



CITY OF HOBART

AGENDA

City Planning Committee Meeting

Open Portion

Monday, 9 May 2022

at 5:00 pm

Council Chamber, Town Hall

THE MISSION

Working together to make Hobart a better place for the community.

THE VALUES

The Council is:

People	We care about people – our community, our customers and colleagues.
Teamwork	We collaborate both within the organisation and with external stakeholders drawing on skills and expertise for the benefit of our community.
Focus and Direction	We have clear goals and plans to achieve sustainable social, environmental and economic outcomes for the Hobart community.
Creativity and Innovation	We embrace new approaches and continuously improve to achieve better outcomes for our community.
Accountability	We are transparent, work to high ethical and professional standards and are accountable for delivering outcomes for our community.

ORDER OF BUSINESS

Business listed on the agenda is to be conducted in the order in which it is set out, unless the committee by simple majority determines otherwise.

APOLOGIES AND LEAVE OF ABSENCE

- 1. CO-OPTION OF A COMMITTEE MEMBER IN THE EVENT OF A VACANCY 5**
- 2. CONFIRMATION OF MINUTES..... 5**
- 3. CONSIDERATION OF SUPPLEMENTARY ITEMS 5**
- 4. INDICATIONS OF PECUNIARY AND CONFLICTS OF INTEREST 6**
- 5. TRANSFER OF AGENDA ITEMS..... 6**
- 6. PLANNING AUTHORITY ITEMS - CONSIDERATION OF ITEMS WITH DEPUTATIONS..... 6**
- 7. COMMITTEE ACTING AS PLANNING AUTHORITY 7**
 - 7.1 APPLICATIONS UNDER THE SULLIVANS COVE PLANNING SCHEME 19978**
 - 7.1.1 17 McVilly Drive, 13 McVilly Drive, 20 McVilly Drive, 1 McVilly Drive, 10 Evans Street and Adjacent Crown Land Hobart - Partial Demolition, Extension, Alterations and Associated Works for Partial Change of Use to Light Industry (Whisky Distillery, Eating Establishment Function Centre, Hotel and Shop).....8
 - 7.2 APPLICATIONS UNDER THE HOBART INTERIM PLANNING SCHEME 2015512**
 - 7.2.1 19 Beach Road, Sandy Bay - Alterations512
 - 7.2.2 1 Powell Street, Sandy Bay - Partial Demolition, Alterations and Extension564
 - 7.2.3 3/180-184 Bathurst Street, Hobart - Change of Use to Visitor Accommodation614
- 8. REPORTS 653**
 - 8.1 Regional Strategy - Adapting to a Changing Coastline in Tasmania 653
 - 8.2 Significant Tree Nomination - Public Walkway between Beaumont Road and Ruth Drive, Lenah Valley 703
 - 8.3 City Planning - Advertising Report 737
 - 8.4 Delegated Decision Report (Planning) 745

9. QUESTIONS WITHOUT NOTICE	748
10. CLOSED PORTION OF THE MEETING.....	749

City Planning Committee Meeting (Open Portion) held Monday, 9 May 2022 at 5:00 pm in the Council Chamber, Town Hall.

This meeting of the City Planning Committee is held in accordance with a Notice issued by the Premier on 3 April 2020 under section 18 of the *COVID-19 Disease Emergency (Miscellaneous Provisions) Act 2020*.

The title Chief Executive Officer is a term of reference for the General Manager as appointed by Council pursuant s.61 of the *Local Government Act 1993* (Tas).

COMMITTEE MEMBERS

Deputy Lord Mayor Councillor H Burnet
(Chairman)
Alderman J R Briscoe
Councillor W F Harvey
Alderman S Behrakis
Councillor M Dutta
Councillor W Coats

Apologies:

Leave of Absence:
Councillor W Coats

NON-MEMBERS

Lord Mayor Councillor A M Reynolds
Alderman M Zucco
Alderman Dr P T Sexton
Alderman D C Thomas
Councillor J Fox
Councillor Dr Z Sherlock

1. CO-OPTION OF A COMMITTEE MEMBER IN THE EVENT OF A VACANCY

2. CONFIRMATION OF MINUTES

The minutes of the Open Portion of the City Planning Committee meeting held on [Tuesday, 26 April 2022](#), are submitted for confirming as an accurate record.

3. CONSIDERATION OF SUPPLEMENTARY ITEMS

Ref: Part 2, Regulation 8(6) of the *Local Government (Meeting Procedures) Regulations 2015*.

Recommendation

That the Committee resolve to deal with any supplementary items not appearing on the agenda, as reported by the Chief Executive Officer.

4. INDICATIONS OF PECUNIARY AND CONFLICTS OF INTEREST

Ref: Part 2, Regulation 8(7) of the *Local Government (Meeting Procedures) Regulations 2015*.

Members of the Committee are requested to indicate where they may have any pecuniary or conflict of interest in respect to any matter appearing on the agenda, or any supplementary item to the agenda, which the Committee has resolved to deal with.

5. TRANSFER OF AGENDA ITEMS

Regulation 15 of the *Local Government (Meeting Procedures) Regulations 2015*.

A Committee may close a part of a meeting to the public where a matter to be discussed falls within 15(2) of the above regulations.

In the event that the Committee transfer an item to the closed portion, the reasons for doing so should be stated.

Are there any items which should be transferred from this agenda to the closed portion of the agenda, or from the closed to the open portion of the agenda?

6. PLANNING AUTHORITY ITEMS - CONSIDERATION OF ITEMS WITH DEPUTATIONS

In accordance with the requirements of Part 2 Regulation 8(3) of the *Local Government (Meeting Procedures) Regulations 2015*, the Chief Executive Officer is to arrange the agenda so that the planning authority items are sequential.

In accordance with Part 2 Regulation 8(4) of the *Local Government (Meeting Procedures) Regulations 2015*, the Committee by simple majority may change the order of any of the items listed on the agenda, but in the case of planning items they must still be considered sequentially – in other words they still have to be dealt with as a single group on the agenda.

Where deputations are to be received in respect to planning items, past practice has been to move consideration of these items to the beginning of the meeting.

RECOMMENDATION

That in accordance with Regulation 8(4) of the *Local Government (Meeting Procedures) Regulations 2015*, the Committee resolve to deal with any items which have deputations by members of the public regarding any planning matter listed on the agenda, to be taken out of sequence in order to deal with deputations at the beginning of the meeting.

7. COMMITTEE ACTING AS PLANNING AUTHORITY

In accordance with the provisions of Part 2 Regulation 25 of the *Local Government (Meeting Procedures) Regulations 2015*, the intention of the Committee to act as a planning authority pursuant to the *Land Use Planning and Approvals Act 1993* is to be noted.

In accordance with Regulation 25, the Committee will act as a planning authority in respect to those matters appearing under this heading on the agenda, inclusive of any supplementary items.

The Committee is reminded that in order to comply with Regulation 25(2), the Chief Executive Officer is to ensure that the reasons for a decision by a Council or Council Committee acting as a planning authority are recorded in the minutes.

7.1 APPLICATIONS UNDER THE SULLIVANS COVE PLANNING SCHEME 1997

7.1.1 17 MCVILLY DRIVE, 13 MCVILLY DRIVE, 20 MCVILLY DRIVE, 1 MCVILLY DRIVE, 10 EVANS STREET AND ADJACENT CROWN LAND HOBART - PARTIAL DEMOLITION, EXTENSION, ALTERATIONS AND ASSOCIATED WORKS FOR PARTIAL CHANGE OF USE TO LIGHT INDUSTRY (WHISKY DISTILLERY, EATING ESTABLISHMENT FUNCTION CENTRE, HOTEL AND SHOP PLN-21-734 - FILE REF: F22/39311

Address:	17 McVilly Drive, 13 McVilly Drive, 20 McVilly Drive, 1 McVilly Drive, 10 Evans Street and Adjacent Crown Land HOBART
Proposal:	Partial Demolition, Extension, Alterations and Associated Works for Partial Change of Use to Light Industry (Whisky Distillery), Eating Establishment, Function Centre, Hotel and Shop
Expiry Date:	17 May 2022
Extension of Time:	Not applicable
Author:	Cameron Sherriff

RECOMMENDATION

That pursuant to the *Sullivans Cove Planning Scheme 1997*, the Council approve the application for partial demolition, extension, alterations and associated works for partial change of use to light industry (whisky distillery), eating establishment, function centre, hotel and shop, at 17 McVilly Drive, 13 McVilly Drive, 20 McVilly Drive, 1 McVilly Drive, 10 Evans Street and adjacent crown land, Hobart 7000 for the reasons outlined in the officer's report and a permit containing the following conditions be issued:

GEN

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-21-734 - 17 MCVILLY DRIVE HOBART TAS 7000 - Final Planning Documents except where modified below.

Reason for condition

To clarify the scope of the permit.

TW

The use and/or development must comply with the requirements of TasWater as detailed in the form Submission to Planning Authority Notice, Reference No. TWDA 2022/00206-HCC dated 23/02/2022 as attached to the permit.

Reason for condition

To clarify the scope of the permit.

THC

The use and/or development must comply with the requirements of the Tasmanian Heritage Council as detailed in the Notice of Heritage Decision, THC Works Ref: 6828 dated 20 April 2022, as attached to the permit.

Reason for condition

To clarify the scope of the permit.

ENG sw1

All stormwater from the proposed development (including but not limited to: roofed areas, ag drains, retaining wall ag drains and impervious surfaces such as driveways and paved areas) must be drained to the Council's stormwater infrastructure prior to first occupation or commencement of use (whichever occurs first).

Advice:

Under section 23 of the Urban Drainage Act 2013 it is an offence for a property owner to direct stormwater onto a neighbouring property.

Reason for condition

To ensure that stormwater from the site will be discharged to a suitable Council approved outlet.

ENG tr1

Traffic management within the access driveway, circulation roadway and parking module (parking spaces and aisles) must be installed prior to the commencement of the use.

Traffic management design drawing(s) (including signage and line marking), must be submitted and approved as a Condition Endorsement, prior to commencement of the use. The design drawing(s) must include (but not be limited to):

1. Be prepared by a suitably qualified person;
2. Signage indicating the construction zone;
3. Pedestrian safety bollards where necessary;
4. Delineation of pedestrian pathways along the shared vehicular circulation roadway (including access to both tenancies off of the circulation roadway); and
5. Traffic calming devices within the car park circulating area to ensure that traffic speeds are low for vehicles circulating within the car park.

All work required by this condition must be undertaken in accordance with the approved traffic management design drawings.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

In the interests of user safety and the amenity of the occupiers of the development.

ENG tr2

A construction traffic and parking management plan must be implemented prior to the commencement of work on the site (including demolition).

The construction traffic (including cars, public transport vehicles, service vehicles, pedestrians and cyclists) and parking management plan must be submitted and approved as a Condition Endorsement, prior to commencement work (including demolition). The construction traffic and parking management plan must:

1. Be prepared by a suitably qualified person.
2. Develop a communications plan to advise the wider community of the traffic and parking impacts during construction.
3. Include a start date and finish dates of various stages of works.
4. Include times that trucks and other traffic associated with the works will be allowed to operate.
5. Nominate a superintendent, or the like, to be responsible for the implementation of the approved traffic management plan and available as a direct contact to Council and/or members of the community regarding day to day construction traffic operations at the site, including any immediate traffic issues or hazards that may arise.

All work required by this condition must be undertaken in accordance with the approved construction traffic and parking management plan.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To ensure the safety of vehicles entering and leaving the development and the safety and access around the development site for the general public and adjacent businesses.

ENG 4

The access driveway, access to the building and parking module for the disabled parking spaces approved by this permit must be constructed to a sealed standard (spray seal, asphalt, concrete, pavers or equivalent Council approved) and surface drained to the Council's stormwater infrastructure prior to first occupation / commencement of use.

Reason for condition

To ensure DDA users can safely navigate the car park and access ramps.

ENG 5

A maximum of one (1) bus parking space is allowed on site at any one time. The number of visitor car parking spaces approved to be used on the site is zero (0).

The number of staff car parking spaces approved to be used on the site is zero (0) during hours that are open to the public.

Reason for condition

To ensure the provision of parking for the use is safe and efficient.

ENG 7

The twelve (12) visitor bicycle parking spaces approved must be constructed in accordance with the Australian Standards AS/NZS 2890.1:2004 prior to first occupation or commencement of use.

Reason for condition

To ensure safe and efficient parking adequate to provide for the use.

ENG 9

Three (3) parking spaces for people with disabilities are approved under this application. All car parking spaces for people with disabilities must be delineated to Australian/NZS Standard, Parking facilities Part 6: Off-street parking for people with disabilities AS/NZS 2890.6: 2009, prior to the commencement of the use.

Reason for condition

In the interests of vehicle user safety and the amenity of the development.

ENG 1

Any damage to council infrastructure resulting from the implementation of this permit, must, at the discretion of the Council:

1. Be met by the owner by way of reimbursement (cost of repair and reinstatement to be paid by the owner to the Council); or
2. Be repaired and reinstated by the owner to the satisfaction of the Council.

A photographic record of the Council's infrastructure adjacent to the subject site must be provided to the Council prior to any commencement of works.

A photographic record of the Council's infrastructure (e.g. existing property service connection points, roads, buildings, stormwater, footpaths, driveway crossovers and nature strips, including if any, pre-existing damage) will be relied upon to establish the extent of damage caused to the Council's infrastructure during construction. In the event that the owner/developer fails to provide to the Council a photographic record of the Council's infrastructure, then any damage to the Council's infrastructure found on completion of works will be deemed to be the responsibility of the owner.

Reason for condition

To ensure that any of the Council's infrastructure and/or site-related service connections affected by the proposal will be altered and/or reinstated at the owner's full cost.

ENV 2

An approved Demolition and Construction Environmental Management Plan must be implemented.

Prior to the commencement of works, a Demolition and Construction Environmental Management Plan must be submitted and approved. The Demolition and Construction Environmental Management Plan must:

- Detail the proposed demolition and construction methodology (particularly where works may have environmental impacts);
- Identify all potential environmental impacts associated with the works including (as relevant) noise, odours, air pollution (including dust), water pollution, land contamination, erosion, land instability, changes to hydrology, habitat degradation and impacts upon flora and fauna; and
- include measures to adequately avoid or mitigate all identified environmental risks.

Particular attention must be given to minimising the risk of airborne emissions and water pollution (including sediment transfer).

If works are to be staged (e.g. demolition stage and construction stage), the plan may also be staged and address only those works associated with the stage about to commence.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To minimise the potential for environmental impacts from the demolition and construction works.

ENV s1

Prior to commencement of the use, an assessment of the sewage pumping station must be conducted by a suitably qualified person to determine if the pumping station will meet best-practice environmental management objectives and targets as specified in the *Sewage Pumping Station Environmental Guidelines* (EPA, October 2019).

If the assessment determines that the pumping station will not meet best-practice environmental management objectives and targets, the station must be upgraded to achieve the objectives and targets, based on the advice of a suitably qualified person.

Any required upgrading of the sewage pumping station must be completed prior to commencement of the use.

Reason for condition

To minimise the risk of environmental harm.

ENV s2

The production hall and external yard must be designed and constructed to ensure that leaks, spills, stormwater, process water, washdown water and cleaning effluent cannot directly enter the stormwater system or the River Derwent.

Reason for condition

To minimise the risk of environmental harm.

ENV s3

Litter baskets must be installed on all new stormwater pits, all existing pits being retained, and upstream of the discharge point, prior to commencement of the use.

Reason for condition

To minimise the risk of environmental harm.

HER 7

Archival quality annotated photographs and drawings of the building and features to be demolished must be undertaken prior to commencement of work.

The photographs and drawings must be submitted and approved as a Condition Endorsement, prior to the commencement of work. The photographs and drawings must include:

1. Each elevation of the building;
2. The interior of the building;
3. Architectural design detailing of the building;
4. Electronic colour images;
5. Photographs of any detail that may be of historical or architectural interest; and
6. Cross referencing of all photographs to “as existing” plan showing the location and orientation of the camera.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To ensure recognition of the historic cultural heritage significance of the place.

HER 20

A revised landscaping plan must be submitted that retains the scale and form of the tennis court in accordance with the site guidelines of the 1995 Conservation Management Plan (p.67)

Prior to the approval under the *Building Act 2016*, revised plans must be submitted and approved as a Condition Endorsement in accordance with the above requirement.

All work required by this condition must be undertaken in accordance with the approved plans.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To ensure that development at a heritage place is undertaken in a sympathetic manner which does not cause loss of historic cultural heritage significance.

HER s3

Additional documentation must be submitted regarding the proposed relocation of Building 4 and include all conservation, maintenance, preservation, restoration and reconstruction measure in accordance with the 1995 Conservation Management Plan (p.69) and ensure its cultural heritage values are enhanced.

Prior to the approval under the *Building Act 2016*, revised plans must be submitted and approved as a Condition Endorsement showing the relocation and all works in accordance with the above requirement.

All work required by this condition must be undertaken in accordance with the approved plans.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To ensure that development at a heritage place is undertaken in a sympathetic manner which does not cause loss of historic cultural heritage significance.

HER s4

An interpretation strategy must be prepared by a suitably qualified person to interpret the place's historic cultural heritage significance.

The strategy must be submitted and approved by Council prior to construction and must provide recommendations and details of interpretation in publicly accessible locations. The interpretation strategy and interpretation must include information regarding (but not limited to) the site's history, occupants, relevant photographs or illustrations and relate to all submitted and approved heritage and conservation documents.

The interpretation must be installed prior to the issue of certificate of occupancy.

Reason for condition

To ensure the proposal is in compliance with the conservation strategy of Conservation Management Plan documents.

OPS s1

The developer is to liaise with the City of Hobart in the design of the new pedestrian path and lighting between the rail line bridge and the pedestrian entrance to the site. A Public Spaces By-law permit will be required for construction of the new path. The permit application to undertake works in a public space can be found [here](#).

Reason for condition

To ensure public safety and maintain public amenity.

ADVICE

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's [website](#) for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

CONDITION ENDORSEMENT

If any condition requires that further documents are submitted and approved, you will need to submit the relevant documentation to satisfy the condition via the Condition Endorsement Submission on Council's [online services e-planning portal](#). Detailed instructions can be found [here](#).

A fee of 2% of the value of the works for new public assets (stormwater infrastructure, roads and related assets) will apply for the condition endorsement application.

Once approved, the Council will respond to you via email that the condition has been endorsed (satisfied).

Where building approval is also required, it is recommended that documentation for condition endorsement be submitted well before submitting documentation for building approval. Failure to address condition endorsement requirements prior to submitting for building approval may result in unexpected delays.

BUILDING PERMIT

You may need building approval in accordance with the *Building Act 2016*. Click [here](#) for more information.

This is a Discretionary Planning Permit issued in accordance with section 57 of the *Land Use Planning and Approvals Act 1993*.

PLUMBING PERMIT

You may need plumbing approval in accordance with the *Building Act 2016*, *Building Regulations 2016* and the National Construction Code. Click [here](#) for more information.

OCCUPATION OF THE PUBLIC HIGHWAY

You may require a Permit to Open Up and Temporarily Occupy a Highway (for work in the road reserve). Click [here](#) for more information.

FEES AND CHARGES

Click [here](#) for information on the Council's fees and charges.





DIAL BEFORE YOU DIG

Click [here](#) for dial before you dig information.

HERITAGE

It is recommended that the trees (shown on drawings DA0011, rev 5; DA0052 rev 5) to be removed are retained.

It is recommended that revised plans be prepared to ensure that the rail bridge connectivity via a pathway shown on drawing DA0012 rev 5 takes into account the approved siting of the adjacent new building for Museum (Tram Depot and Display Facility) PLN-20-182 at 20 McVilly Drive.

- | | |
|---------------|---|
| Attachment A: | PLN-21-734 - 17 MCVILLY DRIVE HOBART TAS
7000 - Planning Committee or Delegated Report ↓
 |
| Attachment B: | PLN-21-734 - 17 MCVILLY DRIVE HOBART TAS
7000 - CPC Agenda Documents ↓  |
| Attachment C: | PLN-21-734 - 17 MCVILLY DRIVE HOBART TAS
7000 - Planning Referral Officer Development
Engineering Report ↓  |
| Attachment D: | PLN-21-734 - 17 MCVILLY DRIVE HOBART TAS
7000 - Planning Referral Officer Cultural Heritage
Report ↓  |

**APPLICATION UNDER SULLIVANS COVE PLANNING SCHEME 1997**

Type of Report:	Committee
Council:	16 May 2022
Expiry Date:	17 May 2022
Application No:	PLN-21-734
Address:	17 MCVILLY DRIVE , HOBART 13 MCVILLY DRIVE , HOBART 20 MCVILLY DRIVE , HOBART 1 MCVILLY DRIVE , HOBART 10 EVANS STREET , HOBART ADJACENT CROWN LAND
Applicant:	(Sullivans Cove Distillery Pty Ltd, by their agent, GHD Pty Ltd) 23 Paterson Street
Proposal:	Partial Demolition, Extension, Alterations and Associated Works for Partial Change of Use to Light Industry (Whisky Distillery), Eating Establishment, Function Centre, Hotel and Shop
Representations:	Nil
Performance criteria:	Activity Area Controls (Use); Schedule 1 - Conservation of Cultural Heritage Values; Schedule 3 - Public Urban Space; Schedule 7 - Demolition; and Schedule 8 Environmental Management

1. Executive Summary

- 1.1 Planning approval is sought for Partial Demolition, Extension, Alterations and Associated Works for Partial Change of Use to Light Industry (Whisky Distillery), Eating Establishment, Function Centre, Hotel and Shop, at 17 McVilly Drive, 13 McVilly Drive, 20 McVilly Drive, 1 McVilly Drive, 10 Evans Street and Adjacent Crown Land, HOBART.

1.2 More specifically the proposal includes:

- The demolition of a number of buildings, some relocated, on the site, along with internal works within the former Drill Hall and the construction of a new building, the Production Hall, on its western side, as well as other external works throughout the site to facilitate a new use for the production of whisky and associated tasting and visitor experience.
- Works within the Drill Hall focus on the use of this building as a tasting venue with provision for access, amenities, kitchen and office facilities. Its smaller rooms and those of a former residence on the site are proposed to be used as private tasting rooms, office space, retail space and an associated library.
- The new Production Hall is configured primarily as the distillery element for the production of whisky, but also allowing for tours of the facility. The 11.0m height of this building is influenced by the specialised equipment housed within it. The height of this building is lower than the 12.45m height of the existing Drill Hall.
- The Drill Hall and the Production Hall are connected via a linking corridor.
- The primarily weatherboard Drill Hall is to be repainted in its original white tone, whilst the new Production Hall will be clad with corrugated galvanised sheet metal, with unfinished timber feature walls to its east and west.
- Landscaping is proposed throughout the site.
- Accessible parking is proposed within the Drill Hall forecourt on the site.
- A pedestrian pathway provides connection between the site and a nearby railway pedestrian bridge which leads to public parking and beyond within the land around the Hobart Cenotaph.
- Vehicle access to the site is limited to that required for the operation of the site - loading and unloading and servicing, as well as that required for visitor buses and for those using the accessible spaces on site, and also for those on bicycles with a bike parking area also proposed within the site. Visitor access is achieved via the roads and pathways entering the site from the South-East, while service and operational vehicles will also achieve access over the adjacent Tasports Domain Shipyards site to the North-West.
- The proposal relies on adjacent Council-owned and Crown-owned land for both access and some associated development.

1.3 The proposal relies on performance criteria to satisfy the following standards and codes:

- 1.3.1 Activity Area 2.0 Sullivans Cove 'Mixed Use' - Use
- 1.3.2 Schedule 1 - Conservation of Cultural Heritage Values - Listed Place
- 1.3.3 Schedule 3 - Public Urban Space
- 1.3.4 Schedule 7 - Demolition
- 1.3.5 Schedule 8 - Environmental Management

- 1.4 No representations were received during the statutory advertising period between 28/03 and 11/04/2022.
- 1.5 The proposal is recommended for approval subject to conditions.
- 1.6 The final decision is delegated to the Council, because the proposal relies on and proposes work upon Council-owned land.

2. Site Detail

Image 1: The general 'site' (highlighted green) as it incorporates the several different adjoining titles both for access and development (Source: Council GIS).



Image 2: Actual works are only proposed on the four separate titles highlighted blue. Primarily the works are proposed on 13 and 17 McVilly Drive (the two titles to the north where the majority of buildings are located) - (Source: Council GIS).



Image 3: The subject site (centre) in context with surrounding properties and land uses (Source: Microsoft Maps).

- 2.1 The Subject site (Images 1, 2 and 3) includes land at 'Huon Quays', 17 and 13 McVilly Drive, in the Queens Domain, situated to the north of the Cenotaph with frontage to the River Derwent. The development is primarily located on the Huon Quays titles with a number of adjoining titles to be included for access to the site.
- 2.2 Photos of the site and surrounds follow below:



Plate 1: The site as view from McVilly Drive and the recently upgraded Intercity Cycleway connection. Note the buildings to be demolished and replaced by the 'Production Hall' building are largely obscured by vegetation in this view (Source: Planner's photo).



Plate 2: Looking northward to the rear of the site and the buildings behind the Drill Hall to be demolished and replaced by the new 'Production Hall' building (Source: Planner's photo).



Plate 3: Wider view of the rear of the site as viewed from the recently constructed cycleway link between the Cenotaph carpark and the railway corridor (Source: Planner's photo).



Plate 4: The site as viewed from the nearby railway pedestrian bridge, which will be connected to the site via a new pathway over the vacant Council-owned land at 20 McVilly Drive to the right of the fence running through the image (Source: Planner's photo).



Plate 5: View of the front of the Drill Hall building to be retained, as taken from the secondary access point to the site from lower McVilly Drive to the south-east of the site (Source: Planner's photo).



Plate 6: The existing forecourt of the site to the lower side of the Drill Hall building. This area is proposed to be utilised for accessible parking and bus drop off/pick up (via lower McVilly Drive to the south east of the site) and will include compatible landscaping (Source: Planner's photo).



