANCE TAS-HOBART (SANDY BAY) PTY LTD  
Registered 03-Jul-2017 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any  
Benefiting easement; right of carriageway over Heathfield  
Avenue on D.106816.  
Benefiting easement;(appt.to the land marked ABCDEFGHJK on D.  
106816) over the land marked "Drainage Easement 0.91  
Wide" and over the land marked "House Connection  
Sewer Easement 1.00 Wide" on D.106816.  
Benefiting easement; right of drainage (appt.to the land  
marked ABCDEFGHJK on D.106816) over the drainage  
easement marked LMQR on D.106816.  
Burdening easement; right of drainage (appt.to the land marked  
ABCDEFGHJK on D.106816) over the drainage easement  
marked LMQR on D.106816.  
Benefiting easement; right of drainage over the drainage  
easement marked MQSTUV on D.106816.  
B607966 ADHESION ORDER under Section 477A of the Local  
Government Act 1962 Registered 26-Nov-1993 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



# FOLIO PLAN

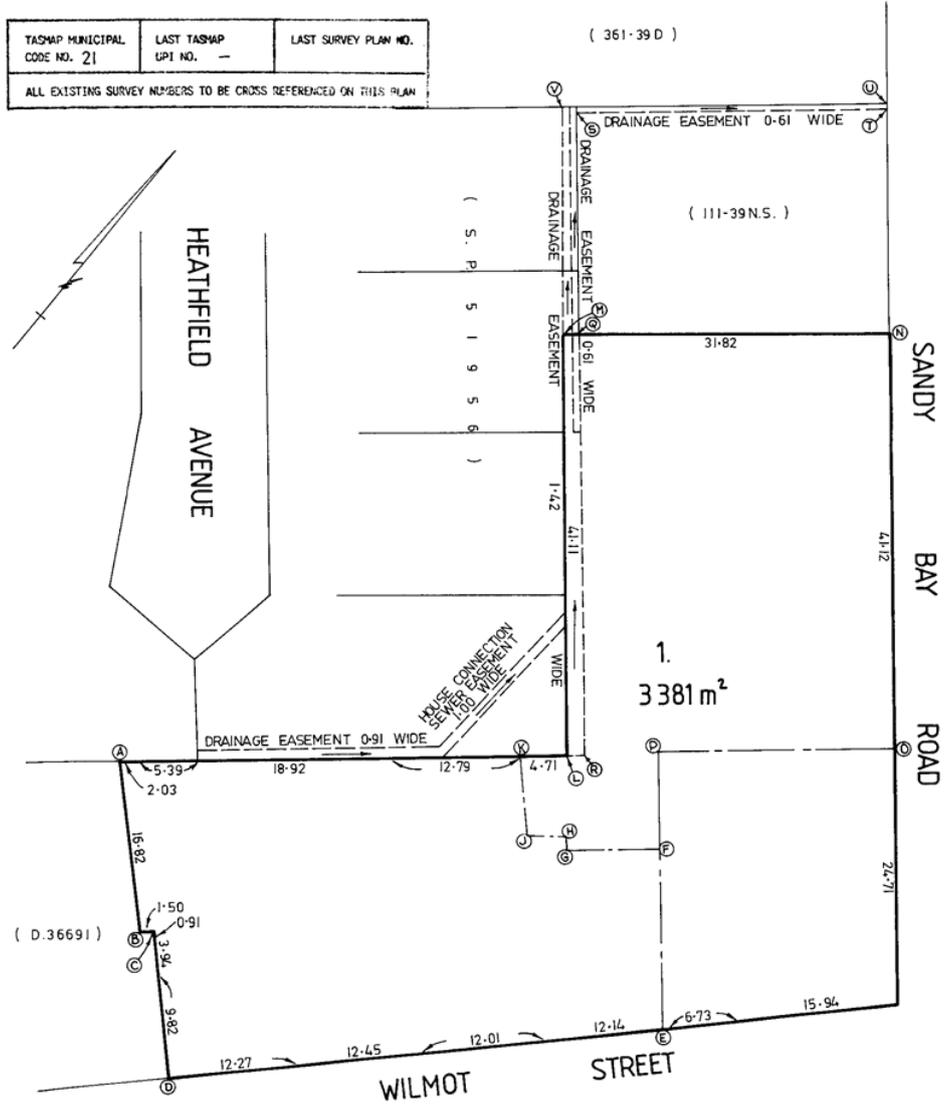
RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



Owner:	<b>PLAN OF TITLE</b> of land situated in the <b>CITY OF HOBART</b> ( SEC.R.3.)  COMPILED FROM...S.P.51956  SCALE 1:400      MEASUREMENTS IN METRES	REGISTERED NUMBER <b>D 106816</b>
Title Reference: C's.T.4823-74, 75 & 77		Approved: <b>19 JUL 1993</b>
Grantee:		<i>Michael Din</i> Recorder of Titles

TASMAP MUNICIPAL CODE NO. 21	LAST TASMAP UPI NO. -	LAST SURVEY PLAN NO.
ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN		



**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



## SEARCH OF TORRENS TITLE

VOLUME	FOLIO
151831	0
EDITION	DATE OF ISSUE
1	25-Jul-2007

SEARCH DATE : 18-Nov-2021

SEARCH TIME : 08.44 AM

DESCRIPTION OF LAND

City of HOBART

The Common Property for Strata Scheme 151831

Derivation : Part of 3A-0R-6Ps. (Section R.3.) Gtd. to A.  
Moodie.

Prior CT 144793/1

SCHEDULE 1

STRATA CORPORATION NUMBER 151831, 1 SANDY BAY ROAD, APARTMENTS

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

151831 FIRST BY-LAWS lodged with the strata plan

SP144793 EASEMENTS in Schedule of Easements

C812033 APPLICATION for registration of change of by-laws  
Registered 12-Sep-2007 at noonC909381 APPLICATION for registration of change of by-laws  
Strata Corporation No. 151831 Registered 04-May-2009  
at noonM445104 APPLICATION by owners to amend strata plan 151831 by  
deleting reference to Lot 405 from the notation  
contained on sheet 8 of the plan regarding 'approved  
use' Registered 17-Jan-2014 at noonE13580 APPLICATION by owners to amend strata plan 151831 by  
deleting Lot 204 as a serviced apartment Registered  
29-Sep-2015 at noonE35677 APPLICATION for registration of change of by-laws  
Registered 10-Feb-2016 at noonE258862 APPLICATION by owners to amend strata plan 151831 by  
deleting from the plan the words 'the approved use of  
Lot 211 is serviced apartment' Registered  
03-Jun-2021 at noonUNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



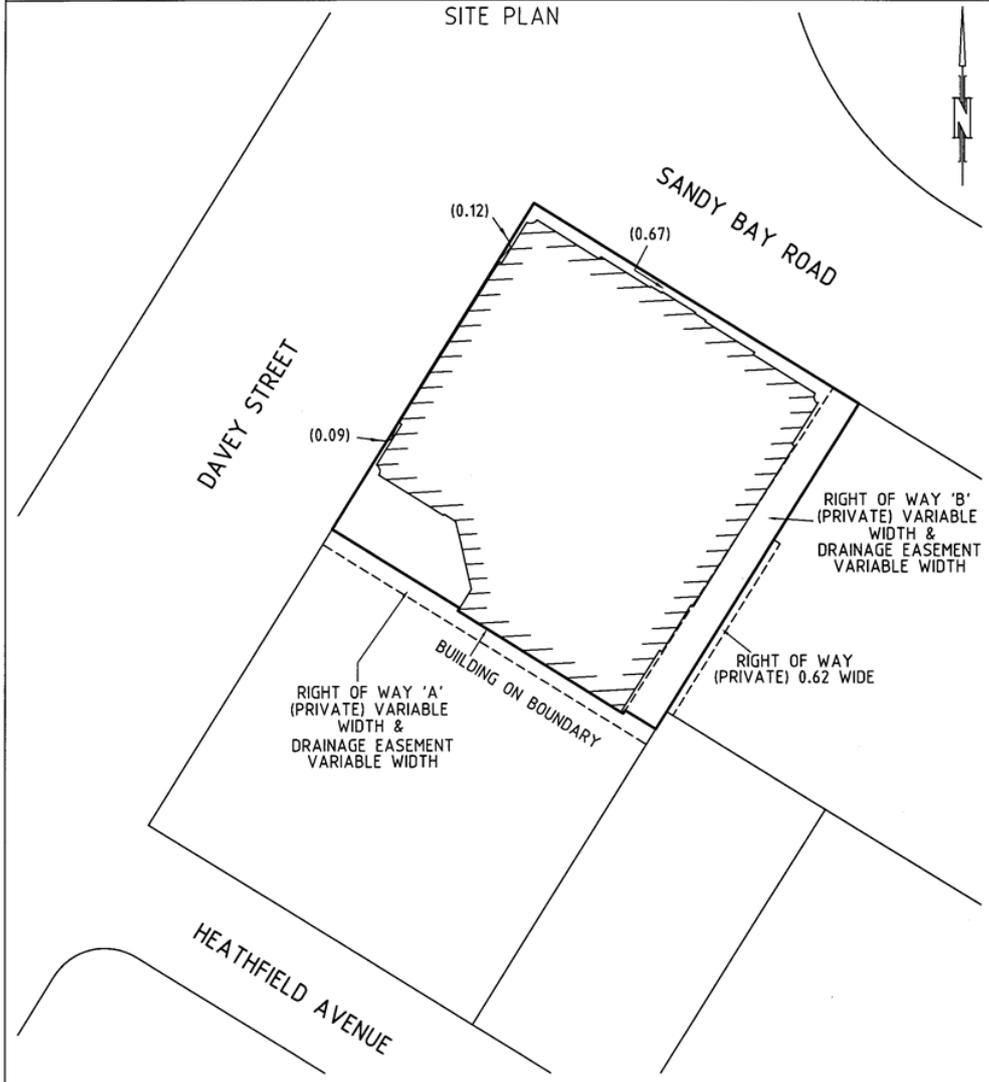
**FOLIO PLAN**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



CITY/TOWN HOBART		<b>STRATA PLAN</b>		REGISTERED NUMBER <b>151831</b>	
SUBURB/LOCALITY SANDY BAY				SHEET 1 OF 11 SHEETS	
FOLIO REFERENCE CT. 144793-1		NAME OF STRATA SCHEME 1 SANDY BAY ROAD, APARTMENTS		STRATA TITLES ACT 1998 REGISTERED 25 JUL 2007	
SITE COMPRISES THE WHOLE OF LOT 1 ON PLAN No. SP.144793				<i>Alice Kawa</i> Recorder of Titles	
MAPSHEET MUNICIPAL CODE No. 115 (5224)	LAST UPI No. GCX19	SCALE 1:300	LENGTHS IN METRES		



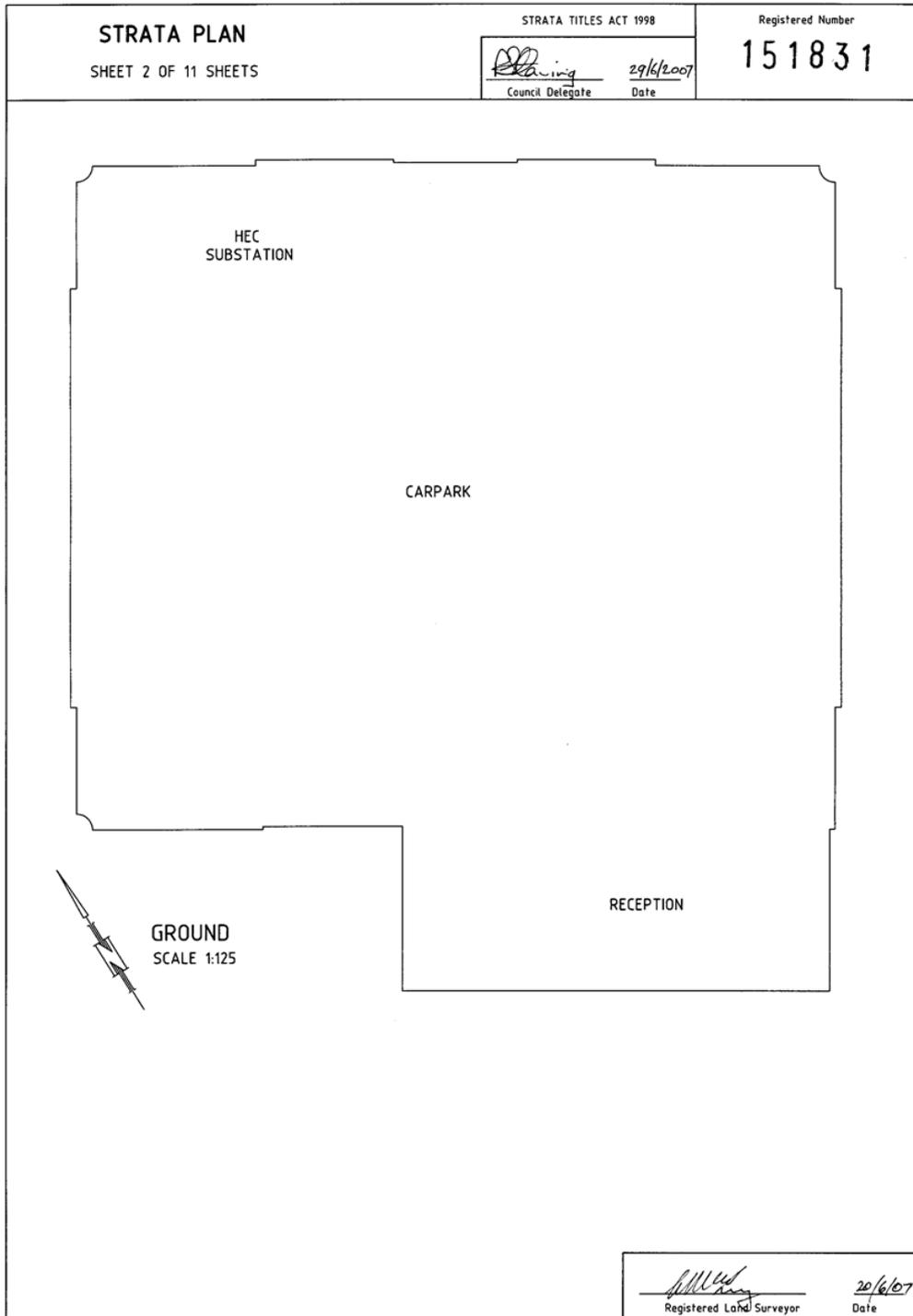
NOTES: (i) ALL BUILDINGS ON THE SITE TO BE SHOWN ON SHEET 1. (ii) BUILDING TO SITE BOUNDARY OFFSETS OF LESS THAN 2.00 METRES TO BE SHOWN ON SHEET 1.	<i>[Signature]</i> Council Delegate	29/6/2007 Date	<i>[Signature]</i> Registered Land Surveyor	20/6/07 Date
	STAGED/COMMUNITY DEVELOPMENT. SCHEME No. (IF APPLICABLE)			
LODGED BY JACKSON TREMAYNE & FAY				



**FOLIO PLAN**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

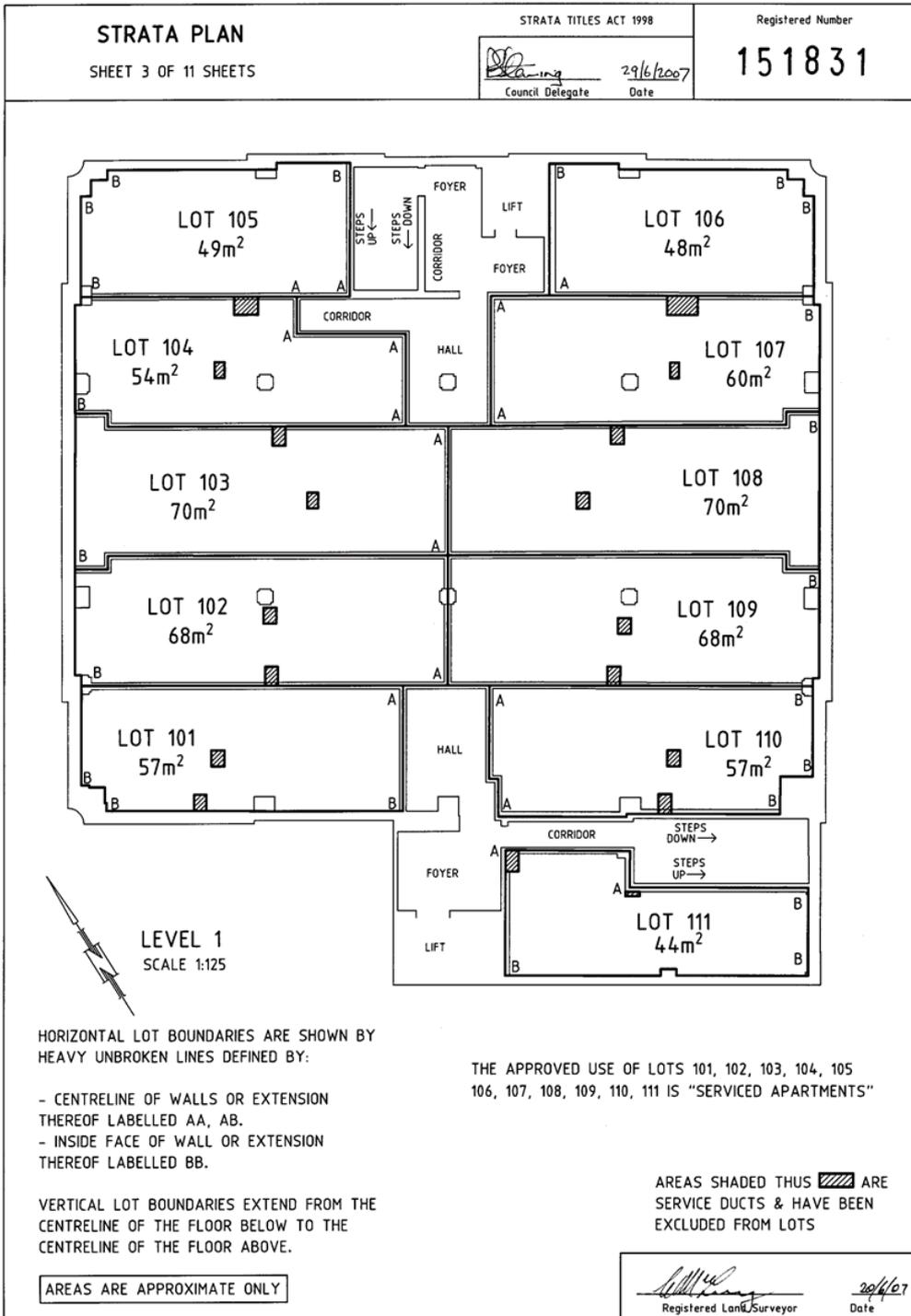




### FOLIO PLAN

RECORDER OF TITLES

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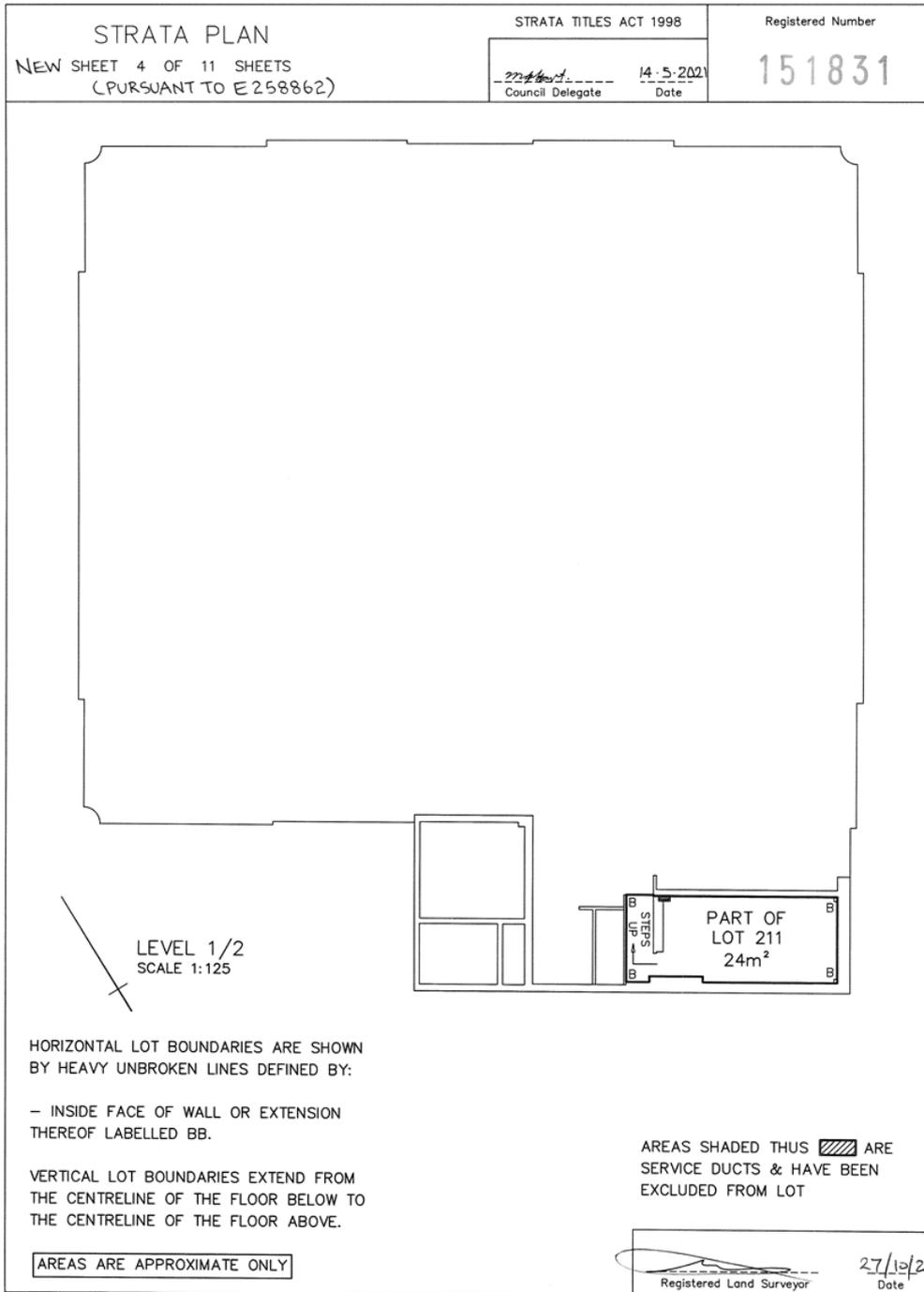




### FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

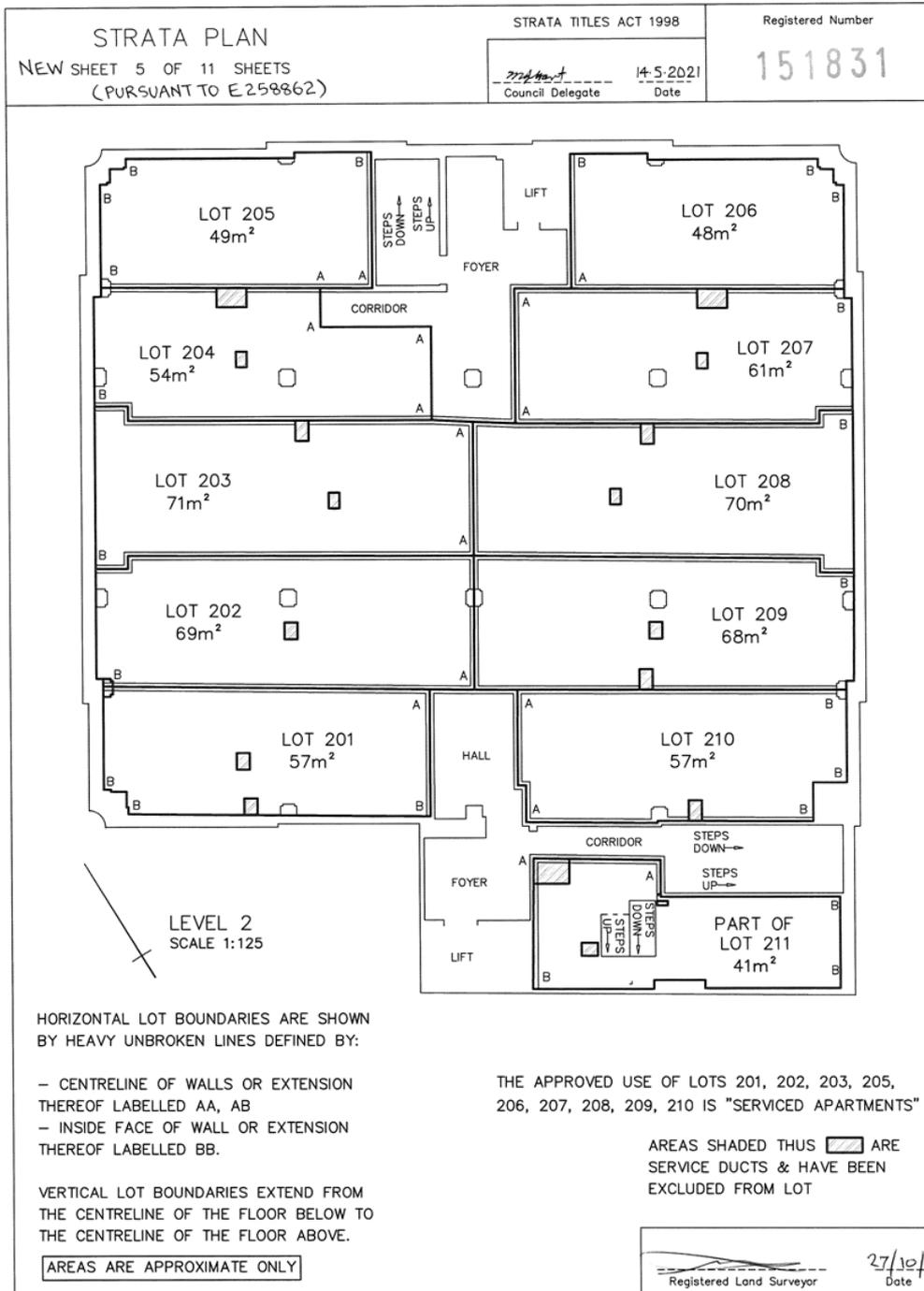




**FOLIO PLAN**

RECORDER OF TITLES

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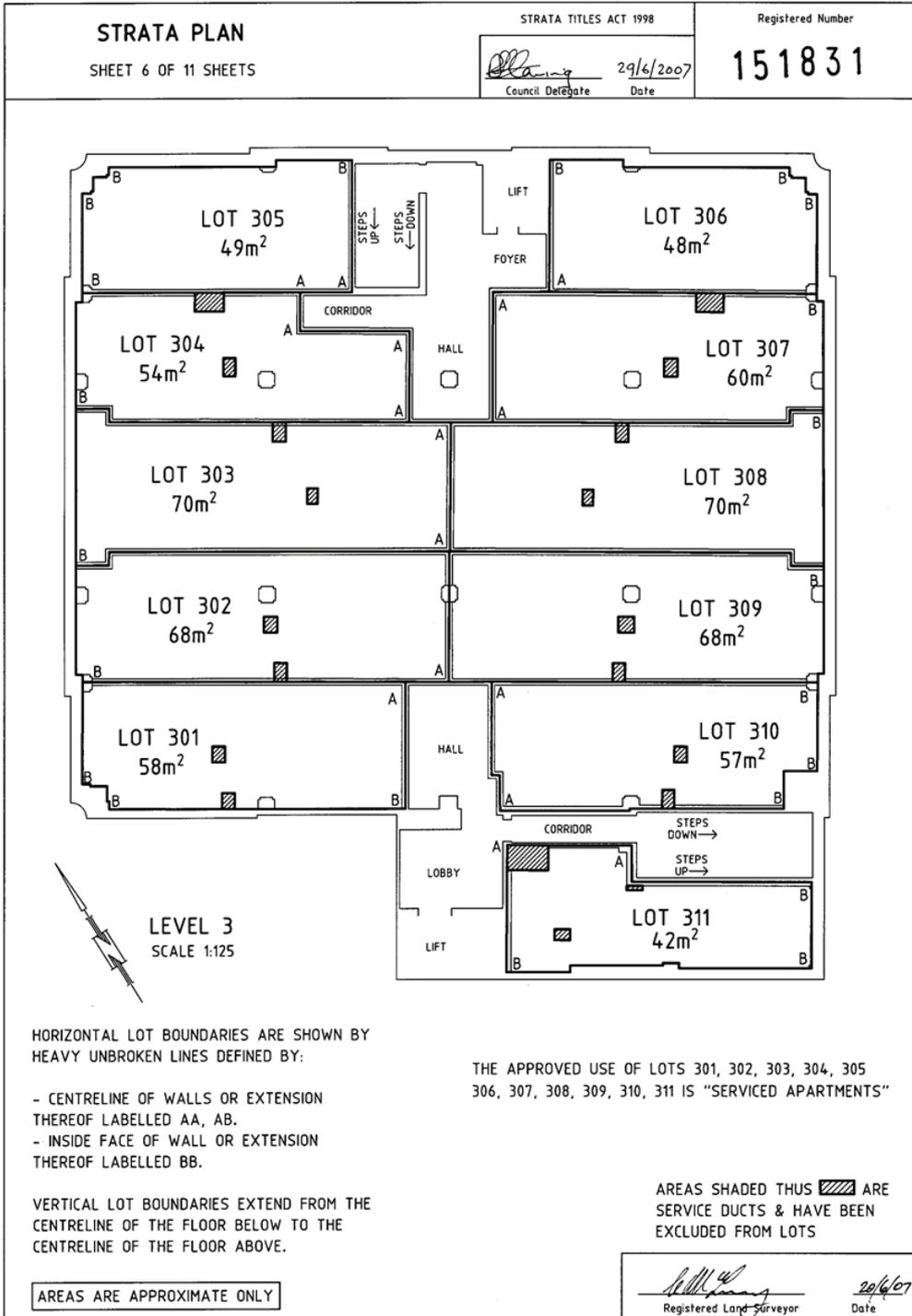




FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980





### FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



<p><b>STRATA PLAN</b></p> <p>SHEET 7 OF 11 SHEETS</p>	<p>STRATA TITLES ACT 1998</p>	<p>Registered Number</p> <p><b>151831</b></p>
<p><i>[Signature]</i>      29/6/2007</p> <p>Council Delegate      Date</p>		

**level 3/4**  
SCALE 1:125

PART OF LOT 411  
25m<sup>2</sup>

HORIZONTAL LOT BOUNDARIES ARE SHOWN BY HEAVY UNBROKEN LINES DEFINED BY:

- INSIDE FACE OF WALL OR EXTENSION THEREOF LABELLED BC.
- EDGE OF LANDING LABELLED CC.

VERTICAL LOT BOUNDARIES EXTEND FROM THE CENTRELINE OF THE FLOOR BELOW TO THE CENTRELINE OF THE FLOOR ABOVE.

THE APPROVED USE OF LOT 411 IS "SERVICED APARTMENT"

AREAS SHADED THUS ARE SERVICE DUCTS & HAVE BEEN EXCLUDED FROM LOTS

AREAS ARE APPROXIMATE ONLY

*[Signature]*      29/6/07

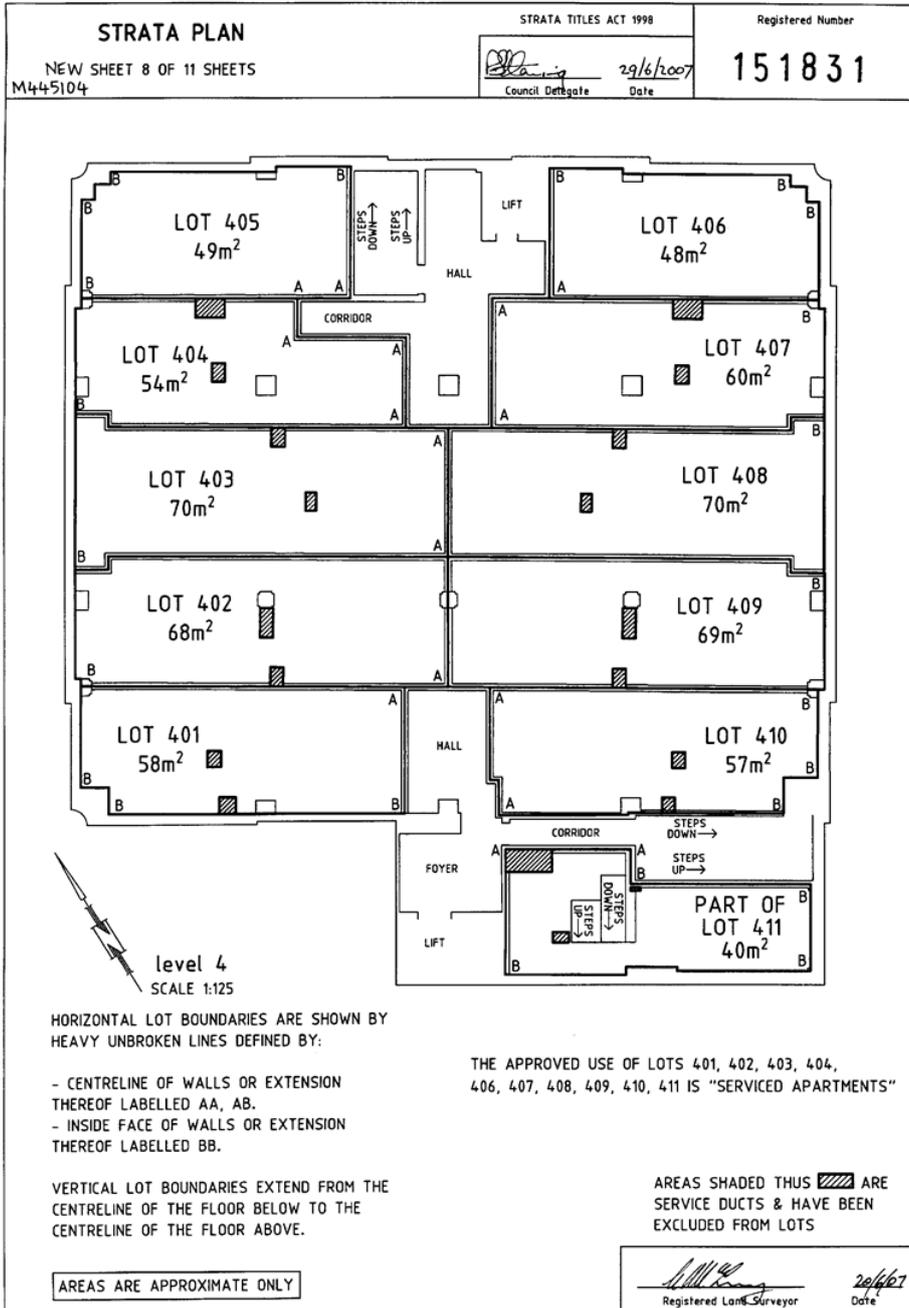
Registered Land Surveyor      Date



### FOLIO PLAN

RECORDER OF TITLES

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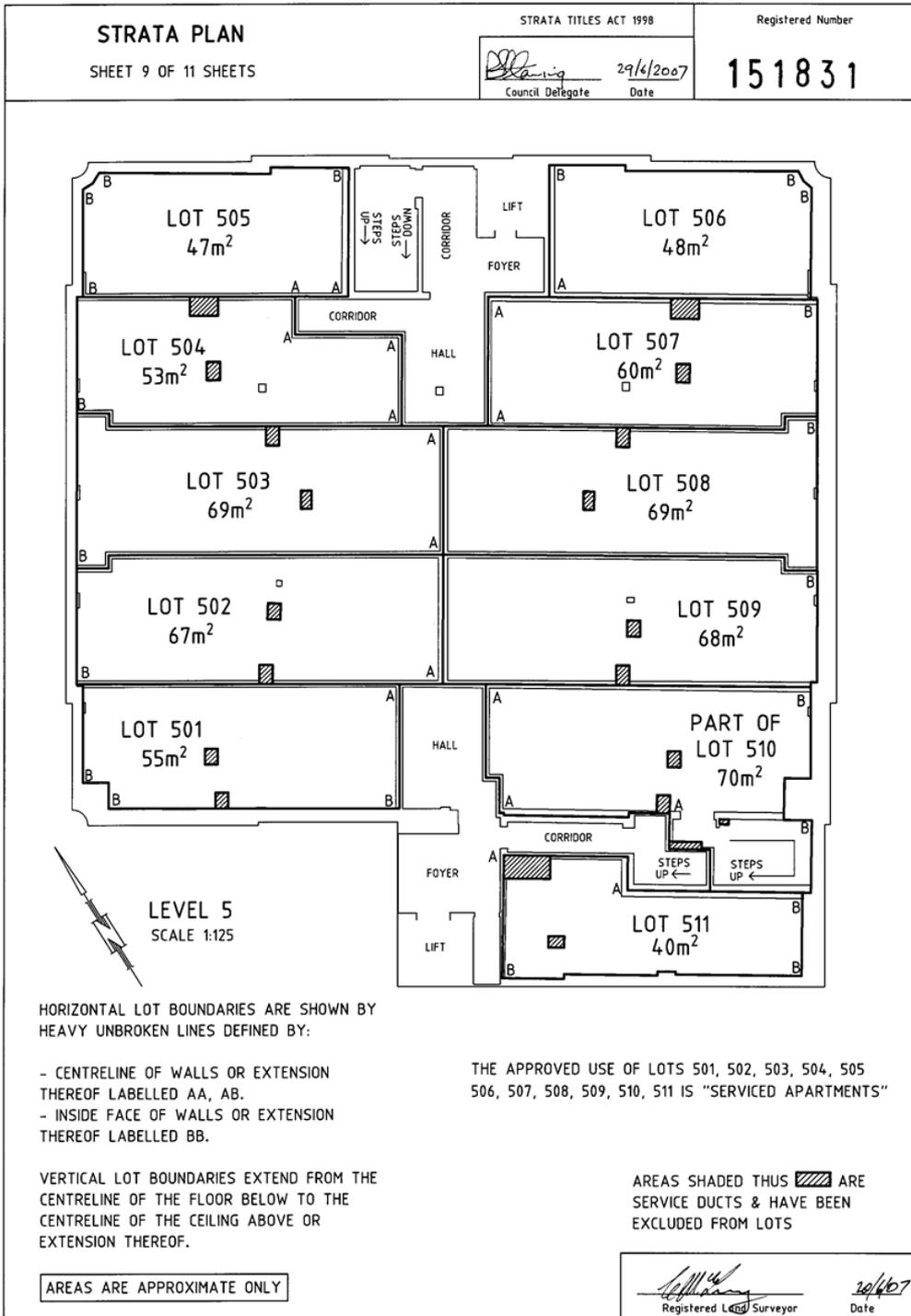




FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980





FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



<p><b>STRATA PLAN</b></p> <p>SHEET 10 OF 11 SHEETS</p>	<p>STRATA TITLES ACT 1998</p>	<p>Registered Number</p> <p><b>151831</b></p>
<p><i>[Signature]</i>      29/4/2007</p> <p>Council Delegate      Date</p>		

**LEVEL 6**  
SCALE 1:125

STEPS  
←  
STEPS  
DOWN

**PART OF  
LOT 510  
39m<sup>2</sup>**

HORIZONTAL LOT BOUNDARIES ARE SHOWN BY HEAVY UNBROKEN LINES DEFINED BY:

- INSIDE FACE OF WALLS OR EXTENSION THEREOF LABELLED BB.

VERTICAL LOT BOUNDARIES EXTEND FROM THE CENTRELINE OF THE FLOOR BELOW TO THE CENTRELINE OF THE CEILING ABOVE.

THE APPROVED USE OF LOT 510 IS "SERVICED APARTMENT"

AREAS ARE APPROXIMATE ONLY

*[Signature]*      20/4/07

Registered Land Surveyor      Date



**FOLIO PLAN**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



<b>STRATA PLAN</b>		STRATA TITLES ACT 1998	Registered Number <b>151831</b>		
SHEET 11 OF 11 SHEETS					
NAME OF BODY CORPORATE: STRATA CORPORATION No. 151831, 1 SANDY BAY ROAD, APARTMENTS					
ADDRESS FOR THE SERVICE OF NOTICES: STRATUM TITLE MANAGEMENT, 127 BATHURST STREET, HOBART, TAS. 7000					
<p style="text-align: center;"><b>SURVEYORS CERTIFICATE</b></p> I CRAIG McDOWALL TERRY of HOBART a surveyor registered under the Surveyors Act 2002 certify that the building or buildings erected on the site and drawn on sheet 1 of this plan are within the site boundaries of the folio stated on sheet 1 and any encroachment beyond those boundaries is properly authorised according to law.			<p style="text-align: center;"><b>COUNCIL CERTIFICATE</b></p> I certify that the HOBART CITY Council has: (a) approved the lots shown in this plan and (b) issued this certificate of approval in accordance with section 31 of the Strata Titles Act 1998		
 Registered Land Surveyor			 SURVEYING SERVICES MANAGER Council Delegate		
29/6/07 Date			29/6/2007 Date		
J535C Ref No			5672447 Ref No		
<b>GENERAL UNIT ENTITLEMENTS</b>					
LOT	UNIT ENTITLEMENT	LOT	UNIT ENTITLEMENT	LOT	UNIT ENTITLEMENT
101	182	301	190	501	217
102	175	302	198	502	209
103	175	303	190	503	209
104	159	304	178	504	198
105	186	305	206	505	221
106	186	306	206	506	221
107	137	307	171	507	209
108	147	308	163	508	209
109	140	309	155	509	209
110	126	310	140	510	206
111	142	311	128	511	147
201	190	401	209		
202	182	402	202		
203	182	403	202		
204	171	404	190		
205	190	405	217		
206	190	406	217		
207	155	407	202		
208	151	408	202		
209	144	409	202		
210	136	410	194		
211	155	411	198		
				TOTAL	10016



## GENERAL INFORMATION - NON-STATUTORY

---

**Status:** Under Review

**Reference**  
 PLANNA-HOB-2025-0352

**Address**  
 3 SANDY BAY RD HOBART TAS 7000

**Titles**  
 71169/1

**Application Reference**  
 PLN-HOB-2021-0710

**Application External Reference**  
 PLN-21-710

### Information Requested

#	Reason	Origin
1	Please provide declarations that you have informed all owners, joint or part-owners of all adjoining properties where development is proposed to occur of your intention to submit this application. The application will be considered valid once these declarations have been provided.	

### Response comments

**Response to Request for Further Information**

Ireneinc Planning & Urban Design notified the landowners of the following properties (which form part of the application) in writing in November 2021, when the application was initially lodged with Council.

- 1 Sandy Bay Road (Mantra) - notifications were sent to the body corporate and individual unit owners.
- 2A Heathfield Avenue (Tellyros Klonis Unit Trust)
- 5-7 Sandy Bay Road (Fragrance-TAS)

Further discussions prior to, and after notification were undertaken with the owners of 2A Heathfield Avenue and 5-7 Sandy Bay Road.  
 However, the project was put on hold for an extended period and the additional title documents and amended information confirming the inclusion of these properties was not provided to Council until the recently submitted RFI response (submitted in January this year).

### Documents

No Documents.



## GENERAL INFORMATION REQUEST

**Status:** Draft

**Reference**  
 PLANNA-HOB-2025-2217

**Address**  
 3 SANDY BAY RD HOBART TAS 7000

**Titles**  
 71169/1

**Application Reference**  
 PLN-HOB-2021-0710

**Application External Reference**  
 PLN-21-710

### Recipient

**Full Name**  
 Phil Gartrell

### General Information Requested

#	Reason	Origin
1	The following general request for information is a consolidation of the matters raised in the meetings with Council's Senior Development Engineer and Senior Cultural Heritage Officer.	
2	<p>Below is a summary of the concerns regarding the Traffic Impact Assessment (TIA) and the assessment against Clause E6.6.1 Car Parking Numbers of the <i>Hobart Interim Planning Scheme 2015(HIPS)</i>, for the proposed development at 3 Sandy Bay Road. The revised TIA must be at a minimum to be considered for assessment, provided in accordance with the Tasmanian Department of State Growth's Traffic Impact Assessment Guide (August 2020), and in line with the latest Austroads Guide to Traffic management Part 12: Integrated Transport Assessments for Developments.</p> <p>Under the Hobart Interim Planning Scheme 2015 (Code E6.0), the development would be required to provide: - 1 car parking space per room/serviced apartment = 33 spaces - 1 car parking space per 3m<sup>2</sup> of public bar floor area = 45 spaces for the rooftop bar. This equates to a total requirement of 78 car parking spaces. The proposal provides zero on-site parking spaces, representing a 100% deficiency against the statutory requirement. This does not include the existing floor area of the Masonic Hall proposed to be utilised.</p> <p>To address the performance criteria of E6.6.1, the TIA will need to consider:</p> <ul style="list-style-type: none"> <li>• The Car Parking Demand proposed by the development, considering all uses and components, including existing. Rationale for rates adopted must be supported by an evidence-based approach.</li> <li>• Parking survey of the surrounding area indicating the availability for the on-street car parking network to cater for the demand produced by the development, having regard for the type and use of the development proposed. This includes providing a survey with a sufficient number of days and variance relevant to the proposal in to indicate whether the demand can be accommodated. 300m radius maximum.</li> <li>• A formal Pick-up/Drop-off area, showing any changes to council road infrastructure required for the proposal, and including a traffic assessment of the location, including its demand, suitability, accessibility, and impact. Noting that the lack of formal Pick-up/Drop-off area for the development will likely result in informal stopping on Sandy Bay Road during the PM Peak period, creating a significant potential for disruption to traffic flow on Sandy Bay Road which is an arterial route through the city, this must be addressed. It is unclear from the documentation if the proposed Pick-up/Drop-off area will be feasible for the development and must thoroughly be investigated to find a suitable option. Note the Pick-up/Drop-off must be able to cater for the demand of the proposal, and depending on location may need a communications plan to advise users of the development of the access location.</li> <li>• Address the impact of the development on the surrounding road network, including any relevant modelling, with emphasis on Davey Street, Sandy Bay Road, and Wilmot Street, with reference to:                         <ul style="list-style-type: none"> <li>◦ Increased pressure on the already limited on-street car parking network.</li> <li>◦ Potential safety issues with vehicle manoeuvring for Pick-up/Drop-off or parking, with consideration of the likely travel path of users.</li> <li>◦ Consider that this would be the only hotel in the area to provide zero on-street car parking or vehicular access, and provide sufficient evidence to support this.</li> </ul> </li> </ul> <p>Additionally, investigations that may improve the traffic and access conditions:</p> <ul style="list-style-type: none"> <li>• Any alternative private parking that could be utilised to accommodate for car parking demand,</li> <li>• Adjacent land that may allow pick-up/drop-off on private land,</li> </ul> <p>Given the site constraints, clear zones on both sides of the road frontage and no vehicle access provided, a sound and well supported traffic assessment will be required to address the performance criteria. If you have any queries on the above, please get in contact, and happy to discuss with the Traffic Engineer.</p>	

3	<p><b>1 - Building height and renders</b></p> <p>Consideration was raised regarding the building height in relation with the nearby existing and approved properties, and siting of the building height in context with the topography of the immediate locality. You advised that you would speak with your clients regarding a reduction in height (although not quantified at the meeting today a reduction could be 'up to 2 floors').</p> <p>It was clarified that the cross-hatching 'roof structure' on Site Plan DA00 and Photomontage DA51 were an annotation error – no roof structures forward of the rooftop level are proposed.</p> <p>Amended 3D renders would be helpful to understand the siting of the proposed addition in relation with the existing building, particularly around the side laneways and over the existing buildings. It was agreed that a render that omits the existing buildings on either side of 3 Sandy Bay Road may assist in showing the addition.</p> <p>It was discussed that the red dotted lines annotated as 'new building over' on Floor Plans – Ground and Level 1 DA05 do not align with the angular building walls of the front building corners. However, it was clarified that this is likely an annotation error.</p> <p><b>2 - Demolition and works within adjoining site</b></p> <p>It was clarified that the structural trusses required modifications and partial demolition of the existing roof.</p> <p>Clarification is required regarding the proposed works and column structures on the 620mm ROW: burdening the allotment of the Mantra One Hotel, benefiting the allotment of the Telstra Exchange building and part of the subject property (3 Sandy Bay Road). The architectural plans indicate that works would be within this ROW. It would be beneficial to understand if this ROW would be partially or fully extinguished. I will need to refer this information to Council's Manager Surveying Services as he may include conditions or advice in the planning permit.</p> <p><b>3 - Connection to the roof</b></p> <p>It was clarified that 'Level 2 – Amenities and Plan / Equipment Floor' required partial demolition of the existing roof. Level 2 would be suspended above the existing roof with finished floor level at 31.09 RL. It would be helpful if cross sections be provided along the fire staircases and 'amenity' area to understand how the floor level works in relation with the existing roof. It was explained that the white annotation on Plan – Level 2 DA06 is the plant and equipment zone.</p> <p>Additional longitudinal sections would assist either side of the ridge line i.e. from Sandy Bay Rd to the rear</p> <p>It was noted that the architectural elements forward the balustrade of the front balconies are moveable mesh / screening structures. It would be helpful if additional 3D renders included these features directly above the existing roof.</p>
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**Documents to be Provided**

No Documents.

From: Richard Crawford | Architects Designhaus <[richard@architectsdesignhaus.com](mailto:richard@architectsdesignhaus.com)>  
Sent: Friday, 11 July 2025 10:08 AM  
To: Christopher Phu <[christopher.phu@hobartcity.com.au](mailto:christopher.phu@hobartcity.com.au)>  
Cc: Phil Gartrell <[phil@ireneinc.com.au](mailto:phil@ireneinc.com.au)>; Mike | Architects Designhaus <[mike@architectsdesignhaus.com](mailto:mike@architectsdesignhaus.com)>  
Subject: 3 Sandy Bay Road- response to email request notes

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Good morning Christopher,  
In response to your email notes I have attached the drawings and some 3D images to more easily comprehend the building details  
In regards to the items:  
Building Height- As indicated the building height relates directly to the number of floors to allow the development to be economically viable given the complexities of the existing building largely occupying the 'whole of site'  
The advice from the client is the number of floors are required  
We have removed the hatching from DA00 and DA51 side laneway diagram  
DA06 reflects the existing roof extent that remains and the low value area where the amenities are located  
DA55 indicates the new floor hung from the new structure for the plant  
We have attached 3D images DA57 which clarifies the relationship between the new and old building  
DA56 indicates the placement of columns on the ROW side of the Freemasons Building being within the building boundary. The columns will not fully extinguish the benefit  
Rather than sections we have attached aerial visualisation but if this does not adequately reflect the demolition we can provide additional information  
DA 57 indicates the sliding screens at balcony level and that the extension between the lower balcony and the existing roof the screens are fixed and are static

Please contact me if you have any further queries

Kind Regards,

Richard Crawford FRAIA  
Director  
(BArch) RMIT. Accredited Building Practitioner No CC968C





Job No. 1802  
12 April 2021

## Architects Design Statement 3 Sandy Bay Road Hobart Adaptive reuse of Masonic Temple

3 Sandy Bay Road was purpose designed for the Freemasons by Launceston architect Lauriston Crisp in 1937 and was constructed in 1938-39

The building is symbolic of the Ancient Order of Freemasons and is largely in its original condition. The layout of the 2 storey building has 2 lodge rooms, 2 supper rooms and all the necessary office/amenities accommodation for the original intended use.

The building façade is Inter-War Stripped Classical and has a stark functional appearance from Sandy Bay Road with discreet embellishments including classical columns. The cream brick features horizontal banding with vertical accentuation. The cream brick returns along the western and eastern sides to the entry doors. The remainder of side and rear walls is red brick.

The building constructed in 1938-39 was purpose built by the Masons as their new Hobart Masonic Temple after their Murray Street building became too small. As with many organisations numbers of members have waned and whilst there has been a small increase in membership the client recognized for the site to be financially sustainable long term there would need to be an additional income stream to maintain the use as a Masonic Temple and allow regular maintenance into the future.

Our strong view is that an essential part of maintaining the context of the building is that the current Masonic presence remains. This also coincides with the wishes of the current Tasmanian Freemasons broadly.

In the preliminary discussions with the client many alternative possibilities were considered including: retail, hospitality, commercial offices and accommodation. Various considerations were investigated, including using the existing building only and/or adding floors above the existing building, the latter proved to be the most viable. The preferred proposal was for a boutique accommodation of 38 suites/rooms, as a commercial minimum, serviced by a lift located on the ROW side of the building.

After much discussion it was considered detrimental to use the existing building for another use due to its high heritage integrity both externally and internally which remain in their original state with original fittings and fixtures, a rare intact example of its time in the Tasmanian context.

The adding of floors was appropriate so long as the new build was subservient and sympathetic to the existing building and we were able to maintain and not adversely affect the high heritage value components/areas of the Masonic Hall. Where demolition is required it has been confined to those areas of low heritage value, allowing the existing building to be readapted to current standards and requirements.

The design responds to the importance of the existing building by:

- The new build is set back from Sandy Bay Road by 7 metres to ensure the existing building maintains its predominance to Sandy Bay Road and surroundings. This setback also aligns with the existing buildings secondary façade walls setback to the street.
- By setting the top most floor area back from the front and rear facades by a further 6 metres it recedes from Sandy Bay Road
- The new façades design responds to the proportions and alignments of the existing front façade with screening to break down the mass of the modern façade
- The roof top bar uses strong symmetry similar to the Masonic square and compass, a clear sky dome similar to the circular entry hall on the ground floor of the existing building and echoes some of the Masonic iconography already used in the existing building fabric.



- Similarly the east and western façade walls reflect the Masonic compass iconography in the cladding fenestration to reflect the subtle and understated use of Masonic symbolism in the existing building fabric.
- The materiality of screening and cladding seek to compliment and reference the existing built heritage fabric and the original custom fixtures.

The above items help create a new building that is subservient and sympathetic to the existing building whilst having a strong design mantra of its time, not trying to mimic an earlier architectural style.

The design responds to the importance of the existing urban context by:

- Limiting the overall building height to correspond with the height of the neighbouring 'Telstra Building' at 2 Healthfield Avenue to create a more consistent built line to the immediate precinct.
- By infilling the 'built gap' between the neighbouring lots, 1 and 5 Sandy Bay Road, the urban built edge to St Davids Park is reinforced to Sandy Bay Road, Davies Street and Salamanca Place. Giving a more clearly defined built surrounding to this important public space.
- The Façade fenestration of the proposed upper additional are broken in an arrangement of solid and transparent elements (positive and negative) in an abstract response to greater Hobarts surrounding urban hillscares of solid built elements (buildings) and open areas of vegetation (gardens/parks).
- The façade balconies overlooking the civic public streets, parks and neighbouring buildings are screened to allow part to full enclosure and reduce the sense of overlooking and visual intrusion to civic and private open space.
- The full height boundary walls to east and west facades block any visual and acoustic intrusion to the neighbouring accommodation and residential buildings in regards the public functions for level 8 rooftop bar and terrace. (5-7 Sandy Bay Road, approved future use)

The above means the new building takes into consideration and respects the sites surrounding built context and amenity to neighbouring sites and public open space.

As the existing building is located on the rear and side boundaries of the site, the new structure is located above the existing parapet walls. The mechanical plant is located in the space/void created between the existing roof and the floor of the first accommodation level to simplify and keep the roof clear of extrusions, reduce overall building height and visual impact from the above surrounding hills.

The proposed redevelopment of the Masonic Hall 3 Sandy Bay Road is an opportunity for renewal both for the site and the Tasmanian Freemasons as owners. The new public use on site through tourism accommodation will reactivate the site and the broader urban surrounds, giving a quality mid scale, bespoke option to Hobart visitors, it also allows a sustainable future for the existing Masonic Hall building and its owners whilst they renew and continue to serve the community at large.

Richard Crawford  
**architects designhaus**

3 SANDY BAY ROAD, SANDY BAY



ireneinc & smithstreetstudio  
PLANNING & URBAN DESIGN

## 3 SANDY BAY ROAD, SANDY BAY

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### Development Application to Hobart City Council

Last Updated - December 2024

Author - Phil Gartrell

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**ireneinc** PLANNING & URBAN DESIGN

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## 1. INTRODUCTION

Ireneinc Planning & Urban Design has been engaged by Architects Designhaus, to prepare a planning report to accompany a development application for the site at 3 Sandy Bay Road.

### 1.1 THE SITE

The subject site is located at 3 Sandy Bay Road, with the following certificate of title:

- CT 71169/1 - Identified as the 'Hobart Masonic Hall'

The site has an area of approximately 658.2m<sup>2</sup> with frontage to Sandy Bay Road and supports the existing Hobart Masonic Hall.

The application also requires works on several adjoining properties due to structural requirements (support columns) that extend marginally beyond the boundary and minor stormwater works. Therefore, the following properties also form part of the application.

- CT 151831/0 - 1 Sandy Bay Road (Mantra) - which includes approximately 55 individually owned apartments managed under a Strata Corporation.
- CT 106816/1 & CT 51956/7 - 5-7 Sandy Bay Road; and
- CT 51956/8 - 2A Heathfield Avenue

The following figure describes the location of the site and properties listed above.



Figure 1: Subject site shown in red. 1 Sandy Bay Road shown blue. 2A Heathfield Avenue shown purple. 5-7 Sandy Bay Road shown orange (inclusive of 4 Heathfield Avenue) (source: [www.theLIST.tas.gov.au](http://www.theLIST.tas.gov.au) © the State of Tasmania)

## 1.2 SITE SURROUNDS

The site is located opposite St David's Park and adjoins the former Conservatorium of Music building to the east and the Mantra building to the north, on the corner of Sandy Bay Road and Davey Street. The site also adjoins two existing cottages to the rear located at 2A Heathfield Avenue and 5 Heathfield Avenue (which now forms part of 5-7 Sandy Bay Road).

A large portion of the block bounded by Hampden Road, Davey Street, Sandy Bay Road and Wilmot Street is located within the H2 Heritage Precinct, as is the block to the east on the south-eastern side of Wilmot Street. These blocks are characterised by generally narrow streets and access ways, with buildings generally built close to side and/or front boundaries which is a relatively consistent theme among most of Hobart's Heritage Precincts.

## 1.3 URBAN DESIGN ANALYSIS

### 1.3.1 EXISTING BUILT FORM AND CONTEXT

The site sits within the natural amphitheatre rising from Sullivans Cove to the Macquarie Street ridge and upper Davey Street ridgeline, which forms part of the visual context of the site.

The primary arterial role of Sandy Bay Road is supported by larger building forms of a more commercial scale, whilst the radiating side and parallel streets contain more domestic scale of dwellings which are either retained for visitor accommodation, residential, or adapted to other uses. The site also forms an edge to St David's Park, forming a built wall of building frontages with buildings generally occupying the full width of lots, and zero or minimal setback. Whilst there is a degree of consistency with building 'frontages' addressing the street and overlooking the park, there is also a variety in building form, height, appearance, architectural style and aesthetic.

Notable built form considerations within the existing local, urban context include:

- **Block structure:** strong pattern of urban blocks, in traditional 'perimeter block' form, with buildings filling blocks of broadly rectilinear form, building frontages addressing the streets.
- **Urban grain:** the urban grain is notable for the relatively dense pattern of urban blocks set in a connected network of streets that are broadly arranged in grid iron pattern in the Hobart city centre / CBD, with some arterial routes such as Sandy Bay Road tapering off to link to areas beyond the city centre.

Within this a finer lot pattern is evident in the built form, even where lots have been amalgamated.

- **Building form:** a wide variety of building forms are present in the local urban context in and around the application site. Variety of built form is related to the great mixture of land uses, and the evolution of buildings within the city structure (of blocks, streets and spaces) over a long period of time, with a great variety of building ages and architectural styles.

This pattern of variety is a common feature of cities of Hobart's age and creates the diversity of individual building forms and appearance cumulatively contribute to the richness in character of the city.

Streets on three sides of the park (Davey Street, Salamanca Place and Sandy Bay Road) share some common characteristics including:

- Consistency of building frontages addressing the street and overlooking the park;
- Variety of building forms, heights, materiality and architectural style;



## 1.3.2 PROPOSED BUILT FORM AND CONTEXT

The proposed built form has been designed to complement the heritage context of the existing Masonic Hall, whilst also acknowledging the development recently approved (but not constructed) at 5-7 Sandy Bay Road.

The heritage status of the Masonic Hall has also been key factor in approaching the design of the proposed building. This is evidenced by the 7m setback of the new building from the frontage to Sandy Bay Road, which ensures the retention of the architectural features and prominence of the building within the streetscape context.

This setback is evident in the 3D render below.



Figure 3: 3D render of the proposal from Davey Street toward the intersection between Sandy Bay Road - Note: Render shows previously approved DA at 5-7 Sandy Bay Road (source: Architects Designhaus)

The proposed building/extension contributes to the variations in built form and the evolving nature of the block. Its appearance within the streetscape, whilst setback, provides a progressive infill between 5-7 Sandy Bay Road and the Mantra, taking into account the context of the surrounding buildings in the locality that also demonstrate a variation to scale, massing, height and appearance.

The following diagram illustrates the massing of the proposed building, in the context with existing and approved buildings within the block.



Figure 4: Massing diagram, illustrating both proposed, approved and existing buildings within the block (source: Ireneinc)

The figure above demonstrates that the proposed building/extension is not inconsistent with the existing/approved massing of buildings within the block, whilst also retaining the variation in built form/height evident within the streetscape. The proposed and approved buildings within the subject block, combined with the larger built form now approved on the opposing site of St. David’s Park (Parliament Square development) serves to frame the park, promoting a strong built edge surrounding St. David’s Park.

The provision of a building of a similar height as the Mantra, would result in a level of uniformity across an extended section of streetscape, which is not consistent with the variations in building form and scale evident elsewhere within Hobart. The variation in scale is also demonstrated from Wilmot Street, as shown below.

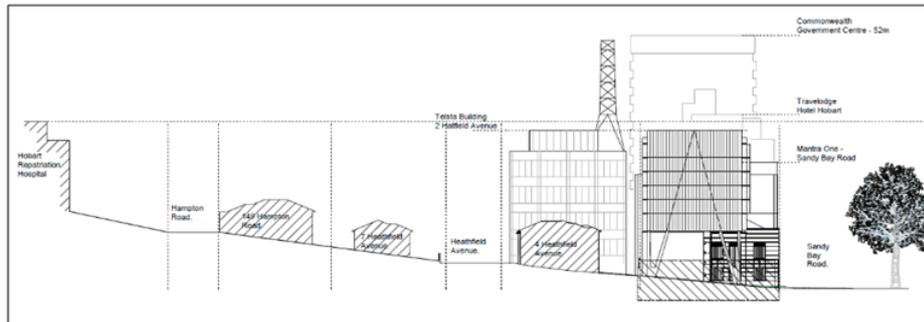


Figure 5: Section view of Wilmot Street, illustrating the relationship between the cottages along Heathfield Avenue, the topographical change in the block and the larger built forms on the corner of the block (note: the section does not show the approved development at 5-7 Sandy Bay Road (source: Architects Designhaus).

## 2. PROPOSAL

---

The application seeks a partial change of use to the existing Masonic Hall, along with alterations and extensions catering for 38 x short-stay apartments and a roof-top bar. The short-stay apartments and bar will be accommodated across seven additional levels. The proposal also includes internal and external modifications to the rear of the existing building to facilitate the provision of a lobby and reception, along with structural elements to support the new upper levels.

The structural works will require new columns which will straddle the existing building on the south-eastern and north-western boundaries. Due to the minimal setback of the existing building from these boundaries, a small portion of the columns will sit partially within the adjoining properties at 5-7 Sandy Bay Road and 2A Heathfield Avenue.

The short-stay apartments are comprised of 33 x two and three bedroom apartments across levels 3-7. Level 8 will provide a rooftop bar and outdoor terrace, which will be open to both guests and the public.

The proposal will have a maximum height of approximately 34.3m above natural ground level. The proposed upper floors have been carefully positioned and setback from Sandy Bay Road to minimise impact on the heritage characteristics of the existing building. This ensures the Masonic Hall retains its presence within the streetscape. The contemporary design of the upper floors and material palette complement the existing built form and whilst the upper levels present additional height, the setback and materiality allow the proposal to appear as a recessive element in the streetscape.

The building will be clad with metal 'brass' screening and glazing, which achieves a clearly contemporary appearance, whilst the brass colour references the existing sandstone materiality of the existing Masonic Hall. The setback of the proposed building from the frontage ensures that the notable heritage characteristics and presence of the Masonic Hall is retained in full.

Due to the site constraints, it is not possible to provide on-site parking. Given the central location of the site in proximity to public transport and key social-cultural event spaces and the CBD, the car parking demand is anticipated to be very low. The accompanying TIA demonstrates that similar hotel/short-stay developments where on-site parking is provided, the total number of spaces is well below that required by the planning scheme. This is not unusual for developments located within inner-city / urban areas where walkability is high and other transport modes are available.

It is expected that drop-off/pick-up will be the primary mode used by guests to travel to/from the site. To cater for this, there are numerous existing zones within the surrounding road network used for this purpose and an option to establish another dedicated zone is outlined in this submission, accompanying cover letter and TIA.

### 3. PLANNING SCHEME PROVISIONS

The area is within the *Hobart Interim Planning Scheme 2015*, and the following provisions are relevant to the site and proposed use and development.

#### 3.1 URBAN MIXED USE ZONE

The subject land is zoned Urban Mixed Use (grey) as is the immediately surrounding area.



Figure 6: Zone plan and site location (source: [www.theLIST.tas.gov.au](http://www.theLIST.tas.gov.au) © the State of Tasmania)

#### 3.1.1 ZONE PURPOSE

The Purpose Statements for the zone are:

##### 15.1.1 Zone Purpose Statements

15.1.1.1 - *To provide for integration of residential, retail, community services and commercial activities in urban locations.*

The proposal will provide additional accommodation options for visitors within close proximity to the CBD and other key socio-cultural sites such as Salamanca Place and wider Sullivan's Cove.

15.1.1.2 - *To encourage use and development at street level that generates activity and pedestrian movement through the area.*

The existing Masonic Hall is heritage listed and will be retained in full. Given that the proposed extensions will be located well above street level, the proposal will not have a substantial street level presence.

However, patrons and guests will access the hotel lobby from street level, thereby generating additional pedestrian movement consistent with the purpose of the zone.

*15.1.1.3 - To provide for design that maximises the amenity at street level including considerations of microclimate, lighting, safety, and pedestrian connectivity.*

As outlined above, access to the proposed hotel will be provided along the northern elevation via the existing laneway between the building and the adjoining Mantra building. This allows the existing Masonic Hall to be retained as is, along with the associated activities undertaken within.

Given that the changes proposed at ground level are minimal, the overall design of the building at street level will remain largely as is. Therefore, there will be no anticipated impact on amenity as a result of the design.

*15.1.1.4 - To ensure that commercial uses are consistent with the activity centre hierarchy.*

No commercial uses are proposed.

*15.1.1.5 - To ensure development is accessible by public transport, walking and cycling.*

The site is within close proximity to the CBD and key cultural areas such as Sullivan's Cove and Battery Point. Sandy Bay Road is a primary public transport corridor and the close proximity of the site to key areas within the CBD ensures that walking and cycling are viable alternatives to vehicle transport.

*15.1.1.6 - To provide for a diversity of uses at densities responsive to the character of streetscapes, historic areas and buildings and which do not compromise the amenity of surrounding residential areas.*

The building form ensures that the development scale suitably responds to the variable density evident in surrounding properties, by allowing a contrast in scale and built form at street level. The proposed roof-top bar will operate in a manner respectful of nearby residential uses. The location of the bar on-top of the building, along with associated acoustic screening, ensures that any noise emissions can be appropriately managed.

Hours of operation can be conditioned to further manage noise emissions if necessary.

*15.1.1.7 - To encourage the retention of existing residential uses and the greater use of underutilised sites as well as the reuse and adaptation of existing buildings for uses with a scale appropriate to the site and area.*

The proposed extension to the existing building allows for adaptation whilst retaining the existing character and functions of the Masonic Hall. The scale is not inappropriate to the area, with two recently approved residential apartment buildings at 9 Sandy Bay Road and 5-7 Sandy Bay Road.

The proposed development will replace an existing gap in the streetscape, whilst still maintaining the existing variations in the streetscape by setting the building back from the frontage and providing a slightly higher built form as a result.

*15.1.1.8 - To ensure that the proportions, materials, openings and decoration of building facades contribute positively to the streetscape and reinforce the built environment of the area in which the site is situated.*

The built form of the proposal will be setback approximately 7m from the frontage to Sandy Bay Road and will be clad with contemporary materials including brass mesh screening to ensure the extension reads as a contemporary addition to the site, whilst referencing the colour palette of the existing Masonic Temple.

The setback of the proposed upper-level extension allows the key heritage character of the Masonic Hall to remain prominent as the primary façade within the streetscape, which reinforces the existing built form which will ensure a degree of harmony with the existing urban fabric, whilst also presenting as a new, contemporary building.

*15.1.1.9 - To maintain an appropriate level of amenity for residential uses without unreasonable restriction or constraint on the nature and hours of commercial activities.*

The proposed development does not result in any restrictions on existing commercial activities in surrounding buildings, predominately to the south-west along Hampden Road.

*15.1.1.10 - To ensure that retail shopping strips do not develop along major arterial roads within the zone.*

The application does not create or contribute to development of a retail shopping strip.

There are no Local Area Objectives or Desired Future Character Statements for the Zone.

3.1.2 USE STATUS

The existing Masonic Hall is used primarily for meetings/events directly associated with the functions of the Freemasons. This use falls under the Community Meeting and Entertainment classification, which is a permitted use in the zone and no changes are proposed.

However, portions of the existing building and proposed additional levels will require a partial change of use to incorporate the following.

**Visitor Accommodation**

*use of land for providing short or medium term accommodation for persons away from their normal place of residence. Examples include a backpackers hostel, bed and breakfast establishment, camping and caravan park, holiday cabin, holiday unit, motel, overnight camping area, residential hotel and serviced apartment.*

The visitor accommodation component applies to the serviced apartments and is a discretionary use within the zone.

**Hotel Industry**

*use of land to sell liquor for consumption on or off the premises. If the land is so used, the use may include accommodation, food for consumption on the premises, entertainment, dancing, amusement machines and gambling. Examples include a hotel, bar, bottle shop, nightclub and tavern.*

The roof-top bar will operate independently from the visitor accommodation and is a discretionary use within the zone.

