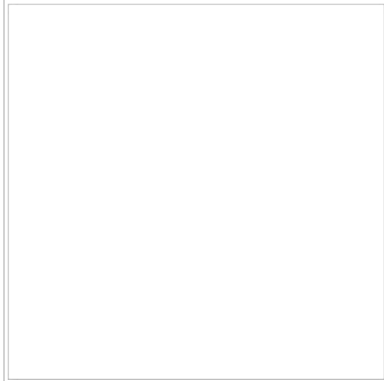


Property

6 MONTELLIER RETREAT BATTERY POINT TAS 7004



People

Applicant
*
e3planning pty ltd
Evan Boardman
PO Box 58
SOUTH HOBART TAS 7004
0438376840
evan@e3planning.com.au

Owner
*
Behrakis Property Holdings
Peter, Victoria, Dennis and Maria Behrakis
Bathurst Street HOBART TAS 7001
HOBART TAS 7001
0438376840
evan@e3planning.com.au

Entered By
EVAN BOARDMAN
0438 376 840
evan@e3planning.com.au

Use

Commercial

Details

Have you obtained pre application advice?

☒ Yes

If YES please provide the pre application advice number eg PAE-17-xx

No pre application given.

Are you applying for permitted visitor accommodation as defined by the State Government Visitor Accommodation Standards? Click on help information button for definition. If you are not the owner of the property you MUST include signed confirmation from the owner that they are aware of this application.

*

☒ No

Is the application for SIGNAGE ONLY? If yes, please enter \$0 in the cost of development, and you must enter the number of signs under Other Details below.

*

☒ No

If this application is related to an enforcement action please enter Enforcement Number

Details

What is the current approved use of the land / building(s)?

*

Vacant

Please provide a full description of the proposed use or development (i.e. demolition and new dwelling, swimming pool and garage)

*

Carpark

Estimated cost of development

*

200000.00

Existing floor area (m2)

513.00

Proposed floor area (m2)

Site area (m2)

Carparking on Site

N/A

Total parking spaces

14

Existing parking spaces

☐ Other (no selection chosen)

Other Details

Does the application include signage?

*

☐ No

How many signs, please enter 0 if there are none involved in this application?

*

0

Tasmania Heritage Register

Is this property on the Tasmanian Heritage Register?

☒ No

Documents

Required Documents

Title (Folio text and Plan and Schedule of Easements)

*

Property Title 6 8 Montpelier Retreat.pdf

Plans (proposed, existing)

*

2103_Montpelier_Carpark_DA.pdf

GM or Crown consent

General Managers Consent request 6 8 Montpelier Retreat 5 July 2021.pdf

Covering Letter

Covering Letter 6 8 Monpelier e3 planning 05 07 21.pdf

Supporting Documents

Traffic Impact Assessment

6-8 Montpelier Retreat TIA.pdf



City of **HOBART**

Enquiries to: City Planning
Phone: (03) 6238 2715
Email: coh@hobartcity.com.au

24 November 2021

Evan Boardman (e3planning Pty Ltd)
GPO Box 58
SOUTH HOBART TAS 7004

mailto: evan@e3planning.com.au

Dear Sir/Madam

**6 MONTELIER RETREAT & 8 MONTELIER RETREAT, BATTERY POINT &
ADJACENT ROAD RESERVE
WORKS IN ROAD RESERVE NOTICE OF LAND OWNER CONSENT TO LODGE A
PLANNING APPLICATION - GMC-21-47**

Site Address:

6 Montpelier Retreat, and adjacent Road Reservation, Battery Point

Description of Proposal:

Partial Demolition, Alterations, Partial Change of Use to Car Parking and Associated Works

Applicant Name:

Evan Boardman
e3 Planning Pty Ltd

PLN (if applicable):

PLN-21-455

I write to advise that pursuant to Section 52 of the *Land Use Planning and Approvals Act 1993*, I grant my consent on behalf of the Hobart City Council as the owner/administrator of the above land for you to make application to the City for a planning permit for the development described above and as per the attached documents. I granted consent pursuant to delegation, a copy of which is enclosed.

Please note that the granting of the consent is only for the making of the application and in no

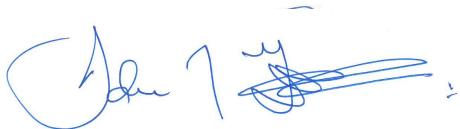
way should such consent be seen as prejudicing any decision the Council is required to make as the statutory planning authority.

This consent does not constitute an approval to undertake any works and does not authorise the owner, developer or their agents any right to enter or conduct works on any Council managed land whether subject to this consent or not.

If planning approval is granted by the planning authority, you will be required to seek approvals and permits from the City as both landlord, land manager, or under other statutory powers (such as other legislation or City By-Laws) that are not granted with the issue of a planning permit under a planning scheme. This includes the requirement for you to reapply for a permit to occupy a public space under the City's Public Spaces By-law if the proposal relates to such an area.

Accordingly, I encourage you to continue to engage with the City about these potential requirements.

Yours faithfully



(John Fisher)

ACTING DIRECTOR CITY AMENITY

Relevant documents/plans:

Plans by Gandy and Roberts C020 Rev C, C021 Rev C, C022 Rev C, C023 Rev C, C024 Rev A, C025 Rev A

City of Hobart

INSTRUMENT OF DELEGATION

General Delegation

Director City Amenity

I, Kelly Grigsby, Chief Executive Officer, being the General Manager of the Hobart City Council as appointed by Council pursuant to section 61 of the *Local Government Act 1993* ("the Act") hereby delegate pursuant to Section 64 of the Act, the following powers and functions to the **Director City Amenity**, or to such persons who may be acting in that position:

1. to sign an application; and
2. to provide written permission to make an application;

pursuant to section 52(1B) of the *Land Use Planning and Approvals Act 1993*,
EXCEPT where an application is recommended for refusal.

Dated this 20 day of August 2021



(Kelly Grigsby)

CHIEF EXECUTIVE OFFICER

Being the General Manager as appointed by the Council pursuant to Section 61 of the Local Government Act 1993 (tas)





Approved - General
Manager Consent Only
[GMC-21-47]
24/11/2021

REV	DESCRIPTION	APP'D	DATE	REV	DESCRIPTION	APP'D	DATE
1	PLAN NO. 21/0317	SP	24/11/2021				
2	PLAN NO. 21/0317	SP	24/11/2021				

THIS DRAWING HAS NOT BEEN
APPROVED FOR CONSTRUCTION



**GANDY AND
ROBERTS
CONSULTING
ENGINEERS**

2/8 DAVY'S ST., HOBART
TASMANIA, AUSTRALIA 7000
www.gandyandroberts.com.au
info@gandyandroberts.com.au
0813 623180 / 5618 0223 7183

**NEW CARPARK
6-8 MONTPELIER RETREAT, SALAMANCA
TASMANIA 7004**
SITE PLAN

1:100 (A1)
DATE: 21/03/17
DRAWN BY: C
CHECKED BY: MC

16A MONTPELIER RETREAT

8 MONTPELIER RETREAT

6 MONTPELIER RETREAT

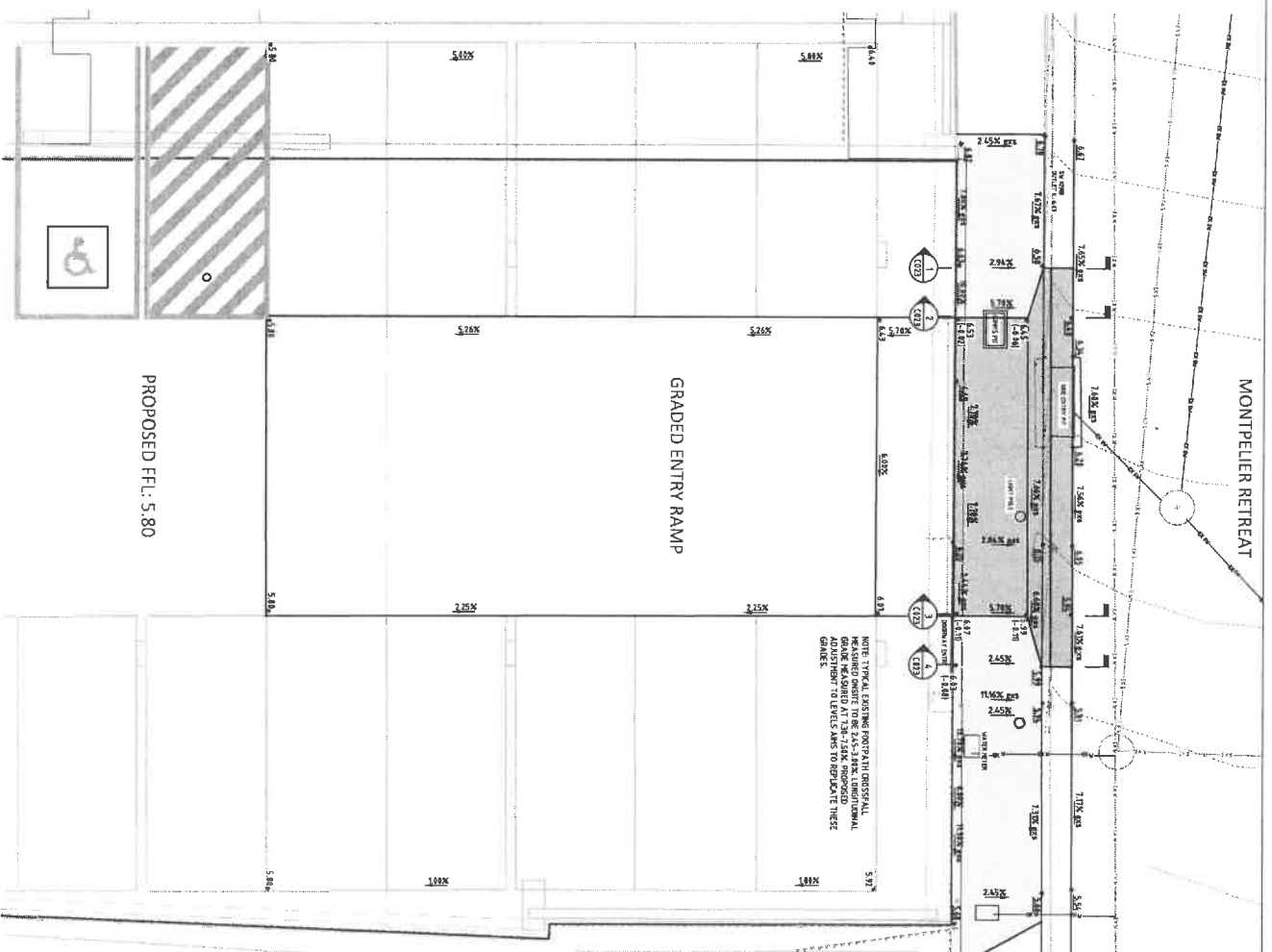
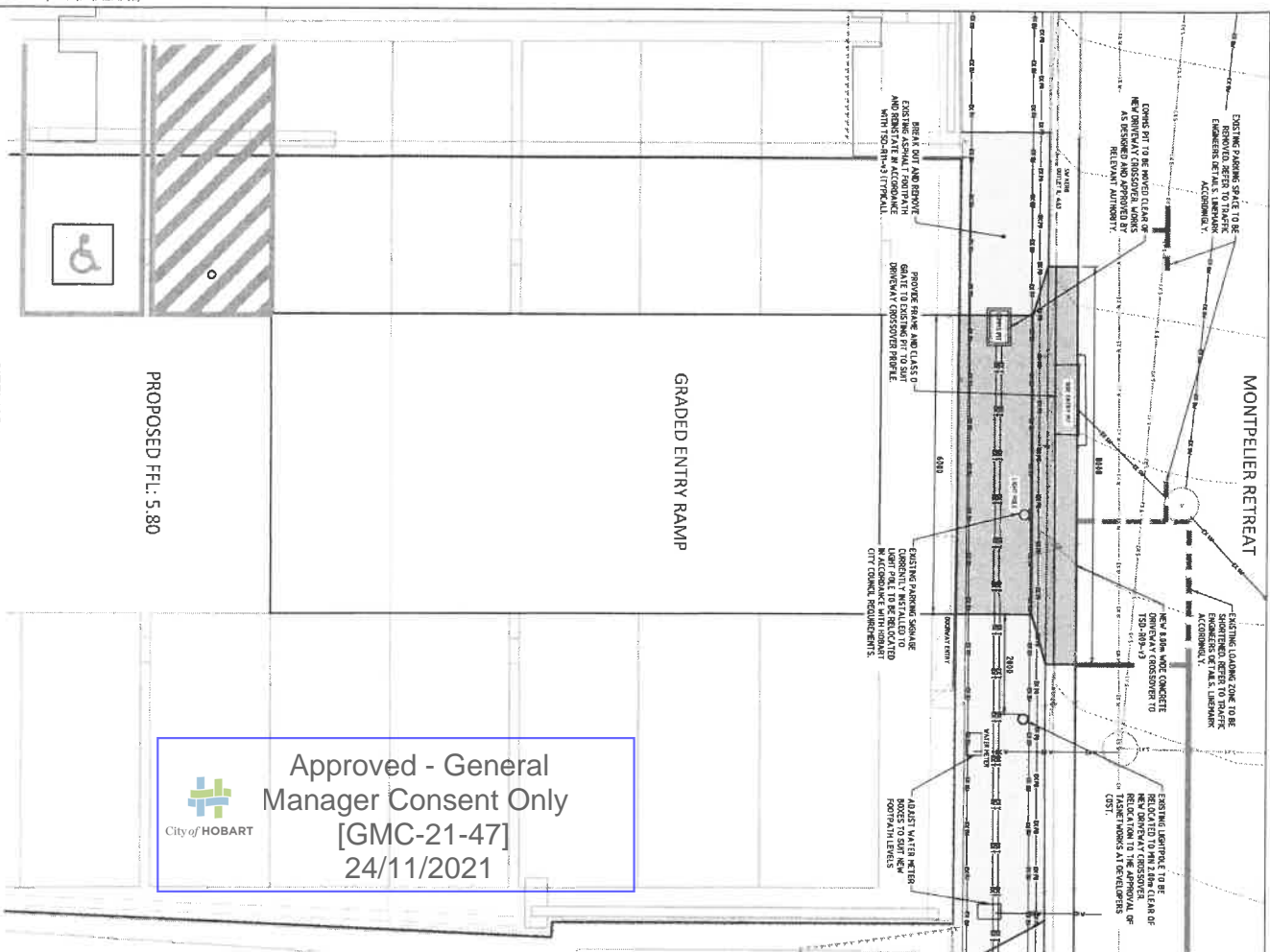
4 MONTPELIER RETREAT 2 MONTPELIER RETREAT

MONTPELIER RETREAT

PROPOSED NEW HIGHWAY CROSSING
AND ASSOCIATED ENTRY AREAS

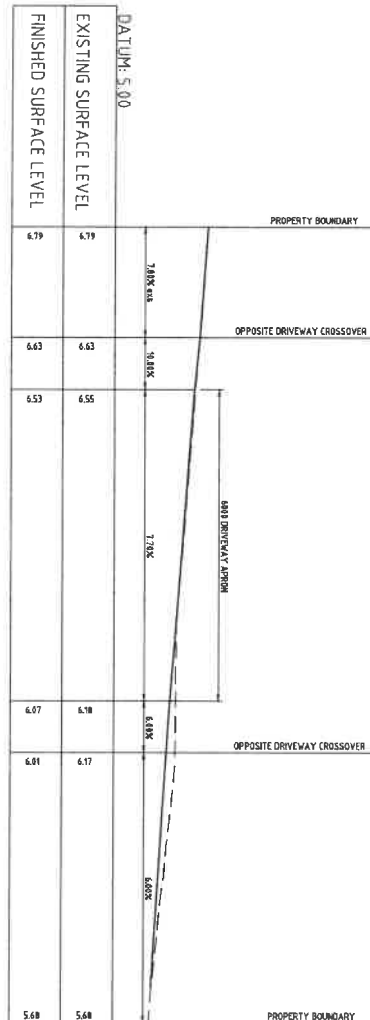
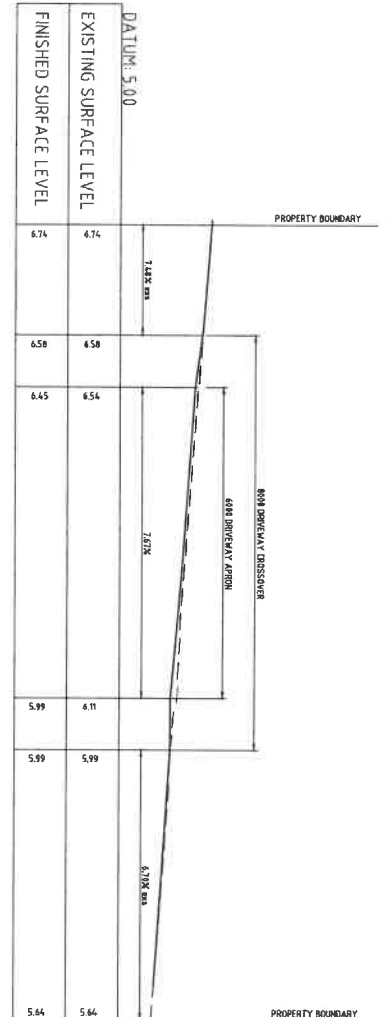
PROPERTY BOUNDARY

PROPOSED NEW PRIVATE CARPARK





Approved - General
Manager Consent Only
[GMC-21-47]
24/11/2021



REV	DESCRIPTION	APP'D	DATE	REV	DESCRIPTION	APP'D	DATE
C	FINAL APPROVAL	SP	12.11.2021				
B	FINAL APPROVAL	SP	08.10.2021				

THIS DRAWING HAS NOT BEEN
APPROVED FOR CONSTRUCTION



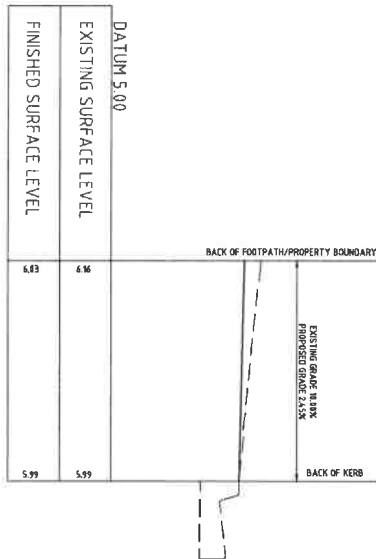
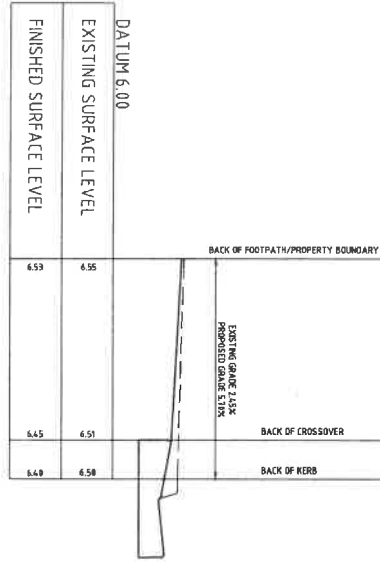
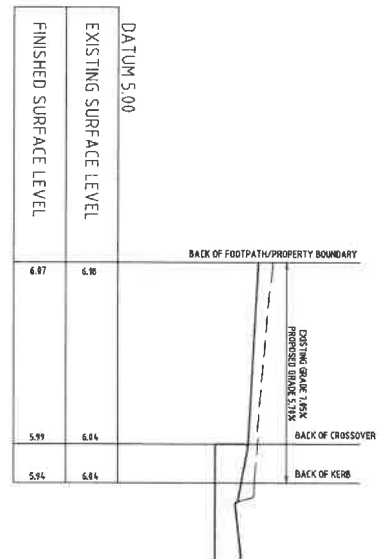
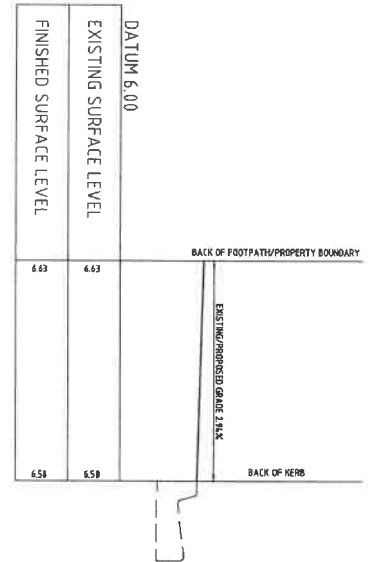
**GANDY AND
ROBERTS
CONSULTING
ENGINEERS**
150 DAVENY ST, HOBART
TASMANIA, AUSTRALIA 7000
www.gandyandroberts.com.au
info@gandyandroberts.com.au
ph 03 6223 8877 / 03 6223 7183

NEW CARPARK
6-8 MONTPELIER RETREAT, SALAMANCA
TASMANIA, 7004
FOOTPATH SECTIONS 01

21.03.17 2022 C



Approved - General
Manager Consent Only
[GMC-21-47]
24/11/2021



REV	DESCRIPTION	DATE	REV	DESCRIPTION	DATE
1	ISSUED FOR PERMIT	12.11.2021			
2	ISSUED FOR PERMIT	08.10.2021			

THIS DRAWING HAS NOT BEEN
APPROVED FOR CONSTRUCTION



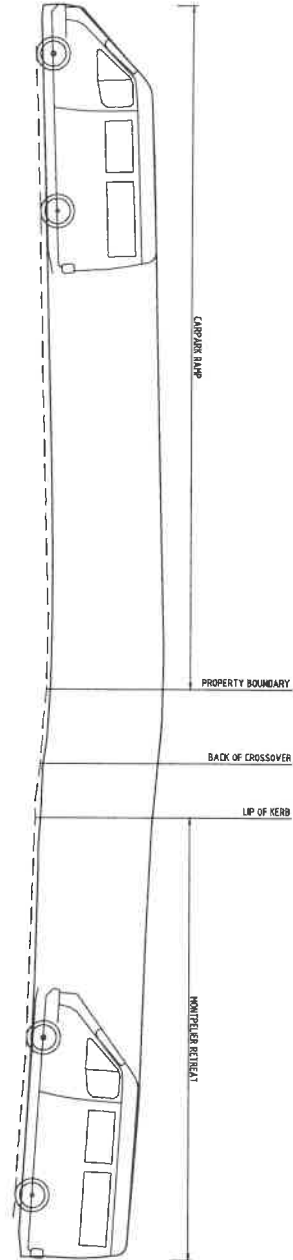
**GANDY AND
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150 DAVY ST., HOBART
TASMANIA, AUSTRALIA 7000
www.gandyandroberts.com.au
m.r@gandyandroberts.com.au
p:03 9273 8877 f:03 9273 7183