Plate 7: The shoreline of the site with the Drill Hall building seen to the rear (Source: Planner's photo).



Plate 8: The existing railway pedestrian bridge providing a link to the site from/to the Cenotaph land via a proposed pedestrian pathway over the Council-owned land in the foreground at 20 McVilly Drive (Source: Planner's photo).

3. Proposal

- 3.1 Planning approval is sought for Partial Demolition, Extension, Alterations and Associated Works for Partial Change of Use to Light Industry (Whisky Distillery), Eating Establishment, Function Centre, Hotel and Shop, at 17 McVilly Drive, 13 McVilly Drive, 20 McVilly Drive, 1 McVilly Drive, 10 Evans Street and Adjacent Crown Land, HOBART.

3.2 More specifically the proposal is for:

- The demolition of a number of buildings, some relocated, on the site, along with internal works within the former Drill Hall and the construction of a new building, the Production Hall, on its western side, as well as other external works throughout the site to facilitate a new use for the production of whisky and associated tasting and visitor experience.
- Works within the Drill Hall focus on the use of this building as a tasting venue with provision for access, amenities, kitchen and office facilities. Its smaller rooms and those of a former residence on the site are proposed to be used as private tasting rooms, office space, retail space and an associated library.
- The new Production Hall is configured primarily as the distillery element for the production of whisky, but also allowing for tours of the facility. The 11.0m height of this building is influenced by the specialised equipment housed within it. The height of this building is lower than the 12.45m height of the existing Drill Hall.
- The Drill Hall and the Production Hall are connected via a linking corridor.
- The primarily weatherboard Drill Hall is to be repainted in its original white tone, whilst the new Production Hall will be clad with corrugated galvanised sheet metal, with unfinished timber feature walls to its east and west.
- Landscaping is proposed throughout the site.
- Accessible parking is proposed within the Drill Hall forecourt on the site.
- A pedestrian pathway provides connection between the site and a nearby railway pedestrian bridge which leads to public parking and beyond within the land around the Hobart Cenotaph.
- Vehicle access to the site is limited to that required for the operation of the site - loading and unloading and servicing, as well as that required for visitor buses and for those using the accessible spaces on site, and also for those on bicycles with a bike parking area also proposed within the site. Visitor access is achieved via the roads and pathways entering the site from the South-East, while service and operational vehicles will also achieve access over the adjacent Tasports Domain Shipyards site to the North-West.
- The proposal relies on adjacent Council-owned and Crown-owned land for both access and some associated development.

3.3 Excerpts from the submitted plans follow below:

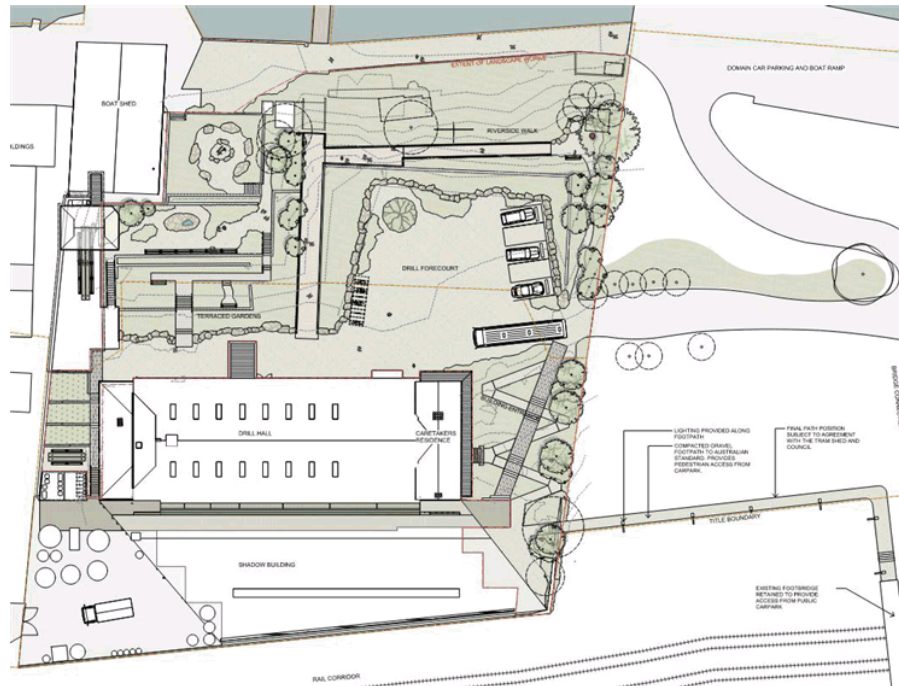


Image 4: The site of the proposed works encompassing 13, 17 and 20 McVilly Drive and the adjacent railway corridor (Source: Application Documents - Realm Studios).



Image 5: North-eastern (front) elevation of the main buildings on the site, including the existing Drill Hall building to the front and the proposed 'Production Hall' building behind (Source: Application Documents - John Wardle Architects).

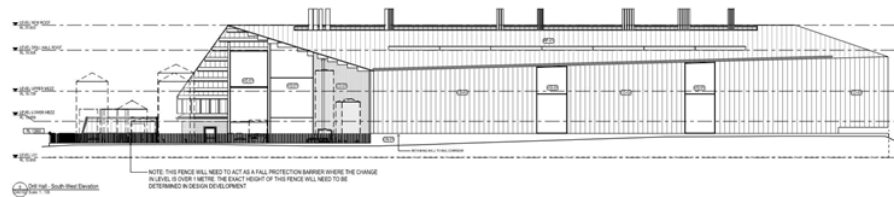


Image 6: South-western (rear) elevation of the proposed 'Production Hall' building (Source: Application Documents - John Wardle Architects).

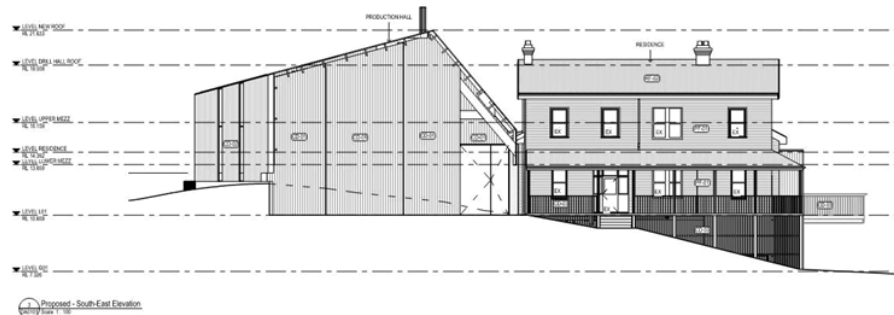


Image 7: South-eastern elevation showing the interaction between the existing Drill Hall building (right) and the proposed 'Production Hall' building (left) (Source: Application Documents - John Wardle Architects).



Image 8: North-western elevation showing the interaction between the existing Drill Hall building and other retained buildings/structures (middle and left) and the proposed 'Production Hall' building (right) (Source: Application Documents - John Wardle Architects).

4. Background

- 4.1 According to the Applicant, buildings on 17 McVilly site are currently vacant, gated and locked with no public access. The HMAS Huon base was originally built in 1913 for the compulsory training of naval cadets and operated until 1994. TasPorts have advised that most recently Building 7, the main Drill Hall, was (informally) a function and meeting area and that there was a caretaker in residence to approximately 2019. Building 7 is the main Drill Hall and Building 8 the original residence, with recent extensions in the eastern corner including a meeting room and occasional operation as a night club. Building 2, to the south was built for accommodation, has been used as a school room, then as a hostel. The building is currently vacant as are buildings 1, 4, 5, 13 and 16. Building 6 has been used as an office.

The site has been largely under utilised since the Drill Hall ceased operation. Limited maintenance of buildings has resulted in them falling into disrepair. On the north east edge of the site is the existing jetty. Buildings include sheds for the sea scouts and navy divers. There is an existing mast on the site in front of the tennis court.

- 4.2 Updated General Manager Consent from Council was granted for the proposal to be considered in February 2022, along with consent from the Crown in March 2022.
- 4.3 In December 2021 Council approved a 'New Building for Museum (Tram Depot and Display Facility)' on the land adjacent to 13 and 17 McVilly Drive at 20 McVilly Drive, which is also part of the overall development site for this application where a pedestrian walkway is proposed to link between the distillery and the nearby railway pedestrian bridge. In the process of considering the granting of General Manager Consent, the alignment of the currently proposed walkway was revised to ensure it did not conflict with the design plans of the tram depot and display facility on the same site.

5. Concerns raised by representors

- 5.1 No representations were received during the statutory advertising period between 28/03 and 11/04/2022.

6. Assessment

- 6.1 The *Sullivans Cove Planning Scheme 1997* is a performance based planning scheme. This approach recognises that there are in many cases a number of ways in which a proposal can satisfy desired environmental, social and economic

standards. In some cases a proposal will be 'permitted' subject to specific 'deemed to comply' provisions being satisfied. Performance criteria are established to provide a means by which the objectives of the planning scheme may be satisfactorily met by a proposal. Where a proposal relies on performance criteria, the Council's ability to approve or refuse the proposal relates only to the performance criteria relied on.

- 6.2 The site of the development/use is located in the Regatta Point Activity Area of the *Sullivans Cove Planning Scheme 1997*. Access to the site is also over land in the Domain Open Space Activity Area (20 McVilly Drive) and the Sullivans Cove - Gateway and Transport Activity Area (10 Evans Street)
- 6.3 There is no existing use occurring on the site as it has been vacant for some time. The proposal incorporates a Light Industry use for its Whisky Distillery element; Eating Establishment, Function Centre, Hotel and Shop uses for its hospitality (tours, sales, function facilities) element, and an ancillary Office use for its administration and amenities elements. The proposed uses are all discretionary uses in the Regatta Point Activity Area.
- 6.4 The proposal has been assessed against:
 - 6.4.1 Parts A and B - Strategic Framework
 - 6.4.2 Part D - Clause 20 - Activity Area Controls
 - 6.4.3 Part E - Schedule 1 - Conservation of Cultural Heritage Values
 - 6.4.4 Part E - Schedule 2 - Urban Form
 - 6.4.5 Part E - Schedule 3 - Public Urban Space
 - 6.4.6 Part E -Schedule 5 - Traffic, Access and Parking
 - 6.4.7 Part E -Schedule 7 - Demolition
 - 6.4.8 Part E -Schedule 8 - Environmental Management
 - 6.4.9 Part F - Key Sites
- 6.5 The proposal relies on the following performance criteria to comply with the applicable standards:
 - 6.5.1 Activity Area 2.0 Sullivans Cove 'Mixed Use' Controls

Use – clause 20.3.3

- 6.5.2 Schedule 1 - Conservation of Cultural Heritage Values

Listed Place - clause 22.4.5; 22.5.5

- 6.5.3 Schedule 3 - Public Urban Space

24.4.6 - Discretionary Building or Works

- 6.5.4 Schedule 7 - Demolition

Demolition - clause 28.6; 28.8.1

- 6.5.5 Schedule 8 - Environmental Management

Level 1 Activity - clause 29.2; 29.5

- 6.6 Each performance criterion is assessed below.

- 6.7 Activity Area Controls (Use) – clause 20.3.3

- 6.7.1 Light Industry; Eating Establishment, Function Centre, Hotel, Shop and Office uses are all discretionary uses in the Regatta Point Activity Area.

- 6.7.2 The proposal includes the introduction of new discretionary uses as part of the adaptive re-use of the site within existing and new buildings.

- 6.7.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.

- 6.7.4 The Objectives and Performance Criteria for the Activity Area at clause 20.2 are as follows:

Primary Objective

To provide for port facilities for ships, shipping and incidental uses taking into account the cultural and visual aspects of the areas as a main road entry to the City of Hobart, the need to accommodate appropriate public access, water-based activities and Regattas.

Performance Criteria

The use and development of the area for ships, shipping and incidental uses may be supported only if the use and development, including the development of structures (wharves, piers, moorings etc) is in keeping with the existing character, scale and context of the existing built and spatial form of the area.

Such use and development must also demonstrate its compatibility with the following:

- *Cultural heritage of the area.*
- *Landscape and aesthetic values of the area.*
- *Visual identity of the area, as viewed from other parts of the Cove Planning Area, and across the Derwent Estuary.*
- *Existing and potential water based activities.*
- *The need to maintain public access to land and water.*
- *Other activities, including recreation and Regatta Day and other festivals.*
- *'Best practice' environmental design and environmental management.*
- *The management objectives of vehicle movement and access.*
- *The provisions of any Outline Development Plan for the Activity Area that may be considered necessary to resolve potential land use and management issues.*

The objectives and recommendations of the Queens Domain Management Plan (1996) must also be taken into consideration in determining the appropriateness of such use.

Supporting Objective (a)

To protect the aesthetic and landscape values of the area, particularly as viewed from the Derwent Estuary.

Performance Criteria

- *All use and development must be consistent with, and where possible, enhance the character of the area, including the character and form of existing buildings and structures, topography and landscape features, particularly as viewed from across the Derwent Estuary and Regatta Grounds.*
- *Development must be compatible with the scale of existing built form (height, bulk and volume).*
- *Development must retain any buildings, structures, spaces or*

landscape features of identified significance.

- *Development must not significantly alter the topography of the area*

Supporting Objective (b)

To conserve and enhance the cultural heritage values of the area, including building, spaces, cultural events (eg. Regatta Day) and archaeological heritage.

Performance Criteria

- *All use and development must be compatible with, conserve, and where possible, enhance the cultural heritage of the area.*
- *Where possible, use and development must incorporate appropriate interpretation of such heritage.*
- *Activities in the area must not restrict or adversely impact upon culturally important events in the area.*

Supporting Objective (c)

To maintain and improve public access to the area, particularly the Regatta grounds and water's edge.

Performance Criteria

- *Activities must not restrict public access to the area, particularly to the water and the Regatta Grounds.*
- *Activities must incorporate features to facilitate public access to, and use of, the area.*
- *Activities which will promote public access to the area are encouraged.*
- *Commercial activities such as restaurants and function centres will generally only be supported where it is clearly demonstrated that such use will allow, and encourage unrestricted public use of the area.*

Supporting Objective (d)

To encourage a range of cultural and community activities, including festivals and Civic functions.

Performance Criteria

- *Cultural and community uses will be encouraged in the area.*

Supporting Objective (e)

To encourage small scale maritime activities and compatible port facilities.

Performance Criteria

- *Enhancement of port facilities and use of the area for maritime activities which do not adversely impact upon public access, amenity, cultural heritage and other identified values are encouraged. Such uses might include slip-yards, boat repair and other small scale maritime industries, and limited mooring of recreational and commercial fishing craft.*

Supporting Objective (f)

To ensure sound environmental planning and management for all activities.

Performance Criteria

- *All use and development to demonstrate the minimisation of on and off site energy requirements resulting from the proposed activity.*
- *All use and development must minimise direct and indirect environmental risk or effects, and where possible, provide a net environmental gain for the wider environment.*

- 6.7.5 The proposed use and development of the site is considered to complement the area and those existing uses around it.

The new building is in keeping with the existing scale of development on and around the site and whilst introducing a new built form to the existing traditional design and layout of the original structures retained in the proposal, the new built form is subservient to the old in terms of scale and does not overpower it.

The proposal integrates well with and does not seek to substantially modify the local landscape and existing topography of the area, and it will not unreasonably modify the appearance of the site and existing development so as to suggest an activity that is inconsistent with the site's previous use or those uses occurring around it.

The proposal does not compromise existing levels of public access to the foreshore and will continue to allow for activities to be held around the regatta grounds while introducing a greater level of activation for the area which in turn may further promote the wider use of the area for events.

The proposed development takes into account the heritage values of the area and will not produce undue impacts upon the local environment. Whilst the proposed development and use are not maritime-based, they will not unduly compromise any such activities which are currently in operation and will continue to allow for any such activity on the waterfront to utilise existing facilities where existing on the site such as jetties and slipways.

The proposed use and development are complementary to the Domain Master Plan and its intentions for the use of the surrounding land. The proposal does not compromise the plan's encouragement of improved access and transit facilities, including the current upgrades to the carpark between the subject site and the Cenotaph, or the potential for the foreshore to achieve a greater degree of public access and amenity.

In an overall sense, the proposal is considered to complement and further the objectives of the Activity Area, taking into account the relevant performance criteria and also the Sullivans Cove Strategic Framework in Part B of the *Sullivans Cove Planning Scheme 1997*. Fundamentally the proposal sensitively adapts what is currently an under-utilised site for reuse that will provide greater opportunities for and promotion of economic, social/cultural, recreational, historical and environmental gains within and around what is an identified Key Site in the Sullivans Cove area.

6.7.6 The proposal complies with the performance criterion.

6.8 Heritage – clause 22.4.5 and 22.5.5 and Demolition - clause 28.6 and 28.8.1

6.8.1 The acceptable solution at clause 22.4.4 requires the proposed 'building or works' to be related to the conservation of a place of cultural significance and which are to be undertaken in accordance with a Conservation Plan accepted by the Planning Authority as satisfying the submission requirements for an application. There is no acceptable solution for demolition occurring on a Place of Cultural Heritage Significance.

6.8.2 The proposal includes works which go beyond the pure conservation of a

place of cultural significance and as such the acceptable solution cannot be met. The proposal includes demolition of various elements within the site, both the partial demolition and also the full demolition of some existing buildings.

6.8.3 The proposal does not comply with the acceptable solution for buildings and works and there is no acceptable solution for demolition; therefore assessment against performance criteria is relied on.

6.8.4 The performance criterion at clause 22.4.5 provides as follows:

'Building or works' on places of cultural significance which cannot satisfy the 'deemed to comply' provisions of Clause 22.4.4 may be approved at the discretion of the Planning Authority.

The following criteria must be taken into consideration in the assessment of all proposals to undertake 'building or works' on places of cultural significance:

- *'Building or works' must complement and contribute to the cultural significance, character and appearance of the place and its setting;*
- *'Building or works' must be in compliance with the conservation strategy of an approved Conservation Plan, where required and/or provided;*
- *The location, bulk and appearance of 'building or works' must not adversely affect the heritage values of any place of cultural significance;*
- *'Building or works' must not reduce the apparent authenticity of places of cultural significance by mimicking historic forms;*
- *'Building or works' may be recognisable as new but must not be individually prominent;*
- *The painting of previously unpainted surfaces is discouraged.*

Further, at clause 22.5.5 of the Sullivans Cove Planning Scheme 1997, the following performance criteria are provided:

The following criteria must be taken into consideration in the assessment of all proposals for 'building or works':

- *'Building or works' adjacent to a place of cultural significance must not dominate that place when viewed from the street or any other public space, or be more prominent in the street than the adjacent*

- place of cultural significance.*
- *The area of a facade of any new building may be permitted to exceed that of the building on an adjacent place of cultural significance where the Planning Authority is satisfied that the visual impact of the apparent disparity of scale is not significant or that historic precedent warrants the scale disparity.*
 - *'Building or works' must complement and contribute to the specific character and appearance of adjacent places of cultural significance and the historic character of the Cove generally.*
 - *The location, bulk and appearance of 'building or works' must not adversely affect the heritage values of any adjacent or nearby place of cultural significance.*
 - *'Building or works' must not reduce the heritage value of any adjacent places of cultural significance by mimicking historic forms.*

The Guidelines for Development Control at clause 28.5 provides as follows:

Any application for demolition:

- (a) Shall be refused if the building is included as a cultural heritage place in Table 1 of the Conservation of Cultural Heritage Values Schedule of this Scheme, unless;*
- (i) The demolition is approved as part of a Conservation Plan approved by the Planning Authority or otherwise in its discretion under the Conservation of Cultural Heritage Values Schedule of this Scheme;*
- (ii) The building clearly detracts from the cultural values or significance of the place; or*
- (iii) There are overriding environmental, economic considerations in terms of the building or practical considerations for its removal, either wholly or in part.*
- (b) May be refused if in the opinion of the Planning Authority the building contributes to the cultural heritage or urban character of the Activity Area and the building is capable of continued beneficial use.*

In addition, Matters to be Considered at clause 28.6 include:

In considering any proposal for demolition, the Planning Authority shall give regard to the following matters:

- *The impact of the proposed demolition on the character of the Activity Area;*
- *The impact of the proposed demolition on the cultural heritage*

values of the Cove;

- *The need to avoid creation of vacant sites and 'lost space' in the Cove.*

With regard to Demolition on a Place of Cultural Heritage Significance; clause 28.8.1 states:

Where the application involves the demolition of a building on a Place included in Table 1 of Schedule 1 – Conservation of Cultural Heritage Values then the application must satisfy the following:

- *The requirement of Clause 22.4.3 for the submission of a Conservation Plan, and*
- *The provision of street elevations or 'true perspectives' to show the scale and impact of the demolition on places of cultural significance and the streetscape.*

- 6.8.5 The Council's Senior Cultural Heritage Officer has assessed these aspects of the application and provides the following comments:

The heritage site:

This application is for demolition and new work to a place listed in Table 1 Places of Cultural Significance (site 122 Former HMAS Huon Naval Depot) of Schedule 1 - Conservation of Cultural Heritage Values in the *Sullivans Cove Planning Scheme 1997*. It has two site addresses of 13 McVilly Drive and 19 McVilly Drive.

The listing applies to the entire site and consists of a number of elements - some of which are of heritage significance and others less significant. Heritage assessments/documentation has been undertaken to assign heritage values and degrees of significance.

History of site:

In 1855 an area including this site was leased for the construction of a patent slip. A patent slip is an inclined place extending from the shore into the water, featuring a cradle into which the ship is floated and secured and then hauled out of the water. It was based on a design by a Scottish shipwright who patented the original design in 1818. For the following 50 years maritime activities were at the centre of this site.

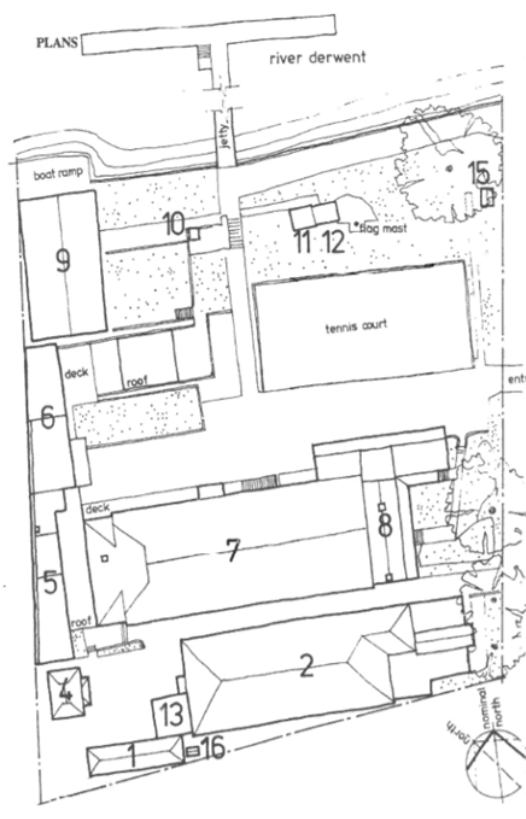
Following the passage of the 1909-1911 *Defence Act*, parts of the

former slipyard lease were made over to the Commonwealth Government for a Naval Depot and a Drill Hall was built for naval cadets in 1912-1913. Compulsory military training followed the disbanding of volunteers after Federation, infrastructure for the force was required throughout the State. The Drill Hall was Tasmania's only naval facility of this type. Numerous additional buildings were building in the 1920s and 1940s, including a new mess and barrack, most of which remain, some of which have been altered. The base was named HMAS Derwent in 1939 and renamed HMAS Huon on 1 March 1942. After the end of WWII activity at the base abated and the HMAS Argent, an Attack class patrol boat was commissioned and stationed at *HMAS Huon* and it was manned by naval reserves. In 1994 when both vessel and base were decommissioned.

Supporting heritage reports:

Following the decommissioning of the site, the Australian Construction Services prepared a *Conservation and Management Plan: HMAS Huon* in 1995.

The following image is from the CMP and the building numbering system from that report is used in the planning report and other documentation associated with this application.



Site and building numbers from the 1995 CMP; Source CMP

In summary, the CMP concludes that the above buildings have the following significance and recommended policy approach.

Building numbers:

- 1 - building - of general significance - if not kept it should be documented
- 2 - building - of general significance - encouraged to be maintained and adapted, but not mandatory.
- 4 - building - of general significance - encouraged to be maintained and adapted, but not mandatory.
- 5 - building - of general significance - southern end - of little significance - recommended for documentation
- 6 - building and deck - of general significance - southern end - of little significance - recommended for documentation
- 7 - Drill hall - crucial to significance - recommended for conservation
- remain the dominant building on the site

- 8 - two storey building - crucial to significance - recommended for conservation - remain the dominant building on the site.
- 9 - building - of general significance, retention is encouraged, but not mandatory, but not enlarged.
- 10 - concrete structure - of little significance - recommended for documentation
- 11/ & 12 - flag mast and flag mast base - of little significance - recommended for documentation
- 13 - carport - of little significance
- 15 - garden shed - of little significance
- 16 - garden shed - of little significance

In addition to the above document, the application is supported by a Heritage Impact Assessment by Praxis Environment, dated October 2021 and includes a review of the heritage issues surrounding the site by Paul Davies Pty Ltd Architects and Heritage Consultants commissioned in 2016 by Tasports.

Relevant assessment provisions:

Internal works are exempt under clause 8.4 of the Scheme.

Under Schedule 1, the works proposal must be assessed against clause 22.4.5 and all criteria (dot points) must be taken into consideration in the assessment of all proposals for 'buildings or works'.

22.4.5 'Discretionary' 'Building or Works'

'Building or works' on places of cultural significance which cannot satisfy the 'deemed to comply' provisions of Clause 22.4.4 may be approved at the discretion of the Planning Authority.