3.1.3 USE STANDARDS

The use standards in the zone apply to non-residential use and are therefore relevant to the proposed use.

**15.3.1 Non-Residential Use**

*Objective: To ensure that non-residential use does not unreasonably impact residential amenity.*

**SCHEME REQUIREMENTS**

**A1**

*Hours of operation must be within:*

- (a) 7.00am to 9.00pm Mondays to Fridays inclusive;*
- (b) 8.00am to 6.00pm Saturdays;*
- (c) 9.00am to 5.00pm Sundays and Public Holidays;*

*except for office and administrative tasks or visitor accommodation.*

*P1 - Hours of operation must not have an unreasonable impact upon the residential amenity through commercial vehicle movements, noise or other emissions that are unreasonable in their timing, duration or extent.*

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**RESPONSE**

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No changes to any existing community meeting and entertainment use is proposed.

The proposed rooftop bar will operate between 6:00am to 12:00am, 7 days per week. During the morning period, the bar will be used to provide breakfast only, so music and patron noise generated is expected to be significantly reduced.

A response to the performance criteria is required.

**P1**

As outlined in the accompanying noise report, the rooftop bar will be located above the adjoining Mantra Hotel and the approved, but not yet constructed, residential apartment building at 5-7 Sandy Bay Road.

Noise emissions from patrons and background music within the building are predicted to be below 20 dBA at all nearby residences. Noise emissions from patrons on the external patio areas are predicted to be between 28 and 38 dBA at the boundaries of site nearest the neighbouring buildings. These levels are below both the day and night time criteria and measured, and therefore comply with the Acceptable Solutions criteria under clause 15.3.1-A2 of the Scheme.

---

*A2 - Noise emissions measured at the boundary of the site must not exceed the following:*

- (a) 55 dB(A) (LAeq) between the hours of 8.00 am to 6.00 pm;*
- (b) 5dB(A) above the background (LA90) level or 40dB(A) (LAeq), whichever is the lower, between the hours of 6.00 pm to 8.00 am;*
- (c) 65dB(A) (LMax) at any time.*

*Measurement of noise levels must be in accordance with the methods in the Tasmanian Noise Measurement Procedures Manual, issued by the Director of Environmental Management, including adjustment of noise levels for tonality and impulsiveness.*

*Noise levels are to be averaged over a 15 minute time interval.*

---

*P2 - Noise emissions measured at the boundary of the site must not cause environmental harm.*

---

**RESPONSE**

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As outlined in the accompanying noise assessment, the noise emissions are anticipated to be within those specified under A2.

The proposal complies with A2.

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*A3 - External lighting must comply with all of the following:*

- (a) be turned off between 10:00pm and 6:00 am, except for security lighting;*
- (b) security lighting must be baffled to ensure they do not cause emission of light into adjoining private land.*

*P3 - External lighting must not adversely affect existing or future residential amenity, having regard to all of the following:*

- (a) level of illumination and duration of lighting;*
  - (b) distance to habitable rooms in an adjacent dwelling.*
- 

**RESPONSE**

---

All external lighting, aside from security lighting will comply with A3.

---

*A4 - Commercial vehicle movements, (including loading and unloading and garbage removal) to or from a site must be limited to within the hours of:*

- (a) 7.00am to 5.00pm Mondays to Fridays inclusive;*
- (b) 8.00am to 5.00pm Saturdays;*
- (c) 9.00am to 12 noon Sundays and Public Holidays.*

*P4 - Commercial vehicle movements, (including loading and unloading and garbage removal) must not result in unreasonable adverse impact upon residential amenity having regard to all of the following:*

- (a) the time and duration of commercial vehicle movements;*
  - (b) the number and frequency of commercial vehicle movements;*
  - (c) the size of commercial vehicles involved;*
  - (d) the ability of the site to accommodate commercial vehicle turning movements, including the amount of reversing (including associated warning noise);*
  - (e) noise reducing structures between vehicle movement areas and dwellings;*
  - (f) the level of traffic on the road;*
  - (g) the potential for conflicts with other traffic.*
- 

**RESPONSE**

---

Commercial movements associated with the bar and servicing for the hotel will be required, as will garbage removal.

It is anticipated that these movements will occur during the hours specific under A4.

#### **15.4.6 Non-Residential Use**

*Objective: To ensure that outdoor storage areas for non-residential use do not detract from the appearance of the site or the locality.*

**SCHEME REQUIREMENTS**

---

*A1 - Outdoor storage areas for non-residential uses must comply with all of the following:*

- (a) be located behind the building line;*
-

- (b) all goods and materials stored must be screened from public view;  
(c) not encroach upon car parking areas, driveways or landscaped areas.

*P1 - Outdoor storage areas for non-residential uses must satisfy all of the following:*

- (a) be located, treated or screened to avoid unreasonable adverse impact on the visual amenity of the locality;  
(b) not encroach upon car parking areas, driveways or landscaped areas.

---

**RESPONSE**

---

No outdoor storage areas are proposed.

3.1.4 DEVELOPMENT STANDARDS

**15.4.1 Building Height**

*Objective: To ensure that building height contributes positively to the streetscape and does not result in unreasonable impact on residential amenity of land in the General Residential Zone or Inner Residential Zone.*

---

**SCHEME REQUIREMENTS**

---

*A1 - Building height must be no more than:  
10m.*

*P1 - Building height must satisfy all of the following:*

- (a) be consistent with any Desired Future Character Statements provided for the area;  
(b) be compatible with the scale of nearby buildings;  
(c) not unreasonably overshadow adjacent public space;  
(d) allow for a transition in height between adjoining buildings, where appropriate;

---

**RESPONSE**

---

The proposal complies with P1 as follows:

- (a) there are no desired future character statements for the zone.  
(b) The proposed extension will sit on the rear portion of the existing Masonic Hall and will increase the total height of the building to approximately 34.3m.

---

**Compatibility and Scale**

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When considered within the wider townscape, the proposal is compatible with the immediately adjoining buildings and the block in which it is situated accommodates a range of building height and scale.

The Tribunal decision *9 Sandy Bay Road Pty Ltd v Hobart City Council & Ors [2017] TASRMPAT 19*, states the following:

*... To be compatible is to be consistent or congruous with that which comparison is required to be made. The Tribunal holds that to be "compatible" requires that the building height be capable of co-existing with the scale of nearby buildings.<sup>1</sup>*

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<sup>1</sup> *9 Sandy Bay Road Pty Ltd v Hobart City Council & Ors [2017] TASRMPAT 19, paragraph 52, p 11.*

The decision also states:

*The Tribunal defined the term 'compatible' in two recent decision: Henry Design & Consulting v Clarence City Council<sup>12</sup> and Flood v George Town Council<sup>13</sup>. In Henry Design, the Tribunal held at [50] that 'compatible' meant "not necessarily the same... but at least similar to, or in harmony or broad correspondence with the surrounding area".<sup>2</sup>*

The terms 'similar to, or in harmony or broad correspondence with the surrounding area' suggests that when considering whether a building is 'compatible', the decision should not be solely based on the relationship between one specific building but rather take into account the wider built form in making a sound determination.

The performance criteria refer to compatibility with the scale of 'nearby buildings', meaning that consideration of just one building between two existing larger built forms should not be the sole factor in determining height compatibility and transition.

To determine the compatibility of the proposed extension, in terms of scale, with that of nearby buildings, two recently approved developments at 9 Sandy Bay Road and 5-7 Sandy Bay Road have been considered, as well as the existing Telstra exchange building and Mantra building. It should also be noted that the determination of what is similar to, in harmony or broad correspondence, in terms of building height, should not be restricted to an assessment or comparison of pure elevation.

For example, the proposed building will sit approximately 4.5 storeys higher than the existing Conservatorium of Music building. However, when viewed from various locations around the subject site, the 7m setback of the proposed building substantially reduces its visual scale and bulk, thereby reducing the perceived height of the building, allowing the additional height proposed to occur without that additional height appearing 'out of scale' with nearby / adjoining buildings.

- **5-7 Sandy Bay Road (Approved Development)**

The building at 5-7 Sandy Bay Road was approved with a height of approximately 27m once constructed.

Discussions with Council have indicated that the proposed development at 3 Sandy Bay Road should not rely on the approved built form at 5-7 Sandy Bay Road, as it is not yet under construction.

The additional height is a result of the need to ensure the proposed hotel/visitor accommodation is commercially viable, given that the first three (3) storeys are occupied by the existing Masonic Hall, aside from the proposed ground floor lobby and reception at the rear. Maintaining the height of the adjoining approved development at 5-7 Sandy Bay Road would substantially reduce the number of rooms from 33 to 19.

The higher building form also allows for a more appropriate vertical separation of the roof-top bar from adjoining buildings, thereby reducing noise emissions. The proposed building represents a minor transition in height between the approved development, before the streetscape steps down to the Mantra at 1 Sandy Bay Road.

- **5-7 Sandy Bay Road - Former Conservatorium of Music**

The existing building at 5-7 Sandy Bay Road is the former Conservatorium of Music, which has now moved to Campbell Street. As a result, the building is currently vacant and a recently approved

<sup>2</sup> 9 Sandy Bay Road Pty Ltd v Hobart City Council & Ors [2017] TASRMPAT 19, paragraph 52, p 11.

permit has been granted to allow the redevelopment of the site for residential apartments, with a maximum height of 27m.

The existing conservatorium and associated buildings which also have frontage to Wilmot Street, step up in height toward the north-east to the junction with Davey Street, with the conservatorium sitting approximately 1.5 storeys lower than the Mantra (excluding the lift overrun).

The Conservatorium has a maximum height of approximately 19.7m.

The proposed development at 3 Sandy Bay responds primarily to the height of the Mantra Building and Telstra Exchange and when viewed in this context, the proposal sits relatively comfortably with the height of these adjoining buildings.

- **Mantra Building & Telstra Exchange Building**

As specified in section 2 of this report, the block is characterised by large built forms (i.e. Telstra and Mantra buildings). The Mantra sits on the corner of Davey Street and Sandy Bay Road and has a height of approximately 29m (if taking into account the raised signage section). The building directly behind the Mantra is the Telstra Exchange Building which also has a roof-top tower structure and sits at a height of approximately 17.2m but appears larger where the natural ground level begins to rise.

The variation in scale, coupled with the topography is evident in the section overleaf, where the Telstra Exchange building sits at a similar height. The section also illustrates the existing Conservatorium of Music building, which allows consideration of the proposal in the event that the approved development at 5-7 Sandy Bay Road were not constructed.

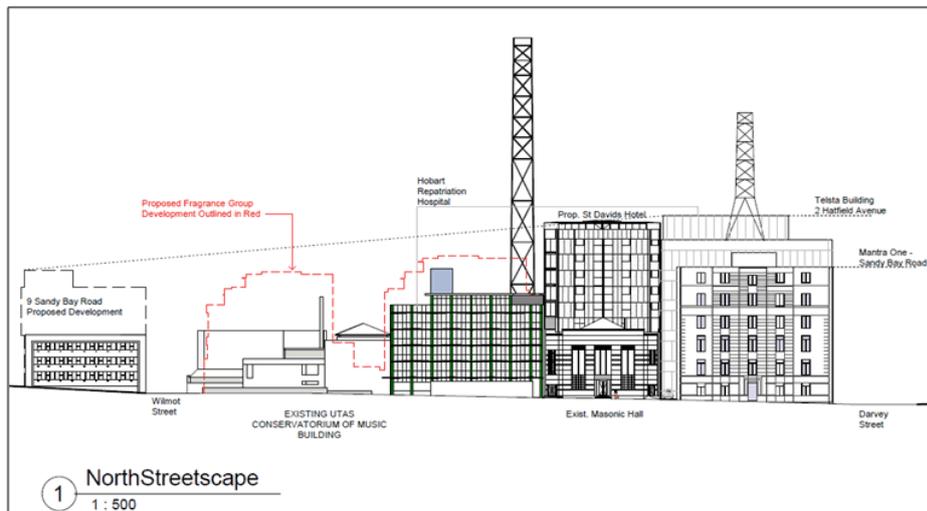


Figure 7: Streetscape elevation along Sandy Bay Road (source: Architects Designhaus)



**Figure 8: Photomontage of the proposed building from Sandy Bay Road (red areas illustrate the extent of the proposed building behind existing Conservatorium of Music) (source: Architects Designhaus)**

This image illustrates the substantial effect the setback of the proposed building from the frontage has on the perceived scale of the building, with only a relatively small section of the building visible behind the existing Conservatorium building. Considered within the Sandy Bay Road streetscape, these height differences are compatible for the following reasons:

- The Masonic Hall currently represents a gap in the streetscape, when considered between the existing Mantra Hotel, existing Conservatorium building at 5-7 Sandy Bay Road (and approved development). As a result, the site is capable of supporting a higher built form to the rear, whilst ensuring the heritage characteristics of the Masonic Hall remain prominent within the streetscape.
- The height and recess of the proposed extension from the frontage creates variation to the built form within the block, reinforcing the prevalence of higher built forms on the corner of Sandy Bay Road and Davey Street.
- The proposed development does not represent a substantial increase in height, being only 3 storeys higher than the existing Mantra building, with the built form then stepping down in height to the existing Conservatorium building.

As outlined under section 2, the built form across the city and city fringes is constantly evolving which allows the city to take shape and take on a distinctive character.

The proposal achieves a variation in building height and form, which is consistent with the existing built character of Hobart, demonstrating highly varied building forms and heights, creating a layered or terracing effect when viewed from the lower slopes of Sullivan's Cove, the Derwent River and approached to the city. This layering and variation is enhanced by the setback of the

building from the lower levels of the Masonic Hall, creating depth and reducing the perceived bulk/form of the proposed extensions.

Whilst the site itself is not within a heritage precinct, it is individually listed. In order to address this, the primary consideration has been to provide a design response that addresses and protects the heritage characteristics of the Masonic Hall and surrounding properties, whilst presenting a contemporary building form that is recessed from the street. This design intent is similar to that proposed as part of the approved and constructed Movenpick Development, which represents the tallest building in the CBD. The upper levels of that building are substantially setback from the frontage, allowing the lower adjoining heritage forms within Elizabeth Street to remain the most prominent from a human streetscape scale.

When considered in a wider context, the form of the building largely responds to the transition of buildings along Sandy Bay Road, with the existing site conditions representing a void in the streetscape. The introduction of the proposed additional levels fills this void and when combined with contemporary materials which respect the colour and finish of the existing Masonic Hall, the proposal provides a rich contribution to the immediate streetscape, adding to the existing variations in height, scale and setback creating a far more layered and dynamic streetscape elevation.

It is important to acknowledge that if the Masonic Hall were to remain as is, the visual prominence of the Hall within the streetscape is at risk of being lost. The proposed extensions bridge the existing gap across the upper levels whilst also ensuring the existing Masonic Hall remains prominent and represents a new lease of life for the existing building. The proposal will also encourage greater public interaction with the building, by virtue of having patrons and members of the public accessing the building.

The following diagram illustrates how the building sits within the existing streetscape, along with an indication of the built form proposed as part of a new application at 5-7 Sandy Bay Road.



Figure 9: 3D modelling of the proposed development from the corner of Davey Street - noting that the telecommunications tower which has been removed)

Whilst there is a difference in height, the proposed development responds appropriately to the Mantra Building and Telstra Building behind, consolidating the extent of larger buildings in the north-eastern corner of the block.

In Hobart City Council's decision to approve the development at 5-7 Sandy Bay Road, it was specifically noted that the Masonic Hall represented a visibly lower building form. As a result, Council raised the question of how appropriate a transition to the Masonic Hall would be, given that the existing Mantra and Conservatorium are generally unarticulated building forms and any attempt to transition to the Masonic Hall would be tokenistic. It is understood that a new application has been lodged for 5-7 Sandy Bay Road, involving the retention of significant portions of the former conservatorium building. That proposal also includes a new extension of the same height and bulk, which will extend the full width of the frontage.

Within the context of existing and proposed buildings in the block, the proposed extension to the Masonic Hall is not significantly higher than adjoining buildings. The proposal allows for a reasonable transition in height between adjoining buildings, whilst retaining the prominence and heritage characteristics of the Masonic Hall. On balance, this creates a greater degree of cohesion and consolidates larger built forms on the bookend of Sandy Bay Road, before the transition into the Central Business Zone. It is noted that the site is not subject to the Central Hobart Plan.

#### Height Transition

##### Heathfield Avenue

With specific regard P1(d) and the relationship with buildings to the rear along Heathfield Avenue, the criteria allow for the consideration of building height transitions, but only where such a transition is appropriate. The proposed extension backs onto two existing properties along Heathfield Avenue, identified as 2A Heathfield Avenue and formerly 4 Heathfield Avenue (which now forms part of 5-7 Sandy Bay Road). 2A Heathfield Avenue has been used as an Airbnb for short-stay accommodation, whilst 4 Heathfield Avenue is utilised as an office. The setback of the two cottages which directly adjoin the rear boundary of the Masonic Hall vary from a minimum of approximately 8.7m to a maximum of 10m, as shown in the figure below.



Figure 10: Existing rear setback of adjoining cottages along Heathfield Avenue (source: [www.thelist.tas.gov.au](http://www.thelist.tas.gov.au) © State of Tasmania).

The following 3D image illustrates the effect of this setback when viewed from Heathfield Avenue.



Figure 11: 3D render from Heathfield Avenue - illustrating the two adjoining cottages and the effect of the setback from the Masonic Hall (source: Architects Designhaus)

In this instance, priority has been given to the Sandy Bay Road façade to provide an appropriate setback which ensures the heritage character and prominence of the Masonic Hall is retained and protected. In Lieu of a physical transition in height between the proposed extension and the existing cottages, these setbacks assist in reducing the overall visual impact of the proposed building, particularly when viewed from street level along Heathfield Avenue, as shown above.

To facilitate a visual, rather than a physical transition to these properties, the rear southern elevation of the proposal has been broken up through the provision of balconies, recessed windows and screening which creates depth and variation to the elevation (this can be seen in figure 14). These design considerations soften the form and perceived scale/bulk of the building. The choice of materials and colours also serves to soften the exterior of the building and is generally consistent with the colour palette found across surrounding properties.

The ability to provide a physical step-down in the form along the rear elevation is problematic due to the restricted floor plate and dimensions of the site. This means additional height is required to accommodate a minimum, commercially viable floor plate. A cost analysis is being prepared to provide further detail regarding the above.

Given the considerations above, it is not appropriate to provide a transition in height to Heathfield Avenue in this instance.

Notwithstanding, the articulation/fenestration of the southern elevation (i.e. recessed windows, balconies and screening) which adjoins the heritage cottages does serve to 'break-up' and soften the overall form of the building when viewed from Heathfield Avenue - to an extent that is broadly compatible with height, bulk, scale and mixed-use character of the block.

**Overshadowing of Public Space**

In response to P1(c), the proposed building does not result in any unreasonable overshadowing of public space. Whilst there will be shadowing of adjoining public streets at certain times during the morning and afternoon (Sandy Bay Road and Heathfield Avenue), the period of time in which these streets will be overshadowed is not unreasonable.

**A2**

*Building height within 10m of a residential zone must be no more than 8.5m.*

**P2**

*Building height within 10 m of a residential zone must be compatible with the building height of existing buildings on adjoining lots in the residential zone.*

The nearest residential zoned land is approximately 230m to the south-east, therefore it is considered that A2 is not applicable.

**15.4.2 Setback**

*Objective: To ensure that building setback contributes positively to the streetscape and does not result in unreasonable impact on residential amenity of land in a residential zone.*

**SCHEME REQUIREMENTS****A1**

*Building setback from frontage must be parallel to the frontage and must be no more than: 1m from the median street setback of all existing buildings on the same side of the street within 100m of the site.*

**P1**

*Building setback from frontage must satisfy all of the following:*

- (a) be consistent with any Desired Future Character Statements provided for the area;*
- (b) be compatible with the setback of adjoining buildings, generally maintaining a continuous building line if evident in the streetscape;*
- (c) enhance the characteristics of the site, adjoining lots and the streetscape;*
- (d) provide for small variations in building alignment only where appropriate to break up long building facades, provided that no potential concealment or entrapment opportunity is created;*
- (e) provide for large variations in building alignment only where appropriate to provide for a forecourt for space for public use, such as outdoor dining or landscaping, provided that no potential concealment or entrapment opportunity is created and the forecourt is afforded very good passive surveillance.*

**PROPOSAL RESPONSE**

No changes to the existing setback of the Masonic Hall are proposed, which is currently built to the boundary with a 0m setback. The median street setback has been calculated based on the existing conditions along Sandy Bay Road.

**Sandy Bay Road**

The existing setback of buildings along Sandy Bay Road varies from a minimum of 0m to a maximum of 11.9m.

However, the site at 5-7 Sandy Bay Road has variable setbacks ranging from 0m to 11.9m. Therefore, to account for this individual variable the median setback of 5-7 Sandy Bay Road has been calculated and inserted into the analysis below:

- 1 Sandy Bay Road (Mantra) - 0m setback
- 5-7 Sandy Bay Road - 6.5m (median) setback
- 9 Sandy Bay Road (existing building) - 3.1m setback

#### Harrington Street

- 58 Harrington Street (Welcome Stranger) - 0m setback

Based on the above, the median street setback within 100m of the site is calculated as:

$$6.5\text{m} + 3.1\text{m} = 9.6/4 \text{ (number of buildings within 100m of the site)} = 2.4\text{m}.$$

(note: the approved development at 5-7 Sandy Road reduces this setback to 0m).

The figure below demonstrates the current variable setback of buildings along Sandy Bay Road.



Figure 12: Evidence of existing variable setbacks along Sandy Bay Road & Harrington Street within 100m of the site - Red box illustrates setback of the proposed extension (source: [www.thelist.tas.gov.au](http://www.thelist.tas.gov.au) © State of Tasmania)

The proposed extension is to be setback approximately 7m from the frontage and requires assessment against P1.

#### P1

(a) there are no desired future character statements for the Zone.

(b) & (c) The existing Masonic Hall maintains a consistent building line within the streetscape, which will be reinforced if/when the development at 5-7 Sandy Bay Road is completed.

The setback of the proposed extension reflects the current variations in building setback (particularly the existing conditions at 5-7 Sandy Bay Road). However, it is recognised that 5-7 Sandy Bay Road presents an undesired variance to this, with A1 seeking to promote strong street-edges, with buildings built to their respective frontages.

The Masonic Hall already presents a strong street-edge to Sandy Bay Road, which is sufficient to maintain a continuous building line within the streetscape. The design of the proposed extension and alterations has been strongly guided by this, to maintain the strong and prominent street-edge provided by the Masonic Hall and ensure the proposed development does not detract from this.

As a result, the proposed extension is able to be read as a contemporary addition, which maintains the enhances the characteristics of the site and the wider streetscape.

(d) & (e) The setback of the proposed extension is considered a small variation to the building line, given that the existing Masonic Hall already provides a strong continuous building line to the street (which is to be further enhanced by the approved development at 5-7 Sandy Bay Road).

The setback of the proposed extension also allows a portion of the existing roof form of the Masonic Hall to be retained.

The proposal will not result in any entrapment spaces.

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#### A2

*Building setback from a residential zone must be no less than:*

- (a) 3m;
  - (b) half the height of the wall,
- whichever is the greater.*

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#### RESPONSE

As detailed above, the site is not located in proximity to a residential zone. Therefore, it is considered that A2 does not apply.

#### 15.4.3 Design

*Objective: To ensure that building design contributes positively to the streetscape, the amenity and safety of the public and adjoining land in a residential zone.*

---

#### SCHEME REQUIREMENTS

#### A1

*Building design must comply with all of the following:*

- (a) *provide the main pedestrian entrance to the building so that it is clearly visible from the road or publicly accessible areas on the site;*
  - (b) *for new building or alterations to an existing facade provide windows and door openings at ground floor level in the front facade no less than 40% of the surface area of the ground floor level facade ;*
  - (c) *for new building or alterations to an existing facade ensure any single expanse of blank wall in the ground level front facade and facades facing other public spaces is not greater than 30% of the length of the facade;*
  - (d) *screen mechanical plant and miscellaneous equipment such as heat pumps, air conditioning units, switchboards, hot water units or similar from view from the street and other public spaces;*
  - (e) *incorporate roof-top service infrastructure, including service plants and lift structures, within the design of the roof;*
  - (f) *provide awnings over the public footpath if existing on the site or on adjoining lots;*
-

(g) not include security shutters over windows or doors with a frontage to a street or public place.

...

---

**RESPONSE**

---

A1(a) The primary pedestrian entrance to the proposed extension will be via the existing side entrance to the Masonic Hall, located on the western elevation at ground level. The entrance is clearly visible from the road.

(b) & (c) No changes are proposed to the façade of the existing Masonic Hall at ground floor level.

(d) No changes to existing mechanical plant or miscellaneous equipment are proposed. The stairs and lift structure for the proposed extension will be screened within the building form.

(e) Service infrastructure will be contained within the building form.

(f) & (g) There are no existing awnings and none are proposed. No security screens are proposed.

The proposal complies with A1.

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**A2**

*Walls of a building facing the General Residential Zone or Inner Residential Zone must be coloured using colours with a light reflectance value not greater than 40 percent.*

...

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**RESPONSE**

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Although there are a number of properties to the south of the site that are used for residential purposes, the nearest land zoned residential is located over 230m away to the east.

Therefore, it is considered that A2 does not apply.

#### 15.4.4 Passive Surveillance

*Objective: To ensure that building design for non-residential uses provides for the safety of the public.*

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**SCHEME REQUIREMENT**

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**A1**

*Building design for non-residential uses must comply with all of the following:*

- (a) *provide the main pedestrian entrance to the building so that it is clearly visible from the road or publicly accessible areas on the site;*
  - (b) *for new buildings or alterations to an existing facade provide windows and door openings at ground floor level in the front façade which amount to no less than 40 % of the surface area of the ground floor level facade;*
  - (c) *for new buildings or alterations to an existing facade provide windows and door openings at ground floor level in the façade of any wall which faces a public space or a car park which amount to no less than 30 % of the surface area of the ground floor level facade;*
  - (d) *avoid creating entrapment spaces around the building site, such as concealed alcoves near public spaces;*
  - (e) *provide external lighting to illuminate car parking areas and pathways;*
-

(f) provide well-lit public access at the ground floor level from any external car park.

...

---

**RESPONSE**

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The proposal complies with A1 as follows:

- (a) As outlined above, access to the proposed extension will be via the existing side entry door to the Masonic Hall on the western elevation of the building. This access point is clearly visible from the road.
- (b) & (c) No changes are proposed at ground floor level.
- (d) The proposal does not create any new entrapment spaces.
- (e) No external parking is proposed.
- (f) Lighting is already provided in this location, however additional lighting will be provided if necessary.

#### 15.4.5 Landscaping

*Objective: To ensure that a safe and attractive landscaping treatment enhances the appearance of the site and if relevant provides a visual break from land in a residential zone.*

---

**SCHEME REQUIREMENT**

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**A1**

*Landscaping along the frontage of a site is not required if all of the following apply:*

- (a) the building extends across the width of the frontage, (except for vehicular access ways);
- (b) the building has a setback from the frontage of no more than 1m.

**P1**

*Landscaping must be provided to satisfy all of the following:*

- (a) enhance the appearance of the development;
- (b) provide a range of plant height and forms to create diversity, interest and amenity;
- (c) not create concealed entrapment spaces;
- (d) be consistent with any Desired Future Character Statements provided for the area.

---

**RESPONSE**

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The existing Masonic Hall extends across the width of the frontage (except for the vehicle access on the eastern side of the site) and has a setback no more than 1m.

Therefore, no landscaping is required along the frontage at street level.

## 4. CODES

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### 4.1 ROAD AND RAILWAY ASSETS CODE

#### 4.1.1 USE STANDARDS

##### **E5.5.1 Existing road accesses and junctions**

*Objective: To ensure that the safety and efficiency of roads is not reduced by increased use of existing accesses and junctions.*

##### SCHEME REQUIREMENTS

###### **A3**

The annual average daily traffic (AADT) of vehicle movements, to and from a site, using an existing access or junction, in an area subject to a speed limit of 60km/h or less, must not increase by more than 20% or 40 vehicle movements per day, whichever is the greater.

###### **P3**

Any increase in vehicle traffic at an existing access or junction in an area subject to a speed limit of 60km/h or less, must be safe and not unreasonably impact on the efficiency of the road, having regard to:

- (a) the increase in traffic caused by the use;
- (b) the nature of the traffic generated by the use;
- (c) the nature and efficiency of the access or the junction;
- (d) the nature and category of the road;
- (e) the speed limit and traffic flow of the road;
- (f) any alternative access to a road;
- (g) the need for the use;
- (h) any traffic impact assessment; and
- (i) any written advice received from the road authority.

##### RESPONSE

The site possesses very limited on-site car parking, consisting primarily of a small laneway adjacent 5-7 Sandy Bay Road which provides jockey parking for two vehicles, which are used by patrons to the Masonic Hall.

The proposed development does not propose any additional car parking. Therefore, the vehicle movements to and from the site using the existing access will not increase by more than 40 movements per day.

#### 4.1.2 DEVELOPMENT STANDARDS

##### **E5.6.2 Road accesses and junctions**

*Objective: To ensure that the safety and efficiency of roads is not reduced by the creation of new accesses and junctions.*

##### SCHEME STANDARDS

###### **A2**

No more than one access providing both entry and exit, or two accesses providing separate entry and exit, to roads in an area subject to a speed limit of 60km/h or less.

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**RESPONSE**

---

No changes to the existing access points to the site are proposed.

**E5.6.4 Sight distance at accesses, junctions and level crossings**

*Objective: To ensure that accesses, junctions and level crossings provide sufficient sight distance between vehicles and between vehicles and trains to enable safe movement of traffic.*

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**SCHEME REQUIREMENTS**

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**A1**

*Sight distances at:*

(a) an access or junction must comply with the Safe Intersection Sight Distance shown in Table E5.1; ...

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**RESPONSE**

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No changes proposed.

4.2 **PARKING AND ACCESS CODE**

4.2.1 **USE STANDARDS**

**E6.6.1 Number of Car Parking Spaces**

*Objective: To ensure that:*

- (a) there is enough car parking to meet the reasonable needs of all users of a use or development, taking into account the level of parking available on or outside of the land and the access afforded by other modes of transport.
- (b) a use or development does not detract from the amenity of users or the locality by:
  - (i) preventing regular parking overspill;
  - (ii) minimising the impact of car parking on heritage and local character.

---

**SCHEME REQUIREMENTS**

---

**P1**

*The number of on-site car parking spaces must be sufficient to meet the reasonable needs of users, having regard to all of the following:*

- (a) car parking demand;
  - (b) the availability of on-street and public car parking in the locality;
  - (c) the availability and frequency of public transport within a 400m walking distance of the site;
  - (d) the availability and likely use of other modes of transport;
  - (e) the availability and suitability of alternative arrangements for car parking provision;
  - (f) any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces;
  - (g) any car parking deficiency or surplus associated with the existing use of the land;
-

- (h) any credit which should be allowed for a car parking demand deemed to have been provided in association with a use which existed before the change of parking requirement, except in the case of substantial redevelopment of a site;
- (i) the appropriateness of a financial contribution in lieu of parking towards the cost of parking facilities or other transport facilities, where such facilities exist or are planned in the vicinity;
- (j) any verified prior payment of a financial contribution in lieu of parking for the land;
- (k) any relevant parking plan for the area adopted by Council;
- (l) the impact on the historic cultural heritage significance of the site if subject to the Local Heritage Code;
- (m) whether the provision of the parking would result in the loss, directly or indirectly, of one or more significant trees listed in the Significant Trees Code.

---

**RESPONSE**


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**P1**

As a result of the existing footprint of the Masonic Hall, it is not possible to provide on-site car parking. The application is accompanied by a traffic impact assessment. The following provides a response to the performance criteria.

(a) The demand for car parking on the site, particularly for patrons to the rooftop bar, is anticipated to be substantially lower than that required by the scheme, for the following reasons:

- The site is within close proximity to the CBD and other key socio-cultural sites such as Salamanca Place and wider Sullivan's Cove, all of which support a high density of existing restaurants, bars, hotels and accommodation. As a result, the site's location supports and encourages walkability rather than a reliance on private vehicles and associated on-site venue parking.
- In areas of high walkability (generally), car pooling and alternate forms of transport such as Uber and other taxi services are heavily relied upon which serves to reduce the demand for car parking.
- The primary purpose for patrons attending the proposed bar is to consume alcohol and enjoy the substantial views offered by the rooftop location. Therefore, the likelihood of patrons driving and requiring on-site parking is anticipated to be substantially lower than that required by the scheme.

This is similarly the case with the proposed accommodation. The physical limitations of the site make the provision of on-site parking impossible. Even if on-site parking could be provided, the existing access to the site is within proximity of the intersection between Sandy Bay Road, Davey Street and Harrington Street. Vehicles entering and exiting from the site would substantially reduce the functionality, efficiency and safety of the road and the intersection, particularly during morning and evening peak hours. During such times, a no stopping zone restriction applies to approximately 100m east of the intersection. It is also noted that hotel guests typically factor into their decisions whether a hotel provides on-site car parking or not. If they require on-site parking, and the hotel does not provide it, they are likely to book at another hotel.

The accompanying Traffic Assessment provides an assessment of parking availability on the road network within a 200-300m radius of the subject site. Spot surveys were carried out during typical weekday mid-day, PM, and Saturday mid day peak periods to determine the on-street parking demand. The results show that the parking demand is low during weekday periods, with a maximum of 113 out of 234 (48.3%) spaces occupied. On weekends (particularly Saturdays), the demand

increases due to the Salamanca Markets with 184 out of 195 (94.4%) spaces were occupied during the Saturday period.

The Traffic Assessment provides further analysis demonstrating that many existing hotels in Hobart do not provide on-site car parking and instead rely on independent parking providers in the area or provide limited on-site parking well-below the statutory requirements, as indicated below:

- Mantra One Sandy Bay Road provides parking at a rate of 0.28 car parking spaces per room.
- Ibis Styles provides parking at a rate of 0.12 car parking spaces per room.

The Mantra provides serviced apartments for residential purposes, which would generally have a greater demand for on-site parking which is not provided. As a result of the above factors, on-site car parking and the resultant increase in vehicle movements to and from the site would not be suitable for such a location and the available on-street parking (including proposed drop-off/pick-up bay) can suitably accommodate the short-stay drop-off and pick-up parking associated with the development, as well as any parking associated with the bar and hotel guests parking nearby overnight.

(b), (c), (d) & (e) The site is near several bus stops, taxi ranks and is well within walking distance of a number of key tourist attractions, thereby substantially reducing the demand for car parking.

There is a reasonable amount of on-street public car parking provided within adjoining streets, specifically Davey Street, Salamanca Place and surrounding streets which are well within walking distance. The parking restrictions in these streets range from 1 - 3hrs. Guests of the hotel will be aware when booking that the site does not offer on-site car parking, which will reduce expectations for such a service, ensuring guests prepare accordingly.

There are also numerous formal and informal pick-up/drop-off locations in the wider area, as illustrated in the Traffic Assessment and there is an opportunity for one (1) on-street parking space on Wilmot Street (near its intersection with Sandy Bay Road) to be converted to a dedicated pick-up/drop-off area - as mentioned in the accompanying cover letter.

(f) n/a

(g) n/a

(h), (i), (j) & (k) n/a

(l) the site is subject to the code, however no on-site car parking is proposed.

(m) n/a.

The proposal seeks a discretion for car parking spaces because of physical site limitations and the accompanying Traffic Assessment demonstrates no on-site parking is acceptable given the factors outlined.

#### ***E6.6.2 Number of Accessible Car Parking Spaces for People with a Disability***

*Objective: To ensure that a use or development provides sufficient accessible car parking for people with a disability.*

#### **SCHEME REQUIREMENTS**

##### **A1**

*Car parking spaces provided for people with a disability must:*

- (a) satisfy the relevant provisions of the Building Code of Australia;*

- (b) be incorporated into the overall car park design;  
(c) be located as close as practicable to the building entrance.

*P1 - No performance criteria.*

---

**RESPONSE**

---

No on-site parking is provided, therefore A1 is not applicable.

**E6.6.3 Number of Motorcycle Parking Spaces**

*Objective: To ensure enough motorcycle parking is provided to meet the needs of likely users of a use or development.*

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**SCHEME REQUIREMENTS**

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*A1 - The number of on-site motorcycle parking spaces provided must be at a rate of 1 space to each 20 car parking spaces after the first 19 car parking spaces except if bulky goods sales, (rounded to the nearest whole number). Where an existing use or development is extended or intensified, the additional number of motorcycle parking spaces provided must be calculated on the amount of extension or intensification, provided the existing number of motorcycle parking spaces is not reduced.*

*P1 - The number of on-site motorcycle parking spaces must be sufficient to meet the needs of likely users having regard to all of the following, as appropriate:*

- (a) motorcycle parking demand;  
(b) the availability of on-street and public motorcycle parking in the locality;  
(c) the availability and likely use of other modes of transport;  
(d) the availability and suitability of alternative arrangements for motorcycle parking provision.

---

**RESPONSE**

---

The proposal generates a requirement for 4 motorcycle parking spaces. However, no motorcycle spaces have been provided. The application responds to the performance criteria as follows:

**P1**

As outlined in the response to Clause E6.6.1, the site is within short walking distance of the CBD and Sullivans Cove, which substantially reduces guest reliance upon private modes of transport. The proposal will cater for guests who do not require private vehicles and whose primary intentions are to explore the city and immediate surrounds. Notwithstanding the above, there is no physical capability to provide motorcycle spaces on the site.

There is ample availability and access to alternate transport modes, to satisfy the needs of guests.

**E6.6.4 Number of Bicycle Parking Spaces**

*Objective: To ensure enough bicycle parking is provided to meet the needs of likely users and by so doing to encourage cycling as a healthy and environmentally friendly mode of transport for commuter, shopping and recreational trips.*

---

**SCHEME REQUIREMENTS**

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*A1 - The number of on-site bicycle parking spaces provided must be no less than the number specified in Table E6.2.*

*P1 - The number of on-site bicycle parking spaces provided must have regard to all of the following:*

- (a) the nature of the use and its operations;*
- (b) the location of the use and its accessibility by cyclists;*
- (c) the balance of the potential need of both those working on a site and clients or other visitors coming to the site.*

---

**RESPONSE**

---

Visitor accommodation requires 1 space per 40 accommodation rooms. The proposal provides 33 rooms, therefore only 1 space is required.

The bar generates the following requirement:

*Staff*

*1 for each 25m<sup>2</sup> bar floor area plus 1 for each 100m<sup>2</sup> lounge/beer garden area.*

*Visitors*

*1 for each 25m<sup>2</sup> bar floor area plus 1 for each 100m<sup>2</sup> lounge, beer garden area 3*

Based on the extent of the indoor bar area and the two outdoor terraces, the proposal generates a total requirement for approximately 16 bicycle spaces.

A response to P1 has been provided.

**P1**

Due to the site constraints, requirement to retain heritage fabric and to maintain the existing function of the building, space for on-site bicycle parking and storage is limited.

Whilst the bar generates the bulk of the bicycle parking requirements, the nature and operation of the use means on limited staffing levels are required. Combined with the hours of operation, the demand for on-site bicycle parking is expected to be very low. The site is also within proximity to surrounding residential areas and public transport routes, which provides a high level of walkability which is expected to reduce demand for parking.

As such, the provision of dedicated bicycle parking spaces has not been provided at this stage.

4.2.2 DEVELOPMENT STANDARDS

**E6.7.1 Number of Vehicular Accesses**

**Objective:** *To ensure that:*

- (a) safe and efficient access is provided to all road network users, including, but not limited to: drivers, passengers, pedestrians, and cyclists, by minimising:*
  - (i) the number of vehicle access points; and*
  - (ii) loss of on-street car parking spaces;*
- (b) vehicle access points do not unreasonably detract from the amenity of adjoining land uses;*
- (c) vehicle access points do not have a dominating impact on local streetscape and character.*

---

**SCHEME REQUIREMENTS**

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A1 - The number of vehicle access points provided for each road frontage must be no more than 1 or the existing number of vehicle access points, whichever is the greater.

---

**RESPONSE**

---

There is one existing vehicle access point to the site from Sandy Bay Road and no changes are proposed.

The proposal complies with A1.

**E6.7.2 Design of Vehicular Accesses**

*Objective: To ensure safe and efficient access for all users, including drivers, passengers, pedestrians and cyclists by locating, designing and constructing vehicle access points safely relative to the road network.*

---

**SCHEME REQUIREMENTS**

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A1 - Design of vehicle access points must comply with all of the following:

(a) in the case of non-commercial vehicle access; the location, sight distance, width and gradient of an access must be designed and constructed to comply with section 3 - "Access Facilities to Off-street Parking Areas and Queuing Areas" of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking;

(b) ...

...

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**RESPONSE**

---

No change to the existing vehicle access to the site is proposed.

**E6.7.3 Vehicular Passing Areas Along an Access**

*Objective: To ensure that:*

(a) the design and location of access and parking areas creates a safe environment for users by minimising the potential for conflicts involving vehicles, pedestrians and cyclists;

(b) use or development does not adversely impact on the safety or efficiency of the road network as a result of delayed turning movements into a site.

---

**SCHEME REQUIREMENTS**

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A1 - Vehicular passing areas must:

(a) be provided if any of the following applies to an access:

(i) it serves more than 5 car parking spaces;

(ii) is more than 30m long;

(iii) it meets a road serving more than 6000 vehicles per day;

(b) be 6m long, 5.5m wide, and taper to the width of the driveway;

(c) have the first passing area constructed at the kerb;

(d) be at intervals of no more than 30 m along the access.

...

---

**RESPONSE**

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No changes are proposed.

**E6.7.4 On-Site Turning**

*Objective: To ensure safe, efficient and convenient access for all users, including drivers, passengers, pedestrians and cyclists, by generally requiring vehicles to enter and exit in a forward direction.*

**SCHEME REQUIREMENTS**

*A1 - On-site turning must be provided to enable vehicles to exit a site in a forward direction, except where the access complies with any of the following:*

- (a) it serves no more than two dwelling units;*
- (b) it meets a road carrying less than 6000 vehicles per day.*

**RESPONSE**

As no additional car parking areas are proposed, A1 is considered not applicable.

Clause E6.7.5, E6.7.6, E6.7.7 & E6.7.8 are not applicable as no on-site car parking is proposed.

**E6.7.11 - Bicycle End of Trip Facilities**

*Objective: To ensure that cyclists are provided with adequate end of trip facilities.*

**SCHEME REQUIREMENTS**

*A1 - For all new buildings where the use requires the provision of more than 5 bicycle parking spaces for employees under Table E6.2, 1 shower and change room facility must be provided, plus 1 additional shower for each 10 additional employee bicycle spaces thereafter.*

*P1 - End of trip facilities must be provided at an adequate level to cater for the reasonable needs of employees having regard to all of the following:*

- (a) the location of the proposed use and the distance a cyclist would need to travel to reach the site;*
- (b) the users of the site and their likely desire to travel by bicycle;*
- (c) whether there are other facilities on the site that could be used by cyclists;*
- (d) opportunity for sharing bicycle facilities by multiple users.*

**RESPONSE**

The proposal generates a requirement for more than 5 bicycle parking spaces.

A response to P1 has been provided:

**P1**

The site is within close proximity to the CBD and Sullivans Cove, as well as existing residential areas in Sandy Bay and Battery Point. This ensures that multiple modes of transport are available including walking and e-scooters. The reliance upon bicycle trips by employees is considered to be relatively low, therefore the need for end of trip facilities is also considered to be low.

Staff managing the visitor accommodation will also have access to the storage areas on the ground floor and at the bar level.

Notwithstanding, there are toilets provided on Level 2 and at the bar level which can be utilised, along with storage areas.

**E6.7.13 Facilities for Commercial Vehicles**

*Objective: To ensure that facilities for commercial vehicles are provided on site, as appropriate.*

**SCHEME REQUIREMENTS**

*A1 - Commercial vehicle facilities for loading, unloading or manoeuvring must be provided on-site in accordance with Australian Standard for Off-street Parking, Part 2 : Commercial Vehicle Facilities AS 2890.2:2002, unless:*

- (a) the delivery of all inward bound goods is by a single person from a vehicle parked in a dedicated loading zone within 50 m of the site;*
- (b) the use is not primarily dependent on outward delivery of goods from the site.*

*P1 - Commercial vehicle arrangements for loading, unloading or manoeuvring must not compromise the safety and convenience of vehicular traffic, cyclists, pedestrians and other road users.*

**RESPONSE**

Whilst the proposal is not reliant on the outward delivery of goods, inward bound goods, such as linen and F&B will be required. It is proposed that deliveries will occur via the loading zone situated within 50m of the site, along Sandy Bay Road.

Commercial movements associated with the proposal can comply with A1.

4.3 **STORMWATER MANAGEMENT CODE**4.3.1 **DEVELOPMENT STANDARDS****E7.7.1 Stormwater Drainage and Disposal**

*Objective: To ensure that stormwater quality and quantity is managed appropriately.*

**SCHEME REQUIREMENTS**

*A1 - Stormwater from new impervious surfaces must be disposed of by gravity to public stormwater infrastructure.*

**RESPONSE**

Stormwater from the site will be directed to the existing public system via gravity and complies with A1.

*A2 - A stormwater system for a new development must incorporate water sensitive urban design principles<sup>R1</sup> for the treatment and disposal of stormwater if any of the following apply:*

- (a) the size of new impervious area is more than 600 m<sup>2</sup>;*
- (b) new car parking is provided for more than 6 cars;*
- (c) a subdivision is for more than 5 lots.*

**RESPONSE**

The site is already impervious and the proposed alterations and extensions will not result in any increase in impervious areas over existing. The proposal does not provide new car parking and no subdivision is proposed.

Therefore, WSUD principals are not required.

---

**A3**

*A minor stormwater drainage system must be designed to comply with all of the following:*

- (a) be able to accommodate a storm with an ARI of 20 years in the case of non-industrial zoned land and an ARI of 50 years in the case of industrial zoned land, when the land serviced by the system is fully developed;*
- (b) stormwater runoff will be no greater than pre-existing runoff or any increase can be accommodated within existing or upgraded public stormwater infrastructure.*

---

**RESPONSE**

The existing stormwater drainage system and proposed minor upgrades will be capable of accommodating a storm with an ARI of 20 years. Stormwater runoff will be no greater than pre-existing runoff, as the extent of impervious areas will remain the same.

The proposal complies with A3.

4.4 HISTORIC HERITAGE CODE

The site is listed under the Code and on the Tasmanian Heritage Register.

Hobart City Council Register:

Ref. No.	Name	Street No.	Street/Location	C.T.	General Description
2776	Masonic Temple	3	Sandy Bay Road	71169/1	

Tasmanian Heritage Register:

Ref. No.	Name	Street No.	Street/Location
7490	Masonic Temple	5-7	Sandy Bay Road



Figure 13: Extent of H2 Heritage Precinct and heritage listings (source: www.thelist.tas.gov.au © State Government of Tasmania)

The following provisions have been addressed.

4.4.1 DEVELOPMENT STANDARDS FOR HERITAGE PLACES

**E13.7.1 - Demolition**

*Objective: To ensure that demolition in whole or part of a heritage place does not result in the loss of historic cultural heritage values unless there are exceptional circumstances.*

**SCHEME REQUIREMENTS**

*P1 - Demolition must not result in the loss of significant fabric, form, items, outbuildings or landscape elements that contribute to the historic cultural heritage significance of the place unless all of the following are satisfied;*

- 
- (a) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;*
  - (b) there are no prudent and feasible alternatives;*
  - (c) important structural or façade elements that can feasibly be retained and reused in a new structure, are to be retained;*
  - (d) significant fabric is documented before demolition.*
- 

#### RESPONSE

---

The proposed extension will require some demolition works, however these works are limited to minor external features at the rear of the building and some internal modifications.

The key features of the Masonic Hall will be retained and protected, and the proposed demolition is not considered to result in any substantial loss of significant fabric, form or items that contribute to the significance of the place. A detailed assessment against the performance criteria is provided in the accompanying Heritage Impact Assessment.

The proposal complies with P1.

#### **E13.7.2 Buildings and Works other than Demolition**

*Objective: To ensure that development at a heritage place is:*

- (a) undertaken in a sympathetic manner which does not cause loss of historic cultural heritage significance; and*
- (b) designed to be subservient to the historic cultural heritage values of the place and responsive to its dominant characteristics.*

#### SCHEME REQUIREMENTS

---

*A1 - No acceptable solution.*

*P1 - Development must not result in any of the following:*

- (a) loss of historic cultural heritage significance to the place through incompatible design, including in height, scale, bulk, form, fenestration, siting, materials, colours and finishes;*
  - (b) substantial diminution of the historic cultural heritage significance of the place through loss of significant streetscape elements including plants, trees, fences, walls, paths, outbuildings and other items that contribute to the significance of the place.*
- 

#### RESPONSE

---

Whilst a detailed response has been provided in the accompanying Heritage Impact Assessment, a summary has been provided below.

The areas of the existing Masonic Hall to be demolished/modified are primarily internal and in areas of least historical heritage value. These areas include the rear portions of the building, including the existing kitchen which has been historically modified. However, the proposed extension will result in a reduction in the roof form, of which only a small portion is discernible from the street. The section of the roof discernible from the street will be retained, as shown in the architectural documentation.

The extension has been designed to ensure the external heritage significance of the site is retained. This has been achieved by setting the extension back from the frontage to allow the architectural details of the existing Masonic Hall to remain prominent in the streetscape and allow retention of a substantial portion of the roof form. The external materials, colours and fenestration

has also been designed to read as a contemporary addition, without overpowering the existing Masonic Hall. This has been achieved by recessing the balconies and glazing, whilst providing external screening which creates a visual pattern which is drawn from the 'terracing' or layering of built form evident within surrounding heritage precincts.

The proposed material palette also responds to the existing colour scheme of the Masonic Lodge. The proposed works will not result in the loss of any streetscape elements.

The proposal complies with P1.

---

*P2 - Development must be designed to be subservient and complementary to the place through characteristics including:*

- (a) scale and bulk, materials, built form and fenestration;*
- (b) setback from frontage;*
- (c) siting with respect to buildings, structures and listed elements;*
- (d) using less dominant materials and colours.*

---

**RESPONSE**

The setback of the proposed extension and choice of external materials, finishes and fenestration ensures that the proposal will be subservient and complementary to the place. The setback allows the dominate features of the Masonic Hall to remain prominent in the streetscape whilst minimising the need to undertake any structural or external modifications to the façade of the Masonic Hall.

The setback also allows a large portion of the existing roof form to be retained, which both protects the significance of the existing building whilst also complementing the visual interpretation of the proposed extension by providing a transition between old and new.

The proposed materials and colours are intrinsically less dominant by virtue of the distance between the façade of the proposed extension and the lower-level façade of the existing Masonic Hall. This is particularly evident at street level, where the existing heritage and architectural features of the Masonic Hall continue to take centre stage. These factors substantially reduce the scale/height of the proposed extensions/alterations, allowing the Masonic Hall to retain its prominence in the streetscape. A further analysis is provided in the accompanying Heritage Impact Assessment.

The proposal complies with P2.

---

*P3 - Materials, built form and fenestration must respond to the dominant heritage characteristics of the place, but any new fabric should be readily identifiable as such.*

---

**RESPONSE**

As outlined above, the materials, built form and fenestration responds to the dominant heritage characteristics of the place and ensures that the new form is readily identifiable as such.

The Heritage Impact Assessment states the following:

*The materials palette has been chosen to provide a more transparent and ephemeral contrast to the solid masonry and minimally fenestrated Masonic temple to promote a materiality hierarchy which emphasises the dominance of the earlier building and promotes subservience of the addition.*

The proposal complies with P3.

---

*P4 - Extensions to existing buildings must not detract from the historic cultural heritage significance of the place.*

---

**RESPONSE**

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As outlined above, the accompanying Heritage Impact Assessment details the key heritage characteristics of the site which are primarily internal, but also include the existing façade and roof elements.

Given that these elements will be retained, no changes are proposed to the exterior façade and the proposal is to be setback from the frontage, the proposed works will not unreasonably detract from the significance of the place. The internal modifications are limited to areas of least historical value. A more detailed response is provided in the accompanying Heritage Impact Assessment.

The proposal complies with P4.

4.4.2 DEVELOPMENT STANDARDS FOR PLACES OF ARCHAEOLOGICAL POTENTIAL

According to the Code, the site is within an area identified as having archaeological potential. Therefore, the following provisions will apply.

**E13.10.1 - Building, Works and Demolition**

*Objective: To ensure that building, works and demolition at a place of archaeological potential is planned and implemented in a manner that seeks to understand, retain, protect, preserve and otherwise appropriately manage significant archaeological evidence.*

---

**SCHEME REQUIREMENTS**

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**P1**

*Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:*

- (a) the nature of the archaeological evidence, either known or predicted;*
  - (b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;*
  - (c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition;*
  - (d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;*
  - (e) measures proposed to preserve significant archaeological evidence 'in situ'.*
- 

**RESPONSE**

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As outlined in the accompanying Heritage Impact Assessment, the proposal will not involve excavation within any areas deemed to be of archaeological potential.

## 5. SUMMARY

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The proposal is for an extension to the existing Masonic Hall, which will consist of visitor accommodation apartments and a roof-top bar.

Both the existing Masonic Hall and proposed extension exceed the permitted building height in the zone, however the planning scheme provides discretion to consider buildings outside of the permitted height.

The proposed extension will be constructed on top of the existing Masonic Hall, but setback approximately 7m from the frontage. This ensures that the architectural features and historic value of the Masonic Hall is retained.

As outlined in this report, the significant setback of the building from the frontage provides a substantial recess, allowing the existing Masonic Hall to remain prominent in the streetscape. The transition in height to the existing Conservatorium of Music building at 5-7 Sandy Bay Road is not considered unreasonable, particularly when consideration is given to the montage images.

The proposal triggers discretion with respect to car parking, as no on-site parking can be provided due to the constraints of the site. The site is well within walking distance of the CBD, Sullivans Cove and Salamanca which substantially reduces the car parking demand. Guests will be aware when booking that on-site parking is not available, and it is anticipated the proposal will primarily cater for guests who do not require a vehicle during their stay.

Notwithstanding, the accompanying TIA and RFI cover letter outline a recommendation for a drop-off/pick-up zone along Wilmot Street. Such a zone will add to the existing network of similar facilities/zones in the surrounding area and will assist in alleviating demand for on-street and/or overnight parking in the area. The designated location of the zone minimises the loss of any existing on-street public parking and will be suitable for shuttle buses, taxis and ride-share pick-up/drop-off.

The existing Masonic Hall will retain its existing functions, and the proposed development will provide additional financial support for ongoing Masonic operations.

ireneinc

PLANNING &amp; URBAN DESIGN



16 December 2024

Hobart City Council  
GPO Box 503  
HOBART, TAS 7000

## COVER LETTER - 3 SANDY BAY ROAD (MASONIC TEMPLE)

The following has been prepared in response to the request for further information received on the 10/11/2021, regarding an application on the abovementioned site (PLN-21-710).

The following responds to the specific requests below:

**Planning**

- Please provide the planner's assessment report referred to in the submitted cover letter.

Please refer to the accompanying report.

- Photomontages of the proposed building in colour, at eye level and from a standard angle of view (images should be equivalent to a camera lens of 4550mm focal length based on a 35mm camera) from multiple locations within the immediate, midrange and long range with the locations shown on an attached annotated map. At least one montage from each location should reflect the current existing built environment relative to the proposal.
- Show existing adjoining built form on elevations, 3d models and site plan.
- Provide shadow diagrams reflecting the existing built form and the proposed development.
- Provide a section through the site to Heathfield Avenue showing the proposed development relevant to 2A Heathfield Avenue.

Please refer to the amended architectural set.

- Please confirm that the construction of the proposal will not rely underpinning or encroachment onto adjoining properties. If it is required then the site will need to form part of the application.

The proposal will require structural footings that extend partially into 5-7 Sandy Bay Road and 2A Heathfield Avenue, as shown in the architectural plans submitted. These properties form part of the application, as outlined in the accompanying planning report.

- Confirm whether commercial vehicle movements, (including loading and unloading and garbage removal) to or from a site are to be to within the hours of:

ireneinc

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PLANNING TAS PTY LTD TRADING AS IRENEINC PLANNING &amp; URBAN DESIGN

(a) 7.00 am to 5.00 pm Mondays to Fridays inclusive;

(b) 8.00 am to 5.00 pm Saturdays;

(c) 9.00 am to 12 noon Sundays and Public Holidays.

Please refer to the accompanying Waste Management Plan. Commercial vehicle movements will occur within the hours specified above.

- Provide a detailed external material palette.

Please refer to the amended architectural plans.

#### Heritage Code

##### HER Fi 1

- Provide a separate site plan showing the location of all ground excavation.

##### HER Fi 2

- Provide photomontages from the locations shown in the attached plan and images. The two locations are directly opposite the side laneways and at right angles to the front facade.

Please refer to the amended architectural plans.

#### Parking and Access

##### PA 5.1

Given that the proposed development is located in a Urban Mixed Use Zone and with regard to the Hobart Interim Planning Scheme 2015 Table E6.1, please provide details to demonstrate that:

- No parking is proposed on site, demonstrate how the applicant will address the deficiency onsite or with alternate arrangements.

*Note: The proposed development has 38 rooms. Given the limited transport options from Hobart to tourist destinations around Tasmania it is unreasonable to assume that all guests will not have their own vehicles whether that be private or hire.*

*It is unlikely that this deficiency will be supported without demonstration of alternate arrangements.*

Due to the size of the site and siting of the existing Masonic Lodge (which is heritage listed at State and local level), on-site parking cannot be provided.

The accompanying Traffic Impact Assessment has been prepared to address the performance criteria and demonstrate no on-site car parking is necessary to cater for the use. The report includes analysis of eight (8) similar hotel developments in Hobart, in which car parking is either:

- Provided on-site, but at a rate significantly below that required under the planning scheme; or
- Is provided off-site, through arrangements with private car parking providers.

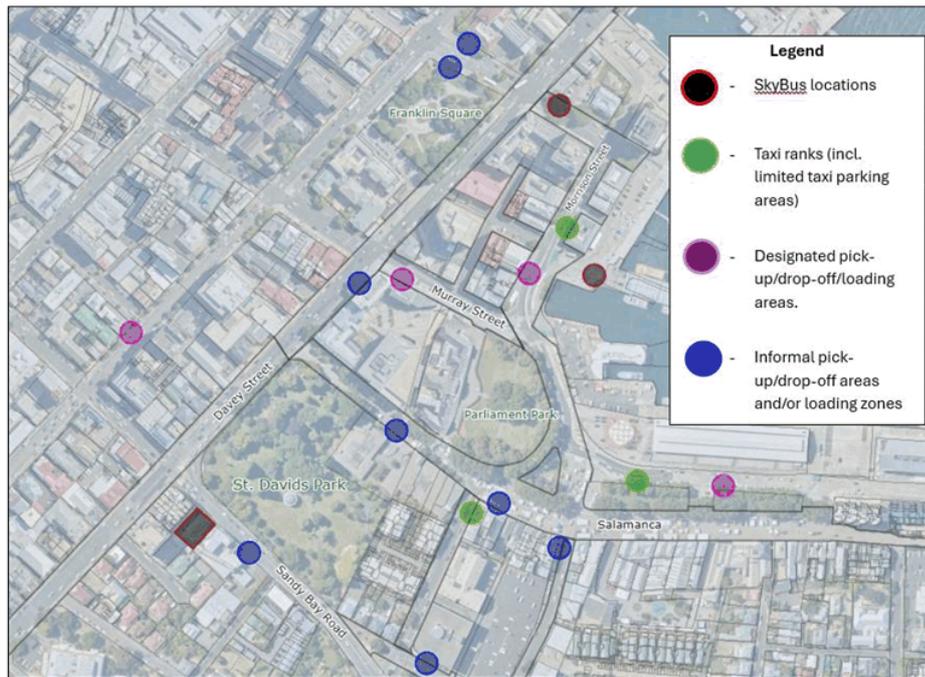
Most of these hotels are within the Central Business Zone, where new development is not required to provide on-site car parking. Whilst the proposed development at 3 Sandy Bay Road is zoned Urban Mixed Use, it is located a mere 53m from the Central Business Zone.

The analysis indicates these hotels provide an average parking rate of just 0.32 spaces per room, despite providing a significantly greater number of rooms and conference facilities which generate a much greater demand for parking.

The proximity of these developments to primary public transport routes and key locations/attractions within the CBD does alleviate demand, however the same applies to the proposed development, whilst catering for just 38 rooms and roof-top bar.

Many of Tasmania’s major tourist attractions and drawcards are located outside the major centres of Hobart and Launceston, meaning it is often necessary for visitors to hire a car during their stay. However, there are also many visitors arriving for specific events or shorter stays who are not reliant upon a private/hire car during their visit or may choose to stay before hiring a vehicle, or after returning a vehicle depending on individual itineraries. Due to its location and walkability, the proposed development specifically caters for this portion of the visitor market, and instead promotes the use of alternate modes of transport which is highly desirable and a preferred approach in inner-city areas.

Given the above, most guests are expected to be dropped off in proximity or within walking distance of the site. The following diagram illustrates the extent of drop-off/pick-up locations in the surrounding area.



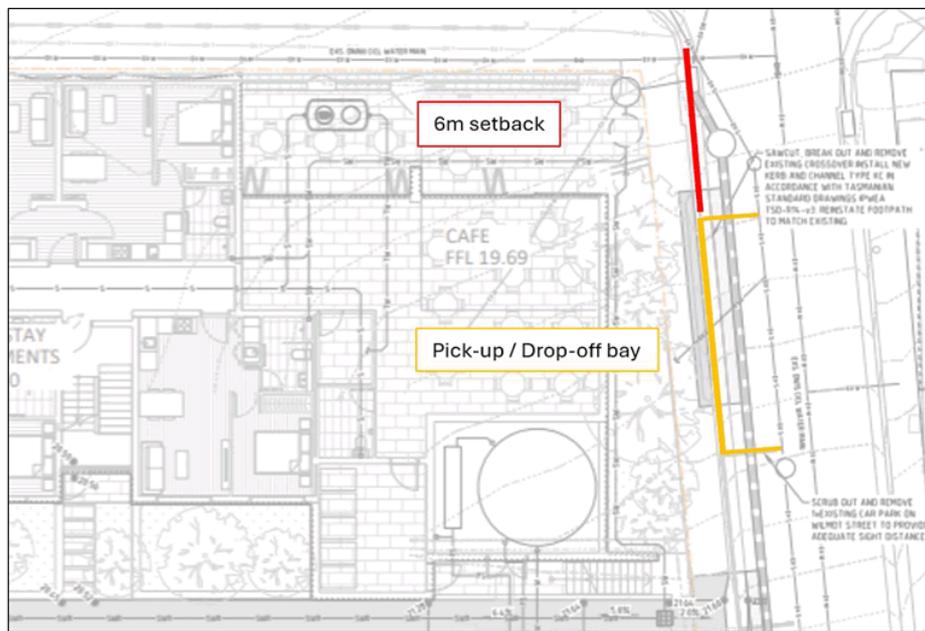
Whilst many of these locations are within walking distance of the site, an additional location is proposed along Wilmot Street, just south of the intersection with Sandy Bay Road.

The area currently supports a crossover and one (1) on-street parking space, which are to be removed and reinstated with kerb and channel as part of a separate but concurrent application at 5-7 Sandy Bay Road.

The designated drop-off/pick-up bay (with signage), will have the following dimensions:

- 8m long x 2.5m wide, with the following setbacks:
  - 6m from the Wilmot/Sandy Bay Road intersection, and;
  - 4m from the vehicle entry to the off-street parking area proposed as part of the 5-7 Sandy Bay Road application.

The bay will benefit not only 3 Sandy Bay Road, but also the development at 5-7 Sandy Bay Road and contribute the network of pick-up/drop-off locations and assist in reducing congestion.





In summary:

- The site is within short walking distance of key tourist and business areas (Sullivans Cove, Salamanca and CBD).
  - With a range of alternate transport methods also available (e-scooters, buses etc).
- There are numerous pick-up/drop-off areas in the vicinity of the site, both formal and informal (attached diagram illustrates where these are).
- If the site were located in the Central Business Zone (less than 50m away), the acceptable solution would require no on-site parking.
- Many similar hotel developments in large cities provide minimal or no on-site parking (which promotes a safer pedestrian environment and uptake of alternate transport modes, whilst reducing reliance upon private vehicles).

Given the above and the assessment provided in the TIA, the proposal is considered to satisfy the performance criteria to Clause E6.6.1, of the Hobart Interim Planning Scheme 2015.

#### PA 8

*Provide a detailed Waste Collection Plan by a suitably qualified person that demonstrates the arrangements for collecting waste do not compromise the safety, amenity and convenience of surrounding occupants, vehicular traffic, cyclists, pedestrians and other road and footpath users, having regard to:*

- a) the number of bins;*
- b) the method of collection;*
- c) the time of day of collection;*
- d) the frequency of collection;*
- e) bin storage areas and access for vehicles to bin storage areas, including*

*consideration of gradient, site lines, manoeuvring, direction of vehicle movements and pedestrian access;*

*f) distance from vehicle stopping point to bins if not collected on site;*

*g) the traffic volume, geometry and gradient of the street; and the volume of pedestrians using the street.*

*To assist in meeting the above requirements, and to tie into the proposed arrangements for waste storage and collection, the plan should also consider and address where possible:*

- *Waste generation and supporting calculations for the number of bins required;*
- *Waste systems Hotel rooms, restaurant/bar, recreation areas, etc.;*
- *Bin quantity, size, colour and collection frequency;*
- *Demonstrate that bins can be manoeuvred by a single person from the storage area to the collection point and provide risk assessments;*
- *Signage;*
- *Waste collection methodology;*
- *Standards & compliance to include ventilation, washing and vermin protection and noise reduction.*

Please refer to the accompanying Waste Management Plan.

#### **Stormwater Code**

##### **SW 1**

*A site plan to demonstrate how stormwater from the existing and proposed development will be disposed of via gravity to public stormwater infrastructure or to a Council approved system*

Please refer to the previously submitted engineering drawings prepared by JMG.

#### **Tasmanian Heritage Council**

*Under s36(4) of the Act, the Tasmanian Heritage Council gives notification that it requires the following additional information:*

1. *Please provide a structural design report and structural design drawings, explaining the structural design in more detail, including the scope of alterations to the existing building, excavation requirements and footing design for the new columns. The report must include consideration of the mitigation of potential heritage impacts.*

*Reason for request: To provide a more detailed understanding about the proposed scope of work and the structural design, which is a key component of the proposal.*

Preliminary structural advice has been prepared and accompanies this response. Investigations to date indicate the structural design requirements can be achieved.

Whilst detailed design drawings will be prepared and submitted as part of any subsequent condition endorsement/building approval process, the cost to prepare these documents is significant.

At this stage our client is reluctant to incur those costs until there is some degree of certainty regarding the outcome of the planning permit process.

2. *Please provide more detailed information about the proposed external finishes, including the proposed metal screening and the proposed overall colour scheme.*

*Reason for request: To provide a more detailed understanding about the external form and finishes of the new addition.*

3. *Please provide east-west sections to explain the relationship of the new extension and existing building at key intersections.*

*Reason for request: To provide further information that more fully explains the relationship of the new work to the existing building.*

Please refer the updated architectural set.

4. *Please provide clarification if new signage is proposed as part of this application and, if so, please provide details of the signage, including location, design and materials.*

No signage is proposed as part of this application.

If you have any further questions in relation to the above, please do not hesitate to contact me on 6234 9281.

Yours sincerely,

Phil Gartrell

Senior Planner  
IRENEINC PLANNING & URBAN DESIGN

ireneinc

PLANNING &amp; URBAN DESIGN



28 April 2025

Hobart City Council  
GPO Box 503  
HOBART, TAS 7000

## COVER LETTER - 3 SANDY BAY ROAD (MASONIC TEMPLE)

The following has been prepared in response to the request for further information, regarding an application on the abovementioned site (PLN-21-710). The following responds to the specific requests below:

*Tasmanian Heritage Council*

*Under s36(4) of the Act, the Tasmanian Heritage Council gives notification that it requires the following additional information:*

- 1. Please provide a structural design report and structural design drawings, explaining the structural design in more detail, including the scope of alterations to the existing building, excavation requirements and footing design for the new columns. The report must include consideration of the mitigation of potential heritage impacts.*

*Reason for request: To provide a more detailed understanding about the proposed scope of work and the structural design, which is a key component of the proposal.*

Whilst detailed design drawings will be prepared and submitted as part of any subsequent condition endorsement/building approval process, the cost to prepare these documents is significant. Notwithstanding, preliminary structural advice has been prepared and provided to Heritage Tasmania. The advice indicates the structural design requirements can be achieved and slight adjustments have been made following further discussions with Heritage Tasmania, confirming that:

- The existing structural trusses to the perimeter and the corridor trusses remain as documented on the structural drawings.
- The architectural drawings have now been amended to reflect this situation.

A copy of the updated section drawing accompanies this response.

If you have any further questions in relation to the above, please do not hesitate to contact me on 6234 9281.

Yours sincerely,

Phil Gartrell

Senior Planner  
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Irene Inc  
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Attention: Phil Gartrell

20 January 2021

Doc 6295

## MASONIC TEMPLE — DA NOISE ASSESSMENT

Addition of visitor accommodation and a rooftop bar to an existing building (the Masonic Temple) located at 3 Sandy Bay Road, Hobart, is proposed. Noise emissions from the proposal have the potential to impact nearby residential amenity, and as such a noise assessment has been requested by the developer to accompany the Development Application submission. The assessment examines compliance against the Hobart Interim Planning Scheme 2015. This letter presents the results of such an assessment, completed by NVC in January 2021.

### 1. BACKGROUND

The proposed site is an existing building located at 3 Sandy Bay Road, Hobart, and is outlined in red in Figure 1. The site is within an Urban Mixed Use zone (grey overlay in figure), and is accessed off Sandy Bay Road. To the north-west of site (A in figure) is Mantra One hotel. To the south-east of site (B) is the University of Tasmania Conservatorium of Music building. A new 8-storey hotel is to be constructed on this site, and thus has the potential to be affected by noise from the proposal. To the south-west of site (C) are residential dwellings, accessed off Hampden Road.