NEW CARPARK
6-8 MONTPELIER RETREAT, SALAMANCA
TASMANIA, 7004
FOOTPATH SECTIONS 02

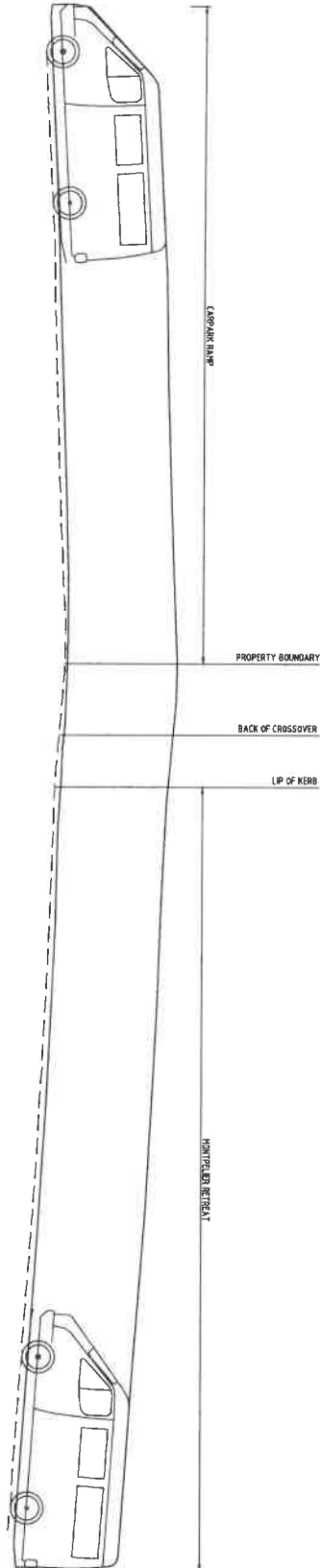
21.03.17	C023	C
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Approved - General
Manager Consent Only
[GMC-21-47]
24/11/2021



ENTRY MOVEMENT B99 (INNER WHEEL PATH)
SCALE 1:50



ENTRY MOVEMENT B99 (OUTER WHEEL PATH)
SCALE 1:50

REV	DESCRIPTION	APPROV	DATE	REV	DESCRIPTION	APPROV	DATE
A	PLANNING/ANNUAL	SP	11/11/2021				

THIS DRAWING HAS NOT BEEN
APPROVED FOR CONSTRUCTION



**GANDY AND
ROBERTS
CONSULTING
ENGINEERS**

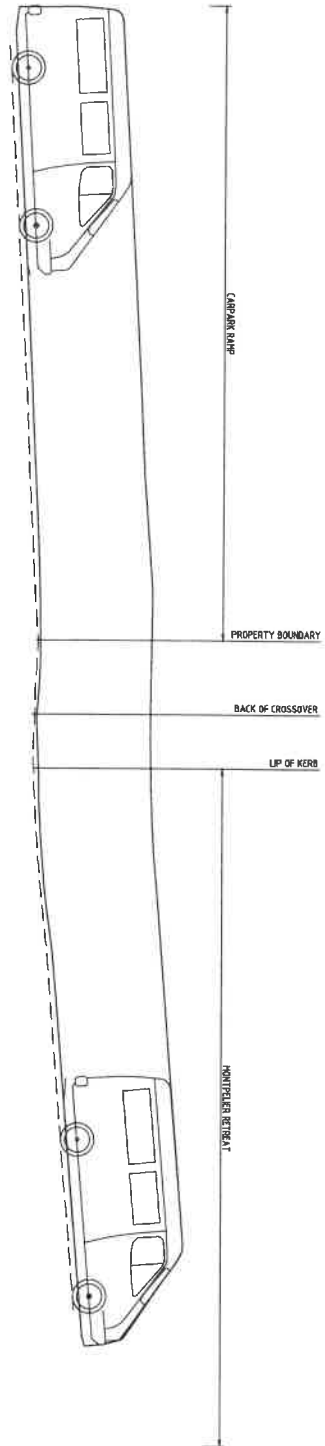
150 DAVEY ST, HOBART
TASMANIA, AUSTRALIA 7000
www.gandyandroberts.com.au
info@gandyandroberts.com.au
PH 03 6232 8877 / 03 6232 7103

NEW CARPARK
6-8 MONTELEER RETREAT, SALAMANCA
TASMANIA, 7004
VERTICAL CLEARANCE TEMPLATES 01

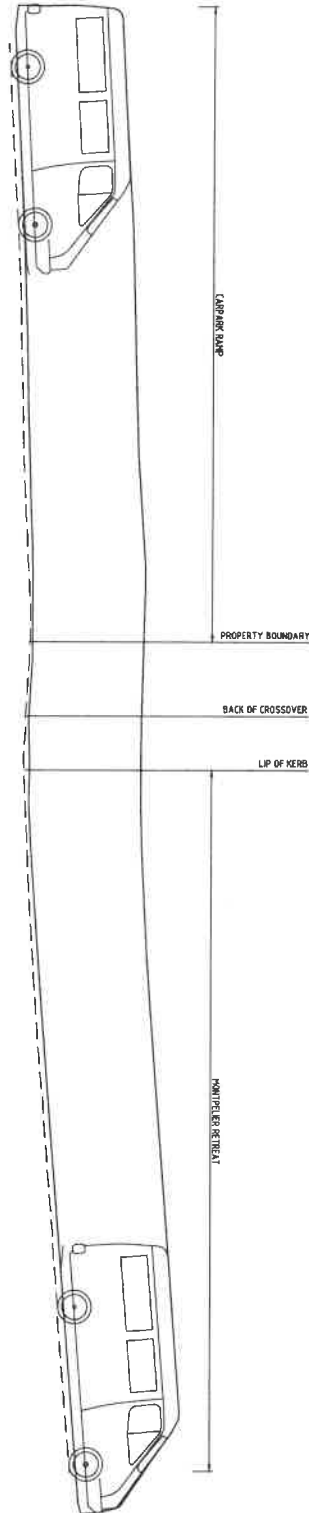
21.0317	0024	A
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Approved - General
Manager Consent Only
[GMC-21-47]
24/11/2021



EXIT MOVEMENT B99 (INNER WHEEL PATH)
SCALE 1:50



EXIT MOVEMENT B99 (OUTER WHEEL PATH)
SCALE 1:50

REV	DESCRIPTION	APPROD	DATE	REV	DESCRIPTION	APPROD	DATE
A	PLANNING APPROVAL	SP	11.11.2021				

THIS DRAWING HAS NOT BEEN
APPROVED FOR CONSTRUCTION



GANDY AND ROBERTS CONSULTING ENGINEERS
153 DAVEY ST, HOBART
TASMANIA, AUSTRALIA 7000
www.gandyandroberts.com.au
ph 03 5223 8877 / 03 5223 7183

**NEW CARPARK
6-8 MONTPELIER RETREAT, SALAMANCA
TASMANIA, 7004**

VERTICAL CLEARANCE TEMPLATES 02

21.0317	C025	A
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SEARCH OF TORRENS TITLE

VOLUME 10357	FOLIO 1
EDITION 11	DATE OF ISSUE 26-Aug-2015

SEARCH DATE : 02-Jul-2021

SEARCH TIME : 11.35 AM

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Diagram 10357

Derivation : Part of 0A-3R-27Ps. Gtd. to T. Hewitt

Prior CT 3667/39

SCHEDULE 1

C386317 TRANSFER to PETER BEHRAKIS, VICTORIA ANN BEHRAKIS,
DENNIS BEHRAKIS and MARIA BEHRAKIS as tenants in
common in equal shares Registered 13-Nov-2002 at
noon

SCHEDULE 2

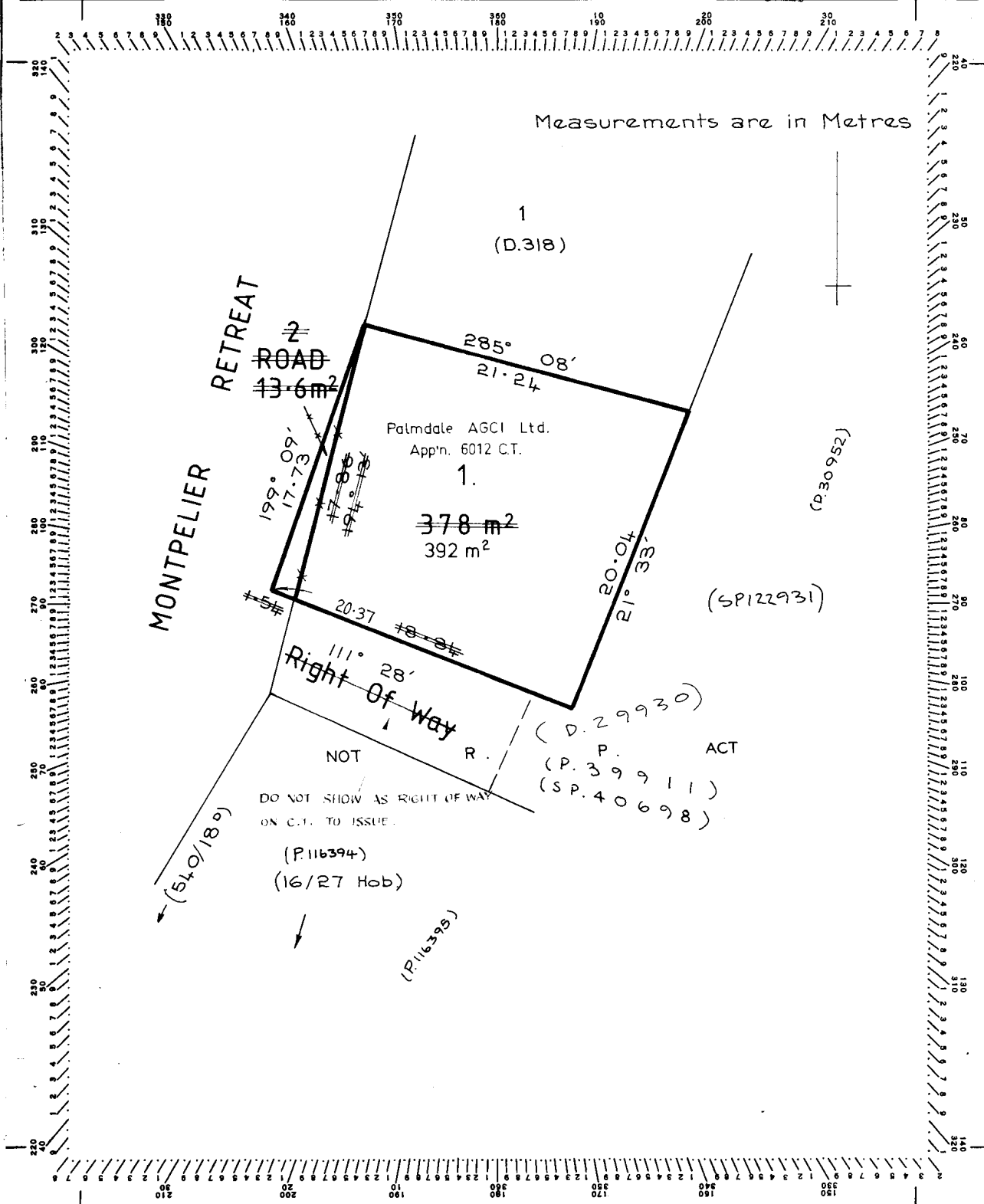
Reservations and conditions in the Crown Grant if any

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

A. 6012 C.T.

<p>Owner: William Holyman and Sons Pty. Ltd. PALMDALE AGCI LTD.</p>	<p>PLAN OF SURVEY by Surveyor <u>Nicholas Griggs</u> of land situated in the</p>	<p>Registered Number: D10357</p>
<p>Title Reference: Conv. 38-94H 51-7767</p>	<p>CITY OF HOBART</p>	<p>APPROVED 3 - FEB 1978 Effective from:</p>
<p>Grantee: Portion of OA.3R.27P gtd. to Thomas Hewitt</p>	<p>Scale 1:250</p>	<p><i>W. Hutchinson</i> Recorder of titles</p>



SEARCH OF TORRENS TITLE

VOLUME 234896	FOLIO 1
EDITION 13	DATE OF ISSUE 26-Aug-2015

SEARCH DATE : 02-Jul-2021

SEARCH TIME : 11.35 AM

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Plan 234896

Derivation : Part of 0A-3R-27Ps. - Sec. W.3. - Gtd. to T.
Hewitt.

Prior CT 3317/65

SCHEDULE 1B932733 & C519432 PETER BEHRAKIS, VICTORIA ANN BEHRAKIS,
DENNIS BEHRAKIS and MARIA BEHRAKIS as tenants in
common in equal sharesSCHEDULE 2

Reservations and conditions in the Crown Grant if any

UNREGISTERED DEALINGS AND NOTATIONSL0210254 PRIORITY NOTICE reserving priority for 60 days
MORTGAGE Peter Behrakis, Victoria Ann Behrakis,
Dennis Behrakis and Maria Behrakis to Retirement
Benefits Fund Board Lodged by Butler McIntyre and
Butler on 02-Feb-2004 BP: L0210254

EP 141

VOL.

FOL.

ANNEXURE TO CERTIFICATE OF TITLE

REGISTERED NUMBER

234896

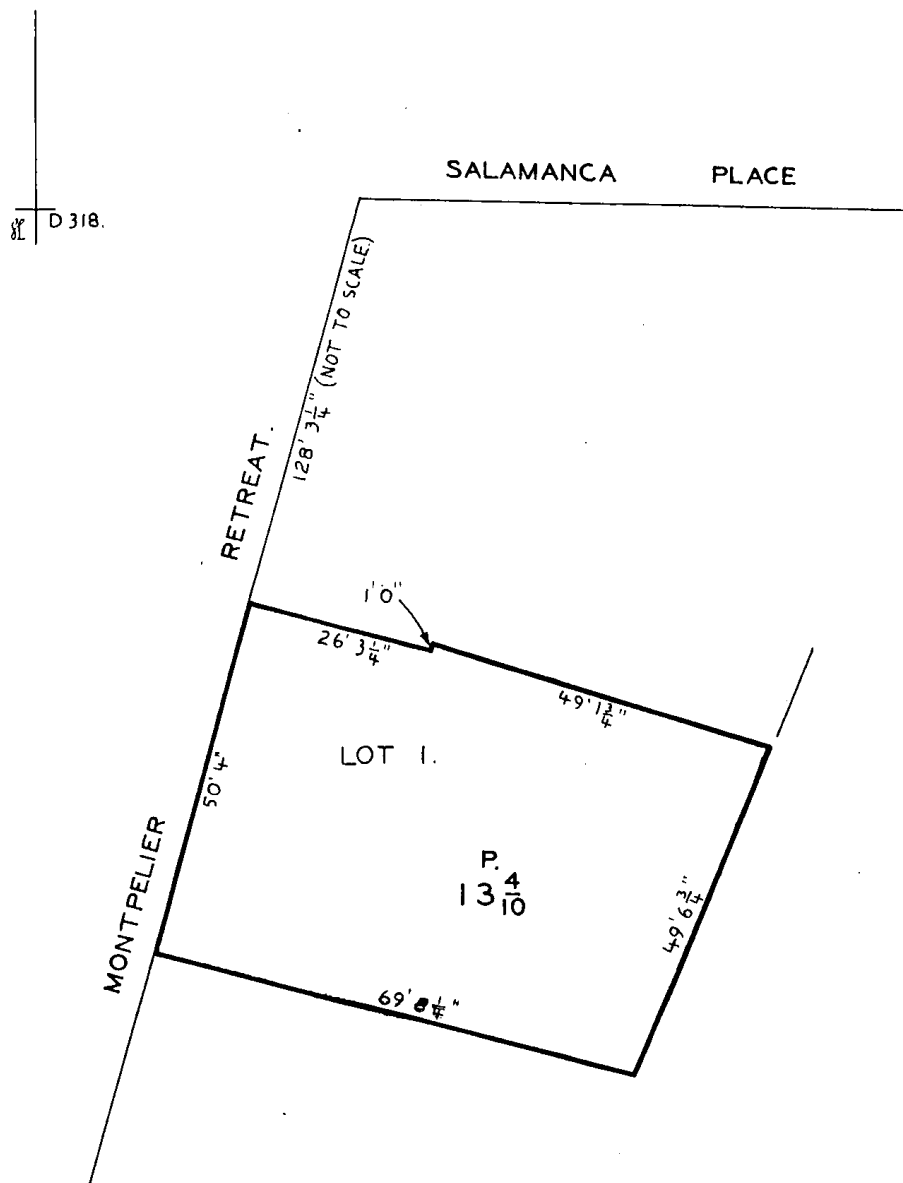
M. Hutchinson
Recorder of Titles



Lot 1 of this plan consists of all the land comprised in the above-mentioned cancelled folio of the Register.

PROPERTY ADDRESS

6 Montpelier Retreat
Hobart



10357

APPROVAL BY LOCAL AUTHORITY

The subdivision shown in this Plan
is approved

Insert here any
qualification to
the approval
under section 468 (12),
section 472 or
section 477B of
the Local Government
Act 1962.
Rule through any
blank space.

Seal

In witness whereof the common seal of

has been hereunto affixed, pursuant to a resolution of the Council of the said
municipality passed the day of 19 , in the presence
of us

..... } Members
..... }
..... } Council Clerk

NEW SERIES TITLES		
LOT/AREA	VOL.	FOL.
1.	3667	39.

**TO BE COMPLETED AND SIGNED
BY COUNCIL CLERK OR OWNER**

For the purposes of section 464 of the Local Govern-
ment Act 1962, the owner has nominated/I nominate
As his/my solicitor

As his/my surveyor G.W. Griggs

..... Council Clerk/Owner

TO BE FILLED IN BY SURVEYOR

Survey commenced 18.11.77

Survey finished 17.11.77

Error of Close see calcs

OFFICE EXAMINATION

Plot Checked 20.11.77

Mathematically Checked 30.11.77

Examined as to boundaries 20.12.77

Entered on Card

05379

Surveyor's Certificate

I, Nicholas Griggs
of 295 Elizabeth Street, N. Hobart 7009
in Tasmania, registered surveyor, hereby certify that this
plan:

1. Has been made from surveys executed by me or a regis-
tered pupil under my personal supervision, inspection,
and field-check, and that both plan and survey are
correct, and have been made in accordance with the
Land Surveyors By-Laws 1966;

2. Complies with all statutory provisions relating to any-
thing appearing thereon; and

3. ~~Requires the approval of the local authority, which has~~
~~been obtained~~ (or, does not require the approval of any
local authority)

Dated this 17th day of November 19 77

Nicholas Griggs
Registered Surveyor

(6281)



5 July 2021

Kelly Grigsby
General Manager
HOBART CITY COUNCIL
Via email: coh@hobartcity.com.au

RE: General Manager Consent 6-8 Montpelier Retreat

Please accept this correspondence as a request for General Managers Consent for alteration to the existing carparking arrangement at Montpelier Retreat. The development application including a Traffic Impact Assessment is attached to this correspondence.

It is proposed to change the use of 6-8 Montpelier to a carpark and for this to occur it is necessary to provide a cross over, across the existing footpath necessitating the removal of one ¼ hour carpark and a reduction in the size of an existing loading bay, this is shown in figure 1.

The buildings in which the carpark will be located were previously used as a Chemist and an office. Both uses generated a significant amount of vehicle movements and demand for carparking.

The loss of the carparking space is not considered to significantly impact the carparking availability in the surrounding area as the ¼ hr carpark was used almost exclusively for carparking by shoppers at the chemist which no longer operates.

If you have any further queries please do not hesitate to contact me on 0438 376 840.