The following criteria must be taken into consideration in the assessment of all proposals to undertake 'building or works' on places of cultural significance:

- *'Building or works' must complement and contribute to the cultural significance, character and appearance of the place and its setting;*
- *'Building or works' must be in compliance with the conservation strategy of an approved Conservation Plan, where required and/or provided;*
- *The location, bulk and appearance of 'building or works' must*

not adversely affect the heritage values of any place of cultural significance;

- *'Building or works' must not reduce the apparent authenticity of places of cultural significance by mimicking historic forms;*
- *'Building or works' may be recognisable as new but must not be individually prominent;*
- *The painting of previously unpainted surfaces is discouraged.*

The demolition must be assessed against 28 Schedule 7 Demolition. Under clause 28.3.1 all demolition is discretionary to ensure the protection and promotion of cultural heritage values. A Conservation Management Plan (1995), review of the 1995 Plan from 2016 and Heritage Impact Assessment are submitted as a mandatory requirement under clause 28.8.1 dot point 1.

28.5 Guidelines for Development Control (Demolition)

The demolition of any building, or works on land shall not be 'permitted' unless; a replacement development has been approved, or such demolition is required by statutory order or is authorised by the Building Surveyor as essential to public safety.

Any application for demolition:

(a) Shall be refused if the building is included as a cultural heritage place in Table 1 of the Conservation of Cultural Heritage Values Schedule of this Scheme, unless;

(i) The demolition is approved as part of a Conservation Plan approved by the Planning Authority or otherwise in its discretion under the Conservation of Cultural Heritage Values Schedule of this Scheme;

(ii) The building clearly detracts from the cultural values or significance of the place; or

(iii) There are overriding environmental, economic considerations in terms of the building or practical considerations for its removal, either wholly or in part.

(b) May be refused if in the opinion of the Planning Authority the building contributes to the cultural heritage or urban character of the Activity Area and the building is capable of continued beneficial use.

Further to this the Scheme states:

28.8 Demolition on a Place of Cultural Significance

28.8.1 Where the application involves the demolition of a building on a Place included in Table 1 of Schedule 1 – Conservation of Cultural Heritage Values then the application must satisfy the following:

- The requirement of Clause 22.4.3 for the submission of a Conservation Plan, and
- The provision of street elevations or 'true perspectives' to show the scale and impact of the demolition on places of cultural significance and the streetscape.

28.8.2 Where the application involves the demolition of any other building then the application must satisfy the following:

- The requirement of Clause 22.5.3 for the submission of a report identifying that the building does not have heritage value, and
- The provision of street elevations or 'true perspectives' to show the scale and impact of the demolition on places of cultural significance and the streetscape.

Heritage values:

The Queens Domain Cultural Heritage Management Plan identified a number of sites for inclusion in the *Sullivans Cove Planning Scheme 1997* including this site. It ascribed the following statements of significance:

The former HMAS Huon Naval Depot is of historic heritage significance for the important role it has played in the development of Tasmania's defence system.

The former HMAS Huon Naval Depot is of historic heritage significance because of its ability to demonstrate the principal characteristics of a Federation period naval base.

Demolition:

The proposal is for the demolition of the following buildings identified in the above site plan: 1, 2, 4 (although it is shown to be relocated), 5, 6, 13 and 16. Partial demolition of buildings 7 and 8 are also proposed, both internally and externally. Trees are also proposed to be removed.

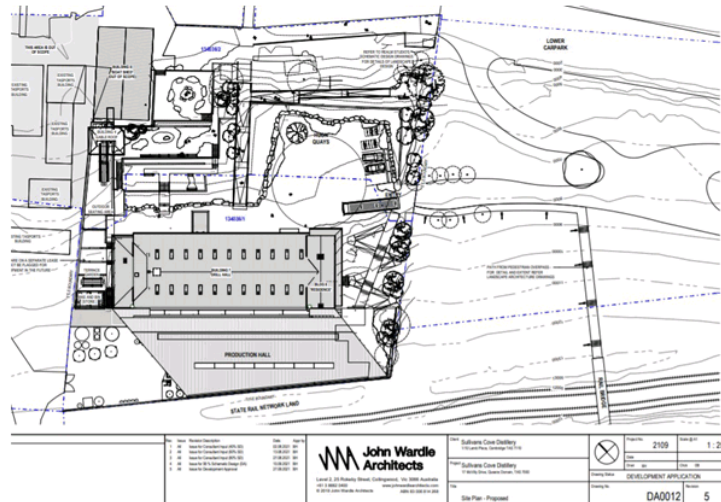
The demolition is assessed within the supporting heritage documents and summarised above. Where demolition is to occur,

the CMP recommends the documentation of the following buildings:
1, 4, 5, 6, 10, 11, 12. This can be achieved by a condition of permit.

The proposal satisfies the guidelines for development control in
Schedule 7 - Demolition (cl 28.5) and cl 28.8 Demolition on a Place
of Cultural Significance.

New work:

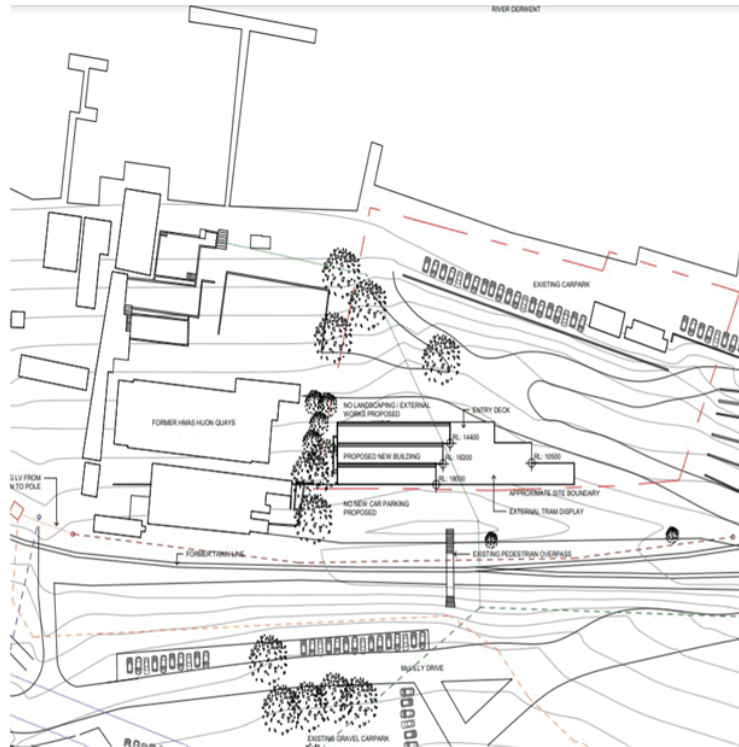
The proposed new production hall connects to the the Drill Hall via a
linking corridor. The site plan is shown below.



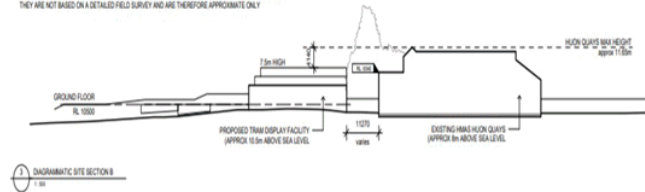
Proposed site plan: Source: Applicant's documentation.

The new building (Production Hall) to the rear of the Drill Hall is a
large building, taller than the Drill Hall. It has a RL 21.633 with an
asymmetrical angled roof. In comparison, the Drill Hall has a RL
19.558.

The following images shows the relationship and an understanding
of comparative heights between the two HMAS Huon site buildings
(Drill Hall and proposed) and the recently approved new building for
the Tram Depot and Display Facility (PLN-20-182) and show a
difference in height of approximately 3.6 metres between the
proposed Production Hall and the Tram Sheds and 1.6 metres
difference between the Drill Hall and the Tram sheds.



*NOTE - ALL HEIGHTS/MEASUREMENTS OF EXISTING BUILDINGS ARE DERIVED FROM SITE OBSERVATIONS. THEY ARE NOT BASED ON A DETAILED FIELD SURVEY AND ARE THEREFORE APPROXIMATE ONLY



The visual studies show the new structure and associated production tanks obscuring the Drill Hall from the cycle path and oblique angles. This is mitigated by topography of the site and in regard, a slightly taller building may be considered acceptable in this situation.



Building 2 in the foreground (to be demolished) and the Drill Hall (with chimney and gable ends) which is currently visible. The proposal will obscure the Drill Hall from this angle. Source: Council image



Building 2 in the foreground (to be demolished) and the Drill Hall (with chimney and gable ends) which is currently visible. The proposal will obscure the Drill Hall from this angle. Source: Council image



Building 2 in the foreground (to be demolished) and the Drill Hall (with chimney and gable ends) which is currently visible. The proposal, including tanks, will obscure the Drill Hall from this angle. Source: Council image.

The design of the Production Hall is a reinterpretation of the long buildings on the site with an asymmetrical gable, while underneath the roof the building footprint is stepped with large expanses of glazing to the ends and hardwood cladding. It does not mimic historic forms.

The contemporary interpretation of the weatherboard Drill Hall could be interpreted as being in compliance with the strategy of the approved Conservation Plan, while the location, bulk and appearance could be reconfigured to be more subservient and lesser in scale, the scheme only calls for the proposal **not** to be 'individually prominent'. Had it been sited elsewhere in the Cove, or been of another form and more extreme height differential, then the proposal might be considered to be individually prominent and of an location, bulk and appearance that does adversely affect the heritage values of the listed place. As such, on balance, the proposal considers the above criteria of 22.4.5

Conclusion:

The following conditions are recommended:

A condition of approval is required to finalise the location of building 4 and for it to be restored/reconstructed to an appropriate and more sympathetic earlier state.

A condition of approval will also require the submission of an interpretation plan to ensure the sites history and heritage values are prepared by the applicant and installed on the site. The interpretation on site must be to the approval of Council.

The landscaping plan be revised to retain the tennis court in scale. The tennis court should not be enlarged or greatly modified.

Documentation of buildings to be demolished must be undertaken.

The following advice clauses are recommended:

It is recommended that the trees on the eastern boundary of the site be retained.

It is recommended that revised plans are required to ensure that the rail bridge connection via a pathway takes into account the approved siting of the adjacent
(PLN-20-182 - 20 McVilly Drive - New Building for Museum (Tram Depot and Display Facility))

6.8.6 The proposal complies with the performance criteria.

6.9 24.4.6 - Discretionary Building or Works

6.9.1 The site is located in the Regatta Point Derwent Shore Public Urban Space Type under Figure 9 of the planning scheme. The table at clause 24.4.2 indicates that 'minor road works' and 'other building and construction works' are discretionary on the Derwent Shore Public Urban Space Type.

6.9.2 The proposed footpath to connect the site to the existing footbridge over the rail line and the Council car park further afield is considered to be 'minor road works' as defined by the planning scheme. The remaining building and works are considered to be 'other building and construction works'.

- 6.9.3 Clause 24.4.6 sets out the requirements for the assessment of discretionary building and works. It requires:

All buildings or works nominated in the table to Clause 24.4.2 as 'D' (Discretionary) require a permit, except where the site is within the area identified by Figure 9a, the building or works are 'permitted'. The Planning Authority may exercise its discretion to approve, approve with conditions, or refuse any application.

In considering such applications, the Planning Authority must satisfy itself that the proposed buildings or works are compatible with the following:

- *The 'Civic Works and Public Street Furniture' Guidelines outlined in Clause 24.4.8.*
- *The function of the Public Urban Space as described in Clause 24.4.10.*
- *The provisions of clause 24.4.9 apply where appropriate.*

- 6.9.4 The minor road works are considered to be civic works, as defined in the planning scheme. As such, clause 24.4.8 relevant provides as follows:

Positioning civic works must allow for convenient pedestrian movement (minimum 2.5 m wide clearance) in designated 'Mixed', 'Pedestrian Movement' or 'Open Space' under Clause 24.4.10.

Sight lines should be provided between pedestrians and drivers and levels of night lighting are to be to the satisfaction of the Planning Authority.

And

Position criteria: Irregular alignment of civic works and public street furniture to the water's edge.

- 6.9.5 The footpath is considered to facilitate convenient pedestrian movement into the site. The siting, design, and sight lines of the footpath have not been objected to by the Council's Engineering officers.

The footpath follows the alignment of the title boundary which is offset, and therefore irregular to, the alignment of the water's edge.

- 6.9.6 The function of the public urban space type for Regatta Point is as follows:

Open Space:

The function of this public urban space type is to facilitate outdoor recreation activities. These spaces are set aside for pedestrian activities, and form an important component of the character and functionality of the Cove. The amenity of these spaces is of utmost importance. Such spaces must be protected from the impacts of overshadowing, traffic noise and other disturbances. Such spaces must also be well connected to other pedestrian oriented spaces in the Cove.

- 6.9.7 The proposal relates primarily to a private parcel of land and as such, will not compromise the recreational activities and opportunities of nearby and adjoining public open space. To the extent that it will likely increase the numbers of people visiting this part of the Cove, it will potentially lead to an increased usage of areas of public open space like the Domain Headland.

The proposal will not have an unreasonable overshadowing impact on adjoining public spaces, as demonstrated in the submitted shadow diagrams.

The proposal is also not providing high levels of on site parking, so impacts arising from traffic noise are not considered likely to be significant, or even significantly greater than what already occurs in this area given the location of the Council car park. Similarly, noise arising from customers attending the site is not considered likely to have any or an unreasonable impact on the surrounding areas of public open space.

The Council's environmental Development Planner has assessed the proposal against the environmental management schedule of the planning scheme, and has not raised any objections to the proposal in terms of environmental impacts arising from the operation of the proposal.

New pedestrian connections to the site are provided, and the proposal is considered likely to enhance pedestrian access to the water's edge.

- 6.9.8 Clause 24.4.9 is not applicable because the proposal does not:
- Include a new public urban space.
 - Include infrastructure works in a street.
 - Relate to Dunn Street car park.
 - Include the closing of a road.
 - Include public street furniture/interpretation.

- 6.9.9 The proposal is considered to satisfy the requirements of clause 24.4.6.

6.10 Environmental Management - Schedule 8

- 6.10.1 There is no acceptable solution for activities considered to be Level 1 activities under the Environment Management & Pollution Control Act 1994.
- 6.10.2 The proposal includes a new Whiskey Distillery which is considered to be a Level 1 Activity.
- 6.10.3 There is no acceptable solution; therefore assessment against the performance criterion, or in the case of Schedule 8 the Objectives and Environmental Objectives of the schedule is relied on.
- 6.10.4 The Objectives at clause 29.2 provide as follows:
- *To ensure that activities are managed in a way which facilitates the ecologically sustainable development of the Cove's natural and physical resources and the maintenance of ecological processes and genetic diversity.*
 - *To ensure that activities are managed in a way which will not cause environmental nuisance or material or serious environmental harm.*

The Environmental Objectives at clause 29.5 address matters such as Air Quality; Energy Efficiency; Flora and Fauna; Hazard and Risk; Land Contamination; Land Reclamation; Noise; Waste Minimisation; and Water Quality. The clause states that where an activity constitutes either a Permissible 'Level 1' activity or a Permissible 'Level 2' activity within the relevant Activity Area, the environmental objectives must be satisfied in determining such an application.

- 6.10.5 The Council's Environmental Development Planner has assessed the proposal against Schedule 8 and provides the following:

Approval is sought for redevelopment of the former navel cadet training facility at 'Huon Quays', 13 and 17 McVilly Drive, Queens Domain, into a whisky distillery.

The application includes:

- Demolition of the buildings on the site except for the former Drill Hall.
- Demolition of landscaping and vegetation.
- Internal alterations and the demolition of additions to the Drill

Hall.

- The construction of a large Production Hall building and external yard for the production of whisky (and tours).
- Use of the Drill Hall for commercial whisky tasting, whisky sales, amenities, kitchen, meeting rooms and offices.
- Extensive landscaping.
- Possible relocation and/or upgrading of an existing private sewage pumping station.

The distillery will produce high-quality, single-cask whiskey. Only distillation of the spirit will occur on-site, with barrel filling, maturation, bottling and packaging occurring elsewhere.

Raw materials required for the production process include grain and water. Water will be treated and stored for use on site. Grain will be delivered by truck and pumped to a storage bin for use. Initial production volumes would call for a grain delivery once a month. Increased production volumes in the future would require more frequent deliveries.

The output from the site will be a 10,000L tanker truck transporting the liquid to the existing Cambridge operation for processing. Initial production volumes will call for this to occur up to once a week.

Distilleries with a capacity to consume 100 kilolitres or more of water in a working day of 8 hours are listed as Level 2 Activities under Schedule 2 of EMPCA. The application states that the distillery will not consume more than 14KL of water per day, therefore the proposed distillery is not a Level 2 activity. The EPA have also confirmed that they do not wish to 'call in' the application.

Objective J Performance Criteria

All use and development to demonstrate the minimisation of on and off-site energy requirements resulting from the proposed activity.

All use and development must minimise direct and indirect environmental risk or effects, and where possible, provide a net environmental gain for the wider environment.

The application addresses the performance criteria as follows:

The proposed works will result in the building and associated

services infrastructure are upgraded to meet contemporary building design standards. The viability of production facilities will be dependent on ensuring energy is managed as efficiently as possible...

The location and quality of the proposed distillery and the associated visitor experience is fundamental to the branding of Sullivans Cove Distillery, and use of sustainable processes that minimise waste and energy consumption are necessary for the viability of production. As such, environmental management is an important part of ensuring delivery of a successful project...

These issues are addressed more thoroughly with regard to Schedule 8 Environmental Management below.

Schedule 8 Environmental Management

Schedule 8 applies to the assessment of all permissible 'Level 1' and 'Level 2' activities. In my opinion, the proposed activity has the potential for environmental harm, due to the potential for nuisance air emissions during demolition, landscaping works and operation, the potential for contaminated stormwater reaching the River Derwent during construction, and the potential for water pollution due to spills at the production facility or failure of the sewage pumping station. It is therefore a Level 1 activity and Schedule 8 applies.

Air Quality

Activities shall demonstrate 'Best Practice Environmental Management' in respect to the minimisation and mitigation of all discharges to the atmosphere.

The application responds to this objective as follows:

The manufacturing process does not produce any emissions of significance. The choice of heating medium, LPG fired boiler, has been designed as to allow the use of a vertical boiler to maximise efficiency and minimise emissions. These types of boilers are common through food manufacture operations.

Grain delivery will arrive via an enclosed truck and will be delivered via pumping thus eliminating any dust emissions. All

components used for grain milling and process transport are enclosed with active dust control equipment being used to capture any airborne particles.

As discussed in the accompanying letter, the risk of whisky fungus has also been raised. Whisky fungus is a mould which can grow in the presence of ethanol and has become an issue in some places in Scotland and Kentucky. There is the potential for this to exist within Tasmania, however it is not an established nor common issue. With respect to the Huon Quays site the production of the alcohol is in effect a closed loop process with produced alcohol going to a steel tank prior to removal from site. Coupled with continuous air exchange within the production building and the location on the shores of a harbour with consistent natural air flow there is minimal to no risk of whisky fungus. All barrel filling, storage and bottling is done at existing sites and limited space at Huon Quays will dictate this remains the case.

One potential environmental issue worth discussing is that of Whiskey Fungus. Major production centres such as Kentucky in the USA have experienced issues with a black mould that develops in conditions with an elevated and consistent amount of ethanol in the environment. This typically occurs around storage and maturation facilities where evaporation from barrels produce the required alcohol in the atmosphere. Huon quays is a closed production loop in a building that requires constant air exchanges (up to 6 air changes per hour within the building), located on a coastal position with prevailing windy conditions. There is no barrel storage at Huon Quays. As such the risk of whiskey fungus is extremely low and does not require further action....

Odours may be present from the Sewer Pump Station (SPS), detailed design will review the existing infrastructure and determine whether upgrades are necessary in accordance with the EPAs Sewage Pumping Station Environmental Guidelines (Oct 2019). Control measures to be considered will include increased storage capacity, standby pumps, power supply, and operation and management plan. Design and ongoing management of the sewer will need to be approved and monitored by TasWater.

Once these mitigation measures are considered, the risk of spills, odour and noise impacts to the environment will be minimised such that the proposed SPS will likely present a significantly lower

risk to the environment than the existing SPS.

The nearest sensitive receptors include dwellings or accommodation to the Subject site are located in Wapping on the other side of the ridge line and exceed 400m in distance...

The production of whiskey relies heavily on the senses. The production team train their sense of smell and taste frequently and the building design of the production facility focusses on neutral areas where taste and smell can be used to guide decisions. Similarly when teaching visitors about the characteristics of whiskey these senses are important. As such the visitor facilities are focussed on this experience and do not include in-house food options nor coffee facilities. The site will also be designated as a no smoking area, inside and outside.

The application has addressed the air quality objective well. Conditions are recommended requiring the implementation of a Council-approved Demolition and Construction Environmental Management Plan to ensure air emissions are minimised during demolition and construction, and for Council-approval of the sewage pumping station to minimise the risk of odour emissions.

Energy Efficiency

Use and development must demonstrate measures undertaken to improve energy efficiency in the design, layout and use of new and existing buildings.

The application responds to this objective as follows:

Energy efficiency is an important component of ensuring the commercial feasibility of the ongoing activities of the Project, and SCD are actively investigating these as part of the design process.

The proposed development and use will include the following measures to improve energy efficiency:

- *Adaptive reuse and retrofit of an existing heritage building.*
- *Site layout options considered in concept stage for the efficiency of process.*
- *The addition of a second roof over the existing roof allows for insulation to be incorporated without compromising the Heritage structure.*

- *The size and type of boiler has been chosen for maximum efficiency.*
- *Use of natural light where possible.*
- *The site is located within an urban area with minimal on-site parking to minimise energy for transport. Bike storage with recharging facilities will be provided for staff.*

Further measures being considered and to be resolved as part of detailed design include the following:

- *Full insulation of all heating pipes.*
- *Efficient design and control of process cooling water equipment to match demand.*
- *Reclamation of waste heat generated in the manufacturing process to provide heating for the Drill Hall building.*
- *Design incorporates use of engineered timber where possible to reduce the energy footprint of construction materials.*
- *The use of variable speed drives on equipment where possible.*
- *Replacement and specification of energy efficiency lighting systems...*

The design process as incorporated as many energy saving options as the heritage site will allow for, including:

- *Process equipment has been designed with modern control systems and energy management components such as variable speed drives.*
- *The distillation process requires heat. The excess heat will be used for hydronic heating (hot water radiators or underfloor heating) of the drill hall to overcome restrictions on insulating the heritage building.*
- *The use of natural light and materials such as engineered timber where possible...*

In order to achieve thermal comfort in the Drill Hall, an additional insulated roof layer of minimal thickness is proposed. This will be aided by hydronic heaters as well as underfloor hydronic heating throughout the Drill Hall and Residence. All external walls will remain unlined and uninsulated to leave visible the existing timber structure and weatherboard cladding. This will help to preserve the simple, utilitarian detailing of the Drill Hall. The new roof will be clad in corrugated, galvanised steel roofing to match the existing profile. New, operable skylights will be installed between the roof's trusses to achieve fresh air requirements and

provide additional natural light. These replace the existing polycarbonate infill sheets which are now well degraded...

Wherever possible, materials will be locally sourced.

The application has addressed the energy efficiency objective well.

Flora And Fauna

Flora and fauna of significance within the Sullivans Cove Planning Area must be protected. In particular, the habitats of the Derwent Estuary and Sullivans Cove water environment must be protected from the adverse environmental impacts of activities.

The application responds to this objective as follows:

Local Landscape Architects have been engaged to design the outside spaces which compliment not only the heritage aspects of the site but respect the pre-colonial conditions. This includes the removal of introduced, non-native plants and replanting with native shrubs, trees and grasses indigenous to the area.

As discussed in Section 2.8 of the Planning Assessment a review of Natural Values for the site was undertaken and there was no threatened flora, fauna or vegetation communities within or adjoining the Subject site that warrants additional controls.

As summarised in the Site services – Concept plan, and discussed in relation to objectives for Water Quality, the proposed stormwater system will have reduced areas of impermeable surfaces, upgrades to improve infrastructure; inclusion of litter baskets; and potentially include rain water harvesting. It is anticipated that the works will improve stormwater runoff above current standards to the benefit of the Derwent Estuary. Any further improvements are capable of being conditioned by Council, as required.

Extensive landscape plantings are proposed for the area between the Drill Hall and the foreshore. Appropriate native species have been selected. The redeveloped site would provide considerably more habitat for native animals than the existing site.

There is a risk to fauna and flora from polluted stormwater reaching

the Derwent, and from wastewater spills to the Derwent from sewage infrastructure (particularly the Sewage Pump Station). However, the application has identified these potential issues and addressed those risks well. Conditions are recommended requiring a Demolition and Construction Management Plan, approval of the use of the sewage pumping station, containment of stormwater from the production and storage facility and the design of stormwater infrastructure.

Hazard And Risk

Land within the Cove must be used and developed in a manner which provides a safe working and living environment. In doing so, best practices must be employed in respect to the handling of dangerous goods and all relevant dangerous goods and environmental laws complied with.

The application responds to this objective as follows:

The manufacturing process is a mature process used around the world and is a relocation of SCD's existing established manufacturing facilities in Cambridge. The industry is highly regulated for Workplace health and safety and requirements are clearly identified. SCD have engaged specialist process consultants to ensure compliance; and building surveyors and fire consultants during the conceptual design stage to make sure all relevant statutory requirements are met. Additionally, they have worked with Lote Consulting, a specialist process risk consultant for early input, and have arranged for a Hazop, safety in design and specialist hazardous material risk review during the detailed design phase. It is also intended to consult with Tasmania Fire Services to review the site during the design phase and as construction progresses in association with the building permit process.

Further to the description of the Proposed Development in Section 3 of the Planning Assessment the process facilities have had to be located in a new building to meet regulatory requirements for safety; as has the separation of activities between the Drill Hall from the Production Hall. Particular efforts have been focussed on containment and the production building and yard areas have been designed to contain and manage all areas dangerous or hazardous goods are active. This includes the need to safely

capture and store any runoff or spillages for both areas to a single point. Cleaning regimes for production and yard areas will likewise be undertaken in accordance with relevant statutory controls.

The application has addressed the hazard and risk objective well.