**FIGURE 1: SITE AND SURROUNDING AREA**

The proposal comprises a seven-storey addition to the existing two-storey building occupying the site. The existing building is to remain largely unaltered, aside from the inclusion of a lift lobby and reception on the ground floor. Level 2 to level 7 inclusive are to be visitor accommodation, with 6-8 rooms per level. Level 9 (the top level) is to comprise a rooftop bar/restaurant, with a central indoor area, and outdoor areas adjacent the north-east and south-west facades. The rooftop bar plan is shown in figure 2 (the top of the image is the north-east facade, i.e. adjacent Sandy Bay Road). Figure 3 shows an elevation of the proposal as seen from Sandy Bay Road.

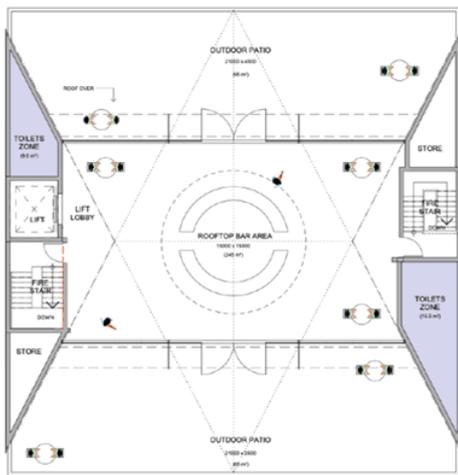
NVC PTY. LTD.

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PO Box 476, ROSNY PARK, TAS 7018

T. 6244 5556

bill@nvc.com.au



**FIGURE 2: ROOFTOP BAR FLOOR PLAN**



**FIGURE 3: NORTH ELEVATION**

The mechanical plant is to be located internally, between the existing Masonic Temple and the new structure above. The facade surrounding the plant is to be metal mesh screen. The proposed plant is to comprise A/C and air handling equipment. Since the location of the mechanical plant provides good screening, and facilitates the simple implementation of additional noise control where required, mechanical plant noise emissions are deemed to be readily mitigated by standard noise control measures (barriers, enclosures, acoustic louvres, silencers, etc), to be considered during the project’s detail design phase. Noise emissions from this plant are thus not considered further here.

The rooftop bar is proposed as a tapas/cocktail bar, comprising a single internal area and two outdoor patios, as shown in figure 2. The north-eastern and south-western facades of the central indoor area are primarily glass, and there is a large glass dome over the central bar area. The remainder of the construction is masonry and as such sound transfer will occur predominantly via the glass. Music is proposed for background ambience only, and speakers will be located in the internal area only.

The patron capacity for the bar is 80 in the internal area, 45 on the northern outdoor patio (facing Sandy Bay Road), and 32 to the southern outdoor patio.

From the above, the primarily noise sources from the proposal are deemed to be patrons of the rooftop bar (both internal and external), and internal background music.

The proposed operating hours are 6:00AM to 12:00AM, 7 days per week. Use during the morning is for breakfast only, so music/patron noise is expected to be significantly reduced compared to evening use.

**2. NOISE PREDICTIONS**

A software model has been created using iNoise software, to calculate noise emissions from the proposed use at the neighbouring receivers. The model implements the ISO9613 algorithm, accounting for geometric spreading, air absorption, building screening and reflections, and ground reflections. Two use cases have been modelled; indoor operations only (patrons and music), and internal operations plus external patrons on the two outdoor patios.

The following comments are relevant to the noise predictions:

- It is assumed that the dominant noise sources from the rooftop bar are patrons occupying the space, and background music.
- It is assumed that all areas are at full occupancy, and that all patrons are speaking simultaneously. This is deemed a worst-case scenario.

- Music is to be background only, and as such is assumed to be at a level of 75 dBA, based on previous measurements by NVC in similar venues. Music is likely to be reproduced at a lower level than this in general service, so this represents a worst-case scenario.
- The music noise assumes a frequency spectrum previously measured during a live band performance by NVC, corrected to achieve an overall level of 75 dBA. The bar's sound system is likely to have reduced bass output compared to a live sound reinforcement system, and as such the model accounts for the worst case.
- Modelling of the internal noise accounts for the transmission loss through the various building elements, as well as the directivity of emissions from each element of the facade.
- The model of indoor operations only assumes that the doors accessing the outdoor patios are closed.
- Predictions are at the boundary to the nearest residences, as stipulated by the Scheme. Separate predictions are made at two heights for each receiver; 1.5m above ground level (as stipulated by the TAS Noise Measurement Procedures Manual), and 1.5m above the highest occupied floor level for each receiver.

Table 2 shows the predicted noise levels at the boundaries to the nearest residences.

**TABLE 2: PREDICTED NOISE LEVELS IN THE RESIDENTIAL ZONE**

Receiver	Sound Pressure Level, dBA			
	Indoor Operations Only		Indoor and Outdoor Operations	
	1.5m AGL	Top Floor Level	1.5m AGL	Top Floor Level
<b>A</b>	<20	<20	28	36
<b>B</b>	<20	<20	28	38
<b>C</b>	<20	<20	28	29

The following comments are relevant to the predicted results:

- The predicted noise emissions from indoor operations are very minor - external noise sources are the dominant noise.
- Noise from internal operations is predicted to be well below the existing background noise level at all times, and thus will be inaudible at the neighbouring residences.
- Predicted worst-case noise emissions from outdoor noise emissions are likely to be below the existing background noise level, and are thus likely to be generally inaudible.

### 3. CRITERIA

Section 15 of the Hobart Interim Planning Scheme 2015 contains criteria for an Urban Mixed Use zone. In particular, clause 15.3.1 details criteria specific to noise emissions, the objective of which is:

*"To ensure that non-residential use does not unreasonably impact residential amenity."*

To satisfy this objective regarding hours of operation, the following Acceptable Solutions criteria are stated under clause 15.3.1-A1:

*"Hours of operation must be within:*

- 7:00AM to 9:00PM Mondays to Fridays inclusive;*
- 8:00AM to 6:00PM Saturdays;*
- 9:00AM to 5:00PM Sundays and Public Holidays  
except office and administrative tasks or visitor accommodation."*



If the Acceptable Solutions criteria are not met, the following Performance Criteria are stated under 15.3.1-P1:

*"Hours of operation must not have an unreasonable impact upon residential amenity through commercial vehicle movements, noise or other emissions that are unreasonable in their timing, duration or extent."*

Pertaining to noise emissions, the following Acceptable Solutions criteria are stated under clause 15.3.1-A2

*"Noise emissions measured at the boundary of the site must not exceed the following:*

- a. 55 dB(A) (LAeq) between the hours of 8:00 am to 6:00 pm;*
- b. 5 dB(A) above the background (LA90) level or 40 dB(A) (LAeq), whichever is the lower, between the hours of 6:00 pm to 8:00 am;*
- c. 65 dB(A) (LAm<sub>ax</sub>) at any time..."*

If these Acceptable Solutions criteria are not met, the following Performance Criteria are stated under 15.3.1-P2:

*"Noise emissions measured at the boundary of the site must not cause environmental harm."*

For commercial vehicle movements, the following Acceptable Solution is provided under clause 15.3.1-A4:

*"Commercial vehicle movements, (including loading and unloading and garbage removal) to or from a site within 50m of a residential zone must be within the hours of:*

- a. 7:00 am to 5:00 pm Mondays to Fridays inclusive;*
- b. 8:00 am to 5:00 pm Saturdays;*
- c. 9:00 am to 5:00 pm Sundays and Public Holidays."*

Commercial vehicle movements are to occur within the acceptable hours above only, and are thus not further considered.

The relevant Acceptable Solutions criteria are then 55 dBA between 8:00AM and 6:00PM (day time) and 40 dBA between 6:00PM and 8:00AM (night time). It is noted that these criteria are independent of the day of the week, i.e. they apply on weekdays, weekends, and public holidays.

#### 4. ASSESSMENT

When assessing the predicted noise emissions, the following is noted:

- The highest occupied level of the Mantra hotel building has a height of approximately 25m above ground level - it is thus significantly lower than the proposed rooftop bar floor level.
- The Mantra hotel has no external balconies, and all windows appear to be inoperable.
- The new hotel to be constructed in place of the existing Conservatorium of Music is proposed as an 8-storey building, with a finished height of approximately 26m above ground level - it is thus significantly lower than the proposed rooftop bar floor level. It is unknown whether the hotel will have operable windows and/or external balconies, and where they may be located.
- The residential dwellings at location C are no greater than two storeys, and thus noise emissions here are assessed at both 1.5m above ground level, and 1.5m above estimated first floor level.

Noise emissions from patrons and background music within the building are predicted to be below 20 dBA at all nearby residences. Noise emissions from patrons on the external patio areas are predicted to be between 28 and 38 dBA at the boundaries of site nearest the neighbouring buildings. These levels are below both the day and night time criteria and measured, and therefore comply with the Acceptable Solutions criteria under clause 15.3.1-A2 of the Scheme.



MASONIC TEMPLE — DA NOISE ASSESSMENT

Further, it is taken that, since the proposal satisfies clause 15.3.1-A2, then noise emissions will not have an unreasonable impact upon residential amenity, and thus the proposal is also deemed to comply with the Performance Criteria under clause 15.3.1-P1 of the Scheme.

**In summary, the predicted noise emissions from the proposal comply with all relevant noise criteria under the Hobart Interim Planning Scheme 2015.**

Should you have any queries, please do not hesitate to contact me directly.

Kind regards,

A handwritten signature in black ink, appearing to read 'Jack Pitt', is written over a light blue circular stamp.

Jack Pitt



praxisenvironment

heritage

planning

archaeology

---

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Conservation Management Policy,  
Heritage Impact Assessment &  
Statement of Compliance

Masonic Temple  
3 Sandy Bay Road

HOBART TASMANIA

Brad Williams  
Heritage Consultant

For Architects Designhaus

December 2020

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This document was written by Brad Williams (BA.Hons Archaeology, G.Dip Maritime Archaeology, MA Cultural Heritage Management)  
Historical Archaeologist, Heritage Consultant and Director of Praxis Environment.

Unless otherwise stated, all photographs were taken by Brad Williams, August 2020

Unless otherwise stated, the north point (or approximate) of maps and plans is to the top of the page – project north is designated as the front wall of the building (although technically that wall is the north-eastern wall).

Cadastral information depicted in this document must not be relied upon without verification by a Surveyor. Rectified aerial imagery has not been used; therefore the actual location as depicted in aerial images may differ to that of actual survey.

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## 1 Introduction, rationale and brief

This report has been commissioned by Architects Designhaus (Hobart) on behalf of the Tasmanian Freemason's Hall Pty. Ltd. in order to provide a sound conservation planning background for the planning of a possible upper-floor development above the Masonic Temple building at 3 Sandy Bay Road, Hobart. A focus of this report is to provide an understanding of the historic cultural heritage significance of the building and to formulate conservation policies and specific recommendations for the management of those values within the context of the possible extension and sustainability of the site for lodge and community uses.

The place is listed on the Tasmanian Heritage Register and on the Heritage Schedule (Table E13.1) of the Hobart Interim Planning Scheme 2015

Accordingly, the owner, management and nominees recognise the responsibility to appropriately manage the heritage values of the place in any forthcoming development. This document aims to provide:

**This, as the first stage of the project (i.e. pre-development design) would include:**

1. An overview **site history** which is the essential basis for (2) and (6) below.
2. An analysis of the built fabric and form of the building via an **inventory of building fabric**, this would include an analysis of relevant spaces, viewlines etc.
3. A **comparative analysis** of relevant comparable buildings via a regional, temporal and thematic framework (e.g. masonic lodge buildings, Lauriston Crisp architect etc.).
4. Formulation of **statements of significance for the building**.
5. **Ranked analysis of building fabric and form** as informed by the statements of significance
6. A **statement of historical archaeological potential** for the site which would consider the site history, key historic themes and the possibility that the site may contain any significant archaeological remains.
7. Formulation of **conservation policy** for the place aligned with the statement of significance and statutory heritage requirements.

**A second stage of the project (i.e. following development design and pre-DA) would include:**

8. If required by (6) – i.e. if archaeological potential is determined an **archaeological impact assessment** against any proposed development, and if archaeological impact is likely the formulation of an **archaeological method statement** to be implemented in any future works program. Note that if no archaeological potential is determined, this step will not be necessary.
9. A **heritage impact assessment** (including a statement of compliance) for any future development plan which is to be measured against the policies of the CMP and SoHAP as well as the statutory requirements.

It is important to note that the above assessment of the place has been formulated independently of the proposed development. Further to the process detailed by the ICOMOS Australia *Burra Charter* and the conservation planning process espoused by J.S. Kerr, Part 1 of this document was developed without reference to any proposed development, then provided to the proponents of the development to inform the design process and to apply the policies of this document to that process.



Figure 1.1 – Aerial photograph of the area (the place denoted in red) Adapted from [www.thelist.tas.gov.au](http://www.thelist.tas.gov.au)



Figure 1.2 – Cadastral layout of the area (the place denoted in orange) Adapted from [www.thelist.tas.gov.au](http://www.thelist.tas.gov.au)

**A note on terminology.**

The terms ‘Masonic Hall’ and ‘Masonic Temple’ appear generally interchangeable. It is noted that 3 Sandy Bay Road boasts the name ‘Masonic Temple’ above the door, which will be used here for the sake of consistency and in respect of that physical nomenclature. Particularly in the comparative analysis the word ‘hall’ is more common as that appears to be more utilised in smaller and rural buildings.

The term ‘lodge’ technically refers to the body or organisation who meet in the ‘hall’ or ‘temple’ rather than the building itself – and it may be more than one ‘lodge’ who uses a single building.

## 2. Statutory heritage requirements

The following heritage listings and overarching legislative provisions are relevant to the management of the historic cultural heritage values of the place:

### 2.1. Hobart Interim Planning Scheme 2015

#### Heritage Place

3 Sandy Bay Road (Masonic Temple) is listed as a *Heritage Place* on Table E13 of the scheme (Place #2776). Any demolition, development or subdivision of the place must be in accordance with the provisions of Part E13.7 of the Scheme (Development Standards for Heritage Places):

	Acceptable Solution	Performance Criteria
E.13.7.1 - Demolition	A1. No Acceptable Solution.	<p>Demolition must not result in the loss of significant fabric, form, items, outbuildings or landscape elements that contribute to the historic cultural heritage significance of the place unless all of the following are satisfied;</p> <ul style="list-style-type: none"> <li>(a) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;</li> <li>(b) there are no prudent and feasible alternatives;</li> <li>(c) important structural or façade elements that can feasibly be retained and reused in a new structure, are to be retained;</li> <li>(d) significant fabric is documented before demolition.</li> </ul>
E.13.7.2 – Building and Works other than Demolition	A1. No Acceptable Solution.	<p>P1. Development must not result in any of the following:</p> <ul style="list-style-type: none"> <li>(a) loss of historic cultural heritage significance to the place through incompatible design, including in height, scale, bulk, form, fenestration, siting, materials, colours and finishes;</li> <li>(b) substantial diminution of the historic cultural heritage significance of the place through loss of significant streetscape elements including plants, trees, fences, walls, paths, outbuildings and other items that contribute to the significance of the place.</li> </ul>

	A2. No Acceptable Solution.	<p>P2. Development must be designed to be subservient and complementary to the place through characteristics including:</p> <ul style="list-style-type: none"> <li>(a) scale and bulk, materials, built form and fenestration;</li> <li>(b) setback from frontage;</li> <li>(c) siting with respect to buildings, structures and listed elements;</li> <li>(d) using less dominant materials and colours.</li> </ul>
	A3. No Acceptable Solution.	<p>P3. Materials, built form and fenestration must respond to the dominant heritage characteristics of the place, but any new fabric should be readily identifiable as such.</p>
	A4. No Acceptable Solution.	<p>P4. Extensions to existing buildings must not detract from the historic cultural heritage significance of the place.</p>
	A5. New front fences and gates must accord with original design, based on photographic, archaeological or other historical evidence.	<p>P5. New front fences and gates must be sympathetic in design, (including height, form, scale and materials), to the style, era and characteristics of the building to which they belong.</p>
	A6. Areas of landscaping between a dwelling and the street must be retained.	<p>P6. The removal of areas of landscaping between a dwelling and the street must not result in the loss of elements of landscaping that contribute to the historic cultural significance of the place.</p>
E.13.7.3 - Subdivision	A3. No Acceptable Solution.	<p>P1. A proposed plan of subdivision must show that historic cultural heritage significance is adequately protected by complying with all of the following:</p> <ul style="list-style-type: none"> <li>(a) ensuring that sufficient curtilage and contributory heritage items (such as outbuildings or significant plantings) are retained as part of any title containing heritage values;</li> <li>(b) ensuring a sympathetic pattern of subdivision;</li> <li>(c) providing a lot size, pattern and configuration with building areas or other development controls that will prevent unsympathetic development on lots adjoining any titles containing heritage values, if required.</li> </ul>

**Place of Archaeological Potential**

The site is included in Table E.13.4 (Places of Archaeological Potential), as defined by Figure E.13.4.1 of the scheme, therefore Clause E.13.10.1 of the scheme applies. This means that any development on the subject site that requires excavation will need to be informed by a *statement of historical archaeological potential* (SoHAP) which will consider the site history, past development, the research potential of such (along a range of regional, thematic and temporal lines), and the disturbance history and propose an *archaeological zoning plan* for the site.

Any future development may require an *archaeological impact assessment* to be undertaken as informed by the SoHAP. If impact is likely, this will require consideration of design amendments to avoid or minimise that impact (particularly on very significant remains) – unless there are no prudent or feasible alternatives to that impact. If impact is likely and unavoidable, then an *archaeological method statement* will be required.

	Acceptable Solution	Performance Criteria
<b>E.13.10.1 – Building and Works other than Demolition</b>	A1. Building and works do not involve excavation or ground disturbance.	<p>P1. Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:</p> <ul style="list-style-type: none"> <li>a) the nature of the archaeological evidence, either known or predicted;</li> <li>b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;</li> <li>c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition;</li> <li>d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;</li> </ul> <p>(a) measures proposed to preserve significant archaeological evidence 'in situ'.</p>
<b>E.13.10.2 – Subdivision</b>	A1. Subdivision provides for building restriction envelopes on titles over land defined as the Place of Archaeological Potential in Table E13.4.	<p>P1. Subdivision must not impact on archaeological resources at Places of Archaeological Potential through demonstrating either of the following:</p> <ul style="list-style-type: none"> <li>(a) that no archaeological evidence exists on the land;</li> <li>(b) that there is no significant impact upon archaeological potential.</li> </ul>

Given that the building has entire site coverage, any use of the building within the current building envelope that does not require excavation is unlikely to trigger this provisions, therefore the need for substantial archaeological inputs in the future are unlikely.

**Heritage Precinct**

The subject site is not within any Heritage Precinct as defined by Table E13.2 and depicted on Map E13.3 of the Scheme, therefore the provisions of Clause E13.8 do not apply.

**Signs Code**

Clause E.17.7.2 *Standards for signs on Heritage Places subject to the Heritage Code or within Heritage Precincts or Cultural Landscape Precincts* is applicable to the place in regard to any proposed signage.

	Acceptable Solution	Performance Criteria
E.17.7.2 – Standards for signs on Heritage Places subject to the Heritage Code or within Heritage Precincts or Cultural Landscape Precincts	A1. No Acceptable Solution.	P1. A sign on a Heritage Place listed in the Historic Heritage Code or within a Heritage Precinct or Cultural Landscape Precinct must satisfy all of the following: <ul style="list-style-type: none"> <li>a) be located in a manner that minimises impact on cultural heritage significance of the place or precinct;</li> <li>b) be placed so as to allow the architectural details of the building to remain prominent;</li> <li>c) be of a size and design that will not substantially diminish the cultural heritage significance of the place or precinct;</li> <li>d) be placed in a location on the building that would traditionally have been used as an advertising area if possible;</li> <li>e) not dominate or obscure any historic signs forming an integral part of a building’s architectural detailing or cultural heritage values;</li> <li>f) have fixtures that do not damage historic building fabric, including but not restricted to attachments to masonry and wood, such as to using non-corrosive fixings inserted in mortar joints;</li> <li>g) not project above an historic parapet or roof line if such a projection impacts on the cultural heritage significance of the building;</li> <li>h) be of a graphic design that minimises modern trademark or proprietary logos not sympathetic to heritage character;</li> <li>i) not use internal illumination in a sign on a Heritage Place unless it is demonstrated that such illumination will not detract from the character and cultural heritage values of the building.</li> </ul>

Further to Clause E13.5.1 of the Scheme, the Planning Authority may require the following to accompany any application for use or development of a Heritage Place:

- (a) a conservation plan;
- (b) photographs, drawings or photomontages necessary to demonstrate the impact of the proposed development on the heritage values of the place;
- (c) a statement of significance;
- (d) a heritage impact statement;
- (e) a statement of compliance;
- (f) a statement of archaeological potential;
- (g) an archaeological impact assessment;
- (h) an archaeological method statement;

## 2.2. Historic Cultural Heritage Act 1995

The Masonic Temple, 3 Sandy Bay Road, is listed on the Tasmanian Heritage Register (place ID 7490); therefore, the place is subject to the provisions of the *Historic Cultural Heritage Act 1995* (HCHA).

Part 6 of the HCHA (Heritage Works) sets the process by which approvals for works may be gained from the Tasmanian Heritage Council (THC):

### 35. Heritage works require heritage approval

- (1) A person must not carry out any heritage works unless those heritage works have heritage approval.
- (2) For the purposes of subsection (1), heritage works are taken to have heritage approval if, and only if –
  - (a) in a case where a certificate of exemption has been issued, the heritage works are carried out in accordance with –
    - (i) that certificate of exemption; and
    - (ii) if a discretionary permit or other permit is required for the heritage works under the Planning Act, that discretionary permit or other permit; or
  - (b) in a case where a certificate of exemption has not been issued, the heritage works are carried out in accordance with a discretionary permit.
- (3) It is a defence in proceedings for an offence under subsection (1) if the defendant establishes that –
  - (a) the heritage works were carried out in response to an emergency; and
  - (b) the heritage works were, both as to nature and extent, reasonably necessary for the purposes of responding to the emergency; and

- (c) in the circumstances, it was not practicable to seek a certificate of exemption; and*
- (d) the defendant, before, while or as soon as practicable after carrying out the heritage works, notified the Heritage Council, in writing, of the emergency and the details of the heritage works.*

Sections 36-41 set the process for the lodgment and assessment of applications for a heritage works permit, via a Discretionary Development Application under the Land Use Planning and Approvals Act 1993.

Section 42 describes the process whereby certain works may be exempt from the requirement of s.35:

#### **42. Certificates of exemption for heritage works**

- (1)** A person may apply to the Heritage Council for a certificate of exemption for heritage works.
- (2)** The exemption certificate application –
  - (a)** is to be in a form provided or approved by the Heritage Council; and
  - (b)** is to be supported by such information as the Heritage Council requires, either at the time of lodgment or subsequently.
- (3)** The Heritage Council may –
  - (a)** approve the exemption certificate application; or
  - (b)** refuse the exemption certificate application.
- (4)** Without limiting its discretion, the Heritage Council must approve the exemption certificate application if it is reasonably satisfied that the heritage works –
  - (a)** are identified in the works guidelines as works that will have no impact or only negligible impact on the historic cultural heritage significance of the relevant registered place or heritage area; and
  - (b)** are capable of being carried out in accordance with the works guidelines.

Whilst the HCHA provides no specific detail as to how particular proposals are considered, nor does it provide any indicative thresholds of what may be considered to have *no or negligible* heritage impact, the THC/Tasmanian Government publication *Works Guidelines for Historic Heritage Places* (November 2015)<sup>1</sup> provides further detail on the application process, guiding principles and the basis for decisions made by the THC.

In addition, the THC has a series of practice notes and technical guides, available via [www.heritage.tas.gov.au](http://www.heritage.tas.gov.au) which provide useful guiding principles for how the THC are expected to assess and determine applications for heritage works.

#### **5.3. Other statutory heritage registers/lists**

The subject site is not listed on any of the following statutory registers:

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<sup>1</sup> [http://heritage.tas.gov.au/Documents/Works\\_Guidelines\\_FINAL\\_Nov2015.pdf](http://heritage.tas.gov.au/Documents/Works_Guidelines_FINAL_Nov2015.pdf)

- The National Heritage List
- The Commonwealth Heritage List
- The World Heritage List

Nor is it included in any buffer zones arising from those lists, therefore is not subject to the historic heritage provisions of the respective Acts which enable statutory input into development of places on those lists.

### 3. Assessment methodology

This assessment has been undertaken in accordance with the ICOMOS Australia **Burra Charter**, which is considered to be the Australian heritage industry’s benchmark for assessing, understanding and managing heritage values.

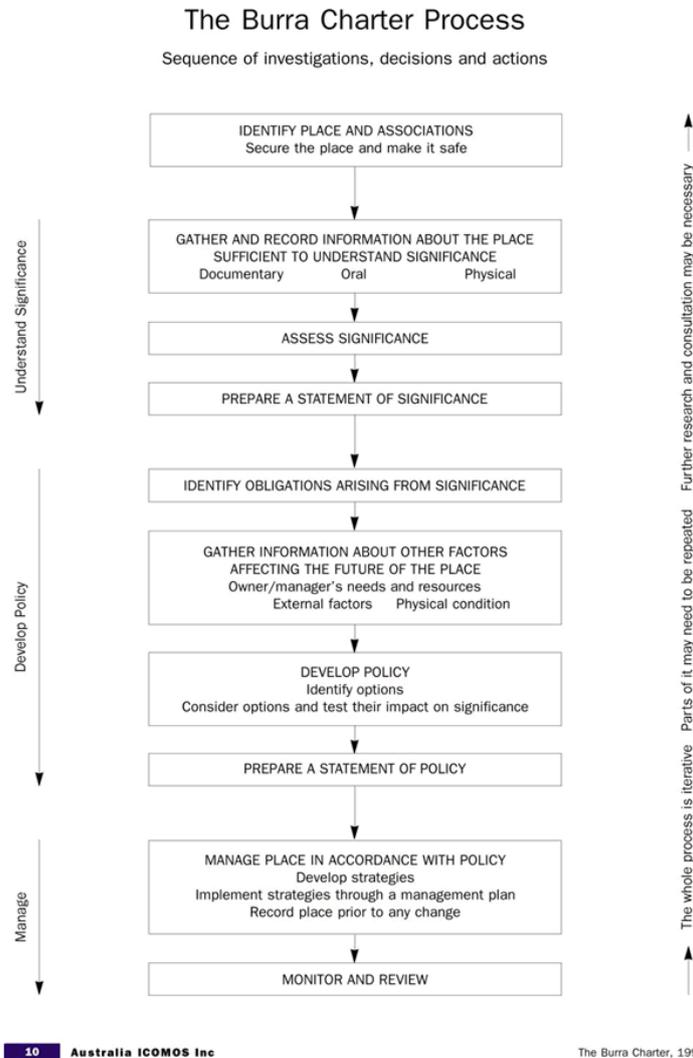


Figure 3.1 – The Burra Charter Process. ICOMOS Australia.

The statutory provisions and consequent responsibilities as outlined in Section 2 have also been considered in formulating this document.

Whilst this document does not intend to be a comprehensive conservation management plan, it takes the basic principles of conservation planning, as outlined in J.S. Kerr's *The Conservation Plan*<sup>2</sup>, in order to develop the policies upon which the conservation of the place (and assessment of development impact) is based. This document has also been developed with regard to the standard content of conservation management plans as detailed by the New South Wales Heritage Office's *A Suggested Table of Contents for a Conservation Management Plan*<sup>3</sup>, as well as the New South Wales Heritage Office guidelines for the preparation of brief conservation management strategies.<sup>4</sup>

Praxis Environment utilises that process in the following flowchart, which is expected to be used by the proponent and design team of any development to understand and respond to historic heritage values in the development process. Figure 2.2 depicts this process:

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<sup>2</sup> KERR, J. (2000): *The Conservation Plan*. National Trust of NSW, Sydney.

<sup>3</sup> [http://www.heritage.nsw.gov.au/docs/cmp\\_contents2.pdf](http://www.heritage.nsw.gov.au/docs/cmp_contents2.pdf)

<sup>4</sup> [http://www.heritage.nsw.gov.au/docs/CMS\\_part1investigation.pdf](http://www.heritage.nsw.gov.au/docs/CMS_part1investigation.pdf)

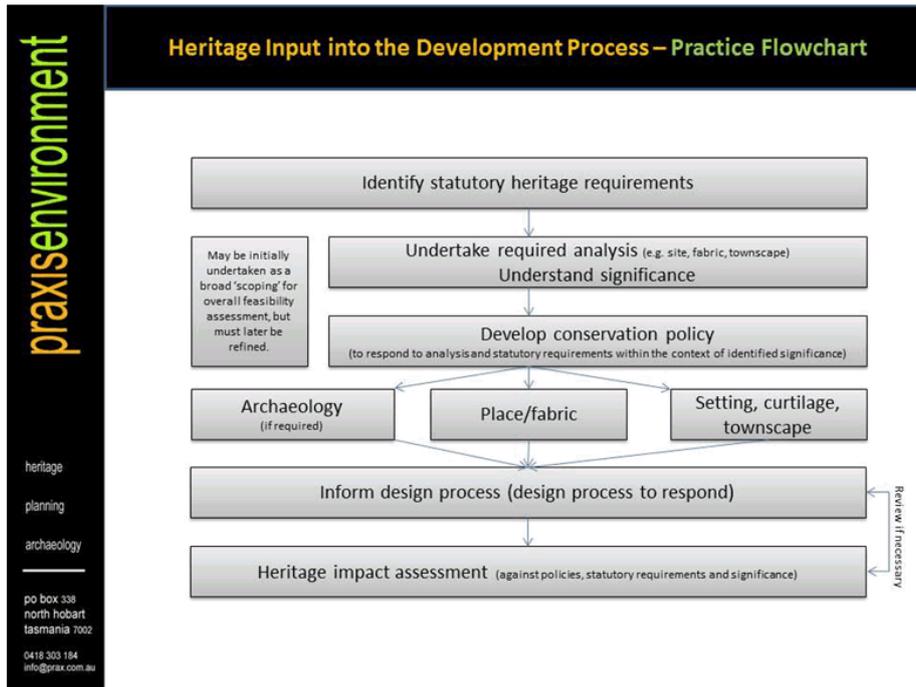


Figure 3.2 – Practice flowchart for the application of conservation management planning in the development process.

## 4. Brief overview of the history and historical context of the place

### 4.1. Historical research methodology

As outlined in the methodology in Section 3, the key to assessing historic heritage significance is to gain an understanding of the history of the place, the context of it within its surrounds, associated thematic contexts, and other intangible values (e.g. community value, value associated with people, events etc.). Whilst the scope of the current assessment did not allow a full and comprehensive historical research project into the place, it has been commissioned to provide sufficient understanding of the physical context of the building and surrounds upon which decisions regarding the management of heritage values can be made. Where relevant, this document suggests where further research may be required to further clarify or articulate particular points (e.g. associative values).

#### Primary source material

The following sources were consulted as part of this project, in relation to the place, and within wider environs of that portion of Sandy Bay Road/Davey Street etc.

- AE417 (Hobart City Council building records) series – Tasmanian Archive and Heritage Office
- Newspaper articles, sketches and photographs from a variety of primary (and secondary) sources, including TAHO cardfile references (i.e. people, place indexes, WAYN Index, Trove etc.).
- Early town maps/surveys of Hobart – held by DPIPWE and TAHO, with particular focus on the Lower Harrington Street/Sandy Bay Road area.
- Aerial photographs – held by DPIPWE (Aerial Photograph Division, 134 Macquarie Street, Hobart).

#### Secondary sources

No secondary sources were located which are of direct relevance to the management of the historic heritage values of the building. The Tasmanian Heritage Register datasheet provides only a very cursory history.

#### Previous heritage studies & archaeological investigations

There are no known previous conservation planning documents, heritage/streetscape assessments or archaeological investigations specifically regarding the place. The Tasmanian Heritage Council Datasheet for the place gives very little information of use – providing an extremely brief history and 'standard' statements of significance.

## 4.2. Overview of the history of the place

### Pre-development of the subject site

The land was the home of the Mouheneener people for tens of thousands of years, prior to displacement by European settlers following 1804.

Subsequent to the settlement of Sullivan's Cove in 1804, following the disbandment of the initial European settlement of Risdon Cove, the settlement of Hobart Town began to grow in a somewhat organic matter. Following Governor Macquarie's inspection of 1811, Surveyor James Meehan was engaged to rationalise the layout of the settlement and install a grid-pattern of streets, as seen on his 1811 survey plan (DPIPWE Hobart 131). At this time, Harrington Street was formalised (the subject site was known as a Harrington Street address until the early c20th before being changed to Sandy Bay Road), however settlement was concentrated further eastward around the Sullivan's Cove area.

Several 1820s survey plans of Hobart (Figures 4.1-4.3) show the subject site as undeveloped and unallotted land, between the barracks and the waterfront.

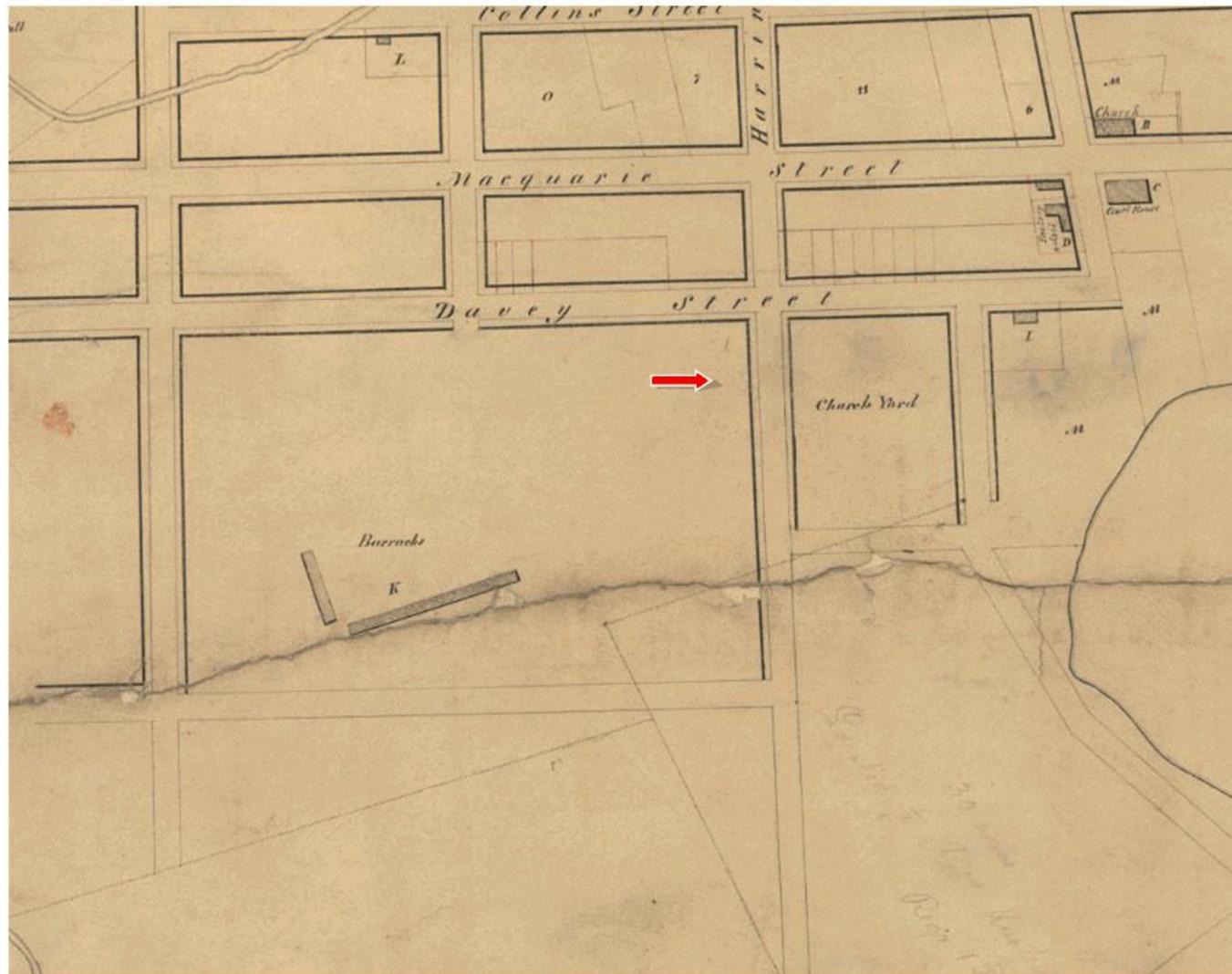


Figure 4.1 – A c1820s survey plan of Hobart Town, showing the subject site (approximately denoted by the red arrow) as unallotted and undeveloped land between the Barracks and the waterfront. DPIPWE Hobart 12.

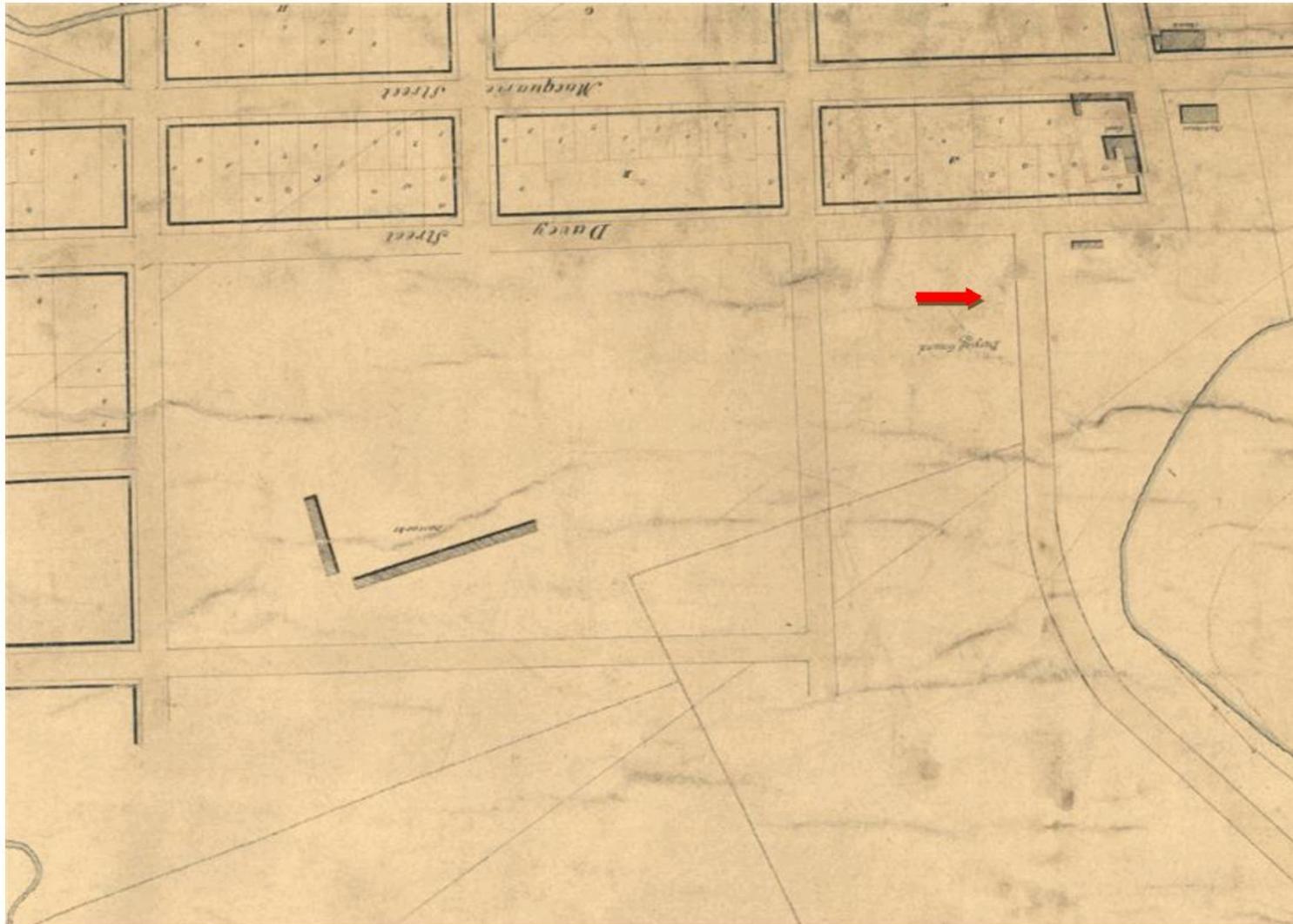


Figure 4.2 – A c1820s survey plan of Hobart Town, showing the subject site (approximately denoted by the red arrow) as unallotted undeveloped land between the Barracks and the waterfront. DPIPWE Hobart 13.



Figure 4.3 – A c1820s survey plan of Hobart Town, showing the subject site (approximately denoted by the red arrow) as undeveloped land between the Barracks and the waterfront. DPIWPE Hobart 4.



Figure 4.4 – Excerpt from a c1830s map of Hobart and surrounds, the subject site denoted in red. DPIPWE Map Hobart 5

By the early 1830s, the unallotted land between the barracks and the waterfront had been subdivided, and the Heathfield estate had been established just north of the subject site (see Figure 4.4) of which the subject site was part. The subject site was still undeveloped at that time but appears to have comprised part of the gardens of Heathfield. By 1839 Wivenhoe (137 Hampden Road) and Devoren Cottage (139 Hampden Road) had been established to the south of the subject site, and Frankland's 1839 survey of Hobart Town shows the area containing the subject site as what appears to be a formal landscaped garden (see Figure 4.5). The 1841 census map (largely based on Frankland's 1839 survey) shows the subject site clearly as part of the landscaped gardens of Heathfield (see Figure 4.6).

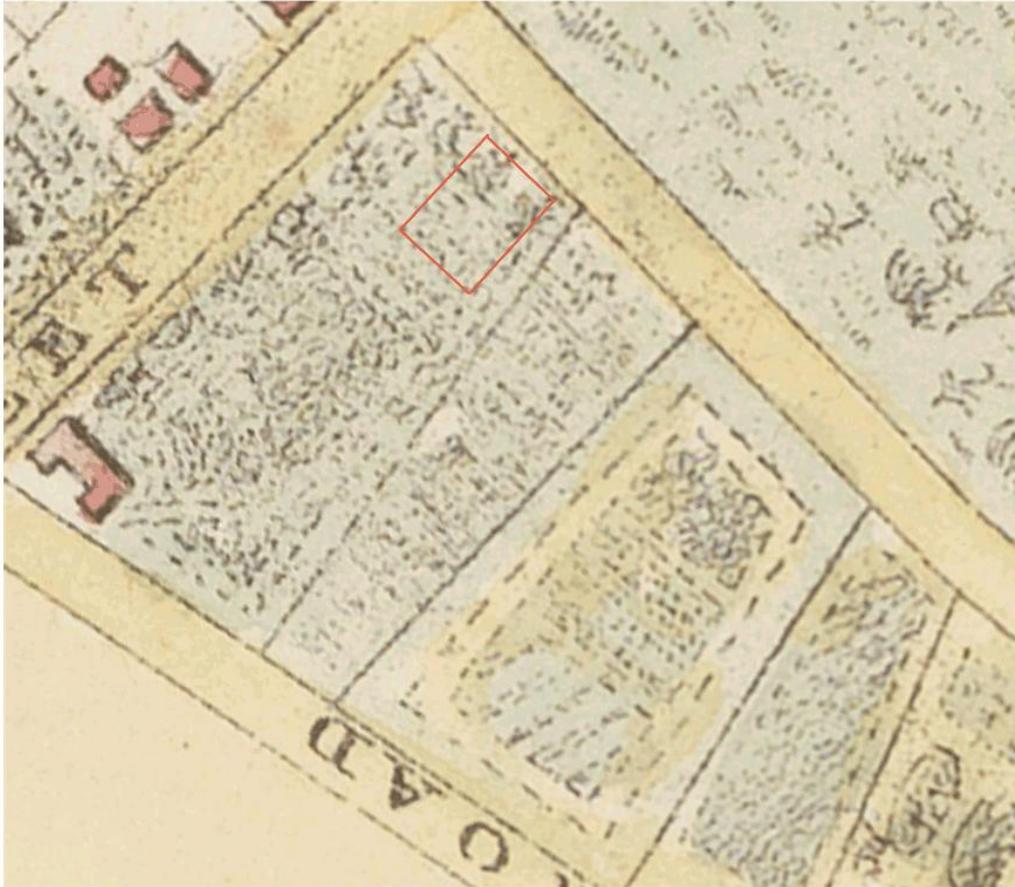


Figure 4.5 – Excerpt from Frankland’s 1839 map of Hobart and surrounds. State Library of Tasmania, Allport Stack 912.94661MAP.



Figure 4.6 - Excerpt from the 1841 census map of Hobart and surrounds. Tasmanian Archive and Heritage Office, CS08-17-578.

#### The Heathfield Estate

Acting Commissary General Affleck Moodie was granted c1828 the 3 acres 6 perch allotment shown in Figure 4.4. Moodie built 'Heathfield' c1829, and the subject area appears to have been part of the gardens. Moodie's grant was confirmed in October 1836<sup>5</sup>, two years later, he died at Heathfield aged 73<sup>6</sup>.

<sup>5</sup> DPIPWE The LIST CT 101/64

<sup>6</sup> TAHO RGD 36/1/1 Number 14



Figure 4.7 - Excerpt from Sprent's 1845 map of Hobart and surrounds, the subject site denoted in red (oddly not showing Heathfield?). ([www.thelist.tas.gov.au](http://www.thelist.tas.gov.au)).

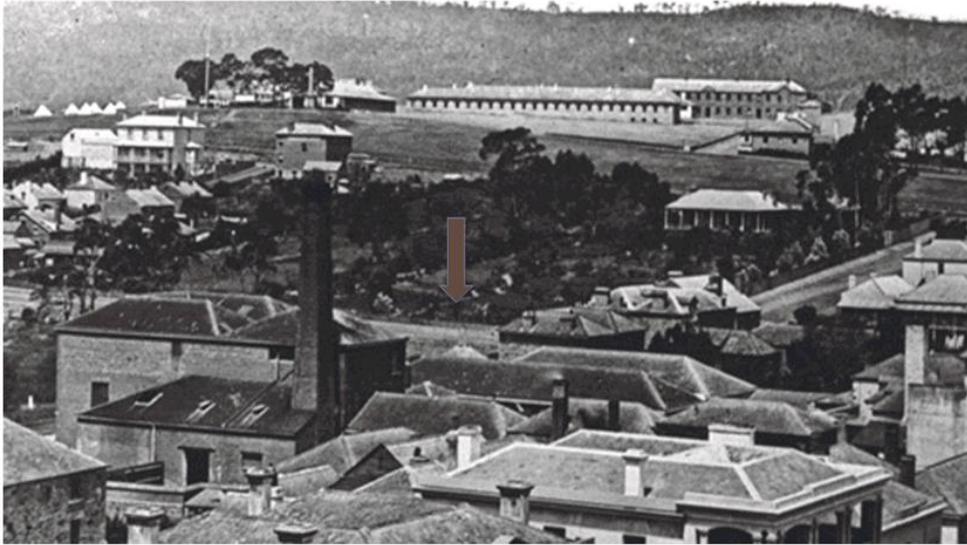


Figure 4.8 – Excerpt from an 1857 Alfred Abbott panorama of Hobart, the red arrow depicting the approximate subject site. Libraries Tasmania AUTAS00116252550W800.



Figure 4.9 – Photograph by Alfred Winter 1870, the red arrow denoting the landscaping of Heathfield at the location of the subject site. Libraries Tasmania, W.L. Crowther Collection, AUTAS001125298679.

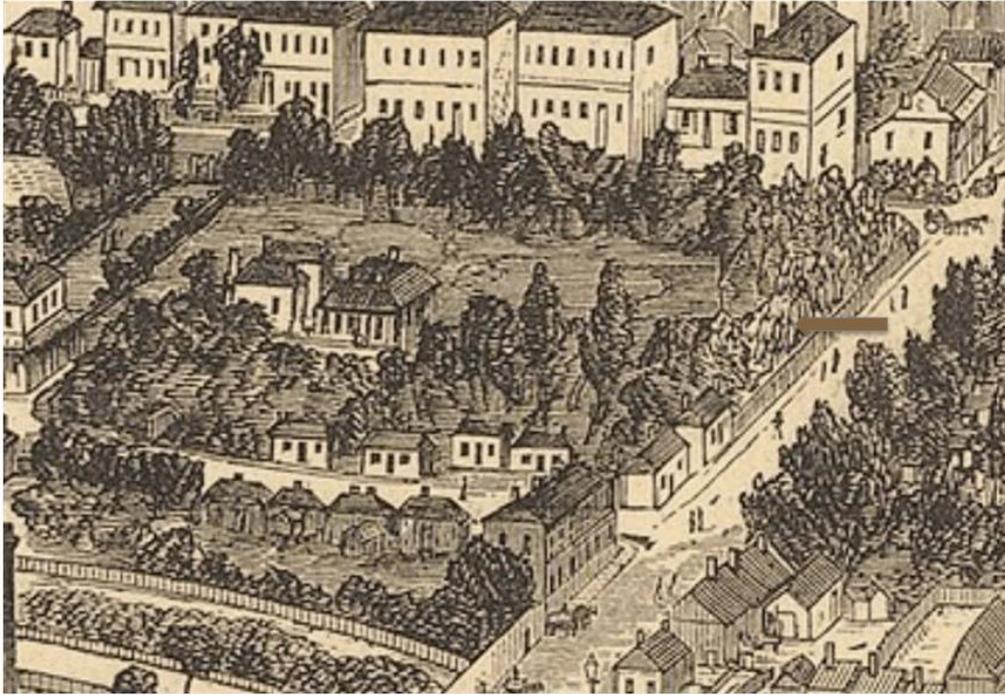


Figure 4.10 – ‘Balloons eye view of Hobart’ showing the landscaped frontage of Heathfield. Australasian Sketcher 10 May 1879



Figure 4.11 – ‘Birds Eye View of Hobart’ showing the landscaped gardens of Heathfield. The Town and Country Journal 17/11/1894:26-27.



Figure 4.12 – Detailed excerpt from the 1907 Metropolitan Drainage Board plan of the Hobart CBD, the subject site denoted by red lines. State Library of Tasmania TL.MAP 881.11 GBBB (Map Hobart 41).

Following Moodie’s death, the property remained in his estate until 1920, eventually passing to his grandson’s wife, Jane Moodie<sup>7</sup>. In January 1920, Jane Moodie instructed Websters to put up for auction the 3 acre 6 perch Heathfield estate. The listing for this sale includes mention of “the nursery gardens of Mr Latham”<sup>8</sup>. Valuation rolls from 1880 and 1890 suggest that these nursery gardens were at least partially on the subject site and it is likely that the site only included ephemeral sheds associated with nursery operations during that time (see Figure 3.16).

<sup>7</sup> *The Argus* 23 May 1888 p12

<sup>8</sup> *The Mercury* 17 January 1920 p12

The Heathfield estate sold to Cecil Walker for £8,000 in April 1920<sup>9</sup>. This block was then transferred in 1922 to Elinor Wayne Walker prior to her marriage to Alan Propsting in August 1922<sup>10</sup>. Between 1922 and 1933, Elinor Propsting (later Lake) slowly sold the estate as shown in Survey Diagram Hobart 8/34. The first major portion on the corner of Harrington and Davey Streets was sold to the Commonwealth of Australia for a telephone exchange in 1925. Elinor Lake sold her remaining interest in Heathfield to Charles Atkins in 1933, who formed the entity *Heathfield Court P/L* and developed Heathfield Court in the central portion of the estate soon after.

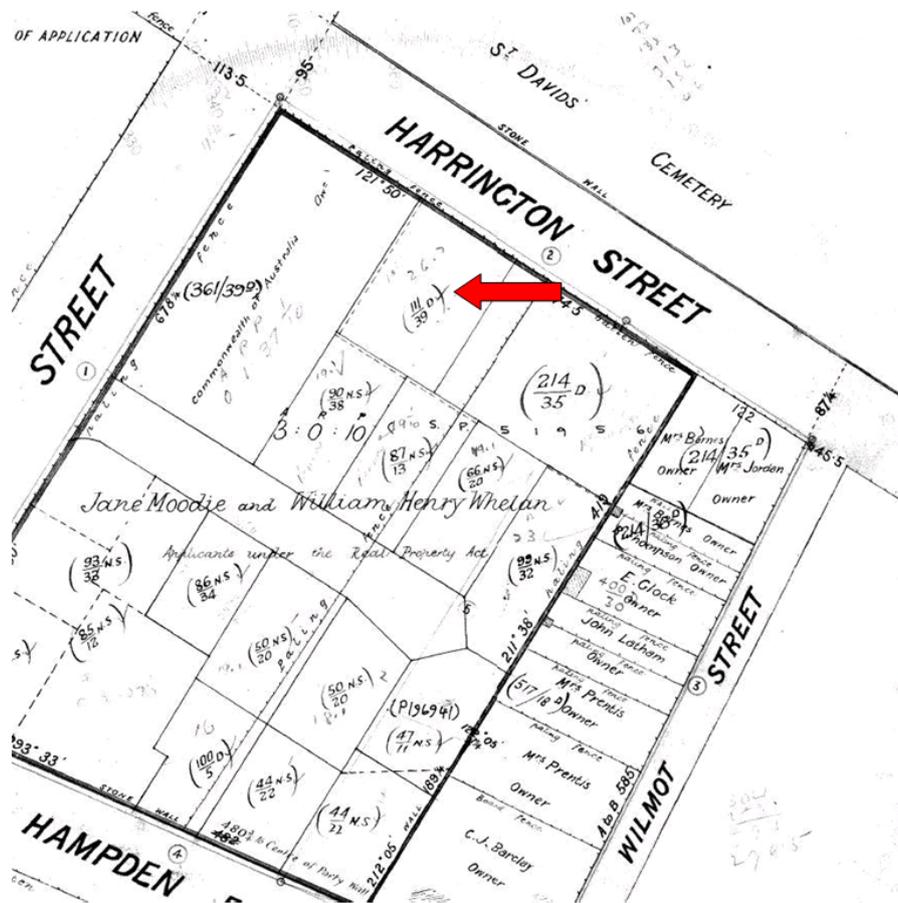


Figure 4.13 – Detail from DPIPWE Hobart 8/34, showing the subdivision of the Heathfield estate into 20 lots, and forming Heathfield Avenue.

<sup>9</sup> DPIPWE The LIST CT247/126

<sup>10</sup> DPIPWE The LIST CT275/74

**The Masonic Hall**

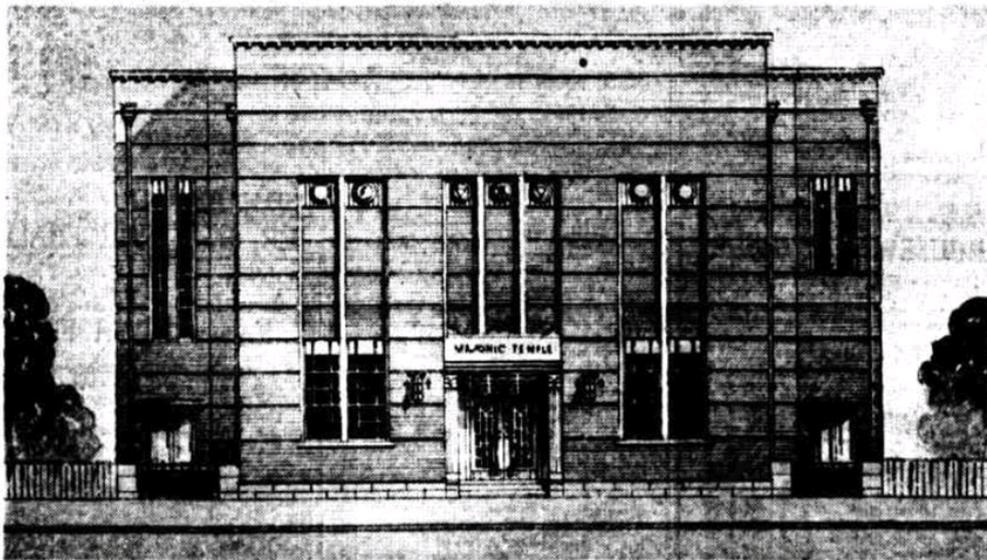
In 1937, Atkins sold a block of the Heathfield estate to the Freemasons for the erection of a new temple. In May 1937, *The Mercury* announced that:

*MASONIC TEMPLE. New Site Acquired in Hobart*

*THE directors of the Tasmanian Freemasons' Hall Co. have purchased a block of land adjoining the Telephone Exchange building on the corner of Harrington and Davey streets, Hobart, for the erection of a new Masonic Temple in place of the Masonic Hall, Murray St., Hobart, which recently was sold. The land acquired has a frontage of 70ft. on Harrington St., facing St. David's Park, and should lend itself to the erection of an imposing building. Mr. A. L. Crisp has been appointed architect.*

In 1937, Architect Lauriston Crisp drew plans for a new Masonic Hall in Harrington Street (on the subject site). The original plans for the building remain showing a great deal of detailing of the building.

## WHAT NEW TEMPLE WILL LOOK LIKE



**PLAN OF THE Masonic Temple to be erected in Harrington St., Hobart. The architect is Mr. A. L. Crisp.**

Figure 4.14 – An artist's impression of the new temple. *The Mercury*, 7<sup>th</sup> March 1938:3.

The contract for construction of the new building was won by Gillham Bros. of Hobart. *The Mercury* on the 22<sup>nd</sup> January 1938 reported:

*Mercury* (Hobart, Tas. : 1860 - 1954), Saturday 22 January 1938, page 13

MASONIC TEMPLE FOR HOBART - Contract Let For New Building - Construction Details

*A contract was signed yesterday for the construction of a new Masonic temple at Hobart. The site chosen is in Harrington St., adjacent to the Tele-phone Exchange, and the building will be constructed by Gillham Bros, of Hobart, the contract price being £12,573. It is expected that the foundation stone of the new temple will be laid by the M. W. Past Grand Master (Sir Ernest Clark) within six weeks, and that the temple will be dedicated by the M. W. Grand Master (Bro. Stanley Dryden) in about six months. The contract was signed by Mr. H. H. Cummins, as chairman of directors of the Tasmanian Freemasons' Hall Co. The architect is Mr. A. L. Crisp. The temple will be devoted entirely to Masonic purposes. The building will be of brick and stone, and the front and sides will be of cream brick with stone facings, with a stone porch and entrance.*

*The plan provides for two lodge rooms, one of large size and the other about the size of the lodge room in the Murray St. premises, which have been sold. The building also will contain a supper room and banquet hall, and offices.*

*The design is new for Hobart. There will be a fine entrance hall into a crush circular hall, from which the entrances to the lodge rooms and supper rooms will lead. Provision also is being made for ample room for a Masonic choir, which it is proposed to form.*

*The Mercury* on the 7<sup>th</sup> March 1938 (p8) reported on the laying of the foundation stone for the new Masonic Temple:

*State Governor Lays Foundation Stone of New Masonic Temple HOBART, Saturday. In the presence of the Grand Master of the Grand Lodge of Tasmania (M.W. Bro. Stanley Dryden), Grand Lodge Officers, and a large number of brethren, the Governor (M.W. Bro. Sir Ernest Clark, Past Grand Master) laid the foundation stone of the new Masonic Temple in Harrington-street this morning, with full Masonic ritual. On the arrival of His Excellency at the site of the building he was welcomed by the Chairman of Directors of Tasmanian Freemasons' Hall Co. Ltd. (W. Bro. H. H. Cummins, P.G.D.). As Past Grand Master, His Excellency then delivered an address*

*"The building of which I am about, to lay the foundation stone, "His Excellency said, "will afford the Masons of Hobart and neighbourhood facilities to increase their usefulness as members of the Ancient and Honourable*

*Order of Freemasons, and as citizens of this State. I doubt whether I can do better than quote the words of Governor du Caine, who, 65 years ago, laid the foundation stone of our present hall. He said 'I find myself bound as a Mason to fear God and honour the Queen, to aid all works of charity and philanthropy, to be a good man and true, and strictly to obey the moral law. I find myself further bound not only to interfere in no way with religious or political opinions professed by others, but bound to uphold the best interests of the community, and zealously to promote the welfare and prosperity of the country in which I reside.' "These words are true of Masonry to-day," he continued; "and the Order remains unaffected by modern society and associations with similar objects. If proof of this were needed it may be found in the fact that last year in England alone (I am not speaking of Great Britain) over a million pounds was expended on benevolence by the Masonic Order, and even in this small State, much has been done, and is being done, in this direction. Great Enterprise "Masonry in Tasmania has a history of rather more than 100 years, but it was not until 1873 that the three lodges which then existed in Hobart resolved to build a Masonic Hall in Murray-street. At that time there were only 200 to 300 Masons to use it, and those who built it not only showed great enterprise, but great foresight and regard for their brethren in time to come. The growth of the Order in Hobart and neighbourhood since that day has made it necessary to provide better accommodation for there are now 10 Lodges and three Chapters meeting in this city and the members number over 1200. "I need not remind most of my hearers," Sir Ernest concluded, "that our ritual and our social intercourse are founded on the Masonic virtues of brotherly love, relief and truth. I trust that those virtues may increase individually in our hearts, and be even more freely exercised than in the past; that by our individual rectitude and reliability the reactions of Masonry on the life of the community may be increasingly evident and beneficial. I should like to voice the thanks of' the Order (where they are mostly due) to the many devoted and enthusiastic workers who have given their brains, their energies, and their time in order to make this building possible." At the conclusion of His Excellency's address the V.W. Grand Chaplain (V.W. Bro. Canon W. R. Barrett) read an Ode and the inscription on the foundation stone was read by the G. Superintendent of Works (W. Bro. R. S. Waring). Coin of the realm and historical documents were deposited in a cavity beneath the stone by plumb rule, level, square, according to practice, and His Excellency then proved the adjustment of the stone by plumb rule, level, square, and mallet, handed him in succession by Grand Lodge Officers. His Excellency, as Past Grand Master, then gave the stone three blows with the mallet, saying: "May the Almighty Architect of the Universe look down with benignity upon our present undertaking and crown the edifice, of which we have now laid the foundation, with every success." Following the singing of an Ode, the P.G.M. (M.W. Bro. H. L. D'Emden) delivered the Cornucopia to His Excellency, who scattered corn from it on to the foundation stone as an emblem of plenty. The S.W.G. (R.W. Bro. W. S. A. Crookall) delivered to him a vase containing oil to be poured on the stone as an emblem of joy and gladness and the J.G.W. (R.Wor. Bro. J. H. Bastick) handed the vase containing oil to be poured on the stone as an emblem of friendship and brotherly love. A prayer by the V.W. Grand Chaplain followed, after which the plan of the building was presented by the G. Superintendent of*

*Works, to the architect. W. Bro. Rt. Rev. Dr. R. S. Hay (Bishop of Tasmania), in the course of an oration, said the very idea of such an edifice as was to be erected on the spot, being made from rough elements, caused them to reflect. The same changes that had been effected in that foundation stone suggested a grander conception of what might be brought about in regard to men. "No matter what may be a man's religion, or his politics," he said, "when he becomes a free, accepted Mason, he enters a temple of brotherhood where is cultivated a true sense of God. Differences of view in regard to politics has no discordant effect in the deliberations of Masons. It is the handmaiden of religion. Whatever one's religion or political party or class, men may meet on the common ground of Masonry so long as they believe in God. "May this building," he said, "prove a school of learning for the transmission of the great principles of the Craft, not only for those who will occupy it in the near future, but for those in years to come. May it be a school in which you, and those who come after you, may study to show themselves approved of God." The singing of the National Anthem concluded the ceremony. Music during the ceremony was supplied by musicians directed by Bro. T. W. Hopkins.*

The building was completed by January 1939, with *The Examiner* reporting on the 10<sup>th</sup> January 1939 (p6):

*TEMPLE AT HOBART New Edifice for Freemasons HOBART, Monday. The dedication of the new Masonic Temple, which has been constructed at a cost of £15,573, will be performed on Sunday next at 3 p.m. by the Grand Master of the Grand Lodge of Tasmania (Most Worshipful Brother Stanley Dryden) assisted by Grand Lodge officers. Brethren from all parts of the state and also representatives of the order on the mainland will attend the ceremony. The new temple, which is adjacent to the telephone exchange building in Harrington-street, has just been completed by Messrs. Gilham Bros., the contractors, the foundation stone having been laid by the Immediate Past Grand Master, Most Worshipful Bro. Sir Ernest Clark, on March 5 last year. The new site was obtained by the Tasmanian Freemasons' Hall Co. Ltd. at a cost of £ 1500. It has a frontage of 70 feet and a depth of 100 feet. The new temple, which is a two-storey building, has been erected primarily for Masonic purposes. there are two lodge rooms, two supper rooms, and all necessary office accommodation and conveniences. The main lodge room is on the ground floor and at the rear of the building. It measures 51 feet by 36 feet, while the secondary room is 35 feet by 27 feet. The difficulty with the former premises at the Masonic Hall in Murray-street was the smallness of the space available, but all needs are now amply met. The building is constructed of brick and stone throughout, the facing being of cream bricks with a stone porch at the entrance of original design. It opens into a circular assembly room which leads to the lodge rooms upstairs. Provision is made in the main lodge room for the choir to be accommodated in a gallery at the end of the temple itself. The architect for the building was Mr. A. Lauriston Crisp. Very Worshipful Brother Aaron Cohen, will represent the Grand Lodge of Victoria at the dedication ceremony.*

THE MERCURY, MONDAY, MARCH 7, 1938

**SETTING OF FOUNDATION STONE FOR TEMPLE**



**HIS EXCELLENCY THE GOVERNOR pouring oil on the foundation stone for the new Masonic Temple at Hobart at the ceremony of setting the stone. (See letterpress.)**

Figure 4.15 – Laying the foundation stone of the new temple. The Mercury, 7/3/1938.