Regards

Evan Boardman
Grad Dip URP, B ScEnv, B Econ MPIA



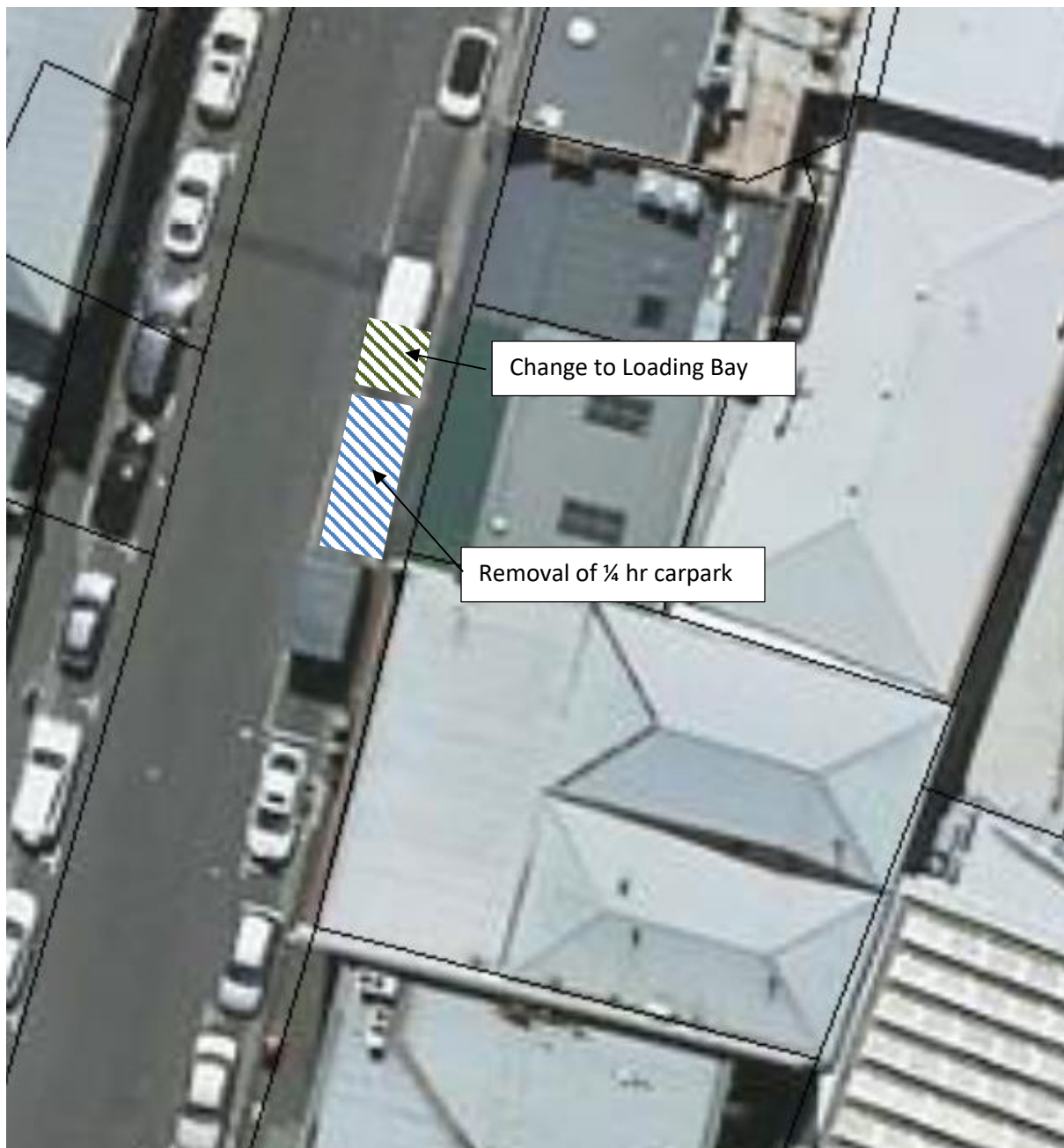


Figure 1: Proposed alteration to on-street carparking



5 July 2021

Kelly Grigsby
Chief Executive Officer
Hobart City Council
16 Elizabeth Street
HOBART TAS 7001

Via email: coh@hobartcity.com.au

Dear Kelly

**RE: Development Application Carpark 6 – 8 Montpelier Retreat
Battery Point**

Please accept this correspondence and associated documentation as a development application for construction of a carpark to accommodate 14 carparking spaces (the Proposal) at 6-8 Montpelier Retreat Battery Point (the Property).

The carpark would provide short term parking for visitors and tourists to Salamanca. Some minor excavation of the ground floor of 8 Montpelier Retreat would be required to provide for the necessary height for a carpark.

A crossover from Montpelier Retreat and a six-metre-wide entrance for the carpark would be constructed and a roller door installed. The roller door would be of a colour and materiality which blends with the existing building façade. The colour and finish of the building would be altered as a result of the proposal.



Attached to this correspondence are the following documents:

- 6-8 Montpelier Retreat Carpark DA drawing numbers 2103-DA00 – DA06, May 2021, Circa morris-nunn architects
- 6-8 Montpelier Retreat Traffic Impact Assessment, June 2021, Midson Traffic Pty Ltd
- Property Titles - CT 10357/1 and CT 234896/1
- Letter to General Manager requesting Council Consent as landowner.

The property location is shown in figure 1, 2 and 3.

Property Location

The property is located at 6-8 Montpelier Retreat Battery Point as shown in figure 1.



Figure 1: Property location source: www.thelist.tas.gov.au

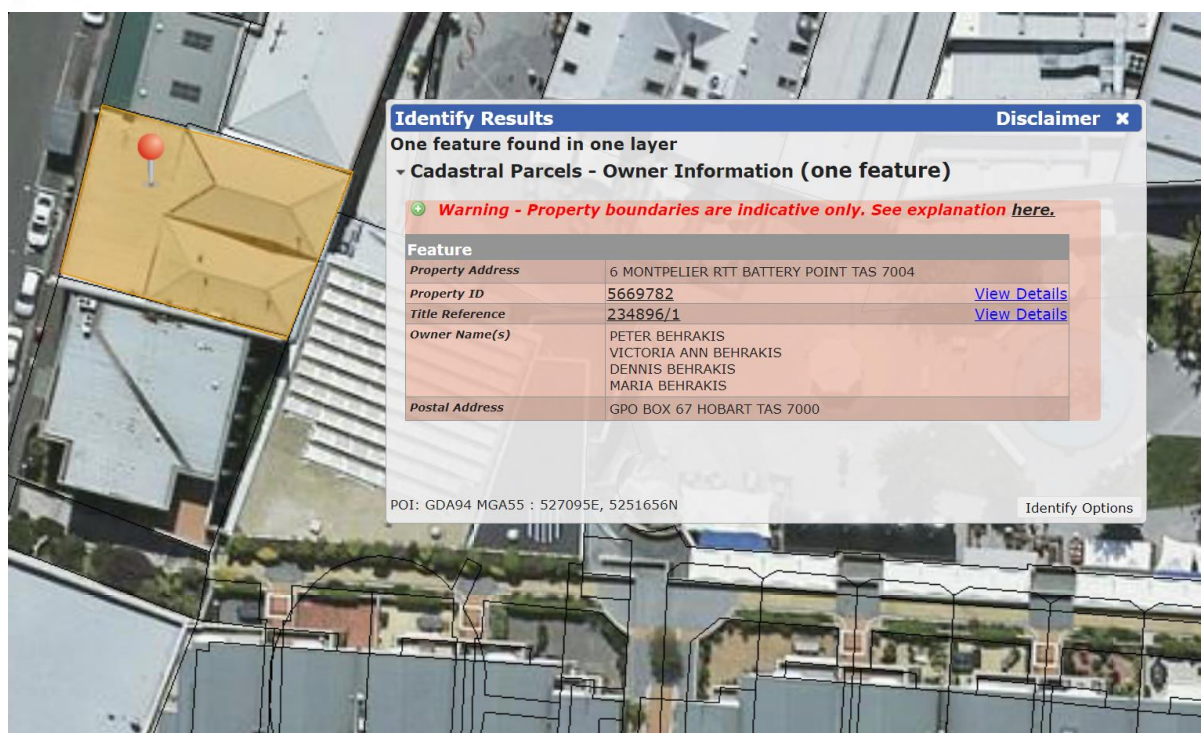

 Figure 2: Property location source: www.thelist.tas.gov.au

 Figure 3: Property location source: www.thelist.tas.gov.au

Zoning

The property is zoned Mixed Use under the *Sullivans Cove Planning Scheme 1997 (the Scheme)* as shown in figure 4.

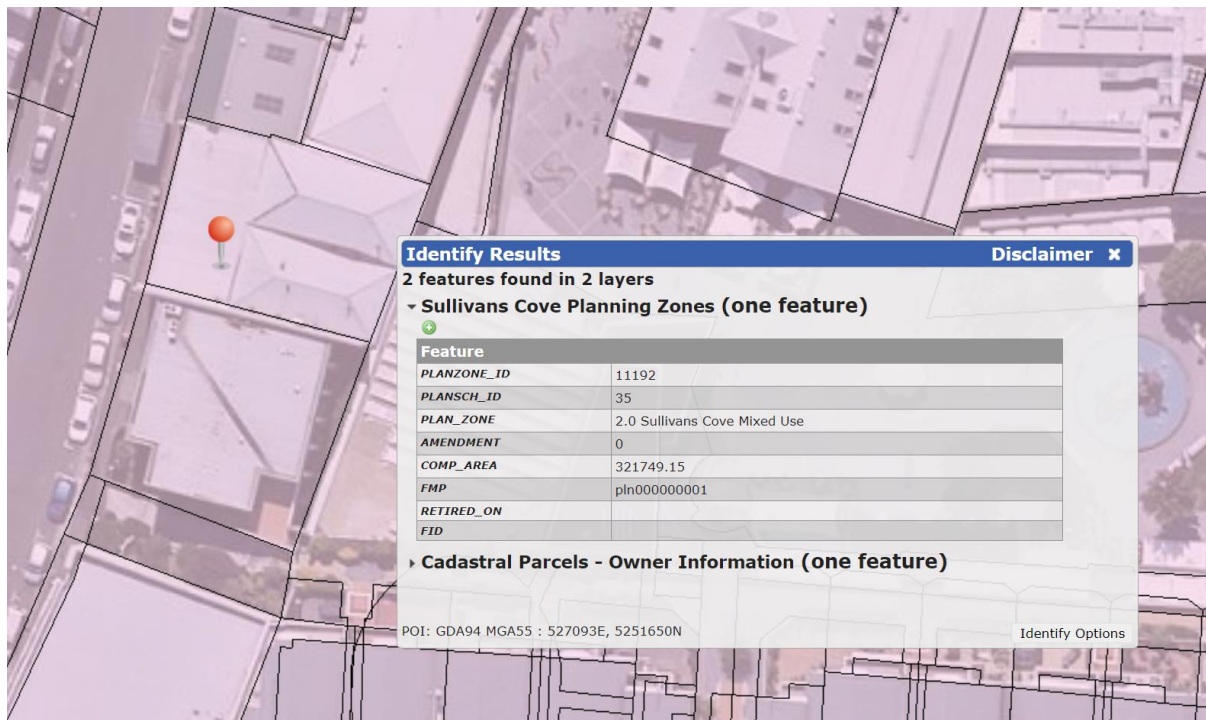


Figure 4: Property zoning source: www.thelist.tas.gov.au

The proposed development is discretionary and must be assessed against the relevant objectives and performance criteria as reproduced and considered below.

Objective

- (a) To ensure that activities within the Cove respect the cultural heritage and character of the Activity Area.

Performance Criteria

- All use and development within the Activity Area must demonstrably contribute to, and enhance the cultural heritage, built form (bulk, height, volume, urban detail) and spatial characteristics of the activity area.

Planning Response

The proposal would not impact upon the existing activities within the Cove as the building is existing and the external appearance of 6 and 8 Montpelier is not proposed to significantly alter, excepting for a carpark entrance off Montpelier Retreat.

Objective

- (b) To ensure that the amenity, character and cultural heritage values of the Cove's roads and other public spaces are conserved and enhanced.

Performance Criteria

- Use and development on road reserves, public parks and other public spaces within the activity area shall only be 'permitted' where they do not detract from the space's amenity or heritage value.

Planning Response

No significant works are proposed in the road reservation. The proposal requires the loss of one ¼ hr carpark. The loss of this space would not detract from the amenity of the area as the carpark was previously used almost exclusively by customers to the chemist which no longer exists.

Objective

- (i) To minimise the use of public land resources of the activity area for commuter and other long term parking.

Performance Criteria

- Public car parking within the Cove shall be provided primarily to meet the short term parking needs of tourists and other visitors to the Cove.

Planning Response

The proposed carpark would not be available for commuter carparking but only for short term carparking to meet the needs of tourists and other visitors.

If approval is granted a condition of approval could be included limiting the time people could park in the carpark.

SCHEDULE 1 – CONSERVATION OF CULTURAL HERITAGE VALUES

- 'Building or works' adjacent to a place of cultural significance must not dominate that place when viewed from the street or any other public space, or be more prominent in the street than the adjacent place of cultural significance.

Planning Response

6 Montpelier Retreat is adjacent to 4 Montpelier Retreat which is an identified place of cultural significance and accordingly the proposal must be assessed against the above provision. The proposed works would not significantly alter the appearance of the exterior façade of the existing building other than creating a 6 metre wide entrance for the proposed carpark. The external materials and colours are not proposed to change. The works would not alter the prominence of the building in the street.

- The area of a facade of any new building may be permitted to exceed that of the building on an adjacent place of cultural significance where the Planning Authority is satisfied that the visual impact of the apparent disparity of scale is not significant or that historic precedent warrants the scale disparity.

Planning Response

No new building is proposed.

- The location, bulk and appearance of 'building or works' must not adversely affect the heritage values of any adjacent or nearby place of cultural significance.

Planning Response

The proposed change of use to a carpark would retain the existing building fabric and the heritage values of the adjacent place of cultural significance would not be impacted.

- 'Building or works' must complement and contribute to the specific character and appearance of adjacent places of cultural significance and the historic character of the Cove generally.

Planning Response

The external appearance of 6 Montpelier Retreat would be altered via the installation of a 6 metre wide roller door which would be designed to match with the existing external appearance of the building.

8 Montpelier Retreat would be unaltered in terms of its external appearance.

Excavation underneath 6 and 8 Montpelier is required to provided for the necessary headroom for the proposed carpark.



- 'Building or works' must not reduce the heritage value of any adjacent places of cultural significance by mimicking historic forms.

Planning Response

No new buildings or external works are proposed.

If you have any further queries, please do not hesitate to contact me on 0438 376 840 or email evan@e3planning.com.au.

Regards

Evan Boardman
Grad Dip URP, B ScEnv, B Econ MPIA



2103

6-8 MONTPELIER RETREAT

DESIGNER

Circa Morris-Nunn Architects
Certified Architect - Robert Morris Nunn CC451P (Delete one)
Address: 27 Hunter Street, Hobart, TAS, 7000

LOCATION

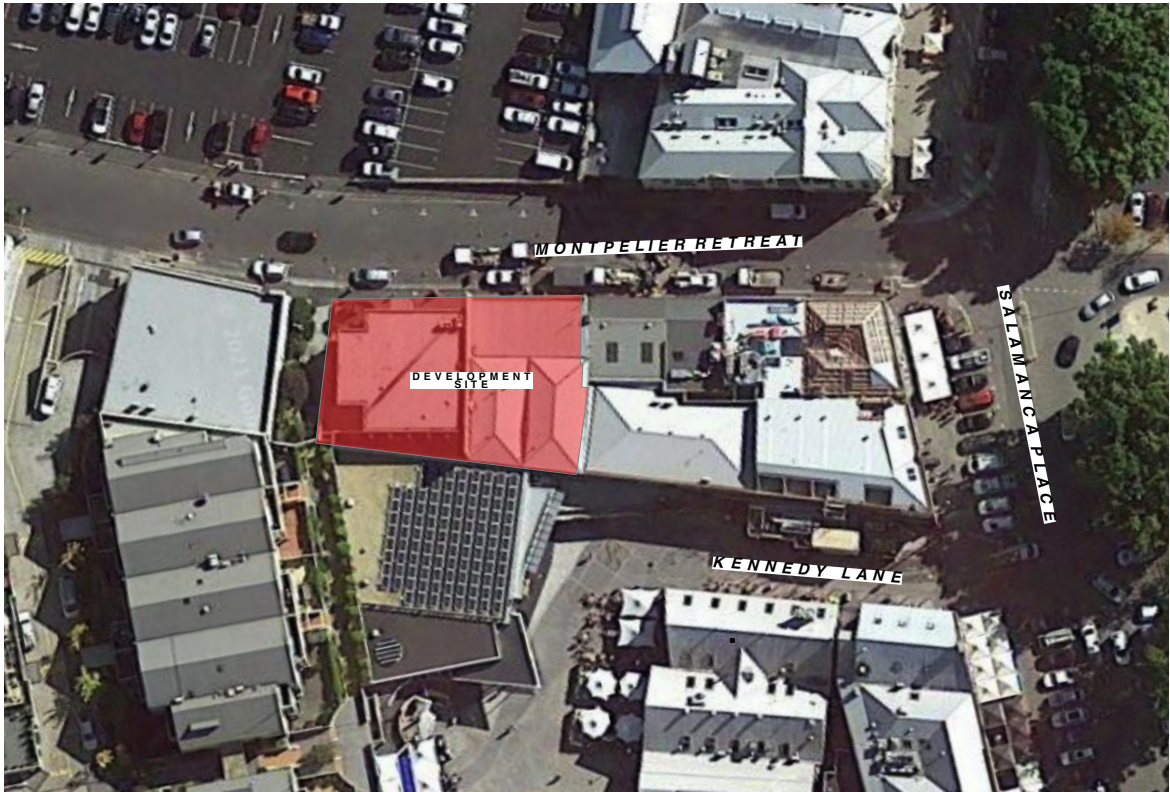
PROJECT NO: 2103
TITLE REFERENCE: 234896/1
SITE AREA: 755 m²
PROPOSED FOOTPRINT: 500 m²
SITE COVERAGE: 100%
CAR PARKING SPACES: 14

SITE DETAILS

CLIMATE ZONE: 7
WIND SPEED N/A
SOIL CLASS: N/A
BAL: N/A
CORROSION: N/A
ALPINE AREA: N/A27

Drawings List

- 2103 - DA00 LOCATION PLAN
2103 - DA01 SITE PLAN
2103 - DA02 EXISTING PLAN
2103 - DA03 DEMOLITION PLAN
2103 - DA04 PROPOSED PARKING PLAN
2103 - DA05 DETAILED PLAN
2103 - DA06 ELEVATION



1

-

LOCATION PLAN

STAGE 1 - CARPARK DA

scale 1:1000

revisions

FOR PLANNING
APPROVAL ONLY

original drawing size

A3

6-8 MONTPELIER RETREATcirca morris-nunn architects

PV + DM Behrakis

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Hobart
TAS, 7001

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info@circamorrisnunn.com.au

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LOCATION PLAN

STAGE 1 - CARPARK DA

status

DEVELOPEMENT APPLICATION

issue date

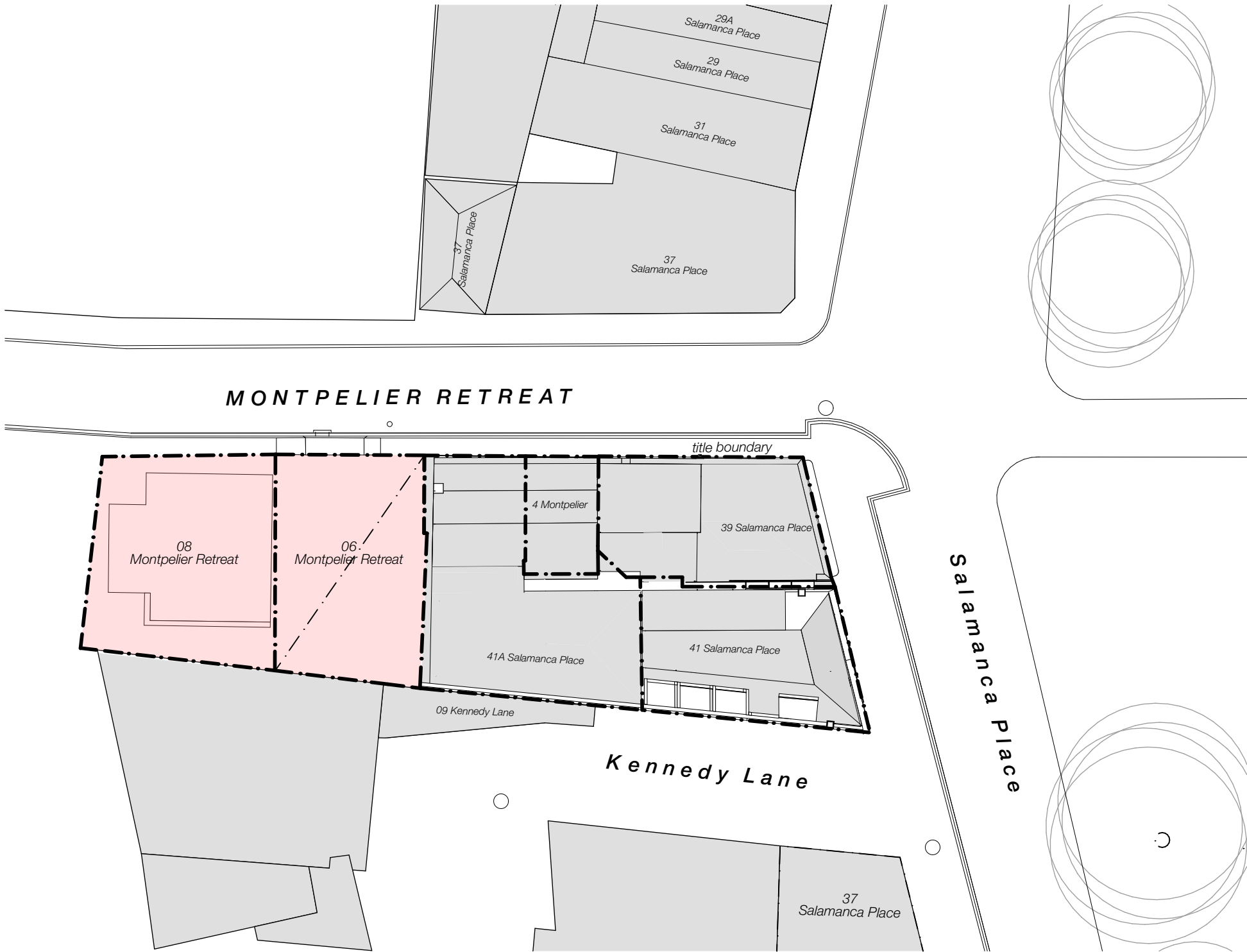
14/5/21

drawing n°

revision

2103-DA00

02



1

-

SITE PLAN 1:500

STAGE 1 - CARPARK DA

scale 1:500

revisions

original drawing size

A3

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SITE PLAN
STAGE 1 - CARPARK DA