Land Contamination

New activities which are proposed on land with a known history of industrial or other use where potential for contamination exists shall be accompanied by an environmental audit including an assessment of site condition.

Contaminated land shall be managed in a manner which is compatible with the intended future use of the area. New activities on known contaminated sites must only be 'permitted' to occur after appropriate clean-up of the site, or where it is clearly demonstrated that the proposed activity will not result in an immediate or likely long term hazard to human health or the environment.

The application responds to this objective as follows:

The Huon Quays site has an active history and this has involved the storage of environmentally sensitive materials. In 2007 a fuel storage tank was removed from the site and the associated testing has shown no site contamination present.

The proposed design utilises much of the existing ground infrastructure as is with the main building works being focussed within the footprint of existing buildings.

With TasPorts as the owner there has been inspection and reporting on any Asbestos used within the site. The asbestos items are small in number and minor in nature. All site works and disposal will be carried out within statutory requirements as well as TasPorts safe work procedures.

The proposed use is not for a sensitive use and it is not considered likely that there is likely to be hazards to human health.

Ammunition storage is not listed as a potentially-contaminating

activity in EPA lists. The fuel storage is a potentially-contaminating activity however the application indicates that testing when the tank was removed found no contamination had occurred.

While there is a possibility that airborne contaminants have been transferred to the proposed development site from the nearby slipyard, there is no evidence I am aware of that the proposed development site is contaminated, and the risk is considered low given the nature of slipway operations, the degree of separation between the sites and the fact that the proposed use is not a sensitive one.

Land Reclamation

Land forming and reclamation activities, where required, shall be carried out in a manner which minimises adverse environmental consequences.

Not applicable.

Noise

Buildings shall be sited and designed having regard to current noise levels in the area as well as their intended use.

Where activities with the potential to generate significant noise are proposed in proximity to residential accommodation and other 'noise sensitive' activities, appropriate measures to mitigate and minimise noise emissions must be undertaken.

New 'noise sensitive' activities such as residential accommodation shall be located and where necessary incorporate acoustic measures to minimise the potentially adverse impacts of existing or likely future activities on nearby land.

With regard to noise, the application states:

The closest residential zone to the Subject site is located 500m away in the Glebe, hours of operation of the distillery during the week are to generally be between 8am and 6pm with the potential for the operation of stills process outside of these hours. Activities that may generate noise are discussed further in Table 2. It is not considered necessary for further investigation to be undertaken in

relation to noise as the operation of the site does not generally result in substantial noise...

As described in Section 2.4 of the Planning Assessment the site adjoins an industrial area used by neighbouring TasPorts slipway where the sandblasting of ship hulls generates a baseline of noise for the area. Noise within the area is further contributed to by the substantial amount of traffic on Tasman Highway and nearby interchanges.

The nearest sensitive receptors include dwellings or accommodation to the Subject site are located in Wapping on the other side of the ridge line and exceed 400m in distance.

The manufacturing process used within the distillery is by its nature a very quiet one. There are some supporting activities which generate noise and the following design considerations are in place to accommodate this:

- Grain milling: The milling equipment is relatively small by industrial standards, operates irregularly (for example it may be 3 times a week for several hours at a time), and will be contained within a sealed, insulated for noise, room designed for this purpose. This containment also allows for effective dust control of the process. The room has been positioned at the Slipway end of the production building minimising any noise projection towards public or other neighbouring properties.*
- Air compressor and boiler: Both the air compressor and boiler are relatively small and have been selected to be the smallest possible for the site. These are located in a specifically designed room which will minimise noise projection from this equipment.*
- Traffic: The geography of the area restricts the size of delivery trucks to 18m and under. The process equipment has been designed around these restrictions to maximise the trucks used and to minimise trips. For example initial grain deliveries will only be needed once a month and bulk spirit can be transferred from Huon Quays to SCD warehousing in Cambridge once a week. The truck movements to the site are determined in the TIA to be low and are restricted to daytime operations.*
- No heavy equipment is required on site.*

Further noise may be generated by the Sewer Pump Station,

detailed design will review the existing infrastructure and determine whether upgrades are necessary in accordance with the EPAs Sewage Pumping Station Environmental Guidelines (Oct 2019). As discussed in relation to Air Quality, upgrades are likely to minimise any impacts from the existing SPS.

The project includes a substantial component for visitors to experience the manufacture of whisky through guided tours including moving through the production building along a mezzanine walkway, as well as tastings. Every design effort has been employed to control noise levels to make this experience safe and comfortable for visitors.

The application has addressed the noise objective well.

Waste Minimisation

Activities must demonstrate how the practices and process associated with the activity will reduce as much as possible the amount of waste generated or the amount which requires subsequent treatment, storage or disposal. Activities must address waste minimisation from the source (source reduction) and recycling.

Where appropriate, applications for new activities must include a waste management plan.

Activities within roads and other public spaces must incorporate where relevant suitable waste and litter management facilities.

The application responds to this objective as follows:

The manufacturing process allows for the re-purposing of much of the 'waste' generated. The inputs are water and grain. Once the grain has been processed it retains a good nutritional value and is typically used as a source of animal feed by local farmers. The storage of the used grain has been designed to allow for efficient truck loading for delivery to farms. This is a similar process used nationally and internationally. Similarly the pot ale, the waste liquid following the fermentation process, retains a nutritional value and is typically used by farmers to increase the nutritional value of feed by spraying it on fields.

Any remaining liquid waste, including wash down and cleaning outputs, are trade waste and consultation with TasWater around management of this has commenced as part of the conceptual design phase so as to ensure adequate effluent treatment processes are incorporated.

With respect to the visitor centre operation much of the goods inwards will be first sent to the main warehouse in Cambridge for receipt and dispatch. This process will minimise not only the need for on-site storage but also minimise packaging delivered to site. SCD currently recycles all waste packaging cardboard via repurposing it into padding for our product delivery.

The Huon Quays Project is fundamentally for the production of whisky and the sharing of the process with visitors. As such there are no plans for food preparation on site. In the case of events it is expected any food will be pre-prepared and waste will accordingly be off site.

The design has allowances for a significant increase in recycling bins over and above what SCD currently utilise at the Cambridge site, these are to be located in the proposed screened loading area of the Subject site...

Waste management at the Huon Quays site will include:

- Commercial collection from within the property using between 6 and 8 wheelie bins. The design includes a bin storage positioned to allow for ease of use from the operation, ease of use during collection and also constructed to minimise opportunities for pest interaction including rodents, sea birds and possums. Please note that as mentioned above the food offering during normal operation is minimal and as such the food waste for disposal will also be minimal. General waste collection and recycling will be through a commercial provider with collection taking place fully within the site boundary. With twice the bin capacity currently used at Cambridge and the ability to use more frequent collections the arrangements provide sufficient capacity for now, increased future operations and any potential events.*
- Any cardboard packaging currently received at Cambridge is shredded on site and repurposed as protective fill when packaging bottles of whiskey. Given the main store will remain*

- at Cambridge this reuse of cardboard will continue.*
- *As discussed above the main waste from the grain process will be used for livestock feed or adding nutrients to pastures. The storage and handling equipment has been designed to include this activity...*

The re-use of pot ale and spent grain would significantly reduce the quantity of solid and liquid wastes from the activity requiring disposal to landfill and sewer. For example, the submitted Effluent Feasibility Study for TasWater estimates that volume of effluent generated per mash would total 13,991L if including the pot ale but only 5,398L if pot ale is excluded.

It is accepted that this will occur whenever possible, as it provides an economic benefit for the operator of the distillery through reduced disposal costs. However, there may be times when there are no suitable receivers available, which would be outside the control of the distillery, so a condition requiring this to occur is not recommended.

The application addresses the waste minimisation objective well.

Water Quality

Activities shall demonstrate 'Best Practice Environmental Management' in respect to water use and management. Water use and disposal shall be managed in a manner which seeks to minimise off site disposal and which seeks to protect and, where possible, improve ambient water quality. The principles of minimising water, sewage and waste water generation and the re-use, recycling and pre-treatment of waste water prior to disposal must be encouraged.

The application responds to this objective as follows:

The control of the quality of water is critical to SCD at their existing facilities and as such there is a very high level of knowledge of the make up of any trade waste or effluent generated, as demonstrated in the Effluent Feasibility Study accompanying the application. Early engagement with TasWater has taken place and the design has in place the collection and treatment of effluent prior to leaving site in accordance with the guidelines provided by TasWater.

As discussed in relation to Air Quality and Noise, the existing SPS infrastructure will be reviewed as part of detailed design to ensure that it meets EPA Guidelines and determine any upgrades that may be required. In addition to the physical equipment to control and monitor water quality leaving site SCD are working through the design and automation package which will include the capture and storage of data for trending and historic review.

As previously discussed the pot ale produced during the process has a valuable nutritional element and it is expected this will be utilised by local farmers.

The capture of rain water from the buildings to storage for use maintaining the landscaped areas. Selection of plantings native to the area will also minimise the maintenance requirements of the landscaping. Litter baskets will be included in the upgraded stormwater management system to improve water quality...

Distillation waste, equipment cleaning outflow, and wash down outflow is moved to an effluent treatment plant prior to discharge to sewer system as trade waste. Continuous monitoring will ensure all discharge is within agreed ranges and historic data can be stored for trending analysis and future reference. Storage of treated waste prior to discharge will allow for control of discharge timing and volumes to assist sewer infrastructure if required.

Fire regulations call for the mash tun, wash still and spirit still area of the production facility to be bunded to contain any potentially flammable liquid in the event of equipment failure. This bunding will enable all liquid waste produced during normal operations and also cleaning practices to be contained and directed to the on-site waste treatment equipment. Similarly regulations call for specific spacing and bunding around tanks and equipment located in the production yard. The design allows for the collection of run-off from this area for treatment prior to discharge from site...

Water use volumes per production run for all process and supporting equipment and activities are approximately 14,000L and this is split over multiple shifts, well under the 100,000L per shift limit. Water from the mains is drawn from the water main, treated and stored for use in production meaning that the draw from the mains supply is spread over time rather than an instantaneous requirement...

Rainwater runoff from the building roofs will be collected and used to water the landscaping. Where possible gravity will be used for this activity to minimise energy input.

The application addresses the water quality objective well. The proposed containment of liquids in the production area and external yard will ensure the risk of water pollution via the stormwater system is massively reduced (condition recommended to ensure this occurs). The proposed pre-treatment of effluent prior to disposal to the sewer will reduce treatment loads on the sewage treatment plant, and reduce the impact on the environment if an overflow were to occur at the private sewage pumping station. Upgrading of the sewage pump station to ensure it meets current best-practice standards is strongly supported and a condition is recommended to ensure this occurs.

The development would lead to a reduction in impervious surfaces on the site, and would re-use stormwater for watering, which should reduce the transfer of contaminants from the site to the River Derwent via the stormwater system compared with the existing situation. The installation of litter baskets in the new stormwater pits and prior to the outfall will also reduce the quantity of gross pollutants being discharged to the River (condition recommended to ensure this occurs).

A further condition is recommended requiring the implementation of an approved Demolition and Construction Environmental Management Plan to ensure the risk of water pollution from air or stormwater emissions is minimised.

6.10.6 The proposal complies with the objectives.

7. Discussion

- 7.1 Planning approval is sought for Partial Demolition, Extension, Alterations and Associated Works for Partial Change of Use to Light Industry (Whisky Distillery), Eating Establishment, Function Centre, Hotel and Shop, at 17 McVilly Drive, 13 McVilly Drive, 20 McVilly Drive, 1 McVilly Drive, 10 Evans Street and Adjacent Crown Land, HOBART.
- 7.2 The application was advertised and no representations were received.

- 7.3 The proposal has been assessed against the relevant provisions of the planning scheme and is considered to perform well. Aside from the discretions triggered regarding heritage, public urban space, demolition, environmental management and use, which have been assessed as supportable, the proposal is otherwise compliant with the permitted standards of the *Sullivans Cove Planning Scheme 1997*.

In its built form the proposal integrates well with the existing site and surrounding landscape. As an adaptive reuse of what is currently an underutilised site, the proposal is considered to have merit in that it will prolong the life of the site and much of the existing heritage values upon it without significantly changing what is a noted Key Site.

Whilst proposing discretionary uses in the applicable activity area, the overall proposal is considered to satisfactorily complement those existing uses around it as well as the strategic framework and objectives of the activity area and scheme overall.

- 7.4 The proposal has been assessed by other Council officers, including the Council's Development Engineer, Stormwater Engineer, Cultural Heritage Officer, Environmental Development Planner and Open Space Planner. The officers have raised no objection to the proposal, subject to conditions.
- 7.5 The proposal is recommended for approval.

8. Conclusion

- 8.1 The proposed Partial Demolition, Extension, Alterations and Associated Works for Partial Change of Use to Light Industry (Whisky Distillery), Eating Establishment, Function Centre, Hotel and Shop, at 17 McVilly Drive, 13 McVilly Drive, 20 McVilly Drive, 1 McVilly Drive, 10 Evans Street and Adjacent Crown Land, HOBART satisfies the relevant provisions of the *Sullivans Cove Planning Scheme 1997*, and as such is recommended for approval.

9. Recommendations

That: Pursuant to the *Sullivans Cove Planning Scheme 1997*, the City Planning Committee, in accordance with the delegations contained in its terms of reference, approve the application for Partial Demolition, Extension, Alterations and Associated Works for Partial Change of Use to Light Industry (Whisky Distillery), Eating Establishment, Function Centre, Hotel and Shop, at 17 McVilly Drive, 13 McVilly Drive, 20 McVilly Drive, 1 McVilly Drive, 10 Evans Street and Adjacent Crown Land, HOBART for the reasons outlined in the officer's report and a permit containing the following conditions be issued:

GEN

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-21-734 - 17 MCVILLY DRIVE HOBART TAS 7000 - Final Planning Documents except where modified below.

Reason for condition

To clarify the scope of the permit.

TW

The use and/or development must comply with the requirements of TasWater as detailed in the form Submission to Planning Authority Notice, Reference No. TWDA 2022/00206-HCC dated 23/02/2022 as attached to the permit.

Reason for condition

To clarify the scope of the permit.

THC

The use and/or development must comply with the requirements of the Tasmanian Heritage Council as detailed in the Notice of Heritage Decision, THC Works Ref: 6828 dated 20 April 2022, as attached to the permit.

Reason for condition

To clarify the scope of the permit.

ENG sw1

All stormwater from the proposed development (including but not limited to: roofed areas, ag drains, retaining wall ag drains and impervious surfaces such as driveways and paved areas) must be drained to the Council's stormwater infrastructure prior to first occupation or commencement of use (whichever occurs first).

Advice: Under section 23 of the Urban Drainage Act 2013 it is an offence for a property owner to direct stormwater onto a neighbouring property.

Reason for condition

To ensure that stormwater from the site will be discharged to a suitable Council approved outlet.

ENG tr1

Traffic management within the access driveway, circulation roadway and parking module (parking spaces and aisles) must be installed prior to the commencement of the use.

Traffic management design drawing(s) (including signage and line marking), must be submitted and approved as a Condition Endorsement, prior to commencement of the use. The design drawing(s) must include (but not be limited to):

- 1. Be prepared by a suitably qualified person.**
- 2. Signage indicating the construction zone.**
- 3. Pedestrian safety bollards where necessary.**
- 4. Delineation of pedestrian pathways along the shared vehicular circulation roadway (including access to both tenancies off of the circulation roadway).**
- 5. Traffic calming devices within the car park circulating area to ensure that traffic speeds are low for vehicles circulating within the car park;**

All work required by this condition must be undertaken in accordance with the approved traffic management design drawings.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

In the interests of user safety and the amenity of the occupiers of the development.

ENG tr2

A construction traffic and parking management plan must be implemented prior to the commencement of work on the site (including demolition).

The construction traffic (including cars, public transport vehicles, service vehicles, pedestrians and cyclists) and parking management plan must be submitted and approved as a Condition Endorsement, prior to commencement work (including demolition). The construction traffic and parking management plan must:

- 1. Be prepared by a suitably qualified person.**
- 2. Develop a communications plan to advise the wider community of the traffic and parking impacts during construction.**
- 3. Include a start date and finish dates of various stages of works.**
- 4. Include times that trucks and other traffic associated with the works will be allowed to operate.**
- 5. Nominate a superintendent, or the like, to be responsible for the implementation of the approved traffic management plan and available as a direct contact to Council and/or members of the community regarding day to day construction traffic operations at the site, including any immediate traffic issues or hazards that may arise.**

All work required by this condition must be undertaken in accordance with the approved construction traffic and parking management plan.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To ensure the safety of vehicles entering and leaving the development and the safety and access around the development site for the general public and adjacent businesses.

ENG 4

The access driveway, access to the building and parking module for the

disabled parking spaces approved by this permit must be constructed to a sealed standard (spray seal, asphalt, concrete, pavers or equivalent Council approved) and surface drained to the Council's stormwater infrastructure prior to first occupation / commencement of use.

Reason for condition

To ensure DDA users can safely navigate the car park and access ramps.

ENG 5

A maximum of one (1) bus parking space is allowed on site at any one time. The number of visitor car parking spaces approved to be used on the site is zero (0).

The number of staff car parking spaces approved to be used on the site is zero (0) during hours that are open to the public.

Reason for condition

To ensure the provision of parking for the use is safe and efficient.

ENG 7

The twelve (12) visitor bicycle parking spaces approved must be constructed in accordance with the Australian Standards AS/NZS 2890.1:2004 prior to first occupation or commencement of use.

Reason for condition

To ensure safe and efficient parking adequate to provided for the use.

ENG 9

Three (3) parking spaces for people with disabilities are approved under this application. All car parking spaces for people with disabilities must be delineated to Australian/NZS Standard, Parking facilities Part 6: Off-street parking for people with disabilities AS/NZS 2890.6: 2009, prior to the commencement of the use.

Reason for condition

In the interests of vehicle user safety and the amenity of the development.

ENG 1

Any damage to council infrastructure resulting from the implementation of this permit, must, at the discretion of the Council:

1. **Be met by the owner by way of reimbursement (cost of repair and reinstatement to be paid by the owner to the Council); or**
2. **Be repaired and reinstated by the owner to the satisfaction of the Council.**

A photographic record of the Council's infrastructure adjacent to the subject site must be provided to the Council prior to any commencement of works.

A photographic record of the Council's infrastructure (e.g. existing property service connection points, roads, buildings, stormwater, footpaths, driveway crossovers and nature strips, including if any, pre-existing damage) will be relied upon to establish the extent of damage caused to the Council's infrastructure during construction. In the event that the owner/developer fails to provide to the Council a photographic record of the Council's infrastructure, then any damage to the Council's infrastructure found on completion of works will be deemed to be the responsibility of the owner.

Reason for condition

To ensure that any of the Council's infrastructure and/or site-related service connections affected by the proposal will be altered and/or reinstated at the owner's full cost.

ENV 2

An approved Demolition and Construction Environmental Management Plan must be implemented.

Prior to the commencement of works, a Demolition and Construction Environmental Management Plan must be submitted and approved. The Demolition and Construction Environmental Management Plan must:

- **detail the proposed demolition and construction methodology (particularly where works may have environmental impacts);**
- **identify all potential environmental impacts associated with the works including (as relevant) noise, odours, air pollution (including dust), water pollution, land contamination, erosion, land instability, changes to hydrology, habitat degradation and impacts upon flora and fauna; and**

- include measures to adequately avoid or mitigate all identified environmental risks.

Particular attention must be given to minimising the risk of airborne emissions and water pollution (including sediment transfer).

If works are to be staged (e.g. demolition stage and construction stage), the plan may also be staged and address only those works associated with the stage about to commence.

Advice: This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To minimise the potential for environmental impacts from the demolition and construction works.

ENV s1

Prior to commencement of the use, an assessment of the sewage pumping station must be conducted by a suitably qualified person to determine if the pumping station will meet best-practice environmental management objectives and targets as specified in the *Sewage Pumping Station Environmental Guidelines* (EPA, October 2019).

If the assessment determines that the pumping station will not meet best-practice environmental management objectives and targets, the station must be upgraded to achieve the objectives and targets, based on the advice of a suitably qualified person.

Any required upgrading of the sewage pumping station must be completed prior to commencement of the use.

Reason for condition

To minimise the risk of environmental harm.

ENV s2

The production hall and external yard must be designed and constructed to ensure that leaks, spills, stormwater, process water, washdown water and cleaning effluent cannot directly enter the stormwater system or the River

Derwent.

Reason for condition

To minimise the risk of environmental harm.

ENV s3

Litter baskets must be installed on all new stormwater pits, all existing pits being retained, and upstream of the discharge point, prior to commencement of the use.

Reason for condition

To minimise the risk of environmental harm.

HER 7

Archival quality annotated photographs and drawings of the building and features to be demolished must be undertaken prior to commencement of work.

The photographs and drawings must be submitted and approved as a Condition Endorsement, prior to the commencement of work. The photographs and drawings must include:

- 1. Each elevation of the building;**
- 2. The interior of the building;**
- 3. Architectural design detailing of the building;**
- 4. Electronic colour images;**
- 5. Photographs of any detail that may be of historical or architectural interest; and**
- 6. Cross referencing of all photographs to “as existing” plan showing the location and orientation of the camera.**

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To ensure recognition of the historic cultural heritage significance of the place.

HER 20

A revised landscaping plan must be submitted that retains the scale and form of the tennis court in accordance with the site guidelines of the 1995 Conservation Management Plan (p.67)

Prior to the approval under the *Building Act 2016*, revised plans must be submitted and approved as a Condition Endorsement in accordance with the above requirement.

All work required by this condition must be undertaken in accordance with the approved plans.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To ensure that development at a heritage place is undertaken in a sympathetic manner which does not cause loss of historic cultural heritage significance.

HER s3

Additional documentation must be submitted regarding the proposed relocation of Building 4 and include all conservation, maintenance, preservation, restoration and reconstruction measure in accordance with the 1995 Conservation Management Plan (p.69) and ensure its cultural heritage values are enhanced.

Prior to the approval under the *Building Act 2016*, revised plans must be submitted and approved as a Condition Endorsement showing the relocation and all works in accordance with the above requirement.

All work required by this condition must be undertaken in accordance with the approved plans.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To ensure that development at a heritage place is undertaken in a sympathetic manner which does not cause loss of historic cultural heritage significance.

HER s4

An interpretation strategy must be prepared by a suitably qualified person to interpret the place's historic cultural heritage significance.

The strategy must be submitted and approved by Council prior to construction and must provide recommendations and details of interpretation in publicly accessible locations. The interpretation strategy and interpretation must include information regarding (but not limited to) the site's history, occupants, relevant photographs or illustrations and relate to all submitted and approved heritage and conservation documents.

The interpretation must be installed prior to the issue of certificate of occupancy.

Reason for condition

To ensure the proposal is in compliance with the conservation strategy of Conservation Management Plan documents.

OPS s1

The developer is to liaise with the City of Hobart in the design of the new pedestrian path and lighting between the rail line bridge and the pedestrian entrance to the site. A Public Spaces By-law permit will be required for construction of the new path. The permit application to undertake works in a public space can be found [here](#).

Reason for condition

To ensure public safety and maintain public amenity.

ADVICE

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to

obtain an approval. Visit the Council's [website](#) for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

CONDITION ENDORSEMENT

If any condition requires that further documents are submitted and approved, you will need to submit the relevant documentation to satisfy the condition via the Condition Endorsement Submission on Council's [online services e-planning portal](#). Detailed instructions can be found [here](#).

A fee of 2% of the value of the works for new public assets (stormwater infrastructure, roads and related assets) will apply for the condition endorsement application.

Once approved, the Council will respond to you via email that the condition has been endorsed (satisfied).

Where building approval is also required, it is recommended that documentation for condition endorsement be submitted well before submitting documentation for building approval. Failure to address condition endorsement requirements prior to submitting for building approval may result in unexpected delays.

BUILDING PERMIT

You may need building approval in accordance with the *Building Act 2016*. Click [here](#) for more information.

This is a Discretionary Planning Permit issued in accordance with section 57 of the *Land Use Planning and Approvals Act 1993*.

PLUMBING PERMIT

You may need plumbing approval in accordance with the *Building Act 2016*, *Building Regulations 2016* and the National Construction Code. Click [here](#) for more information.

OCCUPATION OF THE PUBLIC HIGHWAY

You may require a Permit to Open Up and Temporarily Occupy a Highway (for work in the road reserve). Click [here](#) for more information.

FEES AND CHARGES

Click [here](#) for information on the Council's fees and charges.

DIAL BEFORE YOU DIG

Click [here](#) for dial before you dig information.

HERITAGE

It is recommended that the trees (shown on drawings DA0011, rev 5; DA0052 rev 5) to be removed are retained.

It is recommended that revised plans be prepared to ensure that the rail bridge connectivity via a pathway shown on drawing DA0012 rev 5 takes into account the approved siting of the adjacent new building for Museum (Tram Depot and Display Facility) PLN-20-182 at 20 McVilly Drive.



(Cameron Sherriff)

Development Appraisal Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.



(Ben Ikin)

Senior Statutory Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

Date of Report: 28 April 2022

Attachment(s):

Attachment B - CPC Agenda Documents

Attachment C - Planning Referral Officer Development Engineering Report

Attachment D - Planning Referral Officer Cultural Heritage Report



Tasmanian Heritage Council
GPO Box 618 Hobart Tasmania 7000
Tel: 1300 850 332
enquiries@heritage.tas.gov.au
www.heritage.tas.gov.au

PLANNING REF: PLN-21-734
THC WORKS REF: 6828
REGISTERED PLACE NO: 2932
FILE NO: 07-23-67THC
APPLICANT: Sullivans Cove Distillery Pty Ltd (by their agent, GHD Pty Ltd)
DATE: 20 April 2022

NOTICE OF HERITAGE DECISION

(Historic Cultural Heritage Act 1995)

The Place: Former HMAS Huon Naval Depot (Huon Quays),
13-17 McVilly Drive, Hobart.

Proposed Works: Partial demolition, extension, alterations & associated works.
Application includes partial change of use to light industry
(whiskey distillery), eating establishment, function centre, hotel
and shop.