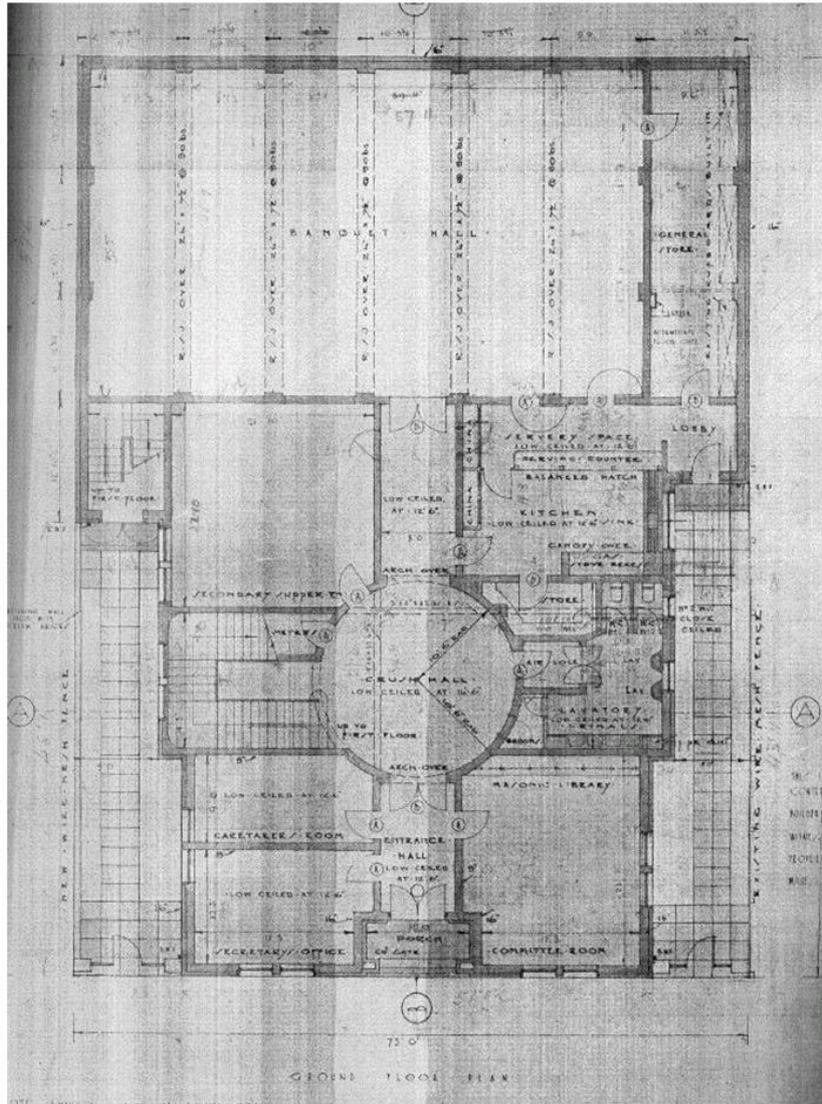


Figure 4.16 – Ground floor plan of proposed Masonic Hall. Tasmanian Archive and Heritage Office AE417/1

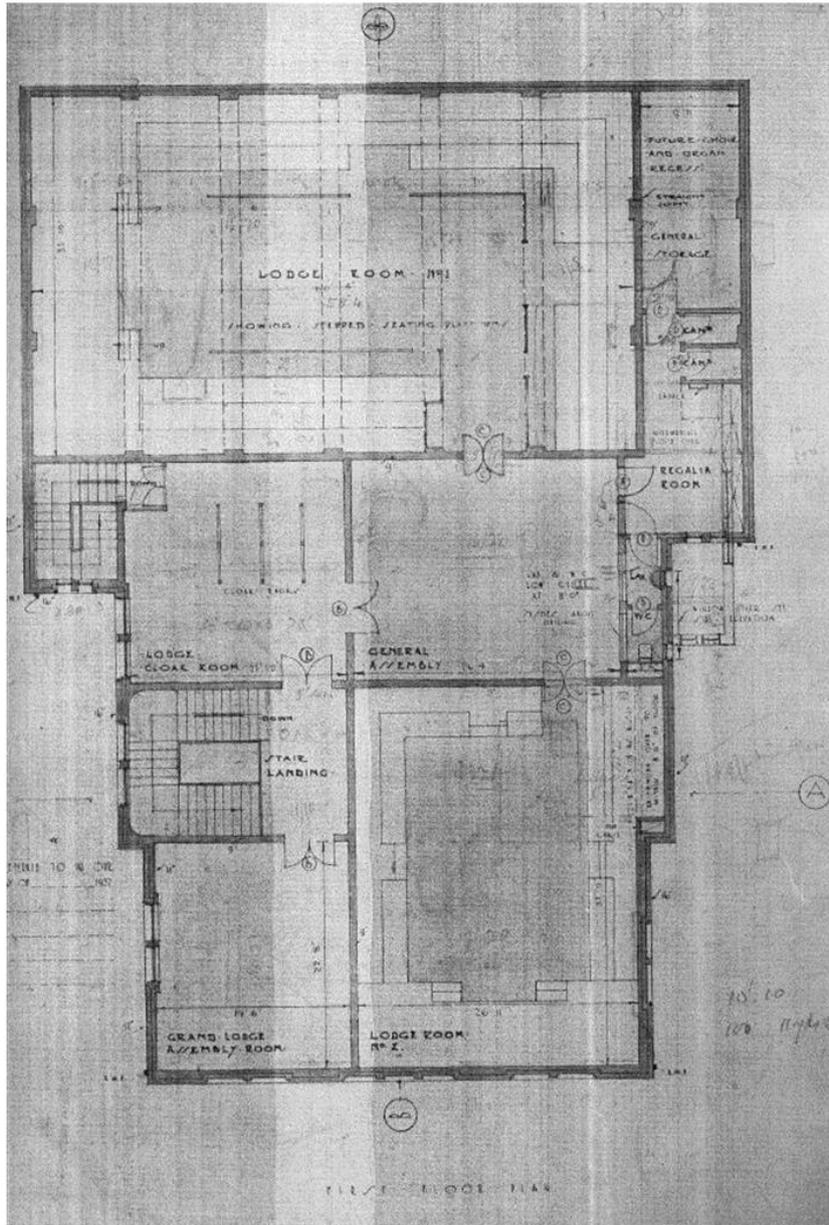


Figure 4.17 – Ground floor plan of proposed Masonic Hall. Tasmanian Archive and Heritage Office AE417/1

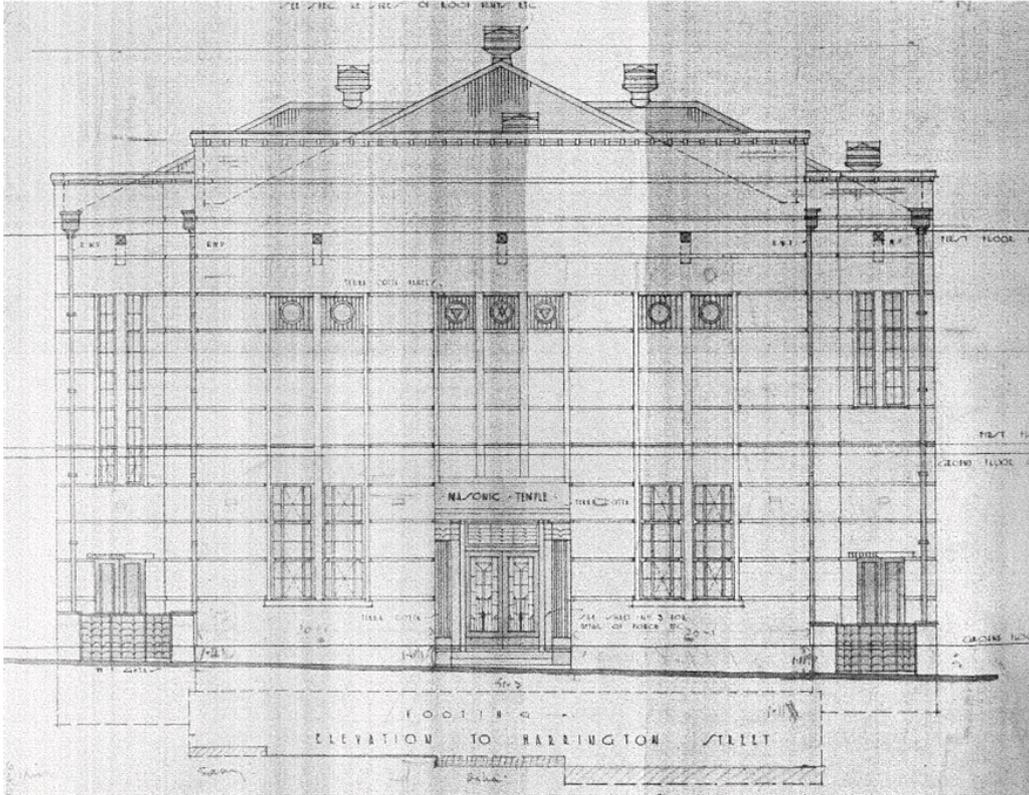


Figure 4.18 – Ground floor plan of proposed Masonic Hall. Tasmanian Archive and Heritage Office AE417/1

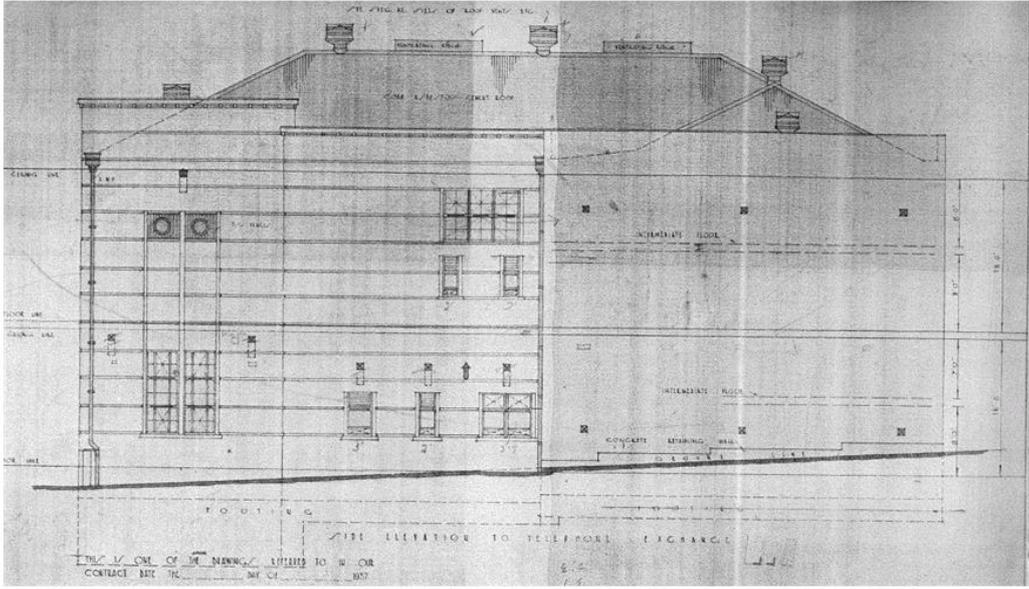


Figure 4.19 – Ground floor plan of proposed Masonic Hall. Tasmanian Archive and Heritage Office AE417/1

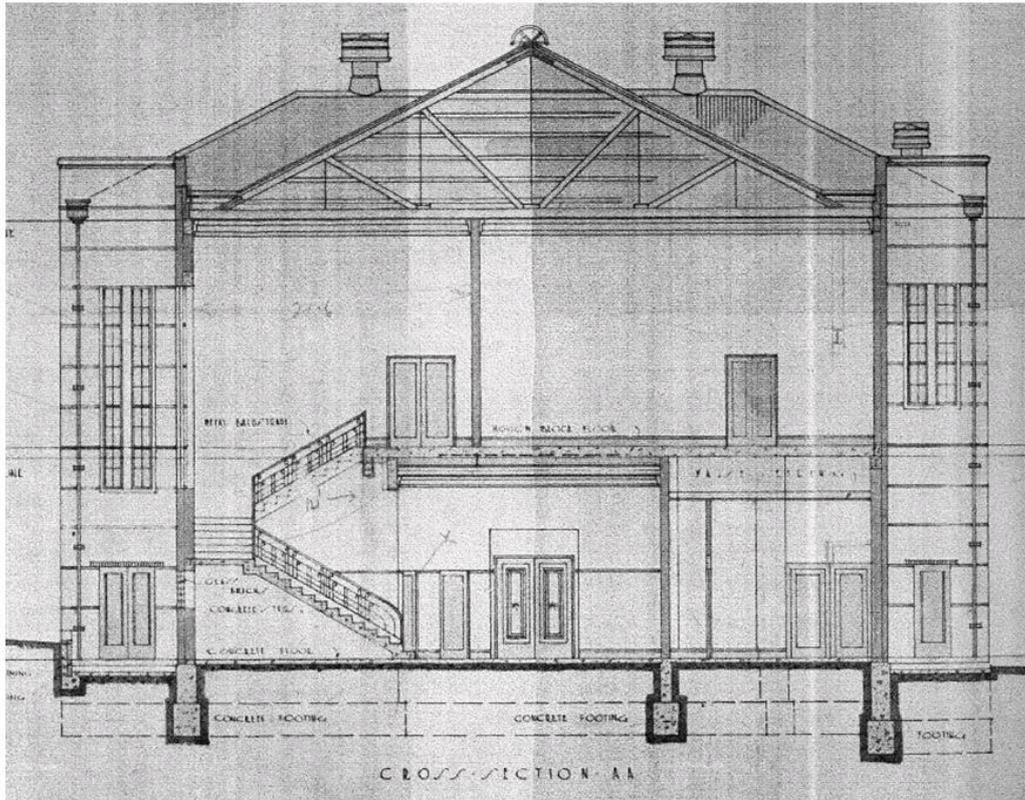


Figure 4.20 – Ground floor plan of proposed Masonic Hall. Tasmanian Archive and Heritage Office AE417/1

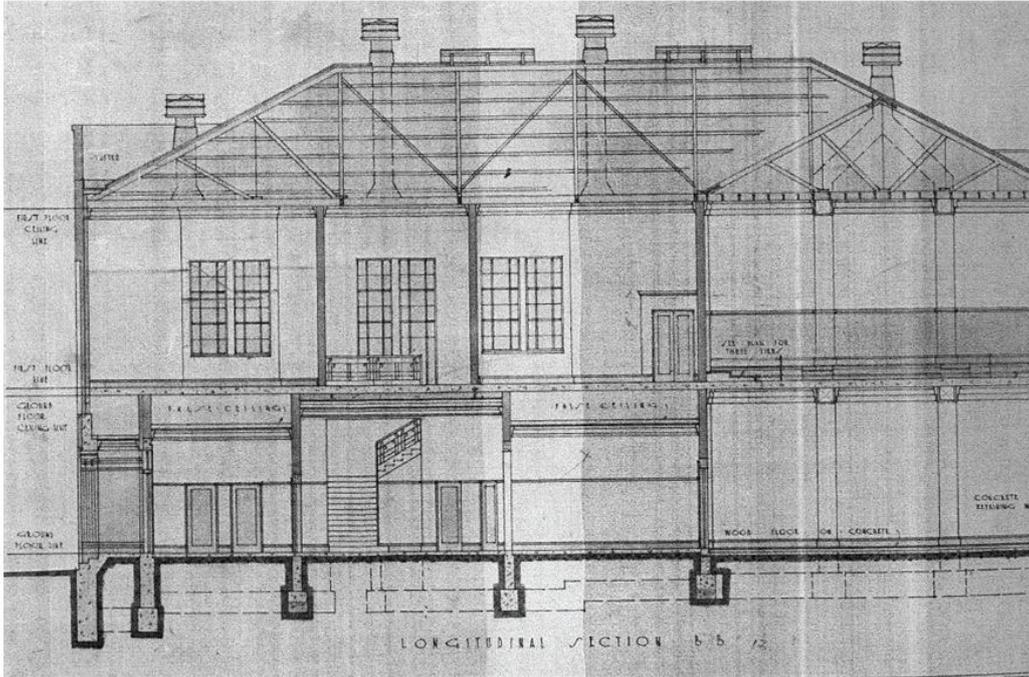


Figure 4.21 – Ground floor plan of proposed Masonic Hall. Tasmanian Archive and Heritage Office AE417/1



Figure 4.22 – A c1940 photograph across the Hobart waterfront, the approximate boundaries of the subject site outlined in red. Tasmanian Archive and Heritage Office AA116-1-45.



Figure 4.23 - Excerpt from the 1946 aerial run of Hobart, the subject site denoted by red lines (Hobart 1946 Run 1, 10892).



Figure 4.24 – Excerpt from the 1958 aerial run of Hobart, the subject site denoted by red lines (Hobart 1958 Run 5, T332-12).



Figure 4.25 – Excerpt from the 1968 aerial run of Hobart, the subject site denoted by red lines (Hobart Metro Run 6, 153 – February 1968).



Figure 4.26 - The Australian Broadcasting Corporation building, Masonic Hall and Telecom Exchange c1965. National Archives of Australia P2813 Album 2, p114.

#### Summary of developmental sequence

The above historical summary shows that the subject site has had a very simple developmental history, namely:

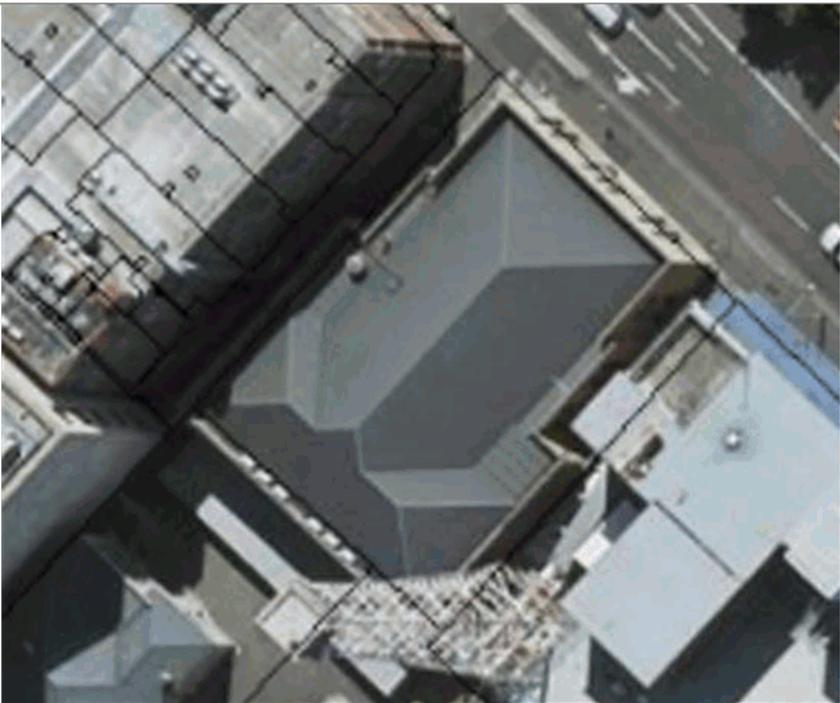
- An early association with larger estates as garden area (e.g. Heathfield as part of the Moodie grant).
- The only pre-mid-c20th development on the portion of Moodie land was the late-c19th Latham's Nursery, which is likely to have only included ephemeral fencing and sheds.
- Masonic lodge, 1937 to present, with very little change to the site.

## 5. Description of the place

### 5.1. The Exterior of the building

Exterior of the building		
<p><b>General description:</b> Building is two storey and occupies almost all of the site footprint, however the front two bays are narrower providing driveways on each side at the front of the site. The main body of the building (front) has a shallow hipped roof with the ridge perpendicular to the street, with the rear (wider) section also a hip running parallel to the street. The roof is clad in iron and hidden from ground-level view by a parapet. The walls are cream brick except where immediately on a boundary where they are red brick. The masonry features a strong vertical tenor, with vertical ribbon windows (and false window panels) and a columnar feel to the principal elevations. The building has a distinct Inter-War Stripped Classical style feel, with almost Brutalist tendencies and features subtle adornment in the form of a dentilled pediment course, use of sandstone in lintels and subtle horizontal banding which is recessive to the verticality of the building. The iron framed windows are restrained with the distinctive lack of glazing of purpose-built Masonic buildings compensated with extensive use of blind windows. The front of the side elevations feature distinct rainheads and downpipes.</p>		
Item/component	Description/Notes	Image
Northern elevation (street façade)	The façade of the building is distinctly Inter-War Stripped Classical in styling and has a very stark and functional appearance which is broken by discreet embellishments such as the classical columns on the portico dentilled pediment course and use of sandstone on signage and lintel panels. This elevation features blind windows on the first floor that continue with a strong verticality to the ground floor windows. The cream brick features subtle horizontal banding at intervals however the overall tenor of the façade is that accentuated verticality. The façade bears the Masonic emblem and the words 'Masonic Temple'.	

<p>Eastern elevation</p>	<p>The eastern elevation continues the cream brick to the front bay and has a more detailed and impressive fenestration pattern than the western, with a near-full-length vertical window to the main stairwell and featuring articulated brickwork to depict columns and a distinct dentilled course at the top. The verticality of this elevation is also highly expressed. The rear bay of the building which is built to boundary is of a plain red brick blank wall.</p>	
<p>Western elevation</p>	<p>The western elevation is also of cream brick to the front bay however is much less articulated than the eastern elevation with a lack of large windows and more sparse articulation of the brickwork. This elevation has a distinctly expressed horizontality whereby the eastern elevation has an expressed verticality in the articulation. Several small windows to service areas feature on this elevation. The rear bay which is built to boundary is of plain red brick and has what may be later added windows to service areas and has no particular architectural merit.</p>	

<p>Southern elevation (rear)</p>	<p>As a boundary wall this wall is completely blank red brick.</p>	
<p>Roof form</p>	<p>The main body of the building (front) has a shallow hipped roof with the ridge perpendicular to the street, with the rear (wider) section also a hip running parallel to the street. The roof is clad in iron and hidden from ground-level view by a parapet.</p>	
<p>Roof cladding &amp; rainwater goods.</p>	<p>The parapet supersedes any use of eaves, fascias etc.</p>	

<p>Windows</p>	<p>A very distinctive part of the building, typical of purpose-built Masonic buildings is the lack of windows to primary meeting rooms. The office and service spaces have windows, and a series of recessed blank windows give the street façade articulation at first floor level but retain a windowless room within. The windows are steel framed and glass bricks have also been extensively utilised.</p>	
<p>Doors (front)</p>	<p>The front doors are distinctly Art-Deco in styling and feature frosted glass panels with Masonic emblems and the frosted glass top-light features a geometric design consistent with the styling of the sandstone lintel panels on the exterior of the building.</p>	

Masonry generally	The walls are cream brick except where immediately on a boundary where they are red brick. The masonry features a strong vertical tenor, with vertical ribbon windows (and false window panels) and a columnar feel to the principal elevations. The building has a distinct Inter-War Stripped Classical style feel, with almost Brutalist tendencies and features subtle adornment in the form of a dentilled pediment course, use of sandstone in lintels and subtle horizontal banding which is recessive to the verticality of the building.	
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## 5.2. The Interior of the Building

For the purposes of this assessment, each room/area in the building has been assigned a number based on the original baseplan of the building, with a 'datasheet' for each room/area providing a broad description and commentary on the integrity of the room. Section 8 will consider these spaces and their integrity for individual significance (see that section for methodology) in-light of the statement of significance for the site as a whole as proposed in Section 7.

### General comments:

Form: The form of the building is unchanged from the original, with all room layouts, service areas etc. as per the original plans.

Walls: All original wall linings (hard render) appear intact and original.

Ceilings: The decorative adornment of ceilings through the building reflect the ceremonial/formal nature of rooms and service areas – all of which appear to be intact.

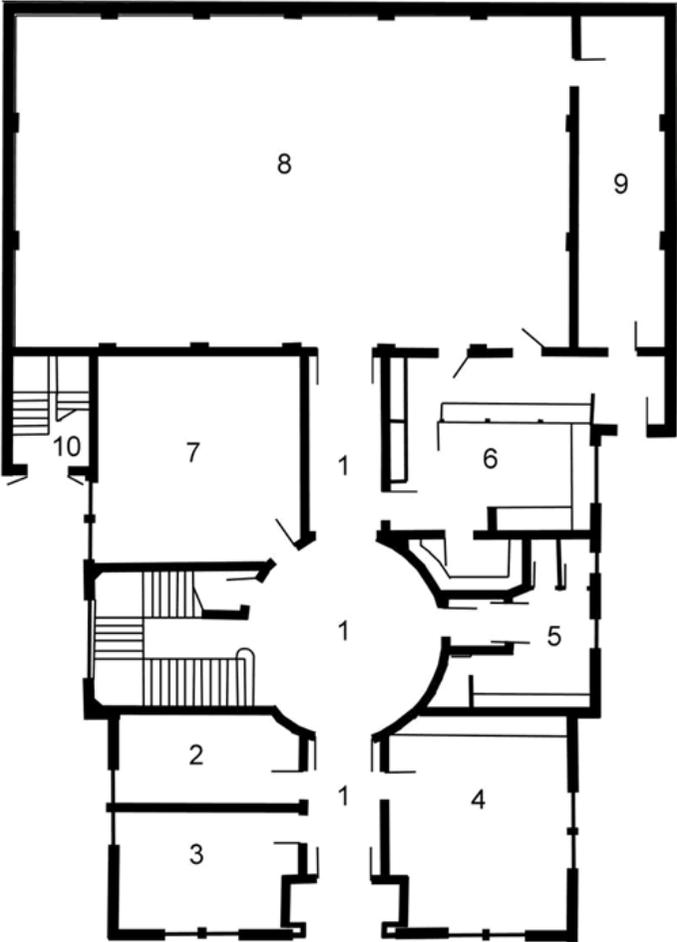
Joinery and hardware: Practically all original joinery remains throughout the building which is distinctly Art-Deco in styling and features extensive use of blackwood.

Stairs, railings etc.: The two stairways feature distinctively Art-Deco styling with iron railings, blackwood bannisters and use of terrazzo in the steps.

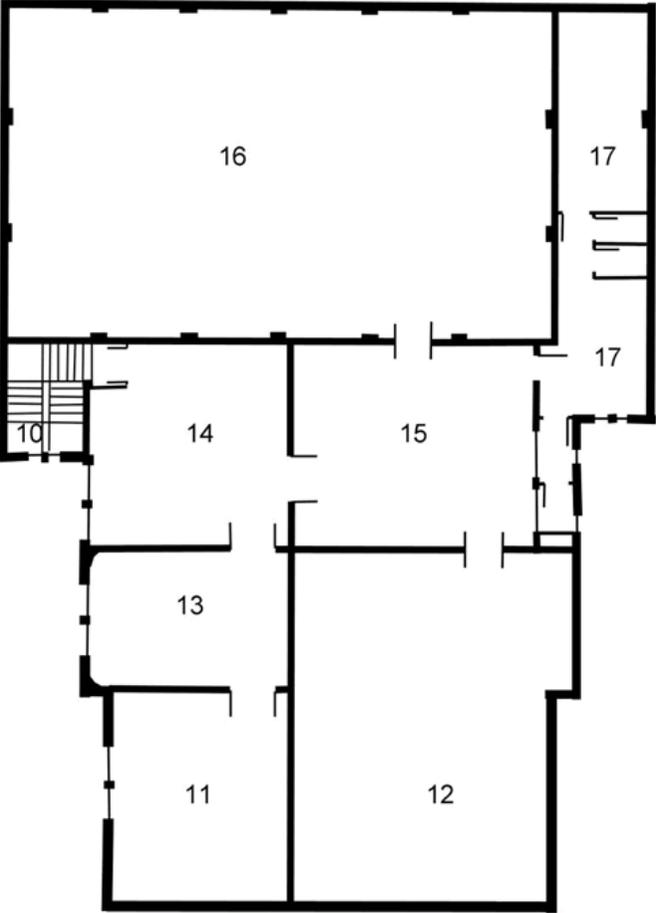
Electrical: The building retains many original light fittings, switches, power points etc. which have a distinctively Art-Deco character.

Services: The building retains early/original services such as bathroom fittings (urinals, basins etc.) which have a distinct Art-Deco character. The kitchen retains much of its original fittings and form but has been subject to upgrades.

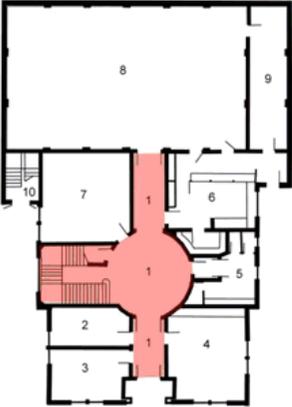
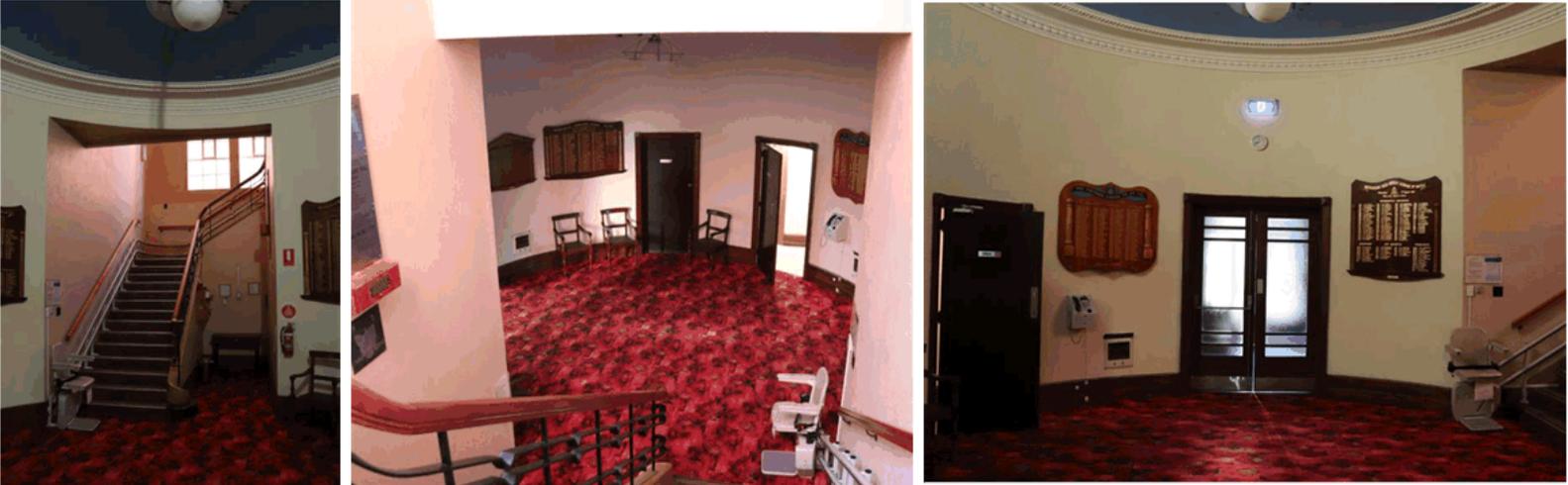
Movable cultural heritage: The building contains a substantial and impressive array of movable cultural heritage representing 150+ years of lodge activity in Hobart (from the predecessor building as well as other lodges now 'resident' in the building).



Ground floor

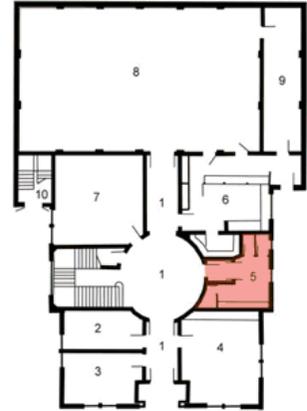


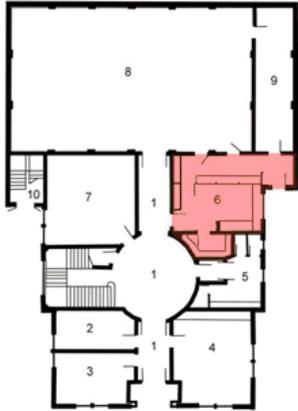
Upper floor

Room/area 1: Foyer, central 'crush hall' and main stairway	
<b>Description</b>	
<p>Features of note include the form of the round 'crush hall'. Diminishing grandeur of ceilings further rearward. The substantial sweeping Art-Deco influenced stair with iron railings and a blackwood bannister. The joinery appears wholly intact with solid doors and all architraves, skirtings in blackwood and in a distinctly Art-Deco style and the floor is carpeted. The room contains a number of honour boards, plaques and a 1999 time capsule.</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this area are almost wholly original or include heritage items (e.g. honour boards) from elsewhere. A later chairlift has been added to the stairs.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area is important as the key circulation space, both transversely and vertically through the building and the main entry point with a distinct architectural form in the round hall.</p>	
<b>Images</b>	
	

Room/areas 2 and 3: Offices	
<b>Description</b>	
Compared to the other gathering and dining spaces these rooms are relatively unadorned in terms of detailing. The joinery features the distinct Art-Deco styling as per elsewhere in the building and the floor is carpeted timber. The furnishings are generally modern and functional relating to informal meeting. As administrative rooms, rather than meeting rooms these rooms have windows.	
<b>Integrity</b>	
The form, fittings etc. in this area appear wholly original.	
<b>Ability to demonstrate key historic themes of the site</b>	
This area is important as an administrative space within the building, however not necessarily relating to primary lodge functions.	
Images	
	

<b>Room/area 4: Masonic library and committee room (Grand Secretary's Room)</b>	
<b>Description</b>	
<p>Compared to the other gathering and dining spaces this room is relatively unadorned in terms of detailing. The joinery features the distinct Art-Deco styling as per elsewhere in the building and the floor is carpeted timber. The furnishings are generally modern and functional relating to informal meeting. As an administrative room, rather than meeting rooms this rooms has windows.</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this room are almost wholly original. Furnishings have been upgraded.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area is important as an administrative space within the building, however not necessarily relating to primary lodge functions.</p>	
<b>Images</b>	
	

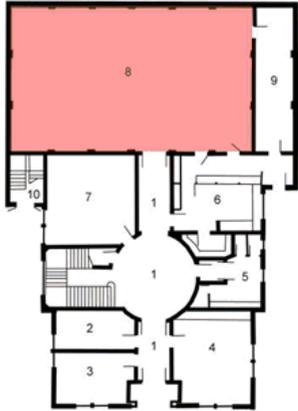
Room/area 5: Toilets	
<b>Description</b>	
The toilets are an unremarkable service area which although retain some of their original fittings/fixtures, have been upgraded.	
<b>Integrity</b>	
The form appears to be original, and the fittings early but probably not original.	
<b>Ability to demonstrate key historic themes of the site</b>	
This area is likely to be of a lower level of importance in demonstrating the primary and significant use of the building, but provides necessary supporting infrastructure for such.	
Images	
	

Room/area 6: Kitchen	
<b>Description</b>	
<p>The layout of the kitchen differs slightly to the original plans, which may indicate some modification or that it was not build according to the original plan. The room contains a modern commercial kitchen which has had upgrades over the decades since construction, although the room still has examples of original joinery etc. A pantry is off the kitchen which retains some original/early joinery.</p>	
<b>Integrity</b>	
<p>The form of the room appears to be largely original – a dividing wall may have been added in later. Original architraves remain although skirtings have been largely removed (i.e. with cabinetry and covered vinyl). The fittings and fixtures have all bene upgraded.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area is likely to be of a lower level of importance in demonstrating the primary and significant use of the building, but provides necessary supporting infrastructure for such.</p>	
Images	
	

Room/area 7: Supper room (Linton Room)	
<b>Description</b>	
<p>The supper room is accessed from the crush hall. Compared to the other gathering and dining spaces this room is relatively unadorned in terms of detailing. The joinery features the distinct Art-Deco styling as per elsewhere in the building and the floor is carpeted timber. The furnishings are generally modern and functional relating to informal meeting. Some lodge memorabilia is hanging in the room..</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this area are almost wholly original. The furnishings have been upgraded from the original.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area is important as a gathering space within the building, however not necessarily relating to primary lodge functions.</p>	

Images



Room/area 8: Banquet hall	
<b>Description</b>	
<p>The banquet hall is the largest room on the ground floor accessed from the rear hallway off the crush hall. The room is serviced from the kitchen and although it has an elaborate ceiling and applied columns to the walls which give it a greater impression of grandeur. The joinery features the distinct Art-Deco styling as per elsewhere in the building and the floor is timber. The furnishings are generally modern and functional relating to dining. Some lodge memorabilia is hanging in the room.</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this area are almost wholly original. The furnishings have been upgraded from the original.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area is important as a gathering space within the building, however not necessarily relating to primary lodge functions.</p>	
Images	
	

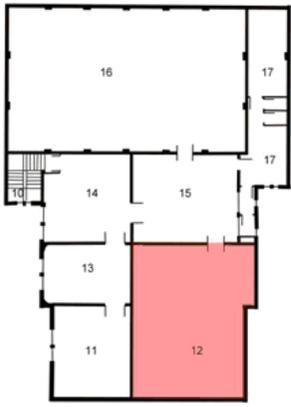
Room/area 9: Store	
<b>Description</b>	
<p>The store is accessed off the kitchen and occupies the area adjacent to the banquet hall. This is a functional and unadorned area with a timber lined ceiling but does still include the typical Aer-Deco styled blackwood joinery. The room is largely filled with cupboards that probably predate the building and contain items relating to various lodges. This room contains heating plant.</p>	
<b>Integrity</b>	
<p>This room appears to be in its original form with original, but less remarkable detailing.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area is likely to be of a lower level of importance in demonstrating the primary and significant use of the building, but provides important supporting infrastructure for such.</p>	
Images	

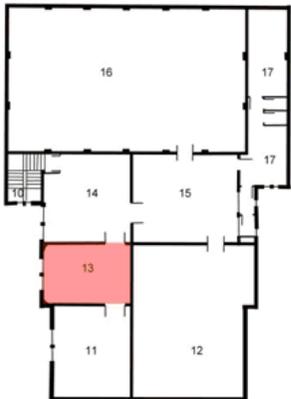
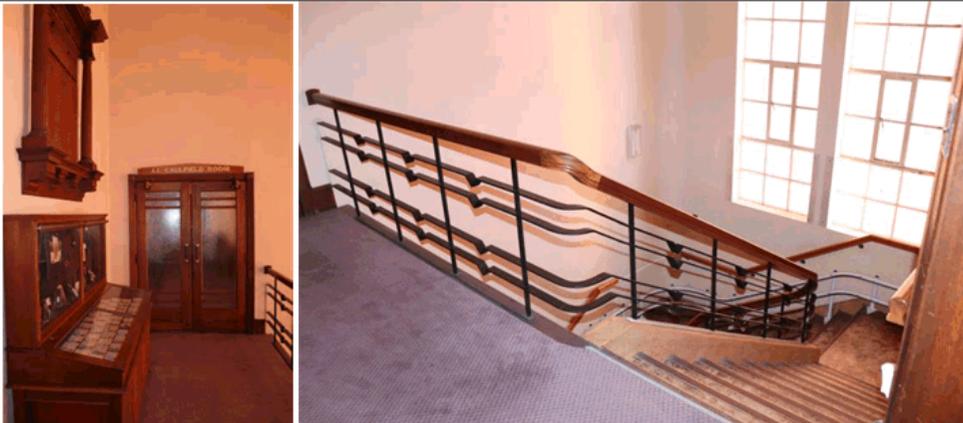
Room/area 10: Secondary stair	
<b>Description</b>	
<p>The secondary stairs are accessed from the exterior and lead to the airlock within the cloak room (room 10). The masonry stairs have an iron railing of a diminished style to that of the main stairs. This area retains the distinctive blackwood Art-Deco styling as per elsewhere in the building.</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this area appear wholly original.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area is secondary to the main stairs in demonstrating vertical circulation space through the building.</p>	
Images	
	

Room/area 11: Grand lodge assembly room (L.L. Caulfield Room)	
<b>Description</b>	
<p>The grand lodge assembly room is relatively unadorned compared to the main meeting rooms, however the room retains distinctive Art-Deco styling in its joinery, window treatment etc. The room contains some early/original furnishings and a large number of framed photographic portraits of prominent lodge leaders.</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this area are almost wholly original and the room is furnished with items directly relating to lodge functions.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This room and its associated contents</p>	

**Images**



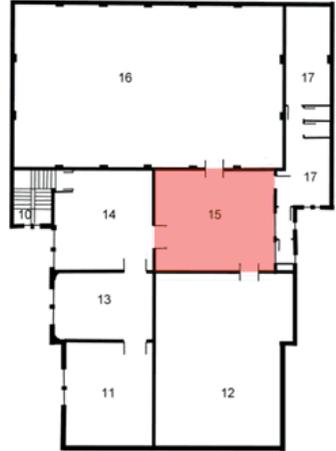
Room/area 12: Lodge room 2	
<b>Description</b>	
<p>This room is entered from the two assembly areas on the first floor. The room retains its original raised platform around the perimeter of the room and has a prominent ornate ceiling with a 'G' light fitting. The room features a timber dado/chair rail, exposed timber flooring and a range of furniture. The entry doors are felt lined for soundproofing. The room distinctly has no windows typical of lodge meeting rooms.</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this area are almost wholly original and the room is furnished with items directly relating to lodge functions.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>As the second main lodge room, this room clearly demonstrates the primary function of the building as a meeting space. The remaining original detailing and furnishings enhance that ability to demonstrate that theme.</p>	
Images	
	

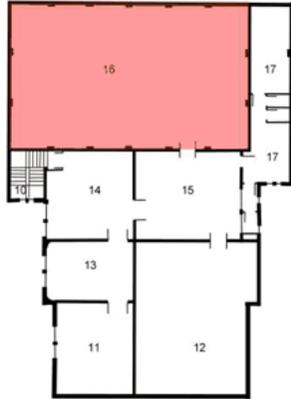
Room/area 13: Stair landing	
<b>Description</b>	
<p>This room is a circulation area between the main staircase, main assembly room and cloak room. The landing of the main staircase is in this area with a distend Art-Deco influenced iron railing and blackwood bannister. The room acts as a lightwell to adjacent rooms via obscure glazed doors. Detailing is diminished from the ground floor area, with a discrete cornice but retaining the typically Art-Deco blackwood joinery. The room has a large honour board and other masonic displays and a prominent Art-Deco styled pendant light.</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this area are almost wholly original. A later chair-lift has been added to the stairs.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area is important as the key circulation space through the building and the entry point of the upper floor.</p>	
Images	
	

Room/area 14: Lodge cloak room	
<b>Description</b>	
<p>This room is the landing area of the secondary stairs and is between the main stair hall and the assembly room. This room is relatively unadorned compared to the lodge meeting and assembly rooms themselves. The most prominent feature being the airlock to the secondary stairs which is paneled blackwood with distinct Art-Deco features and the glazed double doors to the assembly rooms which also are distinctly Art-Deco. There is a modest cornice. The room features several honour boards and masonic displays. This is one of few upper-floor rooms which has windows.</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this area appear wholly original.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area is important as the key circulation space, rather than for primary lodge functions as such.</p>	

**Images**



Room/area 15: General assembly room	
<b>Description</b>	
<p>This room is a circulation space between the two main lodge rooms and leading from the stair hall to the regalia room. The room has less grand adornment than those lodge rooms and has high-level internal windows aligned with the windows of the regalia room. The room has a series of cupboards around the wall holding regalia belonging to different lodges.</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this area are almost wholly original and the room is furnished with items directly relating to lodge functions some of which are likely to pre-date this building (i.e. from earlier/other lodges).</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area is important as key circulation space and storage space for lodge memorabilia.</p>	
Images	
	

Room/area 16: Lodge room 1 (Cummins Lodge Room)	
<b>Description</b>	
<p>As the main lodge room of the building, this room is the most heavily adorned and richly furnished. The ceiling has ornate cornicing and paneling and the walls have applied ionic columns. The north-western end of the room has a gallery accessed from the store below which has been subdivided off the room. The room has prominent Art-Deco light fittings, a dado/chair rail, carpeted floor with masonic motifs and a range of freestanding and in-built seating arranged around a master’s chair on each end of the room. The room distinctly has no windows typical of lodge meeting rooms. The room retains all original joinery and soundproofed entrance doors.</p>	
<b>Integrity</b>	
<p>The form, fittings etc. in this area are almost wholly original. There have been some minor modern interventions for heating, carpet, AV etc. but generally the rooms retains its original feel.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This room is critical in demonstrating the place as a masonic building – being the primary lodge meeting space. The remaining original detailing and furnishings enhance that ability to demonstrate that theme.</p>	
<b>Images</b>	
<div style="display: flex; justify-content: space-around;">   </div>	

Room/area 16: Store and regalia room	
<b>Description</b>	
<p>This area is comprised of several spaces, including a regalia room, toilet (with loft above), robing rooms and a store room. The regalia room contains cupboards which are likely to predate the building and the store contains various plant items.</p>	
<b>Integrity</b>	
<p>This area is a functional area and largely retains its original form, however appears to have had occasional minor upgrades to service the wider building.</p>	
<b>Ability to demonstrate key historic themes of the site</b>	
<p>This area provides support infrastructure to the wider building, rather than directly demonstrating any key aspects of the use of the building.</p>	
Images	
	

### 5.3. Streetscape attributes

The following figure depicts the key vantage points and viewlines to the site which will be considered here:



Figure 5.1 – Key public vantage points and viewlines where any development at the rear and side area of the subject site would be most visible.

Point	Importance to the heritage and streetscape values of the place	Image
1	<p>The building has no streetscape presence upon approach from the bend in Sandy Bay Road and is wholly obscured by neighbouring development (which will be accentuated in the event of the approved development at 5-7 Sandy Bay Road going ahead). The surrounding development is large with the subject building being the smallest element in that cluster of buildings.</p>	
2	<p>Even upon nearer approaches, the building is still a non-prominent element in the streetscape with the larger surrounding development dominating the streetscape. The approach to the building presents it practically as a two-dimensional façade, with the neighbouring development obscuring the side walls providing no indication of the true depth of the building and the parapet hiding the roof form.</p>	

<p>3</p>	<p>The building only has a slight three-dimensional presence from very near environs, where still only the front bay of the building is evident from the limited oblique angles where such is visible.</p> <p>This has almost always been the case – with the adjacent former telephone exchange pre-dating the building (Having also been heightened early in the building’s life) and the adjacent former ABC building being built within 25 years of the construction of the Masonic Temple. The building has almost always been within a cluster of larger buildings which has always diminished its streetscape presence.</p>	 A photograph showing a corner of a building with a light-colored facade. A tall, lattice-structured tower is visible on the roof. To the right, a taller brick building is partially visible. The sky is clear and blue.
<p>4</p>		 A street-level photograph of the same building. A white car is parked on the street. A utility pole with a lattice tower is visible in the background. The scene is set on a city street with other buildings and trees.

<p>5</p>	<p>The prominence of the building is also diminished when viewed from St David’s Park, where it forms part of a grouping of much larger buildings where it appears as a minor streetscape element, also somewhat two-dimensional due to the depth of the building not being evident. Towers behind the building distract the viewline also which is also buffered by the plantings in St David’s Park.</p>	
<p>6</p>		

The following points are concluded from the above analysis of the streetscape qualities of the building:

- The building forms part of a cohesive group of Inter-War – mid-c20th larger-scale commercial/public buildings (i.e. the former ABC building and former Telecom exchange (note that a substantially larger building has been approved to replace the former ABC building).
- The building has minimal streetscape presence upon oblique approaches due to the proximity and streetfront positions of the adjacent buildings.
- The key streetscape attribute of the building is from close oblique angles and directly opposite, where the architectural qualities and purpose of the building are readily apparent.
- Distant views to the building from the front are limited due to the established vegetation of St David’s Park.

## 6. Comparative analysis

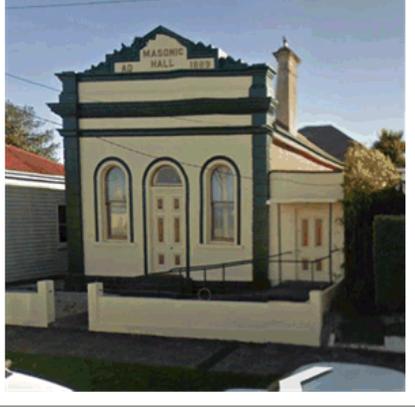
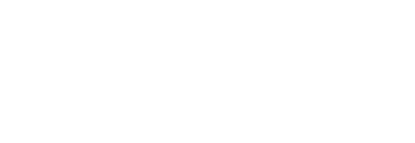
The following comparative analysis is necessary to understand the significance of the Hobart Masonic Hall based on what may be relatively unique attributes – that of a large Masonic Temple/Hall and that of a work of Architect Lauriston Crisp. Those two lines of enquiry will be explored here ahead of an assessment of the significance of the place.

### 6.1. Masonic Temples/Halls of Tasmania

A search of the Tasmanian Heritage Register and selected local heritage schedules reveals the following ‘Masonic Halls’ or similar listed. This search has also been informed by a list of active lodges at freemasonrytasmania.org:

Address	Photo (GoogleEarth)	Brief description/notes	Comparison with 3 Sandy Bay Road
Former Masonic Hall 24 Murray Street Hobart		Substantial mid c19th high-Victorian classically styled building.	The precedent of 3 Sandy Bay Road which was built as the lodge attendance outgrew this earlier building. Important in the context of 3 Sandy Bay Road in demonstrating the growth of the lodge in the first half of the c20th.
Masonic Temple 15 Hamilton Street Latrobe		Late Victorian purpose-built classical styled building with a substantial later extension.	Example of an earlier rural Masonic Lodge.

<p>Masonic Hall Chambers                  39-39A Brisbane Street                  Launceston</p>		<p>Substantial high-Victorian classically styled building with a later similarly styled front extension.</p>	<p>An example of a large and elaborate city lodge from the latter c19th.</p>
<p>Masonic Hall                  21 Cutten Street                  Queenstown</p>		<p>Federation purpose-built building with a well-articulated masonry façade and more modest construction rearward.</p>	<p>Example of an early c20th rural Masonic Lodge.</p>
<p>Masonic Hall                  3348 Huon Highway                  Franklin</p>		<p>A modestly scaled and detailed c1920s lodge building demonstrating the lack of fenestration common on purpose-built lodge buildings.</p>	<p>Example of an early-mid c20th rural Masonic Lodge.</p>

<p>Masonic Hall                  Peltro Street Glenorchy</p>		<p>A restrained Inter-War Stripped Classical styled building of a light brick front bay and a red brick rear. The building bears some resemblance to the Hobart Masonic Hall but in a more restrained manner.</p>	<p>A very similar building to 3 Sandy Bay Road, from a similar period but of a smaller scale. It is possible that this building is also the work of Lauriston Crisp and representing a medium-scale city Masonic Lodge.</p>
<p>Masonic Hall                  87 Emu Bay Road,                  Deloraine.</p>		<p>Late Victorian purpose-built classical styled building of modest proportions and detailing.</p>	<p>Example of an earlier rural Masonic Lodge.</p>
<p>Masonic Hall                  Gay Street Oatlands</p>		<p>Mid-c20th concrete block hall of modest scale and styling demonstrating the lack of fenestration common on purpose-built lodge buildings.</p>	<p>Example of an mid c20th rural Masonic Lodge.</p>

<p>Masonic Hall                  Devonport</p>		<p>Federation purpose-built building with a well-articulated masonry façade.</p>	<p>Example of an early c20th smaller city Masonic Lodge.</p>
<p>Lodge Lauriston                  George Town</p>		<p>Later c20th (1961) modernist building (purpose built).</p>	<p>Example of a larger mid-late c20th Masonic Lodge.</p>
<p>Masonic Lodge                  Penguin</p>		<p>Mid-c20th concrete block hall of modest scale and styling demonstrating the lack of fenestration common on purpose-built lodge buildings.</p>	<p>Example of an early-mid c20th rural Masonic Lodge.</p>

<p>Masonic Hall                  513 Gordon River Road,                  Bushy Park</p>		<p>A modestly scaled and detailed c1920s lodge building demonstrating the lack of fenestration common on purpose-built lodge buildings.</p>	<p>Example of an early-mid c20th rural Masonic Lodge.</p>
<p>Masonic lodge                  10 Patrick Street Ulverstone</p>		<p>A modestly scaled and detailed c1920s lodge building. Front extension obscures main hall. No longer used as a lodge.</p>	<p>Example of an early-mid c20th rural Masonic Lodge.</p>
<p>Masonic Lodge                  High Street Sheffield</p>		<p>Mid-c20th concrete block hall of modest scale and styling demonstrating the lack of fenestration common on purpose-built lodge buildings.</p>	<p>Example of a mid c20th rural Masonic Lodge.</p>

<p>Masonic Centre                  3 Forcett Street Sorell</p>		<p>Mid-c20th concrete block hall of modest scale and styling demonstrating the lack of fenestration common on purpose-built lodge buildings.</p>	<p>Example of a mid c20th rural Masonic Lodge.</p>
<p>Masonic Lodge                  42 Hogg Street, Wynyard.</p>		<p>A modestly scaled and detailed c1930s lodge building demonstrating the lack of fenestration common on purpose-built lodge buildings.</p>	<p>Example of an early-mid c20th rural Masonic Lodge.</p>
<p>Masonic lodge                  11 William Street Longford.</p>		<p>A modestly scaled and detailed c1930s lodge building demonstrating the lack of fenestration common on purpose-built lodge buildings. No longer used as a lodge building.</p>	<p>Example of an early-mid c20th rural Masonic Lodge.</p>

The above examples show a range of the Masonic Lodge buildings in Tasmania. Note that Oddfellows, RAOB (etc.) buildings were not examined here, but may provide a wider dataset of buildings in which lodge activities take place. Whilst Masonic Lodge buildings are not particularly uncommon in Tasmania, they represent a diversity both stylistically and temporally connected to the growth of Tasmanian populations and the fluctuations in lodge attendance over many decades.

The rural examples examined here are generally small and modest buildings, some with notable stylistic flourishes to their facades dating from the first half of the twentieth century and representing the formalisation of lodge premises in growing Tasmanian towns (noting that often lodge activities would have been held in other public gathering spaces prior to that).

In cities, the Masonic Lodges were large, ornate and prominent buildings such as the earlier Hobart building in Murray Street and the Launceston building, the earlier buildings appearing to desire more outward austerity and these are more fenestrated than the later examples where windows were restrained and in general the architectural detailing was much more restrained (yet still subtly present, in particular classical motifs).

3 Sandy Bay Road is an example of a later-generation lodge building replacing an outgrown earlier building – both purpose built which appears to be a unique attribute of such a larger city building – i.e. population/attendance outgrowing a purpose built building, whereas rural lodges of the early c20th are first-generation purpose-built.

## 6.2. Lauriston Crisp Architect

Albert Lauriston Crisp (b.1898, d.1963) was a prominent Tasmanian Architect of the 1930s who held a fine reputation for high-end residences and commercial buildings. He was Secretary of the Tasmanian Institute of Architects in the 1930s.

Amongst his works were:

- The Masonic Temple, 3 Sandy Bay Road, Hobart
- Lincoln House, 26 Cattley Street, Burnie
- Residence, 31 Sandy Bay Road, Hobart
- Iluka (Crisp's own residence), 300 Sandy Bay Road, Hobart
- Hobart Savings Bank (and residence), Sandy Bay
- Convalescent home and clock tower, Millbrook Rise, New Norfolk,
- Barclay Kindergarten, Hobart
- Residence, 107 Main Road, Claremont.
- Service station, Montpelier Retreat, Hobart
- Motors garage, Brisbane Street, Launceston.
- Paragon Theatre, Queenstown

- Derwent Bridge Hotel
- Alterations to Franklin Hall
- Public Buildings, Geeveston
- Public Buildings, New Norfolk
- Gellibrand House, St. John's Park, New Town
- Scots Church Manse, Hobart
- RSL, Glenorchy
- Bothwell Town Hall extensions
- Bank, Cygnet
- Cecilia Button Memorial Hospital, Oatlands
- Additions, Smithton Club
- Queenstown Hall
- Council Clerk's Residence, Swansea

3 Sandy Bay Road is one of several prominent public buildings that Crisp designed and is likely to represent a high-point in his career and a very well executed example of the Inter-War Stripped Classical style of architecture. Crisp's work appears to have been at the tail-end of the Art-Deco movement and much of that work reflects his training during that period – for example Millbrook Rise, Motors Launceston and the Paragon Theatre. It is not known if the Glenorchy Masonic Hall was the work of Crisp, but it bears remarkable similarities to the Hobart example. The current research suggests that 3 Sandy Bay Road is Crisp's only major execution of that style of architecture.

## 7. Statement of historic heritage significance

The following statements of significance are based on the national HERCON standard for statements of significance, based on the amount of information currently at-hand. Note that natural history values have not been assessed here, as these are beyond the scope of this assessment. This statement of significance has been based upon the history of the place as outlined above, however could be further refined with more detailed investigations into the site history.

The assessment methodology for each criterion follows the methodology details in the Tasmanian Government's Assessing Historic Heritage Significance for Application with the Historic Cultural Heritage Act 1995 (October 2011). That document provides a 'tiered' approach to assessing significance at both a state and local level and is considered to represent a sound approach to assessing values.

Where it may initially appear marginal as to whether a place may or may not meet a particular criterion, a further discussion against the Exclusion Parameters as proposed in the Assessing Historic Heritage Significance document is provided.

The statements have also been formulated with consideration to the fabric analysis in Section 5.

<b>A. Importance to the course, or pattern of our cultural or natural history.</b>
A place is of importance to the course or pattern of Tasmania's history if that place is the product of, or is an example of, or was influenced by, or has influenced, or is associated with, or has a symbolic association with, or is the site of – an event, phase, period, process, function, movement, custom or way of life (including values, aspirations, tastes and fashions) which has made a strong, noticeable or influential contribution to the evolution or pattern of the settlement and development of Tasmania.
The Masonic Temple at 3 Sandy Bay Road is of historic heritage significance as it demonstrates almost 100 years of the Freemasonry movement in Tasmania and representative of the early-mid-c20th expansion and popularisation of that movement with the need for larger premises. The continued use of the building for that purpose is a critical part of maintaining the significance of the building and for the building to contribute to that history.

**B. Possession of uncommon, rare or endangered aspects of our cultural or natural history.**

A place demonstrates rare or uncommon aspects of Tasmania's heritage if that place illustrates in its fabric an event, phase, period, process, function, movement, custom or way of life (including values, aspirations, tastes and fashions) which, or an aspect of which: (i) was considered uncommon or unusual at the time of its origin; (ii) is no longer practised AND is of special interest; or (iii) was once commonplace but for which there is little surviving evidence in Tasmania.

3 Sandy Bay Road is the only example of a large-scale early-mid c20th Masonic Temple in Tasmania – with examples in other cities being either substantially smaller (e.g. Devonport, Glenorchy) or being the retained earlier large buildings (e.g. Launceston) where expansion into a newer and larger building did not occur. The intact nature of the building, detailing, contents etc. represents a rare intact culmination of the history of freemasonry in Tasmania – particularly in an age where many Masonic Halls are being closed and sold off for adaptive reuse.

**C. Potential to yield information that will contribute to an understanding of our cultural or natural history.**

A place has the potential to yield information that will contribute to an understanding of Tasmania's history if, through analysis and further examination or research of the place and its fabric (including artefacts), it can provide information that could not be derived from any other source. While this criterion in Tasmania is most often used to define archaeological research potential, it may also be used for the research potential of architectural design, construction techniques, historical gardens, etc.

As per the further consideration in Section 9, the site itself is not considered to have any potential to yield information that would contribute to an understanding of our natural or cultural history.

<b>D. Important in demonstrating the principal characteristics of a class of cultural or natural places or environments.</b>
This criterion is concerned with representativeness. A place included under this criterion should demonstrate the principal characteristics of a particular class of cultural place if that place displays the defining features, qualities or attributes of its type, where type or class of place illustrates a range of human activities including a way of life, a custom, an ideology or philosophy, a process, a land use, a function, a form, a design, a style, a technique or some other activity or achievement. To be considered a good representative example, the place should have a high level of intactness.
3 Sandy Bay Road is of historic heritage significance because of its ability to demonstrate the principal characteristics of an Inter-War Stripped Classical style building. This is evident in the symmetrical exterior arrangement, general spartan character with minimal but well executed classical motifs concentrated at key points and common subtle use of Art Deco and Art Nouveau elements (particularly on the interior). The light tone of the façade and verticality of façade elements accentuate this style. The general lack of fenestration is indicative of a Masonic building which has been skilfully blended with the stylistic attributes of the Inter-War Stripped Classical building.
A remarkable attribute of the building is that it is in practically wholly original condition and still serving the same function for which it was built – that integrity is also supported by the array of related contents.

<b>E. Importance in exhibiting particular aesthetic characteristics</b>
This criterion may be interpreted as a place being important because of its aesthetic significance if that place exhibits sensual qualities that can be judged against various ideals including beauty, picturesqueness, evocativeness, expressiveness, landmark presence, symbolism or some other quality of nature or human endeavour.
3 Sandy Bay Road is not considered to exhibit any particular aesthetic characteristics that may be of historic heritage significance however does form part of a reasonably cohesive grouping of larger buildings where only the facades are generally evident in the streetscape.

<b>F. Importance in demonstrating a high degree of creative or technical achievement at a particular period.</b>
A place is important in demonstrating a high degree of creative or technical achievement if that place illustrates artistic or technical excellence, innovation, accomplishment, extension or creative adaptation in a variety of fields of human endeavour including but not exclusive to art, engineering, architecture, industrial or scientific design, landscape design, evolved design, construction, fabrication, manufacture, or craftsmanship.
Despite being a very good and intact example of Inter-War Stripped Classical architecture, 3 Sandy Bay Road is not considered to exhibit any particular characteristics representing creative nor technical achievement that may be of historic heritage significance for being 'out of the ordinary'.

<b>G. Strong or special association with a particular community or cultural group for social, cultural or spiritual reasons.</b>
A place has a special associational value if it is associated with a person, organisation or group of people who or which is of importance to the history of Tasmania. In this context, importance may relate not only to the great and well-known, but also to the influential, the exemplary, and the innovative.
The Masonic Hall at 3 Sandy Bay Road has a high degree of associative value to the order of Freemasonry – being the largest lodge in the south of Tasmania (at least) and the headquarters for a number of separate lodge bodies.
The townscape associations of the building and its recognisable contribution to the mid-c20th urbanisation of the city fringe through a ribbon of higher development (e.g. the former Telecom exchange and former ABC building) spreading along Sandy Bay Road contributes to the community's sense of place.

<b>H. Special association with the life or works of a person, or group of persons, of importance in our history.</b>
A place has a special associational value if it is associated with a person, organisation or group of people who or which is of importance to the history of Tasmania. In this context, importance may relate not only to the great and well-known, but also to the influential, the exemplary, and the innovative.
The Masonic Hall at 3 Sandy Bay Road is possibly the finest example of Inter-War Stripped Classical architecture by prominent Tasmanian Architect A. Lauriston Crisp. Crisp was known for a substantial number of Tasmanian public/commercial buildings and it is likely that 3 Sandy Bay Road represents the upper-end of examples of his work.
Further investigation into the social history of Freemasonry in Tasmania an prominent persons associated with such (beyond the scope of the current project) is likely to yield associations with persons of importance in our history.

## 8. Significance of individual elements and conservation policy

Based on the overall statements of significance outlined in Section 5, and the broad description and analysis of the evolution of the place as detailed in Section 4, individual and collective elements, and other possible heritage values (e.g. intangible values) of the place will be assessed here, in order to assign or rank specific levels of significance, upon which heritage management policies will be formulated in Section 11, to inform the implementation strategy in Section 12. For the purposes of this section the following scale will be used to assign degrees of significance to individual elements of the fabric and form of the place:

**High** – Elements, forms or spaces which readily demonstrate important aspects of the significance of the place or related important historic theme.

**Medium** – Elements, forms or spaces which less-readily demonstrate important aspects of the history of the place, or readily demonstrate aspects of lower significance (or related important historic theme).

**Low/no** – Elements, forms or spaces which less demonstrate less important aspects of the history of the place.

**Neutral** – Elements, forms or spaces which neither contribute to, nor detract from, the significance of the place.

**Intrusive** – Elements, forms or spaces which obscure the significance or are likely to threaten the longevity/integrity of significant elements, forms or spaces.

Whilst it is noted that the significance of any place need not necessarily be solely embodied in original fabric (i.e. later modifications can contribute to significance through demonstrating the evolution of the place), it is relevant to consider the impact that later modifications may have had on the integrity of more significant elements and whether that has diminished the significance of such. Similarly, decay of significant elements may also have an impact on their ability to demonstrate significance. Accordingly, following scale will be used to rank **levels of integrity** of elements, forms or spaces within the context of the overall significance of the place:

**High:** *Elements which are highly intact and readily demonstrate their respective significance.*

**Medium:** Elements which subsequent modification have obscured or reduced their ability to readily demonstrate their respective significance, however this may be retrievable through restoration without the need for introduction of substantial new fabric which may reduce or obscure significance.

**Low:** Elements which have lost the ability to demonstrate any significance and could not feasibly be restored without conjecture or substantial addition of new fabric.

The following matrix represents the interplay of integrity and significance and introduces colour coding as used in the following tables:

	Integrity		
Significance	High	Medium	Low
High	1	1	2
Medium	2	2	3
Low	3	3	4
Neutral	4	4	4
Intrusive	5	5	5

Accordingly, the following colour code has been adopted to consider significance in-light of the integrity of that particular element:

**Red – High significance (Rank 1)**

**Orange – Medium significance (Rank 2)**

**Green – Low significance (Rank 3)**

**Grey – Neutral (Rank 4)**

**Blue – Intrusive (Rank 5)**

The above is also correlated with the ability to sustain further change, i.e. elements of high significance have a low ability to sustain further change, elements of low significance and neutral elements have a greater ability to sustain change, and intrusive element ought be changed. The following Tables provide an overview of the significance of the elements of the building and brief commentary on their ability to sustain further change. These are further articulated in the heritage

management policies in Section 8. The following colour coding will be used to express the ability of particular elements to sustain change/modification:

**Red – Do not change unless absolutely essential**

**Orange – Some changes may be possible**

**Green – Change may be readily acceptable.**

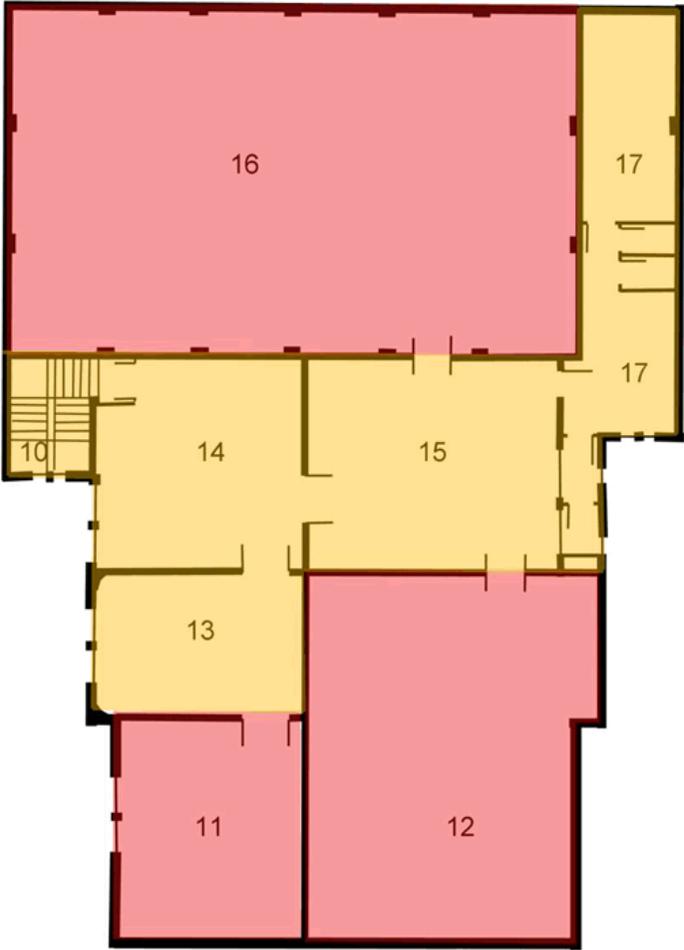
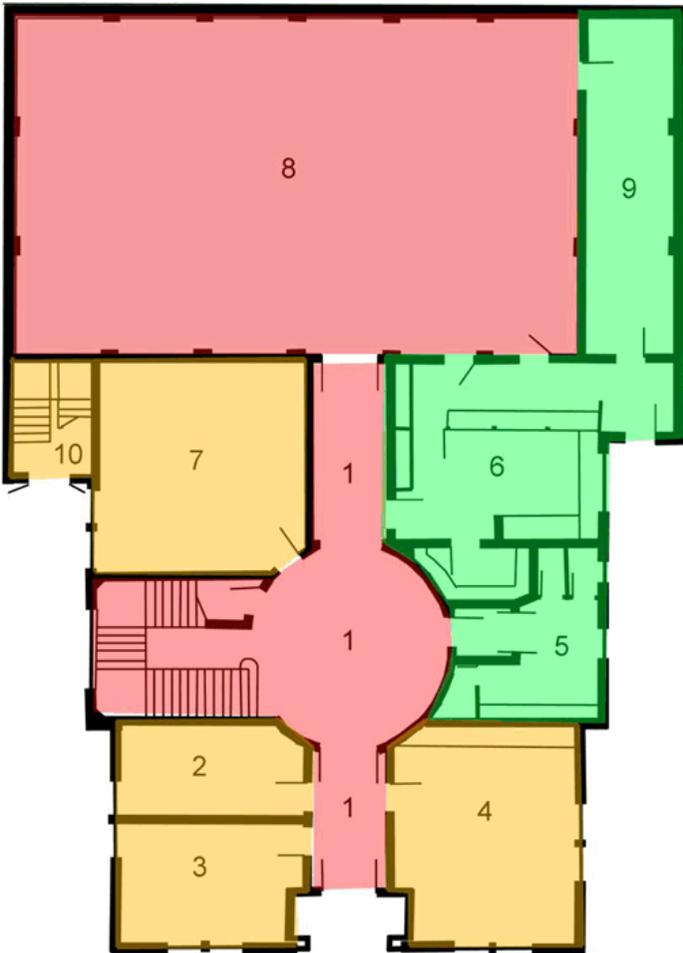
Exterior				
Element		Significance	Ability for change	Recommendations and ability for modification
Overall form		Red	Red	There may be the potential for further change in the rearward bays of the building. No change to the form of the front bay should occur.
Northern elevation		Red	Red	Apart from minor changes for security, soundproofing, thermal efficiency, weatherproofing etc. no changes should be made.
Eastern elevation	Front bays	Red	Red	Apart from minor changes for security, soundproofing, thermal efficiency, weatherproofing etc. no changes should be made.
	Rear bay	Orange	Orange	Modification likely to be possible – noting the wall is on a boundary.
Western elevation	Front bays	Red	Red	Apart from minor changes for security, soundproofing, thermal efficiency, weatherproofing etc. no changes should be made.
	Rear bay	Orange	Orange	Modification likely to be possible – noting the wall is on a boundary.
Southern elevation		Orange	Orange	Modification likely to be possible – noting the wall is on a boundary.
Roof form		Orange	Orange	Modification may be possible given the very low prominence of the roof form.
Rainwater goods		Green	Green	Modification for upgrades, efficiency etc. likely to be possible.
Masonry	Front bays	Red	Red	All cream-brick and sandstone masonry should remain as existing.
	Rear bays	Green	Green	Modifications (e.g. paint, render) may be possible on red brick.
Windows	Glazed windows	Red	Red	Apart from minor modifications for security, thermal efficiency or soundproofing, windows should be

				retained as existing. Obscure glazing ideally should be retained.
	Blind windows			Blind windows should remain as such.
	New apertures			New apertures should generally be avoided unless necessary for critical functional reasons.
Doors	Front			Apart from minor modifications for security, thermal efficiency or soundproofing, the front doors should be retained as existing. Obscure glazing and Masonic motifs must be retained.
	Side			Modification may be possible for functional/compliance reasons.
Driveways				Modification for functional purposes is likely to be acceptable.

Interior				
Room		Significance	Ability for change	Recommendations and ability for modification
1	Entrance foyer, main stairway and crush hall			Should generally remain unchanged, although minor changes for access compliance etc. may be acceptable.
2	Offices			Some modifications may be acceptable as support and administration spaces for the overall building.
3				
4				
4	Library and committee room			
5	Toilets			Upgrades for serviceability/compliance are likely to be acceptable. These areas may also offer feasible space for upgraded vertical circulation infrastructure (e.g. lift).
6	Kitchen and pantry			
7	Supper room			Should remain generally unchanged.
8	Banquet hall			
9	Store			Some modifications may be acceptable as support and servicing spaces for the overall building.
10	Secondary stairs			Should generally remain unchanged, although minor changes for access compliance etc. may be acceptable.
11	Grand Lodge assembly room (Caulfield)			Should remain generally unchanged.
12	Lodge Room 2 (Rennie)			

13	Stair landing			Should generally remain unchanged, although minor changes for access compliance etc. may be acceptable.
14	Cloak room			Should generally remain unchanged, although minor changes for access compliance etc. may be acceptable (e.g. upgraded vertical circulation infrastructure (lift).
15	General assembly room			Should remain generally unchanged.
16	Lodge Room 1 (Cummins)			Some modifications may be acceptable as support and servicing spaces for the overall building.
17	Store and regalia room			

The following floor plans depict the general significance of each room of the building as well as the likely ability to sustain change/modification:



Streetscape attributes			
Vantage Point (as per Figure 5.1)	Significance	Ability for change <sup>11</sup>	Recommendations and ability for modification
1			Changes to the building are unlikely to have any significant impact from these vantage points.
2			
3			Changes to the front bay of the building would have impact upon the streetscape values of the building from these vantage points.
4			Changes to the building further rearward are less likely to have such impact.
5			

<sup>11</sup> To be read in conjunction with the fabric and form analysis above.

## 9. Statement of Archaeological Potential

### 9.1. Archaeological Methodology

This statement of archaeological potential is derived from a process which identifies the potential of the site to yield archaeological remains, the significance of any remains, and their potential to yield meaningful information about the site, and which might contribute to relevant key archaeological and historical themes. The following briefly outlines the methodology followed:

Determining general archaeological potential: Through a desktop analysis of historical data and secondary sources, as well as non-invasive site observations, an understanding of the evolution of the site has been gained which has allowed an assessment of the archaeological potential (however significant) of any part of the site - resulting in substantiated predictions of the likelihood of finding *something* upon any particular part of the site.