status
DEVELOPEMENT APPLICATION

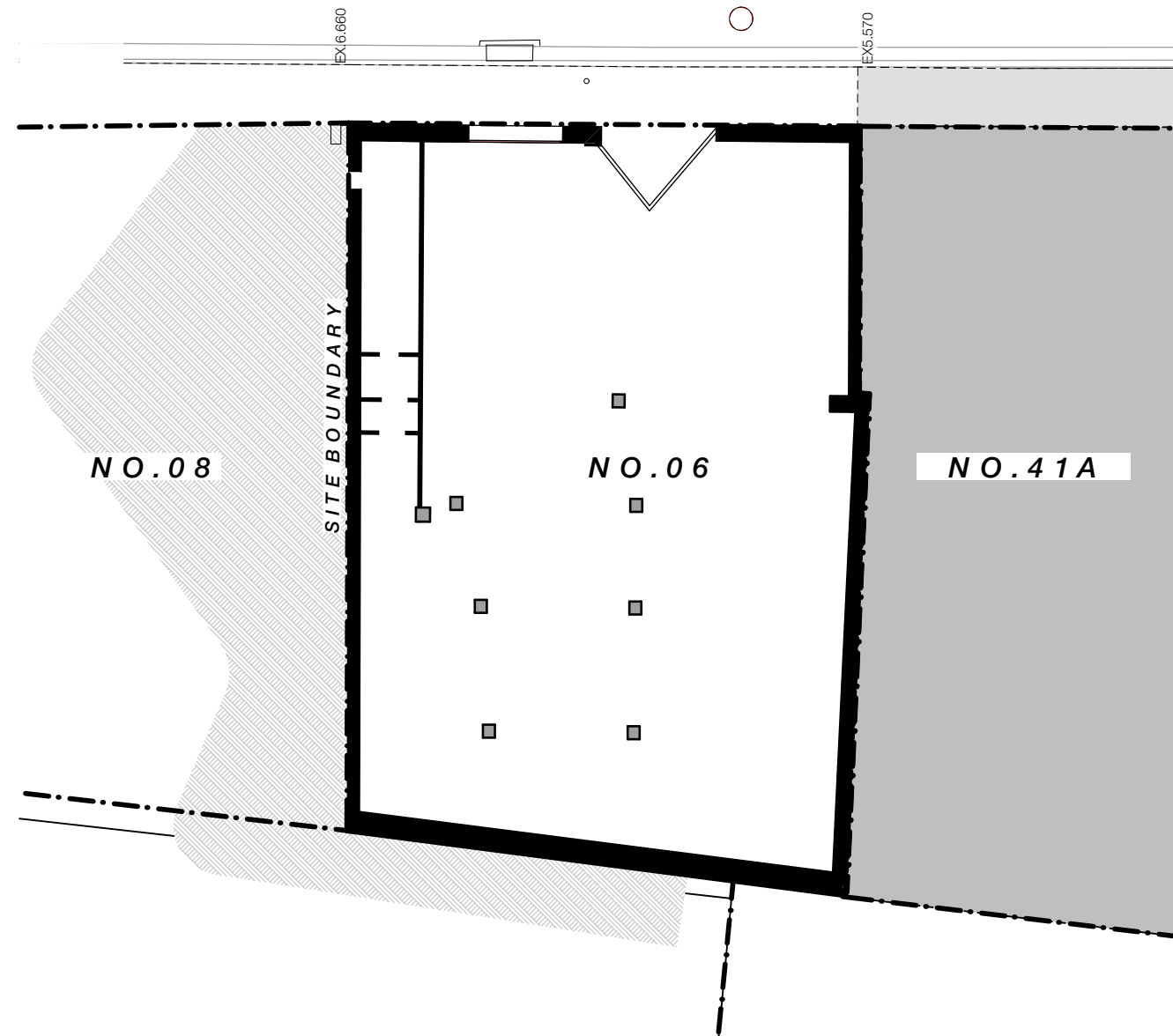
issue date

drawing n°

revision

2103-DA01

MONTPELIER



1	0. GROUND FLOOR EXISTING
-	STAGE 1 - CARPARK DA

scale 1:200

revisions

original drawing size

A3

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6-8 MONTPELIER RETREAT

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EXISTING

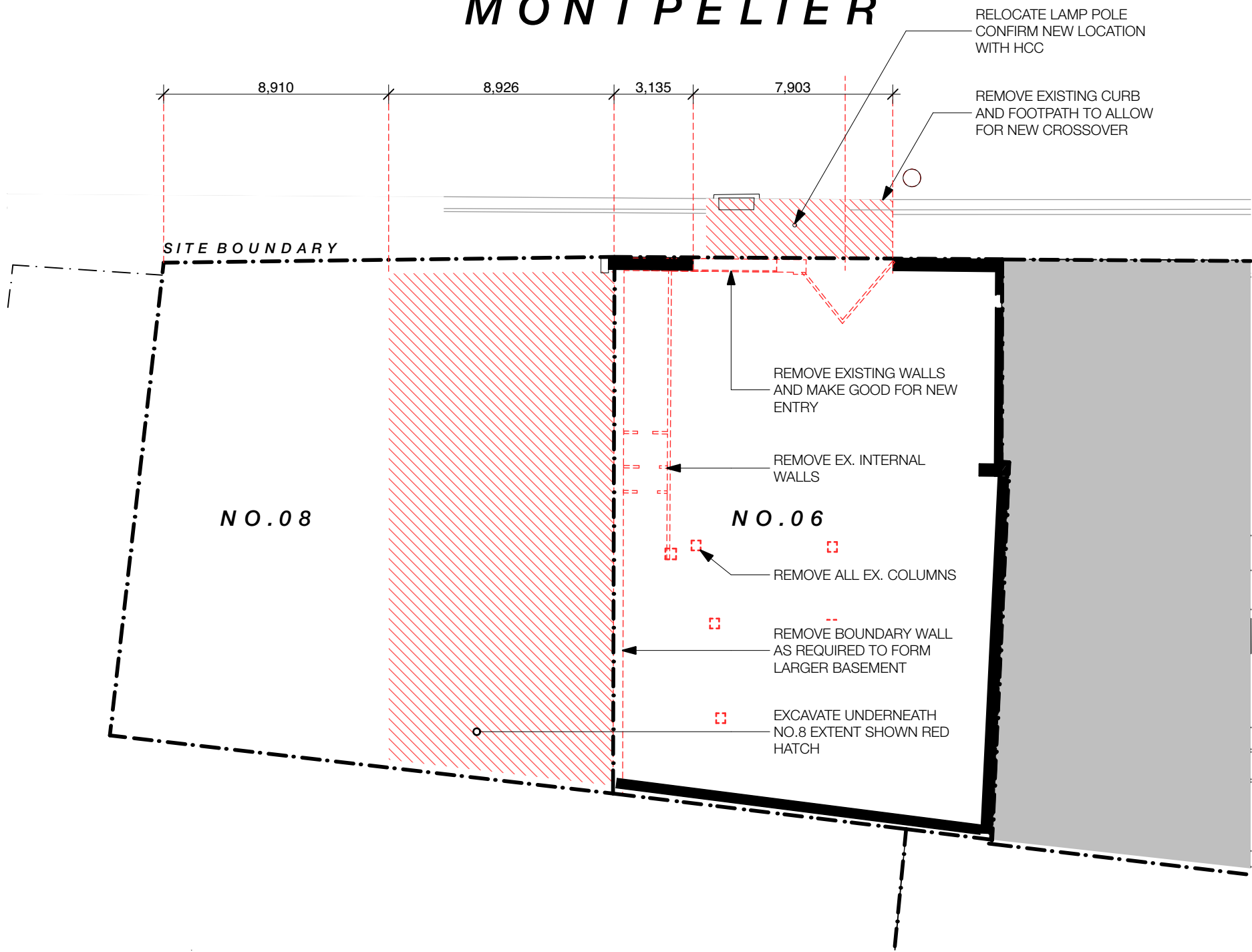
STAGE 1 - CARPARK DA

status **DEVELOPEMENT APPLICATION**

issue date **14/5/21**

drawing n° **2103-DA02** revision **02**

MONTPELIER



1 GROUND FLOOR DEMOLITION
- STAGE 1 - CARPARK DA scale 1:200

revisions

original drawing size

A3

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Hobart
TAS, 7001

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DEMOLITION

STAGE 1 - CARPARK DA

status DEVELOPEMENT APPLICATION

issue date 14/5/21

drawing n° revision

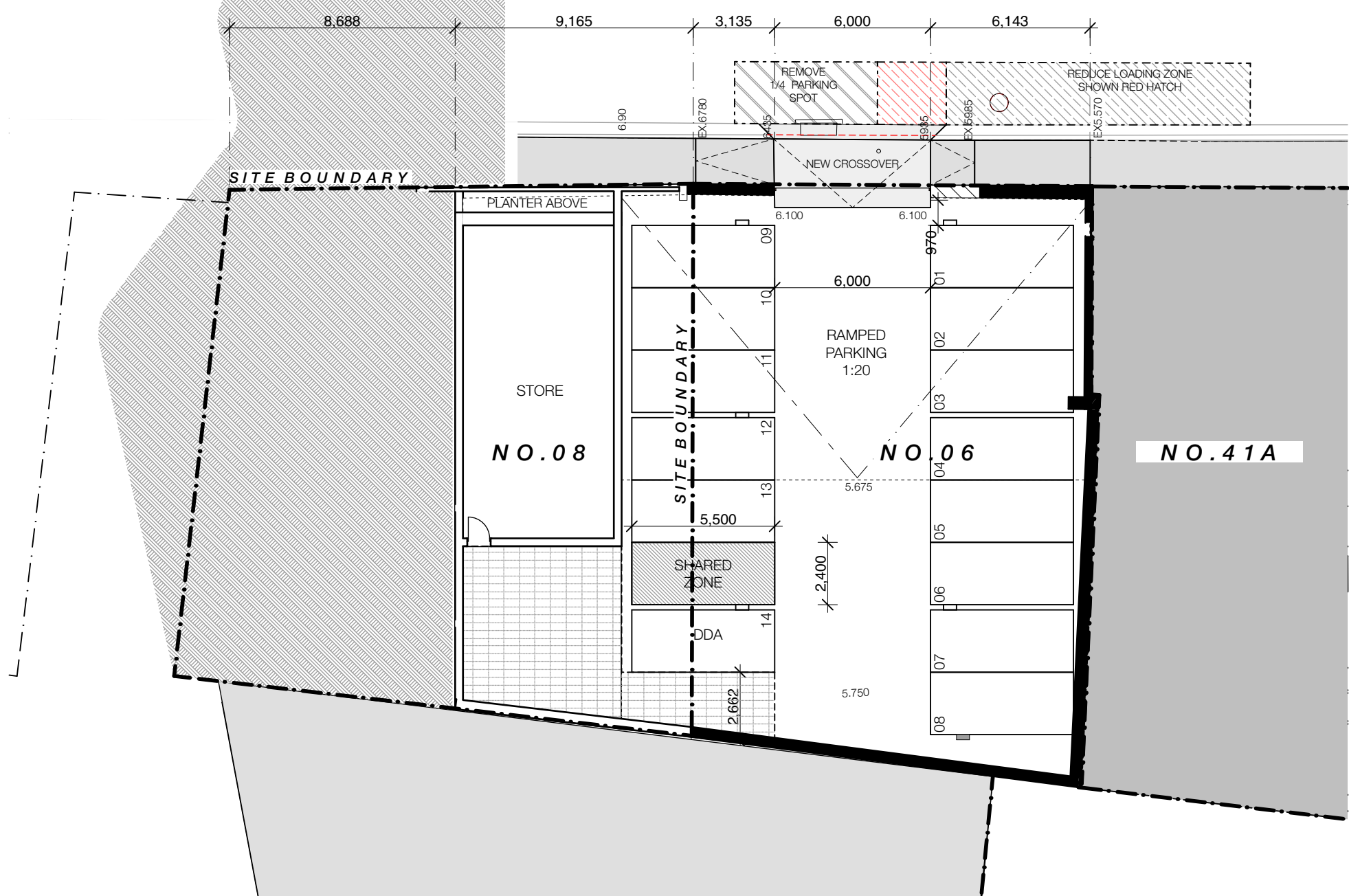
2103-DA03

02

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MONTPELIER



1 GROUND FLOOR PLAN
- STAGE 1 - CARPARK DA scale 1:200

revisions

original drawing size

A3

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6-8 MONTPELIER RETREAT *circa morris-nunn architects*

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PROPOSED PLAN

STAGE 1 - CARPARK DA

status DEVELOPEMENT APPLICATION

issue date 14/5/21

drawing n° revision

2103-DA04 02

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7,200

REMOVE CURB TO ALLOW FOR NEW CROSSOVER TO COMPLY WITH LGAT AND TASMANIAN ROAD STANDARDS

RECONFIGURE STORMWATER GRATED PIT TO ALLOW FOR NEW CROSSOVER TO COMPLY WITH LGAT AND TASMANIAN ROAD STANDARDS

RELOCATE LIGHT POLE, CONFIRM LOCATION WITH HCC

LOADING ZONE TO BE SHORTEN SHOWN RED DASHED

1/4 PARK TO BE REMOVED

EX.6780

6135

5935

EX.5985

EX. FOOTPATH [BITUMEN]

TRANSITION RAMP [BITUMEN]

NEW CROSSOVER [CONCRETE]

MESH ROLLER DOOR

TRANSITION RAMP [BITUMEN]

NEW BLOCKWORK INFILL WALL TO EXISTING OPENING RENDERED FINISH TO MATCH EXISTING

EX. FOOTPATH [BITUMEN]

6120

6100

6,000

6,100

1,000

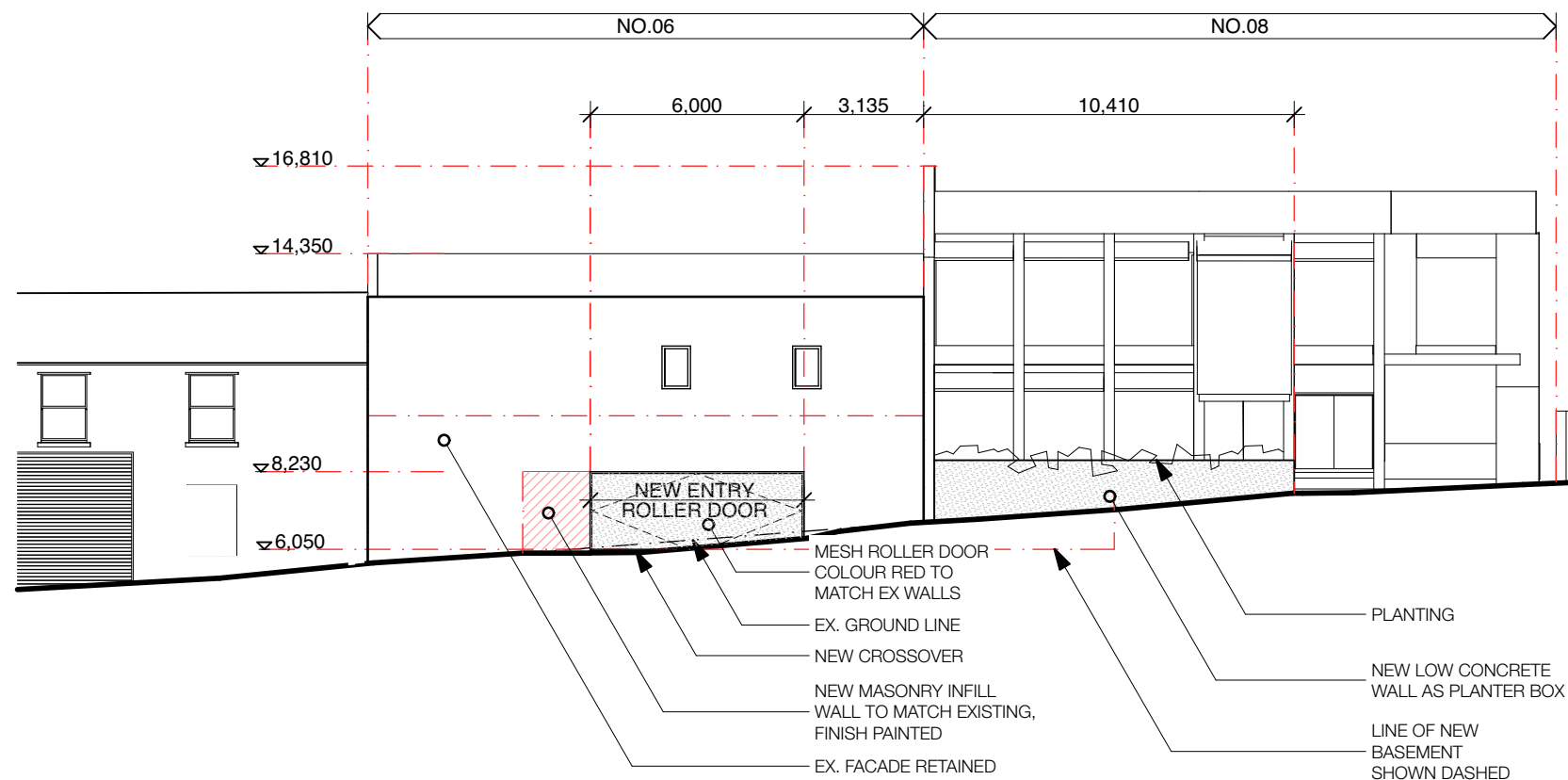
scale 1:50

original drawing size

02

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FOR PLANNING
APPROVAL ONLY



E03 **MONTPELIER RETREAT**
- **STAGE 1 - CARPARK DA** scale 1:200

revisions

original drawing size

A3

FOR PLANNING
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6-8 MONTPELIER RETREAT
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ELEVATION
STAGE 1 - CARPARK DA

status **DEVELOPEMENT APPLICATION**

issue date **17/5/21**

drawing n° revision

2103-DA06 **03**

08 February 2022

Cameron Sheriff
Hobart City Council
GPO Box 503
HOBART TAS 7001

**RE: 6 MONTPELIER RETREAT & 8 MONTPELIER RETREAT, BATTERY POINT & ADJACENT ROAD RESERVE PARTIAL DEMOLITION, ALTERATIONS, PARTIAL CHANGE OF USE TO CAR PARKING, AND ASSOCIATED WORKS
APPLICATION NO. PLN21455**

Please find attached and following a response to Council's request for further information.

HER 1 Detailed drawings of the proposed low concrete planter wall outside no. 8 Montpelier Retreat showing its location, height and materials, colours and finishes.

Response

Amended details drawings showing the proposed appearance of the low concrete planter are attached to this correspondence. The planter box would have a light grey concrete appearance to match with the existing wall behind. The maximum height would be 1.5m and the planter would improve the appearance of the space which is currently plan bitumen.

PA1 To make adequate provisions for vehicular access it is to be demonstrated that sufficient headroom complies with Section 5.3 of Australian Standard AS/NZS 2890.1:2004. Please submitted amended drawings that clearly show the minimum vertical clearance for vehicles accessing the site, particularly at both sides of the vehicle access roller door. To satisfy AS/NZS 2890.1:2004 Section 5.3 a minimum vertical clearance of 2.2m is to be provided.

Response

An amended plan showing the details to the top of the carparking access.

Sw 1 To satisfy the *Sullivans Cove Planning Scheme 1997* (and its associated *Environment Management & Pollution Control Act 1994*), particularly Part E Schedule 8 Environmental Management, please provide best practice treatment of stormwater within the proposed basement carpark. This scheme does not provide specific treatment targets however below is the industry accepted standard as per the interim planning schemes*.

Please provide details of the stormwater connection for this lot including an inspection opening within the lot for the stormwater connection to join to, and including how the stormwater quality treatment device will connect.

This requirement is as per 29.4 (e) so as to ensure use of the site is managed to minimise environmental harm, and also to meet the section 29.5 environmental objective for water quality - best practice environmental management for water ... water use and disposal must be managed to minimise off site disposal protect and improve ambient water quality. Principles include pre-treatment of water prior to disposal.

* Table E7.1 Acceptable Stormwater Quality and Quantity Targets

80% reduction in the average annual load of total suspended solids (TSS) based on typical urban stormwater TSS concentrations.

45% reduction in the average annual load of total phosphorus (TP) based on typical urban stormwater TP concentrations.

45% reduction in the average annual load of total nitrogen (TN) based on typical urban stormwater TN concentrations.

Response

The fall of the carpark toward the road means that it would be extremely unlikely that any stormwater would be able to enter the carpark. Water would need to run uphill or all of Salamanca would need to be in flood.

Given the carpark is fully undercover and below existing roof and no new or additional runoff would be created by the proposal, providing a mechanical stormwater treatment device would therefore be pointless.

The only time that any runoff from the carpark would be generated would be via infrequent hose down or during an emergency.

The Montpelier Street side of the property connects to the existing street pit, there are also two kerb outlets. Three stormwater outlets will be provided within the carpark, connected to a pump that will deliver flow back up to the Montpelier connection in there is any needs for a water release within the carpark, fire, washdown etc. These stormwater outlets will be designed as part of detailed engineering plans including drainage routes, access, council plans.