Under section 39(6)(b) of the *Historic Cultural Heritage Act 1995*, the Heritage Council gives notice that it consents to the discretionary permit being granted in accordance with the documentation submitted with Development Application PLN-21-734, advertised on 28/03/2022, subject to the following conditions:

1. **Original fabric of the drill hall and residence, where not documented for removal or alteration in the application documents, must be retained and conserved; and, a suitably qualified heritage practitioner is to review construction drawings and specifications to ensure that the documentation reflects this intent.**
2. **Prior to the commencement of construction, the applicant is to provide Heritage Tasmania's Works Manager a letter confirming that the requirement of Condition 1 has been met and the identity of the heritage practitioner who has undertaken the review.**

Reason for conditions 1 & 2

To ensure that significant heritage fabric is conserved under the direction of appropriately qualified persons.

3. The joinery of the pair of original doors that are proposed to be removed from the north-east elevation between grids 5 & 6 must be retained and stored in good condition within the building, with a copy of this notice attached and in a sealed protective plastic sleeve. The doors are not to be removed from the place without the approval of the Tasmanian Heritage Council.

Reason for condition

Reason for condition:
To ensure that historic material that is associated with this place is retained for reuse.



4. (i) A strategy for the interpretation of the place's historic cultural heritage significance must be prepared. This strategy must be submitted to Heritage Tasmania within 3 months of occupancy of the site and must be to the satisfaction of the Works Manager; and, (ii) all components of the approved strategy must be fully implemented within 6 months of occupancy of the site, or to an alternative timeframe as agreed to by the Works Manager.

Reason for condition

To ensure that the heritage values and history of the site are communicated to users of the place, as a public benefit.

Advice

(1) Consistent with the recommendation of Policy 6.2.2 of the 1995 Conservation Management Plan, it is recommended that (a) the replacement of non-original windows in the ground floor of the north-east elevation be of the same proportions and in the placement pattern as the originals, to recover the building's traditional architectural character, but to keep the glazing pattern of these new windows plain so that they are distinguishable from the originals; and, (b) the original detailing of the residence verandah be reconstructed, rather than introducing contemporary design details (an approach supported by the recommendations of the 2021 Heritage Impact Statement, on pages 40 and 54).

(2) No permanent signage is be erected or installed on the site without the approval of the Tasmanian Heritage Council.

(3) Other than what is shown in the proposal documentation, no roof-top plant is to be installed on the Shadow Building without further approval of the Tasmanian Heritage Council.

Should you require clarification of any matters contained in this notice, please contact Heritage Tasmania's Works Manager, Ian Boersma, on 1 300 850 332.

By

Brett Torossi
Chair
Tasmanian Heritage Council



Submission to Planning Authority Notice

Council Planning Permit No.	PLN-21-734	Council notice date	16/02/2022
TasWater details			
TasWater Reference No.	TWDA 2022/00206-HCC	Date of response	23/02/2022
TasWater Contact	Al Cole Tim Watson (Trade Waste)	Phone No.	0439605108 0427 812711
Response issued to			
Council name	CITY OF HOBART		
Contact details	coh@hobartcity.com.au		
Development details			
Address	20 MCVILLY DR, QUEENS DOMAIN	Property ID (PID)	2835597
Description of development	Partial Change of Use to Light Industry (Whiskey Distillery)		
Schedule of drawings/documents			
Prepared by	Drawing/document No.	Revision No.	Date of Issue
GHD	Site Plan	A	15/10/2021
Conditions			
Pursuant to the <i>Water and Sewerage Industry Act 2008</i> (TAS) Section 56P(1) TasWater imposes the following conditions on the permit for this application:			
CONNECTIONS, METERING & BACKFLOW			
1. <input type="checkbox"/> A suitably sized water supply with metered connections and sewerage system and connections to the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit.			
Advice: Drawings submitted with the application for a Certificate of Certifiable work will need to clearly show the existing and/or proposed water metering and backflow arrangement.			
2. <input type="checkbox"/> Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost.			
3. <input type="checkbox"/> Prior to commencing construction/use of the development, any water connection utilised for construction/the development must have a backflow prevention device and water meter installed, to the satisfaction of TasWater.			
TRADE WASTE			
4. <input type="checkbox"/> Prior to the commencement of operation the developer/property owner must obtain Consent to discharge Trade Waste from TasWater.			
5. <input type="checkbox"/> Consent to Discharge Trade Waste will be once if the developer ensures use of the site for trade waste purposes is consistent with Appendix B in the planning application (for the avoidance of doubt use of advanced primary Trade Waste pre-treatment will be required for a Consent to discharge Trade Waste to be issued).			
6. <input type="checkbox"/> The Developer/property owner must comply with all TasWater conditions prescribed in the Trade Waste Consent			
DEVELOPMENT ASSESSMENT FEES			

☐

Uncontrolled when printed

Page 1 of 3
Version No: 0.2



7. ☐ The applicant or landowner as the case may be, must pay a development assessment fee of \$363.57, to TasWater, as approved by the Economic Regulator and the fee will be indexed, until the date paid to TasWater.

The payment is required within 30 days of the issue of an invoice by TasWater.

Advice

General

For information on TasWater development standards, please visit <https://www.taswater.com.au/building-and-development/technical-standards>

For application forms please visit <https://www.taswater.com.au/building-and-development/development-application-form>

Service Locations

Please note that the developer is responsible for arranging to locate the existing TasWater infrastructure and clearly showing it on the drawings. Existing TasWater infrastructure may be located by a surveyor and/or a private contractor engaged at the developers cost to locate the infrastructure.

- (a) ☐ A permit is required to work within TasWater's easements or in the vicinity of its infrastructure.

Further information can be obtained from TasWater

- (b) ☐ TasWater has listed a number of service providers who can provide asset detection and location services should you require it. Visit www.taswater.com.au/Development/Service-location for a list of companies

- (c) ☐ TasWater will locate residential water stop taps free of charge

- (d) ☐ Sewer drainage plans or Inspection Openings (IO) for residential properties are available from your local council.

Trade Waste

Prior to any Building and/or Plumbing work being undertaken, the applicant will need to make an application to TasWater for a Certificate for Certifiable Work (Building and/or Plumbing). The Certificate for Certifiable Work (Building and/or Plumbing) must accompany all documentation submitted to Council. Documentation must include a floor and site plan with:

- ☐ Schematic drawings and specification (including the size and type or PID drawing) of any proposed pre-treatment device and drainage design; and
- ☐ Location of an accessible sampling point including Trade Waste Flow Meter and in-line monitoring probes, a flow meter with integrated data logger s to be included on the outlet of the spent pot ale holding tank.

Owing to disposal of spent pot ale off-site, this proposal has been assessed under the same criteria as a brewery, and as such advanced primary treatment (filtration, dissolved air flotation or enhanced gravity separation in a clarifier) to reduce filterable suspended solids will be the requirement for a Trade Waste Consent. A TSS limit of 400mg/L applies in this scenario to ensure that aeration demand is minimised by removing filterable BOD.

At the time of submitting the Certificate for Certifiable Work (Building and/or Plumbing) a Trade Waste Application form is required.

If the nature of the business changes or the business is sold, TasWater is required to be informed in order



to review the pre-treatment assessment.

The application forms are available at <http://www.taswater.com.au/Customers/Liquid-Trade-Waste/Commercial>.

Boundary Trap Area

The proposed development is within a boundary trap area and the developer will need to provide a boundary trap that prevents noxious gases or persistent odours back venting into the property's sanitary drain. The boundary trap is to be contained within the property boundaries and the property owner remains responsible for the ownership, operation and maintenance of the boundary trap.

Declaration

The drawings/documents and conditions stated above constitute TasWater's Submission to Planning Authority Notice.

Authorised by

A handwritten signature in black ink, appearing to read "J. Taylor".

Jason Taylor

Development Assessment Manager

TasWater Contact Details

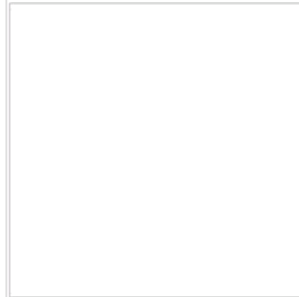
Phone	13 6992	Email	development@taswater.com.au
Mail	GPO Box 1393 Hobart TAS 7001	Web	www.taswater.com.au

☐

Planning: #244430

Property

1 MCVILLY DRIVE HOBART TAS 7000

**People**

Applicant

*

Sullivans Cove Distillery Pty Ltd, by their agent, GHD Pty
Ltd23 Paterson Street
LAUNCESTON TAS 7250
6332 5547
jen.welch@ghd.com

Owner

*

Tasmanian Ports Corporation Pty Ltd

C/- 23 Paterson Street
LAUNCESTON TAS 7250
6332 5547
jen.welch@ghd.com

Owner

*

City of Hobart

c/o 23 Paterson Street
Launceston Tasmania 7025
63325547
jen.welch@ghd.com

Owner

*

The Crown

c/o 23 Paterson Street
Launceston Tasmania 7025
63325547
jen.welch@ghd.com

Entered By

JEN WELCH
6332 5547
jen.welch@ghd.com**Use**

Industrial

Details

Have you obtained pre application advice?

☒ Yes

If YES please provide the pre application advice number eg PAE-17-xx

Ben Ikin

Are you applying for permitted visitor accommodation as defined by the State Government Visitor Accommodation Standards? Click on help information button for definition. If you are not the owner of the property you MUST include signed confirmation from the owner that they are aware of this application.

*

☒ No

Is the application for SIGNAGE ONLY? If yes, please enter \$0 in the cost of development, and you must enter the number of signs under Other Details below.

*

☒ No

If this application is related to an enforcement action please enter Enforcement Number

Details

What is the current approved use of the land / building(s)?

*

former Cadet Drill Hall and Visitor Accommodation

Please provide a full description of the proposed use or development (i.e. demolition and new dwelling, swimming pool and garage)

*

Refer attached

Estimated cost of development

*

15000000.00

Existing floor area (m2)

Proposed floor area (m2)

Site area (m2)

1085.00

1330.00

6871

Carparking on Site

N/A

Total parking spaces

3

Existing parking spaces

☐ Other (no selection chosen)
Other Details

Does the application include signage?

*

No

How many signs, please enter 0 if there are none involved in this application?

*

0

Tasmania Heritage Register

Is this property on the Tasmanian Heritage Register?

☒ Yes
Documents**Required Documents**

Title (Folio text and Plan and Schedule of Easements)

*

0. Titles Combined.pdf

Plans (proposed, existing)
*
3. Architectural Drawing_000 - 302.pdf
GM or Crown consent
Letter of Consent - TasPorts - SCD - 20211027.pdf
Covering Letter
12533276-LET_ Council consent request.pdf
Supporting Documents
Concept Servicing Plan
11. 12533276-W001-SITE SERVICES A.pdf
Traffic Impact Assessment
2. 12533276-REP-0_Huon_Quays_Traffic_Impact_Assessment.pdf
Architectural Description
7. 2109 - Sullivans_DA9101 Architect's Design Statement.pdf
Landscape Plan
10. Landscape Architect_DA Plans.pdf
Planning Report
1. 12533276-REP-1_Planning Assessment.pdf
Heritage Report
8. Heritage Impact Assessment v3.pdf
Plans
4. Architectural Drawings_501 - 505.pdf
Plans
5. Architectural Drawings_601 - 605.pdf
Plans
6. Architectural Drawings_701 - 801.pdf
Landscape Report
9. Landscape Architect_DA Report.pdf



23 Paterson Street,
Launceston, Tasmania 7250
Australia
www.ghd.com

Your ref:
Our ref: 12533276

28 October 2021

General Manager
City of Hobart
GPO Box 503
Hobart TAS 7001

Landowner Consent Request – Permit application for McVilly Drive, Queens Domain

Dear Ms Grigsby,

This letter accompanies a permit application for use and development of land at McVilly Drive, Queens Domain, which incorporates land the City of Hobart has authority for. In accordance with section 52 of the *Land Use Planning and Approvals Act 1993*, it is requested that consent is provided for that part of the proposed development that relates to City of Hobart land.

Development on Council land is proposed at 20 McVilly Drive, Queens Domain (CT135056/3). The proposed development is to provide a pedestrian connection between the frontage of the proposed redevelopment of the Huon Quays building at 13 and 17 McVilly Drive to the existing pedestrian overpass over the rail corridor. The detail of the proposed link is provided in the accompanying documentation in Landscape Plan (drawing L001).

Should you wish to discuss this matter please do not hesitate to contact me.

Regards

A handwritten signature in black ink, appearing to read 'Jen Welch', written in a cursive style.

Jen Welch
Senior Planner
+61 3 63325547
jen.welch@ghd.com

This document is in draft form. The contents, including any opinions, conclusions or recommendations contained in, or which may be implied from, this draft document must not be relied upon. GHD reserves the right, at any time, without notice, to modify or retract any part or all of the draft document. To the maximum extent permitted by law, GHD disclaims any responsibility or liability arising from or in connection with this draft document.



Enquiries to: City Planning
Phone: (03) 6238 2711
Email: coh@hobartcity.com.au

16 February 2022

(Sullivans Cove Distillery Pty Ltd, by their agent, GHD Pty Ltd) mailto: jen.welch@ghd.com
23 Paterson Street
Launceston Tasmania 7025

Dear Sir/Madam

**20 MCVILLY DRIVE, HOBART - WORKS ON COUNCIL LAND NOTICE OF LAND
OWNER CONSENT TO LODGE A PLANNING APPLICATION - GMC-22-5**

Site Address:

20 McVilly Drive, Hobart

Description of Proposal:

Partial Change of Use to Light Industry (Whiskey Distillery), Eating Establishment, Function Centre, Hotel and Shop, Partial Demolition, Extension, Alterations & Associated Works

Applicant Name:

Sullivans Cove Distillery Pty Ltd
C/- GHD Pty Ltd

PLN (if applicable):

PLN-21-734

I write to advise that pursuant to Section 52 of the *Land Use Planning and Approvals Act 1993*, I grant my consent on behalf of the Hobart City Council as the owner/administrator of the above land for you to make application to the City for a planning permit for the development described above and as per the attached documents. I granted consent pursuant to delegation, a copy of which is enclosed.

Please note that the granting of the consent is only for the making of the application and in no way should such consent be seen as prejudicing any decision the Council is required to make

Hobart Town Hall
50 Macquarie Street
Hobart TAS 7000

Hobart Council Centre
16 Elizabeth Street
Hobart TAS 7000

City of Hobart
GPO Box 503
Hobart TAS 7001

T 03 6238 2711
F 03 6234 7109
E coh@hobartcity.com.au
W hobartcity.com.au

CityofHobartOfficial
ABN 39 055 343 428
Hobart City Council

as the statutory planning authority.

This consent does not constitute an approval to undertake any works and does not authorise the owner, developer or their agents any right to enter or conduct works on any Council managed land whether subject to this consent or not.

If planning approval is granted by the planning authority, you will be required to seek approvals and permits from the City as both landlord, land manager, or under other statutory powers (such as other legislation or City By-Laws) that are not granted with the issue of a planning permit under a planning scheme. This includes the requirement for you to reapply for a permit to occupy a public space under the City's Public Spaces By-law if the proposal relates to such an area.

Accordingly, I encourage you to continue to engage with the City about these potential requirements.

Yours faithfully



(John Fisher)

ACTING DIRECTOR CITY AMENITY

Relevant documents/plans:

Landscape Site Plan - Phase 1 Works Only by Realm Studios



City of Hobart

INSTRUMENT OF DELEGATION

General Delegation

Director City Amenity

I, Kelly Grigsby, Chief Executive Officer, being the General Manager of the Hobart City Council as appointed by Council pursuant to section 61 of the *Local Government Act 1993* ("the Act") hereby delegate pursuant to Section 64 of the Act, the following powers and functions to the **Director City Amenity**, or to such persons who may be acting in that position:

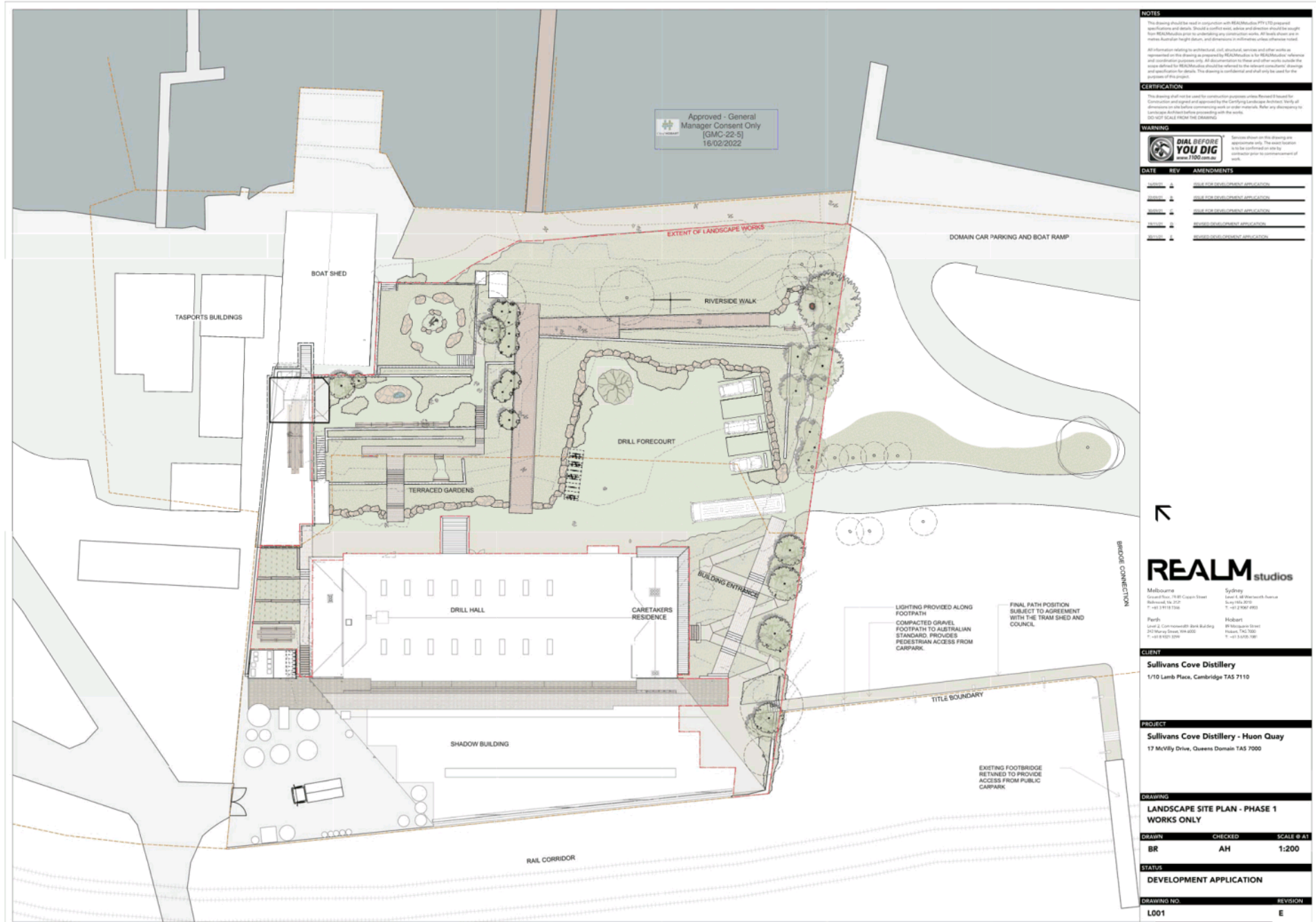
1. to sign an application; and
2. to provide written permission to make an application;

pursuant to section 52(1B) of the *Land Use Planning and Approvals Act 1993*,
EXCEPT where an application is recommended for refusal.

Dated this 20 day of August 2021

(Kelly Grigsby)
CHIEF EXECUTIVE OFFICER

Being the General Manager as appointed by the Council pursuant to Section 61 of the Local Government Act 1993 (tas)





Salamanca Building Parliament Square
4 Salamanca Place, Hobart TAS
GPO Box 536, Hobart TAS 7001 Australia
Email permits@stategrowth.tas.gov.au Web www.stategrowth.tas.gov.au
Ref: SRA-21-574

Jen Welch
Sullivans Cove Distillery Pty Ltd, by their agent, GHD Pty Ltd
By email: jen.welch@ghd.com

Dear Jen

Crown Landowner Consent Granted - 1, 13, 17 and 20 McVilly Drive (Tasman Highway), Queens Domain

I refer to your recent request for Crown landowner consent relating to the development application at 1, 13, 17 and 20 McVilly Drive (Tasman Highway), Queens Domain for Distillery and associated visitor experience and works for a footpath to connect to existing pedestrian overpass

I, Fiona McLeod, Director Asset Management, State Roads, having been duly delegated by the Minister under Section 52 (1F) of the *Land Use Planning and Approvals Act 1993* (the Act), and in accordance with the provisions of Section 52 (1B) (b) of the Act, hereby give my consent to the making of the application, insofar as it affects the State road network and any Crown land under the jurisdiction of this Department.

The consent given by this letter is for the **making of the application only** insofar as that it impacts Department of State Growth administered Crown land and is with reference to your application dated 28 October 2021, and the documents approved, as follows:

Approved Document Name	Author	Date Received	Notes
Crown Landowner Consent Application Form -	Jen Welch (Sullivans Cove Distillery Pty Ltd)	29-10-21	
Planning Permit Application Form	Jen Welch	13-12-21	Online form, PDF receipt (#244430)
Certificate of Title Documents		29-10-21	134036/2 134036/1 134037/1 118002/1 26915/2 26915/3 135056/3 Untitled Crown land (NRET) PID 1699706
Planning Assessment Report	GHD	29-10-21	Huon Quays, Planning Assessment, Sullivans Cove Distillery, Pty Ltd, Rev 1, Dated 29/10/2021
Traffic Impact Assessment	GHD	29-10-21	Huon Quays, Traffic Impact Assessment, Sullivans Cove Distillery Pty Ltd, 1 Rev 0, Dated 31/10/2021
Architectural Drawings	John Wardle Architects	29-10-21	Sullivans Cove Distillery, Development Application Approval, DA Drawing List Sheets: DA0000 Cover Sheet Rev3 27.09.2021 DA0001 Locality Plan - Existing Rev4 27.09.2021

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			DA0011 Site Plan - Demolition Rev5 27.09.2021 DA0012 Site Plan - Proposed Rev5 27.09.2021 DA0051 Demolition - Ground Floor Plan Rev5 27.09.2021 DA0052 Demolition - First Floor Plan Rev5 27.09.2021 DA0053 Demolition - Second Floor Plan Rev5 27.09.2021 DA0054 Demolition - Roof Plan Rev5 27.09.2021 DA0055 Demolition - Elevations Rev5 27.09.2021 DA0056 Demolition - Elevations Rev5 27.09.2021 DA0057 Demolition - Sections Rev5 27.09.2021 DA0101 Proposed - Ground Floor Plan Rev6 27.09.2021 DA0102 Proposed - First Floor Plan Rev6 27.09.2021 DA0103 Proposed - Second Floor Plan Rev6 27.09.2021 DA0104 Proposed - Roof Plan Rev5 27.09.2021 DA0201 Proposed - Drill Hall Elevations Rev5 27.09.2021 DA0202 Proposed - Production Hall Elevations Rev5 27.09.2021 DA0203 Proposed - Cross Elevations Rev5 27.09.2021 DA0301 Proposed - Drill Hall Sections Rev5 27.09.2021 DA0302 Proposed - Cross Sections Rev5 27.09.2021 DA0501 Artist Impression - Exterior View 01, Site Arrival Rev3 27.09.2021 DA0502 Artist Impression - Exterior View 02, Main Entry Rev3 27.09.2021 DA0503 Artist Impression - Interior View 03, Drill Hall facing West Rev3 27.09.2021 DA0504 Artist Impression - Interior View 04, Drill Hall facing East Rev3 27.09.2021 DA0505 Artist Impression - Interior View 05, Drill Hall facing South Rev3 27.09.2021 DA0601 Visual Impact Study - View from Cycle Path Rev4 27.09.2021 DA0602 Visual Impact Study - View from Rail Corridor Rev4 27.09.2021 DA0603 Visual Impact Study - View from Eastern Entry Road Rev3 27.09.2021 DA0604 Visual Impact Study - View from River Derwent Rev3 27.09.2021 DA0605 Visual Impact Study - View from Cenotaph Rev1 27.09.2021 DA0701 Proposed Palette of Materials and Finishes Rev2 27.09.2021 DA0801 Shadow Studies Rev4 27.09.2021
Architectural Design Statement	John Wardle Architects	29-10-21	2019 Sullivans Cover Distillery, Document No. DA9101, Architect's Design Statement for DA Rev (1), Issues 10/10/21
Heritage Impact Assessment	Praxis Environment	29-10-21	Heritage Impact Assessment, Huon Quays Redevelopment, Sullivan Cover Distillery, October 2021
Landscape Statement	REALM Studios	29-10-21	Sullivans Cove Distillery, Huon (Sic) Quays, Landscape Development Application Issue C, Dated 28/09/2021,.
Landscape Site Plan	REALM Studios	01-12-21	Landscape Site Plan – Phase 1 Works Only

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			<i>Drawings No. L001, Rev E, dated 30/11/2021</i>
Site Services Concept Plan	GHD	29-10-21	<i>Site Services Concept Plan, Drawing NO. 1253276-W001, Rev A</i>

In giving consent to lodge the subject development application, the Department notes the following applicable advice:

Works in/occupancy of the rail corridor:

The revised pathway plan (211130_Sullivans Cove_DA Site Plan – Rev E) received 1/12/21 aligns with TasRail advice.

Prior to undertaking works, TasRail works, and occupancy approvals will be required. For further information contact TasRail at property@tasrail.com.au

Other:

The applicant should note that the site is located adjacent to an area likely to be developed as a significant transport corridor, which may have implications for movements around and across the corridor.

The Department reserves the right to make a representation to the relevant Council in relation to any aspect of the proposed development relating to its road network and/or property.