This has been done by analysing primary source material, summarizing the developmental history of the site and developing a chronological narrative detailing an overview of the history of all known features to have ever existed on the site. Where possible, developmental overlays have been developed from historic maps, plans, photographs and other visual documentation. This overlay has been supported by other observations providing supplementary information, and also includes processes such as demolition and disturbance which may have removed or destroyed potential remains – and may have diminished the archaeological potential.

Assessing the significance and potential of any likely archaeological resources to yield meaningful information: Upon understanding the archaeological potential through desktop and site analysis, the next step was to understand its relationship to any aspect of the identified significance of the place – e.g. do the remains have the potential to demonstrate an aspect of the significance of the site or related key historic theme? The potential for any of the archaeological remains to demonstrate important aspects of the history of the site, whether in a state, regional or thematic context, is to be considered.

Understanding possible impact of development and formulation of management strategies: Based on any identified archaeological potential and significance of the site, consideration will be given as to whether the proposed development will impact upon any likely archaeological remains and if necessary broad management strategies will be proposed to manage any impact.

Table 1 (below) demonstrates the steps of this assessment:

Methodology for formulation of the statement of archaeological potential		
	If 'no'	If 'yes'
<b>1. Archaeological potential.</b> Are you likely to find something if you dig here? (i.e. a <u>Statement of Archaeological Potential</u> ).	Further action may not be required, although a contingency plan may be required for unexpected finds.	The significance of the archaeological potential should be investigated.
<b>2. Significance.</b> Could anything you find here greatly contribute to our understanding of the site or related significant theme?	Further action may not be required.	The likely integrity of the archaeological remains should be investigated.
<b>3. Integrity.</b> Are any archaeological remains likely to be intact?	Further action may not be required, although a contingency plan is required for unexpected integrity.	The likelihood of significant archaeological remains is confirmed.
<b>4. Impact</b> Will proposed works impact upon the significant archaeological remains? i.e. an <u>Archaeological Impact Assessment</u> .	Further action may not be required, although a contingency plan may be required for unexpected impacts.	An <u>Archaeological Method Statement</u> will be required to detail how impact will be managed/mitigated.

## 9.2. Background History

As per the methodology above, the historical background of the subject site has been provided here in Section 4 – which provides the basis for the formulation of the statement of historical archaeological potential. It is known that the site was part of the gardens of Heathfield which was established around 1829, and remained as such for almost a century until Heathfield was sold and subdivided in the 1920s. The only development of the site prior to the division of Heathfield was the Latham nursery and seed production operation of the early 1900s, which probably only included ephemeral buildings of no archaeological consequence.

### **9.3. Possible archaeological remains and their significance and research framework**

As per the site history outlined in Section 3, the development of the subject site is very simple – and in effect the current building is the only major phase of development having ever been on that site. The existing building and its use is not considered likely to have any archaeological research potential in its own right.

**In conclusion, it is considered that the site has no archaeological potential.**

**No archaeological input is considered necessary in any future development of the site.**

## 10. Heritage Management Policies

### 10.1. Conservation objectives

The following objectives drive the conservation management process for the subject site:

- The desire to retain the significant heritage elements of the building.
- To maintain the contribution that the building makes to the wider site environs
- To reduce or alleviate the need for heritage input in elements of the site which are clearly of no heritage value.
- Ideally to maintain the ongoing viable use of the building for Freemasonry purposes.
- To guide the use and/or adaptive reuse of the building and in particular the use of the building for public purposes.
- To guide any future development in a direction likely to gain statutory approvals (and have no heritage impact).

### 10.2. Purpose of policy and definitions and role of statement of significance

It is expected that any administrators and professionals planning and undertaking physical works on site will first familiarise themselves with all general conservation policies, then the specific implementation recommendation (based on these policies) relating to the particular element on which works are being planned.

Any conservation policy strongly favours the conservation of elements of primary significance, and the removal of elements which may be of detriment to the conservation or interpretation of elements of a higher significance. A thorough understanding of the statement of significance, and the specific significance of individual items, is therefore essential in appreciating how specific policies have been developed, and how these should be applied to the physical attributes of the place.

The statement of significance has defined and ranked the eras and themes which that place represents, and the analysis of the physical attributes has detailed what represents such. Each element of the physical fabric has been assigned its own significance level, based on its ability to demonstrate the significance of the place, and thresholds for assignment of this significance have been kept consistent in the assessment of all elements.

The policies below, therefore, broadly guide how this fabric should be treated in order to allow it to better conserve and demonstrate the statement of significance.

### 10.3. Development of policy

Having ascertained the ability for fabric to demonstrate the statement of significance, constraints, opportunities and requirements are considered, alongside stakeholder requirements, to develop the broad conservation policies below. Whilst conservation policies may be contrary to the constraints, opportunities or requirements, the policies aim to best address these whilst still maintaining appropriate conservation practice. Any unresolved conflict is then specifically debated in the implementation strategy.

### 10.4. Policies

As per the methodology in Section 3, the underlying principle by which all conservation practices should be guided is the ICOMOS Australia *Burra Charter*. The statement of significance has defined the attributes of the site of which greatest significance is assigned, thereby the priority of conserving attributes associated with such should be considered paramount compared to those of lesser significance. This, however, must be balanced with retention of elements of lesser significance where guided by the conservation policies. With the statement of significance in mind and with the adoption of Burra Charter principles this section will introduce the conservation policies developed specifically for the subject site.

1	General Policies		Objective of Policy
1.1	Approach to works	The approach to managing any works on the place must be guided by the principles of the ICOMOS Australia <i>Burra Charter</i> . <sup>42</sup>	To ensure that all works are undertaken in a manner which seeks to enhance and not diminish the heritage values of the place and that adequate heritage input is included in any future development of the place.
1.2	Use/development	Any use or development of the place must not have any unreasonable adverse heritage impact upon identified values of the place.	
1.3	Supervision	All works to the significant elements of the place, and planning for such works, must be guided by a conservation architect, heritage consultant or other person(s) qualified and experienced in the conservation of historic heritage places.	
1.4	Ongoing use	The <b>critical</b> aspect of maintaining the holistic heritage values of the building, including architecture, fabric, associations and contents, is the <b><u>continued use of the building for Freemasonry purposes</u></b> . The loss or abandonment of such use would have an overarching negative heritage impact upon all values of the place.	To ensure that the significant use that maintains the values of the building (and contents) is retained.
1.5	Compatible use	Whilst it is considered imperative that the building continue to be used for its original purpose. However, if for whatever reason that becomes untenable, then a suitable adaptive reuse for the building should be found. Ideally this should be for publicly accessible purposes which may include a range of commercial activities or meeting/gathering spaces.	To allow the feasible adaptive reuse of the building if necessary.

<sup>42</sup> <http://australia.icomos.org/wp-content/uploads/The-Burra-Charter-2013-Adopted-31.10.2013.pdf>

1.6	Balance of heritage impact	<p>It is acknowledged that some heritage impact may arise from the desire for the place to have a suitable compatible use, particularly in the context of upgrades required for amenity, compliance etc. Any such impacts must be balanced with whether that heritage impact is necessary, under the overarching necessity of a compatible use.</p> <p>It must be acknowledged that non-use or underutilisation of the building may result in a more adverse impact in dereliction or vulnerability of the building.</p>	To allow maximum amenity and compliance of the building in facilitating an appropriate use.
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2	Significant Fabric & forms (to be read in conjunction with Section 8)	Objective of Policy	
2.1	Significant fabric and forms	Parts of the building deemed to be of high significance must be conserved provided that can continue to accommodate the traditional use of the building.	To retain the integrity of the building but to ensure that it remains fit-for-purpose.
2.2	Non-significant fabric and forms	Elements of the building which are of low or no significance may be retained, modified or demolished as desired particularly is this assists in the ongoing significant use of the site.	To recognise that there are portions of the site which have not considered to be of heritage value.

3	Exterior Building Envelope		Objective of Policy
3.1	Original building form	The form and fenestration of the front bay of the exterior building envelope of the building should be retained. Modifications may be possible further rearward.	To recognise that although the form of the building is significant, modifications rear of the prominent front portion of the building may be acceptable (i.e. given that the rear/bulk of the building has no prominence).
3.2	Roof form	The existing roof form may be modified if desired however any such modifications should be rear of the front bay of the building. If required,	

		mechanical plant may be mounted on the roof as discretely as possible (e.g. behind the parapet).	
3.3	Facade	The façade of the building should remain unaltered, particularly the lack of fenestration to the upper floor, the characteristic Inter-War Stripped Classical features and Masonic motifs.	To conserve an important feature that makes the building recognisable as a Masonic building.
3.4	Fenestrative pattern.	The sides of the building rear of the front bay may have the potential for modification of doors/windows particularly if that is necessary to support the ongoing significant use of the building.	To recognise that the sides of the building are less prominent rear of the front bay and that changes to the fenestrative pattern may be necessary to support the ongoing use of the building.
		No additional openings should be installed on the front bay of the building.	To acknowledge that the existing fenestrative pattern is of significance.
3.5	Windows and doors	All original/significant windows and doors must be maintained provided that can occur within a feasible suitable adaptive reuse for the building and ideally where elements are missing these should be reinstated with replica or sympathetic elements.  Any works required for security, thermal, acoustic properties (e.g. double glazing) etc. should be undertaken as discretely and reversibly as possible.	To maintain and/or reinstate missing significant architectural detailing and elevation form.
3.6	Detailing	Significant exterior detailing should be retained, unless replacement is necessary for repair, weatherproofing or security purposes, in which case compatible styling and materials must be used. New work should be sympathetic to, but not necessarily imitative of, the original form, detailing and materials.	

4	Interior		Objective of Policy
4.1	General layout	The general layout of the interior should remain largely unchanged. Any necessary modifications should be limited to areas of lower significance if possible.	To retain the significant and original layout of the building, but to give some scope for modifications if necessary.
4.2.	Significant detailing	Significant detailing, particularly joinery, stairs, fittings and fixtures should remain largely unchanged if feasible, particularly in rooms/areas of the highest significance. Modifications required for ongoing feasible lodge use are likely to be acceptable if a balanced heritage outcome can be planned.	To retain significant interior detailing but to also allow use to maintain the significant original use of the building if necessary.
4.3	General maintenance	General maintenance such as painting, carpets, general upgrades of electrical, plumbing etc. may be done as necessary with no heritage input.	To recognise that general maintenance may occur as needed.

5	Services and National Construction Code compliance		Objective of Policy
5.1	Service installation	The installation of plumbing, electrical, fire detection/protection, security, acoustics and environmental (etc.) services are likely to be acceptable however these should be installed in an unobtrusive manner as possible and in as reversible manner as possible.	To allow for a range of necessary services and upgrades but to seek to minimise any possible heritage impact arising from such.
5.2	Kitchen and bathroom installations	<p>Installation of necessary kitchens and bathrooms is likely to be acceptable (subject to Policy 6.1). These should be discretely located and as reversible as possible.</p> <p>The existing toilet facilities may be removed or modified as desired.</p>	
5.3	Access	The installation of compliant access infrastructure (e.g. a lift) may be undertaken and may also have associated heritage impact. Such impact must be balanced to be as reversible and unobtrusive as possible (preferably in an area of lower significance/integrity).	To acknowledge that such infrastructure is likely to be necessary for the ongoing feasible use of the building and to mitigate the inevitable impacts of such.
5.4	Building surveyor and compliance	Where substantial adverse heritage impact would arise from meeting NCC compliance, detailed dialogue must occur between the building surveyor, heritage consultant, client and permit authorities to balance heritage impact and compliance and to consider feasible alternatives, performance-based solutions and a pragmatic balanced outcome.	To acknowledge that NCC compliance may be more difficult within a heritage building and that the NCC has provisions for a relaxation of requirements based on a satisfactory performance solution where adverse heritage impact may arise.

6	Other policies		Objective of Policy
6.1	Archaeology	Archaeological input is not considered necessary in any future works to the site.	To acknowledge that the site is not likely to have significant archaeological remains present.
6.2	Interpretation	Interpretation of the heritage values of the site is considered to be important, however the ongoing significant use of the site is likely to be the most effective interpretive tool.	To convey the rich social (and other) history of the site.
6.3	Movable cultural heritage	Regular curatorial input should be sought to ensure the effective management of movable cultural heritage. The movable cultural heritage items within the building are a critical contributory element to the overall significance of the place.	To recognise the high significance of the movable cultural heritage items in the building.

### 9. Alignment with statutory heritage provisions

The following table demonstrates how the conservation policies of this document seek to align with the statutory heritage provisions as detailed in Section 2.

#### Clauses E.13.7 (1-3) – Heritage Place

	Policy Guidance	Performance Criteria
<b>E.13.7.1 - Demolition</b>	1.6. Balance of heritage impact.  2.1. Retain significant fabric and forms.  2.2. Remove/modify non-significant fabric and forms.  3.1. Retain form of the forward part of the building.  3.2. Limit modification of roof form to the rear of the building.  3.3. No alterations to the façade.  3.4. Retain fenestrative pattern of the front section of the building.  3.5. Retain significant windows and doors.  3.6. Retain significant exterior detailing.  4.1.. Retain generally interior layout.  4.2. Retain significant interior detailing.	<i>Demolition must not result in the loss of significant fabric, form, items, outbuildings or landscape elements that contribute to the historic cultural heritage significance of the place unless all of the following are satisfied;</i>  (a) <i>there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;</i> (b) <i>there are no prudent and feasible alternatives;</i> (c) <i>important structural or façade elements that can feasibly be retained and reused in a new structure, are to be retained;</i> (d) <i>significant fabric is documented before demolition.</i>

<i>E.13.7.2 – Building and Works other than Demolition</i>	3.1. Retain form of the forward part of the building.	<p><i>P1. Development must not result in any of the following:</i></p> <ul style="list-style-type: none"> <li><i>(a) loss of historic cultural heritage significance to the place through incompatible design, including in height, scale, bulk, form, fenestration, siting, materials, colours and finishes;</i></li> <li><i>(b) substantial diminution of the historic cultural heritage significance of the place through loss of significant streetscape elements including plants, trees, fences, walls, paths, outbuildings and other items that contribute to the significance of the place.</i></li> </ul> <p><i>P2. Development must be designed to be subservient and complementary to the place through characteristics including:</i></p> <ul style="list-style-type: none"> <li><i>(a) scale and bulk, materials, built form and fenestration;</i></li> <li><i>(b) setback from frontage;</i></li> <li><i>(c) siting with respect to buildings, structures and listed elements;</i></li> <li><i>(d) using less dominant materials and colours.</i></li> <li><i>(e)</i></li> </ul> <p><i>P3. Materials, built form and fenestration must respond to the dominant heritage characteristics of the place, but any new fabric should be readily identifiable as such.</i></p> <p><i>P4. Extensions to existing buildings must not detract from the historic cultural heritage significance of the place.</i></p>
	3.2. Limit modification of roof form to the rear of the building.	
	3.3. No alterations to the façade.	
	3.4. Retain fenestrative pattern of the front section of the building.	
	3.5. Retain significant windows and doors.	
	Not applicable	<p><i>P5. New front fences and gates must be sympathetic in design, (including height, form, scale and materials), to the style, period and characteristics of the building to which they belong.</i></p> <p><i>P6. The removal of areas of landscaping between a dwelling and the street must not result in the loss of elements of landscaping that contribute to the historic cultural significance of the place.</i></p>

<b>E.13.7.3 - Subdivision</b>	Not applicable.	<p><i>P1. A proposed plan of subdivision must show that historic cultural heritage significance is adequately protected by complying with all of the following:</i></p> <ul style="list-style-type: none"> <li><i>(a) ensuring that sufficient curtilage and contributory heritage items (such as outbuildings or significant plantings) are retained as part of any title containing heritage values;</i></li> <li><i>(b) ensuring a sympathetic pattern of subdivision;</i></li> <li><i>(c) providing a lot size, pattern and configuration with building areas or other development controls that will prevent unsympathetic development on lots adjoining any titles containing heritage values, if required.</i></li> </ul>
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**Clause E.13.10 - Place of Archaeological Potential**

	<b>Policy Guidance</b>	<b>Performance Criteria</b>
<b>E.13.10.1 – Building and Works other than Demolition</b>	6.1 - Archaeology	<p><i>P1. Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:</i></p> <ul style="list-style-type: none"> <li><i>a) the nature of the archaeological evidence, either known or predicted;</i></li> <li><i>b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;</i></li> <li><i>c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition;</i></li> <li><i>d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;</i></li> <li><i>(b) measures proposed to preserve significant archaeological evidence ‘in situ’.</i></li> </ul>
<b>E.13.10.2 – Subdivision</b>	Not applicable	<p><i>P1. Subdivision must not impact on archaeological resources at Places of Archaeological Potential through demonstrating either of the following:</i></p> <ul style="list-style-type: none"> <li><i>(c) that no archaeological evidence exists on the land;</i></li> <li><i>(d) that there is no significant impact upon archaeological potential.</i></li> </ul>

## 10. The proposed development and heritage impact assessment

### 10.1. The proposed development

A proposal has been formulated by Architects Designhaus (Hobart) on behalf of the Tasmanian Freemason Hall Company Limited (the owner) for the construction of a 48 room hotel development to be built on top of the rear portion of the Masonic Temple building. This would involve a 7-level hotel building (plus rooftop commercial tenancy) to be constructed above the existing lodge building.

The following heritage impact assessment is based on the following documents to be submitted as part of the forthcoming development application to Hobart City Council (and Works Application to the Tasmanian Heritage Council):

St David's Hotel, 3 Sandy Bay Road, Hobart, Architects Designhaus drawings (Job 1802):

1802, DA00	Site plan
1802, DA01	Demolition
1802, DA05	Floor Plans – Ground and Level 1
1802, DA06	Floor plans levels 2-7
1802, DA07	Plans level 8 and roof
1802, DA15	North elevation
1802, DA16	East elevation
1802, DA17	South elevation
1802, DA18	West elevation
1802, DA19	Street elevations
1802, DA25	3D Perspectives
1802, DA26	3D Perspectives

(All Rev 1).

Photomontages as provided by Architects Designhaus (from HCC model) and forming part of the development application.

Architects Design Statement, 3 Sandy May Road, Adaptive Reuse of Masonic Temple

**10.2. Consideration of impact upon individual elements**

This section will consider the proposal against the recommendations of Section 8 of the current document in terms of the individual elements of the building. Firstly, the table of recommendations from Section 8 will be considered, then an annotated commentary of the proposal will be similarly considered:

Exterior					
Element		Significance	Ability for change	Recommendations and ability for modification	Commentary on proposal
Overall form				There may be the potential for further change in the rearward bays of the building. No change to the form of the front bay should occur.	The interventions required to achieve this proposal are almost wholly limited to lower significance areas of the building – which have traditionally been used for ancillary/service areas.
Northern elevation				Apart from minor changes for security, soundproofing, thermal efficiency, weatherproofing etc. no changes should be made.	Apart from a backdropping element, the proposal has no impact upon the northern elevation. There will be no change or fabric intervention to that elevation.
Eastern elevation	Front bays				The front bay of the building will not be changed or impacted.
	Rear bay			Modification likely to be possible – noting the wall is on a boundary.	Any fabric interventions associated with the proposal are limited to the rear bay.
Western elevation	Front bays			Apart from minor changes for security, soundproofing, thermal efficiency, weatherproofing etc. no changes should be made.	The front bay of the building will not be changed or impacted. The interventions on this elevation are minor and limited to the rear bay and rear portion of the central bay.
	Rear bay				

Southern elevation				Modification likely to be possible – noting the wall is on a boundary.	Apart from partial obscuring by columns, this elevation will remain unchanged.
Roof form				Modification may be possible given the very low prominence of the roof form.	The proposal has some impact on the roof form, limited to the rear section which is not prominent.
Rainwater goods				Modification for upgrades, efficiency etc. likely to be possible.	
Masonry	Front bays			All cream-brick and sandstone masonry should remain as existing.	Generally, the front bays will not be impacted. The very front bay will not be subject to any change at all.
	Rear bays			Modifications (e.g. paint, render) may be possible on red brick.	Mostly, modifications are limited to the rear bay.
Windows	Glazed windows			Apart from minor modifications for security, thermal efficiency or soundproofing, windows should be retained as existing. Obscure glazing ideally should be retained.	No significant/prominent windows or blind windows will be impacted by the proposal.
	Blind windows			Blind windows should remain as such.	
	New apertures			New apertures should generally be avoided unless necessary for critical functional reasons.	New apertures are limited to the rear of the central bay and are minor – necessary for the improved access arrangements for the building.
Doors	Front			Apart from minor modifications for security, thermal efficiency or soundproofing, the front doors should be retained as existing. Obscure glazing and Masonic motifs must be retained.	The front doors will not be impacted.
	Side			Modification may be possible for functional/compliance reasons.	One set of side doors will be replaced for the new access arrangements.

Driveways				Modification for functional purposes is likely to be acceptable.	The western driveway will have increased prominence as the main entry for the proposed development.
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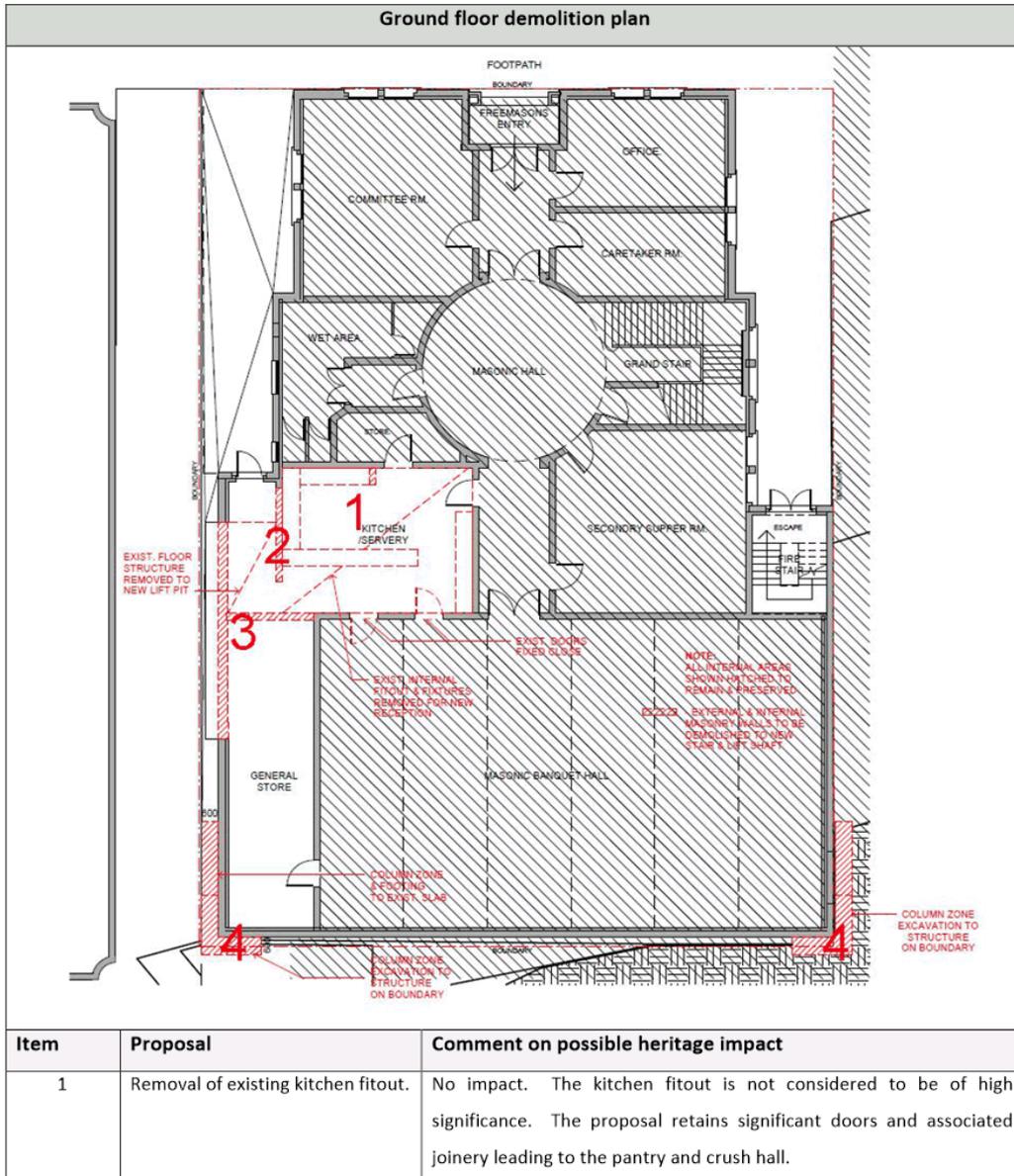
Interior					
Room		Significance	Ability for change	Recommendations and ability for modification	Commentary on proposal
1	Entrance foyer, main stairway and crush hall			Should generally remain unchanged, although minor changes for access compliance etc. may be acceptable.	No impact.
2	Offices			Some modifications may be acceptable as support and administration spaces for the overall building.	No impact
3					
4					
4	Library and committee room				
5	Toilets			Upgrades for serviceability/compliance are likely to be acceptable. These areas may also offer feasible space for upgraded vertical circulation infrastructure (e.g. lift).	No impact
6	Kitchen and pantry				The kitchen will be removed for the installation of the hotel reception in this area. Generally, significant fabric (e.g. doors etc. to other rooms) will be retained and this area will be enhanced as a focal point for entry into the building.
7	Supper room			Should remain generally unchanged.	No impact
8	Banquet hall				No impact
9	Store			Some modifications may be acceptable as support and servicing spaces for the overall building.	The store area will be modified to accommodate the new lift and stairway. Any impact arising is

					considered offset by the improved access arrangements.
10	Secondary stairs			Should generally remain unchanged, although minor changes for access compliance etc. may be acceptable.	No impact
11	Grand Lodge assembly room (Caulfield)			Should remain generally unchanged.	No impact
12	Lodge Room 2 (Rennie)				No impact
13	Stair landing			Should generally remain unchanged, although minor changes for access compliance etc. may be acceptable.	No impact
14	Cloak room				No impact
15	General assembly room			Should generally remain unchanged, although minor changes for access compliance etc. may be acceptable (e.g. upgraded vertical circulation infrastructure (lift).	Some impact will arise with the removal of a wall in this room, and the need to relocate joinery elements and movable cultural heritage. However the use of the general assembly room as a lift lobby (and continuing its original function) is considered to outweigh that impact in that it will now allow compliant access to the first floor of the building to assist in ensuring the feasible ongoing use of the building for its original and significant purpose.
16	Lodge Room 1 (Cummins)			Should remain generally unchanged.	No impact
17	Store and regalia room			Some modifications may be acceptable as support and servicing spaces for the overall building.	Some impact will arise with the removal of a wall in this room, and the need to relocate joinery elements and movable cultural heritage.

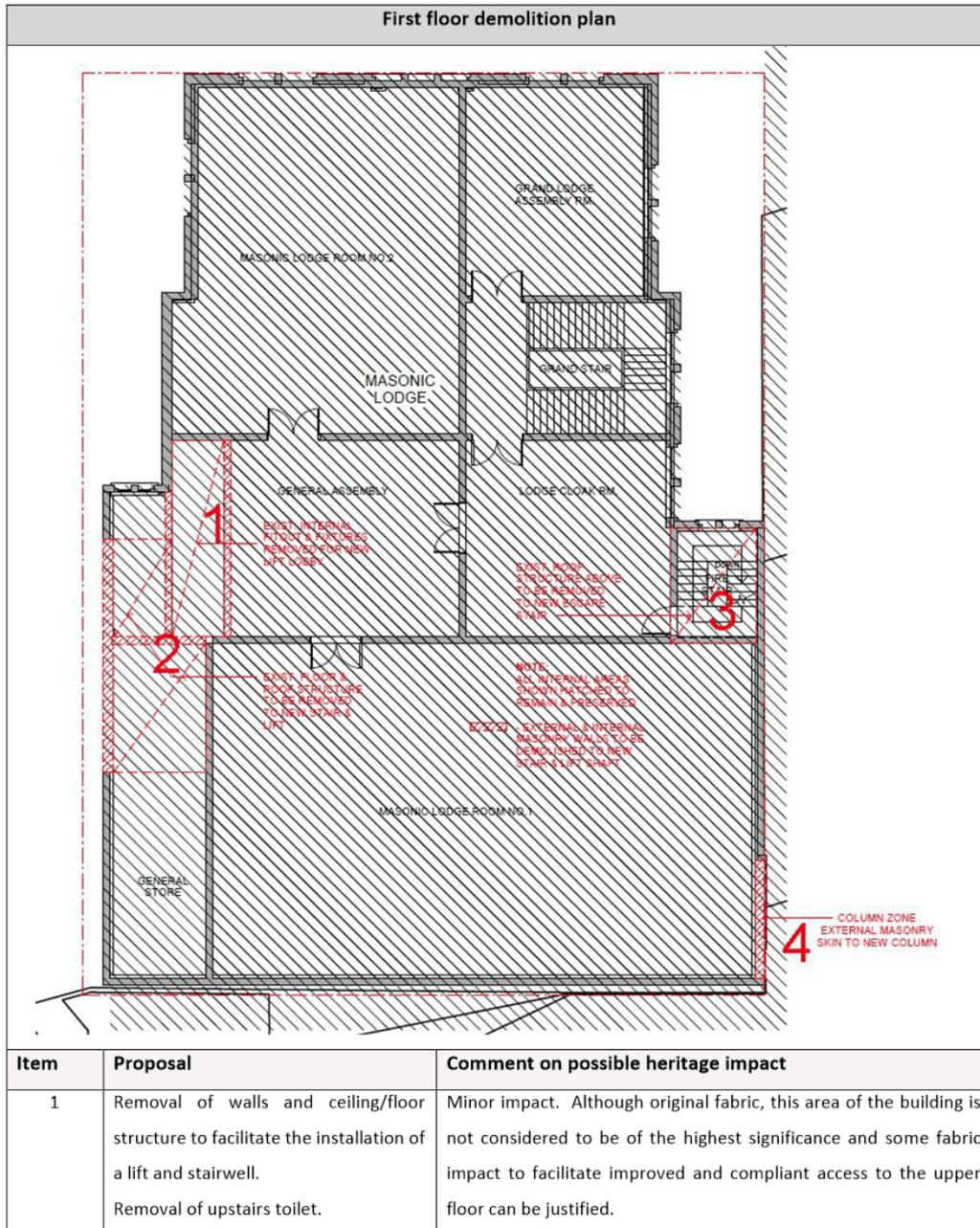
					However the use of this area for the installation of the lift and stairwell is considered to outweigh that impact in that it will now allow compliant access to the first floor of the building to assist in ensuring the feasible ongoing use of the building for its original and significant purpose.
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Streetscape attributes					
Vantage Point (as per Figure 5.1)	Significance	Ability for change <sup>13</sup>	Recommendations and ability for modification	Commentary on proposal	
1			Changes to the building are unlikely to have any significant impact from these vantage points.	The proposed development will be largely indiscernible from these vantage points.	
2					
3			Changes to the front bay of the building would have impact upon the streetscape values of the building from these vantage points.  Changes to the building further rearward are less likely to have such impact.	The proposed development will be discernible as a backdropping element to the existing building from these vantage points, however the existing building will retain the streetscape dominance of the site and retain its place in the streetscape as being surrounded by larger buildings.	
4					
5					

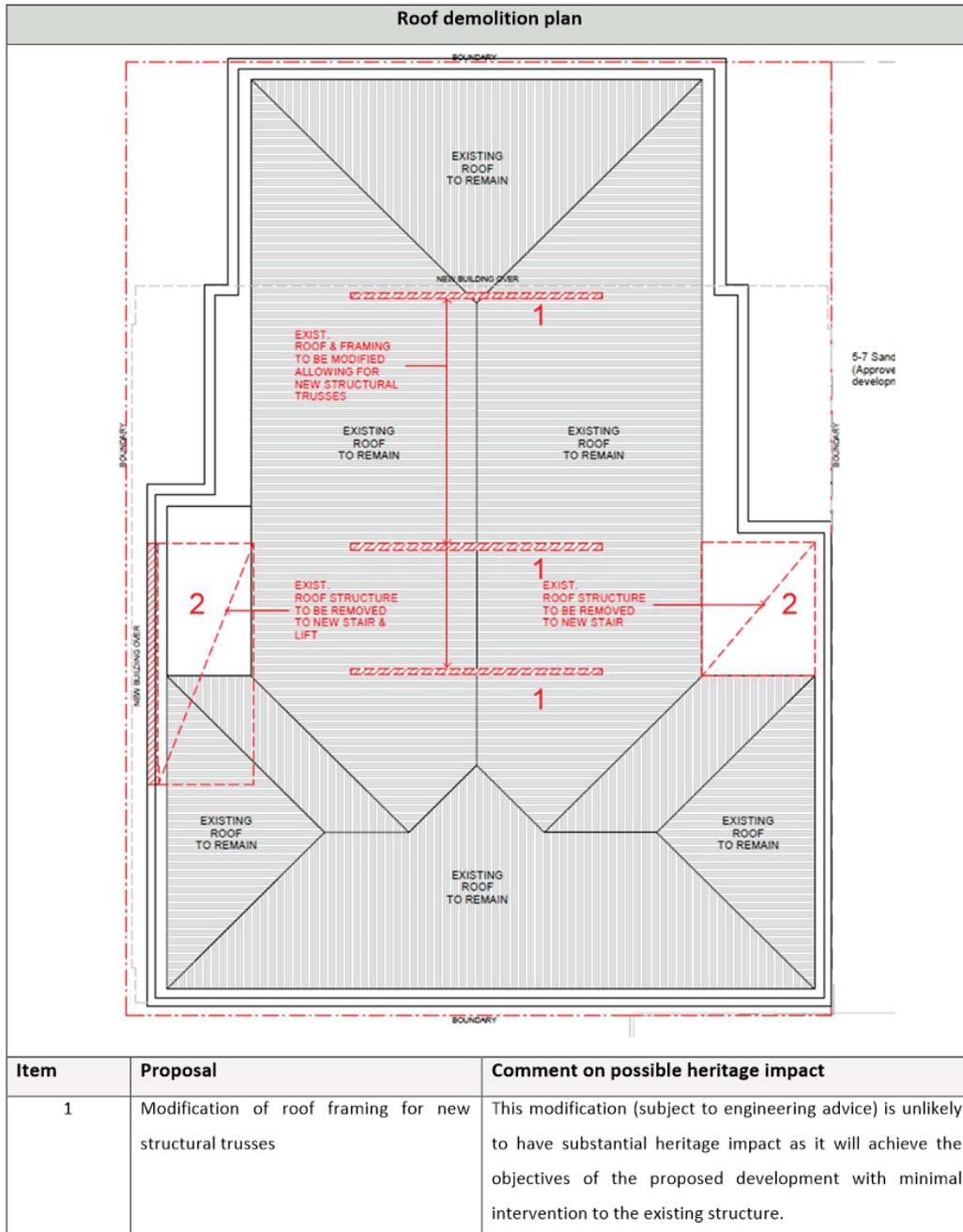
<sup>13</sup> To be read in conjunction with the fabric and form analysis above.



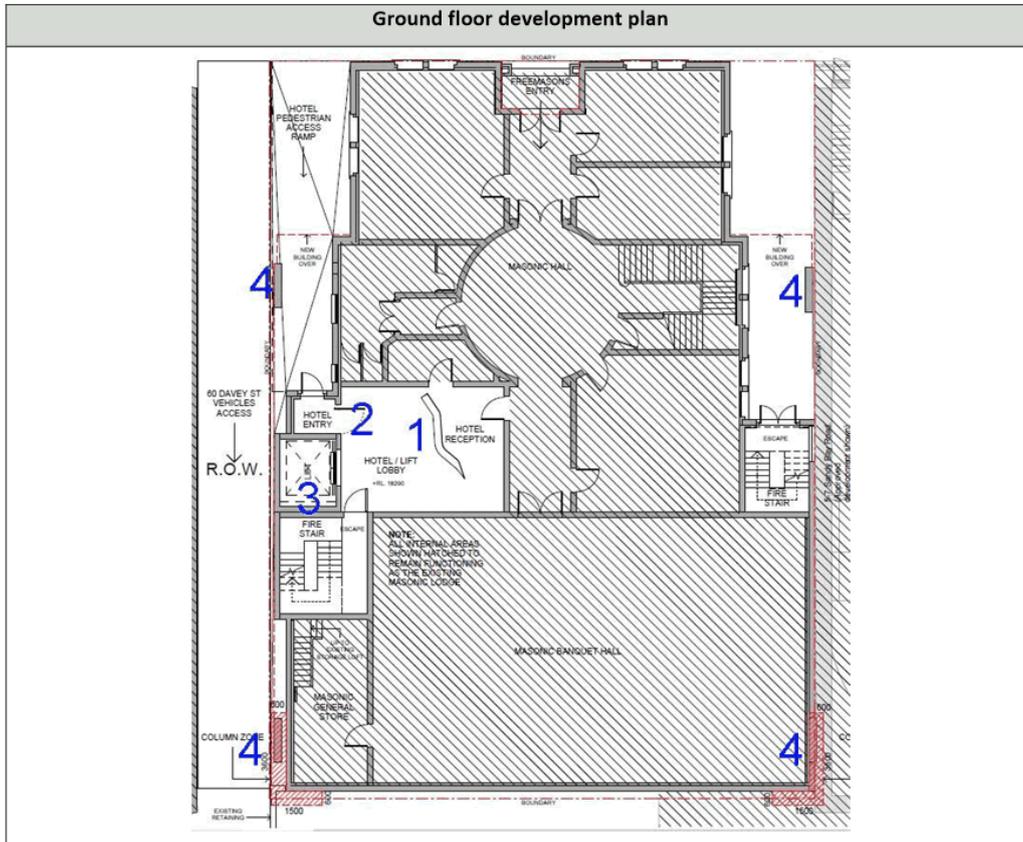
2	Removal of walls and ceiling/floor structure to facilitate the installation of a lift and stairwell.	Minor impact. Although original fabric, this area of the building is not considered to be of the highest significance and some fabric impact to facilitate improved and compliant access to the upper floor can be justified.
3	Removal of a section of exterior wall to facilitate the installation of a lift and stairwell.	Minor impact. This is part of the less articulated portion of the building and partial removal of the wall to facilitate improved and compliant access to the upper floor can be justified.
4	Excavation for columns.	No impact. Provided the building is protected during works. The site has no archaeological potential therefore excavation will have no impact.



		Significant joinery from that wall should be retained for possible reuse/future repairs elsewhere in the building and movable cultural heritage (furniture and regalia) will need to be carefully relocated.
2	Removal of a section of exterior wall to facilitate the installation of a lift and stairwell.	Minor impact. This is part of the less articulated portion of the building and partial removal of the wall to facilitate improved and compliant access to the upper floor can be justified.
3	Removal of roof above stairwell.	Minor impact. This is not a prominent part of the roof being far back on the site and hidden by the parapet.
4	Partial demolition of exterior wall for column.	Minor impact. This is part of the less articulated portion of the building and partial removal of the wall to facilitate development can be justified.

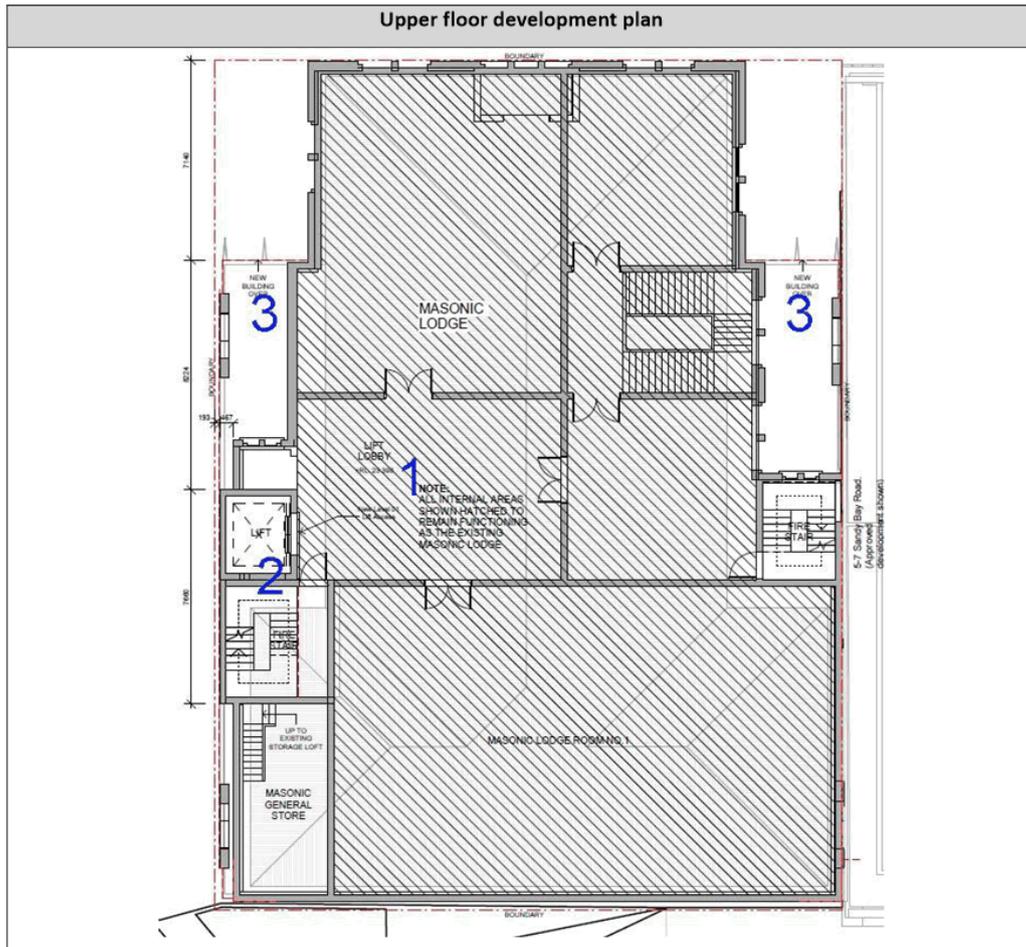


2	Modification of roof structure for the accommodation of new vertical circulation infrastructure.	Minor impact. Although original fabric, this area of the building is not considered to be of the highest significance and some fabric impact to facilitate improved and compliant access to the upper floor can be justified.
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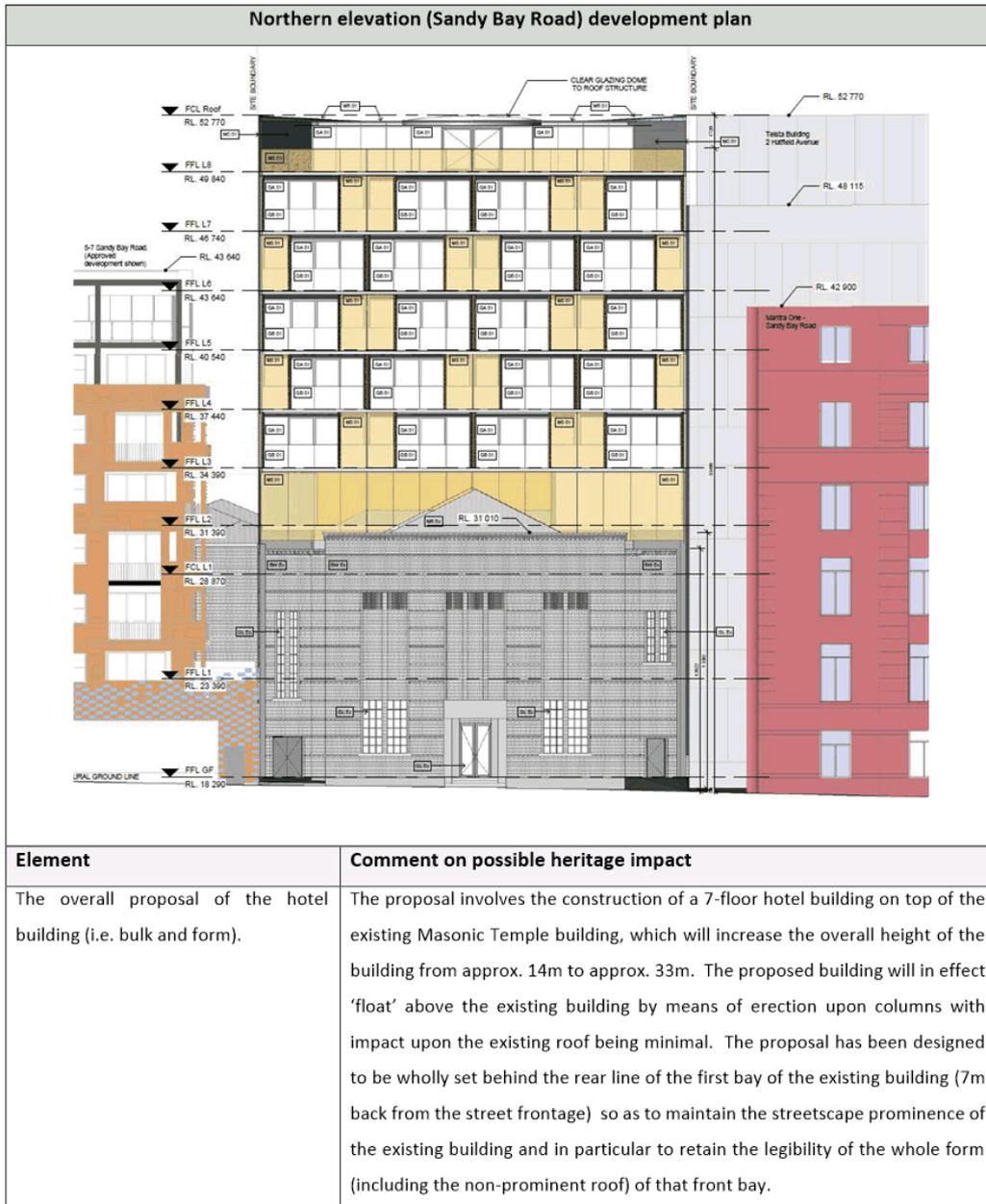
Item	Proposal	Comment on possible heritage impact
1-2	Reconfiguration of former kitchen for hotel reception and lobby and new side entrance.	The repurposing of this ancillary area as the main focal point for hotel entry – as distinct from the more prominent entrance at front to the masonic building, is considered an appropriate response to providing the necessary hotel entry whilst maintaining the prominence of the traditional use and entrance to the building. Note that detail of any future new kitchen facilities to support ongoing Masonic use of the building will need to be detailed as part of a future proposal.
3	Installation of new stairs and lift in former storage area.	This is considered a logical place for installation of new vertical circulation infrastructure as this is a non-prominent part of the

		building and provides the requisite infrastructure without being overtly obvious in the more formal/forward parts of the building (note that this will also serve the ongoing Masonic purposes on-site which is considered a substantial advantage.
4	New structural columns for the proposed hotel building.	These columns are independent of the existing building and provide as minimalist and as low impact as possible solution to providing the requisite structure for the proposed development.

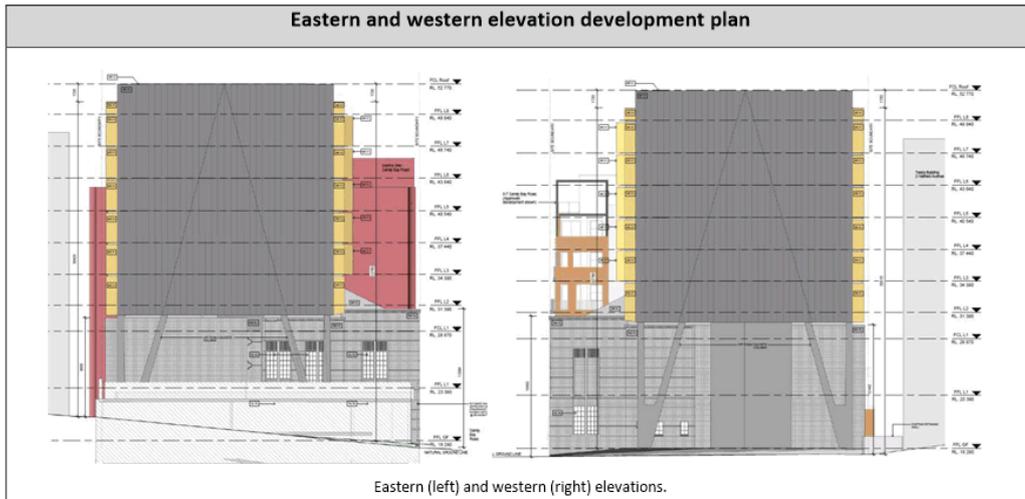


Item	Proposal	Comment on possible heritage impact
1	Reconfigured assembly hall area for the Masonic temple.	A positive heritage outcome in improving the circulation and accessibility of the building to support its traditional and significant use.
2	Installation of new stairs and lift in former storage area.	This is considered a logical place for installation of new vertical circulation infrastructure as this is a non-prominent part of the building and provides the requisite infrastructure without being overtly obvious in the more formal/forward parts of the building (note that this will also serve the ongoing Masonic purposes on-site which is considered a substantial advantage.

3	Building above forming an undercroft approach area to the rear access points to the building.	As per the discussion below regarding the overall consideration of impact of the proposed hotel building above, the setback of the building rear of the front bay of the existing building is considered to be a sympathetic approach to retaining the streetscape prominence of the existing building and forming such an undercroft area to the approaches to the rear doors of the building is not considered detrimental.
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	<p>The presence of the building will read almost wholly as two dimensional given that side-distant and oblique views are very limited owing to the large buildings to either side of the existing building. Close views of the proposed building will be even further limited as from street level the setback of the proposed building behind the front bay of the existing building will retain the dominance of the existing building from near environ views. Straight front-on views are very limited, being only possible from a limited stretch of Sandy Bay Road and filtered through established vegetation through St Davids Park.</p> <p>Although not evident from street level, the building uses masonic motifs on the rooftop and a domes roof interpretive of the dome in the entrance hall of the existing building as a means of providing some interpretation of the significant use and</p>
<p>Materials palette</p>	<p>This elevation will be dominated by glazing, however this will be broken-up by way of mesh screen panels set in somewhat of a chequerboard pattern to minimise any large expanses of glass and also to relate to the geometric elements of the existing building. The floor plates of each level will provide some horizontality akin to the horizontal brick banding of the existing building and providing some interpretation of that design element.</p>



Eastern (left) and western (right) elevations.

Element	Comment on possible heritage impact
<p>The overall proposal of the hotel building (i.e. bulk and form).</p>	<p>Apart from the very limited glimpses of the front two bays of the building gained from near oblique approaches and within the laneways beside the building, the side elevation of the building are not, and never have been a prominent element which is anticipated in the design of the original building with blank walls to the rear (near boundary) bay. These elevations are somewhat lost in the two-dimensional nature of views of the building (i.e. from the front) and are only visible from a limited range of oblique approaches along Harrington Street and Sandy Bay Road.</p>
<p>Materials palette</p>	<p>The side elevations will be clad in zinc as blank walls – these will be largely covered from view by adjacent buildings and will only be discernible from distant views above those buildings. Mesh screens projecting front and rear will provide a softening of the corners of these blank walls.</p>

Southern elevation development plan	
Element	Comment on possible heritage impact
The overall proposal of the hotel building (i.e. bulk and form).	The southern (rear) elevation of the existing building is completely unadorned as a rear boundary wall. This elevation of the building is not considered to have any significant qualities and the construction of the proposed building above will have no unreasonable impact. The mesh screening will allow glimpses of the rear of the original roof form which will be maintained beyond.
Materials palette	This elevation will be dominated by glazing; however this will be broken-up by way of mesh screen panels set in somewhat of a chequerboard pattern to minimise any large expanses of glass. The floor plates of each level will provide some horizontality akin to the horizontal brick banding of the existing building and providing some interpretation of that design element.

### 10.3. Consideration of streetscape impact

Note that the ‘proposed development’ drawings are supplied here with the adjacent approved development at 5-9 Sandy Bay Road included.

Point	Importance to the place	Current situation	Proposed development
1 <sup>14</sup>	The building has no streetscape presence upon approach from the bend in Sandy Bay Road and is wholly obscured by neighbouring development (which will be accentuated in the event of the approved development at 5-7 Sandy Bay Road going ahead). The surrounding development is large with the subject building being the smallest element in that cluster of buildings.	 <p style="text-align: center;"><b>Impact assessment</b></p> <p>Whilst largely hidden by the proposed adjacent development (as well as the existing former ABC building) the proposed development would be discerned as part of an established cluster of larger buildings with the Masonic Temple being barely visible from this point.</p> <p><b>It is concluded that the proposed development has no adverse heritage impact from this vantage point.</b></p>	

<sup>14</sup> Vantage point numbering relates to those points introduced as being of significance in Section 6 of the current document.

Point	Importance to the place	Current situation	Proposed development
2	<p>Even upon nearer approaches, the building is still a non-prominent element in the streetscape with the larger surrounding development dominating the streetscape. The approach to the building presents it practically as a two-dimensional façade, with the neighbouring development obscuring the side walls providing no indication of the true depth of the building and the parapet hiding the roof form.</p>		
<b>Impact assessment</b>			
<p>The Masonic Temple is not a prominent streetscape element from this vantage point and appears largely as a two-dimensional element with the depth of the building not able to be discerned. With the existing (former) ABC building and the larger proposed development in the foreground of this vantage point, the proposed development will be largely indiscernible from this vantage point and not appear to be within the same visual context as the visible portion of the Masonic Temple – which would remain the dominant streetfront element amongst larger buildings (which is the current situation).</p> <p><b>It is concluded that the proposed development has no adverse heritage impact from this vantage point.</b></p>			

Point	Importance to the place	Current situation	Proposed development
3.	<p>The building only has a slight three-dimensional presence from very near environs, where still only the front bay of the building is evident from the limited oblique angles where such is visible.</p> <p>This has almost always been the case – with the adjacent former telephone exchange pre-dating the building (Having also been heightened early in the building’s life) and the adjacent former ABC building being built within 25 years of the construction of the Masonic Temple. The building has almost always been within a cluster of larger buildings which has always diminished its streetscape presence.</p>		 <p>Note the elevated angle of this montage exaggerates the ground-level perception of the building.</p>
<b>Impact assessment</b>			
<p>Whilst the proposed development would be a backdropping element in the skyline behind the Masonic Temple, the front bay of the existing building would remain as the dominant streetfront element. The current situation only presents the front bay of the building as a three-dimensional element – with the remainder of the building largely obscured by adjacent development and the roof form hidden by the parapet. The setback of the proposed development would maintain the front portion of the Masonic Temple as the dominant streetfront element and retain that limited sense of the depth of the building.</p> <p><b>It is concluded that the proposed development has no adverse heritage impact from this vantage point.</b></p>			

Point	Importance to the place	Current situation	Proposed development
4	<p>The building only has a slight three-dimensional presence from very near environs, where still only the front bay of the building is evident from the limited oblique angles where such is visible.</p> <p>This has almost always been the case – with the adjacent former telephone exchange pre-dating the building (Having also been heightened early in the building’s life) and the adjacent former ABC building being built within 25 years of the construction of the Masonic Temple. The building has almost always been within a cluster of larger buildings which has always diminished its streetscape presence.</p>		
<b>Impact assessment</b>			
<p>Whilst the proposed development would be a backdropping element in the skyline behind the Masonic Temple, the front bay of the existing building would remain as the dominant streetfront element. The current situation only presents the front bay of the building as a three-dimensional element – with the remainder of the building largely obscured by adjacent development and the roof form hidden by the parapet. The setback of the proposed development would maintain the front portion of the Masonic Temple as the dominant streetfront element and retain that limited sense of the depth of the building. This view of the building has always been obscured by larger development with the adjacent (former) Telecom building built in the 1920s.</p> <p><b>It is concluded that the proposed development has no adverse heritage impact from this vantage point.</b></p>			

Point	Importance to the place	Current situation	Proposed development
5	<p>The prominence of the building is also diminished when viewed from St David’s Park, where it forms part of a grouping of much larger buildings where it appears as a minor streetscape element, also somewhat two-dimensional due to the depth of the building not being evident. Towers behind the building distract the viewline also which is also buffered by the plantings in St David’s Park.</p>		<p style="text-align: center;"><b>Impact assessment</b></p> <p>Whilst the proposed development would be a backdropping element in the skyline behind the Masonic Temple, the front bay of the existing building would remain as the dominant streetfront element. The building has a very two-dimensional presence from this point which is largely obscured by the trees of St Davids Park. The skyline surrounding the building is already dominated by high development and the proposed development would read as a contiguous part of that.</p> <p><b>It is concluded that the proposed development has no adverse heritage impact from this vantage point.</b></p>

Point	Importance to the place	Current situation	Proposed development
6	<p>The prominence of the building is also diminished when viewed from St David’s Park, where it forms part of a grouping of much larger buildings where it appears as a minor streetscape element, also somewhat two-dimensional due to the depth of the building not being evidence. Towers behind the building distract the viewline also which is also buffered by the plantings in St David’s Park.</p>	 <p style="text-align: center;"><b>Impact assessment</b></p> <p>Whilst the proposed development would be a backdropping element in the skyline behind the Masonic Temple, the front bay of the existing building would remain as the dominant streetfront element. The building has a very two-dimensional presence from this point which is largely obscured by the trees of St Davids Park. The skyline surrounding the building is already dominated by high development and the proposed development would read as a contiguous part of that.</p> <p><b>It is concluded that the proposed development has no adverse heritage impact from this vantage point.</b></p> <p><b>It is concluded that the proposed development has no adverse heritage impact from this vantage point.</b></p>	

## 10.4. Consideration of proposal against heritage management policies

1	General Policies		Commentary on proposal
1.1	Approach to works	The approach to managing any works on the place must be guided by the principles of the ICOMOS Australia <i>Burra Charter</i> . <sup>15</sup>	The design process for the place has been guided by the principles of the Burra Charter – in particular the desire for the continue use of the heritage place for the original use intended.
1.2	Use/development	Any use or development of the place must not have any unreasonable adverse heritage impact upon identified values of the place.	The proposed development is intended to provide the owners and users of the Masonic Temple with a reliable and perpetual income stream to make viable the continued occupation of the building and for Masonic traditions to continue on that site – which is a critical part of the significance of the building. That cultural continuity of use within the largely unchanged interior of the existing building, maintaining the streetscape presence of the building and allowing a gentle evolution of the building to suit lodge needs is a far better heritage outcome than the abandonment of the building and adaptive reuse for other purposes – which would largely sever the meaning of the building from its traditional use and probably result in more heavy-handed interventions to the existing building for an adaptive reuse. Some heritage impact to the fabric and setting of the existing building is likely to be acceptable to achieve a means by which the continued occupation of the building for lodge purposes. Refer to the Architects Design Statement for further discussion on this point.
1.3	Supervision	All works to the significant elements of the place, and planning for such works, must be guided by a conservation architect, heritage consultant or other person(s) qualified and experienced in the conservation of historic heritage places.	The proposal has been formulated by an architect with heritage experience and guided by the current document which has been authored by a heritage consultant.
1.4	Ongoing use	The <b>critical</b> aspect of maintaining the holistic heritage values of the building, including	See comments at 1.2.

<sup>15</sup> <http://australia.icomos.org/wp-content/uploads/The-Burra-Charter-2013-Adopted-31.10.2013.pdf>

		<p>architecture, fabric, associations and contents, is the <b>continued use of the building for Freemasonry purposes</b>. The loss or abandonment of such use would have an overarching negative heritage impact upon all values of the place.</p>	
1.5	Compatible use	<p>Whilst it is considered imperative that the building continue to be used for its original purpose. However, if for whatever reason that becomes untenable, then a suitable adaptive reuse for the building should be found. Ideally this should be for publicly accessible purposes which may include a range of commercial activities or meeting/gathering spaces.</p>	<p>The proposal only involves the modification of the current kitchen (a less significant area) to the hotel lobby. All other parts of the Masonic Temple building remain in-use for their original and significant purpose. The installation of the lift and stairwell enhance the ability to use the building for its traditional purpose with installing compliant access to the upper floor. Presumably the intrusive stairlift currently installed in the main stairway will be removed in favour of the use of the lift which is an advantage in reinstating the original tenor of the stairs.</p>
1.6	Balance of heritage impact	<p>It is acknowledged that some heritage impact may arise from the desire for the place to have a suitable compatible use, particularly in the context of upgrades required for amenity, compliance etc. Any such impacts must be balanced with whether that heritage impact is necessary, under the overarching necessity of a compatible use.</p> <p>It must be acknowledged that non-use or underutilisation of the building may result in a more adverse impact in dereliction or vulnerability of the building.</p>	<p>It is acknowledged that the proposal results in some heritage impact in that there is the removal of some original (but lower significance) fabric of the existing building and that the proposed development will act as a backdrop element to the existing building. The form and placement of the proposed building has been considered and located in a manner which maintains the perceived independence and streetscape prominence of the front bay of the existing building (which is generally the current situation in any case). A certain size of development is the reality when considering the viability of the development with the overarching motivation of providing an income stream for the ongoing lodge use of the original building. It is not uncommon that community organisations, societies and places of worship must undertake development in order to maintain the viability of their sites to ensure continued use for traditional purposes (e.g. the Baptist Tabernacle in Elizabeth Street has recently successfully done so). The abandonment of these buildings for their traditional use in favor of adaptive reuse is often a far less favorable outcome in comparison.</p>

2	Significant Fabric (to be read in conjunction with Section 8)		Commentary on proposal
2.1	Significant fabric and forms	Parts of the building deemed to be of high significance must be conserved provided that can continue to accommodate the traditional use of the building.	The proposed development has explicitly been designed to limit fabric impact to those areas of the building identified as being of lower significance and retains all of the highly significant fabric and forms as existing. Practically all recommendations in Section 8 of this document have been accommodated in the proposal.
2.2	Non-significant fabric and forms	Elements of the building which are of low or no significance may be retained, modified or demolished as desired particularly is this assists in the ongoing significant use of the site.	

3	Exterior Building Envelope		Commentary on proposal
3.1	Original building form	The form and fenestration of the front bay of the exterior building envelope of the building should be retained. Modifications may be possible further rearward.	The proposed retains the front bay of the building without alteration.
3.2	Roof form	The existing roof form may be modified if desired however any such modifications should be rear of the front bay of the building. If required, mechanical plant may be mounted on the roof as discretely as possible (e.g. behind the parapet).	It is acknowledged that the proposal results in a major change to the roofscape of the building, however much of the roof structure is to be retained with the proposed development above the existing roof. The proposed development is set back from the front line of the building so as the front portion of the existing roof is retained – although this is not a prominent element in any case.
3.3	Facade	The façade of the building should remain unaltered, particularly the lack of fenestration to the upper floor, the characteristic Inter-War Stripped Classical features and Masonic motifs.	The façade of the building will remain unchanged and all proposed work is rear of the front bay.
3.4	Fenestrative pattern.	The sides of the building rear of the front bay may have the potential for modification of doors/windows particularly if that is necessary to support the ongoing significant use of the building.	Some minor modifications are proposed in the current area around the doorway near the kitchen. This is well back from the front of the building and does not result in the unreasonable loss of any significant fabric or detailing. That loss can be justified on-balance with the advantages of the new access arrangements both for the first floor of the Masonic Temple and for the hotel extension itself.
		No additional openings should be installed on the front bay of the building.	No additional openings are proposed on the front bay of the building.
3.5	Windows and doors	All original/significant windows and doors must be maintained provided that can occur within a feasible suitable adaptive reuse for the building and ideally where elements are missing these	All original and significant windows and doors will be retained.

		<p>should be reinstated with replica or sympathetic elements.</p> <p>Any works required for security, thermal, acoustic properties (e.g. double glazing) etc. should be undertaken as discretely and reversibly as possible.</p>	
3.6	Detailing	<p>Significant exterior detailing should be retained, unless replacement is necessary for repair, weatherproofing or security purposes, in which case compatible styling and materials must be used. New work should be sympathetic to, but not necessarily imitative of, the original form, detailing and materials.</p>	<p>All significant exterior detailing will be retained. The main interventions to the existing building envelope are on the side walls which are not articulated and to the rear of the roof form which although is original is not prominent and has no remarkable architectural qualities.</p>

4	Interior		Commentary on proposal
4.1	General layout	The general layout of the interior should remain largely unchanged. Any necessary modifications should be limited to areas of lower significance if possible.	The changes to the internal layout of the building are considered minor and are limited to areas of the lowest significance within the building. All principal rooms of the building (including all spaces used primarily for lodge functions) will be retained as existing.
4.2.	Significant detailing	Significant detailing, particularly joinery, stairs, fittings and fixtures should remain largely unchanged if feasible, particularly in rooms/areas of the highest significance. Modifications required for ongoing feasible lodge use are likely to be acceptable if a balanced heritage outcome can be planned.	A small amount of significant detailing (e.g. in the current storage areas and upstairs assembly area) will be removed – mostly to facilitate the installation of the lift and stairs. It is recommended that any significant joinery and detailing be retained for reuse on the site (e.g. in the refit of the kitchen as the hotel lobby) or stored for possible future use.
4.3	General maintenance	General maintenance such as painting, carpets, general upgrades of electrical, plumbing etc. may be done as necessary with no heritage input.	As per routine.

5	Services and National Construction Code compliance		Commentary on proposal
5.1	Service installation	The installation of plumbing, electrical, fire detection/protection, security, acoustics and environmental (etc.) services are likely to be acceptable however these should be installed in an unobtrusive manner as possible and in as reversible manner as possible.	Further heritage input during the Building Application documentation stage will be required to ensure compliance with this policy – which is considered entirely possible.
5.2	Kitchen and bathroom installations	Installation of necessary kitchens and bathrooms is likely to be acceptable (subject to Policy 6.1). These should be discretely located and as reversible as possible.  The existing toilet facilities may be removed or modified as desired.	The existing kitchen will be removed to form the hotel lobby. Whilst this replaces the traditional use of that room, it will in effect focus public attention on that room and will only involve the removal of later and non-significant elements.  Note that a new kitchen to serve the Masonic Temple is not part of the current application.  The downstairs toilets will remain as-existing. The upstairs toilet will be removed which is not considered to have any adverse impact.
5.3	Access	The installation of compliant access infrastructure (e.g. a lift) may be undertaken and may also have associated heritage impact. Such impact must be balanced to be as reversible and unobtrusive as possible (preferably in an area of lower significance/integrity).	The proposal involves the installation of a lift and another stairwell to serve both the hotel, but for the first time to introduce compliant access to the first floor of the Masonic Temple. This is to be introduced into an area of the building of lower significance that has traditionally been used for service/storage purposes and will not interfere with the main lodge spaces within the building and will also not impact the main hallway and crush hall. Whilst a wall will be removed into the upstairs gathering space, this is considered a logical location for the lift and will enhance the use of the Masonic Temple with minimal and justifiable impact.
5.4	Building surveyor and compliance	Where substantial adverse heritage impact would arise from meeting NCC compliance, detailed dialogue must occur between the building surveyor, heritage consultant, client and permit authorities to balance heritage impact and compliance and to consider feasible alternatives,	This policy will need to be considered in the Building Permit application documentation phase ahead of the application for a certificate of compliance. It is recommended that a building surveyor with heritage experience be engaged for this process and that the required dialogue between building surveyor, client, permit authorities and heritage consultant be a rigorous and iterative process consistent with this policy.

		performance-based solutions and a pragmatic balanced outcome.	
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6		Other policies	Commentary on proposal
6.1	Archaeology	Archaeological input is not considered necessary in any future works to the site.	The proposal involves minimal excavation and there is not considered to be any historical archaeological potential on the site in any case.
6.2	Interpretation	Interpretation of the heritage values of the site is considered to be important, however the ongoing significant use of the site is likely to be the most effective interpretive tool.	It is recommended that an interpretation plan be considered as part of the hotel fitout.
6.3	Movable cultural heritage	Regular curatorial input should be sought to ensure the effective management of movable cultural heritage. The movable cultural heritage items within the building are a critical contributory element to the overall significance of the place.	It is recommended that curatorial input be sought for the relocation and protection of movable cultural heritage required as a consequence of these works.