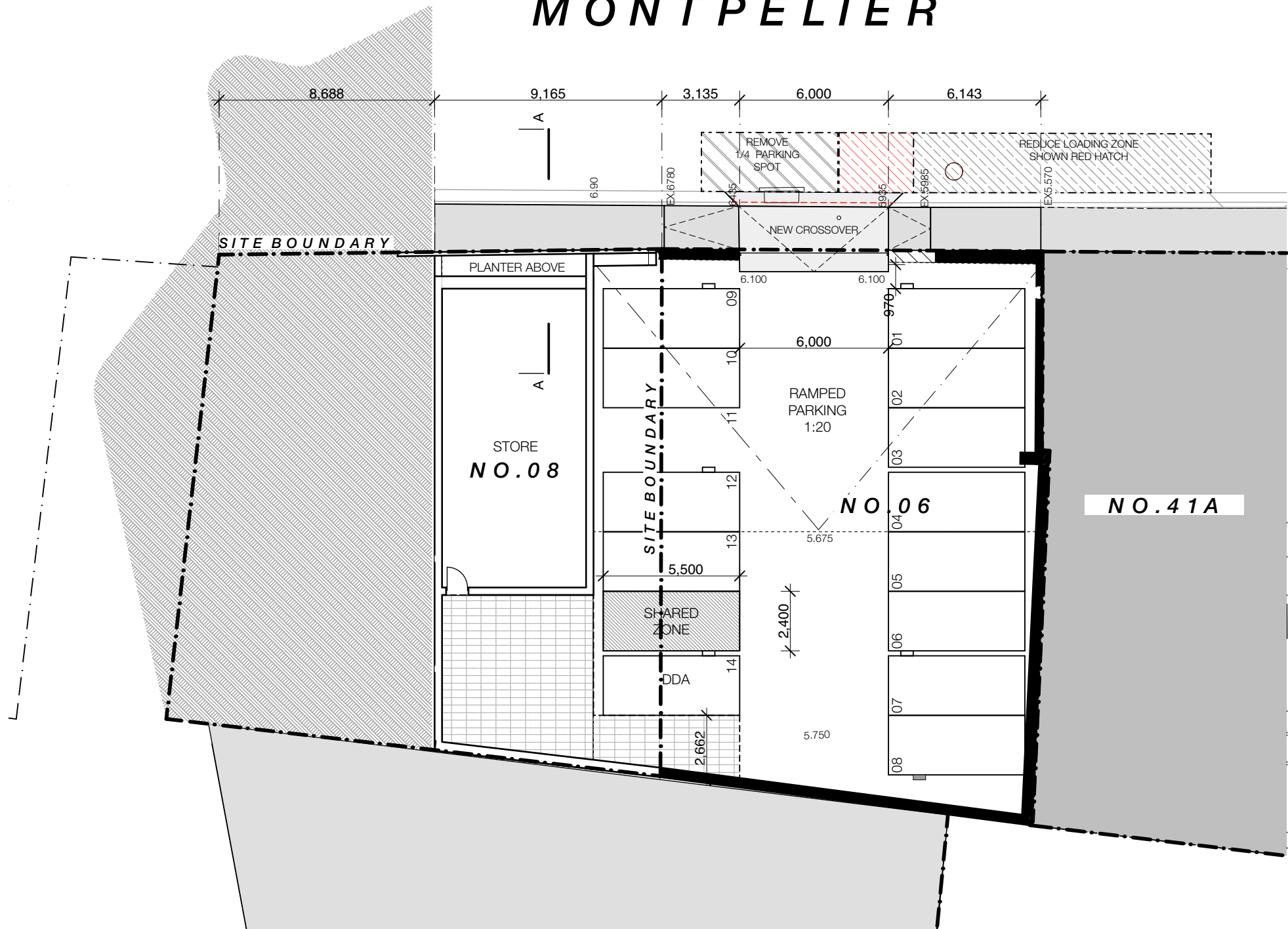
I trust that this information now fully satisfies Council's request for further information. If you have any further queries, please do not hesitate to contact me on 0438 376 840 or email evan@e3planning.com.au.

Regards



Evan Boardman
Grad Dip URP, B ScEnv, B Econ

MONTPELIER



1 GROUND FLOOR PLAN
- STAGE 1 - CARPARK DA scale 1:200

revisions

FOR PLANNING
APPROVAL ONLY

original drawing size

A3

6-8 MONTPELIER RETREAT
PV + DM Behrakis

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Hobart
TAS, 7001

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PROPOSED PLAN
STAGE 1 - CARPARK DA

status DEVELOPEMENT APPLICATION

issue date

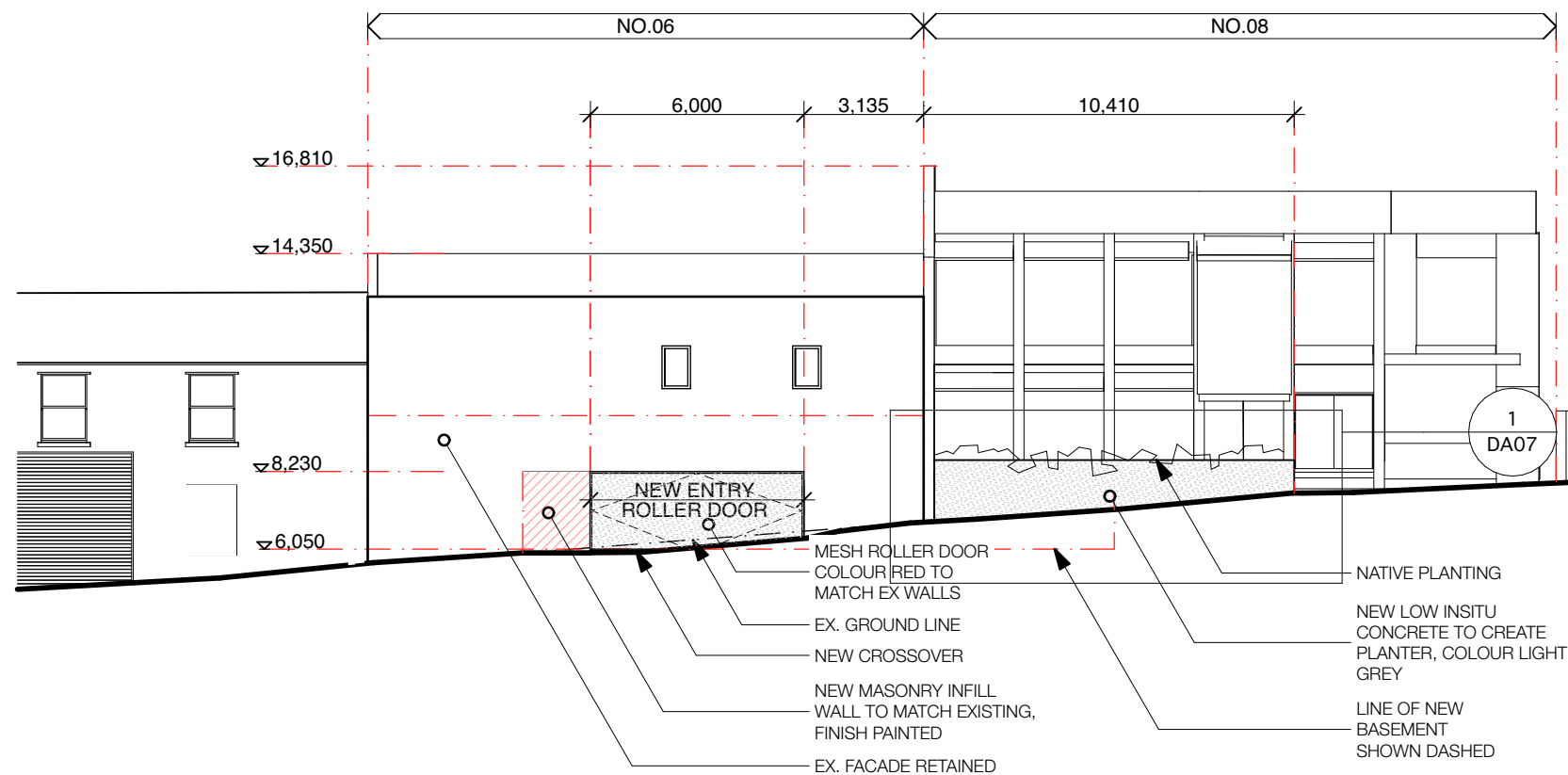
drawing n°

2103-DA04

1/12/21

Revision

03



E03 **MONTPELIER RETREAT**
- **STAGE 1 - CARPARK DA** scale 1:200

revisions

original drawing size

FOR PLANNING
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A3

6-8 MONTPELIER RETREAT PV + DM Behrakis

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ELEVATION

STAGE 1 - CARPARK DA

DEVELOPEMENT APPLICATION

status
issue date

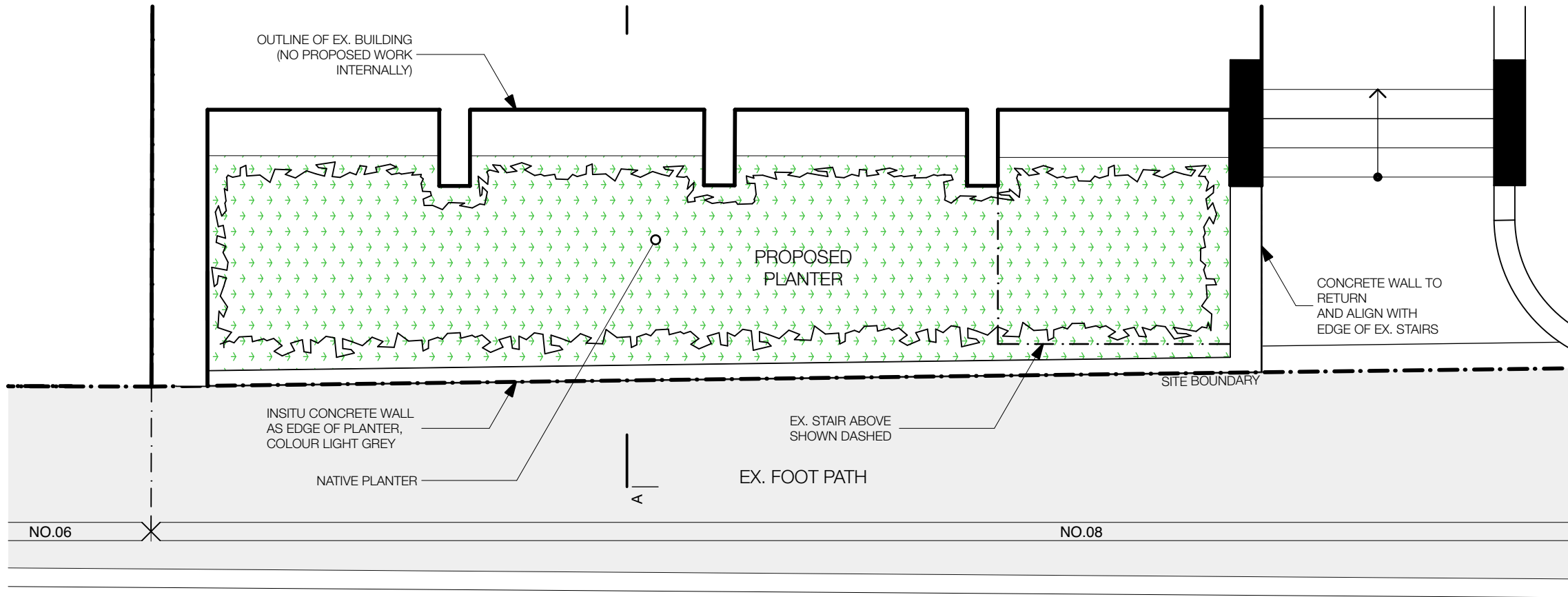
drawing n°

2103-DA06

1/12/21

Revision

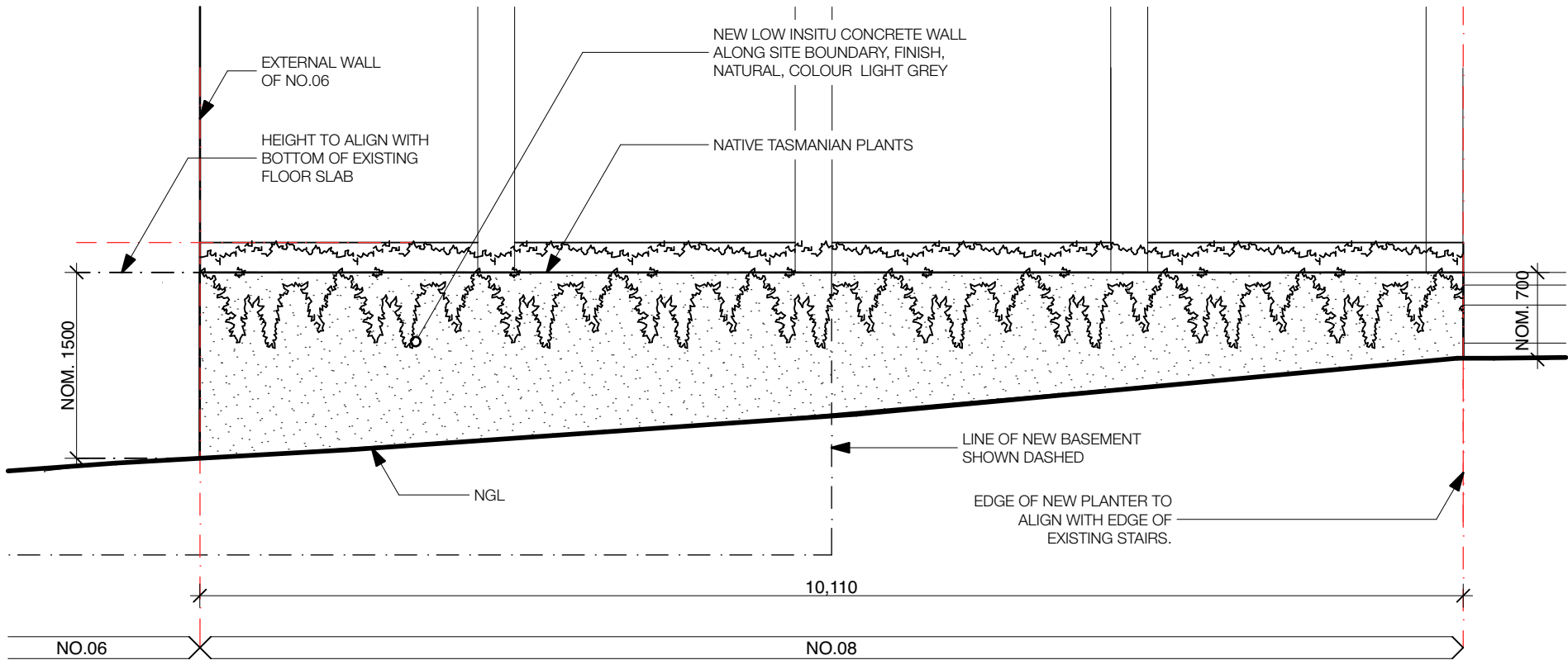
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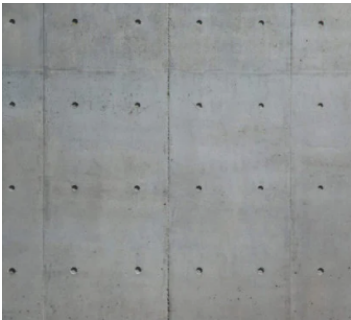
original drawing size

A3

DETAILED PLAN
- **STAGE 1 - CARPARK DA** scale 1:50



DETAILED ELEVATION
- **STAGE 1 - CARPARK DA** scale 1:50



CONCRETE EXAMPLE



PLANTING EXAMPLE

6-8 MONTPELIER RETREAT
PV + DM Behrakis

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Hobart
TAS, 7001

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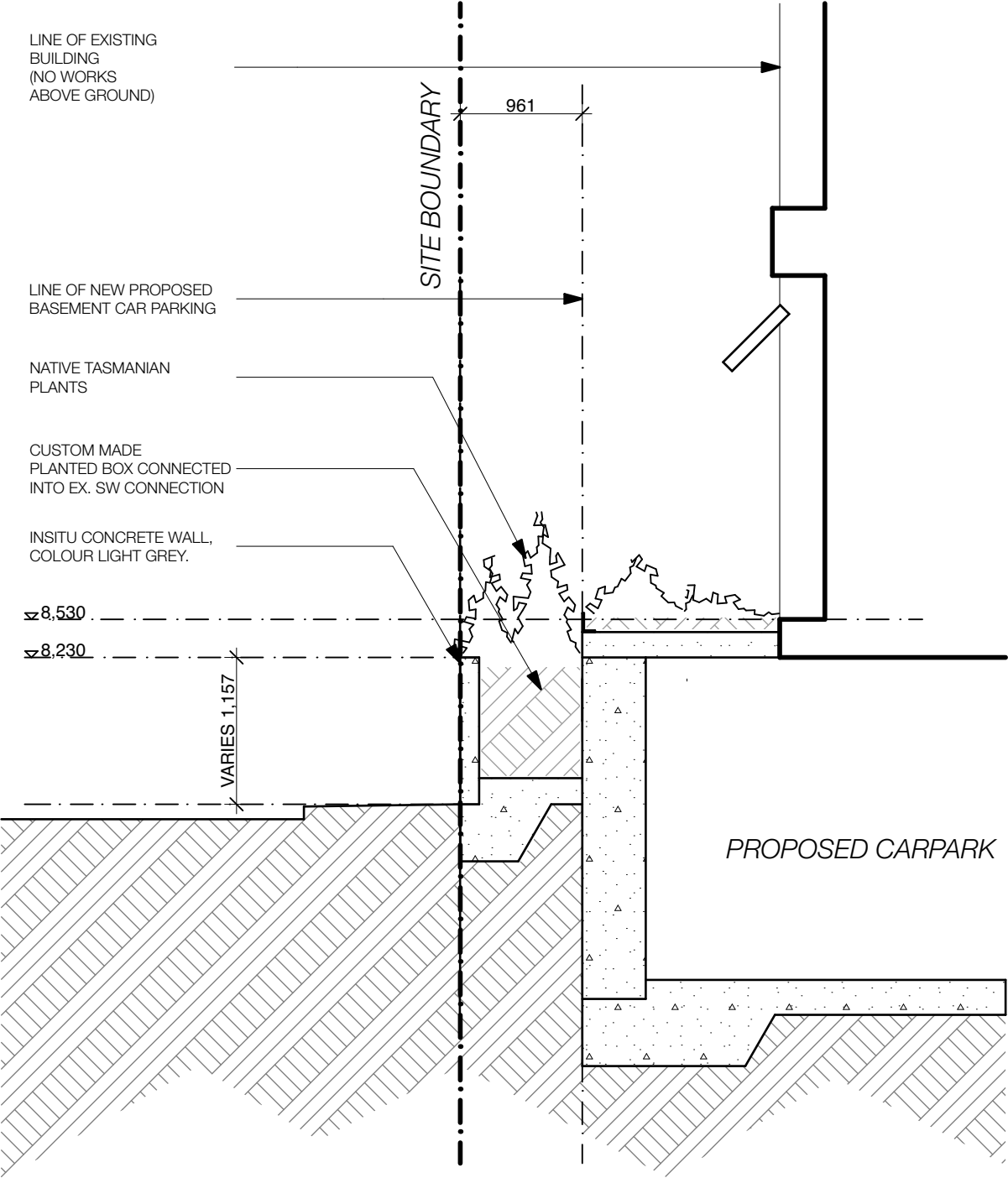
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DETAILED DRAWINGS
STAGE 1 - CARPARK DA

status	DEVELOPEMENT APPLICATION
issue date	1/12/21
drawing n°	Revision

2103-DA07

01



PLANTER DETAIL
- **STAGE 1 - CARPARK DA** scale 1:50

revisions

original drawing size

FOR PLANNING
APPROVAL ONLY

A3

6-8 MONTEPELIER RETREAT
PV + DM Behrakis

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DETAIL SECTION A_A
STAGE 1 - CARPARK DA

status **DEVELOPEMENT APPLICATION**

issue date

drawing n°

2103-DA08

1/12/21

Revision

01



06 March 2022

Hobart City Council

Via email

**RE: 6 MONTPELIER RETREAT & 8 MONTPELIER RETREAT, BATTERY POINT
& ADJACENT ROAD RESERVE PARTIAL DEMOLITION, ALTERATIONS,
PARTIAL CHANGE OF USE TO CAR PARKING, AND ASSOCIATED
WORKS APPLICATION NO. PLN-21-455**

Please find attached an amended plan which provides details of the stormwater connection for this lot including an inspection opening within the lot for the stormwater connection to join to, and including how the stormwater quality treatment device will connect.

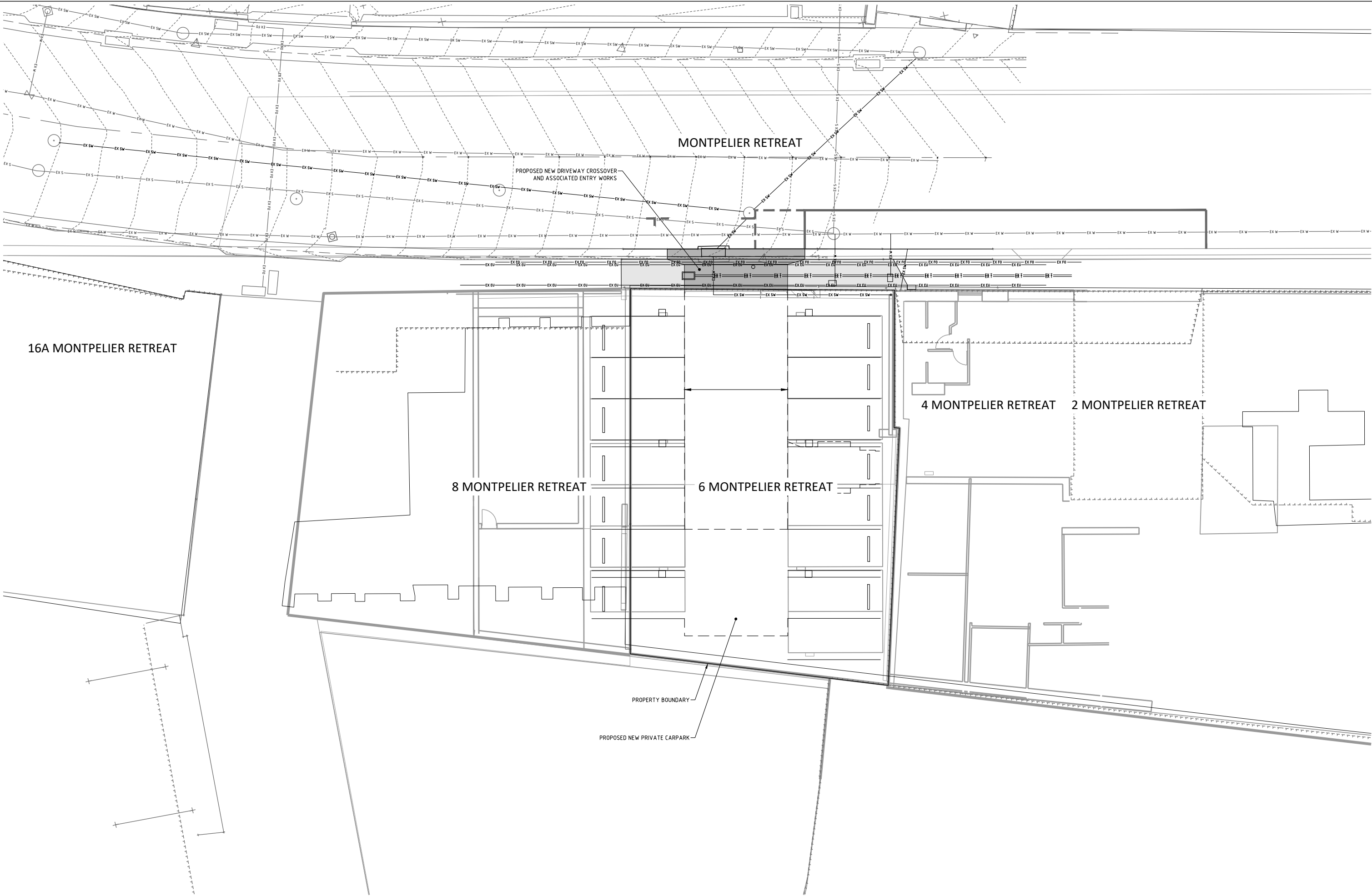
It is not proposed to connect the planter box to Council's stormwater infrastructure, but rather provide detention at the base of the planter box for water and a filtered outlet to ensure that during heavy rainfall events no soil or other material escapes onto the footpath.