Yours sincerely

M. Good

Fiona McLeod
DIRECTOR ASSET MANAGEMENT

Delegate of
Minister for Infrastructure and Transport
Michael Ferguson MP

21 March 2022

cc: General Manager, Hobart City Council

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[illegible]



Crown Landowner Consent Application

Department of State Growth

Applicant Details

Applicant First Name: Jen

Applicant Last Name: Welch

Company Name: Sullivans Cove Distillery Pty Ltd,
by their agent, GHD Pty Ltd

Postal Address: 23 Paterson Street, Launceston
TAS 7250

Contact Phone Number: 63325547

Contact Email: jen.welch@ghd.com

Application Details

Type of Application to Planning permit application
Council:

Development Involve Any Of
The Following:

Details of Proposal

Street Address: 1, 13, 17 and 20 McVilly Drive, Queens Domain

Description of Site: Refer to attached Planning Assessment report, Section 2

Impact on Crown Land or Minor works for a footpath to connect to existing pedestrian overpass.
State Road:

Description of Proposal: Distillery and associated visitor experience as described further in the attached Planning Assessment report, Section 3.

Local Council Area: Hobart City

Previous Contact With
Anyone At The Department
Of State Growth:

Supporting Documents

Development Involve Any Of
The Following:

Drainage: No

Sewer: No

Altered Access To State Road No
Network:

Completed Council Planning DSG consent.pdf
Application Form:

Files to be send separately Yes
due to size limit:

All Plans, Reports And
Supporting Documentation

Forming The Planning
Application:

Current Certificate of Title Certificate of Title Volume 134036 Folio 1 - Hobart - Huon Quays (landside).PDF
details:



Wednesday, 27 October 2021

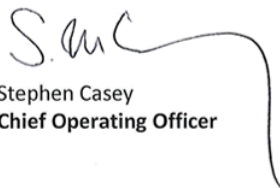
City of Hobart
C/- Planning Department
GPO Box 503
HOBART TAS 7001

Dear Sir/Madam,

Re: TASPORTS CONSENT FOR LODGEMENT OF PERMIT APPLICATION - PROPOSED REDEVELOPMENT OF LAND AT 1,13 AND 17 MCVILLY DRIVE, QUEENS DOMAIN

This letter, issued pursuant to section 53 of the Land Use Planning and Approvals Act 1993, is to confirm TasPorts support for the permit application, insofar as the proposed development relates to TasPorts land. The permit application is for Sullivans Cove Distillery Pty Ltd to redevelop the land as a whisky distillery and associated visitor experience.

Yours sincerely



Stephen Casey
Chief Operating Officer

Head Office
90-110 Willis Street,
Launceston
PO Box 1060
Launceston
Tasmania 7250
reception@tasports.com.au

Port of Devonport
48 Formby Road,
Devonport
PO Box 478
Devonport
Tasmania 7310

Port of Bell Bay
Mobil Road,
Bell Bay
Locked Bag 4
George Town
Tasmania 7253

Port of Burnie
Port Road,
Burnie
PO Box 216
Burnie
Tasmania 7320

Port of Hobart
Level 13,
Trafalgar Building
110 Collins Street
GPO Box 202 Hobart
Tasmania 7001



23 Paterson Street,
Launceston, Tasmania 7250
Australia
www.ghd.com

Your ref: PLN-21-734
Our ref: 12533276

23 March 2022

Cameron Sherriff
City of Hobart
16 Elizabeth Street
Hobart TAS 7000

Owner Notification – Proposed Huon Quays Redevelopment

Dear Cameron,

I am writing on behalf of Sullivans Cove Distillery in relation to the proposed redevelopment of Huon Quays (permit application no PLN-21-734). I declare that a letter of notification was sent to the Macquarie Point Development Corporation as the Landowners of title 179192 folio 4 on the 22 March 2022.

Notification was provided to the landowner of the intention to make an application inclusive of their land in accordance with Section 52 of the *Land Use Planning and Approvals Act 1993*.

If you have any queries in relation to this matter, please do not hesitate to contact me.

Regards

A handwritten signature in black ink, appearing to read 'Jen Welch', written in a cursive style.

Jen Welch
Senior Planner

+61 3 63325547
jen.welch@ghd.com

**RESULT OF SEARCH**

REGISTERED INFORMATION

Issued Pursuant to the Land Titles Act 1980



REGISTERED INFORMATION

MO 2	MO 1
MO 2	MO 1

SEARCH DATE : 01-Sep-2021

SEARCH TIME : 11.02 AM

DESCRIPTION OF LAND

City of HOBART

Lot 3 on Plan 26915

Derivation : Whole of 4693m2 (Lot 3) Vested in The Australian
National Railways Commission

Prior CT 4385/3

SCHEDULE 1

C139362 TRANSFER to THE CROWN Registered 16-Oct-1998 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

UNREGISTERED DEALINGS AND NOTATIONS

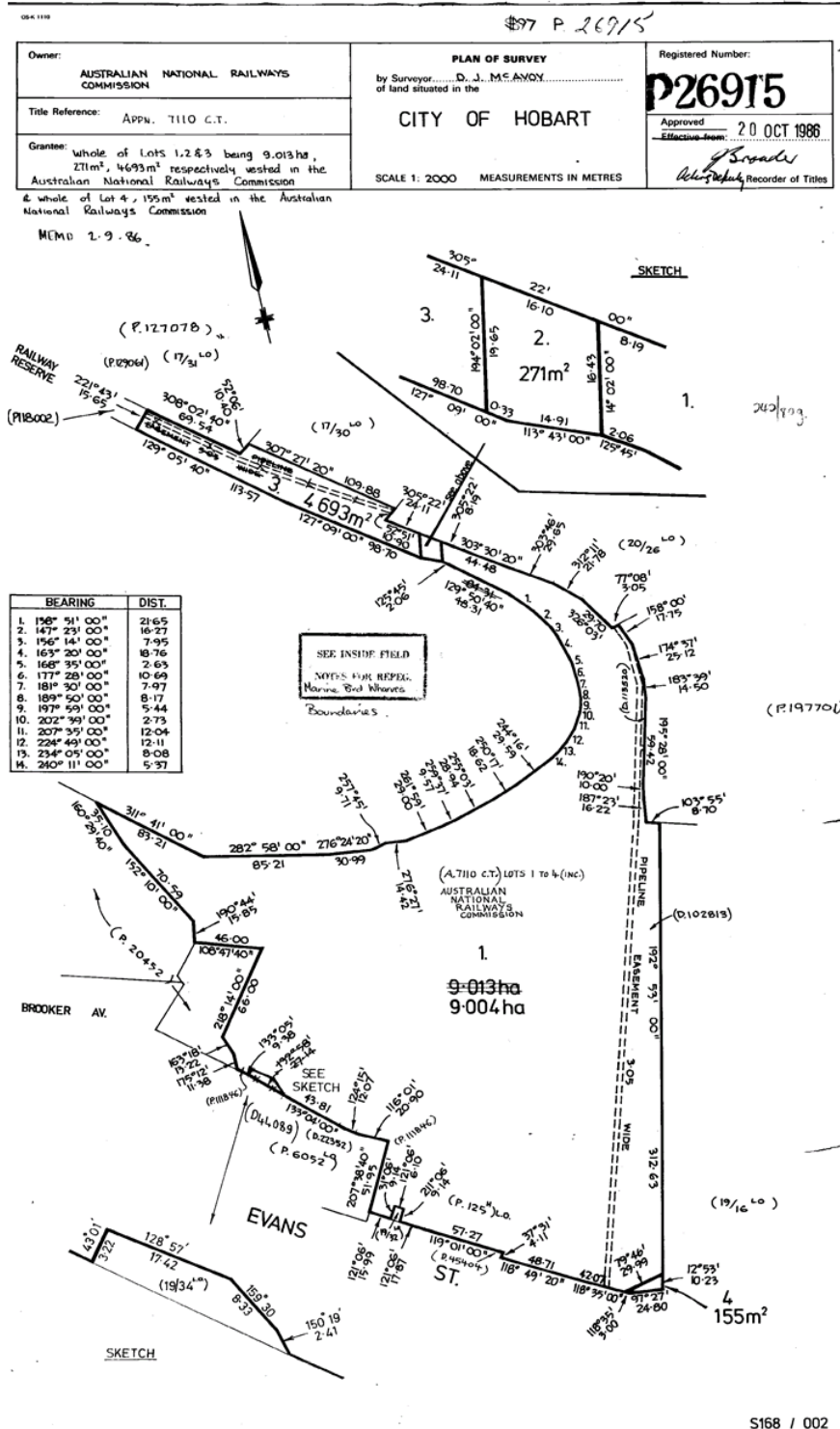
No unregistered dealings or other notations



FOLIO PLAN

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Issued Pursuant to the Land Titles Act 1980



**RESULT OF SEARCH**

REGISTERED INFORMATION

Issued Pursuant to the Land Titles Act 1980



REGISTERED INFORMATION

MO 002	0000 0
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SEARCH DATE : 28-Oct-2021

SEARCH TIME : 11.37 AM

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Plan 118002

Being the land described in Conveyance 8/2393

Derivation : Whole of Lot 1,784m2 vested in the Australian
National Railways Commission

Derived from A 14369

SCHEDULE 1

C139362 TRANSFER to THE CROWN Registered 16-Oct-1998 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

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www.thelist.tas.gov.au



FOLIO PLAN

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Issued Pursuant to the Land Titles Act 1980



FILE NUMBER A.14369 GRANTEE WHOLE OF LOT 1, 784m ² VESTED IN THE AUSTRALIAN NATIONAL RAILWAYS COMMISSION	CONVERSION PLAN LOCATION CITY OF HOBART CONVERTED FROM 8/2393 NOT TO SCALE LENGTHS IN METRES		REGISTERED NUMBER P118002
			APPROVED 20 FEB 1996 <i>Michael Dine</i> Recorder of Titles
MAPSHEET MUNICIPAL CODE No. 114	LAST UPI No.	ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN	DRAWN MC

SKETCH BY WAY OF ILLUSTRATION ONLY
"EXCEPTED LANDS"

**RESULT OF SEARCH**

REGISTERED INFORMATION

Issued Pursuant to the Land Titles Act 1980



REGISTERED INFORMATION

SEARCHED	INDEXED
SERIALIZED	FILED
2	2021 OCT 28

SEARCH DATE : 28-Oct-2021

SEARCH TIME : 11.32 AM

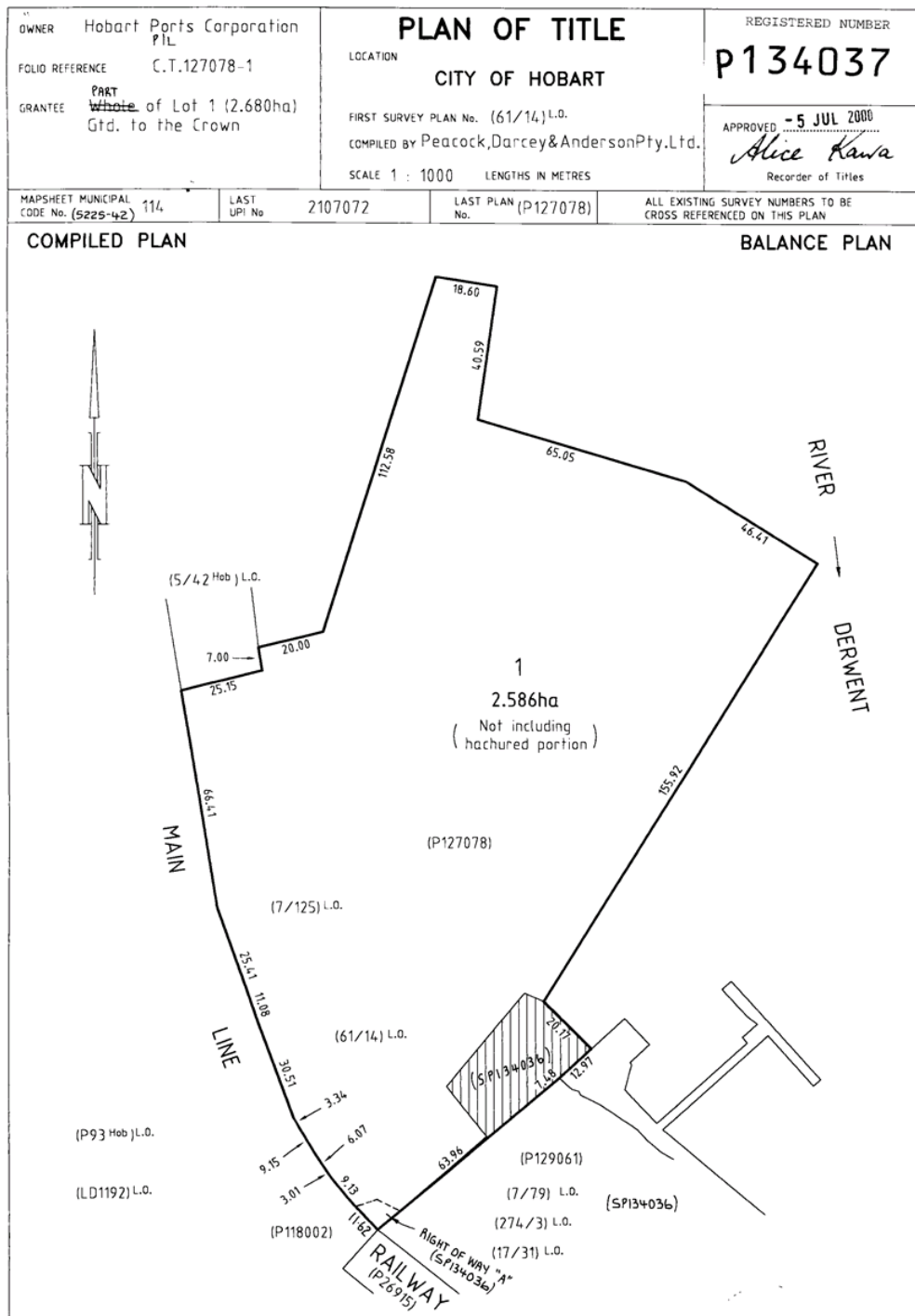
DESCRIPTION OF LAND

City of HOBART

Lot 1 on Plan 134037

Derivation : Part of Lot 1 on Plan 127078 Gtd. to The Crown
Prior CT 127078/1SCHEDULE 1C631907 TASMANIAN PORTS CORPORATION PTY LTD Registered
11-Apr-2006 at noonSCHEDULE 2Reservations and conditions in the Crown Grant if any
SP 134036 BURDENING EASEMENT: Right of Carriageway
[appurtenant to Lot 2 on Sealed Plan No. 134036] over
Right of Way A shown on Plan No. 134037
B838908 FENCING PROVISION in TransferUNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations





RESULT OF SEARCH

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Issued Pursuant to the Land Titles Act 1980



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SEARCH DATE : 28-Oct-2021

SEARCH TIME : 11.38 AM

DESCRIPTION OF LAND

City of HOBART

Lot 3 on Plan 135056

Derivation : Whole of Lot 3 Gtd to the Hobart City Council

Derived from Grant No.358/26

SCHEDULE 1

HOBART CITY COUNCIL

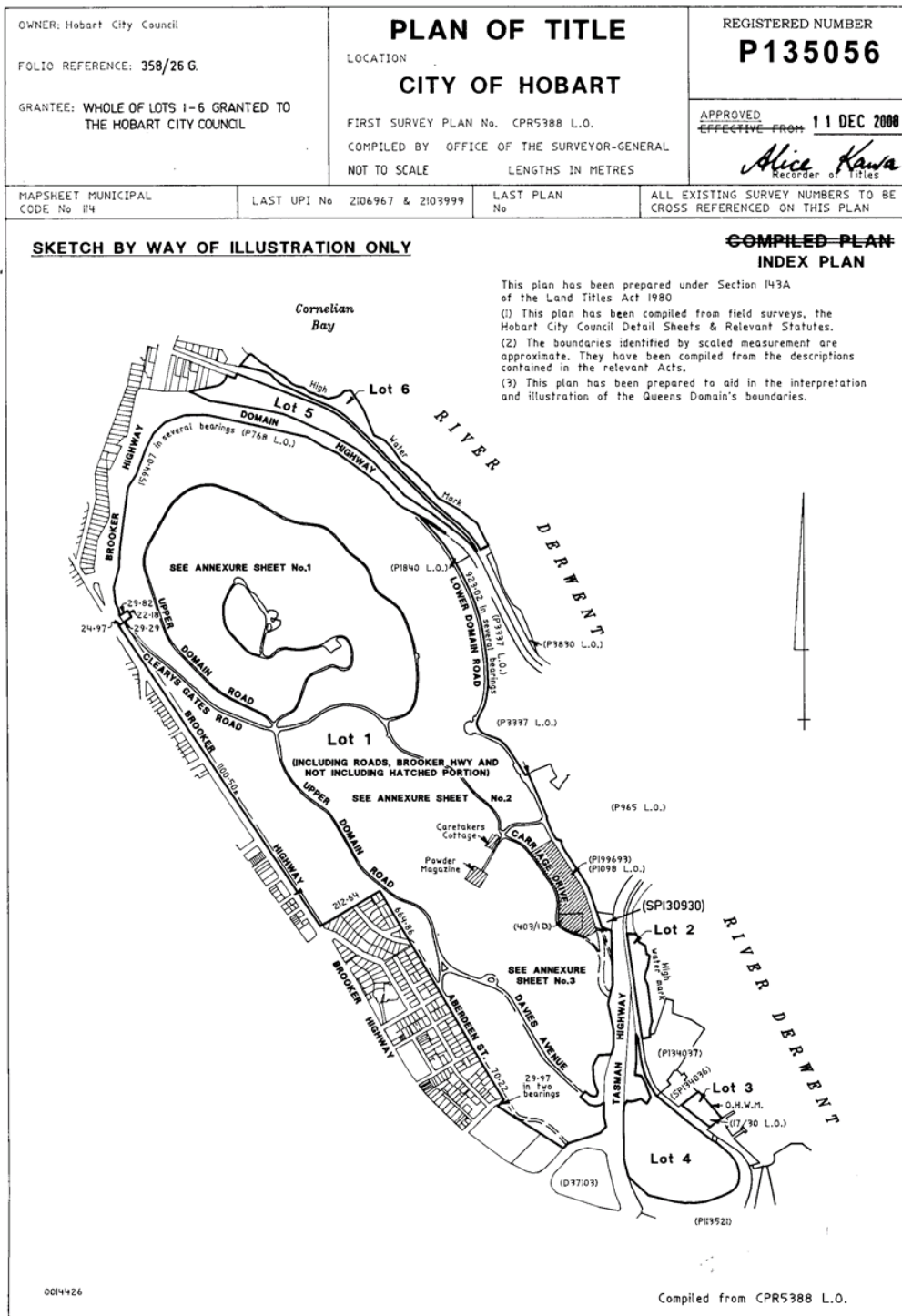
SCHEDULE 2

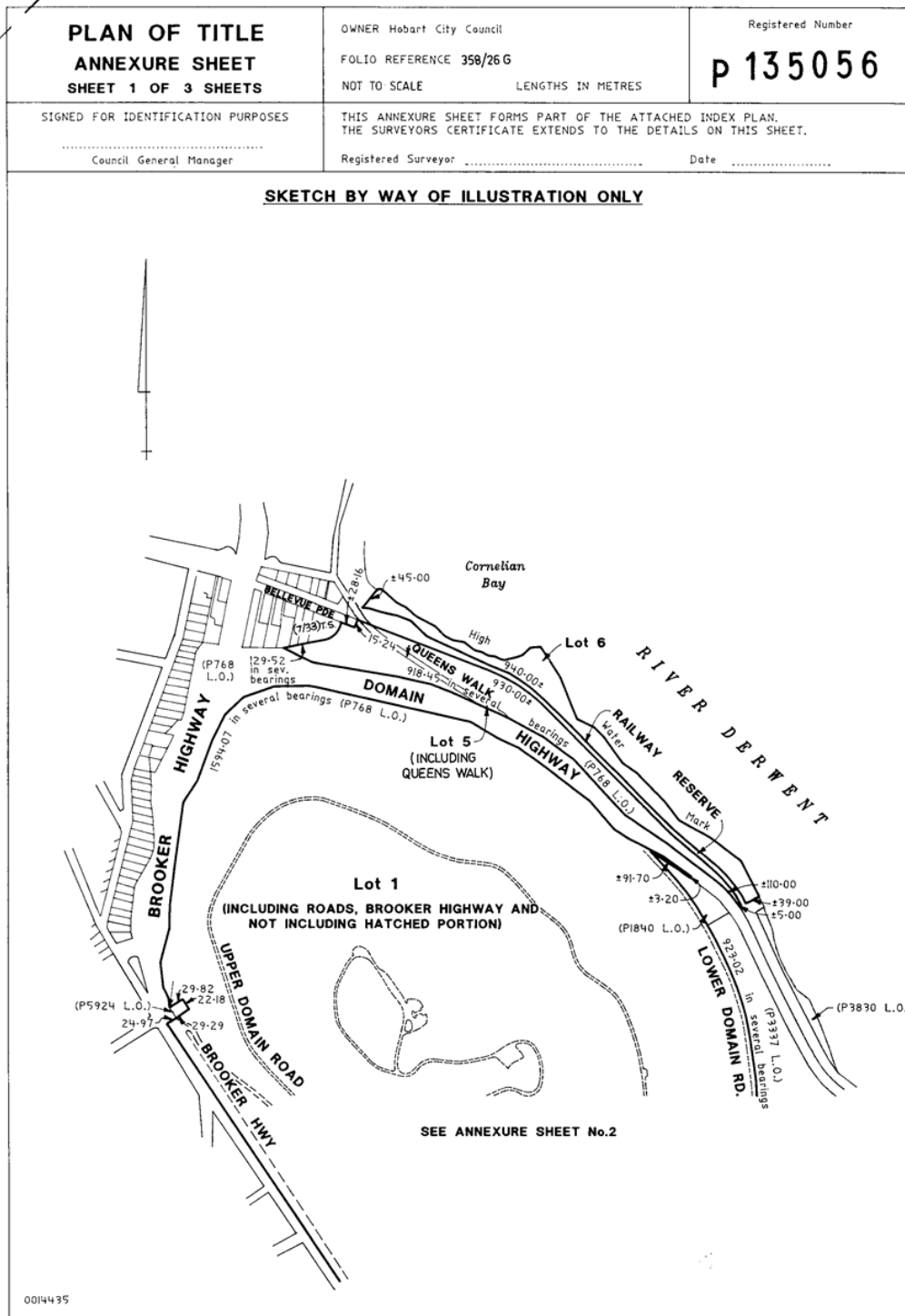
358/26 Land is limited in depth to 15 metres, excludes minerals and is subject to reservations relating to drains sewers and waterways in favour of the Crown

358/26 Grant made Subject To use (subject to the Hobart Corporation Act 1963) for the recreation, amusement, health & enjoyment of the inhabitants of the city and other persons resident in or coming into the city

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



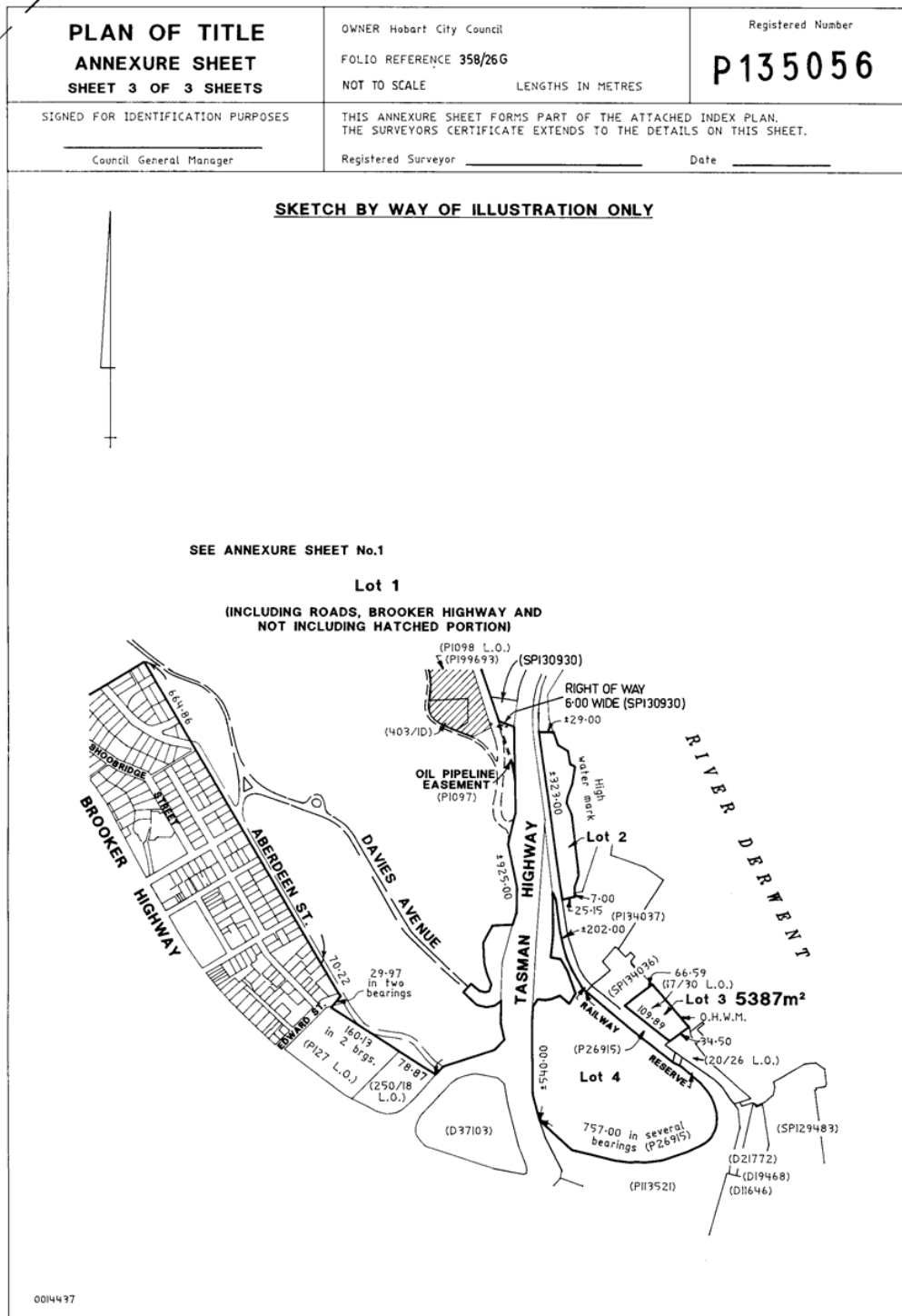


FOLIO PLAN

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Issued Pursuant to the Land Titles Act 1980