**10.4. Assessment against statutory heritage provisions.**

**Heritage Place**

3 Sandy Bay Road is listed as a *Heritage Place* on Table E13 of the scheme (Place #2053). Any demolition, development or subdivision of the place must be in accordance with the provisions of Part E13.7 of the Scheme (Development Standards for Heritage Places):

	<b>Performance Criteria</b>	<b>Commentary on Proposal</b>
<b>E.13.7.1 - Demolition</b>	<p><i>Demolition must not result in the loss of significant fabric, form, items, outbuildings or landscape elements that contribute to the historic cultural heritage significance of the place unless all of the following are satisfied;</i></p> <p><i>(a) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;</i></p> <p><i>(b) there are no prudent and feasible alternatives;</i></p> <p><i>(c) important structural or façade elements that can feasibly be retained and reused in a new structure, are to be retained;</i></p> <p><i>(d) significant fabric is documented before demolition.</i></p>	<p>The proposed demolition is considered to be minor given that the vast majority of the buildings' interior and exterior remain largely unchanged. The interior demolition is limited to the service areas of the building (i.e. kitchen, stores) as well as the upper assembly hall – no demolition will occur in the principal lodge/ceremonial rooms. This demolition is considered acceptable to achieve the objectives of the proposal which has the added advantage of providing efficient and compliant vertical circulation infrastructure which is currently lacking in the building and will enhance the ongoing use of the building for its traditional and significant purpose. Specifically, aligned with the performance criteria:</p> <ul style="list-style-type: none"> <li>a. The proposed demolition is aligned with the objective of providing an addition to the building which will generate income that is needed for the ongoing viability of Masonic operations in the site – which is a critical part of the heritage value of the building. The extent of demolition can be justified as a means of achieving this economic objective. Providing compliant access to all parts of the building has a social benefit in ensuring equitable access to the building to continue and promote its traditional &amp; significant use.</li> <li>b. The need for income generation to support Masonic activities on the site has considered this action as the most feasible means of providing essential income streams to allow that continued use. Please refer to the Architects Design Statement for further detail on this point (and point a above).</li> <li>c. The entire façade of the building (and much more) is retained.</li> <li>d. A detailed survey of the existing parts of the building affected must be undertaken prior to demolition commencing.</li> </ul>

E.13.7.2 – Building and Works other than Demolition	<p>P1. Development must not result in any of the following:</p> <p>(a) loss of historic cultural heritage significance to the place through incompatible design, including in height, scale, bulk, form, fenestration, siting, materials, colours and finishes;</p> <p>(b) substantial diminution of the historic cultural heritage significance of the place through loss of significant streetscape elements including plants, trees, fences, walls, paths, outbuildings and other items that contribute to the significance of the place.</p>	<p>a) It has always been clear that the proposed addition to the existing building will be comparatively large – and a key driver of the design process was to ensure that the streetscape presence of the existing building be retained. This has resulted in the design elements such as setback behind the front bay, retention of the roof form of the existing building and working with the ability of the lack of oblique and distant views to the building to somewhat hide the extension and maintain the streetfront presence of the existing building. Given that the building is generally only seen in two dimensions, the proposed extension will read more as a backdropping element from the front elevation (i.e. rather than as part of the original building). From other angles this prominence is less obtrusive given that the side and rear elevations of the building are intentionally blank with the anticipation of near-large development. Please refer to the Architects Design Statement for further discussion.</p> <p>b) Not applicable.</p>
	<p>P2. Development must be designed to be subservient and complementary to the place through characteristics including:</p> <p>(a) scale and bulk, materials, built form and fenestration;</p> <p>(b) setback from frontage;</p> <p>(c) siting with respect to buildings, structures and listed elements;</p> <p>(d) using less dominant materials and colours</p>	<p>Whilst acknowledging that the proposed addition is comparatively large, some subservience is achieved by the setback of the proposed extension to maintain the front bay of the existing building to remain as the dominant streetfront element of the site. The lack of oblique views to the building has resulted in this front bay generally being the only prominent part of the site - i.e. the side and rear views are not prominent or (in part) even visible, and are relatively unadorned in anticipation of and response to such. The proposal; retains that characteristic of the building.</p> <p>The materials palette has been chosen to provide a more transparent and ephemeral contrast to the solid masonry and minimally fenestrated Masonic temple to promote a materiality hierarchy which emphasises the dominance of the earlier building and promotes subservience of the addition.</p> <p>Please refer to the Architects Design Statement for further discussion.</p>

	<p><i>P3. Materials, built form and fenestration must respond to the dominant heritage characteristics of the place, but any new fabric should be readily identifiable as such.</i></p>	
	<p><i>P4. Extensions to existing buildings must not detract from the historic cultural heritage significance of the place.</i></p>	
	<p><i>P5. New front fences and gates must be sympathetic in design, (including height, form, scale and materials), to the style, era and characteristics of the building to which they belong.</i></p>	<p>Not applicable.</p>
	<p><i>P6. The removal of areas of landscaping between a dwelling and the street must not result in the loss of elements of landscaping that contribute to the historic cultural significance of the place.</i></p>	<p>Not applicable.</p>

<p><b>E.13.7.3 - Subdivision</b></p>	<p><i>P1. A proposed plan of subdivision must show that historic cultural heritage significance is adequately protected by complying with all of the following:</i></p> <ul style="list-style-type: none"> <li><i>(d) ensuring that sufficient curtilage and contributory heritage items (such as outbuildings or significant plantings) are retained as part of any title containing heritage values;</i></li> <li><i>(e) ensuring a sympathetic pattern of subdivision;</i></li> <li><i>(f) providing a lot size, pattern and configuration with building areas or other development controls that will prevent unsympathetic development on lots adjoining any titles containing heritage values, if required.</i></li> </ul>	<p>Not applicable.</p>
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**Place of Archaeological Potential**

	<b>Performance Criteria</b>	<b>Commentary on Proposal</b>
<b>E.13.10.1 – Building and Works other than Demolition</b>	<p><i>P1. Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:</i></p> <ul style="list-style-type: none"> <li><i>a) the nature of the archaeological evidence, either known or predicted;</i></li> <li><i>b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;</i></li> <li><i>c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition;</i></li> <li><i>d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;</i></li> <li><i>(c) measures proposed to preserve significant archaeological evidence ‘in situ’.</i></li> </ul>	<p>The proposal will not involve excavation in an area considered to be of archaeological potential. Excavation for the plant room at the rear of the site is outside the area of archaeological potential. No excavation is required for the slab for the proposed toilets within the area considered to be of archaeological potential.</p>
<b>E.13.10.2 – Subdivision</b>	<p><i>P1. Subdivision must not impact on archaeological resources at Places of Archaeological Potential through demonstrating either of the following:</i></p> <ul style="list-style-type: none"> <li><i>(a) that no archaeological evidence exists on the land;</i></li> <li><i>(b) that there is no significant impact upon archaeological potential.</i></li> </ul>	<p>Not applicable.</p>



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Service Engineer	JAG
Client's Engineer	WT Partnership
Building Services	Pat & Cheryl
Lead Designer	TBD
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 WWW: www.architectsdesignhaus.com.au

- DRAWINGS:**
- DA 00 - SITE PLAN
  - DA 01 - DEMOLITION PLAN
  - DA 03 - SITE EXCAVATION PLAN
  - DA 05 - FLOOR PLANS - GROUND & LEVEL 1
  - DA 06 - EXIST. ROOF & LEVEL 2 PLANS
  - DA 07 - FLOOR PLANS - LEVEL 2 & LEVEL 3 - LEVEL 7 PLANS
  - DA 08 - FLOOR PLANS - LEVEL 8 & ROOF
  - DA 15 - NORTH ELEVATION
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  - DA 25 - PERSPECTIVE WITH THE EX UTAS CONSERVATORIUM OF MUSIC BUILDING - SANDY BAY ROAD
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  - DA 51 - PHOTOMONTAGE 1 OF 3
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  - DA 53 - PHOTOMONTAGE 3 OF 3

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02	Issue for Information	JAG		24/09/2025
03	Issue for Information	JAG		24/09/2025

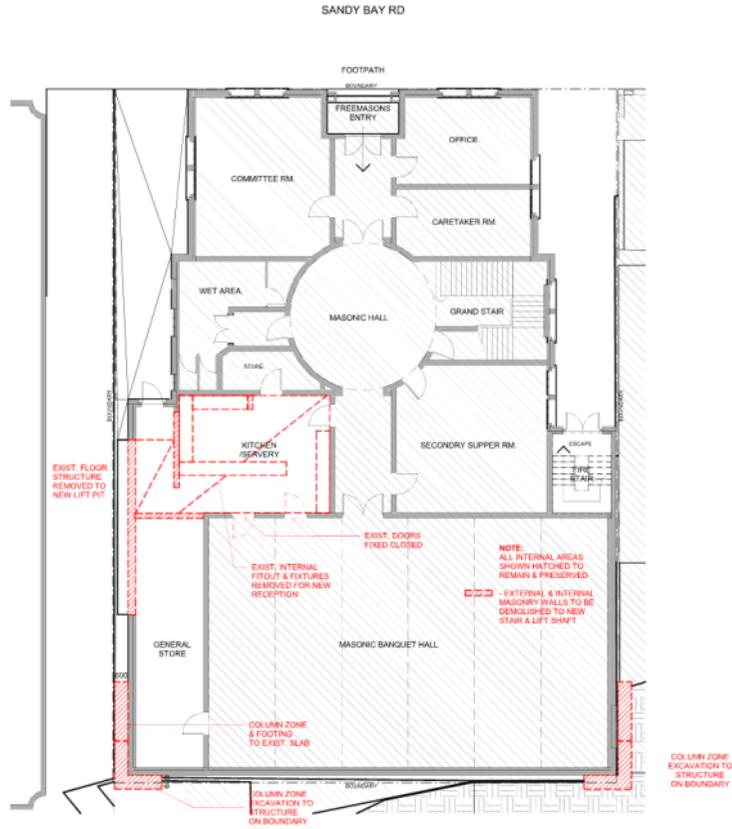
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Cover Page

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1 PLAN - GROUND DEMOLITION  
 1:100



2 PLAN - LEVEL 1 DEMOLITION  
 1:100

Structural Engineer	Gandy & Roberts
Services Engineer	JWG
Quantity Surveyor	WT Partnership
Building Services	PK & Sherry
Landscape Architect	TBD
Other Consultants	



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02	ISSUE FOR PERMIT	19/10/2021	CR	RC
03	ISSUE FOR PERMIT	19/10/2021	CR	RC

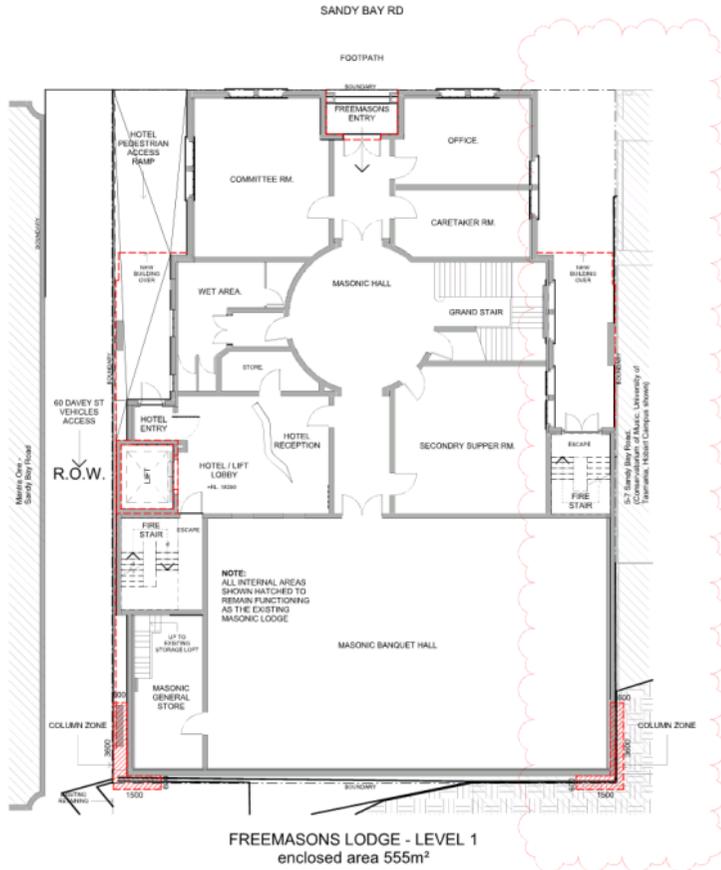
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DEMOLITION

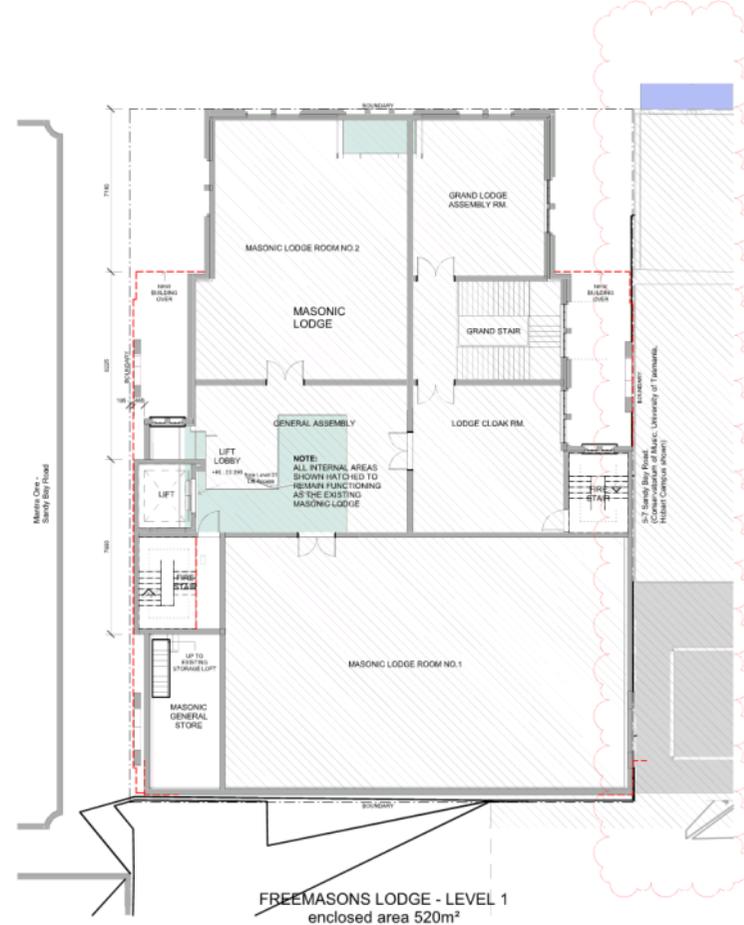
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1802	DA 01	PA1		

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1 PLAN - GROUND FLOOR  
 1 : 100



2 PLAN - LEVEL 1  
 1 : 100

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Service Engineer	JMS
Quality Engineer	WT Partnership
Building Services	PK & Sherry
Lift Engineer	TBO
Other Consultants	



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01	ISSUE FOR PERMIT APPLICATION	19/10/2021	AT	RC
02	ISSUE FOR PERMIT APPLICATION	19/10/2021	AT	RC
03	ISSUE FOR PERMIT APPLICATION	19/10/2021	AT	RC

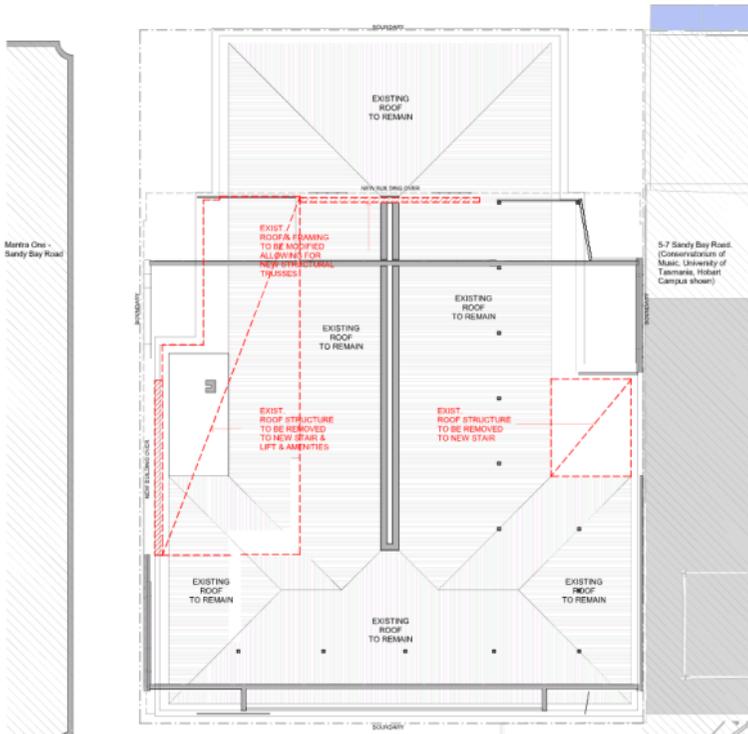
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FLOOR PLANS - GROUND & LEVEL 1

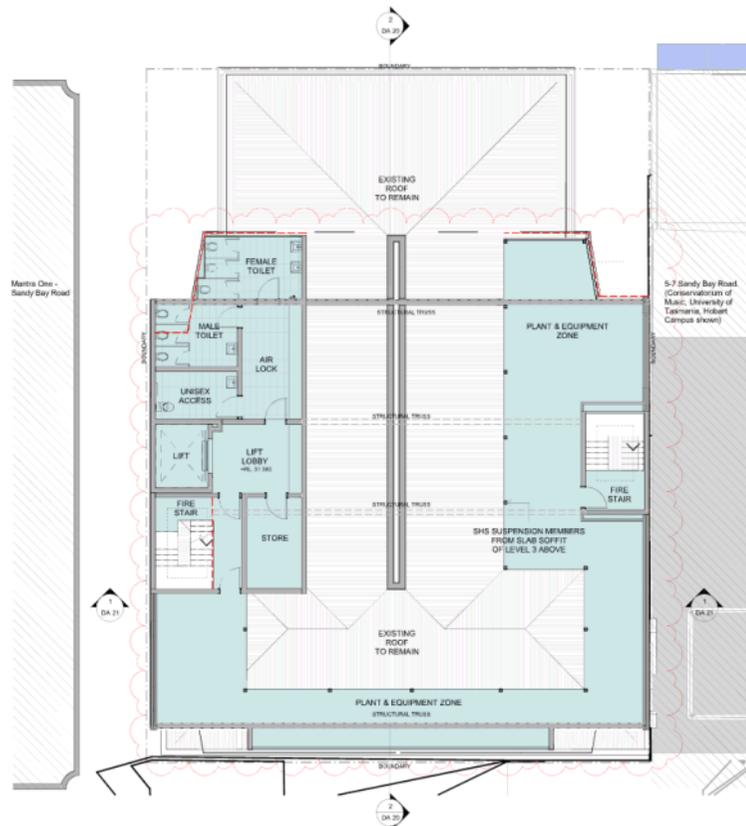
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UDAP	1802	DA 05	PA1		

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**1 PLAN - EXIST. ROOF**  
 1 : 100



**2 PLAN - LEVEL 2**  
 1 : 100

Client/Engineer	Clarity & Roberts
Service Engineer	JMG
Quantity Surveyor	WT Partnership
Building Surveyor	Phil & Sherry
Lead Designer	TBD
Other Consultants	



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01	Issue for Construction	19/10/2021	CR	CR
02	Revised for Construction	19/10/2021	CR	CR

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EXIST. ROOF & LEVEL 2 PLANS

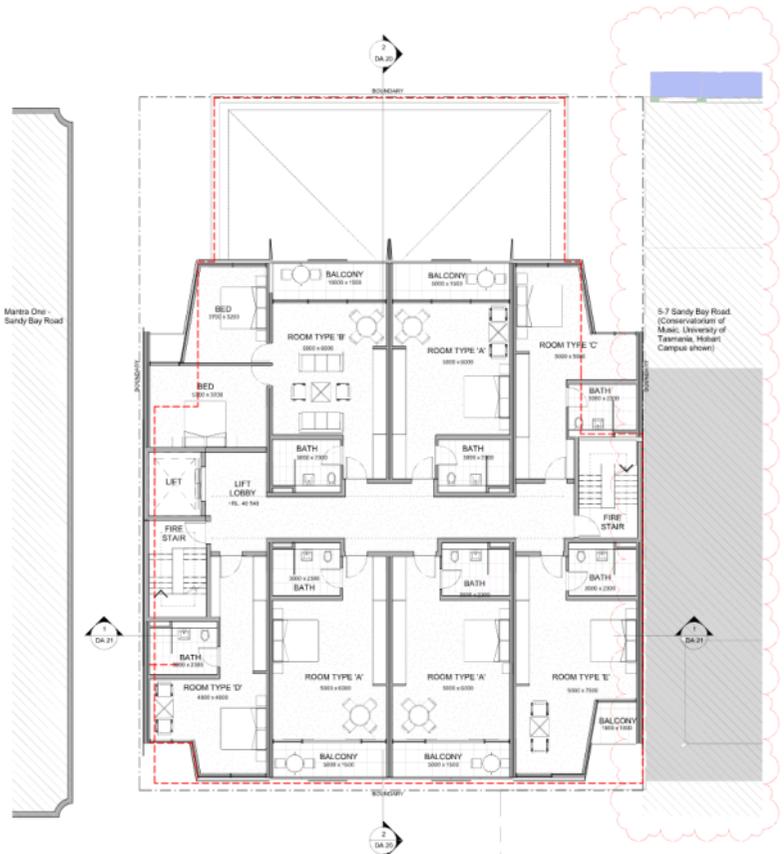
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TYPICAL ACCOMMODATION LEVEL  
 (LEVELS 3 TO 4 INCLUSIVE)  
 enclosed area 425m<sup>2</sup>  
 balconies 46m<sup>2</sup> total area per floor

1 L3-4 Plan - Accommodation  
 1 : 100



TYPICAL ACCOMMODATION LEVEL  
 (LEVELS 5 TO 7 INCLUSIVE)  
 enclosed area 420m<sup>2</sup>  
 balconies 37m<sup>2</sup> total area per floor

2 L5-7 Plan - Accommodation  
 1 : 100

Structural Engineer	Gandy & Roberts
Services Engineer	JMS
Quantity Surveyor	WT Partnership
Building Services	PK & Sherry
Lift Designer	TBD
Other Consultants	



REV	DESCRIPTION	DATE	BY	CHECKED
01	ISSUE FOR PERMIT	19/10/2021	DA	RC
02	ISSUE FOR PERMIT	19/10/2021	DA	RC
03	ISSUE FOR PERMIT	19/10/2021	DA	RC

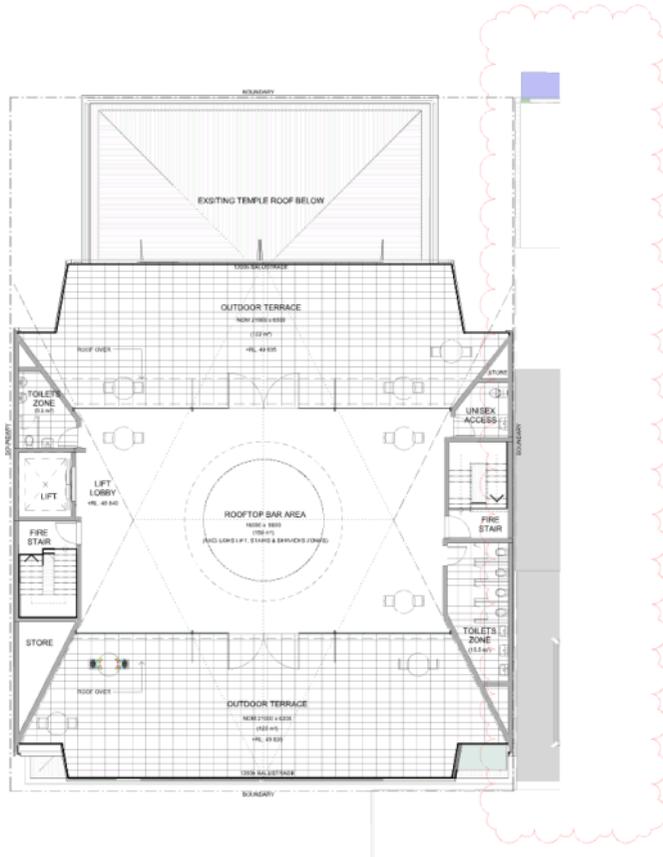
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FLOOR PLANS - LEVEL 2 & L3 - L7 INCL

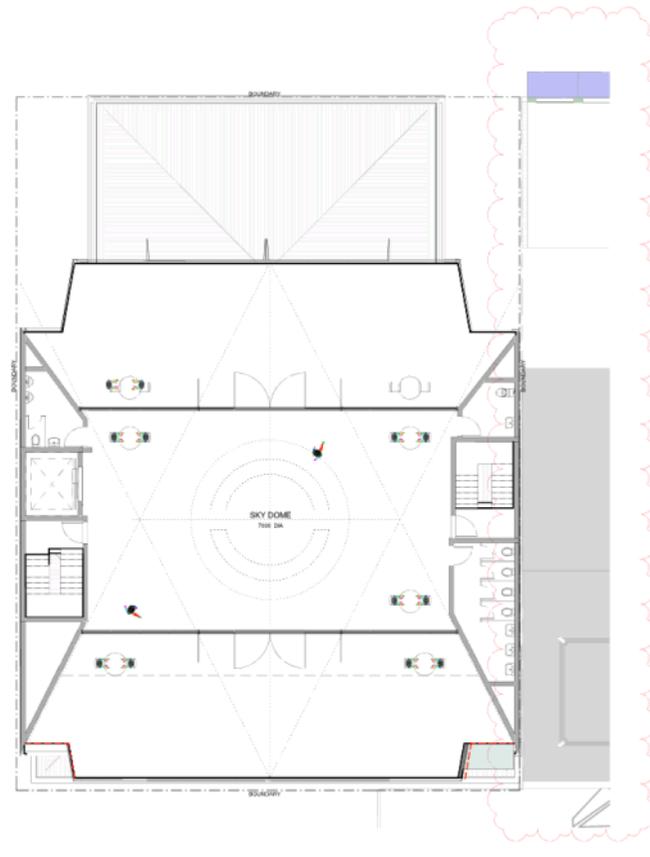
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Author	AT	Checked	CR	RC
DA	1802	DA	07	PA1

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**1 FLOOR PLAN - LEVEL 8**  
 1 : 100



**2 ROOF - LEVEL 8**  
 1 : 100

Structural Engineer	Gearty & Roberts
Services Engineer	JMS
Quantity Surveyor	WT Partnership
Building Surveyor	PK & Sherry
Landscape Architect	TBD
Other Consultants	

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01	ISSUE FOR PERMIT APPLICATION	19/10/2021	AT	RC
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PLANS - LEVEL 8 & ROOF

UDAP Assessment				
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1 : 100	A1	19/10/2021	CR	RC
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1802	DA 08	19/10/2021	CR	PA1

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3 North  
 1 : 100

Notes:  
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 2. THE DESIGNER HAS CONDUCTED VISUAL IMPACT ASSESSMENT AND HAS IDENTIFIED VISUAL IMPACT MITIGATION MEASURES.  
 3. THE DESIGNER HAS CONDUCTED VISUAL IMPACT ASSESSMENT AND HAS IDENTIFIED VISUAL IMPACT MITIGATION MEASURES.  
 4. THE DESIGNER HAS CONDUCTED VISUAL IMPACT ASSESSMENT AND HAS IDENTIFIED VISUAL IMPACT MITIGATION MEASURES.

Structural Engineer	Gandy & Roberts
Services Engineer	JMS
Quantity Surveyor	WT Partnership
Building Surveyor	Pie & Shery
Land Surveyor	TBD
Other Consultants	



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- LEGEND - EXTERNAL FINISHES
- MC-01 METAL ZINC CLADDING PROFILE
  - MR-01 METAL ZINC ROOFING PROFILE
  - MS-01 METAL MESH SCREENING WITH PERFORATED PANELS
  - GR-01 GLAZING QUALITY FRAMEWORK WITH STEEL BRACKET & FINISH & HANDRAIL CLEAR
  - GA-01 GLAZING WITH ALUMINUM FRAMES, CLEAN WITH POLYURETHANE FINISH
  - BR-EX EXISTING EXTERNAL WALL BRICKWORK
  - SR-EX EXISTING STEEL ROOF PROFILE, CORRUGATED IRON FINISH
  - GL-EX EXISTING METAL FRAMED GLAZING

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01	ISSUED FOR PERMIT	19/10/2021	DA	RC

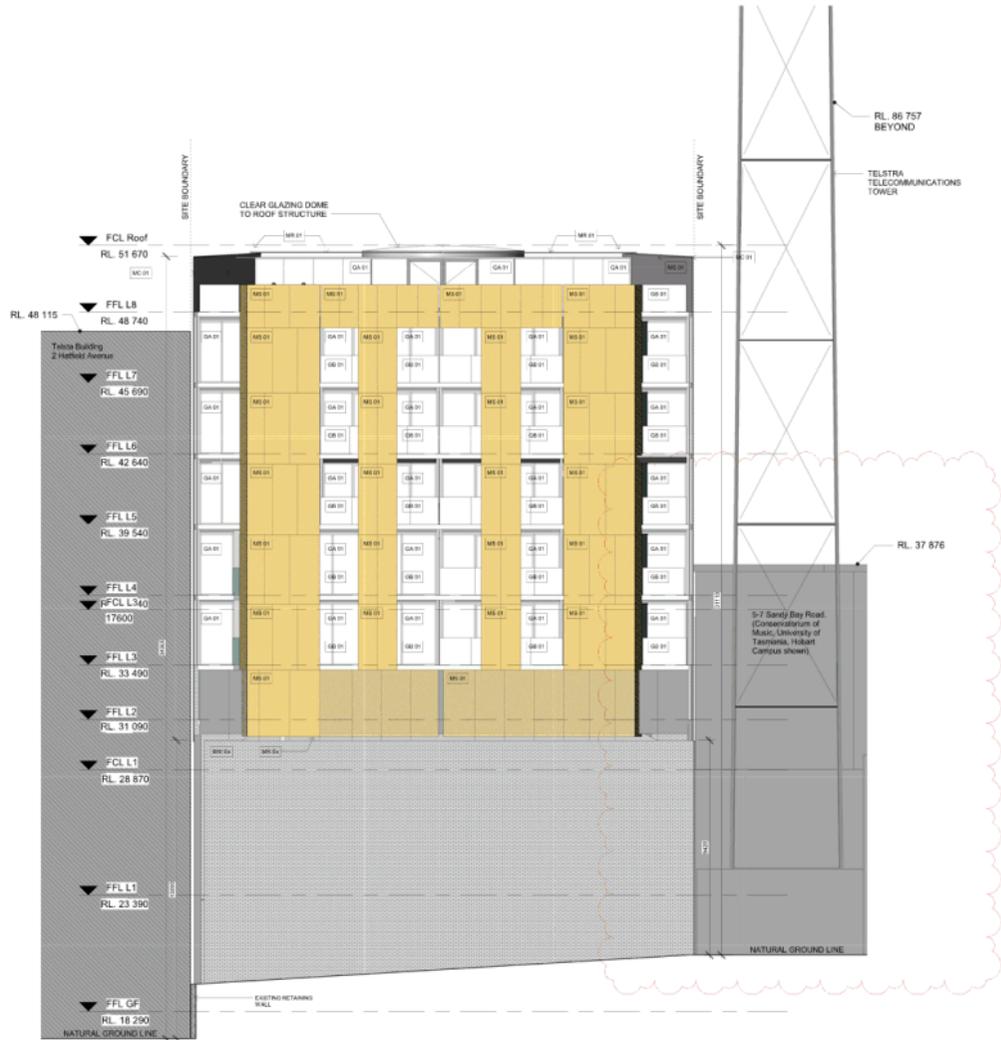
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NORTH ELEVATION

UDAP Assessment					
Scale	Page No	Date	Status	Checked	
1 : 100	A1	19/10/2021	CR	RC	
DA	DA 15	PA1			

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1 South  
 1 : 100

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 3. THE DESIGNER HAS CONDUCTED VISUAL IMPACT ASSESSMENT AND HAS CONSIDERED THE VISUAL IMPACT OF THE PROPOSED DEVELOPMENT.  
 4. THE DESIGNER HAS CONDUCTED VISUAL IMPACT ASSESSMENT AND HAS CONSIDERED THE VISUAL IMPACT OF THE PROPOSED DEVELOPMENT.

Structural Engineer	Gandy & Roberts
Services Engineer	JMG
Quantity Surveyor	WT Partnership
Building Surveyor	PAE & Sherry
Land Surveyor	TBD
Other Consultants	



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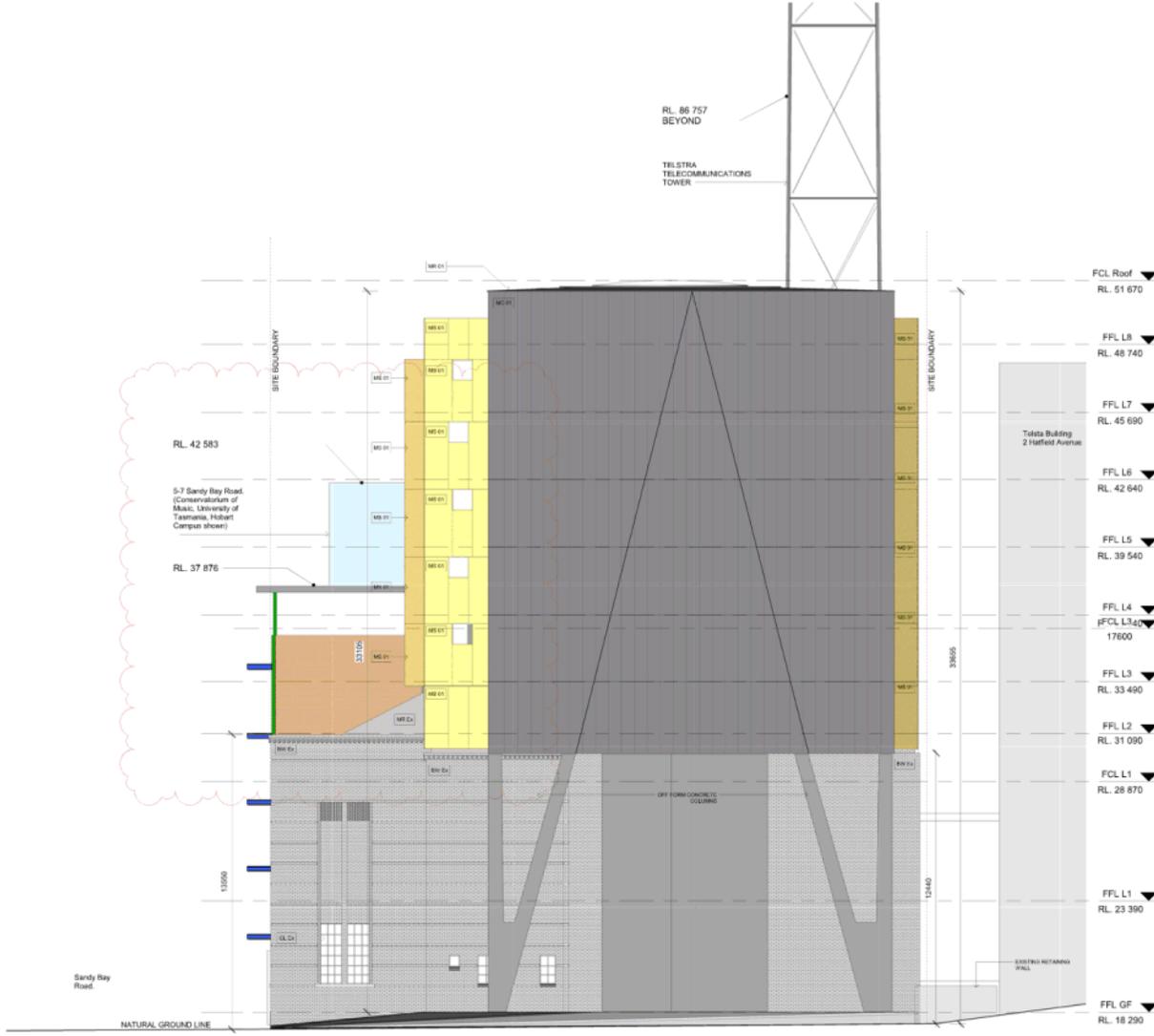
- LEGEND - EXTERNAL FINISHES**
- MS-C-01 METAL ZINC CLADDING PROFILE
  - MS-R-01 METAL ZINC ROOFING PROFILE
  - MS-S-01 METAL MESH SCREENING WITH FINISH - HORIZONTAL RIBS ON 100MM
  - MS-T-01 GLAZED GLAZING FRAMES WITH FINISH - BRONZE FINISH & HORIZONTAL RIBS
  - GA-01 GLAZING WITH ALUMINIUM FRAMES, CLEAR WITH FINE PERFORATE FINISH
  - MS-EX-01 EXISTING EXTERNAL WALL BRICKWORK
  - MS-EX-02 EXISTING STEEL ROOF PROFILE, CORRUGATED 45 DEG PAINT FINISH
  - MS-EX-03 EXISTING METAL FRAMED GLAZING

REV	DESCRIPTION	DATE
01	ISSUED FOR PERMIT	19/10/2021
02	REVISED PERMIT CONDITIONS	01/11/2021
03	REVISED PERMIT CONDITIONS	01/11/2021
04	REVISED PERMIT CONDITIONS	01/11/2021
05	REVISED PERMIT CONDITIONS	01/11/2021

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**SOUTH ELEVATION**

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1 West  
 1 : 100

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 3. THE DESIGNER HAS CONDUCTED VISUAL IMPACT ASSESSMENT AND HAS CONSIDERED THE VISUAL IMPACT OF THE PROPOSED DEVELOPMENT.  
 4. THE DESIGNER HAS CONDUCTED VISUAL IMPACT ASSESSMENT AND HAS CONSIDERED THE VISUAL IMPACT OF THE PROPOSED DEVELOPMENT.

Structural Engineer	Gearty & Roberts
Services Engineer	JMS
Quantity Surveyor	WT Partnership
Building Services	PAE & Sherry
Lighting Engineer	TBD
Other Consultants	



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- LEGEND - EXTERNAL FINISHES
- MC-01 METAL ZINC CLADDING PROFILE
  - MR-01 METAL ZINC ROOFING PROFILE
  - MS-01 METAL MESH SCREENING WITH POLYURETHANE INSULATION
  - CS-01 CLADDING QUALITY FINISHING WITH STEEL BRACKET FINISH & HANDRAILS & CLEARS
  - GA-01 CLADDING WITH ALUMINIUM FINISHES, CLEAR WITH POLYURETHANE FINISH
  - EW-EX EXISTING EXTERNAL WALL BRICKWORK
  - SR-EX EXISTING STEEL ROOF PROFILE, CORRUGATED 45 DEG PAINT FINISH
  - GL-EX EXISTING METAL FRAMED GLAZING

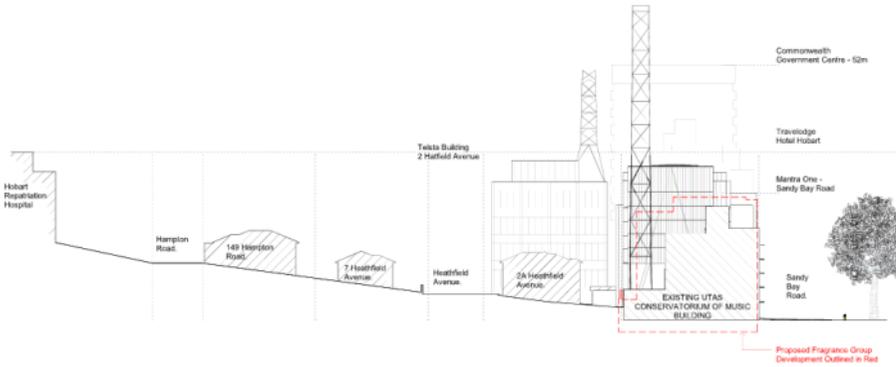
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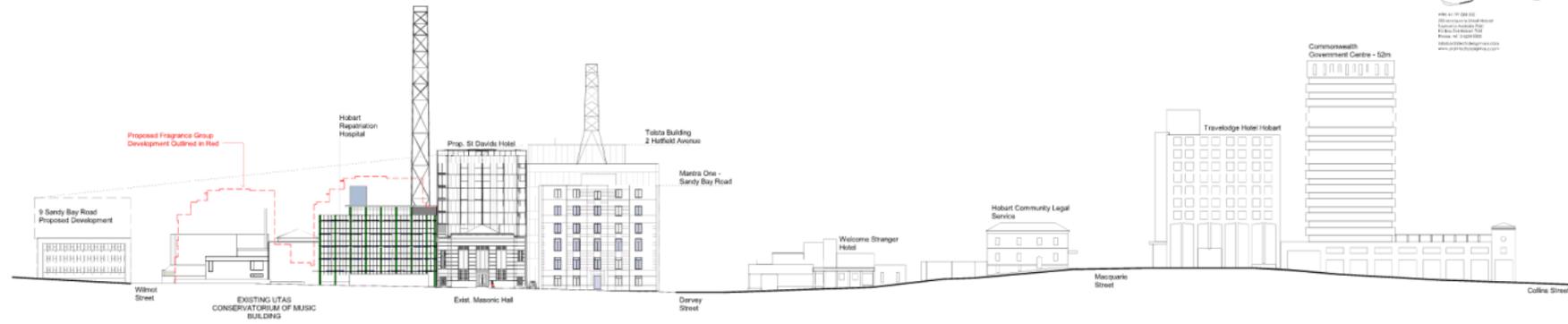
WEST ELEVATION

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1802	DA 18	PA1			

DATE PLOTTED: 10/10/2021 10:00 AM



2 Site Section (EAST BOUNDARY)  
 1 : 500



1 NorthStreetscape  
 1 : 500

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Client/Engineer	Gandy & Roberts
Service Engineer	JWG
Quantity Surveyor	WT Partnership
Building Services	PAE & Sherry
Lead Designer	TBD
Other Consultants	



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1	Issue for Planning Authority	19/10/2021
2	Issue for Planning Authority	19/10/2021
3	Issue for Planning Authority	19/10/2021

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STREET ELEVATIONS

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1802	DA 19	PA1		



2 Section Longitudinal  
 1 : 100

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Client/Engineer	Gandy & Roberts
Service Engineer	JMG
Quality Engineer	WT Partnership
Building Services	Phil & Sherry
Lead Designer	TBD
Other Consultant	



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- LEGEND - EXTERNAL FINISHES
- MS-C-01 METAL ZINC CLADDING PROFILE
  - MS-R-01 METAL ZINC ROOFING PROFILE
  - MS-S-01 METAL MESH SCREENING WITH FINISH - HORIZONTAL SECTION OF FRAME
  - MS-U-01 GLASS BALUSTRADES WITH STEEL BRACKET FILING & HANDRAILS CLEAR
  - GA-S-01 GLAZING WITH ALUMINIUM FRAMES, CLEAR WITH FINGERGLAZE FINISH
  - MS-E-01 EXISTING EXTERNAL WALL BRICKWORK
  - MS-E-02 EXISTING STEEL ROOF PROFILE, CORRUGATED 45 DEG PAINT FINISH
  - GL-E-01 EXISTING METAL FRAMED GLAZING

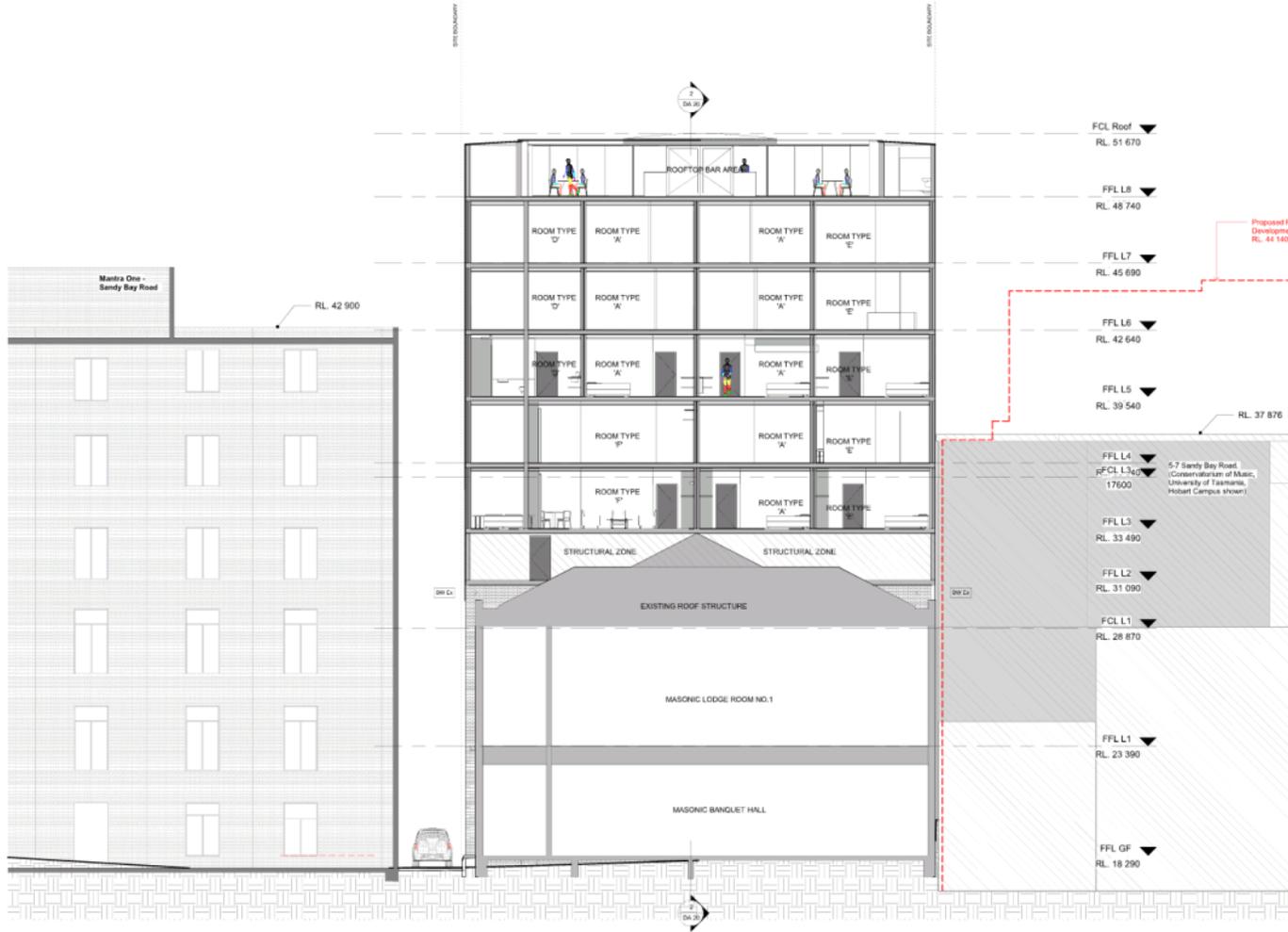
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02	REVISION TO PERMIT	19/10/2021	JMG	TBD
03	REVISION TO PERMIT	19/10/2021	JMG	TBD
04	REVISION TO PERMIT	19/10/2021	JMG	TBD
05	REVISION TO PERMIT	19/10/2021	JMG	TBD

ST. DAVID'S HOTEL  
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SECTION

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1 : 100	A1	19/10/2021	CR	RC	
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Structural Engineer	Gandy & Roberts
Services Engineer	JMG
Quantity Surveyor	WT Partnership
Building Surveyor	PAE & Shery
Land Surveyor	TBO
Other Consultants	



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**ST. DAVID'S HOTEL**  
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CROSS SECTION

UDAP Assessment					
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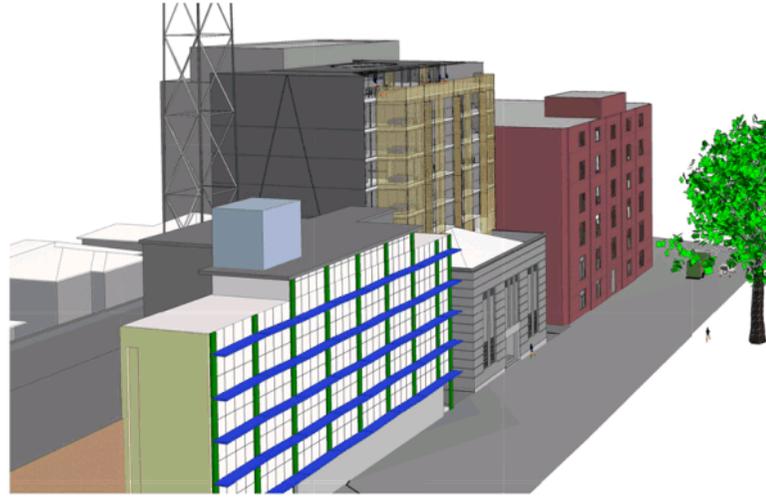
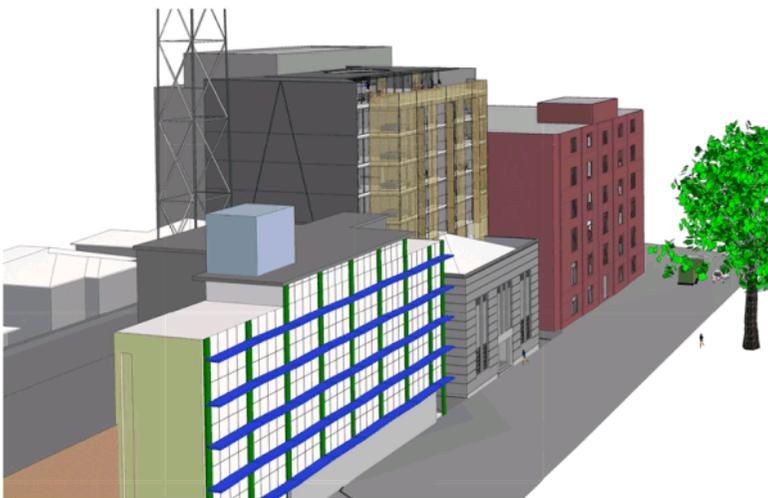
1 3D Sandy Bay Rd - City

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 DATE: 19/10/2021  
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Client/Developer	Gearty & Roberts
Service Engineer	JMG
Quantity Surveyor	WT Partnership
Building Surveyor	PK & Sherry
Landscape Architect	TBD
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PERSPECTIVES WITH THE EXLUTAS  
 CONSERVATORIUM OF MUSIC BUILDING -  
 SANDY BAY RD

UDAP Assessment				
Case No	Plan No	Date	Scale	Class
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1802		DA 25	PA1	



2 3D Davey - Sandy Bay Rd.



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Other Consultants	



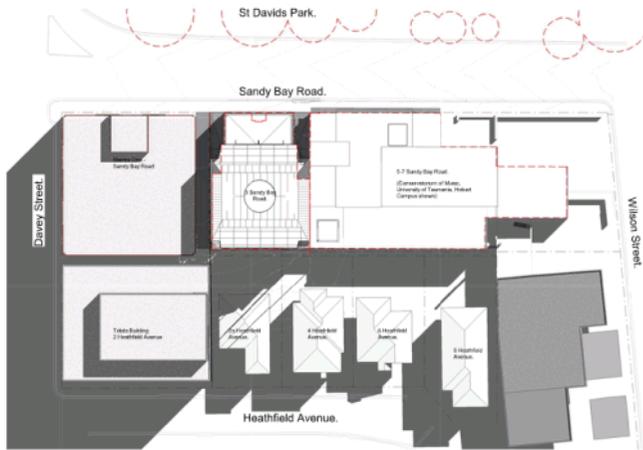
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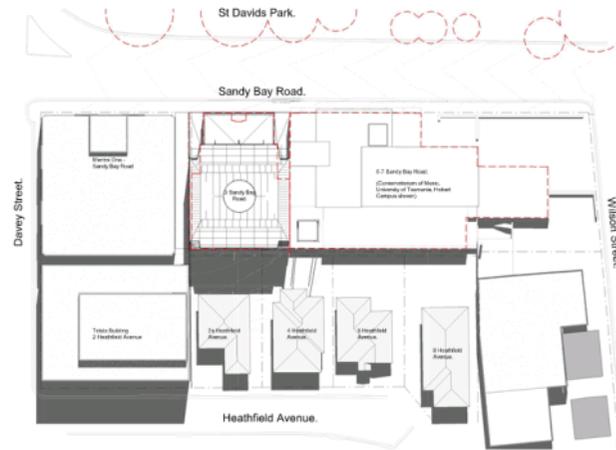
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PERSPECTIVES WITH THE EX.LUTAS  
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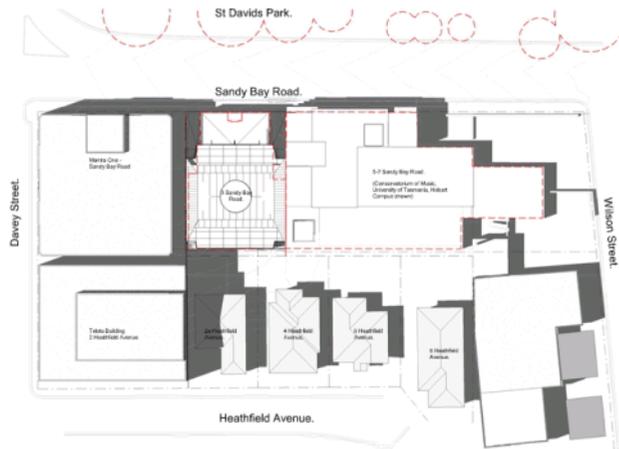
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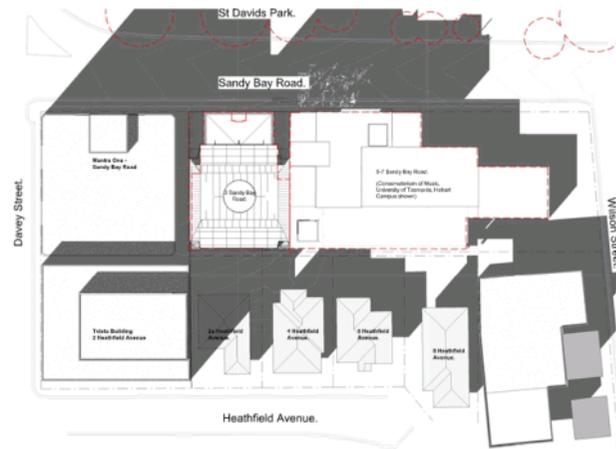
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4 22 Dec @ 17.00  
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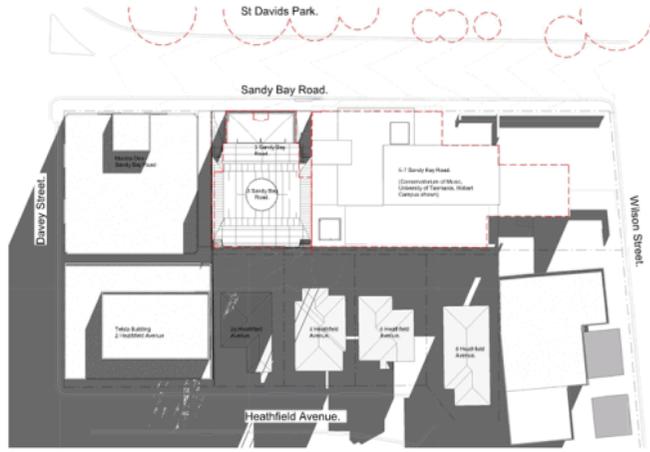
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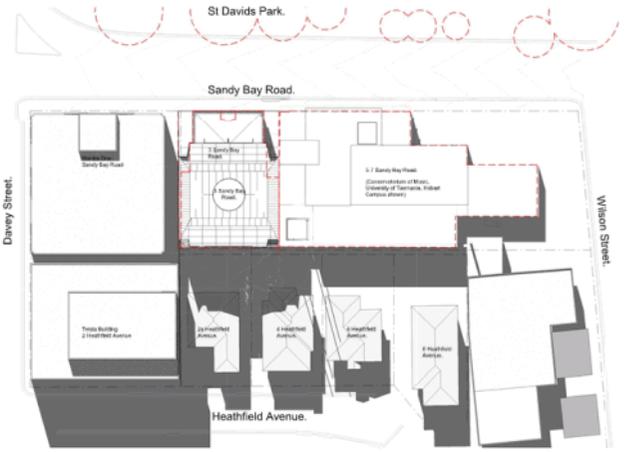
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 STUDY DECEMBER 22ND

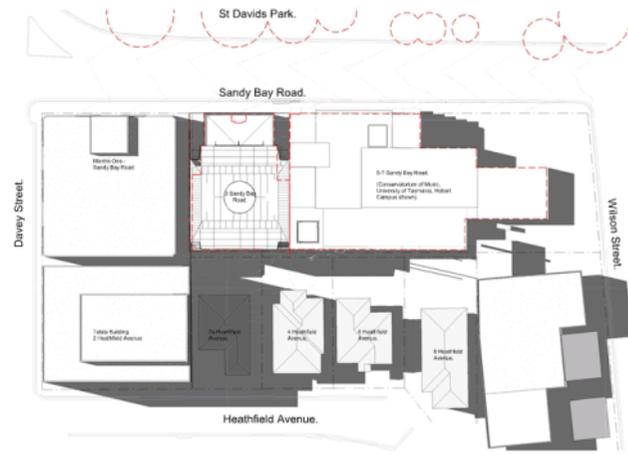
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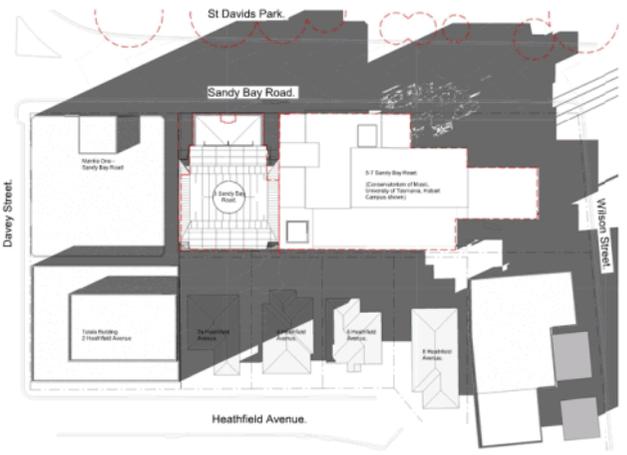
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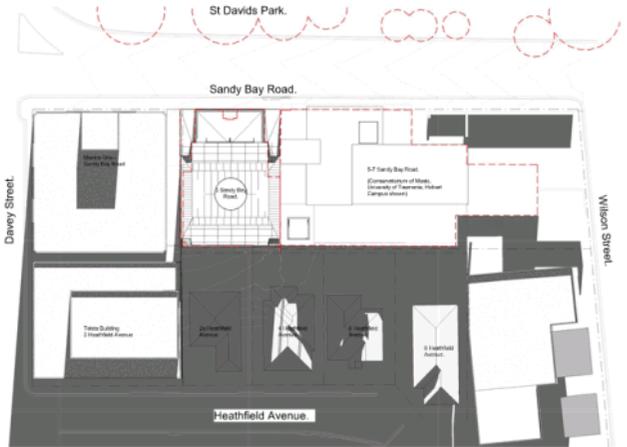
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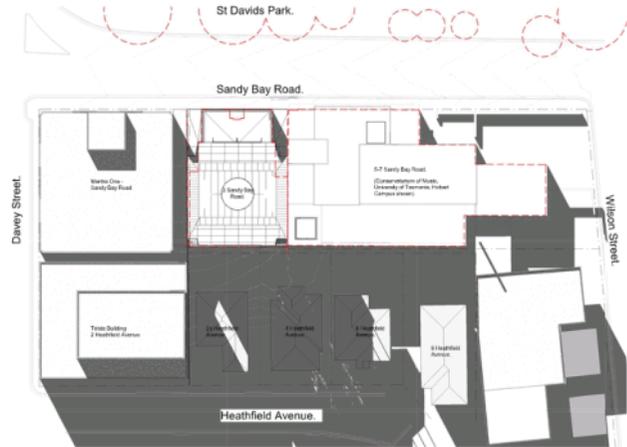
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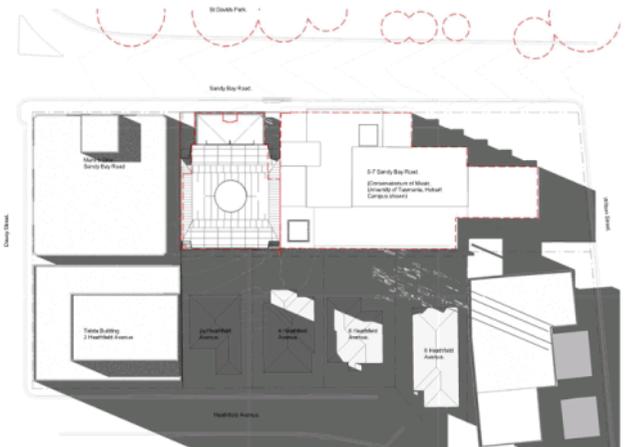
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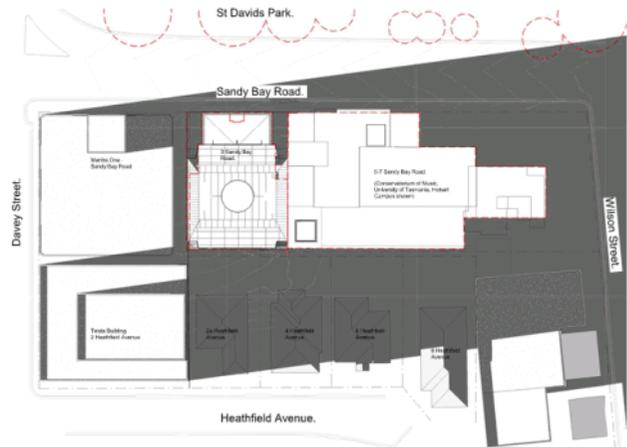
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Services Engineer	JMS
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Land Surveyor	TBD
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02	Issue for Planning Authority	19/10/2021
03	Issue for Planning Authority	19/10/2021
04	Issue for Planning Authority	19/10/2021
05	Issue for Planning Authority	19/10/2021

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PROPOSED CONDITIONS - SHADOW  
 STUDY JUNE 20TH

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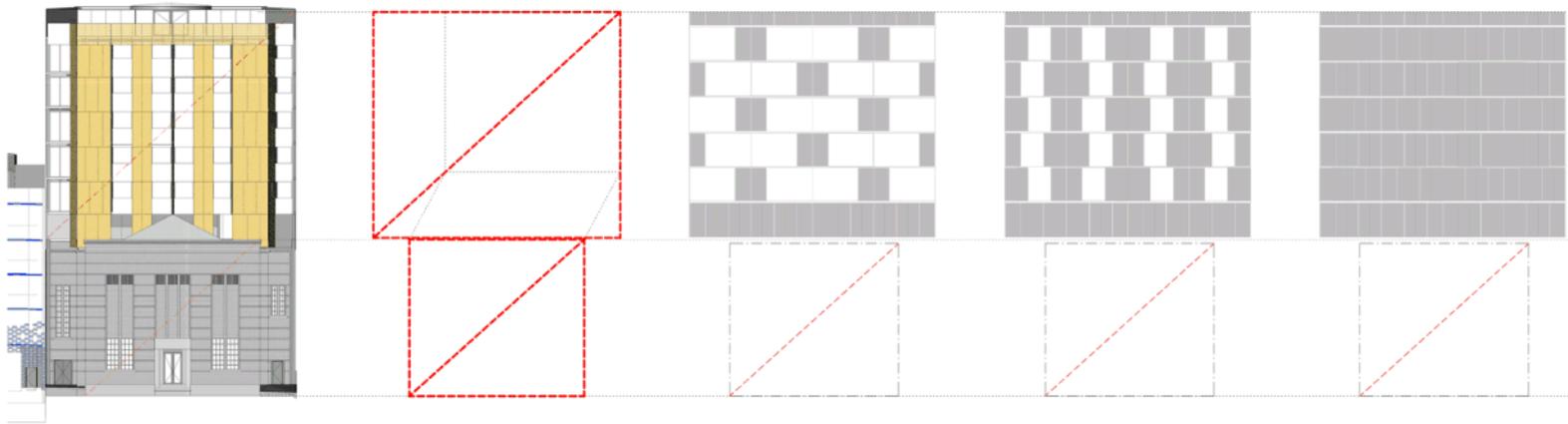


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 Proportional Analysis

Fig. 2  
 Facade Opening 100%

Fig. 3  
 Facade Opening 50%

Fig. 4  
 Facade Opening 0%

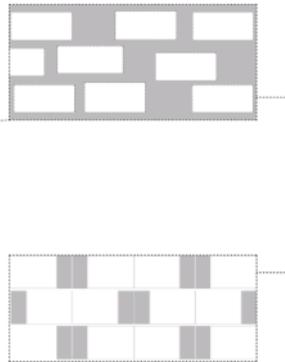


Fig. 5  
 Abstraction to Urban Hillside

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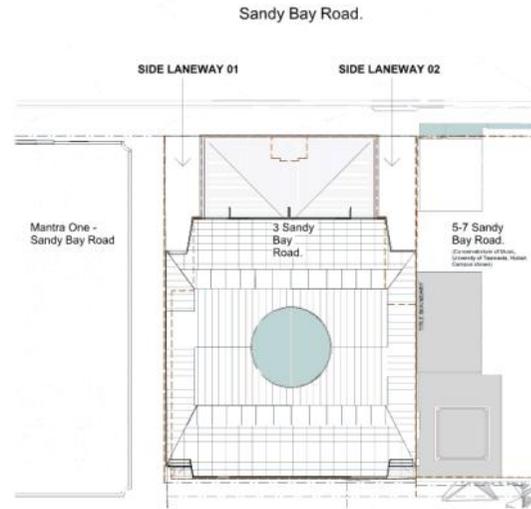
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**EXTERNAL MATERIAL PALETTE**

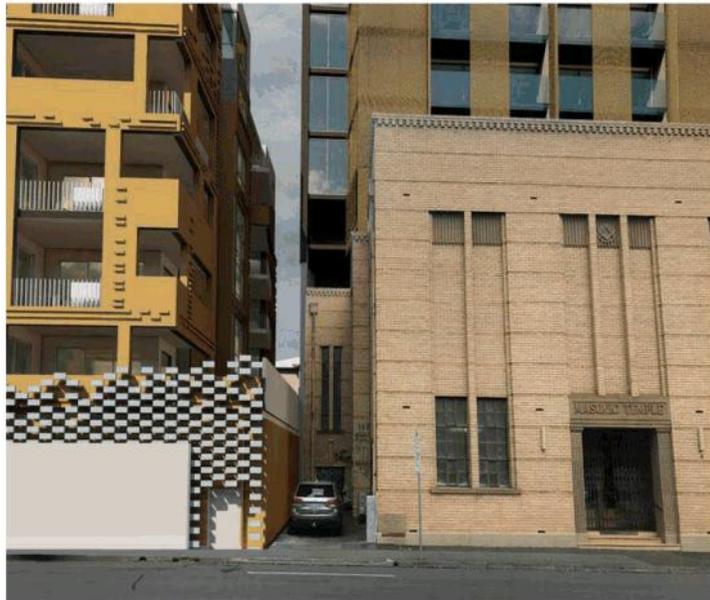
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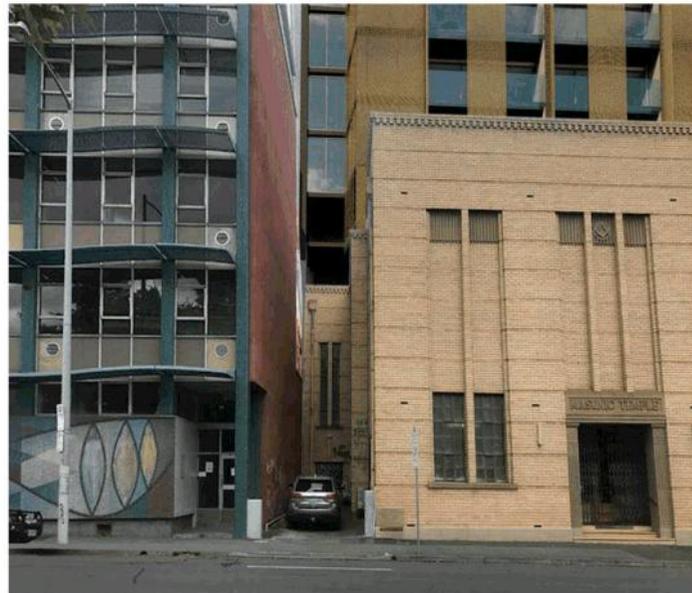
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SIDE LANEWAY - DIAGRAM  
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SIDE LANEWAY 02 - PROPOSED CONDITIONS  
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- Building Surveyor
- Land Surveyor
- Other Consultants

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**ST. DAVID'S HOTEL**  
 3 SANDY BAY RD HOBART  
 THE TASMANIAN FREEMASONS HALL  
 COMPANY LIMITED - OWNER

COLUMNS IN R.O.W

Scale	Sheet No.	Date	Drawn	Checked
1:800	AT			Checker
1802	DA 56			







**VIEW FROM ST. DAVID'S PARK**  
 PROPOSED CONDITIONS



**VIEW FROM SANDY BAY RD**  
 PROPOSED CONDITIONS

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Client/Engineer	Gearty & Roberts
Service Engineer	JMG
Client's Contact	WT Partnership
Building Services	Phil & Cheryl
Lead Designer	TBD
Other Contacts	



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DATE: 02/02/2022

**ST. DAVID'S HOTEL**  
 3 SANDY BAY RD HOBART  
 THE TASMANIAN FREEMASONS HALL  
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PHOTOMONTAGE 2 OF 2

UDAP Assessment				
Case No	Plan No	Date	Issue	Checked
1802	A1	02/02/2022	AH	RC
DA 52	PA1			

ENGINEER: ROBERT HAY



**VIEW FROM HEATHFIELD AVENUE**  
 PROPOSED CONDITIONS



**VIEW FROM SANDY BAY RD (SOUTH)**  
 PROPOSED CONDITIONS

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Client/Engineer	Gearty & Roberts
Service Engineer	JMS
Quality Control	WT Partnership
Building Services	Pat & Gerry
Light Services	TBD
Other Conditions	

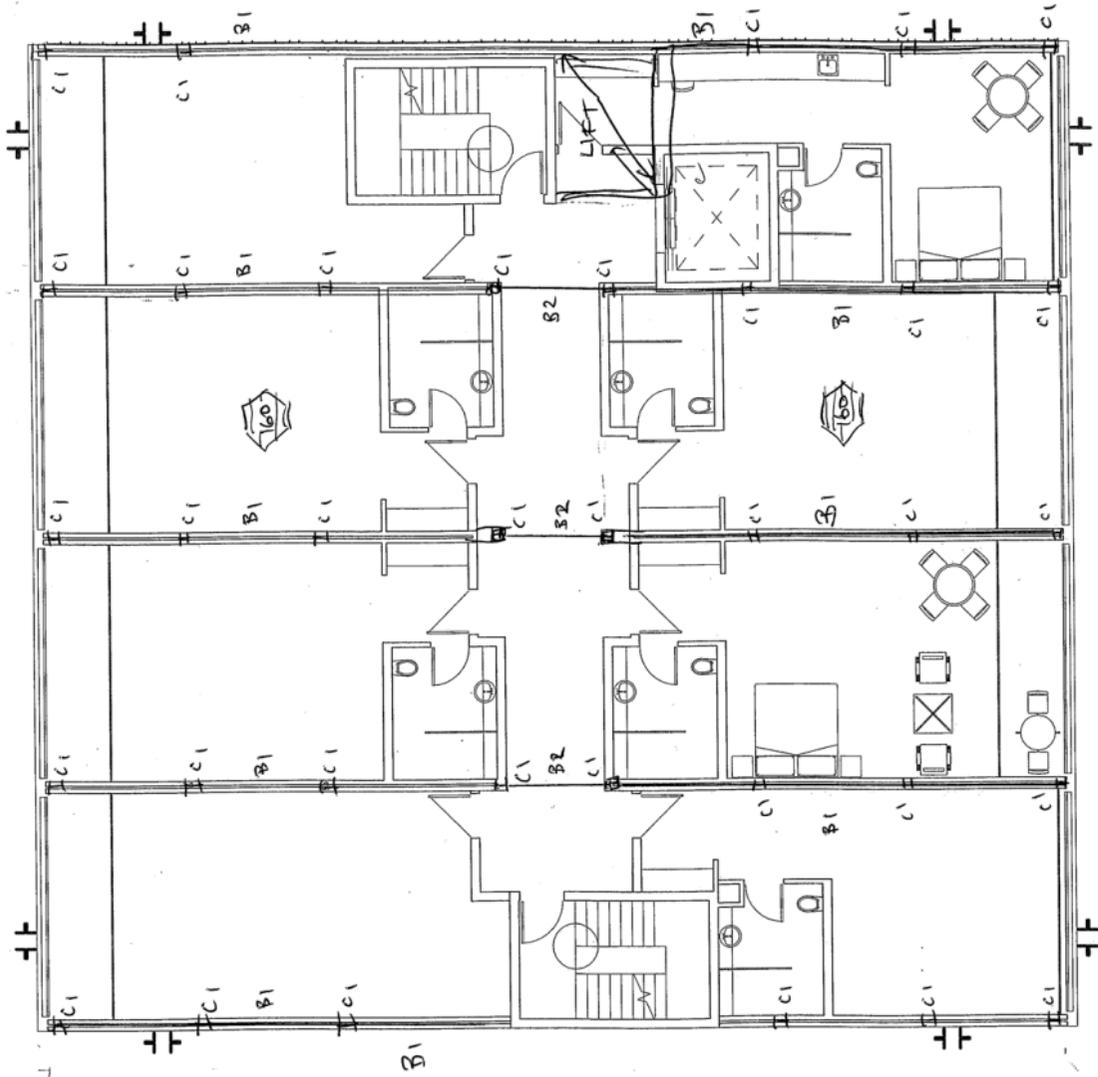


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PHOTOMONTAGE 3 OF 3

UDAP Assessment				
Case No	Plan No	Date	Area	Class
1802	A1	02/02/2022	AH	RC
DA	DA 53	PA1		