If you have any further queries, please do not hesitate to contact me on 0438 376 840 or email evan@e3planning.com.au.

Regards

Evan Boardman
Grad Dip URP, B ScEnv, B Econ

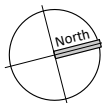




C	PLANNING APPROVAL	SP	12.11.2021		
B	PLANNING APPROVAL	SP	08.10.2021		
REV	DESCRIPTION	APP'D	DATE		

REV	DESCRIPTION	APP'D	DATE		

THIS DRAWING HAS NOT BEEN
APPROVED FOR CONSTRUCTION

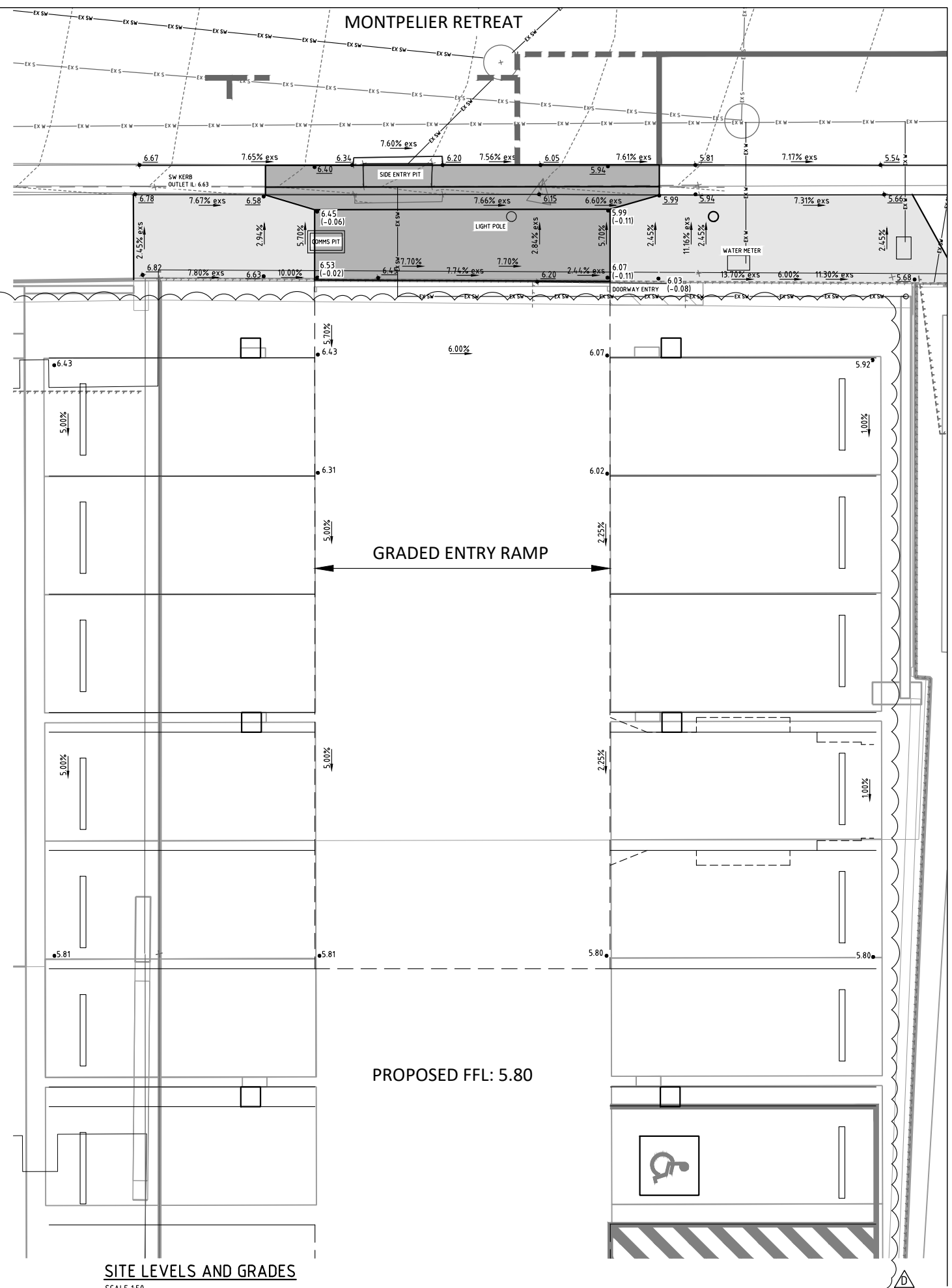


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ROBERTS**
CONSULTING
ENGINEERS

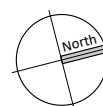
159 DAVEY ST, HOBART
TASMANIA, AUSTRALIA 7000
www.gandyandroberts.com.au
mail@gandyandroberts.com.au
ph 03 6223 8877 fx 03 6223 7183

NEW CARPARK
6-8 MONTEPELIER RETREAT, SALAMANCA
TASMANIA, 7004
DRAWING TITLE
SITE PLAN

0 50mm SCALE 1:100 @A1		
DESIGNED DH	DRAWN DH	CHECKED AC
PROJECT 21.0317	DRAWING C020	REVISION C



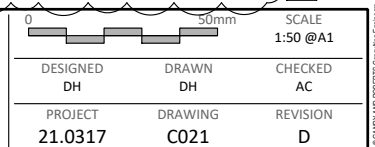
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NEW CARPARK
6-8 MONTEPIELIER RETREAT, SALAMANCA
TASMANIA, 7004
DRAWING TITLE
CONCEPT SITEWORKS PLAN 01



UGANDI AND HUBERT IS CONSULTING ENGINEERS.

3/3/2022 3:12:25 PM - S:\projects\2021\10\101714-Montpelier-Retreat\10-02-Drainage\DWG - Working drawings_montpelier_101714.dwg

C	PLANNING APPROVAL	SP	12.11.2021
B	PLANNING APPROVAL	SP	08.10.2021
REV	DESCRIPTION	APP'D	DATE

REV	DESCRIPTION	APP'D	DATE

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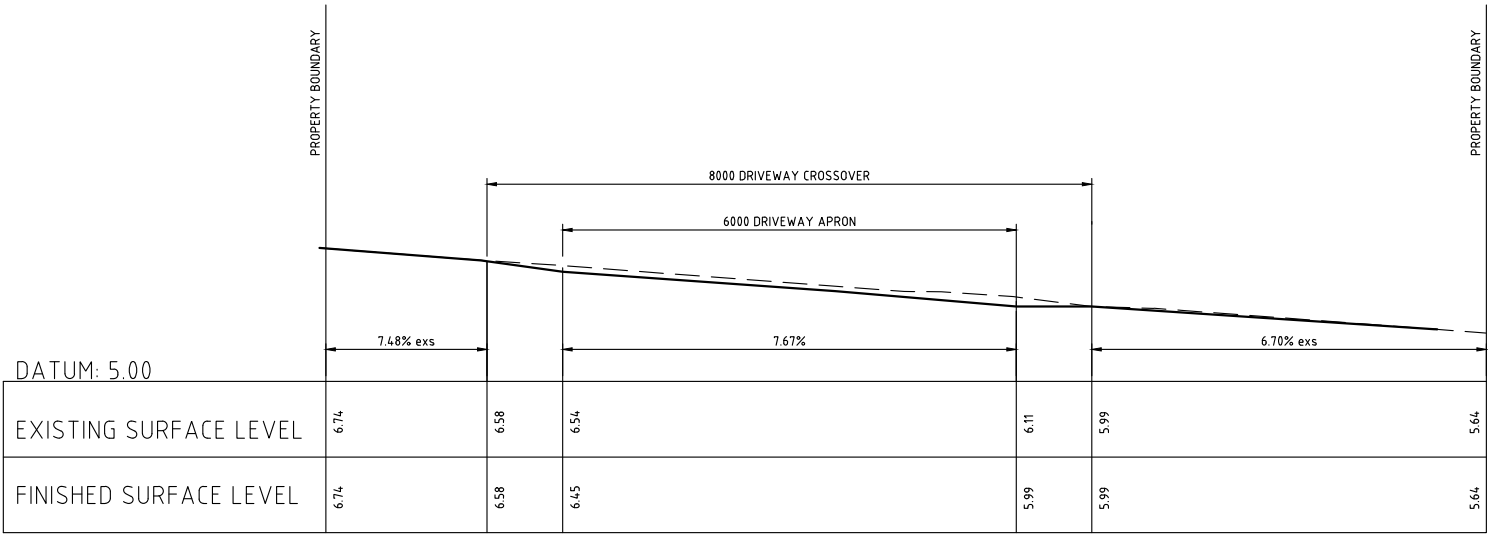
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NEW CARPARK
6-8 MONTEPELIER RETREAT, SALAMANCA
TASMANIA, 7004
DRAWING TITLE
FOOTPATH SECTIONS 01

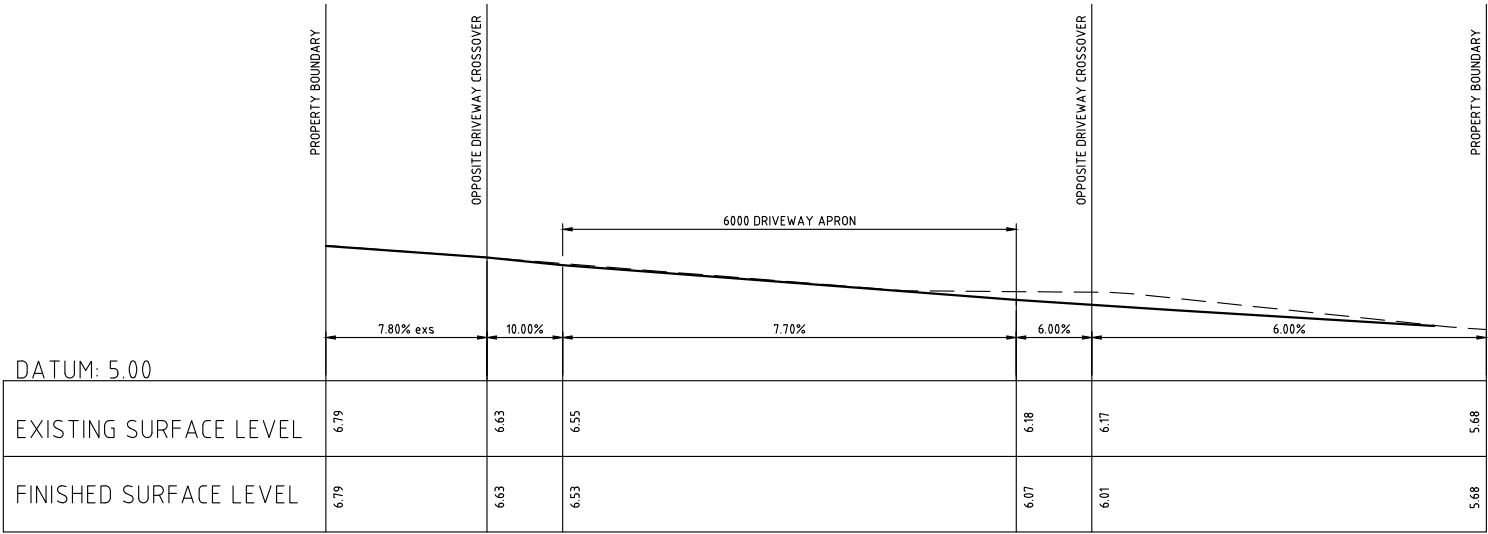
050mm			SCALE
DESIGNED DH			CHECKED AC
PROJECT 21.0317			REVISION C

GANDY AND ROBERTS Consulting Engineers



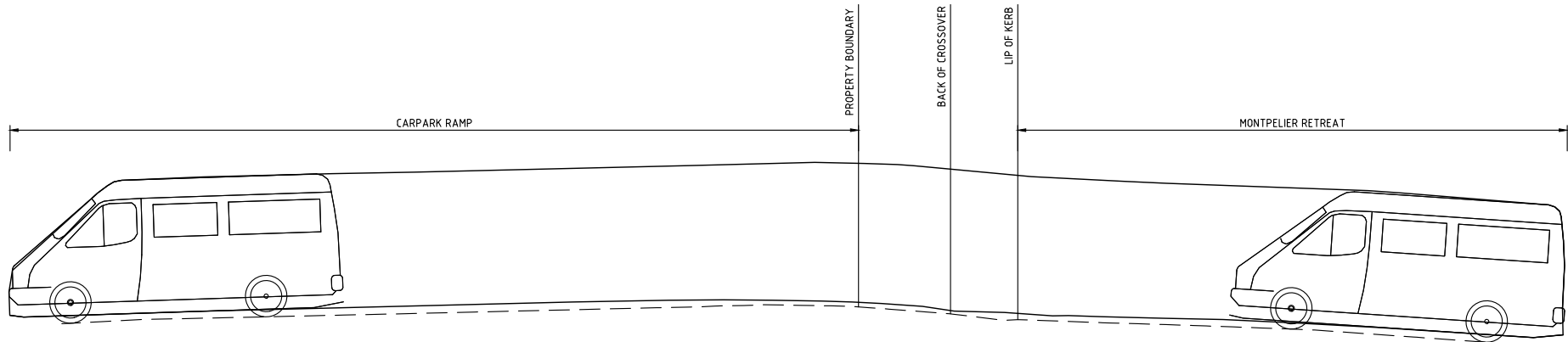
FOOTPATH LONG SECTION: BACK OF EXISTING KERB/NEW CROSSOVER

SCALE: 1:50

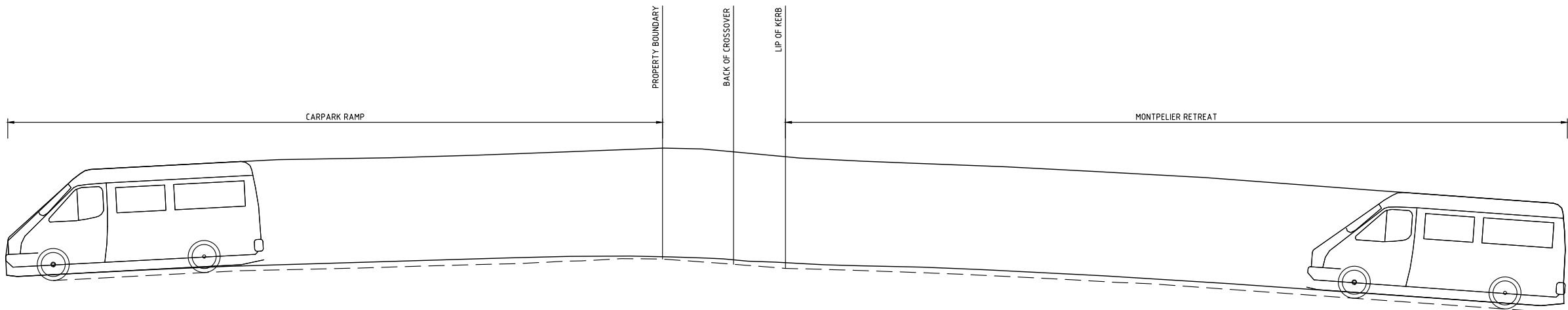


FOOTPATH LONG SECTION: BACK OF PATH

SCALE: 1:50



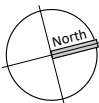
ENTRY MOVEMENT B99 (INNER WHEEL PATH)
SCALE: 1:50



ENTRY MOVEMENT B99 (OUTER WHEEL PATH)
SCALE: 1:50

A	PLANNING APPROVAL	SP	12.11.2021				
REV	DESCRIPTION	APP'D	DATE	REV	DESCRIPTION	APP'D	DATE

THIS DRAWING HAS NOT BEEN
APPROVED FOR CONSTRUCTION

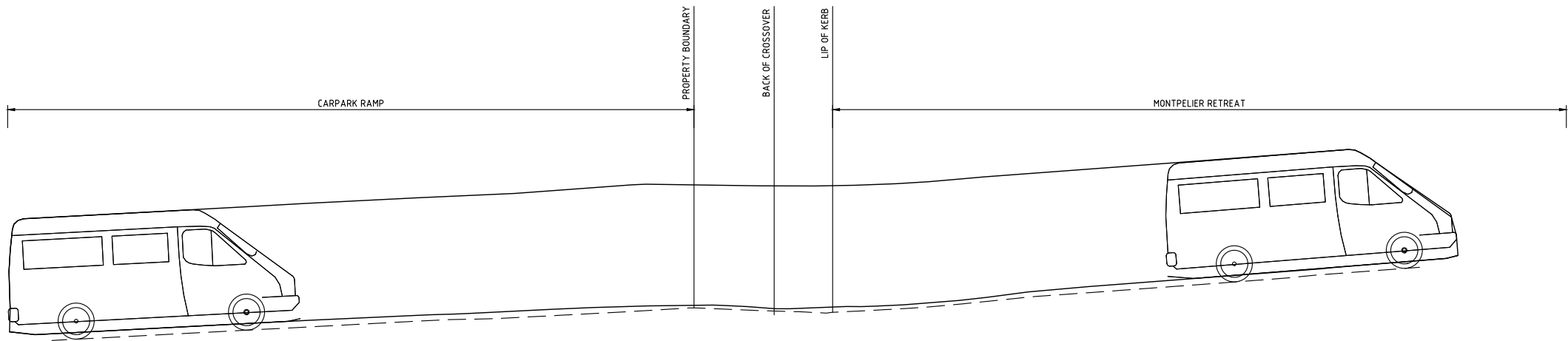


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ENGINEERS

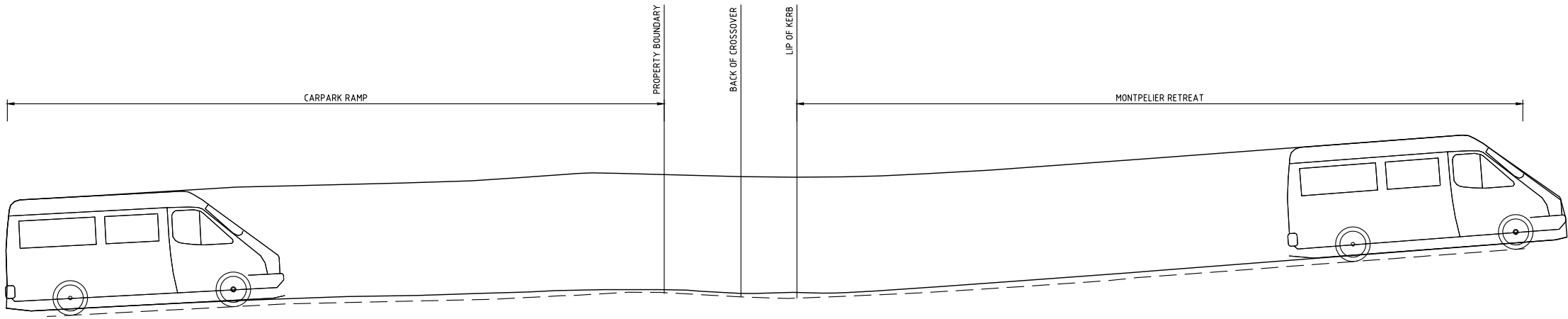
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ph 03 6223 8877 fx 03 6223 7183

NEW CARPARK
6-8 MONTPELIER RETREAT, SALAMANCA
TASMANIA, 7004
DRAWING TITLE
VERTICAL CLEARANCE TEMPLATES 01

0 50mm		SCALE 1:50 @A1
DESIGNED DH	DRAWN DH	CHECKED AC
PROJECT 21.0317	DRAWING C024	REVISION A



EXIT MOVEMENT B99 (INNER WHEEL PATH)
SCALE: 1:50

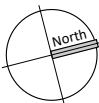


EXIT MOVEMENT B99 (OUTER WHEEL PATH)
SCALE: 1:50

A	PLANNING APPROVAL	SP	12.11.2021
REV	DESCRIPTION	APP'D	DATE

REV	DESCRIPTION	APP'D	DATE

THIS DRAWING HAS NOT BEEN
APPROVED FOR CONSTRUCTION

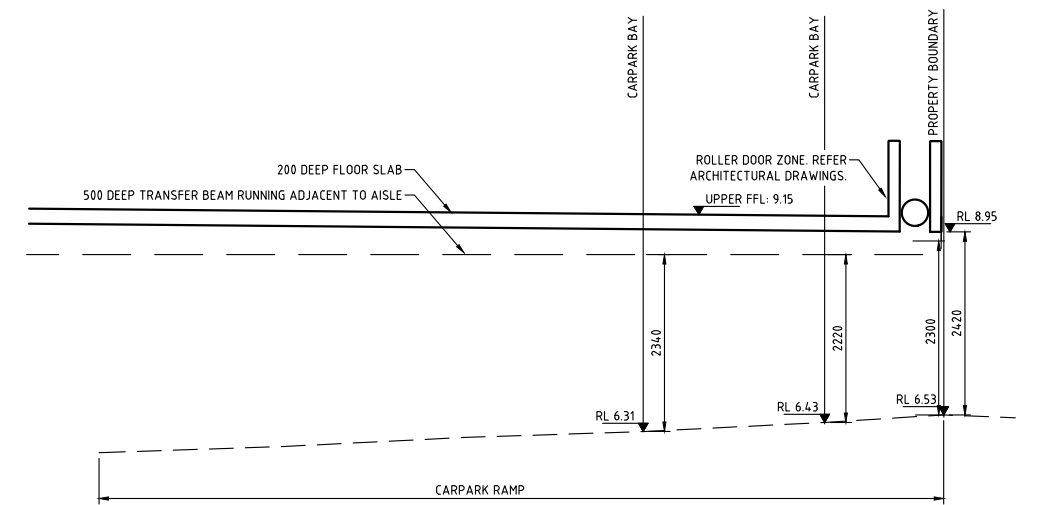


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NEW CARPARK
6-8 MONTELIER RETREAT, SALAMANCA
TASMANIA, 7004
DRAWING TITLE
VERTICAL CLEARANCE TEMPLATES 02