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RESULT OF SEARCH

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Issued Pursuant to the Land Titles Act 1980



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SEARCH DATE : 16-Dec-2021
SEARCH TIME : 12.35 PM

DESCRIPTION OF LAND

City of HOBART
Lot 1 on Sealed Plan 134036
Derivation : Whole of Lot 2 on Plan 129061 Gtd. to The Crown,
Part of Lot 1 on Plan 129061 Gtd. to The Crown, part of Lot 1
on Plan 127078 Gtd. to The Crown
Prior CTs 127078/1, 129061/1 and 129061/2

SCHEDULE 1

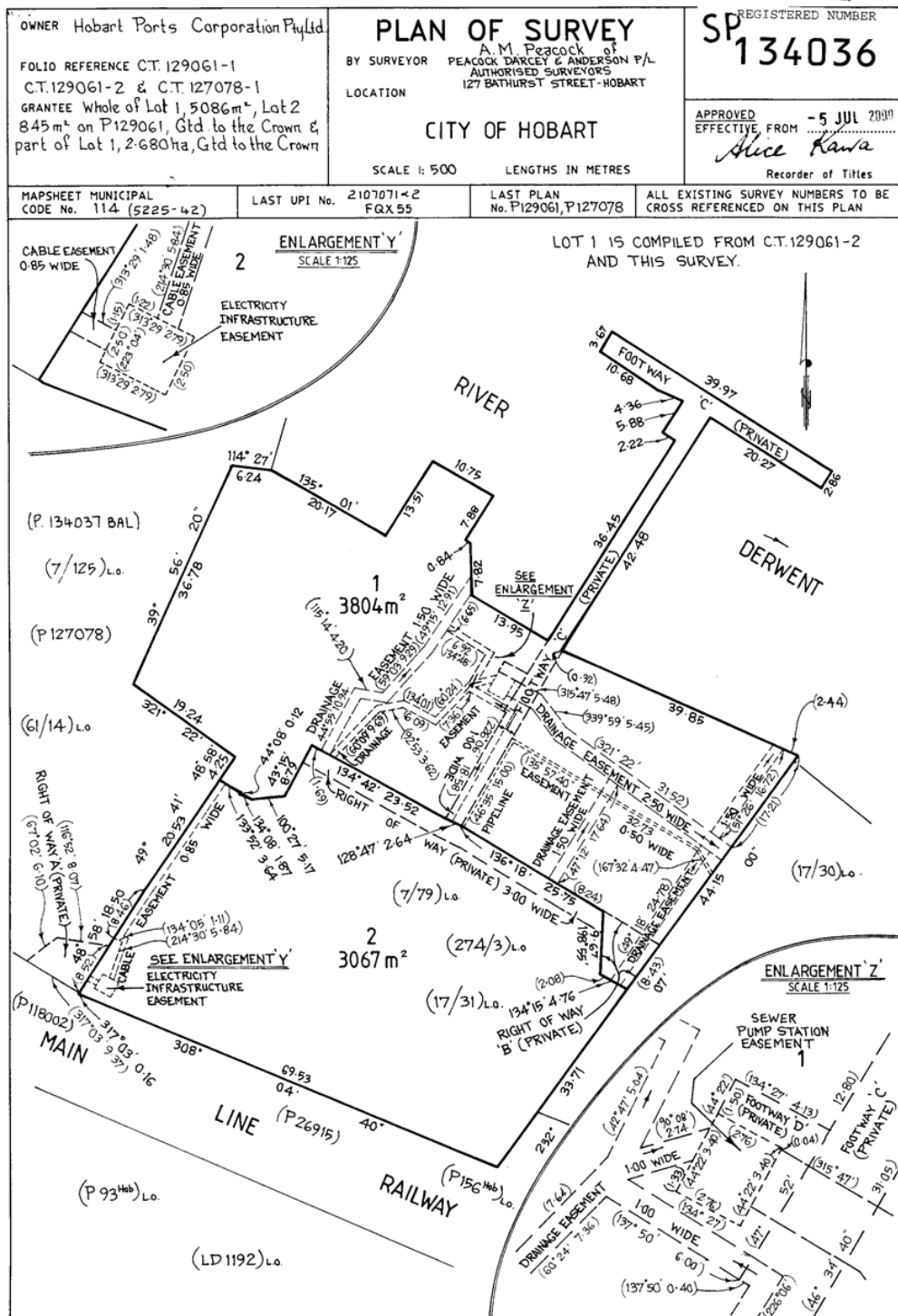
C631907 TASMANIAN PORTS CORPORATION PTY LTD Registered
11-Apr-2006 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
SP 134036 EASEMENTS in Schedule of Easements
SP 134036 FENCING PROVISION in Schedule of Easements
B838908 FENCING PROVISION in Transfer
C298894 CAVEAT by Commonwealth of Australia Registered
23-May-2001 at noon
C356197 INSTRUMENT Creating Restrictive Covenants Registered
27-Jun-2002 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations





SCHEDULE OF EASEMENTS

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Issued Pursuant to the Land Titles Act 1980



<p align="center">SCHEDULE OF EASEMENTS</p> <p>NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.</p>	<p align="right">SP REGISTERED NUMBER 134036</p>
--	--

PAGE 1 OF 5 PAGE/S

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage ~~over~~ the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

' 1 on the plan is:

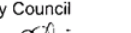
Together with a right of carriageway on the Right of Way (Private) 3.00 wide shown on the plan.

Together with an Electricity Infrastructure Easement (as herein defined) over the Electricity Infrastructure Easement shown on the plan.

together with a Cable Easement (as herein defined) over the Cable Easement 0.85 wide shown on the plan.

Subject to a Pipeline Easement (as herein defined and appurtenant to Lot 2 on the plan) over the Pipeline Easement 0.50 wide shown on the plan.

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: Hobart Ports Corporation Pty Ltd FOLIO REF: Volume 127078 Folio 1 and Volume 129061 Folios 1 and 2 SOLICITOR & REFERENCE: Page Seager Steven J Latham	PLAN SEALED BY: Hobart City Council DATE: 20 April 2000 773.2 REF NO.	 Council Delegate SURVEYING SERVICES MANAGER
NOTE: The Council Delegate must sign the Certificate for the purposes of identification.		



SCHEDULE OF EASEMENTS

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Issued Pursuant to the Land Titles Act 1980



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 2 OF 5 PAGE/S</p>	<p>Registered Number</p> <p>SP134036</p>
<p>SUBDIVIDER: Hobart Ports Corporation Pty Ltd</p> <p>FOLIO REFERENCE: Volume 127078 Folio 1 and Volume 129061 Folios 1 and 2.</p>	

Subject to a Right of Footway (appurtenant to Lot 2 on the plan) over the footways (Private) "C" and "D" shown on the plan.

subject to a Sewer Pump Station Easement (as defined and appurtenant to Lot 2 on the plan) over the Sewer Pump Station Easement shown on the plan.

Subject to a right of carriageway (appurtenant to Lot 2 on the plan) over the Right of Way "B" (Private) shown on the plan.

At 2 on the plan is:

Together with a Pipeline Easement (as herein defined) over the Pipeline Easement 0.50 wide shown on the plan.

Together with a right of footway over the Footways (Private) marked "C" and "D" on the plan.

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

Am. Mus. Nat. Hist.



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 3 OF 5 PAGE/S</p>	<p>Registered Number</p> <p>SP 134036</p>
<p>SUBDIVIDER: Hobart Ports Corporation Pty Ltd FOLIO REFERENCE: Volume 127078 Folio 1 and Volume 129061 Folios 1 and 2</p>	



SCHEDULE OF EASEMENTS

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Issued Pursuant to the Land Titles Act 1980



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 4 OF 5 PAGE/S</p>	<p>Registered Number</p> <p>SI 134036</p>
<p>SUBDIVIDER: Hobart Ports Corporation Pty Ltd FOLIO REFERENCE: Volume 127078 Folio 1 and Volume 129061 Folios 1 and 2</p>	

Fencing Provision:

In respect of the lots on the plan the Vendor (Hobart Ports Corporation Pty Ltd) shall not be required to fence.

Definitions:

Sewer Pump Station Easement means:

The right for the owner for the time being of the dominant lot together with workers, surveyors and others to enter upon the land shown on the plan as Sewer Pump Station Easement and to construct, install and maintain pumps, pipes, valves, wires and fittings together with such ancillary items in connection therewith may be necessary for the passage of sewerage together with the right to repair, replace, amend or renew the same.

Cable Easement means:

The right for the owner for the time being of the dominant lot together with workers, surveyors and others to enter upon the land shown on the plan as Cable Easement and to lay, construct and maintain cables, wires and fittings together with such ancillary items in connection therewith as may be necessary for the conduct of electricity together with the right to repair, replace, amend or renew the same.

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

Answer



SCHEDULE OF EASEMENTS

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Issued Pursuant to the Land Titles Act 1980



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 5 OF 5 PAGE/S</p>	<p>Registered Number</p> <p>SP 134036</p>
<p>SUBDIVIDER: Hobart Ports Corporation Pty Ltd FOLIO REFERENCE: Volume 127078 Folio 1 and Volume 129061 Folios 1 and 2</p>	

Electricity Infrastructure Easement means:

The right for the owner for the time being of the dominant lot together with workers, surveyors and others to enter upon the land shown on the plan as Electricity Infrastructure Easement and to install, construct and maintain wires, cables, apparatus and fittings together with such ancillary items in connection therewith as may be necessary together with the right to repair, replace, amend or renew the same.

Pipeline Easement means:

The right for the owner for the time being of the dominant lot together with workers, surveyors and others to enter upon the land shown on the plan as Pipeline Easement and to lay pipes, valves and fittings together with such ancillary items as may be necessary to enable the passage of workers together with the right to repair, replace, cleanse, amend or renew the same.

THE COMMON SEAL of HOBART PORTS
CORPORATION being the registered proprietor
of the land in Folios of the Register Volume 129061
Folios 1 and 2 and Volume 127078 Folio 1 is
hereunto affixed in the presence of:



By _____

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

the dealing.



RESULT OF SEARCH

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Issued Pursuant to the Land Titles Act 1980

[illegible]

SEARCH DATE : 16-Dec-2021
SEARCH TIME : 12.38 PM

DESCRIPTION OF LAND

City of HOBART
Lot 2 on Sealed Plan 134036
Derivation : Part of Lot 2 on Plan 129061 Gtd. to The Crown
Prior CT 129061/2

SCHEDULE 1

M501438 TRANSFER to TASMANIAN PORTS CORPORATION PTY LTD
Registered 06-Jan-2015 at 12.01 PM

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 SP 134036 EASEMENTS in Schedule of Easements
 SP 134036 FENCING PROVISION in Schedule of Easements
 C356197 INSTRUMENT Creating Restrictive Covenants Registered
 27-Jun-2002 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



SCHEDULE OF EASEMENTS

R□□□RD□R□□□□□□□□□□

Issued Pursuant to the Land Titles Act 1980



<p align="center">SCHEDULE OF EASEMENTS</p> <p>NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.</p>	<p align="right">SP REGISTERED NUMBER 134036</p>
--	--

PAGE 1 OF 5 PAGE/S

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage ~~over~~ the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

Point 1 on the plan is:


Together with a right of carriageway on the Right of Way (Private) 3.00 wide shown on the plan.

Together with an Electricity Infrastructure Easement (as herein defined) over the Electricity Infrastructure Easement shown on the plan.

together with a Cable Easement (as herein defined) over the Cable Easement 0.85 wide shown on the plan.

Subject to a Pipeline Easement (as herein defined and appurtenant to Lot 2 on the plan) over the Pipeline Easement 0.50 wide shown on the plan.

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: Hobart Ports Corporation Pty Ltd FOLIO REF: Volume 127078 Folio 1 and Volume 129061 Folios 1 and 2 SOLICITOR & REFERENCE: Page Seager Steven J Latham	PLAN SEALED BY: Hobart City Council DATE: 20 April 2000 773.2 REF NO. <div style="text-align: right;">  Council Delegate SURVEYING SERVICES MANAGER </div>
NOTE: The Council Delegate must sign the Certificate for the purposes of identification.	





**SCHEDULE OF EASEMENTS**

REGISTERED

Issued Pursuant to the Land Titles Act 1980



ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 4 OF 5 PAGE/S	Registered Number SI 134036
SUBDIVIDER: Hobart Ports Corporation Pty Ltd FOLIO REFERENCE: Volume 127078 Folio 1 and Volume 129061 Folios 1 and 2	

Fencing Provision:

In respect of the lots on the plan the Vendor (Hobart Ports Corporation Pty Ltd) shall not be required to fence.

Definitions:**Sewer Pump Station Easement means:**

The right for the owner for the time being of the dominant lot together with workers, surveyors and others to enter upon the land shown on the plan as Sewer Pump Station Easement and to construct, install and maintain pumps, pipes, valves, wires and fittings together with such ancillary items in connection therewith may be necessary for the passage of sewerage together with the right to repair, replace, amend or renew the same.

Cable Easement means:

The right for the owner for the time being of the dominant lot together with workers, surveyors and others to enter upon the land shown on the plan as Cable Easement and to lay, construct and maintain cables, wires and fittings together with such ancillary items in connection therewith as may be necessary for the conduct of electricity together with the right to repair, replace, amend or renew the same.

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.



SCHEDULE OF EASEMENTS

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<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 5 OF 5 PAGE/S</p>	<p>Registered Number</p> <p>SP 134036</p>
<p>SUBDIVIDER: Hobart Ports Corporation Pty Ltd FOLIO REFERENCE: Volume 127078 Folio 1 and Volume 129061 Folios 1 and 2</p>	

Electricity Infrastructure Easement means:

The right for the owner for the time being of the dominant lot together with workers, surveyors and others to enter upon the land shown on the plan as Electricity Infrastructure Easement and to install, construct and maintain wires, cables, apparatus and fittings together with such ancillary items in connection therewith as may be necessary together with the right to repair, replace, amend or renew the same.

Pipeline Easement means:

The right for the owner for the time being of the dominant lot together with workers, surveyors and others to enter upon the land shown on the plan as Pipeline Easement and to lay pipes, valves and fittings together with such ancillary items as may be necessary to enable the passage of workers together with the right to repair, replace, cleanse, amend or renew the same.

THE COMMON SEAL of HOBART PORTS
CORPORATION being the registered proprietor
of the land in Folios of the Register Volume 129061
Folios 1 and 2 and Volume 127078 Folio 1 is
hereunto affixed in the presence of:



By

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

the dealing.



RESULT OF SEARCH

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SEARCH DATE : 16-Dec-2021

SEARCH TIME : 12.39 PM

DESCRIPTION OF LAND

City of HOBART

Lot 4 on Plan 135056

Derivation : Whole of Lot 4 Gtd to the Hobart City Council

Derived from Grant No.358/26

SCHEDULE 1

HOBART CITY COUNCIL

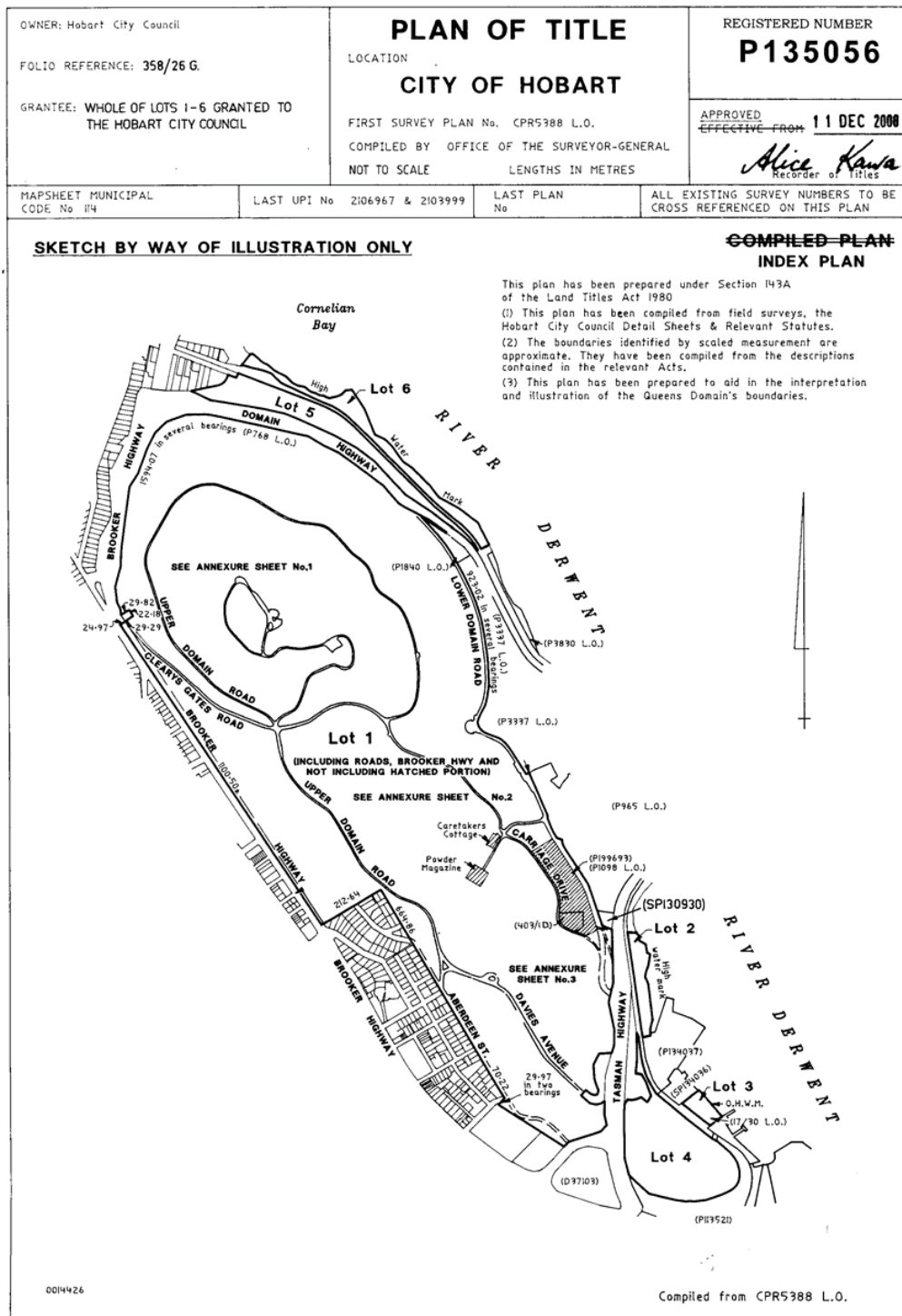
SCHEDULE 2

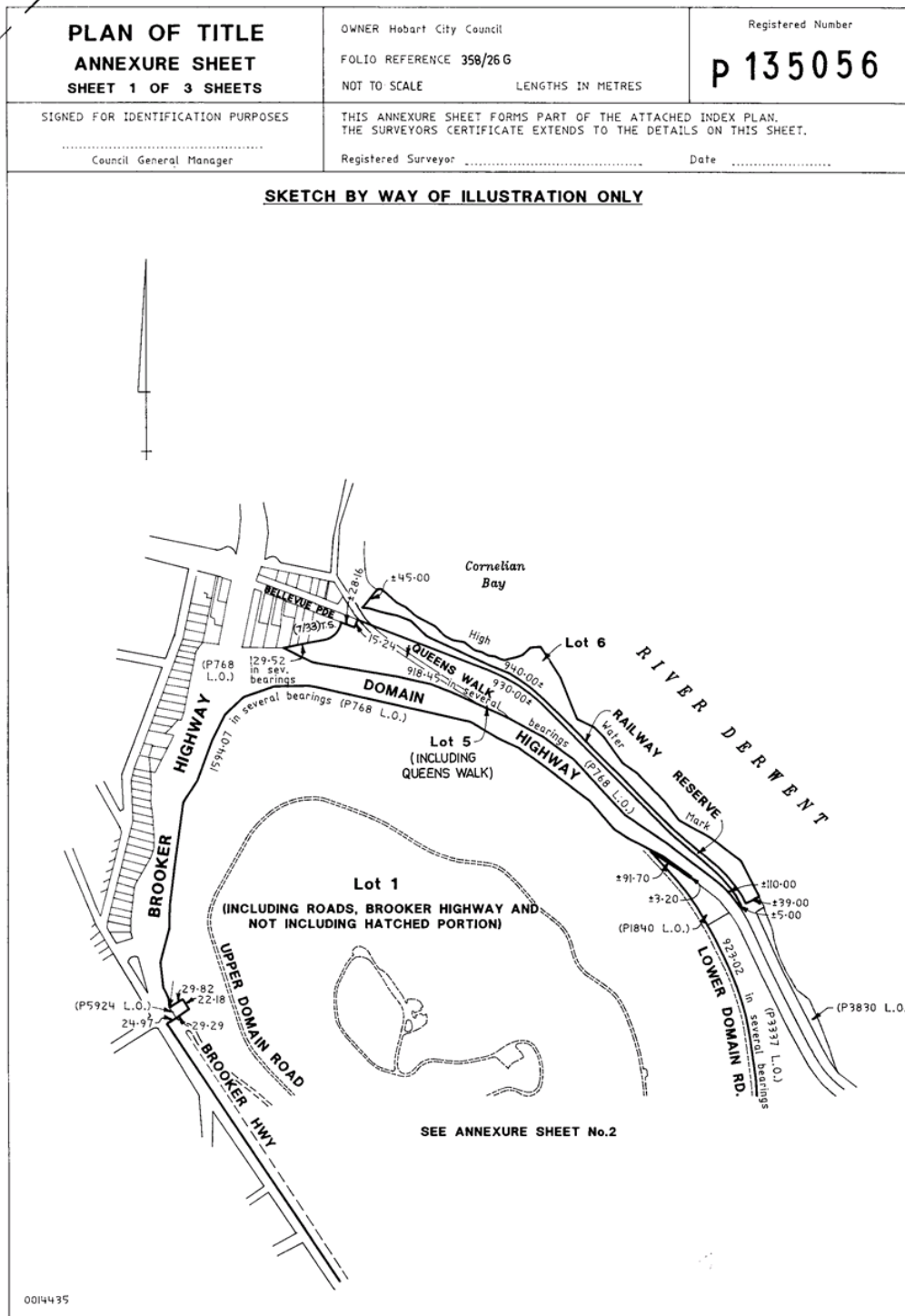
358/26 Land is limited in depth to 15 metres, excludes minerals and is subject to reservations relating to drains sewers and waterways in favour of the Crown

358/26 Grant made Subject To use (subject to the Hobart Corporation Act 1963) for the recreation, amusement, health & enjoyment of the inhabitants of the city and other persons resident in or coming into the city

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations







FOLIO PLAN

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<p>PLAN OF TITLE ANNEXURE SHEET SHEET 2 OF 3 SHEETS</p>	<p>OWNER Hobart City Council FOLIO REFERENCE 358/266 NOT TO SCALE LENGTHS IN METRES</p>	<p>Registered Number P135056</p>
<p>SIGNED FOR IDENTIFICATION PURPOSES _____ Council General Manager</p>	<p>THIS ANNEXURE SHEET FORMS PART OF THE ATTACHED INDEX PLAN. THE SURVEYORS CERTIFICATE EXTENDS TO THE DETAILS ON THIS SHEET. Registered Surveyor _____ Date _____</p>	

SKETCH BY WAY OF ILLUSTRATION ONLY

SEE ANNEXURE SHEET No.1

UPPER DOMAIN ROAD

CLEARY'S GATES BROOK

Lot 1
 (INCLUDING ROADS, BROOKER HWY AND
 NOT INCLUDING HATCHED PORTIONS)

UPPER DOMAIN ROAD

400.50

HIGHWAY

212-64

161-86

SHOEBRIDGE

LOWER DOMAIN ROAD

(P3337 L.O.)
 923.02 in
 several bearings

(P3337 L.O.)

(P965 L.O.)

Caretakers Cottage

Powder Magazine

CARRIAGE DRIVE

474.70 in several
 bearings (P965 L.O.)