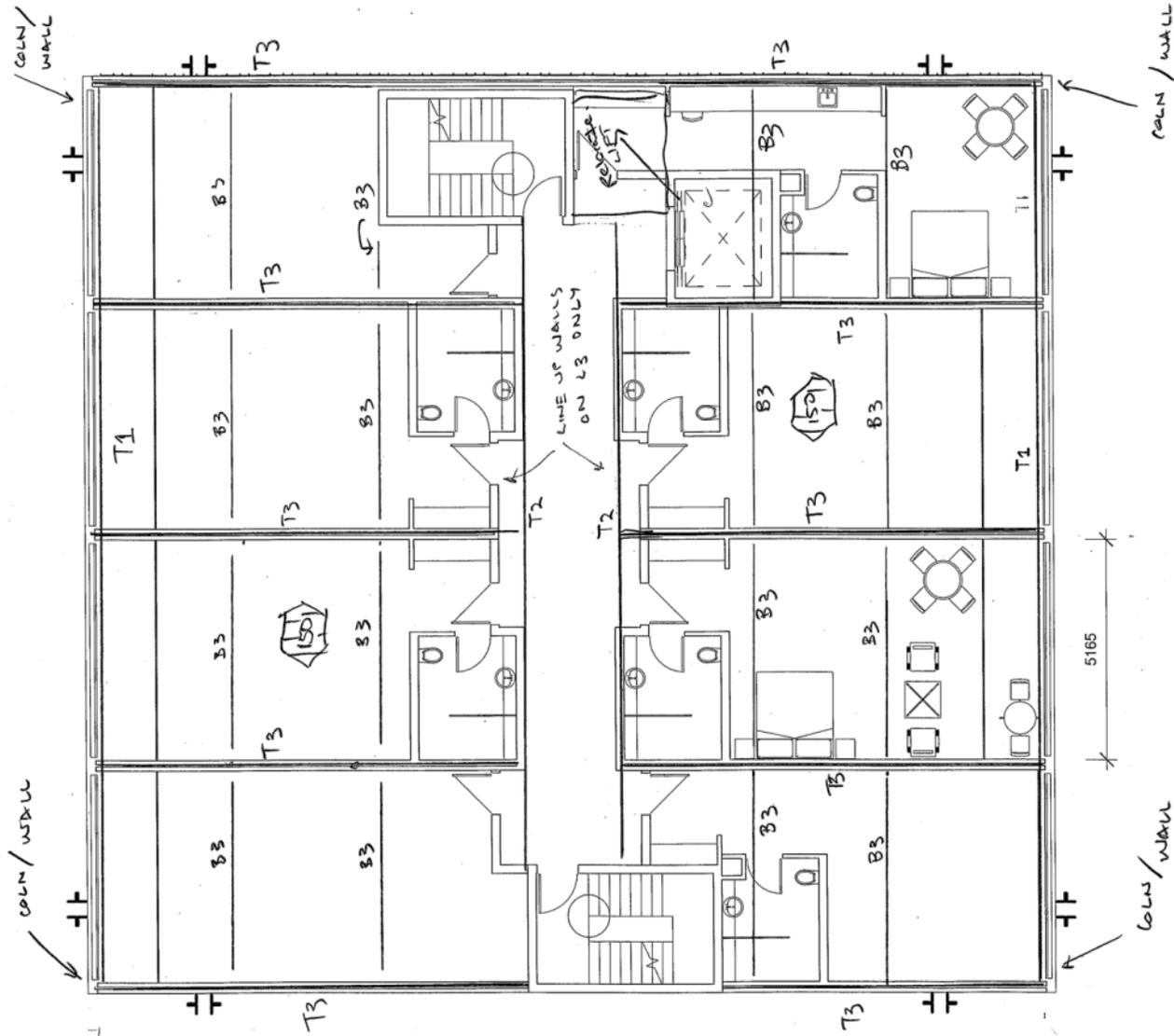


TYPICAL LEVEL



160 KINGFISH SLABS  
KF70

- C1 150 UC 2
- B1 150 UB/UC
- B2 200x100 RNS ON FLAT



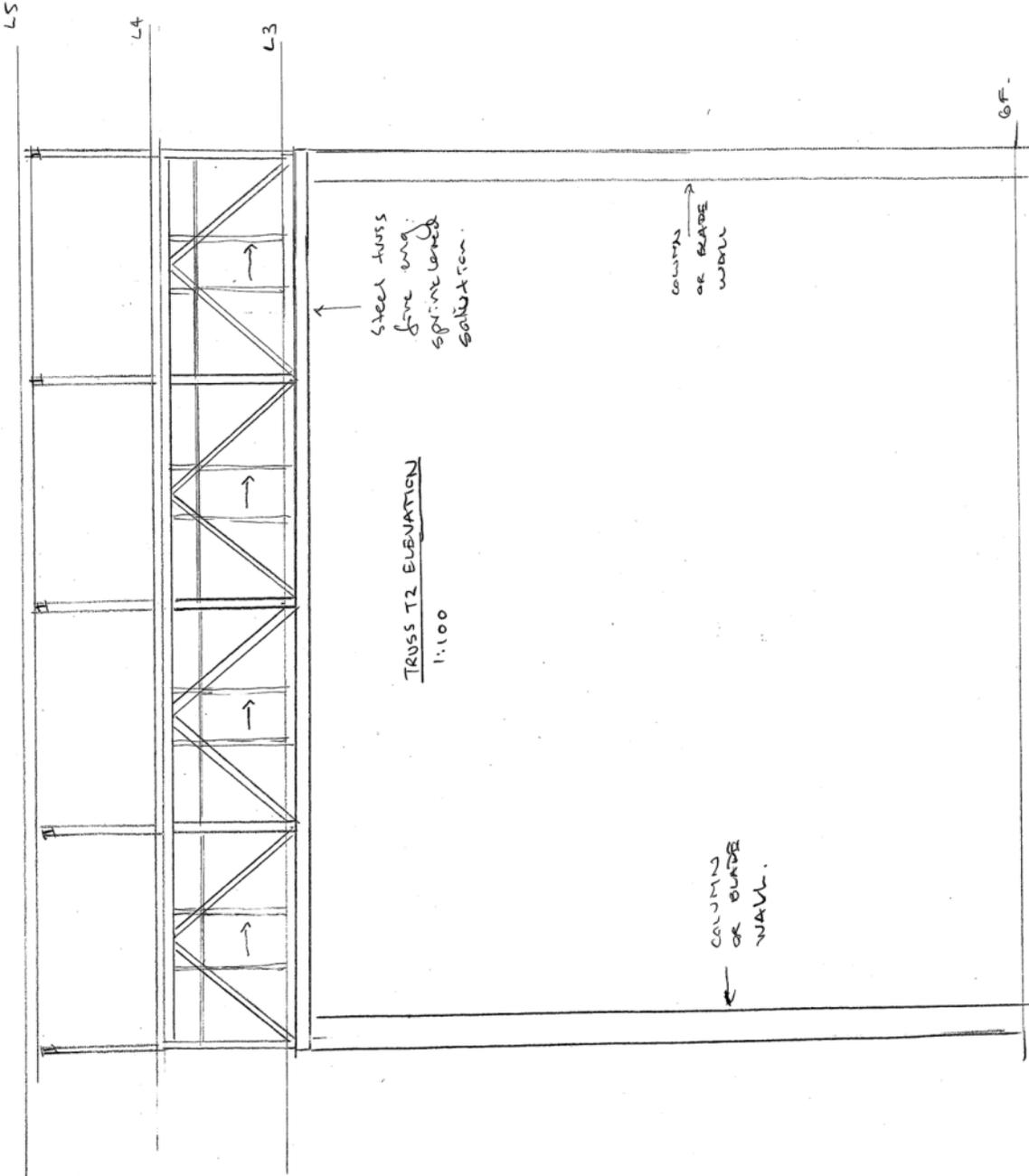
LEVEL 3

T1 / T2 ± 310 UC CHORDS + DIAGONALS

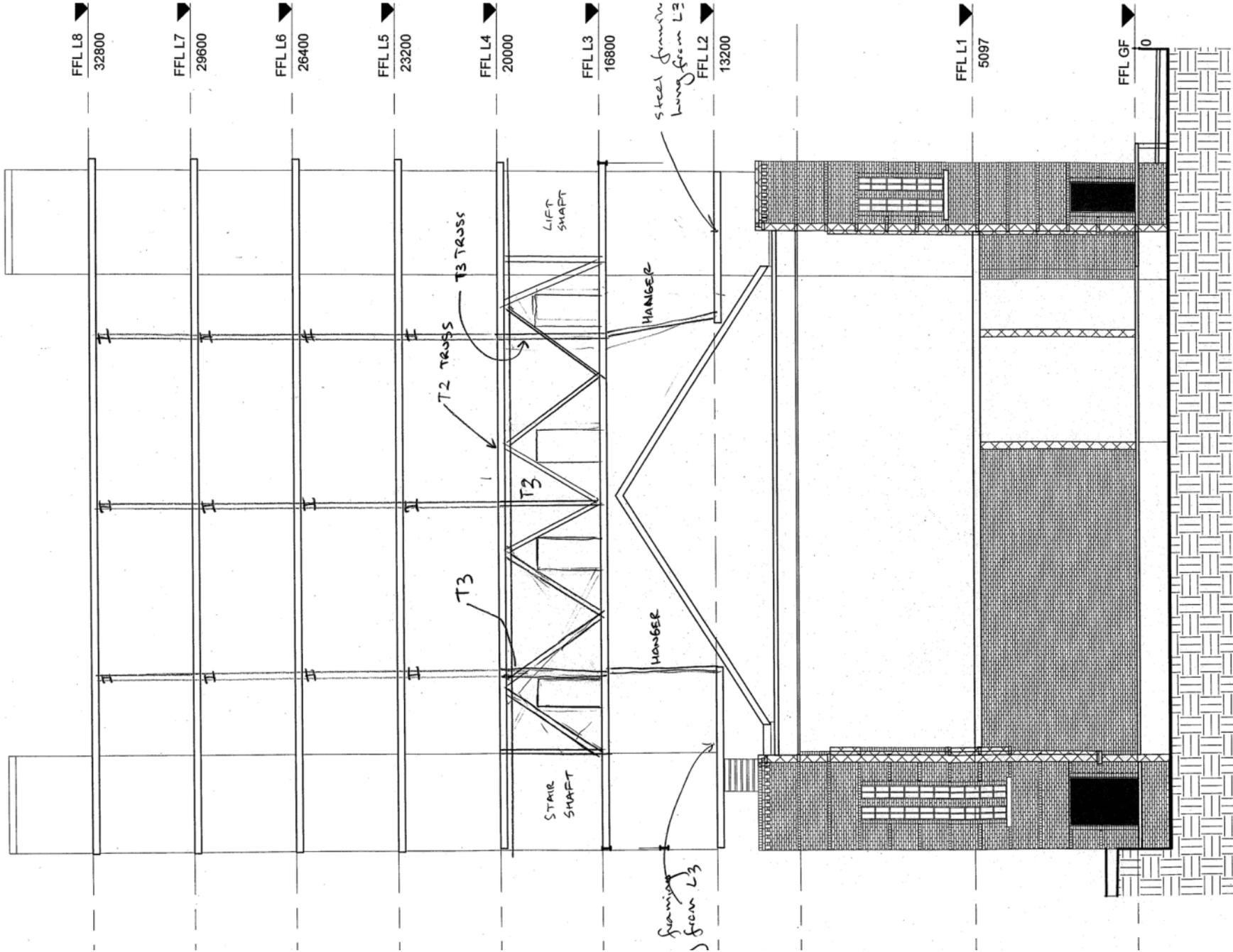
T3 200 UC CHORDS + DIAGONALS

B3 : 200 UB

 150 KINGFLY SLAB 10FT0







SECTION 3



2 September 2025

Phil Gartrell  
Senior Planner  
Ireneinc Planning & Urban Design  
49 Tasma Street  
North Hobart TAS 7001

Dear Phil

**Re: 3 Sandy Bay Road, Hobart – Traffic Engineering Assessment**  
**Project No: 24554**

I refer to your request for SALT to undertake an updated traffic engineering assessment in relation to the proposed hotel rooms/serviced apartments (visitor accommodation) development at 3 Sandy Bay Road in Hobart.

SALT has reviewed the development plans, carried out parking occupancy surveys, and provided design advice to ensure a satisfactory arrangement from a traffic engineering perspective. Our assessment is provided as follows.

## 1 EXISTING CONDITIONS

### 1.1 LOCATION & LAND USE

The subject site is in Hobart with frontage onto Sandy Bay Road in the northeast. It is bordered by a hotel in the northwest, the University of Tasmania Conservatorium of Music in the southeast, and mixed uses in the southwest.

The site is rectangular in shape, covers an area of approximately 676 m<sup>2</sup>, and is currently occupied by the Hobart Masonic Hall, which has a floor area of approximately 1,145 m<sup>2</sup> over two (2) storeys. Vehicular access to the site is via two (2) crossovers to Sandy Bay Road, although an on-site carpark has not been provided.

The location of the site with respect to the surrounding road network is shown in **Figure 1**, followed by an aerial view of the site in **Figure 2**

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Level 6, 350 Collins St Melbourne VIC 3000  
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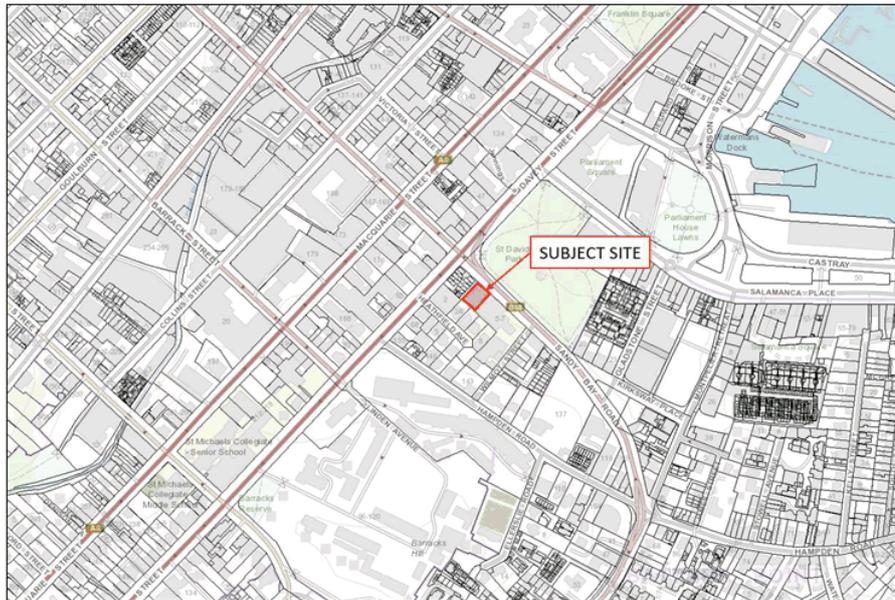


Figure 1 Subject site location (Source: The LIST)



Figure 2 Aerial view of subject site (Source: The LIST)

## 1.2 ZONING & OVERLAYS

Hobart City Council is the responsible authority, and the zoning of the land is 'Urban Mixed Zone'. The site is not subject to any overlays.



### 1.3 ROAD NETWORK

**Sandy Bay Road** is a sealed arterial road that follows a northwest-southeast alignment and is under the care and management of Council. Along the site frontage the carriageway is approximately 13.6 – 13.8m wide and provides three lanes in the north-westbound direction and one lane in the south-eastbound direction, which is due to the layout of the nearby Sandy Bay Road / Davey Street intersection. Further southwest, the carriageway provides two lanes in each direction, with parking permitted on both sides. A ~1.8m wide paved footpath has been provided on the north-eastern side and a ~2.5m wide paved footpath has been provided on the south-western side. The posted speed limit is 50 km/h.

**Wilmot Street** is a sealed local road that follows a northeast-southwest alignment and is under the care and management of Council. The carriageway is approximately 5.9m wide and accommodates one-way traffic in the north-eastbound direction with parking permitted on the north-western side. A ~1.4m wide paved footpath has been provided on the north-western side and a ~1.2m wide paved footpath has been provided on the south-eastern side. There is no posted speed limit and therefore the default speed limit of 50 km/h in urban areas applies.

### 1.4 SUSTAINABLE TRANSPORT

#### 1.4.1 Walkability

The site has very good access to pedestrian facilities with paved footpaths provided on both sides of most of the surrounding roads and the wider area encompassing a well-connected footpath network. Furthermore, pedestrian crossings have been provided at the nearby Sandy Bay Road / Davey Street intersection.

The walkability of the area has been assessed using the 'Walk Score' performance tool ([www.walkscore.com](http://www.walkscore.com)), which is a web-based assessment tool that provides an indication on the walkability of an area. 'Walk Score' provides a numerical score between 0 and 100, with a score close to 100 indicating that various facilities / amenities are easily accessible to the site, including the following categories:

- Drinking & dining;
- Groceries;
- Shopping;
- Errands;
- Parks;
- Schools; and
- Culture & entertainment.

The subject site has been designated as *walker's paradise* with a 'score' of 97, meaning that 'daily errands do not require a car.'

#### 1.4.2 Cycling

A bike lane has recently been implemented on the southern side of Davey Street between Sandy Bay Road and Linden Avenue, which includes a physical separator between the bike lane and adjoining traffic lane. The implementation of the bike lane required the removal of a bus stop as well as on-street car parking spaces along this section of Davey Street.

There are no other bicycle lanes or paths in the vicinity of the site and cycling generally occurs as a shared mode of transport on the roadways and footpaths.

#### 1.4.3 Public Transport

The site has very good access to public transport with bus stops on both sides of Sandy Bay Road in the vicinity of the site. The nearest bus stop is on the western side of Sandy Bay Road (buses TO Hobart Interchange) and is located about 66m (1-minute walk) from the subject site. The bus stop on the eastern side of Sandy Bay Road (buses FROM Hobart Interchange) is about 140m (2-minute walk) from the subject site.

The following Metro Tasmania bus routes operate in the area:

- 401 – Lower Sandy Bay via Sandy Bay, University, Churchill Avenue;
- 402 – Lower Sandy Bay via Sandy Bay Road (evening and weekend services travel via University);
- 422 – Margate via Sandy Bay Road, Taroona, Kingston Central, Huntingfield;



- 426 – Taroona via Sandy Bay Road;
- 427 – Blackmans Bay via Taroona, Kingston Central, Kingston Beach;
- 428 – Blackmans Bay via Taroona, Kingston Central, Maranoa Heights; and
- 429 – Summerleas via Taroona, Kingston Central, Summerleas Road.

1.4.4 E-scooters

Beam Mobility has recently been permitted by Hobart City Council to provide hire-and-ride e-scooter services in the city. According to the Tasmanian road rules, e-scooters may be used on most footpaths, shared paths, bike lanes, and roads with speed limits under 50 km/h.

The site has very good access to e-scooters, with physical parking bays provided at the St Davids Park entrance near the site, as shown in Figure 3.

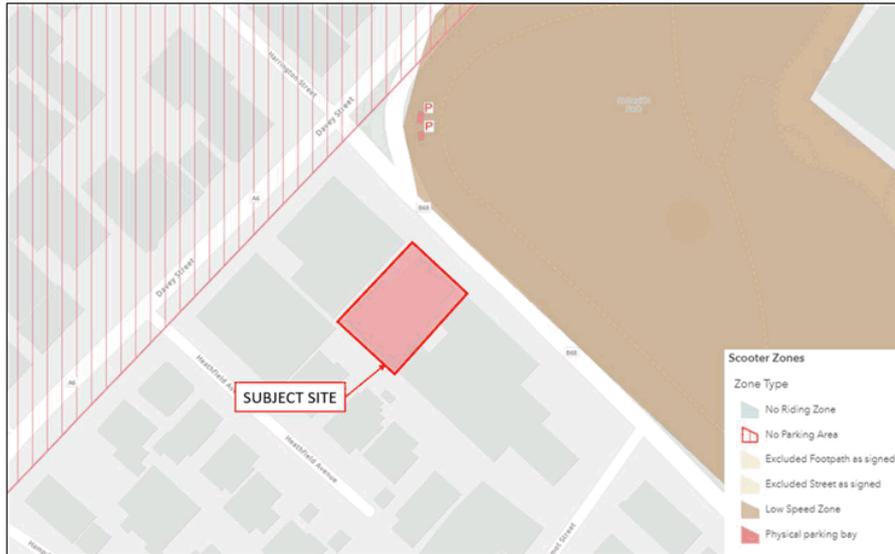


Figure 3 Locality of e-scooter parking bays

1.5 PARKING AVAILABILITY

SALT has undertaken an assessment of parking availability on the road network within a 200-300m radius of the subject site. Spot surveys were carried out during typical weekday mid-day, PM, and Saturday mid-day peak periods to determine the on-street parking demand.

The parking survey area is shown in Figure 4. Parking restrictions details are summarised in Table 1.



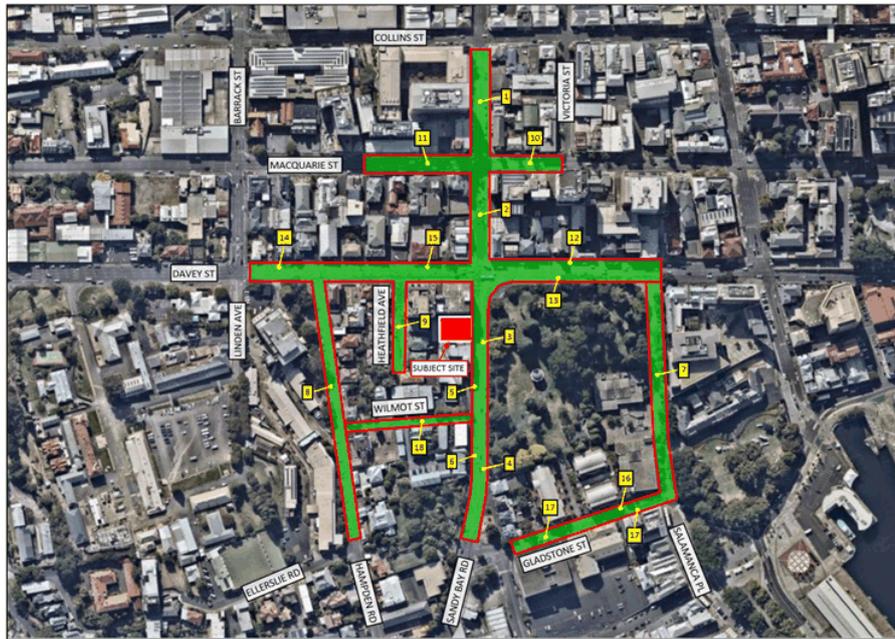


Figure 4 On-street parking survey area (Source: Nearmap)

Table 1 On-street parking restrictions

Point	Road	Section	Description
1	Harrington St	Eastern side, 55m north of Macquarie St	1P paid parking, 8:30am – 6pm, Monday to Saturday. No parking, 7am – 4pm, Sunday (permit holders excepted).
		Eastern side, 30m south of Collins St	1P paid parking, 8:30am – 6pm, Monday to Saturday, 7am – 4pm, Sunday.
		Western side, 30-60m north of Macquarie St	No parking, 8am – 6pm Monday to Saturday.
		Western side, 40m south of Collins St	1P paid parking, 8:30am – 6pm, Monday to Saturday, 9am – 4pm, Sunday.
2	Harrington St	Both sides b/w Macquarie St and Davey St	2P paid parking, 8:30am – 6pm, Monday to Friday.
3	Sandy Bay Rd	Eastern side b/w Davey St and Wilmot St	1P parking, 8:30am – 6pm, Monday to Friday.
4	Sandy Bay Rd	Eastern side b/w Wilmot St and Gladstone St	8P paid parking, 8:30am – 4:30pm, Monday to Friday. No stopping, 4:30-6pm, Monday to Friday.
5	Sandy Bay Rd	Western side b/w Wilmot St and Davey St	2P parking, 9:30am – 1pm Monday to Friday. No stopping, 6:30-9:30am and 1-6pm, Monday to Friday, 9am – 3pm Saturday.
6	Sandy Bay Rd	Western side b/w Wilmot St and Gladstone St	1/2P parking, 9:30am – 4pm, Monday to Friday. No stopping, 6:30-9:30am and 4-6pm, Monday to Friday.
7	Salamanca Pl	Both sides b/w Davey St and Gladstone St	2P paid parking, 8:30am – 6pm Monday to Friday, 9am – 4pm Sunday. Tow away area 5:30am – 6pm Saturdays.



Point	Road	Section	Description
8	Hampden Rd	Western side b/w Davey St and Ellerslie Rd	3P paid parking, 8:30am - 6pm, Monday to Friday.
9	Heathfield Ave	Eastern side b/w Davey St and court bowl	2P paid parking, 8:30am - 6pm, Monday to Friday.
		Western side b/w Davey St and court bowl	2P parking, 8am - 6pm, Monday to Friday (authorised residents vehicles excepted area 11).
10	Macquarie St	Northern side, 30m east of Harrington St	1P paid parking, 8:30am - 6pm, Monday to Saturday. No parking, 7am - 4pm, Sunday (permit holders excepted).
		Northern side, 35-50m east of Harrington St	1P paid parking, 8:30am - 6pm, Monday to Saturday, 7am - 4pm, Sunday.
		Southern side, 20m east of Harrington St	1/2P paid parking, 8:30am - 6pm, Monday to Saturday, 9am - 4pm, Sunday.
		Southern side, 20-35m east of Harrington St	1P paid parking, 8:30am - 6pm, Monday to Saturday, 9am - 4pm, Sunday.
11	Macquarie St	Southern side b/w Harrington St and no. 178	1P paid parking, 8:30am - 6pm, Monday to Friday.
12	Davey St	Northern side, 40m east of Harrington St	2P paid parking, 9am - 4pm, Monday to Friday. No stopping, 7:30-9 am and 4-6pm, Monday to Friday
		Northern side, 40-55m east of Harrington St	1/2P paid parking, 9am - 4pm, Monday to Friday. No stopping, 7:30-9 am and 4-6pm, Monday to Friday.
		Northern side, 65m west of Salamanca Pl	2P paid parking, 8:30am - 6pm, Monday to Friday.
13	Davey St	Southern side b/w Salamanca Pl and Harrington St	3P paid parking, 9am - 6pm, Monday to Friday. No stopping, 7:30-9am, Monday to Friday. Taxi Zone, 9am - 3pm, Saturday.
		Southern side b/w Salamanca Pl and Harrington St	3P paid parking, 9am - 6pm, Monday to Friday and 9am-3pm Saturday. No stopping, 7:30-9am, Monday to Friday.
		Southern side b/w Salamanca Pl and Harrington St	3P paid parking, 9am - 6pm, Monday to Saturday. No stopping, 7:30-9am, Monday to Friday.
14	Davey St	Northern side b/w Barrack St and Harrington St	1P paid parking, 8:30am - 6pm, Monday to Friday.
15	Davey St	Northern side b/w Barrack St and Harrington St	2P paid parking, 8:30am - 6pm, Monday to Friday.
16	Gladstone St	Northern side, b/w Salamanca Pl and Kirksway Pl	2P paid parking, 8:30am - 6pm, Monday to Friday.
			3P disabled parking.
			1/2P paid parking, 8:30am - 6pm, Monday to Friday
17	Gladstone St	Southern side, b/w Salamanca Pl and Kirksway Pl	2P paid parking, 8:30am - 6pm, Monday to Friday. Taxi Zone, 10pm-6am Thursday to Sunday
		Southern side, b/w Salamanca Pl and Kirksway Pl	2P paid parking, 8:30am - 6pm, Monday to Friday.
18	Wilmot St	Northern side b/w Sandy Bay Rd and Hampden Rd	2P parking, 8:30am - 6pm, Monday to Friday.
			2P paid parking, 8:30am - 6pm, Monday to Friday.



Table 1 shows that most parking in the vicinity of the site requires payment, with the most common time limits being 1-2 hours during weekday business hours (typically terminating at 6pm). Notably, 3-hour parking is available on the southern side of Davey Street between Salamanca Place and Sandy Bay Road, while 8-hour parking is available on the eastern side of Sandy Bay Road between Wilmot Street and Gladstone Street.

The results of the spot surveys are summarised in Table 2. It is noted that the survey results excluded any loading zone parking restrictions, bus zone parking restrictions and any parking restrictions less than 30-minute parking.

Table 2 Results of on-street parking surveys

Road	Side	Parking Supply (Weekday)	Parking Supply (Saturday)	Thursday 26/9/2024 12:15 - 14:00	Thursday 26/9/2024 17:45 - 18:30	Saturday 28/9/2024 10:30 - 11:15
Harrington St b/w Collins St & Macquarie St	East	10	10	3	5	3
	West	5	5	2	3	3
Harrington St b/w Macquarie St & Davey St	East	6	6	3	2	6
	West	6	6	2	1	6
Sandy Bay Rd b/w Davey St & Wilmot St	East	5 <sup>2</sup>	5	0	0	5
	West	4 <sup>2</sup>	0	0	0	0
Sandy Bay Rd b/w Wilmot St & Gladstone St	East	8	8	8	2	9
	West	7 <sup>2</sup>	7	0	0	10
Salamanca Pl b/w Davey St & Gladstone St	East	31	- <sup>1</sup>	19	28	-
	West	7	- <sup>1</sup>	6	7	-
Hampden Rd b/w Davey St & Ellerslie Rd	West	32	32	12	6	31
Heathfield Ave b/w Davey St & court bowl	East	8	8	3	6	8
	West	7	7	7	5	8
Macquarie St b/w Victoria St & Harrington St	North	6	6	2	4	2
	South	8	6	3	0	5
Macquarie St b/w Harrington St & no. 178	South	9	7	4	6	7
Davey St b/w Salamanca Pl & Sandy Bay Rd	North	16	16	9	3	16
	South	11	9	9	9	9
Davey St b/w Sandy Bay Rd & Barrack St	North	17	17	3	2	18
Gladstone St b/w Salamanca Pl & Kirksway Pl	North	9	9	5	9	12
	South	11	11	7	11	11
Wilmot St b/w Sandy Bay Rd & Hampden Rd	North	7	7	6	3	7
<b>Total Occupied</b>		-		113	112	176
<b>Total Available</b>		230	182	117	118	6 <sup>1</sup>
<b>% Occupancy</b>		-		49%	49%	97%

<sup>1</sup> There is no parking on Salamanca Place on Saturdays due to the Salamanca Market.

<sup>2</sup> Estimated parking supply as no line marked bays have been provided.



Table 2 shows that in the vicinity of the site, the parking is in medium demand during typically weekday periods, while the demand is high during Saturdays. A maximum of 113 out of 230 (49%) spaces were occupied during the weekday periods, and 176 out of 182 (97%) spaces were occupied during the Saturday period.

To supplement this data, additional spot parking surveys were undertaken within the same survey area at the following times:

- Tuesday 22 July, 2025 from 9am–12noon (at hourly intervals); and
- Friday 25 July, 2025 from 5pm–7pm (at hourly intervals).

The results of these surveys are summarised in Tables 3 and 4.

Table 3 Results of on-street parking surveys (Tuesday 22 July, 2025)

Road	Side	Parking Supply	Tuesday 22 July, 2025			
			9am	10am	11am	12noon
Harrington St b/w Collins St & Macquarie St	East	10	4	8	8	5
	West	5	4	4	1	3
Harrington St b/w Macquarie St & Davey St	East	6	6	5	4	6
	West	6	3	1	3	4
Sandy Bay Rd b/w Davey St & Wilmot St	East	5	1	3	3	5
	West	4	0	0	1	1
Sandy Bay Rd b/w Wilmot St & Gladstone St	East	8	8	8	8	8
	West	7	N/A <sup>1</sup>	0	0	0
Salamanca Pl b/w Davey St & Gladstone St	East	31	24	25	25	21
	West	7	3	5	3	6
Hampden Rd b/w Davey St & Ellerslie Rd	West	32	22	21	22	19
Heathfield Ave b/w Davey St & court bowl	East	8	2	0	1	1
	West	7	2	4	3	2
Macquarie St b/w Victoria St & Harrington St	North	6	1	1	2	4
	South	8	4	4	4	4
Macquarie St b/w Harrington St & no. 178	South	9	4	4	6	3
Davey St b/w Salamanca Pl & Sandy Bay Rd	North	16	11	12	11	11
	South	11	8	7	9	10
Davey St b/w Sandy Bay Rd & Barrack St	North	17	5	9	1	5
Gladstone St b/w Salamanca Pl & Kirksway Pl	North	9	7	5	5	3
	South	11	4	4	3	0
Wilmot St b/w Sandy Bay Rd & Hampden Rd	North	7	6	6	5	4
<b>Total Occupied</b>		–	129	136	128	125
<b>Total Available</b>		230	101	94	102	105
<b>% Occupancy</b>		–	56%	59%	56%	54%

<sup>1</sup> No Stopping restrictions apply during this time period.



Table 4 Results of on-street parking surveys (Friday 25 July, 2025)

Road	Side	Parking Supply	Friday 25 July, 2025		
			5pm	6pm	7pm
Harrington St <i>b/w Collins St &amp; Macquarie St</i>	East	10	10	10	10
	West	5	5	5	5
Harrington St <i>b/w Macquarie St &amp; Davey St</i>	East	6	1	5	4
	West	6	0	0	2
Sandy Bay Rd <i>b/w Davey St &amp; Wilmot St</i>	East	5	2	0	0
	West	4	N/A <sup>1</sup>	0	0
Sandy Bay Rd <i>b/w Wilmot St &amp; Gladstone St</i>	East	8	N/A <sup>1</sup>	0	6
	West	7	N/A <sup>1</sup>	0	0
Salamanca Pl <i>b/w Davey St &amp; Gladstone St</i>	East	31	28	31	31
	West	7	7	7	7
Hampden Rd <i>b/w Davey St &amp; Ellerslie Rd</i>	West	32	11	7	5
Heathfield Ave <i>b/w Davey St &amp; court bowl</i>	East	8	2	3	4
	West	7	1	1	1
Macquarie St <i>b/w Victoria St &amp; Harrington St</i>	North	6	4	6	6
	South	8	0	5	5
Macquarie St <i>b/w Harrington St &amp; no. 178</i>	South	9	2	9	9
Davey St <i>b/w Salamanca Pl &amp; Sandy Bay Rd</i>	North	11-16	8	10	16
	South	11	2	10	11
Davey St <i>b/w Sandy Bay Rd &amp; Barrack St</i>	North	17	1	1	3
Gladstone St <i>b/w Salamanca Pl &amp; Kirksway Pl</i>	North	9	5	8	8
	South	11	10	11	11
Wilmot St <i>b/w Sandy Bay Rd &amp; Hampden Rd</i>	North	7	2	1	3
<b>Total Occupied</b>		-	<b>101</b>	<b>130</b>	<b>147</b>
<b>Total Available</b>		<b>206-230</b>	<b>105</b>	<b>100</b>	<b>83</b>
<b>% Occupancy</b>		-	<b>49%</b>	<b>57%</b>	<b>64%</b>

<sup>1</sup> No Stopping restrictions apply during this time period.

Table 3 and 4 show that in the vicinity of the site, the parking is in medium demand during the surveyed periods, with the highest parking demand recorded on Friday at 7pm (64% occupancy).



### 1.6 EXISTING TRAFFIC CONDITIONS

To establish existing traffic conditions within the area, SALT commissioned traffic counts at the intersection of Sandy Bay Road and Wilmot Street adjacent to the site.

The traffic counts were undertaken on Friday 25 July, 2025 from 5pm-7pm. A summary of the PM peak hour volumes at the intersection is provided in Figure 5.

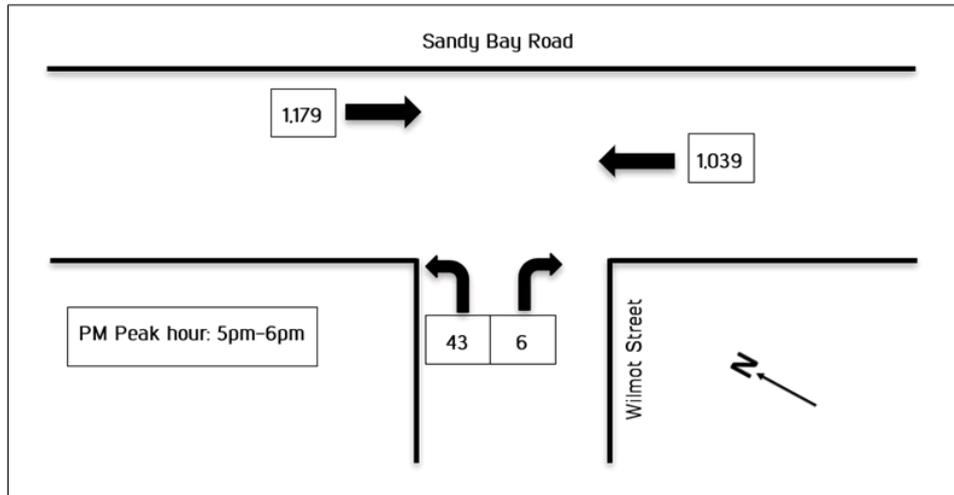


Figure 5 PM Peak Hour Traffic Volumes

## 2 PROPOSAL

It is proposed to construct seven (7) additional storeys to the existing 2-storey building on the site for the purpose of providing 33 hotel rooms/serviced apartments (visitor accommodation) and a rooftop bar. The proposal will increase the floor area by approximately 3,141.5 m<sup>2</sup> for a total floor area of about 4,286.5 m<sup>2</sup>.

The development proposal is outline in Table 5

Table 5 Development proposal

Use	Rooms	Floor Area
Visitor Accommodation	33 no.	2,630 m <sup>2</sup>
Bar (Hotel industry) <sup>1</sup>	-	136.2 m <sup>2</sup>
<b>TOTAL</b>	<b>33</b>	<b>2,766.2 m<sup>2</sup></b>

<sup>1</sup> The bar floor area is taken as the public bar room floor area as per Table E6.1 of the Planning Scheme and excludes the lift, stairs, services, toilets, and outdoor terraces.

Provisionally, the bar will operate between 6am - 12am (midnight) Monday to Sunday and will be open to both hotel guests and people who are not staying at the hotel.

The proposal does not include the provision of on-site car parking spaces due to the current site constraints. It is instead proposed to provide a dedicated pick-up/drop-off area on Wilmot Street (for the accommodation component) near its intersection with Sandy Bay Road, which will result in the loss of one (1) on-street car parking space. The location of the proposed pick-up/drop-off area is shown in Figure 6





Figure 6 Location of proposed pick-up/drop-off area

### 3 CAR PARKING MATTERS

#### 3.1 APPLIED CAR PARKING REQUIREMENTS

##### 3.1.1 Review of Parking at Existing Hotels in Hobart

SALT has carried out a desktop review of the parking provision of existing hotels in Hobart. Our findings are summarised in Table 6.

Table 6 Parking provision at hotels in Hobart

Hotel	Address	Parking Provision
Mantra One Sandy Bay Road (50 rooms)	1 Sandy Bay Rd, Hobart	On-site, 14 parking spaces, requires pre-booking.
Quest Savoy (32 serviced apartments)	38 Elizabeth St, Hobart	Off-site at the Market Place Car Park, shared public parking (4-6 Market Pl, Hobart).
Somerset on the Pier (56 serviced apartments)	1 Elizabeth St Pier, Hobart	No on-site parking.
Movenpick Hotel (221 rooms)	28 Elizabeth St, Hobart	On-site at the Trafalgar Place Car Park, shared public parking, requires pre-booking (Trafalgar Pl, Hobart).
Vibe Hotel (142 rooms)	36 Argyle St, Hobart	Off-site at the Market Place Car Park, shared public parking (4-6 Market Pl, Hobart).
ibis Styles (296 rooms)	173 Macquarie St, Hobart	On-site, 35 parking spaces, subject to availability.
Mantra on Collins (80 rooms)	58 Collins St, Hobart	Off-site at the Market Place Car Park, shared public parking (4-6 Market Pl, Hobart).
The Alabama Hotel (17 rooms)	1/72 Liverpool St, Hobart	No on-site parking.



Table 6 shows that many existing hotels/services apartment developments in Hobart do not provide on-site car parking and instead rely on independent parking providers in the area. Furthermore, the sites that do offer parking on-site, provide parking at rates much lower than the statutory requirements:

- Mantra One Sandy Bay Road provides parking at a rate of 0.28 car parking spaces per room.
- ibis Styles provides parking at a rate of 0.12 car parking spaces per room.

It is noted that the statutory requirements regarding parking provision for visitor accommodation are 1 car parking space per room / serviced apartment. Applying this rate to the proposal equates to a parking requirement of 33 car parking spaces for the visitor accommodation component of the proposed development.

### 3.1.2 Case Study Data for Serviced Apartment Developments

SALT has previously obtained parking data from Punthill, which is an established operator of serviced apartments in urban locations such as Melbourne, Sydney, and Brisbane. Parking rates for several existing developments are provided in Table 7.

Table 7 Parking rates at Punthill-operated serviced apartments

Location	Rooms	Parking Spaces	Parking Rate (spaces/room)
Punthill Parammatta	52	17	0.33
Punthill Ivanhoe	54	20	0.37
Punthill Essendon	56	29	0.52
Punthill Essendon North	40	11	0.28
Veriu Queen Victoria Market	110	18	0.16
Veriu Collingwood	95	28	0.29
Punthill Sunshine Melbourne	90	28	0.31
Veriu Macquarie Park	184	50	0.27
Average Parking Provision Rate (spaces per room)			0.32

The data in Table 7 shows that the average parking provision rate at similar developments in metropolitan settings is roughly one third of the statutory requirement. Furthermore, the average parking provision rate in the table is similar to the parking provision of Mantra One Sandy Bay Road, which neighbours the subject site in the northwest.

### 3.1.3 Parking Requirements for a Bar

The applicable statutory requirement for the rooftop bar is 1 car parking space for each 3 m<sup>2</sup> of public bar room floor area, which equates to a parking requirement of 46 car parking spaces for the rooftop bar component of the proposed development. This requirement is considered highly excessive.

When determining the parking requirements for a bar, it is important to consider its location, transport mode options in the area, as well as the likely mode of travel that patrons will choose. In this instance, the bar will be located on the top floor of a hotel and as such a portion of its patronage will include hotel guests. Furthermore, the site is situated within central Hobart (for all intents and purposes) and is close to various business-, tourist-, and retail destinations that are conveniently accessible by sustainable transport (walking, public transport, e-scooters, etc.). Additionally, it is common practice for the majority of bar patrons not to drive when consuming alcohol and instead opt for carpooling (with a 'designated driver'), taxis/rideshare, Uber, etc.

Based on the considerations above, we believe that adopting a parking requirement that is 10% of the statutory requirement, i.e., 1 car parking space for each 30 m<sup>2</sup> of public bar room floor area, would be appropriate for the proposed development. This will adequately account for any potential staff parking demands as well as the parking demands of patrons who nevertheless choose to drive.



### 3.1.4 Summary

Based on the relevant statutory car parking requirements (parking rates), the proposal would be required to provide a total of 79 car parking spaces on-site. This is deemed excessive for the proposed development given its location and the parking provisions at similar existing developments.

#### *Serviced Apartments*

A review of car parking provisions at existing hotels in Hobart has shown that it is commonplace for hotels not to provide parking on-site but instead rely on parking facilities located elsewhere – these facilities are typically operated by independent businesses. In instances where on-site parking is available, car spaces are provided at rates much lower than the statutory requirements.

A review of case study data related to the provision of parking at similar serviced apartment developments in metropolitan settings has revealed an average parking rate of about *0.32 spaces per room* as being typical. This is roughly one third of the statutory requirement for the proposal. However, it is also based on hotels that provide on-site parking.

In this case, no on-site car parking is to be provided for the proposed serviced apartments. Accordingly, and when considering the fact that the majority of parking within the surrounding area is typically subject short-term (paid and unpaid) parking restrictions, particularly during weekdays, any guests who wish to drive to/from the site will not have many suitable car parking to use. Therefore, guests will be discouraged from driving to/from the site and will instead rely on other modes of transport (such as public transport, taxi, uber, etc.) to access the site.

It is also noted that as part of the facilities' operational management plan (i.e. online booking platform or over the phone), when rooms are booked by guests, they are typically advised about the different ways available to access the site and if there is any suitable on-site and/or off-site parking that could be used. Guests will therefore know in advance that driving and parking a vehicle within the surrounding area would not be suitable and adjust accordingly.

Based on the foregoing, the proposed serviced apartments component of the development is therefore not projected to generate any parking demands associated with guests and/or staff. Guests are expected to arrive by public transport or via taxis, ubers, etc. with these drivers capable of using any of the available nearby on-street parking resources to drop-off and/or pick-up guests, including the proposed pick-up/drop-off bay on Wilmot Street near its intersection with Sandy Bay Road.

#### *Rooftop Bar*

With respect to the rooftop bar, the location and setting of the bar, as well as the likely travel characteristics of patrons should be accounted for. The rooftop bar will be located on the top floor of the proposed development, which itself is centrally within Hobart and therefore situated in close proximity to numerous attractions.

Furthermore, the bar will be located on the top floor of a hotel and as such a portion of its patronage will include hotel guests. It is also noted that it is common practice for the majority of bar patrons not to drive when consuming alcohol and instead opt for carpooling (with a 'designated driver'), taxis/rideshare, Uber, etc.

As such, an adjusted parking requirement of 1 car parking space for each 30 m<sup>3</sup> of public bar room floor area (i.e., 10% of the statutory requirement) is considered appropriate for the rooftop bar in this instance, which means the proposed development is anticipated to generate a parking demand for up to five (5) car spaces associated with the rooftop bar. This demand would typically occur during weekday evenings.

## 3.2 STATUTORY CAR PARKING REQUIREMENTS

Statutory car parking requirements are specified in Code E6.0 (Parking and Access Code) of the Hobart Interim Planning Scheme 2015. The application triggers Clause E6.6 (Use Standards) and requires assessment against Clause E6.6.1 (Number of Car Parking Spaces) – this is provided below in **Table 8**.



Table 8 Clause E6.6 (Use Standards)

Clause E6.6.1 – Number of Car Parking Spaces	
<p>Objective:</p> <p>To ensure that:</p> <ul style="list-style-type: none"> <li>there is enough car parking to meet the reasonable needs of all users of a use or development, taking into account the level of parking available on or outside of the land and the access afforded by other modes of transport.</li> <li>a use or development does not detract from the amenity of users or the locality by:                             <ul style="list-style-type: none"> <li>preventing regular parking overspill;</li> <li>minimising the impact of car parking on heritage and local character.</li> </ul> </li> </ul>	
Acceptable Solution	Performance Criterion
<p><b>A1</b></p> <p>The number of on-site car parking spaces must be:</p> <ul style="list-style-type: none"> <li>no less than and no greater than the number specified in Table E6.1; except if                             <ul style="list-style-type: none"> <li>the site is subject to a parking plan for the area adopted by Council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;</li> <li>the site is subject to clauses E6.6.5, E6.6.6, E6.6.7, E6.6.8, E6.6.9 or E6.6.10 of this planning scheme.</li> </ul> </li> </ul>	<p><b>P1</b></p> <p>The number of on-site car parking spaces must be sufficient to meet the reasonable needs of users, having regard to all of the following:</p> <ul style="list-style-type: none"> <li>car parking demand;</li> <li>the availability of on-street and public car parking in the locality;</li> <li>the availability and frequency of public transport within a 400m walking distance of the site;</li> <li>the availability and likely use of other modes of transport;</li> <li>the availability and suitability of alternative arrangements for car parking provision;</li> <li>any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces;</li> <li>any car parking deficiency or surplus associated with the existing use of the land;</li> <li>any credit which should be allowed for a car parking demand deemed to have been provided in association with a use which existed before the change of parking requirement, except in the case of substantial redevelopment of the site;</li> <li>the appropriateness of a financial contribution in lieu of parking towards the cost of parking facilities or other transport facilities, where such facilities exist or are planned in the vicinity;</li> <li>any verified prior payment of a financial contribution in lieu of parking for the land;</li> <li>any relevant parking plan for the area adopted by Council;</li> </ul>



- the impact on the historic cultural heritage significance of the site if subject to the Local Heritage Code;
- whether the provision of the parking would result in the loss, directly or indirectly, of one or more significant trees listed in the Significant Trees Code.

**Response**A1

Table E6.1 specifies the following requirements in terms of parking provision for visitor accommodation and hotel industry:

- Residential hotel – 1 car parking space for each bedroom; or alternatively
- Serviced apartment – 1 car parking space for each serviced apartment unit; and
- Hotel industry – 1 car parking space for each 3 m<sup>2</sup> of public bar room floor area.

Accordingly, the proposal has a statutory requirement to provide the following in terms of parking spaces:

- Visitor accommodation – 33 spaces; and
- Bar – 46 spaces.

The proposal is thus required to provide a total of 79 parking spaces. As no on-site car parking spaces will be provided, assessment against the Performance Criteria is required.

P1

- It is anticipated that the actual car parking demands would be substantially lower than the statutory requirement as discussed in **Section 3.1**. The site is within walking distance to key tourist and business areas, such as Salamanca, Hobart CBD, Sullivans Cove, etc., which lends itself to guests walking to/from these areas instead of using private vehicles. Furthermore, the southern boundary of the 'Central Business Zone' is situated on the northern side of Davey Street, only about 60m from the subject site. Acceptable Solution A1 to Clause E6.6.5 (Number of Car Parking Spaces – Central Business Zone) includes the provision of no on-site car parking for developments in this area and given the site's proximity to the central business zone, it is anticipated that the proposal will exhibit parking characteristics and demands similar to developments within this area.
- As detailed in **Section 1.4**, the site has very good access to public transport, with bus stops serving buses both to and from Hobart Interchange located within convenient walking distances – the furthest bus stop is about 140m walking distance from the site. Buses arrive at these stops in approximately 30 – 60-minute intervals, depending on the time and day, with services being more frequent during peak periods. Furthermore, the surrounding area encompasses a well-connected footpath network that adequately serves active transport alternatives, which is further enhanced by the recent provision of a bike lane on Davey Street between Sandy Bay Road and Linden Avenue. Additionally, e-scooters provide another transport option, and the physical parking bays provided at the St Davids Park entrance, about 80m walking distance from the site, ensures that this alternative is readily accessible.
- In relation to the proposed hotel/serviced apartments, no on-site car parking is to be provided, and when considering the fact that the majority of parking within the surrounding area is typically subject short-term (paid and unpaid) parking restrictions, particularly on weekdays and Saturdays, any guests who wish to drive to/from the site will not have many suitable car parking to use. Therefore, guests will be discouraged from driving to/from the site and will instead rely on other modes of transport (such as public transport, taxi, uber, etc.) to access the site.

It is also noted that as part of the facilities' operational management plan (i.e. online booking platform, hotel website and/or over the phone booking), when rooms are booked by guests they are typically advised about the different ways available to access the site and if there is any suitable on-site and/or off-site parking that could be used. Guests will therefore know in advance that driving and parking a vehicle within the surrounding area would not be suitable and adjust accordingly. In other words, guests that require a parking space will unlikely book at the facility and will therefore book another hotel.



Based on the foregoing, the proposed serviced apartments component of the development is therefore not projected to generate any parking demands associated with guests and/or staff (as staff parking is typically long-term in nature and there is no long-term on-street parking available within the surrounding area). Guests are expected to arrive by public transport or via taxis, ubers, etc. with these drivers capable of using any of the available nearby on-street parking resources to drop-off and/or pick-up guests, including the proposed pick-up/drop-off bay on Wilmot Street near its intersection with Sandy Bay Road.

- With respect to the rooftop bar, the location and setting of the bar, as well as the likely travel characteristics of patrons should be accounted for. The rooftop bar will be located on the top floor of the proposed development, which itself is centrally within Hobart and therefore situated in close proximity to numerous attractions.

Furthermore, the bar will be located on the top floor of a hotel and as such a portion of its patronage will include hotel guests. It is also noted that it is common practice for the majority of bar patrons not to drive when consuming alcohol and instead opt for carpooling (with a 'designated driver'), taxis/rideshare, Uber, etc.

As such, an adjusted parking requirement of 1 car parking space for each 30 m<sup>3</sup> of public bar room floor area (i.e., 10% of the statutory requirement) is considered appropriate for the rooftop bay in this instance, which means the proposed development is anticipated to generate a parking demand for up to five (5) car spaces associated with the rooftop bar. This demand would typically occur during weekday evenings.

- As detailed in **Section 1.5**, there is abundant on-street parking available during the surveyed weekday periods in the vicinity of the site. Specifically, the surveys undertaken during the Friday evening period showed that there were no fewer than 83 vacant car spaces within the survey area (64% Occupancy). The available on-street parking resources surrounding the site can therefore readily accommodate the projected bar parking demands.
- No on-site car parking has been provided for the existing use of the land (Hobart Masonic Hall).
- Credit associated with a previous car parking demand is not applicable to the proposal.
- A financial contribution in lieu of parking is not applicable to the proposal.
- There is no known prior payment of a financial contribution in lieu of parking for the land.
- A parking plan for the area is not applicable to the proposal.
- The site is subject to the Local Heritage Code but no on-site car parking is proposed, meaning there are no heritage impacts.
- The Significant Trees Code is not applicable to the proposal.

Based on the assessment provided in **Table 8**, it is deemed acceptable for the proposed development to provide zero (0) on-site car parking spaces. The location of the site within central Hobart, proximity to key tourist and business areas, accessibility to public transport, and availability of alternative modes of travel (e.g., e-scooters, rideshare, etc.) will ensure that the lack of dedicated parking will not adversely affect the operations and functioning of the proposed development. Furthermore, the proposed provision of a dedicated pick-up/drop-off area in Wilmot Street will improve guests' access to the site.

### 3.3 DESIGN OF PICK-UP/DROP-OFF AREA

It is proposed to provide a dedicated pick-up/drop-off area on Wilmot Street (for the accommodation component) near its intersection with Sandy Bay Road. We understand that as part of the approved development at the corner of the Sandy Bay Road/Wilmot Street intersection, there will be a distance of approximately 22.0m between the proposed crossover on Wilmot Street and Sandy Bay Road (measured to the face of the kerb along Sandy Bay Road). This is illustrated in Figure 7.

Allowing for a standard 10.0m 'No Stopping' clearance from the intersection as per the road rules, there would be at least 12.0m of available length north of the crossover to accommodate the dedicated pick-up/drop-off area on Wilmot Street. This is sufficient to accommodate at up to two (2) unobstructed on-street spaces as per the requirements of the AS/NZS 2890.5:2020 (i.e. 5.4m long per space). Alternatively, a single 12.0m long area could be provided.

As per existing conditions along Wilmot Street, the width of the dedicated pick-up/drop-off area will be 2.1m which is consistent with the other parking bays along Wilmot Street.



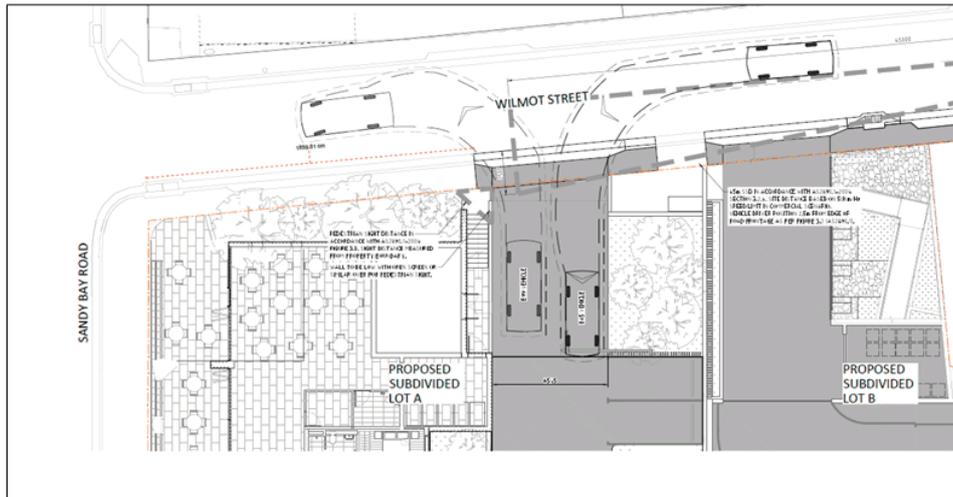


Figure 7 Approved Development Access Arrangements

We are therefore satisfied that an appropriate and compliant design outcome can be achieved for the proposed to dedicated pick-up/drop-off area on Wilmot Street.

#### 4 WASTE COLLECTION

Waste collection will be undertaken by a private service provider. An on-street loading zone is located on the western side of Sandy Bay Road, about 40-50m south of the site – bins will be transferred to this area on collection days for kerbside pick-up.

#### 5 TRAFFIC CONSIDERATIONS

No on-site car parking is proposed and therefore, the site itself will not generate any traffic. However, it is acknowledged that the proposed development will result in an increase in general traffic within the area, albeit, this increase is expected to be negligible given the site’s location and the nature of the proposed uses.

The rooftop bar is only expected to generate a modest demand for up to five (5) car spaces (as discussed in Section 3 of this report), which would typically occur during weekday evenings outside of the commuter peak hour.

In relation to the proposed serviced apartments, it is generally accepted that peak traffic tends to occur outside the commuter peak hours, i.e. between 10am and 2pm during typical check-in and check-out times.

Based on the foregoing, any traffic associated with the proposed development will be spread throughout the day and will not have any noticeable impacts on the capacity and/or operation of the surrounding road network.

Notwithstanding the above and conservatively assuming that:

- 1/3<sup>rd</sup> of the rooms check-in during the afternoon peak hour (assuming all guests arrive by taxi/uber). This accounts for the fact that the arrival and departure patterns of hotels can vary greatly and be quite random.
- All guests will be picked up from the proposed pick-up and drop-off bay on Wilmot Street.

In the order of 11 peak hour-trips are projected to be generated by the proposed development via Wilmot Street, inbound from Hampden Road and outbound to Sandy Bay Road (given the one-way operation of Wilmot Street).

The addition of 11 vehicle movements to/from the Wilmot Street/Sandy Bay Road intersection is a very modest level of traffic and will not have any material impacts on the capacity and/or operation of the intersection. This equates to one additional vehicle being generated, on average, every 5.5 minutes.



Whilst it is acknowledged that vehicles turning right out from Wilmot Street can face some delays when finding an appropriate gap in the through traffic lanes, the nearby signalised intersections of Sandy Bay Road with Davey Street and Gladstone Street tend to result in the bunching of through traffic which then leads to large gaps being available for vehicles turning out from Wilmot Street.

We are therefore satisfied that the traffic likely to be generated by the proposed development will be readily accommodated by the surrounding road network and intersections without resulting in any detrimental impacts.

## 6 CONCLUSION

Based on the considerations outlined in this letter, it is concluded that:

- The proposal relates to the development of a hotel that will provide 33 rooms/serviced apartments and a bar with a floor area of about 136.2 m<sup>2</sup>.
- The proposed development will involve a vertical extension of the existing Hobart Masonic Hall.
- The proposed development has a statutory requirement to provide a total of 79 car parking spaces.
- With zero (0) car parking spaces provided on the site, the proposal falls short of the statutory requirements.
- Given there is no on-site parking and the surrounding on-street parking restrictions make it impractical for guests and/or staff to park their car within the surrounding area, parking demands associated with the serviced apartments will only be limited to drop-off/pick-up activity (e.g. taxis/ubers).
- During weekday evenings, parking demands will be mainly associated with the rooftop bar (up to 5 spaces projected) noting that some patrons of the bar are expected to be residents of the hotel and that the majority of other patrons will be people who arrive/depart the site via taxis or ubers as they are likely to be consuming alcohol.
- Parking occupancy surveys of the available on-street car parking within the surrounding area show that there is more than sufficient parking availability to accommodate the proposed development's anticipated parking generation, particularly during weekday evenings.
- The provision of a dedicated pick-up/drop-off area near the Sandy Bay Road/Wilmot Street intersection will improve guests' access to the site. This will result in the loss of one (1) on-street car parking space; however, sufficient on-street parking will be available in the area to offset this loss of parking.
- An appropriate and compliant design outcome can be achieved for the proposed to dedicated pick-up/drop-off area on Wilmot Street.
- The level of traffic that is likely to be generated by the proposed development is small and will be readily accommodated by the surrounding road network and intersections without resulting in any detrimental impacts.

If there are any enquiries in relation to this assessment, I can be contacted on the number below.

Yours sincerely,



Tony Togany  
Senior Associate  
SALT  
M: 0458 340 274  
[tony.togany@salt3.com.au](mailto:tony.togany@salt3.com.au)





## Submission to Planning Authority Notice

<b>Council Planning Permit No.</b>	PLN-21-710	<b>Council notice date</b>	25/10/2021
<b>TasWater details</b>			
<b>TasWater Reference No.</b>	TWDA 2021/01834-HCC	<b>Date of response</b>	05/11/2021
<b>TasWater Contact</b>	Phil Papps	<b>Phone No.</b>	0474 931 272
<b>Response issued to</b>			
<b>Council name</b>	CITY OF HOBART		
<b>Contact details</b>	coh@hobartcity.com.au		
<b>Development details</b>			
<b>Address</b>	3 SANDY BAY RD, HOBART	<b>Property ID (PID)</b>	5672455
<b>Description of development</b>	Partial demolition, alterations, extension, and partial change of use to visitor accommodation and hotel industry		
<b>Schedule of drawings/documents</b>			
<b>Prepared by</b>	<b>Drawing/document No.</b>	<b>Revision No.</b>	<b>Date of Issue</b>
Designhaus	Site Plan / DA00	PA	19/10/2021
<b>Conditions</b>			
Pursuant to the <i>Water and Sewerage Industry Act 2008</i> (TAS) Section 56P(1) TasWater imposes the following conditions on the permit for this application:			
<b>CONNECTIONS, METERING &amp; BACKFLOW</b>			
1. A suitably sized water supply with metered connections and sewerage system and connections to the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit.			
<i>Advice: TasWater will not accept direct fire boosting from the network due to the periodic testing of the system causing a significant negative effect on our network and the minimum service requirements of other customers serviced by the network. To this end break tanks may be required with the rate of flow into the break tank controlled so that peak flows to fill the tank do not also cause negative effect on the network.</i>			
2. Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost.			
3. Prior to use of the development, any water connection utilised for the development must have a backflow prevention device and water meter installed, to the satisfaction of TasWater.			
<b>ASSET CREATION &amp; INFRASTRUCTURE WORKS</b>			
4. In the event that TasWater's existing water and/or sewerage network does not have the capacity to service the required demands for the proposed development the following asset creation and infrastructure works conditions will apply.			
<i>Advice: In accordance with TasWater's 'Developer Charges Policy' for developments located within Serviced Land where insufficient capacity is available within an existing system, the developer pays the costs of Extension, including connection, to that system and Expansion of the system to the level of capacity required to service the development.</i>			
5. Plans submitted with the application for Engineering Design Approval must, to the satisfaction of			



TasWater show, all existing, redundant and/or proposed property services and mains.

6. Prior to applying for a Permit to Construct the new infrastructure the developer must obtain from TasWater Engineering Design Approval for the new TasWater infrastructure. The application for Engineering Design Approval must include engineering design plans prepared by a suitably qualified person showing the hydraulic servicing requirements for water and/or sewerage to TasWater's satisfaction.
7. Prior to works commencing, a Permit to Construct must be applied for and issued by TasWater. All infrastructure works must be inspected by TasWater and be to TasWater's satisfaction.
8. In addition to any other conditions in this permit, all works must be constructed under the supervision of a suitably qualified person in accordance with TasWater's requirements.
9. Prior to the issue of a Certificate of Water and Sewerage Compliance (Building and/or Plumbing) all additions, extensions, alterations or upgrades to TasWater's water and sewerage infrastructure required to service the development, are to be constructed at the expense of the developer to the satisfaction of TasWater, with live connections performed by TasWater.
10. After testing/disinfection, to TasWater's requirements, of newly created works, the developer must apply to TasWater for connection of these works to existing TasWater infrastructure, at the developer's cost.
11. At practical completion of the water and sewerage works and prior to applying to TasWater for a Certificate of Water and Sewerage Compliance (Building and/or Plumbing), the developer must obtain a Certificate of Practical Completion from TasWater for the works that will be transferred to TasWater. To obtain a Certificate of Practical Completion:
  - a. Written confirmation from the supervising suitably qualified person certifying that the works have been constructed in accordance with the TasWater approved plans and specifications and that the appropriate level of workmanship has been achieved;
  - b. A request for a joint on-site inspection with TasWater's authorised representative must be made;
  - c. Security for the twelve (12) month defects liability period to the value of 10% of the works must be lodged with TasWater. This security must be in the form of a bank guarantee;
  - d. Work As Constructed drawings and documentation must be prepared by a suitably qualified person to TasWater's satisfaction and forwarded to TasWater.
12. After the Certificate of Practical Completion has been issued, a 12 month defects liability period applies to this infrastructure. During this period all defects must be rectified at the developer's cost and to the satisfaction of TasWater. A further 12 month defects liability period may be applied to defects after rectification. TasWater may, at its discretion, undertake rectification of any defects at the developer's cost. Upon completion, of the defects liability period the developer must request TasWater to issue a "Certificate of Final Acceptance". The newly constructed infrastructure will be transferred to TasWater upon issue of this certificate and TasWater will release any security held for the defects liability period.
13. The developer must take all precautions to protect existing TasWater infrastructure. Any damage caused to existing TasWater infrastructure during the construction period must be promptly reported to TasWater and repaired by TasWater at the developer's cost.
14. Ground levels over the TasWater assets and/or easements must not be altered without the written approval of TasWater.
15. A construction management plan must be submitted with the application for TasWater Engineering Design Approval. The construction management plan must detail how the new TasWater



infrastructure will be constructed while maintaining current levels of services provided by TasWater to the community. The construction plan must also include a risk assessment and contingency plans covering major risks to TasWater during any works. The construction plan must be to the satisfaction of TasWater prior to TasWater’s Engineering Design Approval being issued.

**DEVELOPMENT ASSESSMENT FEES**

- 16. The applicant or landowner as the case may be, must pay a development assessment fee of \$699.36 to TasWater, as approved by the Economic Regulator and the fee will be indexed, until the date paid to TasWater. The payment is required within 30 days of the issue of an invoice by TasWater.

**Advice**

**General**

For information on TasWater development standards, please visit <https://www.taswater.com.au/building-and-development/technical-standards>

For application forms please visit <https://www.taswater.com.au/building-and-development/development-application-form>

**Service Locations**

Please note that the developer is responsible for arranging to locate the existing TasWater infrastructure and clearly showing it on the drawings. Existing TasWater infrastructure may be located by a surveyor and/or a private contractor engaged at the developers cost to locate the infrastructure.

The location of this infrastructure as shown on the GIS is indicative only.

- (a) A permit is required to work within TasWater’s easements or in the vicinity of its infrastructure. Further information can be obtained from TasWater
- (b) TasWater has listed a number of service providers who can provide asset detection and location services should you require it. Visit [www.taswater.com.au/Development/Service-location](http://www.taswater.com.au/Development/Service-location) for a list of companies.

**Declaration**

The drawings/documents and conditions stated above constitute TasWater’s Submission to Planning Authority Notice.

**Authorised by**

**Jason Taylor**  
 Development Assessment Manager

**TasWater Contact Details**

Phone	13 6992	Email	development@taswater.com.au
Mail	GPO Box 1393 Hobart TAS 7001	Web	www.taswater.com.au

## Attachment A

**PLN-HOB-2024-0710 – 3 Sandy Bay Road**

In person:

Phil Gartrell

IreneInc

Richard Crawford

Architects Designhaus

The Panel met to discuss the proposal in detail and the report below is a summary of the Panel's views and is provided for the consideration of the proponents and Council officers.

**Description**

The proposal is to construct an additional seven storeys above part of the existing building on the site, a Masonic Temple. The additional levels would accommodate a hotel, including a "rooftop bar area", on the top floor. Five levels of accommodation would be provided below this level, including rooms with one, two, and three bedrooms. The proposed level 2 would include only toilets, storage, and a lift lobby as it would not extend the full width of the building. The roof of the existing building would be mostly retained with the additional floors to be built above it.

The existing building on the site is the heritage listed Hobart Masonic Hall. The proposed changes to the existing building would be limited to structural changes to allow for the proposed additional levels and the conversion of a kitchen/servery on the ground floor to the hotel reception/lift lobby. The proposal also includes the construction of columns that would encroach onto the adjoining properties to the north-west (1 Sandy Bay Road), south-east (5-7 Sandy Bay Road), and the south-west (2A Heathfield Avenue) at ground level.

No vehicular access to the development or on-site car parking is proposed. Pedestrian access to the proposed hotel would be via an access ramp on the north-western side of the building. The existing main entrance to the Masonic Hall would be retained.

Proposed external materials include zinc metal cladding, perforated façade metal screening, and tinted glazing.