0 50mm		SCALE 1:50 @A1
DESIGNED DH	DRAWN DH	CHECKED AC
PROJECT 21.0317	DRAWING C025	REVISION A



200 DEEP FLOOR SLAB

500 DEEP TRANSFER BEAM RUNNING ADJACENT TO AISLE

UPPER FFL: 9.15

2420

2880

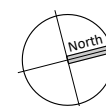
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6000 DRIVEWAY ENTRY

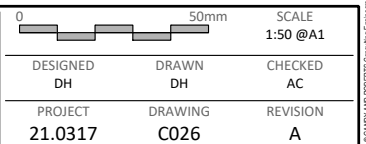
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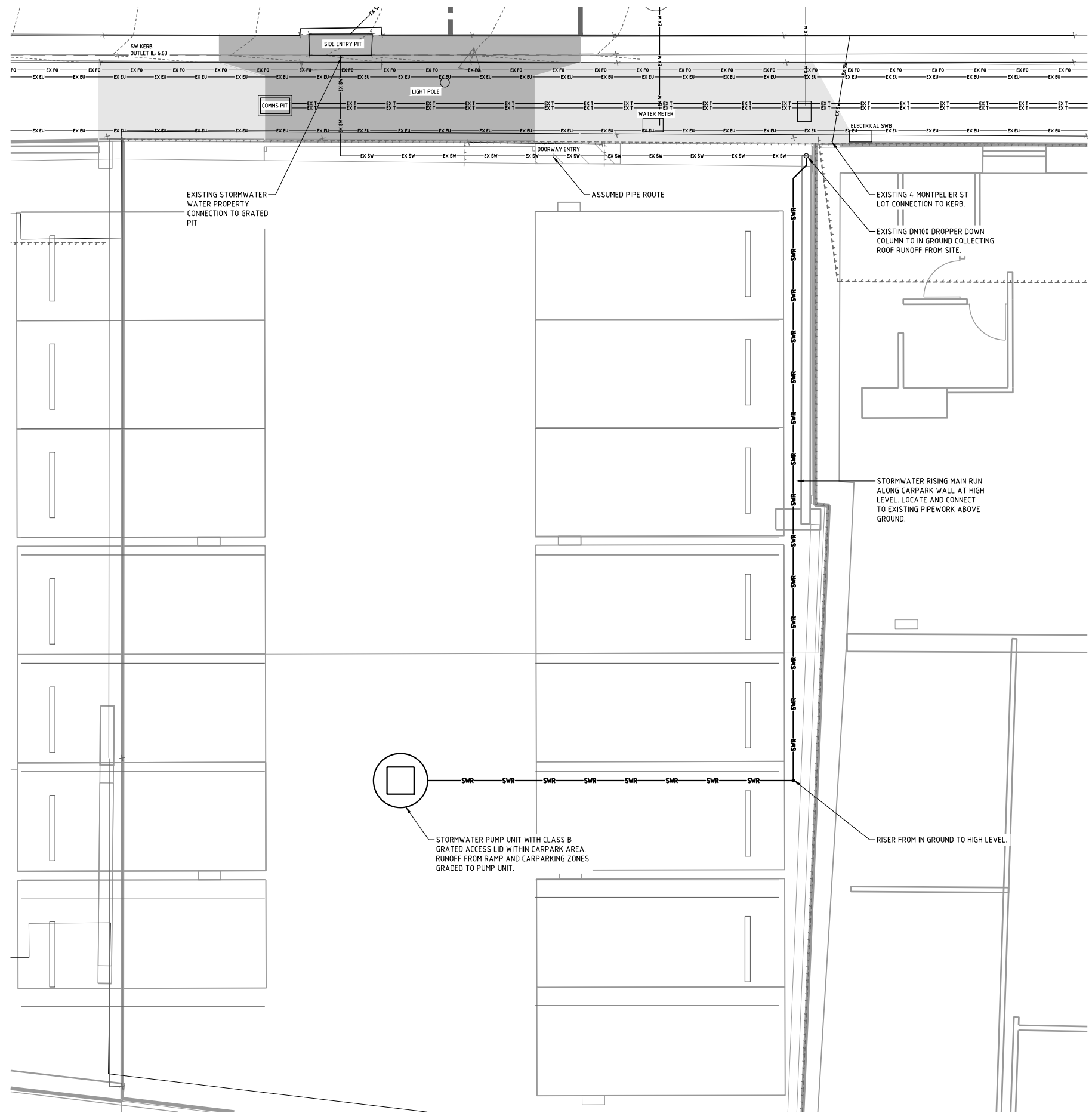
**GANDY AND
ROBERTS**
**CONSULTING
ENGINEERS**

NEW CARPARK
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TASMANIA, 7004
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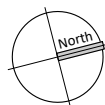
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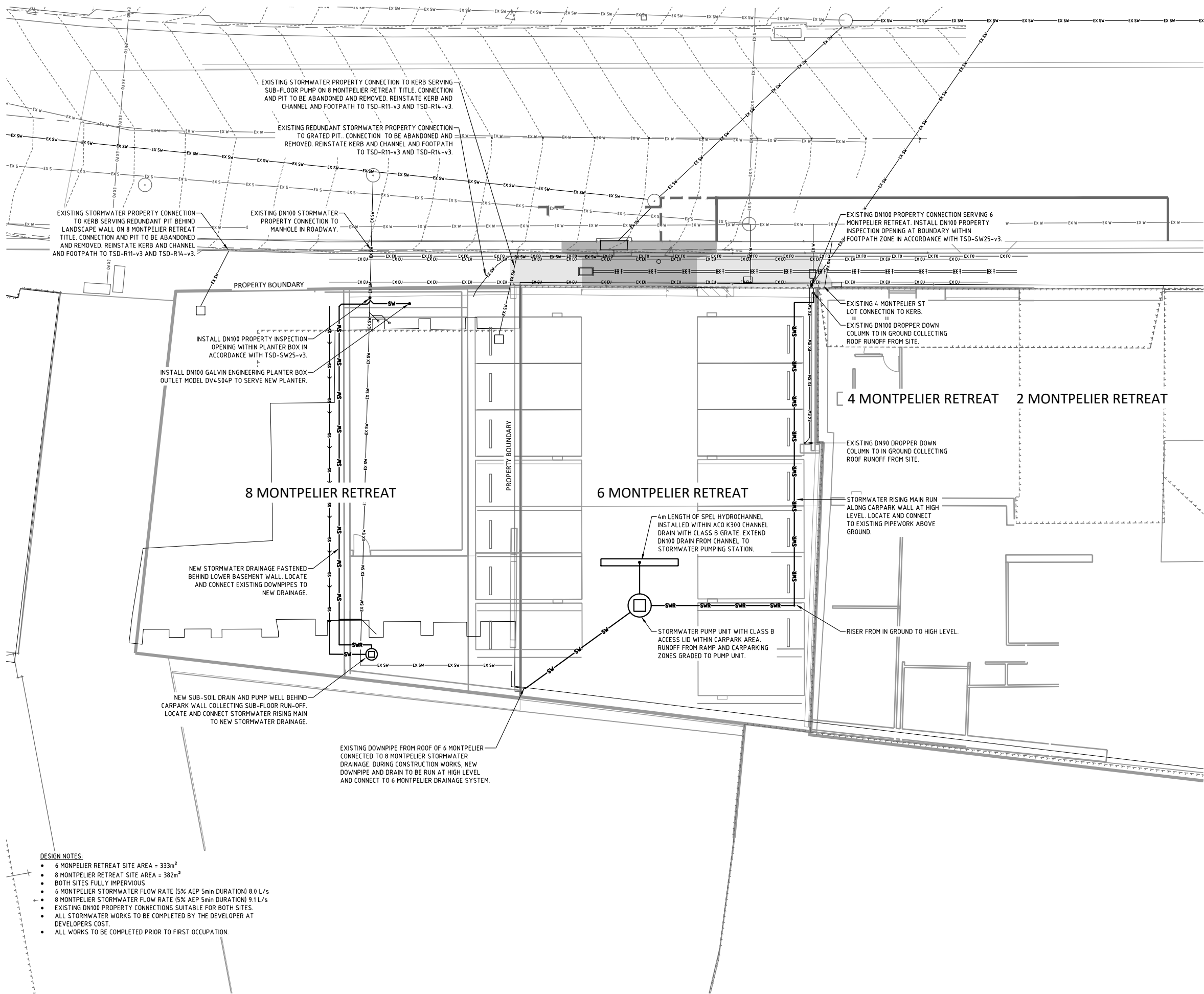
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NEW CARPARK
6-8 MONTELIER RETREAT, SALAMANCA
TASMANIA, 7004
DRAWING TITLE
CONCEPT STORMWATER PLAN

0 50mm		SCALE 1:50 @A1
DESIGNED DH	DRAWN DH	CHECKED AC
PROJECT 21.0317	DRAWING C027	REVISION A

GANDY AND ROBERTS Consulting Engineers



STORMWATER TREATMENT TRAIN EFFECTIVENESS

TREATMENT TRAIN DESCRIPTION:
INSTALLATION OF 4M OF SPEL HYDROCHANNEL WITHIN UNDERGROUND CARPARK AREA. ALL RUNOFF FROM CARPARK ZONE TO BE DIRECTED TO CHANNELS. FOR ANALYSIS PURPOSES, IT HAS BEEN CONSIDERED THAT 25% OF THE UNDERGROUND CARPARK (TOTAL AREA=400M²) IS FULLY OPEN TO RAINFALL.

	SOURCES	RESIDUAL LOAD	% REDUCTION
FLOW (ML/yr)	0.433	0.433	0
TOTAL SUSPENDED SOLIDS (kg/yr)	8.95	1.43	84
TOTAL PHOSPHORUS (kg/yr)	0.0182	0.003	80.900
TOTAL NITROGEN (kg/yr)	0.126	0.059	53
GROSS POLLUTANTS (kg/yr)	1.660	0.000	100

TREATMENT TRAIN EFFECTIVENESS MODELLED USING MUSIC V6.2.

ANALYSIS PARAMETERS:

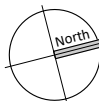
ROOF AREA	0	m ²
DRIVEWAY AREA	100	m ²
GARDEN AREA	0	m ²

COMPLIANCE WITH THE REQUIREMENTS OF PERFORMANCE CRITERIA P2 OF CLAUSE E7.7.1 OF THE «COUNCIL» INTERIM PLANNING SCHEME 2015 CAN BE ACHIEVED BY USING THE INDICATED STORMWATER TREATMENT TRAIN.

DESIGN NOTES:

- 6 MONPELIER RETREAT SITE AREA = 333m²
- 8 MONPELIER RETREAT SITE AREA = 382m²
- BOTH SITES FULLY IMPERVIOUS
- 6 MONPELIER STORMWATER FLOW RATE (5% AEP 5min DURATION) 8.0 L/s
- 8 MONPELIER STORMWATER FLOW RATE (5% AEP 5min DURATION) 9.1 L/s
- EXISTING DN100 PROPERTY CONNECTIONS SUITABLE FOR BOTH SITES.
- ALL STORMWATER WORKS TO BE COMPLETED BY THE DEVELOPER AT DEVELOPERS COST.
- ALL WORKS TO BE COMPLETED PRIOR TO FIRST OCCUPATION.

THIS DRAWING HAS NOT BEEN
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ROBERTS**
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NEW CARPARK
6-8 MONPELIER RETREAT, SALAMANCA
TASMANIA, 7004
DRAWING TITLE
CONCEPT STORMWATER PLAN

0	50mm	SCALE
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PROJECT 21.0317	DRAWING C027	REVISION A



Circa Morris-Nunn Architects

**6-8 Montpelier Retreat
Traffic Impact Assessment**

June 2021



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1. Introduction

1.1 Background

Midson Traffic were engaged by Circa Morris-Nunn Architects to prepare a traffic impact assessment for a proposed car park development at 6-8 Montpelier Retreat, Battery Point.

1.2 Traffic Impact Assessment (TIA)

A traffic impact assessment (TIA) is a process of compiling and analysing information on the impacts that a specific development proposal is likely to have on the operation of roads and transport networks. A TIA should not only include general impacts relating to traffic management, but should also consider specific impacts on all road users, including on-road public transport, pedestrians, cyclists and heavy vehicles.

This TIA has been prepared in accordance with the Department of State Growth (DSG) publication, *Traffic Impact Assessment Guidelines*, August 2020. This TIA has also been prepared with reference to the Austroads publication, *Guide to Traffic Management*, Part 12: *Traffic Impacts of Developments*, 2019.

Land use developments generate traffic movements as people move to, from and within a development. Without a clear understanding of the type of traffic movements (including cars, pedestrians, trucks, etc), the scale of their movements, timing, duration and location, there is a risk that this traffic movement may contribute to safety issues, unforeseen congestion or other problems where the development connects to the road system or elsewhere on the road network. A TIA attempts to forecast these movements and their impact on the surrounding transport network.

A TIA is not a promotional exercise undertaken on behalf of a developer; a TIA must provide an impartial and objective description of the impacts and traffic effects of a proposed development. A full and detailed assessment of how vehicle and person movements to and from a development site might affect existing road and pedestrian networks is required. An objective consideration of the traffic impact of a proposal is vital to enable planning decisions to be based upon the principles of sustainable development.

This TIA also addresses the relevant clauses in the Sullivans Cove Planning Scheme 1997.

1.3 Statement of Qualification and Experience

This TIA has been prepared by an experienced and qualified traffic engineer in accordance with the requirements of Council's Planning Scheme and The Department of State Growth's, *Traffic Impact Assessment Guidelines*, August 2020, as well as Council's requirements.

The TIA was prepared by Keith Midson. Keith's experience and qualifications are briefly outlined as follows:

- 25 years professional experience in traffic engineering and transport planning.
- Master of Transport, Monash University, 2006
- Master of Traffic, Monash University, 2004
- Bachelor of Civil Engineering, University of Tasmania, 1995

- Engineers Australia: Fellow (FIEAust); Chartered Professional Engineer (CPEng); Engineering Executive (EngExec); National Engineers Register (NER)

1.4 Project Scope

The project scope of this TIA is outlined as follows:

- Review of the existing road environment in the vicinity of the site and the traffic conditions on the road network.
- Provision of information on the proposed development with regards to traffic movements and activity.
- Identification of the traffic generation potential of the proposal with respect to the surrounding road network in terms of road network capacity.
- Review of the parking requirements of the proposed development. Assessment of this parking supply with Planning Scheme requirements.
- Traffic implications of the proposal with respect to the external road network in terms of traffic efficiency and road safety.

1.5 Subject Site

The subject site is located at 6-8 Montpelier Retreat, Battery Point. The site is currently a commercial premises (retail).

The subject site and surrounding road network is shown in Figure 1.

Figure 1 Subject Site & Surrounding Road Network

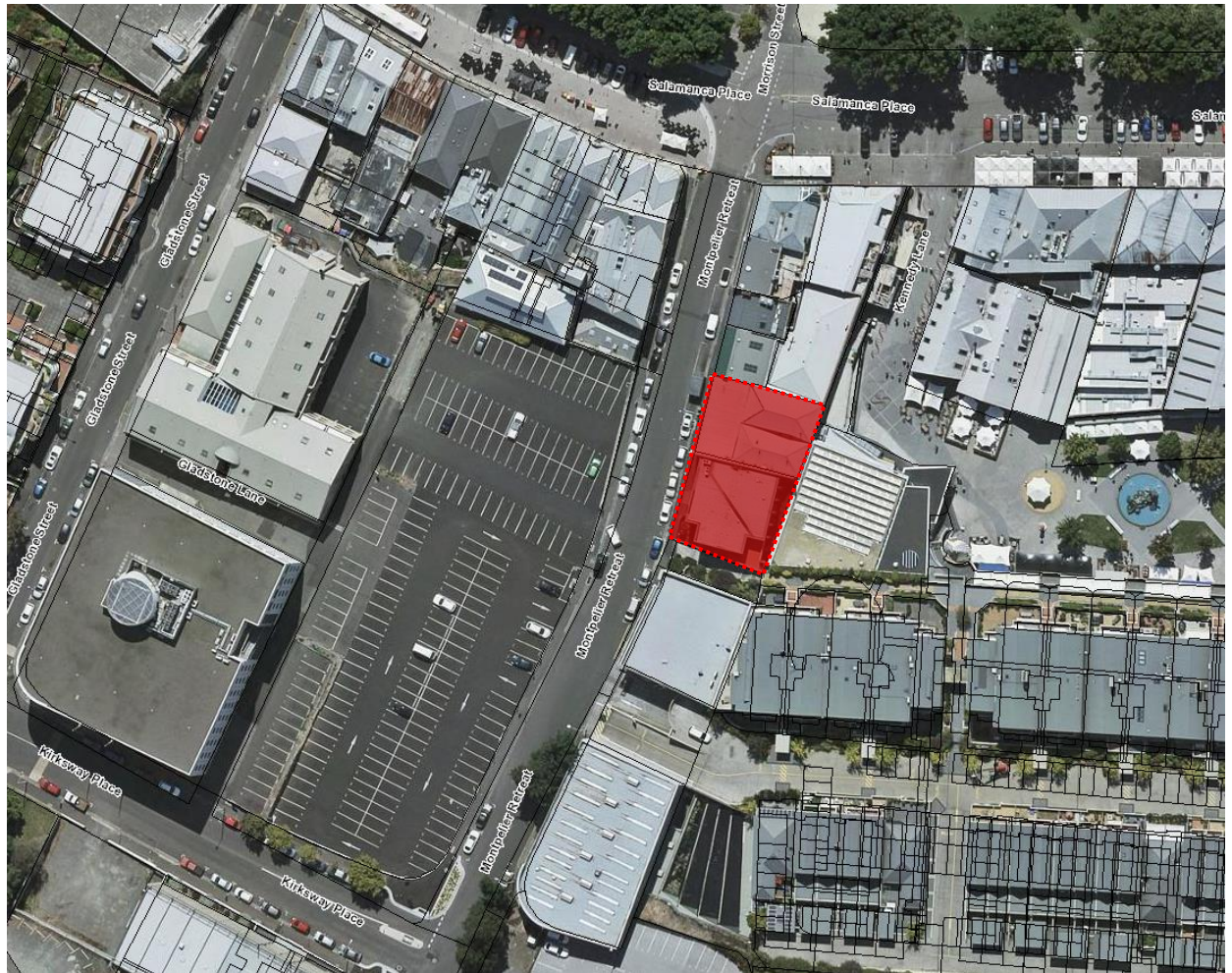


Image Source: LIST Map, DPIPW

1.6 Reference Resources

The following references were used in the preparation of this TIA:

- Sullivans Cove Planning Scheme, 1997 (Planning Scheme)
- Austroads, *Guide to Traffic Management*, Part 12: *Traffic Impacts of Developments*, 2019
- Austroads, *Guide to Road Design*, Part 4A: Unsignalised and Signalised Intersections, 2017
- Department of State Growth, *Traffic Impact Assessment Guidelines*, 2020
- Roads and Maritime Services NSW, *Guide to Traffic Generating Developments*, 2002 (RMS Guide)
- Roads and Maritime Services NSW, *Updated Traffic Surveys*, 2013 (Updated RMS Guide)
- Australian Standards, AS2890.1, *Off-Street Parking*, 2004 (AS2890.1:2004)

2. Existing Conditions

2.1 Transport Network

For the purposes of this report, the transport network consists of Montpelier Retreat, Salamanca Place and Kirksway Place.

2.1.1 Montpelier Retreat

Montpelier Retreat connects between Salamanca Place and Hampden Road. It provides connection to Knopwood Street and Kirksway Place via two intersections that form a 'staggered T' configuration.

Montpelier Retreat is a local street connecting between Castray Esplanade and Hampden Road, running in a predominantly north-south direction. It is a two-lane, two-way road with an average pavement width of approximately 11.5 metres.

Montpelier Retreat provides a large amount of time-limited parking on both sides of the road, with the exception of standing restrictions directly adjacent to the subject site. There are pedestrian footpaths provided on both sides of Montpelier Retreat.

Montpelier Retreat carries in the order of 3,800 vehicles per day with a morning peak of 350 vehicles per hour and an evening peak of 390 vehicles per hour (based on short duration traffic surveys). Traffic volumes reduce dramatically on Montpelier Retreat during the operation of the Salamanca Market due to the associated road closures. The section of Montpelier Retreat between Kirksway Place and Hampden Road carries a significantly lower volume, in the order of 1,000 vehicles per day.

Montpelier Retreat adjacent to the subject site is shown in Figure 2

Figure 2 Montpelier Retreat



2.1.2 Kirksway Place

Kirksway Place is a minor collector road connecting between Montpelier Retreat and Gladstone Street, running in a predominantly east-west direction. It is a two-lane, two-way road with an average pavement width of approximately 14 metres.

Kirksway Place provides time-limited on-street parking and pedestrian footpaths on both sides of the road along its length.

Kirksway Place carries in the order of 3,000 vehicles per day with a morning peak of 240 vehicles per hour and an evening peak of 325 vehicles per hour (based on short duration traffic surveys).

2.2 Road Safety Performance

Crash data can provide valuable information on the road safety performance of a road network. Existing road safety deficiencies can be highlighted through the examination of crash data, which can assist in determining whether traffic generation from the proposed development may exacerbate any identified issues.