**Panel Report**

*Please Note: The Panel's views expressed below include those discussed with the Applicant and those discussed between Panel members in the absence of the Applicant.*

The Panel welcomed this application having previously seen it as a pre-application and noted the proponent's response to previous issues raised.

The Panel were generally of the view that the scale, massing and the architectural approach in response to the heritage of the Masonic Hall was acceptable. The proposed design has more vertical articulation in the new tower form when compared to previous proposed designs, which, along with the more limited materials palette being proposed, the Panel acknowledges is an improvement.

The Panel appreciate the significant complexities arising from such a restricted site. Whilst outside the scope of the Panel's review, the Panel noted the possible impact on the public realm during construction and ongoing operations where all servicing of the hotel will be via the front door and Sandy Bay Rd footpath.

The Panel were surprised that there is a significant lack of design thinking and detail in the proposal for the area of public realm at the entry, along Sandy Bay Road. The Panel would have expected to see some detail surrounding this, considering also that this space has to fulfil a lot functions; not only as the public "front" for the proposed building but also given it is proposed that building waste and laundry will exit the building via this forecourt.

The Panel thought it would be good to know how the design of the laneway could be improved to make it more inviting, but still respectful to its heritage context. The use of public art, lighting, seating, high quality materials and finishes could be used to draw people in. It is important to make the space intriguing and also make people feel safer.

Due to Sandy Bay Road being a clearway for significant amounts of time during the day, the applicant suggested that the hotel would look to combine waste collection services with the adjoining property at 5-7 Sandy Bay Road for rubbish removal, including using Wilmot Street as a loading area for commercial collection of the waste. The Panel questioned the operational viability of the proposed solution, however if this is to be the case, the Panel noted that a precinct plan would have been beneficial to see how this would look and work.

The application has defined a base material palette and the Panel see value and merit in the proposed materials for the cladding. The Panel recognise at

this stage there are still some aspects to be developed. There is a certain 'devil is in the detail' both in terms of the engineering and architectural resolution, and how this relates to the application of the proposed material palette. The Panel advised that if the proposal deviated too far from what's been proposed, they would have concerns and suggest such a deviation should be considered further by the Panel.

The Panel did note that the proposed blank wall along the south-eastern elevation of the tower component will be quite imposing and visible in the distance. It was suggested some articulation for that wall.

The Panel questioned if the plant level façade, just above the existing building's roofline, would be better in a recessive/darker colour.

The Panel drew attention to roof and overall height presented; the lack of lift overrun and possible service infrastructure would have an impact on the building height and therefore should be included within the drawing set.

The Panel suggest that Council request additional detail regarding the height, so that it is assessed as part of the application rather than conditioned. If so, the Panel noted they would not need to see it again in its current form, as it is not seen as a significant impact on the overall built form.

As an additional matter, the Panel noted the inconsistencies in the drawings provided. For example, codes were wrong, and street names were incorrect, and it was unclear what material was being used on the plant level. Due to the potential for misunderstandings and poor assessment drawings would benefit from some additional review and improvements, including updating the street elevations to include the new design for the approved 5-7 Sandy Bay Road development application.

## DEVELOPMENT ENGINEERING

## ASSESSMENT SUMMARY:

CLAUSE	DESCRIPTION	ASSESSMENT
<b>E5.0 Road and railway access code –DOES APPLY</b>		
E5.5.1	Existing road accesses and junctions	Acceptable Solution
E5.5.2	Existing level crossings	N/A
E5.6.1	Development adjacent to roads and railways	N/A
E5.6.2	Road accesses and junctions	N/A
E5.6.3	New level crossings	N/A
E5.6.4	Sight distance at accesses, junctions and level crossings	N/A
<b>E6.0 Parking and Access Code - DOES APPLY</b>		
E6.6	Parking number assessment	Performance Criteria
E6.7.1	Number of vehicle accesses	N/A
E6.7.2	Design of vehicle accesses	N/A
E6.7.3	Vehicle passing area along an access	N/A
E6.7.4	On-site turning	N/A
E6.7.5	Layout of parking areas	N/A
E6.7.6	Surface treatment of parking areas	N/A
E6.7.7	Lighting of parking areas	N/A
E6.7.8	Landscaping of parking areas	N/A
E6.7.9	Design of motorcycle parking areas	N/A
E6.7.10	Design of bicycle parking areas	N/A
E6.7.11	Bicycle end trip facilities	Planner to assess
E6.7.12	Siting of car parking	N/A
E6.7.13	Facilities for commercial vehicles	Acceptable Solution
E6.7.14	Access to a road	N/A
E6.7.15	Access to Niree Lane Sandy Bay	N/A
<b>E7.0 Stormwater - DOES APPLY – Assesed</b>		

## RECOMMENDATION:

Approval with conditions

**Clause(s) 6.6's - PERFORMANCE CRITERIA** Acceptable solution - A1: - **DOES NOT COMPLY: The proposal includes 0 on-site car parking spaces, which is seventy nine (79) less than required by Table E6.1.**

- Table E6.1 the following requirements for parking provision related to visitor accommodation and hotel industry:

Visitor accommodation – 1 car parking space for each bedroom; or alternatively  
Serviced apartment – 1 car parking space for each serviced apartment unit; and  
Hotel industry – 1 car parking space for each 3 m<sup>2</sup> of public bar room floor area.  
Accordingly, the proposal has a statutory requirement to provide:

Visitor accommodation – 33 spaces

Bar – 46 spaces

Total required parking spaces: 79

Performance Criteria - P1: - **ACCEPTED AS MEETING THE PERFORMANCE CRITERIA**

The number of on-site car parking spaces must be sufficient to meet the reasonable needs of users, having regard to all of the following:

**The proponent has submitted a Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1**

(a) car parking demand;

**- The Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1 states the below in relation (a) in principal councils development engineering officer accepts the statement provided by the proponents traffic expert to meet p1 (a)**

**It is anticipated that the actual car parking demands would be substantially lower than the statutory requirement, as discussed in Section 3.1. The site is within walking distance to key tourist and business areas, such as Salamanca, Hobart CBD, Sullivans Cove, etc., which lends itself to guests walking to/from these areas instead of using private vehicles. Furthermore, the southern boundary of the 'Central Business Zone' is situated on the northern side of Davey Street, only about 60m from the subject site. Acceptable Solution A1 to Clause E6.6.5 (Number of Car Parking Spaces – Central Business Zone) includes the provision of no on-site car parking for developments in this area and given the site's proximity to the central business zone, it is anticipated that the proposal will exhibit parking characteristics and demands similar to developments within this area.**

***In relation to proposed hotel/serviced apartments, no on-site car parking is provided when considering the fact that the majority of parking within the surrounding area is typically subject to short-term (paid/unpaid) parking restrictions, particularly on weekdays/Saturdays. Any guests who wish to drive to/from the site will not have many suitable car parking options to use; therefore, guests will be discouraged from driving to/from the site and will rely on other modes of transport (such as public transport, taxi, Uber, etc.) to access the site.***

***It is also noted that as part of the facilities operational management plan (i.e., online booking platform, hotel website, or phone booking), rooms booked by guests are typically advised of different ways available to access the site. If suitable on-site/off-site parking is not available, guests will therefore know in advance that driving/parking a vehicle in the surrounding area would not be suitable and adjust accordingly. In other words, if they require space and are unwilling to book ahead, they may book another hotel***

***Based on the foregoing, the proposed serviced apartments component of the development is therefore not projected to generate any parking demands associated with guests and/or staff (as staff parking is typically long-term in nature and there is no long-term on-street parking available within the surrounding area). Guests are expected to arrive by public transport or via taxis, Ubers, etc., with these drivers capable of using any of the available nearby on-street parking resources to drop-off and/or pick-up guests, including the proposed pick-up/drop-off bay on Wilmot Street near its intersection with Sandy Bay Road.***

***With respect to the rooftop bar, the location and setting of the bar, as well as the likely travel characteristics of patrons should be accounted for. The rooftop bar will be located on the top floor of the proposed development, which itself is centrally within Hobart and therefore situated in close proximity to numerous attractions. Furthermore, the bar will be located on the top floor of a hotel and as such a portion of its patronage will include hotel guests. It is also noted that it is common practice for the majority of bar patrons not to drive when consuming alcohol and instead opt for carpooling (with a 'designated driver'), taxis/rideshare, Uber, etc. As such, an adjusted parking requirement of 1 car parking space for each 30 m<sup>2</sup> public bar room floor area (i.e., 10% of the statutory requirement) is considered appropriate for the rooftop bar in this instance, which means the proposed development is anticipated to generate a parking demand of up to five (5) car spaces associated with the rooftop bar. This demand would typically occur during weekday evenings.***

(b) the availability of on-street and public car parking in the locality;

- There is a relatively large supply of on-street parking in the surrounding road network. Much of the available parking is in the form of time-restricted parking, with authorised residents excepted. Observations indicate that there is a large pool of parking that would be available to meet the potential demands of visitor and overflow parking, particularly after normal working hours.

The Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1 states the below in relation (b) in principal councils development engineering officer accepts the statement provided by the proponents traffic expert to meet p1 (b)

*As detailed in Section 1.5, there is abundant on-street parking available during surveyed weekday periods in the vicinity of the site. Specifically, surveys undertaken during the Friday evening period showed there were no fewer than 83 vacant car spaces within the survey area (64% occupancy). The available on-street parking resources surrounding the site can therefore readily accommodate projected bar parking demands.*

(c) the availability and frequency of public transport within a 400m walking distance of the site;

- Metro Tasmania operate regular bus services along *sandy pay road and Davey street* which is within 400 metres of the subject site.

The Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1 states the below in relation (c) in principal councils development engineering officer accepts the statement provided by the proponents traffic expert to meet p1 (c)

*As detailed in Section 1.4, the site has very good access to public transport, with bus stops serving buses both to and from Hobart Interchange located within convenient walking distances – the furthest bus stop is about 140m walking distance from the site. Buses arrive at these stops in approximately 30–60-minute intervals, depending on time and day, with services being more frequent during peak periods. Furthermore, surrounding area encompasses a well-connected footpath network that adequately serves active transport alternatives which is further enhanced by recent provision of a bike lane on Davey Street between Sandy Bay Road and Linden Avenue. Additionally, e-scooters provide another transport option and physical parking bays provided at St Davids Park entrance about 80m walking distance from site ensures that this alternative is readily accessible.*

(d) the availability and likely use of other modes of transport;

- The site is located a convenient walking distance from shops, and services.

(e) the availability and suitability of alternative arrangements for car parking provision;

- No alternative parking provision is available or considered necessary.

(f) any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces;

- Not applicable.

(g) any car parking deficiency or surplus associated with the existing use of the land;

- Not applicable.

(h) any credit which should be allowed for a car parking demand deemed to have been provided in association with a use which existed before the change of parking requirement, except in the case of substantial redevelopment of a site;

- Not applicable.

(i) the appropriateness of a financial contribution in lieu of parking towards the cost of parking facilities or other transport facilities, where such facilities exist or are planned in the vicinity;

- Not applicable.

(j) any verified prior payment of a financial contribution in lieu of parking for the land;  
- **The City's current position is not to support a financial contribution in lieu of parking for developments.**

(k) any relevant parking plan for the area adopted by Council;  
- **Not applicable.**

(l) the impact on the historic cultural heritage significance of the site if subject to the Local Heritage Code; and  
- **The site is subject to the Local Heritage Code, but no on-site car park is proposed, meaning heritage significance is not impacted.**

(m) whether the provision of the parking would result in the loss, directly or indirectly, of one or more significant trees listed in the Significant Trees Code.  
- **No impact.**

**Councils Traffic engineering officer has made the following assessment**

*“Under the current planning scheme, the proposed short-term accommodation requires 33 parking spaces and the rooftop bar requires 46 parking spaces. It is important to note that the requirement of 46 parking spaces for the rooftop bar appears excessive given the nature of the development and its proximity to the Central Business Zone. Despite the apparent lack of on-site car parking, this is not identified as the primary concern for this development due to the proximity of the development to the Central Business zone (approximately 60m from the zone).*

*The assessment provided by SALT – Traffic Impact Assessment – demonstrates capacity in the availability of the on-street parking network to cater for the traffic generated by the development and for the pick up and drop off activity for the hotel development. Based on the above summary, there is no grounds to refuse the development applications, however, there are a number of concerns traffic engineering officers would like to raise in respect to this development.*

*During Salamanca Market days on Saturdays the parking demand in the Battery Point area is significantly high and this may make it difficult for guests to park and check in at those particular days.*

*Officers have raised concerns around compliance with the clearway zone on Sandy Bay Road fronting the proposed development which may present a significant potential issue when the short-term parking on Wilmot Street is utilised or drivers have turned into Sandy Bay Road to access the hotel and find that they are not able to turn right into Wilmot Street.*

*To mitigate conflicts arising from uninformed guests it is suggested that a communication strategy is developed that include the following:*

- (1) specific conditions regarding the communication of clearway zone times by the hotel should be stipulated. The clearway operates from 6:30 am to 9:30 am and 1:00 pm to 6:00 pm on weekdays, and from 9:00 am to 3:00 pm on Saturdays and compliance with the clearway is essential for the AM and PM traffic flow for Hobart's traffic network.*
- (2) Communicate to guests that there is no parking available off-street and provide information on parking available that is suitable for guests (i.e. nearby multi-storey car parks).*
- (3) Provide information on the route to access the short-term parking on Wilmot Street.*

*Furthermore, the existing loading zone times remain unaltered to ensure that the clearway is in operation during the peak period of traffic during the weekdays. A such, deliveries and rubbish collection for the development must be scheduled to occur between 9:30 am and 1:00 pm, Monday to Friday, aligning with the existing loading zone operational times.*

*Finally, the on-street parking is a public asset and therefore the proposed dedicated pick-up and drop-off zone cannot be exclusively for the hotels use; it must remain available for general public use and it will be sign posted as a 10 minute parking zone. With the development of 5-7 Sandy Bay Rd, it is possible to assume that the new short-term parking will be well highly utilised, especially for delivery drivers who want to pick up and drop off outside the period when the loading zone in Sandy Bay Road is in operation (during the clearway times)."*

Based on the documentation submitted to date and given the above assessment, the parking provision is accepted as meeting the *Performance Criteria P1:E6.6.1* of the Planning Scheme. This is particularly due to the actual parking demands that will be generated by the development.

#### REPRESENTATIONS:

Below is a summary of all representations related to Development engineering matters (parking and access) that were received during the advertising period

- *Inadequate loading zone: Too small and too far from the site to meet operational needs.*
- *Risk of encroachment: Deliveries may block or infringe on the narrow laneway and Right of Way.*
- *Lack of parking: No sufficient parking facilities, likely to increase congestion and traffic safety issues on:*
  - *Wilmot Street*
  - *Sandy Bay Road*
  - *Davey Street*
- *Insufficient rubbish storage and collection areas:*
  - *May lead to obstruction of the footpath and laneway Right of Way.*

#### Response to representations below

-Inadequate loading zone – too small and too far to meet operational needs.

#### Response:

- Waste collection will be handled by a private service provider, with bins transferred to the existing loading zone on Sandy Bay Road for kerbside pickup. (Condition ENG 13)
- The proposal includes time restricted pick-up/drop-off area on Wilmot Street, approximately 100m from the site, designed to accommodate up to two vehicles
- Whilst the proposal is not reliant on the outward delivery of goods, inward bound goods, such as linen and Food and beverage will be required. It is proposed that deliveries will occur via the loading zone situated within 50m of the site, along Sandy Bay Road. Therefore, Commercial movements associated with the proposal can comply with A1 of the HIPS

-Risk of encroachment on the laneway and Right of Way due to deliveries.

Response:

- The proposal and Salt traffic impact assessment does not propose any use of the laneway for deliveries or access.
- All drop-offs and waste collection are planned via Wilmot Street and Sandy Bay Road, minimizing risk of encroachment.

-Lack of parking will increase congestion and traffic safety issues on nearby streets.

Response:

- Guests will be informed to use public transport, taxis, Uber, or e-scooters, reducing reliance on private vehicles.
- Parking surveys (SALT Traffic Impact assessment) show sufficient on-street availability, especially during weekday evenings
- The development provides no on-site parking, but this is consistent with some other Hobart hotels located within the nearby CBZ
- Under the HIPS, the proposed short-term accommodation requires 33 parking spaces and the rooftop bar requires 46 parking spaces. It is important to note that the requirement of 46 parking spaces for the rooftop bar has been determined to be excessive given the nature of the development and its proximity to the Central Business Zone. Despite the lack of on-site car parking, this is not identified as the primary concern for this development due to the proximity of the development to the Central Business zone (approximately 60m from the zone).
- The report provided by SALT – Traffic Impact Assessment – demonstrates capacity in the availability of the on-street parking network to cater for the traffic generated by the development and for the pick up and drop off activity for the hotel development.

-Rubbish storage and collection zones are inadequate and may encroach on footpaths and laneway.

Response:

- Waste will be collected by a private contractor using the existing loading zone on Sandy Bay Road.
- Bins will be transferred only on collection days, and no permanent encroachment is planned.
- No use of the laneway or footpath for storage is indicated in the proposal.

#### COMMENTS:

Due to the scope of the proposal, the application has been referred to the Council's Manager Waterways, Manager City Infrastructure, Manager City Mobility, Manager Open Space and Manager

Surveying Services. The delegated officers' responses, including recommendations are inserted in the respective referral reports.

**CONDITIONS:**

- ENG 1 – Protection of Council assets
- ENG 12: Construction waste management
- ENGTR2 - Construction Traffic and Parking Management
- ENG s1: On-street Parking
- ENG s2 - Commercial vehicle movements

**ADVICE:**

- Dial before you dig
- Fees and charges
- Building Permit
- Plumbing Permit
- Condition endorsement

**DETAILED ASSESSMENT:**

E5.0 Road and Railway Assets Code

E5.2 Application of this Code	YES	This Code applies to use or development of land:
		(a) that will require a new vehicle crossing, junction or level crossing; or
		(b) that intensifies the use of an existing access; or
		(c) that involves a sensitive use, a building, works or subdivision within 50m metres of a Utilities zone that is part of:
		(i) a rail network;
		(ii) a category 1 - Trunk Road or a category 2 - Regional Freight Road, that is subject to a speed limit of more than 60km/h kilometres per hour.

Clause for Assessment	Comments / Discussion (In bold)
<b>Clause E5.5.1:</b> Existing road accesses and junctions NOT APPLICABLE	Documentation submitted to date does not invoke clause E5.5.1 (A3). No intensification of existing road accesses and/or junctions proposed.
<b>Clause E5.5.2:</b> Existing level crossings NOT APPLICABLE	Documentation submitted to date does not invoke clause E5.5.2. No intensification of an existing level crossings proposed.

<b>Clause E5.6.1:</b> Development adjacent to roads and railways NOT APPLICABLE	<u>Documentation submitted to date does not invoke clause E5.6.1.</u> No development adjacent to category 1 or category 2 road proposed.
<b>Clause E5.6.2:</b> Road accesses and junctions NOT APPLICABLE	<u>Documentation submitted to date does not invoke clause E5.6.2.</u> No new accesses or access junctions proposed.
<b>Clause E5.6.3:</b> New level crossings NOT APPLICABLE	<u>Documentation submitted to date does not invoke clause E5.6.3.</u> No new level crossings proposed.
<b>Clause E5.6.4:</b> Sight distance at accesses, junctions and level crossings NOT APPLICABLE	<u>Documentation submitted to date does not invoke clause E5.6.4.</u> No new accesses (road) and/or junctions proposed.

## E6.0 Parking and Access Code

<b>E6.2 Application of this Code</b>	This code applies to all use and development.
<b>Clause for Assessment</b>	<b>Comments / Discussion (In Bold)</b>
<b>Clause(s) 6.6's</b> are all to do with parking number assessment  <b>PERFORMANCE CRITERIA</b>	<p>The parking number assessment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the <i>Hobart Interim Planning Scheme 2015 (HIPS 2015)</i>.</p> <p><b><u>Documentation submitted to date does not comply with the Acceptable Solution, therefore assessment against the Performance Criterion is relied on for clause E6.6.1 (a).</u></b></p> <p>Acceptable solution - A1: - <b><u>DOES NOT COMPLY: The proposal includes 0on-site car parking spaces, which is seventy nine (79) less than required by Table E6.1.</u></b></p> <p>The number of on-site car parking spaces must be:</p> <p>(a) No less than the number specified in Table E6.1, minus the number of car parking spaces that cannot be provided due to the site including container refund scheme space;</p> <p>Except if:</p> <p>(i) The site is subject to a parking plan for the area adopted by Council, in which case parking provision (spaces or cashin-lieu) must be in accordance with that plan.</p> <p>(ii) The site is subject to clauses E6.6.5, E6.6.6, E6.6.7, E6.6.8, E6.6.9 or E6.6.10 of this planning scheme.</p> <p>- <b>Table E6.1 the following requirements for parking provision related to visitor accommodation and hotel industry:</b></p> <p><b>Visitor accommodation – 1 car parking space for each bedroom; or alternatively Serviced apartment – 1 car parking space for each serviced apartment unit; and Hotel industry – 1 car parking space for each 3 m<sup>2</sup> of public bar room floor area. Accordingly, the proposal has a statutory requirement to provide:</b></p>

Visitor accommodation – 33 spaces  
Bar – 46 spaces  
Total required parking spaces: 79

Performance Criteria - P1: - **ACCEPTED AS MEETING THE PERFORMANCE CRITERIA**

The number of on-site car parking spaces must be sufficient to meet the reasonable needs of users, having regard to all of the following:

**The proponent has submitted a Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1**

(a) car parking demand;  
- **The Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1 states the below in relation (a) in principal councils development engineering officer accepts the statement provided by the proponents traffic expert to meet p1 (a)**

**It is anticipated that the actual car parking demands would be substantially lower than the statutory requirement, as discussed in Section 3.1. The site is within walking distance to key tourist and business areas, such as Salamanca, Hobart CBD, Sullivans Cove, etc., which lends itself to guests walking to/from these areas instead of using private vehicles. Furthermore, the southern boundary of the 'Central Business Zone' is situated on the northern side of Davey Street, only about 60m from the subject site. Acceptable Solution A1 to Clause E6.6.5 (Number of Car Parking Spaces – Central Business Zone) includes the provision of no on-site car parking for developments in this area and given the site's proximity to the central business zone, it is anticipated that the proposal will exhibit parking characteristics and demands similar to developments within this area.**

***In relation to proposed hotel/serviced apartments, no on-site car parking is provided when considering the fact that the majority of parking within the surrounding area is typically subject to short-term (paid/unpaid) parking restrictions, particularly on weekdays/Saturdays. Any guests who wish to drive to/from the site will not have many suitable car parking options to use; therefore, guests will be discouraged from driving to/from the site and will rely on other modes of transport (such as public transport, taxi, Uber, etc.) to access the site.***

***It is also noted that as part of the facilities operational management plan (i.e., online booking platform, hotel website, or phone booking), rooms booked by guests are typically advised of different ways available to access the site. If suitable on-site/off-site parking is not available, guests will therefore know in advance that driving/parking a vehicle in the surrounding area would not be suitable and adjust accordingly. In other words, if they require space and are unwilling to book ahead, they may book another hotel***

***Based on the foregoing, the proposed serviced apartments component of the development is therefore not projected to generate any parking demands associated with guests and/or staff (as staff parking is typically long-term in nature and there is no long-term on-street parking available within the surrounding area). Guests are expected to arrive by public transport or via taxis, Ubers, etc., with these drivers capable of using any of the available nearby on-street parking resources to drop-off and/or pick-up guests, including the proposed pick-up/drop-off bay on Wilmot Street near its intersection with Sandy Bay Road.***

*With respect to the rooftop bar, the location and setting of the bar, as well as the likely travel characteristics of patrons should be accounted for. The rooftop bar will be located on the top floor of the proposed development, which itself is centrally within Hobart and therefore situated in close proximity to numerous attractions. Furthermore, the bar will be located on the top floor of a hotel and as such a portion of its patronage will include hotel guests. It is also noted that it is common practice for the majority of bar patrons not to drive when consuming alcohol and instead opt for carpooling (with a 'designated driver'), taxis/rideshare, Uber, etc. As such, an adjusted parking requirement of 1 car parking space for each 30 m<sup>2</sup> public bar room floor area (i.e., 10% of the statutory requirement) is considered appropriate for the rooftop bar in this instance, which means the proposed development is anticipated to generate a parking demand of up to five (5) car spaces associated with the rooftop bar. This demand would typically occur during weekday evenings.*

(b) the availability of on-street and public car parking in the locality;

- **There is a relatively large supply of on-street parking in the surrounding road network. Much of the available parking is in the form of time-restricted parking, with authorised residents excepted. Observations indicate that there is a large pool of parking that would be available to meet the potential demands of visitor and overflow parking, particularly after normal working hours.**

**The Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1 states the below in relation (b) in principal councils development engineering officer accepts the statement provided by the proponents traffic expert to meet p1 (b)**

*As detailed in Section 1.5, there is abundant on-street parking available during surveyed weekday periods in the vicinity of the site. Specifically, surveys undertaken during the Friday evening period showed there were no fewer than 83 vacant car spaces within the survey area (64% occupancy). The available on-street parking resources surrounding the site can therefore readily accommodate projected bar parking demands.*

(c) the availability and frequency of public transport within a 400m walking distance of the site;

- **Metro Tasmania operate regular bus services along *sandy pay road and Davey street* which is within 400 metres of the subject site.**

**The Traffic impact assessment By SALT (dated September 2025) to address the performance criteria P1 states the below in relation (c) in principal councils development engineering officer accepts the statement provided by the proponents traffic expert to meet p1 (c)**

*As detailed in Section 1.4, the site has very good access to public transport, with bus stops serving buses both to and from Hobart Interchange located within convenient walking distances – the furthest bus stop is about 140m walking distance from the site. Buses arrive at these stops in approximately 30–60-minute intervals, depending on time and day, with services being more frequent during peak periods. Furthermore, surrounding area encompasses a well-connected footpath network that adequately serves active transport alternatives which is further enhanced by recent provision of a bike lane on Davey Street between Sandy Bay Road and Linden Avenue. Additionally, e-scooters provide another transport option and physical parking bays provided at St Davids Park entrance about 80m walking distance from site ensures that this alternative is readily accessible.*

- (d) the availability and likely use of other modes of transport;  
- **The site is located a convenient walking distance from shops, and services.**
- (e) the availability and suitability of alternative arrangements for car parking provision;  
- **No alternative parking provision is available or considered necessary.**
- (f) any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces;  
- **Not applicable.**
- (g) any car parking deficiency or surplus associated with the existing use of the land;  
- **Not applicable.**
- (h) any credit which should be allowed for a car parking demand deemed to have been provided in association with a use which existed before the change of parking requirement, except in the case of substantial redevelopment of a site;  
- **Not applicable.**
- (i) the appropriateness of a financial contribution in lieu of parking towards the cost of parking facilities or other transport facilities, where such facilities exist or are planned in the vicinity;  
- **Not applicable.**
- (j) any verified prior payment of a financial contribution in lieu of parking for the land;  
- **The City's current position is not to support a financial contribution in lieu of parking for developments.**
- (k) any relevant parking plan for the area adopted by Council;  
- **Not applicable.**
- (l) the impact on the historic cultural heritage significance of the site if subject to the Local Heritage Code; and  
- **The site is subject to the Local Heritage Code, but no on-site car park is proposed, meaning heritage significance is not impacted.**
- (m) whether the provision of the parking would result in the loss, directly or indirectly, of one or more significant trees listed in the Significant Trees Code.  
- **No impact.**

**Councils Traffic engineering officer has made the following assessment**

**“Under the current planning scheme, the proposed short-term accommodation requires 33 parking spaces and the rooftop bar requires 46 parking spaces. It is important to note that the requirement of 46 parking spaces for the rooftop bar appears excessive given the nature of the development and its proximity to the Central Business Zone. Despite the apparent lack of on-site car parking, this is not identified as the primary concern for this development due to the proximity of the development to the Central Business zone (approximately 60m from the zone).**

**The assessment provided by SALT – Traffic Impact Assessment – demonstrates capacity in the availability of the on-street parking network to cater for the traffic generated by the development and for the pick up and drop off activity for the hotel development. Based on the above summary, there is no grounds to refuse the development applications, however, there are a number of concerns traffic engineering officers would like to raise in respect to this development.**

	<p>During Salamanca Market days on Saturdays the parking demand in the Battery Point area is significantly high and this may make it difficult for guests to park and check in at those particular days.</p> <p>Officers have raised concerns around compliance with the clearway zone on Sandy Bay Road fronting the proposed development which may present a significant potential issue when the short-term parking on Wilmot Street is utilised or drivers have turned into Sandy Bay Road to access the hotel and find that they are not able to turn right into Wilmot Street.</p> <p>To mitigate conflicts arising from uninformed guests it is suggested that a communication strategy is developed that include the following:</p> <ul style="list-style-type: none"> <li>(4) specific conditions regarding the communication of clearway zone times by the hotel should be stipulated. The clearway operates from 6:30 am to 9:30 am and 1:00 pm to 6:00 pm on weekdays, and from 9:00 am to 3:00 pm on Saturdays and compliance with the clearway is essential for the AM and PM traffic flow for Hobart’s traffic network.</li> <li>(5) Communicate to guests that there is no parking available off-street and provide information on parking available that is suitable for guests (i.e. nearby multi-storey car parks).</li> <li>(6) Provide information on the route to access the short-term parking on Wilmot Street.</li> </ul> <p>Furthermore, the existing loading zone times remain unaltered to ensure that the clearway is in operation during the peak period of traffic during the weekdays. A such, deliveries and rubbish collection for the development must be scheduled to occur between 9:30 am and 1:00 pm, Monday to Friday, aligning with the existing loading zone operational times.</p> <p>Finally, the on-street parking is a public asset and therefore the proposed dedicated pick-up and drop-off zone cannot be exclusively for the hotels use; it must remain available for general public use and it will be sign posted as a 10 minute parking zone. With the development of 5-7 Sandy Bay Rd, it is possible to assume that the new short-term parking will be well highly utilised, especially for delivery drivers who want to pick up and drop off outside the period when the loading zone in Sandy Bay Road is in operation (during the clearway times).”</p> <p>Based on the documentation submitted to date and given the above assessment, the parking provision is accepted as meeting the <i>Performance Criteria P1:E6.6.1</i> of the Planning Scheme. This is particularly due to the actual parking demands that will be generated by the development.</p>
<p><b>Clause E6.7.1:</b>                  Number of vehicle accesses</p> <p style="text-align: center;">NOT                  APPLICABLE</p>	<p>The design of the vehicle access must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p><u>Documentation submitted to date does not invoke clause E6.7.1.</u></p> <p>Documentation submitted to date does not indicate any changes proposed to the number of vehicle accesses.</p>
<p><b>Clause E6.7.2:</b>                  Design of vehicle accesses</p>	<p>The design of the vehicle access must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p>

NOT APPLICABLE	<p>Documentation submitted to date does not invoke clause E6.7.2.</p> <p>Documentation submitted to date does not indicate any vehicle access requirement.</p> <p>Documentation submitted to date does not indicate any changes proposed to the existing vehicle access.</p>
<p><b>Clause E6.7.3:</b> Vehicle passing area along an access</p> <p>NOT APPLICABLE</p>	<p>Vehicle passing must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p>Documentation submitted to date does not invoke clause E6.7.3.</p> <p>Documentation submitted to date does not indicate any facility / requirement for vehicle passing.</p> <p>Acceptable solution - A1: Vehicular passing areas must: (a) be provided if any of the following applies to an access: (i) it serves more than 5 car parking spaces; - <b>No</b> (ii) is more than 30 m long; - <b>No</b> (iii) it meets a road serving more than 6000 vehicles per day; - <b>No</b> (b) be 6 m long, 5.5 m wide, and taper to the width of the driveway; - <b>N/A</b> (c) have the first passing area constructed at the kerb; - <b>N/A</b> (d) be at intervals of no more than 30 m along the access. - <b>N/A</b></p>
<p><b>Clause E6.7.4:</b> On-site turning</p> <p>NOT APPLICABLE</p>	<p>On-site turning must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p>Documentation submitted to date does not invoke clause E6.7.4.</p> <p>Acceptable solution - A1: On-site turning must be provided to enable vehicles to exit a site in a forward direction, except where the access complies with any of the following: (a) it serves no more than two dwelling units; - <b>COMPLIES</b> (b) it meets a road carrying less than 6000 vehicles per day. - <b>COMPLIES</b></p> <p>Documentation submitted to date does not indicate any facility / requirement for on-site turning.</p>
<p><b>Clause E6.7.5:</b> Layout of parking areas</p> <p>NOT APPLICABLE</p>	<p>The layout of the parking area must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p>Documentation submitted to date does not invoke clause 6.7.5.</p> <p>Documentation submitted to date indicates no new parking area(s).</p>
<p><b>Clause E6.7.6:</b> Surface treatment of parking areas</p> <p>NOT APPLICABLE</p>	<p>The surface treatment must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p>Documentation submitted to date does not invoke clause E6.7.6.</p> <p>Documentation submitted to date indicates no new hard stand area(s) proposed or new development within a car parking area.</p>

<p><b>Clause E6.7.7:</b> Lighting of parking areas</p> <p>NOT APPLICABLE</p>	<p>The lighting of parking areas must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p><b>Documentation submitted to date does not invoke clause E6.7.7.</b></p> <p><u>Acceptable Solution A1:</u> Parking and vehicle circulation roadways and pedestrian paths serving 5 or more car parking spaces, used outside daylight hours, must be provided with lighting in accordance with clause 3.1 "Basis of Design" and clause 3.6 "Car Parks" in AS/NZS 1158.3.1:2005 Lighting for roads and public spaces Part 3.1: Pedestrian area (Category P) lighting.</p> <p><b>Documentation submitted to date indicates no new (&gt; 5 spaces) parking area(s).</b></p> <p><i>(Planner to assess)</i></p>
<p><b>Clause E6.7.8:</b> Landscaping of parking areas</p> <p>NOT APPLICABLE</p>	<p>The landscaping of parking areas must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p><b>Documentation submitted to date does not invoke clause E6.7.8.</b></p> <p><u>Acceptable Solution A1:</u> Landscaping of parking and circulation areas must be provided where more than 5 car parking spaces are proposed. This landscaping must be no less than 5 percent of the area of the car park, except in the Central Business Zone where no landscaping is required.</p> <p><b>Documentation submitted to date indicates no new (&gt; 5 spaces) parking area(s).</b></p> <p><i>(Planner to assess)</i></p>
<p><b>Clause E6.7.9:</b> Design of motorcycle parking areas</p> <p>NOT APPLICABLE</p>	<p>The motor bike parking must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p><b>Documentation submitted to date does not invoke clause E6.7.9.</b></p> <p><u>Acceptable Solution A1 (E6.6.3):</u> The number of on-site motorcycle parking spaces provided must be at a rate of 1 space to each 20 car parking spaces after the first 19 car parking spaces except if bulky goods sales, (rounded to the nearest whole number). Where an existing use or development is extended or intensified, the additional number of motorcycle parking spaces provided must be calculated on the amount of extension or intensification, provided the existing number of motorcycle parking spaces is not reduced.</p> <p><b>NO REQUIREMENT (&lt;19 car parking spaces).</b></p>
<p><b>Clause E6.7.10:</b> Design of bicycle parking areas</p> <p>NOT APPLICABLE</p>	<p>The bicycle parking must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p><b>Documentation submitted to date does not invoke clause E6.7.10.</b></p> <p><u>Acceptable Solution A1:</u> The number of on-site bicycle parking spaces provided must be no less than the number specified in Table E6.2.</p> <p><u>Acceptable Solution A2:</u> The design of bicycle parking spaces must be to the class specified in table 1.1 of AS2890.3-1993 Parking facilities Part 3: Bicycle parking facilities in compliance with section 2 "Design of Parking Facilities" and clauses 3.1 "Security" and 3.3 "Ease of Use" of the same Standard.</p>

	<p>User Class: <b>Residential</b></p> <p><b>NO REQUIREMENT</b></p> <p>Table E6.2 sets out the number of bicycle parking spaces required. The requirement for spaces for a use or development listed in the first column of the table is set out in the second and fourth columns of the table with the corresponding class set out in the third and fifth columns. If the result is not a whole number, the required number of (spaces) is the nearest whole number. If the fraction is one-half, the requirement is the next whole number.</p>
<p><b>Clause E6.7.11:</b> Bicycle end trip facilities</p> <p><b>NOT APPLICABLE</b></p>	<p>The bicycle end trip facilities must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <b>Documentation submitted to date does not invoke clause E6.7.11.</b></p> <p><u>Acceptable Solution A1:</u> For all new buildings where the use requires the provision of more than 5 bicycle parking spaces for employees under Table E6.2, 1 shower and change room facility must be provided, plus 1 additional shower for each 10 additional employee bicycle spaces thereafter.</p> <p><b>Documentation submitted to date indicates no new (&gt; 5 spaces) bicycle parking area(s).</b></p> <p><i>(Planner to assess)</i></p>
<p><b>Clause 6.7.12:</b> Siting of car parking</p> <p><b>NOT APPLICABLE</b></p>	<p>The siting of car parking must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <b>Documentation submitted to date does not invoke clause E6.7.12.</b></p> <p><u>Acceptable Solution A1:</u> Parking spaces and vehicle turning areas, including garages or covered parking areas in the Inner Residential Zone, Urban Mixed Use Zone, Village Zone, Local Business Zone and General Business Zone must be located behind the building line of buildings located or proposed on a site except if a parking area is already provided in front of the building line of a shopping centre.</p> <p><b>Documentation submitted to date indicates no new parking area(s) within the respective zones.</b></p> <p><i>(Planner to assess based on DE assessment of Clause 6.7.5 layout of parking area)</i></p>
<p><b>Clause E6.7.13:</b> Facilities for commercial vehicles</p> <p><b>ACCEPTABLE SOLUTION</b></p>	<p>The facilities for commercial vehicles must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <b>Documentation submitted to date does comply with the Acceptable Solution for clause E6.7.13.</b></p> <p><u>Acceptable Solution A1: - DOES COMPLY</u> Commercial vehicle facilities for loading, unloading or manoeuvring must be provided on-site in accordance with Australian Standard for Off-street Parking, Part 2 : Commercial. Vehicle Facilities AS 2890.2:2002, unless: (a) the delivery of all inward bound goods is by a single person from a vehicle parked in a dedicated loading zone within 50 m of the site; and (b) the use is not primarily dependent on outward delivery of goods from the site.</p>

<p><b>Clause E6.7.14:</b> Access to a road</p> <p>NOT APPLICABLE</p>	<p>The access to a road must satisfy the Acceptable Solutions of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <u>Documentation submitted to date does not invoke clause E6.7.14.</u></p> <p>Documentation submitted to date does not indicate any changes proposed to the existing access to a road.</p>
<p><b>Clause E6.7.15:</b> Access to Niree Lane Sandy Bay</p> <p>NOT APPLICABLE</p>	<p>The access to Niree Lane must comply with the Acceptable Solutions or meet the Performance Criteria (where applicable) for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <u>Documentation submitted to date does not invoke clause E6.7.15.</u></p> <p>Documentation submitted to date does not indicate any proposed development within Niree Lane.</p>

**Application Referral Cultural Heritage Response**

From:	Gray Planning obo Senior Cultural Heritage Officer
Recommendation:	Proposal is acceptable subject to conditions
Date Completed:	16 July 2025
Address:	3 SANDY BAY RD HOBART TAS 7000
Proposal:	Partial Demolition, Alterations, Extension, and Partial Change of Use to Visitor Accommodation and Hotel Industry
Application Reference:	PLN-HOB-2021-0710
External Reference:	PLN-21-710
Assessment Officer:	Christopher Phu

**Referral comments:****Overview:**

The application seeks approval for a new Hotel Industry use to be housed within a new building located over, and to the rear of the Masonic Temple building.

The development involves the property 3 Sandy Bay Road, Hobart.

The Masonic Temple building was constructed between 1937 and 1939 for the purposes of a Masonic meeting place for the use of multiple lodges and remains in that original use to the current time.

The property 3 Sandy Bay Road is listed as a Heritage Place in Table E13.1 under the Historic Heritage Code. The property is also listed on the Tasmanian Heritage Register (THR).

The property (subject site) is also listed within a Place of Archaeological sensitivity.

The subject site is not contained within a Heritage Precinct.

The proposal involves demolition, alteration and new works at the subject site.

**Brief Description of the Proposal:**

The proposal includes partial internal and external demolition of the Masonic Temple building and the construction of a new hotel building to the rear of, and over the Temple building to a height of 34.3m above NGL. This new hotel building is set back 7m from the Sandy Bay Road frontage boundary.

The application seeks approval for 38 short stay commercial accommodation (Hotel Industry) units with a roof-top bar across 7 additional levels.

The proposal also includes internal and external modifications to the Masonic Temple building to provide for a lobby and reception area and associated works.

Owing to the narrowness of the subject site, structural elements to support the new building will straddle the existing Masonic Temp building with supporting elements constructed partially outside the subject site onto 5-7 Sandy Bay Road and 2A Heathfield Avenue.

The primary Sandy Bay Road street facing façade of the new building will be primarily clad in glazing but with metal 'brass' screening elements in a checkerboard configuration across the façade. These have been intended to 'break up' the façade and provide a degree of textural façade treatment with vertical elements rather than a wall of glazing that faces Sandy Bay Road.

Side walls will be clad in zinc panels.

The proposed new building to be set behind the Masonic Temple is described as being 4.5 storeys higher than the existing Masonic Temple building.

#### **Brief Description of the Heritage Elements:**

The Masonic Temple site was formerly part of the Heathfield Estate with the dwelling 'Heathfield' still existing on a much reduced lot now only fronting Davey Street.

The Heathfield Estate was developed by the early 1830's and the land now occupied by 3 Sandy Bay Road was part of the Estate gardens. This part of the property may have been used for the purposes of a small nursery and may have contained minor infrastructure related to the nursery use such as sheds and fencing. Historical review by the proponent's heritage consultant Praxis has uncovered historical images and mapping of the subject site that has not included any evidence of any earlier buildings or structures within the land that now makes up the subject site.

The Heathfield Estate was sold in 1920 and subsequently developed into 20 lots.

In 1937 one of these lots was sold to the Freemasons who had outgrown their nearby Murray Street premises.

Later in the year, The Mercury reported that Architect Lauriston Crisp had drawn plans for a new Masonic Hall in Harrington Street (as the section of Sandy Bay Road was then called) with the foundation stone of the Masonic Temple laid in March 1938. The building was completed by January 1939.

The building since then has had virtually no changes both internal and external and remains substantially intact with the original Masonic use for the Freemasonry movement still in operation.

The architectural style of the building is 'Stripped Classicism' which was a building style often associated with large civic projects in the Interwar period in the early to mid 20<sup>th</sup> century. Stripped Classicism exhibited a pared back architectural style largely devoid of decoration and draws on Art Deco for inspiration, particularly in using vertical façade and simplified elements generally free of architectural ornamentation.

The Temple building at 3 Sandy Bay Road exhibits these vertical elements in its Sandy Bay Road fronting façade and side facades and uses a sandstone hue brick with red brick used toward the rear of the site. The façade has minimal articulation and is constructed of a single external construction material of cream coloured brick which increases its visual bulk and presence within the streetscape.

The building is double storey but equivalent to 3 or 4 storeys in form in terms of the overall height of the building and employs the use of high ceilings in its internal layout. This accentuated height is common in masonic lodge buildings.

The building, according to the Heritage Impact Assessment prepared by Praxis Environment, retains the majority of internal detailing and floor plan and is remarkably intact.

The form of the building, like many Masonic temples or halls, is substantial in form, mass and bulk.

Due to neighbouring land being developed close, or on shared title boundaries, its visual prominence in the streetscape is restricted to views directly opposite or along Sandy Bay Road or Harrington Street within a few hundred metres. Nonetheless, the front facade of the building provides a strongly defined façade that directly, and severely, addresses the street frontage that is not of a pedestrian scale. The façade is considered imposing and grand in its proportions.

The building is particularly substantial in scale and mass and bulk as far as Masonic buildings are concerned and it is likely the architect intended the scale to appropriately reflect the inner city location and importance of the Temple, as opposed to smaller and less visually intimidating Masonic buildings that are more frequently seen in country towns and rural locations.

The Masonic Temple located at 3 Sandy Bay Road has multiple historic and cultural heritage values that include:

- A rare example of Stripped Classicism architecture;
- Represents the work of architect Lauriston Crisp;
- Associated with the order of Freemasonry movement and resulting construction of buildings to house Masonic Lodges in Tasmania in the 20<sup>th</sup> century. The Temple has continuously operated as a meeting place for Freemasons from the premises since the building's completion in 1939; and
- The Temple is a substantially intact building Interwar Stripped Classicism building which includes virtually all internal original detailing and original floor plan layout

intact as well as maintaining its original external construction façade and setting in the streetscape.

The Temple building is not considered to be a visually dominant building in the streetscape overall owing to larger, bulkier and substantially taller buildings that have been constructed either side during the 20<sup>th</sup> century and these neighbouring buildings largely shroud the Temple building from larger and wider public views within the streetscape.

**Scheme Provisions:**

The following provisions of the Planning Scheme (*Hobart Interim Planning Scheme 2015*) apply to the application:

- E13.7.1.P1 Demolition to a Heritage Place;
- E13.7.1.P1, P2 and P3 Buildings and Works other than Demolition to a Heritage Place;
- E13.10.1.P1 Development Standards for Places of Archaeological Potential

The following heritage focused documentation was submitted by the applicant in support of the application:

*Conservation Management Policy, Heritage Impact Assessment & Statement of Compliance* prepared by Praxis Environment (author Brad Williams) dated December 2020.

Mr Williams is an archaeologist as well as heritage consultant.

**Site inspection:**

The subject site was inspected by the report author on 15 July 2025.

This inspection included an inspection of the interior of the building, a site inspection around the perimeters of the building and inspection of the area surrounding the building in order to appreciate the presence of the Temple building in the streetscape from various vantage points.

The Temple building is not considered to be a highly visible or dominant building in the landscape owing to neighbouring development which largely and progressively shrouds the Temple building the further one moves away from the subject site along either Sandy Bay Road or Harrington Street.

The construction of the proposed development will be set back 7m from the Sandy Bay Road frontage of the subject site and will therefore comfortably sit within the background of the subject site.

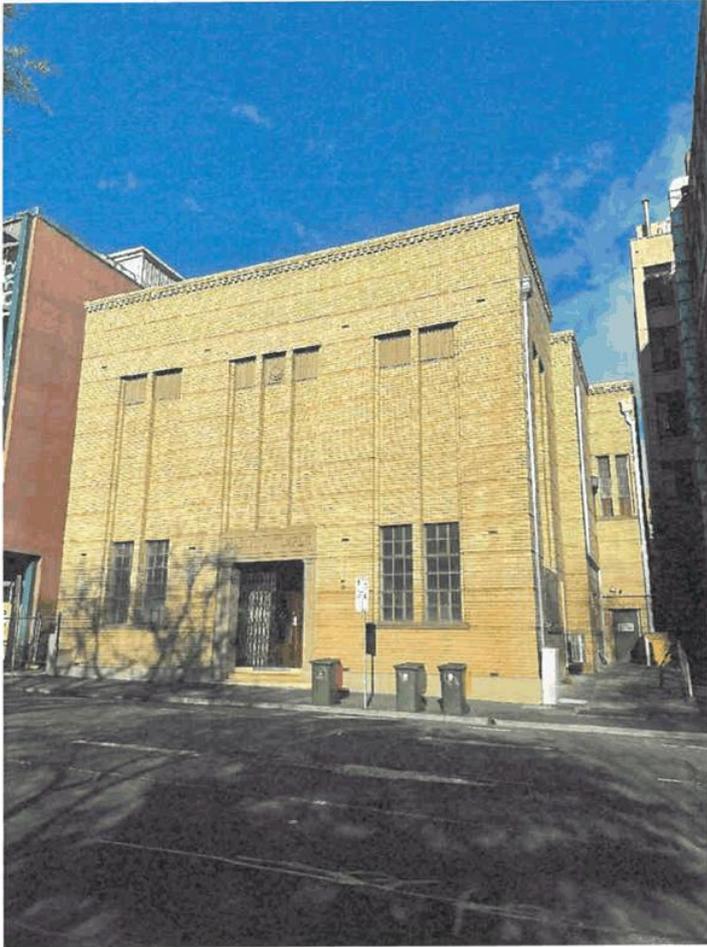
The proposed development when viewed on the photo montages provided by the project architect will not present as a dominant feature in the streetscape owing to the neighbouring buildings and the proposed height which seeks to be of a comparable height to neighbouring buildings.

The location of the proposed development is considered to be set back a sufficient distance to enable the Temple building to be the visually dominant building on the subject site when viewed from within Sandy Bay Road, Harrington Street and St David's Park.

The following images are provided as part of this inspection:



**Image.1. View looking north west along Sandy Bay Road to the intersection with Harrington Street and Davey Street. Taken 15 July 2025. No image modification.**



**Image.2. View toward the Temple façade from within Sandy Bay Road. Taken 15 July 2025. No image modification.**



**Image.3. View looking north west along Sandy Bay Road to the intersection with Harrington Street and Davey Street. The Temple building primarily appears as a two dimensional element in the streetscape. Taken 15 July 2025. No image modification.**



**Image.4. View looking south east along Sandy Bay Road from the Macquarie and Harrington Street intersection. Taken 15 July 2025. No image modification.**

**Representations:**

The proposal received multiple representations, one of which raised heritage and streetscape based objections.

A total of 29 representations were received during the public notification period, all of which objected to the development.

Many of the concerns relate to impact on the adjacent apartments at 1 Sandy Bay Road.

One objection outlined concerns that included impact on streetscape and heritage values.

The following heritage and streetscape related issues were raised:

- “The Masonic Hall is a heritage-listed building with a unique architectural style. Adding significant height above it diminishes its heritage prominence within the streetscape. Despite the setback measures proposed, the additional levels introduce a visual bulk that overshadows the hall's historic character.”
- “Approval of this height increase sets a concerning precedent for future developments in Hobart. Such developments risk undermining the city's careful balance of heritage preservation and modern urban design.”
- “I urge the council to reject the proposed height increase to safeguard the amenity and heritage value of the area.”

The above issues are discussed below in the assessment of the proposal against triggered Planning Scheme development standards.

**Provision Assessment:**

The proposal seeks approval for demolition and new works to a Heritage Place and works within a Place of Archaeological sensitivity and has been accordingly assessed against the following development standards:

*E13.7.1 Demolition*

*Objective: To ensure that demolition in whole or part of a heritage place does not result in the loss of historic cultural heritage values unless there are exceptional circumstances.*

No A1 Acceptable Solution.

*P1 Performance Criteria*

*Demolition must not result in the loss of significant fabric, form, items, outbuildings or landscape elements that contribute to the historic cultural heritage significance of the place unless all of the following are satisfied; (a) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place; (b) there are no prudent and feasible alternatives; (c) important structural or façade elements that can feasibly be retained and reused in a new structure, are to be retained; (d) significant fabric is documented before demolition.*

Demolition is primarily limited to the kitchen room on the ground floor to accommodate a new side entrance and reception area as well as a lift. None of the items proposed to be demolished or impacted by alteration works are considered to be important or highly significant fabric.

In terms of the first floor, demolition works are restricted to existing internal fit-outs and ceiling and internal walls in the General Assembly room and Store room. A toilet is also proposed to be removed.

The fabric to be removed although original, is not considered to be highly significant fabric. This fabric is evident elsewhere in the building. The removal of this internal fabric will enable the entrance reception and a lift.

These demolition works will involve removal of some original joinery in affected areas which appears throughout the site. The joinery to be impacted is not unique in the building.

However, the removal of original furniture, regalia and any original joinery should be documented in a Conservation Management Plan to be prepared by the proponent's heritage consultant so that relocation, storage or reuse of these items can be documented and effectively managed to avoid them being lost as part of building works and ongoing use of the subject site post development.

Exterior demolition to rear external walls and the roof is also proposed for the purposes of installation of the support columns and a lift. These parts of the building are not visually prominent and located well away from the façade.

There is also proposed to be reasonably substantial demolition to the roof form of the Temple building. The affected portion is in the middle to rear section to enable the proposed development to effectively sit over the Temple building. The portion of roofing to be removed is effectively hidden behind the parapet of the Temple façade.

The following comments are made against each of the P1 Performance Criteria:

*Demolition must not result in the loss of significant fabric, form, items, outbuildings or landscape elements that contribute to the historic cultural heritage significance of the place unless all of the following are satisfied;*

The form of the Temple building including its front façade will not be impacted by the proposed development. While some of the roof form will be lost, the roof form to be affected is located behind the front facing section of roof directly behind the parapet.

Any items such as regalia or internal detailing or original furniture required to be removed as a result of demolition works need to be documented in a Conservation Management Plan, with details provided on the fabric to be removed and its ability to be reused, relocated within the building or alternatively stored.

*(a) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;*

It is considered that the degree of demolition is relatively minor and has been restricted to areas of lower value within the Temple building.

*(b) there are no prudent and feasible alternatives;*

It is considered that the extent of demolition is relatively minimal and will not impact the overall integrity of the Temple building or result in an unreasonable degree of impact of any fabric of moderate or high value.

*(c) important structural or façade elements that can feasibly be retained and reused in a new structure, are to be retained;*

The façade of the Masonic Temple building will remain intact as part of the proposed development. No façade elements are proposed to be impacted by the proposed development which has set back the new building 7m from the façade. Where works will occur to side walls, these are considered unavoidable due to site constraints and the location of adjacent development and are required to enable access and egress into the new building. These are considered to have been designed and located to minimise impact to the Temple building.

Side and rear access into the development is considered highly beneficial to avoid the façade and its central entrance being affected.

*(d) significant fabric is documented before demolition.*

Some original detailing will be required to be unavoidably removed as part of the demolition works. The extent of this detailing to be impacted is minor and occurs in rooms of lower value within the building.

Nonetheless, any items such as regalia or internal detailing or original furniture required to be removed as a result of demolition works need to be documented in a Conservation Management Plan, with details provided on the fabric to be removed and its ability to be reused, relocated within the building or alternatively stored

#### *E13.7.2 Buildings and Works other than Demolition*

*Objective: To ensure that development at a heritage place is: (a) undertaken in a sympathetic manner which does not cause loss of historic cultural heritage significance; and (b) designed to be subservient to the historic cultural heritage values of the place and responsive to its dominant characteristics.*

No A1, A2, A3 or A4 Acceptable Solution.

A5/P5 and A6/P6 are not relevant to the proposal as the development does not seek approval for any front fences or gates and does not seek to remove any vegetation between a dwelling and the street (the subject site contains no such dwelling owing to the location of the Temple building).

#### *P1 Performance Criteria*

*Development must not result in any of the following: (a) loss of historic cultural heritage significance to the place through incompatible design, including in height, scale, bulk, form, fenestration, siting, materials, colours and finishes; (b) substantial diminution of the historic cultural heritage significance of the place through loss of significant streetscape elements including plants, trees, fences, walls, paths, outbuildings and other items that contribute to the significance of the place.*

‘Streetscape’ is defined in the Scheme as:

“the visual quality of a street depicted by road width, street planting, characteristics and features, public utilities constructed within the road reserve, the setbacks of buildings and structures from the lot boundaries, the quality, scale, bulk and design of buildings and structures fronting the road reserve. For the purposes of determining streetscape with respect to a particular site, the above factors are relevant if within 100 m of the site.”

'Incompatible' is not defined in the Planning Scheme but is generally defined as follows"  
'two things being so different in nature as to be incapable of coexisting'.

In terms of P1(a) the cultural significance of the Temple building has been summarised in this assessment as well as the Praxis report. Those identified values are broadly consistent with each other between the proponent's position and the one summarised in this assessment. It is considered that the proposed development will not result in a loss of those values through incompatible design.

In terms of scale, bulk and form, the proposed new building will sit within a cluster of comparable scale, height and massed buildings and behind the Masonic Temple building which by virtue of its intended purpose, is considered a substantial building itself in terms of bulk and mass, despite being only two storeys.

Consideration has been given to the external cladding of the proposed building which will be largely glass clad broken up by brass mesh screening across the street facing façade.

While it is not agreed that the proposed building will 'float' above the Temple building, its proposed cladding, colours and materials will reduce the visual appearance of the building and soften its appearance providing a more transparent cladding as opposed to solid opaque materials or finishes that would increase the visible presence of the new development.

In terms of height, the proposed 4.5 storey increase behind the Temple building is considered to be at the limit of what would be considered appropriate in terms of proposed height increase of a new building. This proposed height will sit comfortably against immediately adjacent buildings and owing to its set back from the Temple building, will result in the building being a background element rather than appearing as an extension to the Temple building that overwhelms the original street facing portion of the Temple. The Temple building will remain the dominant built form within the subject site when viewed from within the streetscape surrounding the subject site.

The 4.5 storey increase is not considered substantial and is comparable to a doubling of the height of the temple building itself.

The Temple building currently has a minimal streetscape presence that further reduces upon moving away from the subject site in either direction as a result of neighbouring development. Its form is primarily two dimensional in the streetscape owing to neighbouring development and its roof form is effectively hidden behind a parapet.

The setback behind the façade of the Temple building is considered effective is providing a visual separation that based on photo montages provided by the project architect, will not present as being visually prominent in the streetscape but rather, sit within an existing cluster of taller buildings without rising above any neighbour.

In terms of streetscape presence, the proposed building will only become really visible within the streetscape as a backdrop element to the Masonic Temple building when viewing the Temple building directly opposite or in close proximity.

The proposal will neither screen or obscure the limited streetscape views of the Temple. The Temple building will retain its current extent of visible presence within the streetscape. Furthermore, the proposed building has importantly not sought to copy the architectural design of the temple or mimic the lighter sandstone hued bricks that accentuates the Temple building within the streetscape.

The relatively simple façade treatment and limited materials of the proposed building will not provide a visual distraction from the Temple building.

On this basis, the proposal is considered acceptable against P1(a).

In terms of P1(b), no streetscape elements will be impacted by the proposed development.

#### *P2 Performance Criteria*

*Development must be designed to be subservient and complementary to the place through characteristics including: (a) scale and bulk, materials, built form and fenestration; (b) setback from frontage; (c) siting with respect to buildings, structures and listed elements; (d) using less dominant materials and colours.*

Neither 'subservient' or 'complementary' are defined in the Planning Scheme.

Subservient is defined as:

"serving or acting in a subordinate capacity"

Complementary is defined as:

"something which completes or makes perfect"

The assessment of the development requires an assessment of heritage values of the Place and a comparison exercise of building forms.

The bulk and scale of the proposed building is considered complementary to the Masonic Temple form which also exhibits a substantial bulk and form for a two storey building.

The scale and accentuated height of the Heritage Place is part of its value and relates to its function as a meeting place for the Freemasonry movement. Such buildings typically were neither diminutive in scale or located so as to be inconspicuous in the streetscape or their setting.

The proposal seeks to place a larger (in height) building to the rear of a building lower in height.

In terms of the height, the height of the proposed building will not result in a loss or substantial reduction of any of the identified heritage values of the Temple building.

The difference in height is calculated by the project architect as being 4.5 floors that is set back a further 7m from the Sandy Bay Road frontage and building line of the Temple building.

It is considered that the proposed building has been designed and located to be responsive to the dominant characteristics of the Temple. The quite substantial height of the Temple building itself, which is a two storey building internally, but has a much higher form than normally expected for two storey buildings, is one of its values. On that basis, it is considered that the height of the proposed building to be located behind the main Temple form is complementary to the Temple building.

In terms of subservience, multiple factors have been considered in making an assessment of subservience. These relate to the scale, height, bulk and form of the Temple building itself, the scale, height and form of surrounding development and the presence of the Temple building within the streetscape, both before and after development.

The proposed building would not be considered appropriate for a diminutive one or two storey cottage exhibiting pitched roof forms or domestic scale when an assessment of comparison is undertaken against existing and proposed height and form.

However, the relatively substantial height, form and bulk of the Temple building itself affords a greater ability to consider a taller form adjacent which is further addressed by the proposed building being recessed into the subject site behind the main Temple form.

On the basis of a consideration of all values of the Temple building and a comparison exercise in terms of height when coupled with the location of the proposed building which will sit well behind the Temple building and not be visually prominent in the streetscape, it is considered that the proposed building is able to be considered both complimentary and subservient.

*P3 Performance Criteria*

*Materials, built form and fenestration must respond to the dominant heritage characteristics of the place, but any new fabric should be readily identifiable as such.*

The proposed new building is to be constructed with glazing and brass mesh screening on the front street facing façade and zinc on side walls.

The proponent's heritage consultant Praxis describes the proposed materials as follows:

“The materials palette has been chosen to provide a more transparent and ephemeral contrast to the solid masonry and minimally fenestrated Masonic temple to promote a materiality hierarchy which emphasises the dominance of the earlier building and promotes subservience of the addition.”

It is agreed that the predominantly glazed materials with brass metal mesh screening along with zinc will provide for a new fabric which does not visually dominate or compete with the visual hierarchy of the cream hued bricks which will remain as the primary visual focus in the streetscape.

The proposed new building provides vertical linear elements that reflect the strongly linear elements of the front façade and side walls of the temple building. The new building does not place strong emphasis on new façade elements but this approach is preferable to ensure the prominence and dominance of the architectural details of the Temple building which express strong vertical elements.

*P4 Performance Criteria*

*Extensions to existing buildings must not detract from the historic cultural heritage significance of the place.*

As previously noted, the proposed new building which is technically an extension of the Temple building that for the most part, lightly touches the Temple as far as possible, and will not result in a loss or detracting of the identified cultural or historic heritage values of the Temple, its presence in the streetscape or its existing use.

*E13.10.1 Building, Works and Demolition*

*Objective: To ensure that building, works and demolition at a place of archaeological potential is planned and implemented in a manner that seeks to understand, retain, protect, preserve and otherwise appropriately manage significant archaeological evidence.*

*A1 Acceptable Solution*

*Building and works do not involve excavation or ground disturbance.*

The proposal will involve excavation and ground disturbance for the foundations of the proposed building to the rear of the site. Therefore, the proposal needs assessment against the P1 Performance Criteria.

*P1 Performance Criteria*

*Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to: (a) the nature of the archaeological evidence, either known or predicted; (b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential; (c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition; (d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation; (e) measures proposed to preserve significant archaeological evidence 'in situ'.*

The *Conservation Management Policy, Heritage Impact Assessment & Statement of Compliance* report prepared by Praxis Environment (author Mr Brad Williams) provides a detailed history of the subject site.

The subject site was formerly part of the gardens of the Highfield Estate with Frankland's 1839 map of Hobart and surrounds showing the subject site as being laid out with formal gardens below the Highfield dwelling.

The 1841 Census map of Hobart and surrounds likewise shows the subject site laid out as gardens.

Photographic images as early as 1857 show the subject site accordingly landscaped:

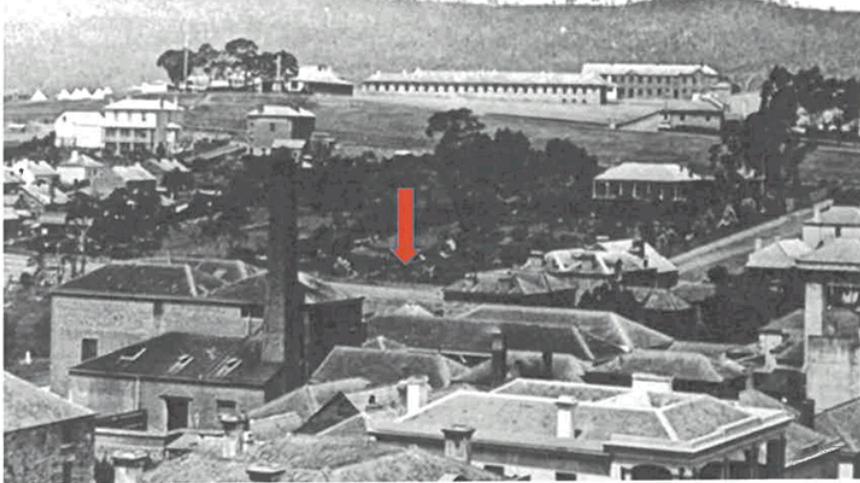


Figure 4.8 – Excerpt from an 1857 Alfred Abbott panorama of Hobart, the red arrow depicting the approximate subject site. Libraries Tasmania AUTAS00116252550W800.

Source: Praxis HIT, page 7.

Metropolitan Drainage Board plans originally held by Council show the subject site still undeveloped as of 1907.

No buildings are definitely known to have occupied the subject site prior to the construction of the Temple. Aside from being part of the Highfield Estate gardens, the subject site may (unconfirmed) have been part of a small nursery run by a Mr Latham with valuation rolls between 1880 and 1890 suggesting this use may have been partially located within the subject site. However, there is no firm evidence that this use ever included any buildings.

The submitted Praxis report provides detailed information about the history of the subject site and the subsequent development of the subject site as a Masonic Temple.

In light of the lack of evidence of any buildings or development known to have occurred on the subject site at any point prior to the construction of the T

emple building in the late 1930's, it is considered that the subject site is almost certain to not include any archaeological potential.

On that basis, the proposal is not considered to be problematic against the P1 Performance Criteria of clause E13.10.1 and is appropriate for an approval against the standard without the requirement for any conditional approval.

**Summary:**

It is considered that the proposed development is satisfactory against the development standards for demolition and alterations and extension to a Heritage Place. The extent and location of proposed demolition has been primarily restricted to areas of the building where heritage values are less important.

While located as being in an area of Archaeological Potential, it is considered the proponent's heritage consultant has capably demonstrated that there is nil likelihood of any significant archaeological potential for the subject site.

In terms of development standards for extending and altering a Heritage Place, an assessment has been taken against the values of the Place and its built form, height and scale versus the form, height and scale of the proposed new building.

The surrounding area has a chequered history of refusals of development in recent years based on the scale, height and extent of proposed commercial development. Consideration of the Tribunal outcomes of these cases has been undertaken to ensure a consistent approach has been applied in the heritage approach and assessment of this proposal.

It should also be acknowledged that the historic and cultural heritage values of the building also comprise items of moveable heritage such as regalia items associated with the Freemasonry movement. Therefore, it is considered important that such items have an ongoing appropriate management plan to ensure their protection, reuse or appropriate storage within the Temple building. This can be achieved through the lodgement of a Conservation Management Plan.

While a comprehensive Heritage Impact Assessment has been prepared by the proponent by their heritage consultant Praxis, a Conservation Management Plan is required to effectively manage works that may become relevant at the building stage (such as installation of services or NCC compliance) as well as provide a clear framework for the management, removal and storage of detailing and moveable items proposed to be removed or relocated as part of the development.

The proposal is recommended for approval, subject to conditions as outlined below.

**Recommended Conditions of approval:**

- 1 A Conservation Management Plan must be prepared to the satisfaction of the Senior Cultural Heritage Officer, and be submitted with plans and documents lodged to Council for building approval.

This CMP should include the following:

- o Consideration of all proposed plumbing, electrical, fire detection and protection requirements, security, acoustics and environmental requirements to ensure that installation is reversible and is undertaken in as unobtrusive a manner as possible.
- o Documentation of any relocated or removed detailing, regalia or Masonic furnishings, confirming the extent removed and details of reuse as part of the development or details of cataloguing and storage within the Temple building for possible future use.
- o Where it is determined by the project heritage consultant that NCC compliance will result in substantial heritage impact, alternatives are considered and documented to reduce impact accordingly to an acceptable limit.
- o Details of proposed interpretation panels or displays to be incorporated within the proposed Hotel building.
- o A program of curatorial input for moveable heritage items to be effectively managed and protected.
- o Details on how the removal of any significant detailing (e.g. in the current storage areas and upstairs assembly area) to facilitate the installation of the lift and stairs will be either retained for reuse on the site (e.g. in the refit of the kitchen as the hotel lobby) or alternatively stored for possible future use.

*Reason for condition:*

*To ensure the ongoing protection and appropriate management of identified historic and cultural heritage values of 3 Sandy Bay Road.*

2. Details of finalised colours, materials and finishes must be prepared to the satisfaction of the Senior Cultural Heritage Officer, and be submitted with plans and documents lodged to Council for building approval. No colour tinted or mirror style reflective glazing finishes are permitted for any external glazing.

*Reason for condition:*

*To clarify the extent and nature of the proposed approved works.*





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PLANNING REF: PLN-HOB-2021-0710  
THC WORKS REF: 8593  
REGISTERED PLACE NO: 7490  
APPLICANT: Philip Oddie  
DATE: 4 June 2025

## NOTICE OF HERITAGE DECISION

*(Historic Cultural Heritage Act 1995)*

The Place: Masonic Temple, 3 Sandy Bay Road, Sandy Bay.  
Proposed Works: Partial demolition, alterations and multi-storey additions.

Under section 39(6)(b) of the *Historic Cultural Heritage Act 1995*, the Heritage Council gives notice that it consents to the discretionary permit being granted in accordance with the documentation submitted with Development Application PLN-HOB-2021-0710, advertised on 12/05/2025, subject to the following conditions:

- 1. Items of original joinery, including doors and architraves, etc., which are approved to be removed as part of the demolition must be:**
  - (i) Dismantled and removed with the least possible damage so as to enable their reinstatement; and,**
  - (ii) Stored in good condition at the place with a copy of this notice attached in a protective plastic sleeve; and,**
  - (iii) Not removed from storage except to be re-used with approval of the Tasmanian Heritage Council.**

### Reason for condition

To ensure that these significant original elements are preserved so as to allow for their reinstatement at some future date, in accordance with the appropriate outcomes described in Sections 6.2 and 9.4 of the *Works Guidelines*.

- 2. The structural alterations within the roof space must be designed and built in a manner that minimises damage to the original roof structure and supporting walls.**

### Reason for condition

To minimise physical impact on the historic fabric of the place, consistent with the appropriate outcomes described in Section 9.5 of the *Works Guidelines*.

Advice

This consent does not include any works to the Masonic Temple that are not indicated in the application, including building upgrades in response to the requirements of the *Building Act 2016*. Please contact Heritage Tasmania for further advice.

Should you require clarification of any matters contained in this notice, please contact Heritage Tasmania's Regional Heritage Advisor (South), Russell Dobie, on 1300 850 332.



Peter Scott

**Chair – Works Committee**

*Under delegation of the Tasmanian Heritage Council*