Crash data was obtained from the Department of State Growth for a 5+ year period between 1st January 2016 and 31st May 2021 for the full length of Montpelier Retreat.

The findings of the crash data is summarised as follows:

- A total of 13 crashes were reported during this time.
- Severity. No injury crashes were reported – all crashes involved property damage only.
- Day of week. 4 crashes were reported on Mondays; 2 crashes were reported on Wednesdays, Thursdays; Fridays and Saturdays; 1 crash was reported on a Tuesday; no crashes were reported on Sundays.
- Time of day. The majority of crashes were reported between 7:00am and 7:00pm (9 crashes). Two crashes were reported prior to 7:00am and 3 crashes were reported after 7:00pm.
- Crash types. There were a range of crash types reported. 3 crashes involved 'parking-vehicles-only'; 3 crashes involved 'other-maneuvring'; 3 crashes involved 'cross-traffic'.
- Crash locations. The reported crashes were disbursed in Montpelier Retreat. 5 crashes were reported at the Salamanca Place intersection. The crash locations are shown in Figure 3.
- Vulnerable road users. 3 crashes involved a bicycle. No crashes were reported that involved a pedestrian or motorcyclist.

The crash data is reasonably typical of a busy collector road network through a commercial area and does not indicate that there are any specific road safety deficiencies that might be exacerbated by traffic generated by the development proposal.

Figure 3 Montpelier Retreat Crash Locations



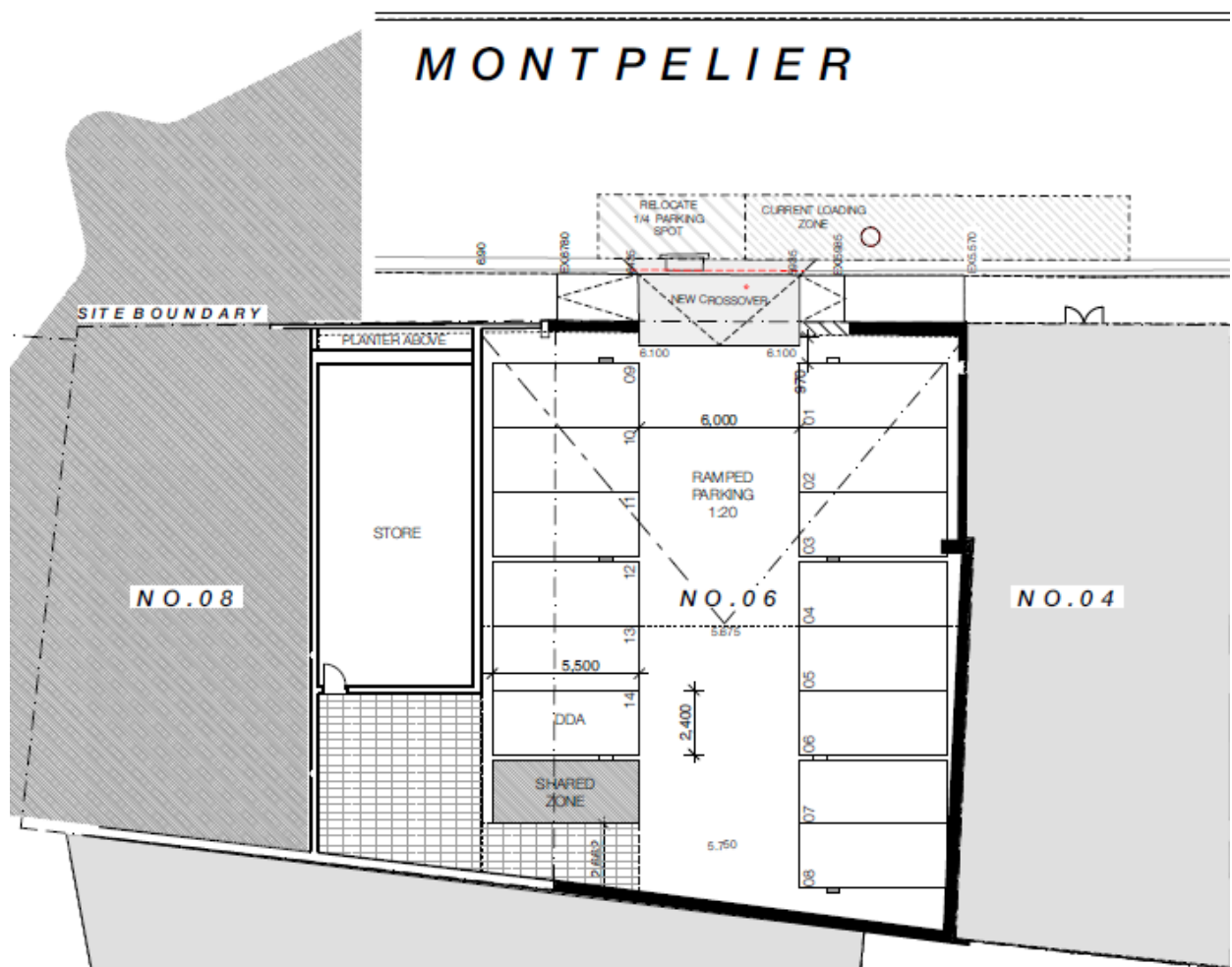
Image Source: Department of State Growth

3. Proposed Development

3.1 Development Proposal

The proposed development involves the conversion of the retail shop to a car park with 14 spaces. The proposed development is shown in Figure 4.

Figure 4 Proposed Development Plans



4. Traffic Impacts

4.1 Trip Generation

The car park will be used by permit holders who are likely to utilise the car parking for commuter use.

Trip generation was estimated using first principles, as follows:

- 80% of the spaces will fill during the AM peak period. 10% of trips will be outward.
- The permit holder spaces have an average turnover of 1.2 times per day (ie. most spaces will be fully occupied all day, however some spaces will turnover when permit holders leave and return during the day).
- 80% of the spaces will exit during the PM peak period. 10% of trips will be inward.

This results in the following traffic generation:

- Daily traffic generation - 34 vehicles per day
- AM peak generation - 12 vehicles per hour
- PM peak generation - 12 vehicles per hour

4.2 Trip Assignment

Based on the connectivity of the surrounding road network, it is likely that there will be a relatively even proportion of traffic from/ to Salamanca Place and Kirksway Place.

4.3 Traffic Capacity Analysis

Montpelier Retreat has an AM peak volume of 350 vehicles per hour and a PM peak of 390 vehicles per hour. The addition of 12 vehicles per hour during peak periods will not have any significant adverse impact on the capacity of Montpelier Retreat or the surrounding road network.

4.4 Access Impacts

Clause 26.4.1 of the Planning Scheme states "*Where a development (including subdivision) is approved that involves the provision of on site car parking, servicing or emergency access facilities that will result in a material increase in the volume of vehicular traffic entering or leaving a public road, then provision shall be made to accommodate such additional vehicles and their movement to the satisfaction of the Planning Authority, having regard to traffic safety or amenity as appropriate. Similar provision shall be made for pedestrians*".

The proposed development will have a traffic generation of approximately 34 vehicles per day with a peak of 12 vehicles per hour. Based on the existing traffic volumes on Montpelier Retreat, the traffic generated by the development will not have any significant adverse impacts on traffic efficiency of the network.

The access design is in accordance with AS2890.1 requirements (as documented in following sections of this TIA). The traffic generation utilising the access therefore complies with the requirements of Clause 26.4.1 of the Planning Scheme.

4.5 Access Requirements

Clause 26.4.2 of the Planning Scheme requires the maximum vehicular access widths and minimum footway widths to be provided in accordance with Table 1.

The development provides 14 car parking spaces an access width of 6.0 metres. Table 1 requires a 1.0 metre pedestrian path and an access width less than 5.0 metres (through the access driveway). The total requirement is 6.0 metres, therefore the access complies with the requirements of Clause 26.4.2 of the Planning Scheme.

Table 1 Planning Scheme Access and Footway Widths

Number of Parking Spaces Provided on Site	Maximum Allowable Vehicular Access width	Minimum Number and Width of Footways
1-5 Inclusive	One 3.0m lane	One 0.6m
6-20 Inclusive	Two 2.5m lanes	One 1.0m
21 and over	Two 3.0m lanes	Two 1.5m

4.6 Sight Distance

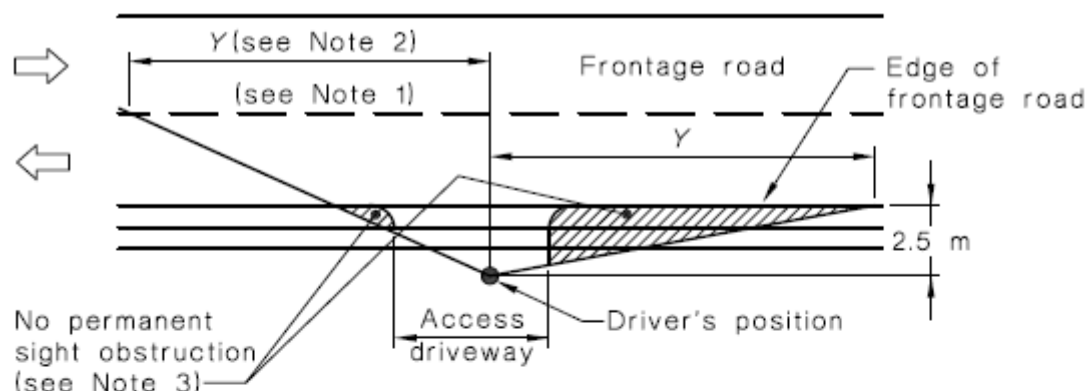
Access to the proposed development is located in Montpelier Retreat. The requirements for sight distance at a car park access are contained in Australian Standard, AS2890.1. These requirements are reproduced in Figure 5. For a frontage road speed of 40-km/h, the requirement is 55 metres (desirable sight distance), with a minimum requirements of 35 metres.

Unrestricted sight distance is available in accordance with AS2890.1 requirements noting that 2.5 metres back from the kerb places the driver's position a short distance within the building structure (the footpath width is approximately 1.8 metres wide adjacent to the site).

On-street car parking can have an impact on the available sight distance from accesses and intersections in an urban environment. Typically, parked cars partly obscure available sight distance however vision is often available between or through parked cars. It would be impractical and economically unviable to remove on-street car parking near every access in an urban environment to meet the sight distance requirements. In a practical sense, whilst on-street car parking should be a consideration, the sight

distance requirements relate more to the vertical and horizontal alignment of the road as well as other physical (and permanent) obstructions in the road environment.

Figure 5 AS2890.1 Sight Distance Requirements



Frontage road speed (Note 4) km/h	Distance (Y) along frontage road m		
	Access driveways other than domestic (Note 5)		Domestic property access (Note 6)
	Desirable 5 s gap	Minimum SSD	
40	55	35	30
50	69	45	40
60	83	65	55
70	97	85	70
80	111	105	95
90	125	130	Use values from 2 nd and 3 rd columns
100	139	160	
110	153	190	

4.7 Pedestrian Sight Distance Requirements

The pedestrian sight distance requirements are provided in AS2890.1. With the 1.0 metre footpath provision located on the southern side of the access (adjacent to the exit lane of the car park), then the footpath sight line provision are almost met. This can be seen in Figure 7.

This is considered acceptable on the basis that vehicles will be exiting the site at very low speed and sight distance will be available from the driver's position within the pedestrian sight triangle requirements on the approach to the footpath.

Figure 6 AS2890.1 Pedestrian Sight Distance Requirements

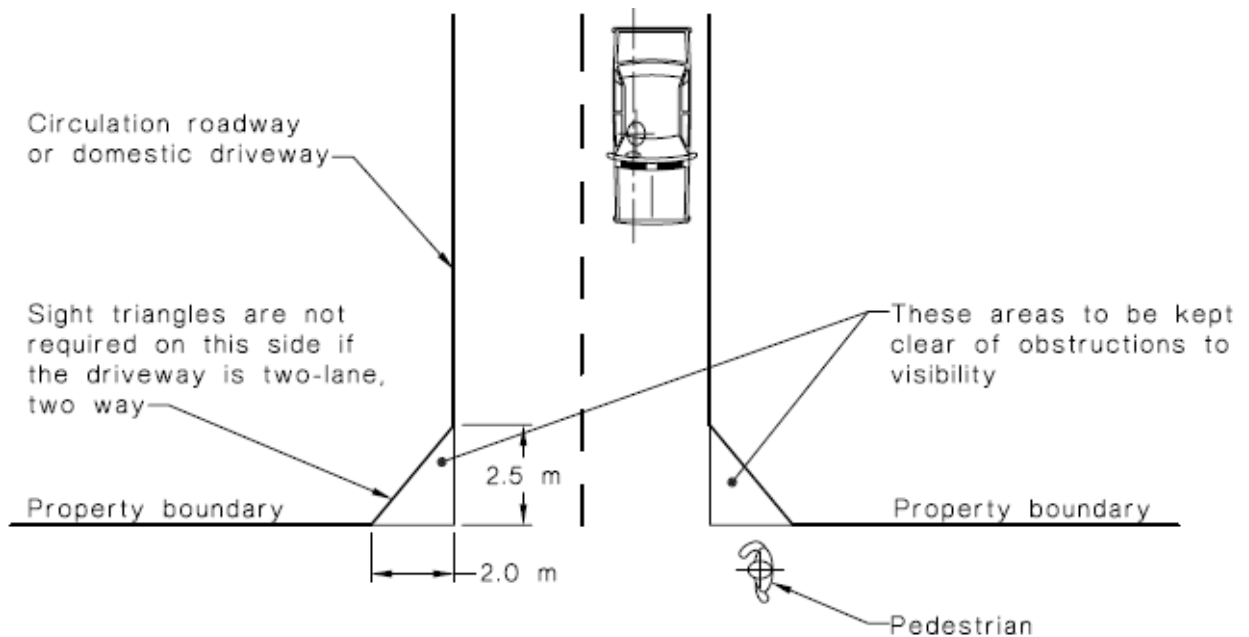
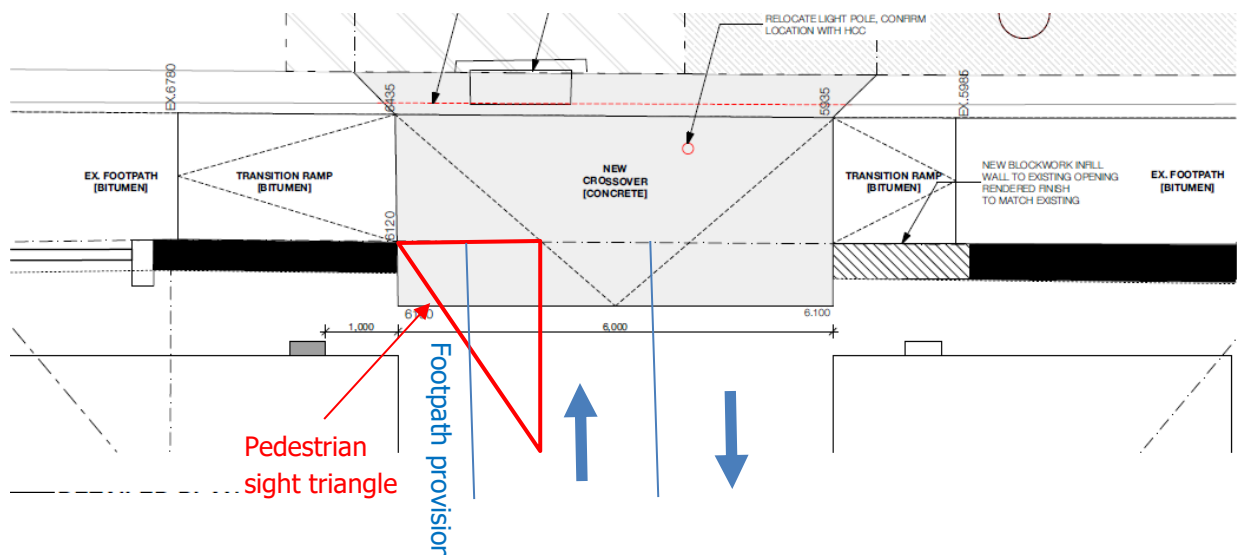


Figure 7 Pedestrian Sight Line Provision at Access



4.8 Road Safety Impacts

No significant road safety impacts are foreseen for the proposed development. This is based on the following:

- The surrounding road transport network is capable of absorbing the estimated traffic generation of the fully developed subdivision (noting a relatively low peak of 12 vehicles per hour).
- The access is considered to be relatively similar to other accesses in Montpelier Retreat. The movement of vehicles into and out of the site will therefore not be seen as 'unusual' for motorists utilising Montpelier Retreat.
- The crash history of the surrounding road network near the subject site does not indicate that there are any specific road safety issues that are likely to be exacerbated by the proposed development. Specifically there is not an existing crash rate involving pedestrians or property access.

5. Parking Assessment

5.1 Parking Provision

The existing site does not have any provision for on-site car parking.

The proposed development provides a total of 14 on-site car parking spaces. These are arranged in two rows (with 6 spaces and 8 spaces in each row). A turning area is provided at the end of the aisle on the southern row of spaces.

5.2 Planning Scheme Requirements

Section 26.4.4 of the Planning Scheme states:

"The Planning Authority may require the parking spaces approved as part of any development to be of a particular size, type, proportion and location to be suited to the likely nature of demand including turnover of use, type of use and user or visitor vehicles to be accommodated, and servicing needs. This will be determined by the Planning Authority after taking into consideration matters such as:

- *Location of the site;*
- *The nature of the site;*
- *The nature of the surrounding area;*
- *Existing traffic and parking patterns;*
- *The nature of the operation and future growth plans or opportunities;*
- *Existing on street restrictions;*
- *Future on street or off street public parking proposals.*

The Planning Authority may require specific parking spaces to be reserved for exclusive use by visitors to the development or for people with disabilities. Any car parking spaces sought to be provided on site will only be approved if located in a manner appropriate to the character and appearance of development in the vicinity and/or with the Principles of this Schedule and the parking and other Objectives for the relevant Activity Area. The siting of parking spaces between the building line and the street boundary line shall generally not be 'permitted'".

In this case the development is exclusively a car park with no associated land use. The car parking will generally support users of nearby land use (staff, commuter car parking, etc).

5.3 Car Parking Layout

Schedule 26.4.3 of the Planning Scheme states: *"The number of parking spaces which can be accommodated in a car parking area and the aisle widths within it, shall comply with the relevant Australian*

Standard, unless otherwise determined by the Planning Authority having regard to the impact on the site and its environs".

In this case, the car park was assessed against the requirements of AS2890.1. The typical car parking dimensions are as follows:

- Space width - 2.4 metres
- Space length - 5.5 metres
- Aisle width - 6.0 metres

These dimensions comply with AS2890.1 requirements for User Class 1A (residential, domestic and employee parking).

The parking dimensions therefore meet the requirements of Schedule 26.4.3 of the Planning Scheme.

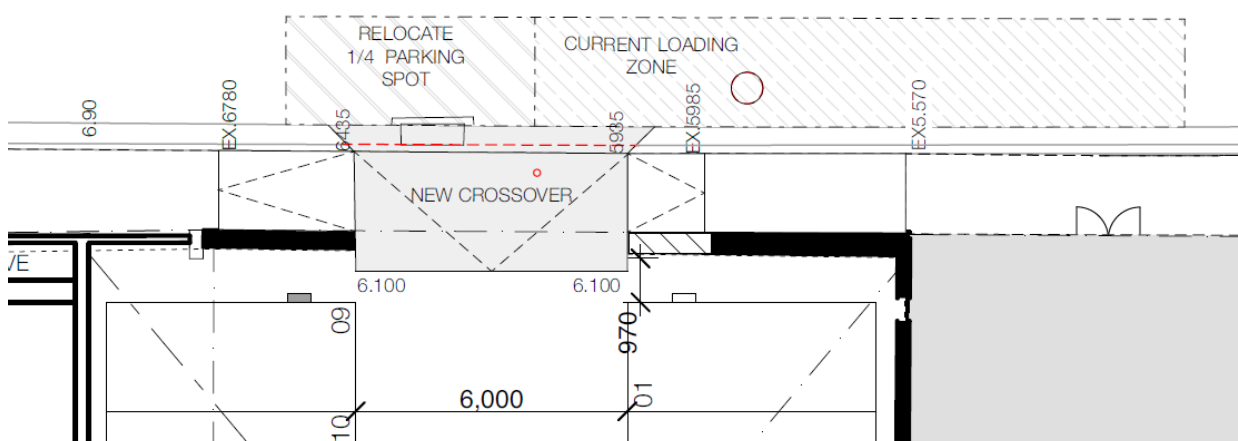
5.4 On Street Car Parking

The construction of a new access to service the site will require the removal or relocation of existing on-street car parking in the form of:

- 1 x 1/4P car parking space
- Loading zone

This is shown in Figure 8.

Figure 8 Required On-Street Car Parking Modifications



The on-street parking modifications can be achieved by reconfiguring spaces to the north of the access, noting that the loading zone requirements are reduced through the removal of the existing retail premises (previous use was a pharmacy with frequent loading requirements).

6. Conclusions

This traffic impact assessment (TIA) investigated the traffic and parking impacts of a proposed car park development at 6-8 Montpelier Retreat, Battery Point.

The key findings of the TIA are summarised as follows:

- The proposed development will remove the existing retail use and convert the site to car parking with a total of 14 spaces.
- The traffic generation is likely to be 34 vehicles per day with a peak of 12 vehicles per hour.
- The development provides a new access on Montpelier Retreat. The access complies with the requirements of Clause 26.4.2 of the Planning Scheme.
- The available sight distance at the development's access on Montpelier Retreat is deemed to be acceptable.
- The layout and dimensions of the car parking meets the requirements of Schedule 26.4.3 of the Planning Scheme.
- The construction of the new access requires the removal or relocation of one on-street car parking space and a loading zone. It is noted that the loading zone requirements for the area will be reduced through the removal of the existing retail premises on the development site.

Based on the findings of this report and subject to the recommendations above, the proposed development is supported on traffic grounds.

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Document Status

Revision	Author	Review	Date
0	Keith Midson	Zara Kacic-Midson	10 June 2021