RIGHT OF WAY
 6.00 WIDE
 (SP130930)

(4030/1)

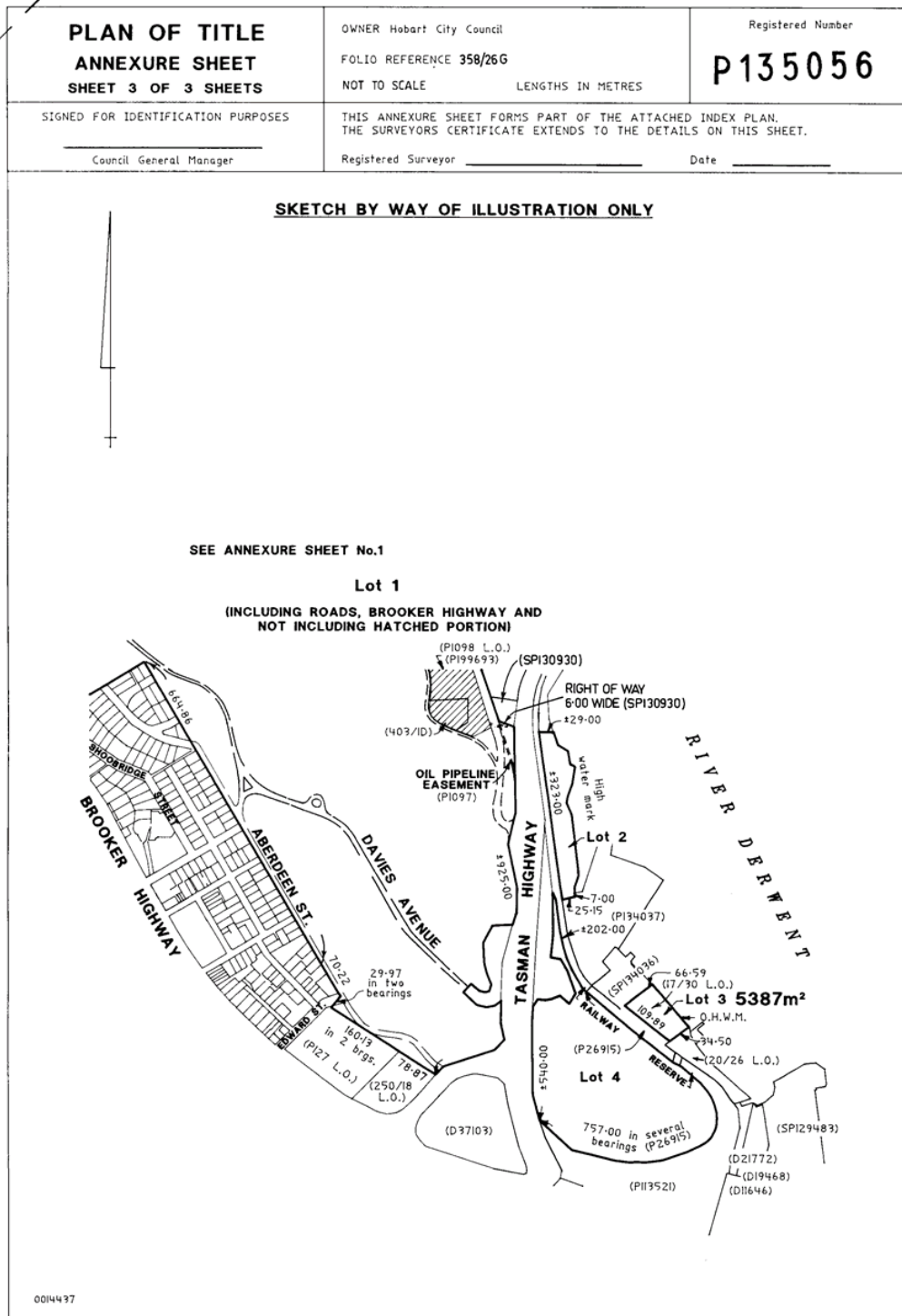
(SP130930)

31-08

OIL PIPELINE
 EASEMENT
 (PI097)

SEE ANNEXURE SHEET No.3

0014496





RESULT OF SEARCH

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SEARCH DATE : 22-Mar-2022

SEARCH TIME : 02.40 PM

DESCRIPTION OF LAND

City of HOBART
Lot 4 on Sealed Plan 179192
Derivation : Part of Lot 1, 9.004ha vested in Australian
National Railways Commission
Prior CTs 176538/3 and 176538/4

SCHEDULE 1

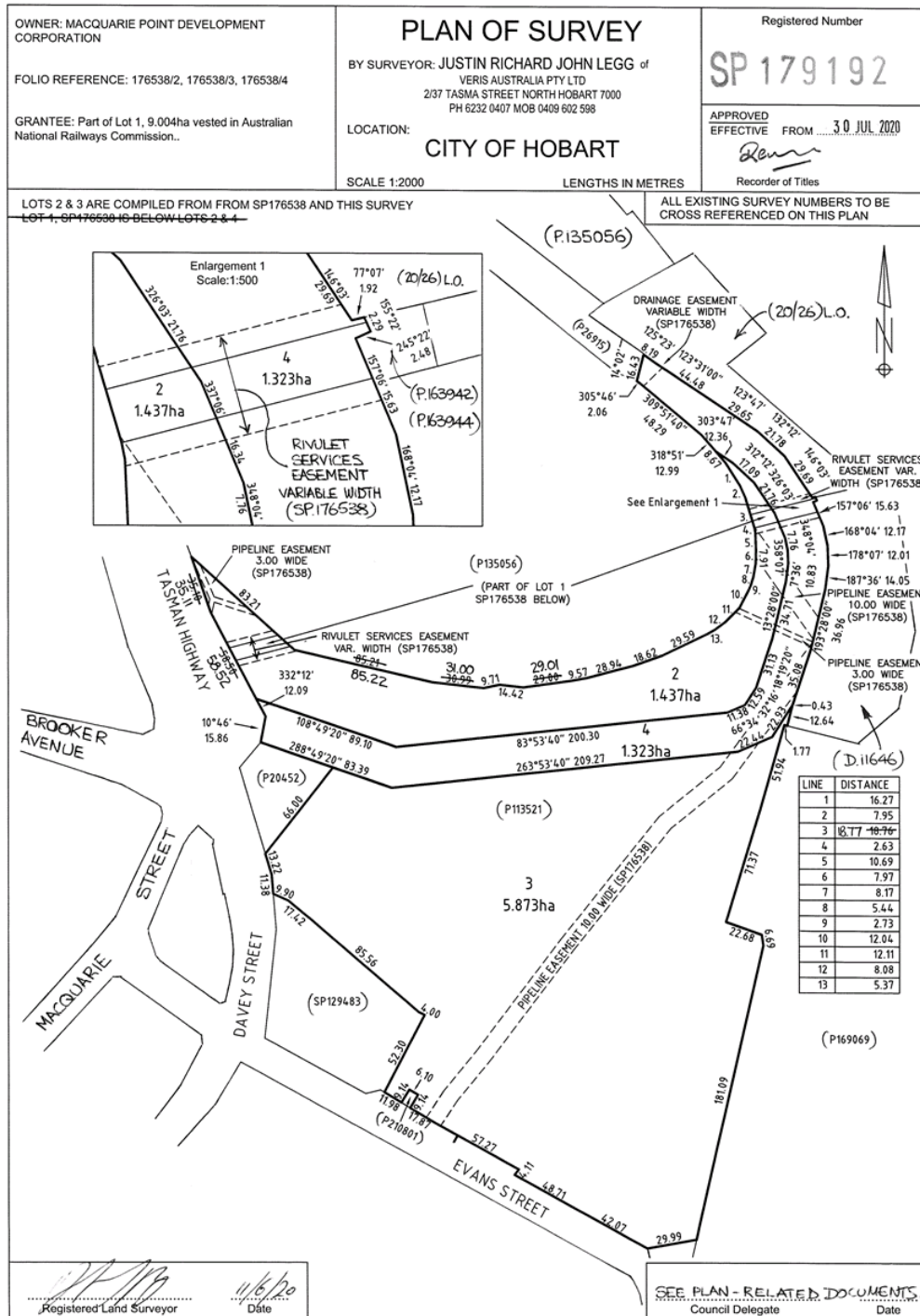
E109578 MACQUARIE POINT DEVELOPMENT CORPORATION Registered
13-Mar-2020 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
E109578 SUBJECT to exceptions, reservations, limitations and
conditions more fully set forth therein
SP179192 EASEMENTS in Schedule of Easements
SP179192 WATER SUPPLY RESTRICTION
SP179192 SEWERAGE AND/OR DRAINAGE RESTRICTION
SP176538 WATER SUPPLY RESTRICTION
SP176538 SEWERAGE AND/OR DRAINAGE RESTRICTION
E229541 INSTRUMENT Creating Covenants in favour of the Crown
Registered 02-Mar-2021 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations





SCHEDULE OF EASEMENTS

R□□□RD□R□□□□□□□□

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<p>SCHEDULE OF EASEMENTS</p> <p>NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.</p>	<p>Registered Number</p> <p>SP 179192</p>
---	---

PAGE 1 OF 7 PAGE/S

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

LOT 2 on the Plan is subject to:

- (a) a Rivulet Services Easement (appurtenant to the land comprised in folio of the Register Volume 176538 Folio 1) over such part of Lot 2 that is shown marked as "Rivulet Services Easement Var. Width (SP176538)" on the Plan;
- (b) a Pipeline Easement (~~hereinafter defined~~) for the benefit of Tasmanian Water and Sewerage Corporation Pty Ltd (ACN 162 220 653) over the land marked "Pipeline Easement 10.00 Wide (SP176538)" passing through Lot 2 on the Plan; and
- (c) a Pipeline Easement for the benefit of Tasmanian Water and Sewerage Corporation Pty Ltd (ACN 162 220 653) over each of the two instances of land marked "Pipeline Easement 3.00 Wide (SP176538)" passing through Lot 2 on the Plan.

LOT 3 on the Plan is subject to a Pipeline Easement for the benefit of Tasmanian Water and Sewerage Corporation Pty Ltd (ACN 162 220 653) over the land marked "Pipeline Easement 10.00 Wide" passing through Lot 3 on the Plan.

(SP176538)

LOT 4 on the Plan is subject to:

- (a) a Rivulet Services Easement (appurtenant to the land comprised in folio of the Register Volume 176538 Folio 1) over such part of Lot 4 that is shown marked as “Rivulet Services Easement Var. Width (SP176538)” on the Plan;
- (b) a Pipeline Easement for the benefit of Tasmanian Water and Sewerage Corporation Pty Ltd (ACN 162 220 653) over the land marked “Pipeline Easement 10.00 Wide” and “Pipeline Easement 10.00 Wide (SP176538)” passing through Lot 4 on the Plan; (SP176538)

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: Macquarie Point Development Corporation FOLIO REF: 176538/2, 176538/3, 176534/4 SOLICITOR & REFERENCE: Office of the Crown Solicitor	PLAN SEALED BY: Hobart City Council DATE: 12-5-2020 PLN-19-925 & STR-20-23 REF NO. <i>M. Anderson</i> Council Delegate
NOTE: The Council Delegate must sign the Certificate for the purposes of identification.	



SCHEDULE OF EASEMENTS

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ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 2 OF 7 PAGES	Registered Number SP 179192
SUBDIVIDER: Macquarie Point Development Corporation FOLIO REFERENCE: 176538/2, 176538/3, 176538/4	

- (c) a Pipeline Easement for the benefit of Tasmanian Water and Sewerage Corporation Pty Ltd (ACN 162 220 653) over the land marked "Pipeline Easement 3.00 Wide (SP176538)" passing through Lot 4 on the Plan; and
- (d) a Drainage Right Easement (~~hereinafter defined~~) for the benefit of the Hobart City Council over the land marked "Drainage Easement Variable Width" passing through Lot 4 on the Plan.
(SP176538)

Interpretation:

"Rivulet Services Easement" means the full right and liberty for Authorised Persons to, in respect of the Easement Land, enter and remain upon the Easement Land with or without machinery, vehicles, plant and equipment for the purpose of inspecting, repairing and/or replacing as reasonably necessary the Hobart Rivulet tunnel (including any infrastructure and equipment) forming part of folio of the Register Volume 176538 Folio 1 in the case of any collapse or non-trivial damage thereto provided always that:

- (a) in exercising such rights and liberties, Authorised Persons must:
- (i) act as expeditiously as possible;
 - (ii) do as little damage to the Easement Land (including for the avoidance of doubt using all reasonable endeavours to not enter onto any buildings nor impact or adversely affect any buildings, structures and improvements (including transit infrastructure) on or under the Easement Land) as is reasonably practicable in the relevant circumstances;
 - (iii) make good any damage caused to the Easement Land to the extent that it is reasonably practicable to do so;
 - (iv) give such prior notice as is reasonable in the circumstances before seeking to enter onto the Easement Land or to exercise of the rights conferred by this Rivulet Services Easement; and
 - (v) if any part of the Easement Land includes any rail infrastructure (within the meaning of the *Rail Safety National Law (Tasmania)*) and/or any other transit infrastructure, comply with all reasonable notices and directions of the Registered Proprietor of the Easement Land relating to or affecting the rail infrastructure and/or any other transit infrastructure (including so as not to unreasonably interfere with the operation of that infrastructure and so as not to create any danger to health and safety of any person in respect thereof), and
- (b) the registered proprietors of the Easement Land ("the Owner"):
- (i) must not (except as provided for in subclause (ii)) construct, install, erect or carry out major alterations to, any building, structure or other improvement on the Easement Land without the written consent of the registered proprietor of folio of the Register Volume 176538 Folio 1 first had and obtained and only in compliance with any conditions which form such consent provided however:
 - (A) any building structure or other improvement on the Easement Land prior to the creation of this Rivulet Services Easement may remain *in situ* and will not be a breach of the requirements of subparagraph (b)(i);

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal or that body to the dealing.



SCHEDULE OF EASEMENTS

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ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 3 OF 7 PAGES	Registered Number SP 179192
SUBDIVIDER: Macquarie Point Development Corporation FOLIO REFERENCE: 176538/2, 176538/3, 176538/4	

- (B) the written consent of the registered proprietor of folio of the Register Volume 176538 Folio 1 is not to be withheld in respect of any building structure or other improvement being constructed, installed, erected or majorly altered after this Rivulet Services Easement takes effect where the Owner is able to demonstrate to the satisfaction of the registered proprietor of folio of the Register Volume 176538 Folio 1 (acting reasonably) that such building or works will not (in a non-trivial manner) damage the Hobart Rivulet tunnel passing through the land comprising folio of the Register Volume 176538 Folio 1 or be reasonably likely to cause any collapse or other non-trivial damage to the Hobart Rivulet tunnel passing through the land comprising folio of the Register Volume 176538 Folio 1 as a result of its subsequent use or operation;
- (ii) may, in respect of the Lot 4 Area, construct, install, use, maintain, repair and replace land transportation infrastructure (including a roadway, bike track and/or rail line) on such Lot 4 Area without the need for any prior written consent from the registered proprietor of folio of the Register Volume 176538 Folio 1 where such works (and their use) will not (in a non-trivial manner) directly damage the Hobart Rivulet tunnel passing through the land comprising folio of the Register Volume 176538 Folio 1 or be reasonably likely to cause any collapse or other non-trivial damage to the Hobart Rivulet tunnel passing through the land comprising folio of the Register Volume 176538 Folio 1 as a result of its subsequent use or operation; and
- (iii) must not to do anything else (other than that which may be allowed by subclauses (i) and (ii)) on the Easement Land that may (in a non-trivial manner) damage the Hobart Rivulet tunnel passing through the land comprising folio of the Register Volume 176538 Folio 1, and
- (c) neither the Owner nor the registered proprietor of folio of the Register Volume 176538 Folio 1 is required to fence any part of the Easement Land.

For the purposes of this definition of Rivulet Service Easement:

Authorised Persons means the registered proprietor of folio of the Register Volume 176538 Folio 1 and where the context permits and requires, the employees, agents and contractors of the registered proprietor of folio of the Register Volume 176538 Folio 1.

Easement Land means such parts of Lot 2 and Lot 4 on the Plan that are shown marked as "Rivulet Services Easement Variable Width" on the Plan.

Lot 4 Area means all of the Easement Land area existing over Lot 4 on the Plan.

“Pipeline Easement” means the full right and liberty for Tasmanian Water and Sewerage Corporation Pty Ltd (ACN 162 220 653) (“TasWater”) (and its successors) to, at all times in respect of that land over which a Pipeline Easement is expressed to exist (jointly and where the context requires severally, the “Easement Land”);

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Issued Pursuant to the Land Titles Act 1980



SCHEDULE OF EASEMENTS

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<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 5 OF 7 PAGES</p>	<p>Registered Number</p> <p>SP 179192</p>
<p>SUBDIVIDER: Macquarie Point Development Corporation</p> <p>FOLIO REFERENCE: 176538/2, 176538/3, 176538/4</p>	

- (d) comply with all reasonable notices and directions of the relevant registered proprietor of the Easement Land ("the Owner") concerning safety and any other relevant matter so as not to adversely interfere with the operation of the land surrounding (and including) the Easement Land (including any transportation based infrastructure) or create any danger to health and safety in respect thereof; and
 - (e) should any part of the Easement Land also be subject to another easement, act co-operatively and in good faith with any entity having the benefit of such other easement rights and to use all reasonable endeavours to ensure that neither party is adversely affected by the other exercising their easement rights over such Easement Land;
- (9) The registered proprietors of the Easement Land ("the Owner") must not, without the written consent of TasWater first had and obtained (which consent is not to be unreasonably withheld) and only in compliance with any conditions which form the consent:
 - (a) (subject to clause 9(b)) alter, excavate, plough, drill or otherwise penetrate the ground level of the Easement Land in a manner that could damage or interfere (in a non-trivial manner) with the condition or other proper operation of the Improvements;
 - (b) install or erect any building or structure on or in the Easement Land provided however such restriction does not extend to surfacing works, roads and pathways works, installation of signage, landscaping works, fencing works and car parking works, and in respect of such part of the servient land that exists on Lot 4, any remediation works and other construction or installation works of a transportation nature, all of which works may be undertaken by the Owner where doing so will not damage or contribute to damage (in a non-trivial manner) to any of the Infrastructure in the Easement Land;
 - (c) remove any thing that supports, protects or covers any Infrastructure on or in the Easement Land;
 - (d) (subject to clause 9(b)) do any thing which will or might damage or contribute to damage (in a non-trivial manner) to any of the Infrastructure in the Easement Land;
 - (e) (subject to all other terms herein) prevent or interfere with the proper exercise and benefit of the Easement Land as allowed for by the terms of this Pipeline Easement by TasWater (or its employees, contractors, agents and all other persons duly authorised by it); or
 - (f) permit or allow any action which the Owner must not do or acquiesce in that action, it being acknowledged that any building, structure, fence, pit, well, footing, pipeline, paving, tree, shrub or other object on or in the Easement Land prior to the creation of such Pipeline Easement may remain *in situ* and will not be a breach of the requirements of this subclause (9) or allow any action to be taken under of subclause (14).
- (10) Neither the Owner or TasWater is required to fence any part of the Easement Land.
- (11) The Owner may erect a fence across the Easement Land at its boundaries.

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SCHEDULE OF EASEMENTS

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<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 6 OF 7 PAGES</p>	<p>Registered Number</p> <p>SP 179192</p>
<p>SUBDIVIDER: Macquarie Point Development Corporation</p> <p>FOLIO REFERENCE: 176538/2, 176538/3, 176538/4</p>	

- (12) The Owner may erect a gate across any part of the Easement Land subject to these conditions:

 - (a) the Owner must provide TasWater with a key to any lock which would prevent the opening of the gate (with TasWater to close and lock any such gate after use if TasWater has opened and unlocked such gate); and
 - (b) if the Owner does not provide TasWater with that key or the key provided does not fit the lock, TasWater may cut the lock from the gate (provided that TasWater must firstly attempt to notify the Owner of such action and again notify the Owner as soon as possible after cutting the lock).
- (13) If the Owner causes damage to any of the Infrastructure in breach of its obligations herein contained or otherwise as a result of any wrongful (including negligent) act or omission by the Owner, the Owner is liable for the actual cost reasonably incurred by TasWater in the repair of the Infrastructure damaged.
- (14) If the Owner fails to comply with of its obligations under subclause (9), without forfeiting any right of action, damages or otherwise against the Owner, TasWater may:

 - (a) reinstate the ground level of the Easement Land; or
 - (b) remove from the Easement Land any building, structure or other object in place in breach of the requirements of subclause (9); or
 - (c) replace any thing that supported, protected or covered the Infrastructure as relevant.

Interpretation (for the purposes of this definition of “Pipeline Easement”):

"Infrastructure" means such pipes and ancillary equipment and infrastructure owned or for which TasWater is responsible existing in the relevant Easement Land as at 12 April 2019 associated with the passing of water and sewerage and includes but is not limited to:

- (a) any thing reasonably required to support, protect or cover any of the Infrastructure; and
- (b) where the context permits, any part of the Infrastructure.

“Drainage Right Easement” means a right of drainage as defined in Schedule 8 of the *Conveyancing and Law of Property Act 1884* (Tas) qualified by it being agreed that:

- (a) (for the avoidance of doubt) the owner of the servient land may construct, install, use, maintain, repair and replace land transportation infrastructure (including a roadway, bike track and/or rail line) on such area affected by the Drainage Right Easement without the need for any prior written consent from the Hobart City Council where such works (and their use) will not (in a non-trivial manner) directly damage any existing drainage infrastructure; and

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SCHEDULE OF EASEMENTS

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ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 7 OF 7 PAGES	Registered Number SP 179192
SUBDIVIDER: Macquarie Point Development Corporation FOLIO REFERENCE: 176538/2, 176538/3, 176538/4	

- (b) the Hobart City Council must make good any damage caused to the Easement Land (including any infrastructure thereon of the type referred to in subclause (a)) as a result of exercising any of these easement rights to the extent that it is reasonably practicable to do so.

The seal of the Macquarie Point Development Corporation was hereunto
affixed in accordance with an authorisation
given by its Board in the presence of:



Greg Cooper
Signature of witness

Greg Cooper
Name of witness (block letters)

41 Evans St Hobart Tas 7000
Address of witness

Public Servant
Occupation

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

**RESULT OF SEARCH**

REGISTERED INFORMATION

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REGISTERED INFORMATION

LOT 2	2
2	DEVELOPMENT RIGHTS

SEARCH DATE : 22-Mar-2022

SEARCH TIME : 02.40 PM

DESCRIPTION OF LAND

City of HOBART

Lot 2 on Plan 26915

Derivation : Whole of 271m2 (Lot 2) Vested in The Australian
National Railways Commission

Prior CT 4385/2

SCHEDULE 1

C139362 TRANSFER to THE CROWN Registered 16-Oct-1998 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

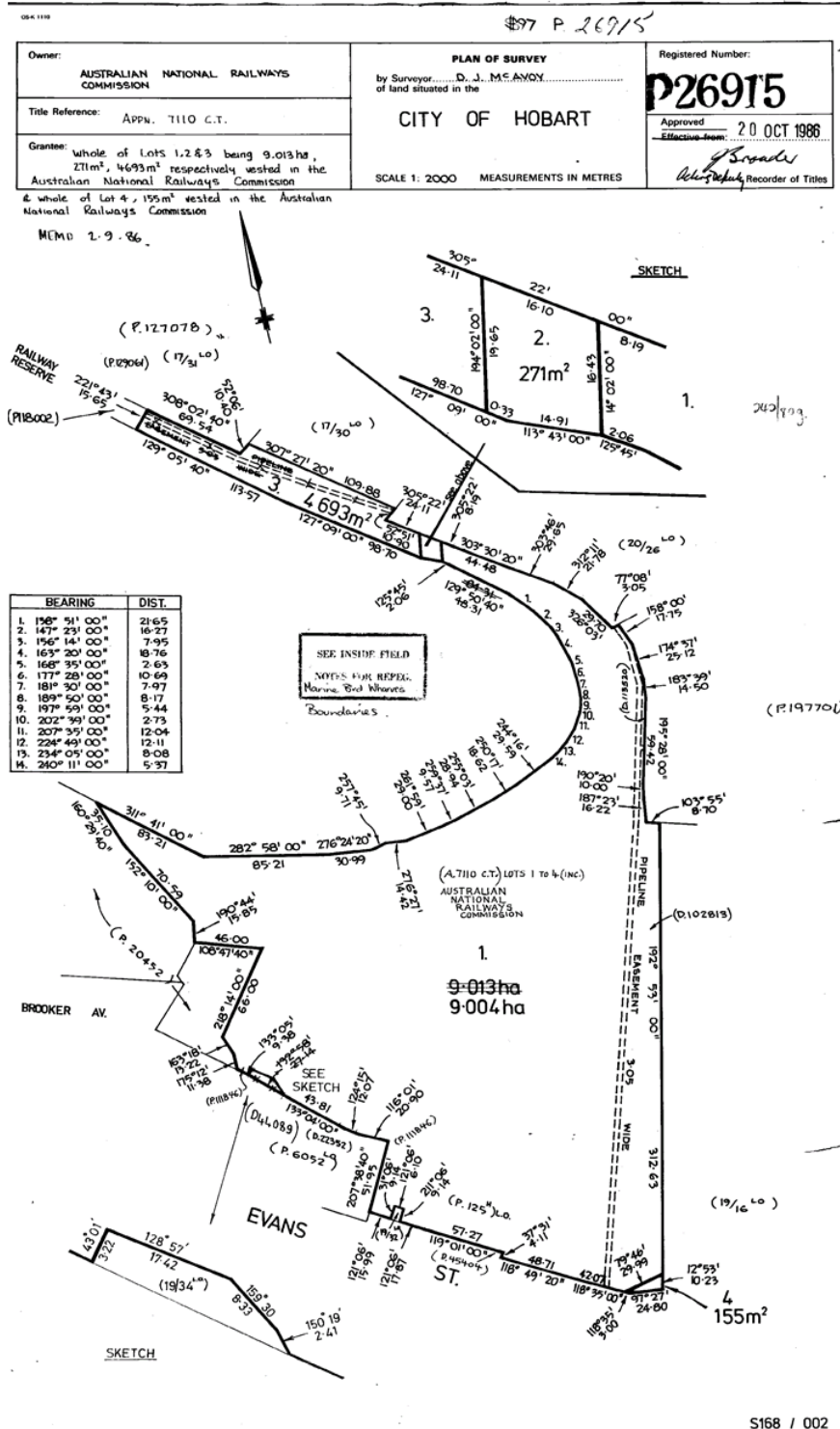
www.thelist.tas.gov.au



FOLIO PLAN

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2109

SULLIVANS COVE DISTILLERY

DEVELOPMENT APPROVAL APPLICATION

DA - DRAWING LIST

Sheet No.	Sheet Name	Rev.	Date
DA0000	Cover Sheet	3	27.09.2021
DA0001	Locality Plan - Existing	4	27.09.2021
DA0011	Site Plan - Demolition	5	27.09.2021
DA0012	Site Plan - Proposed	5	27.09.2021
DA0051	Demolition - Ground Floor Plan	5	27.09.2021
DA0052	Demolition - First Floor Plan	5	27.09.2021
DA0053	Demolition - Second Floor Plan	5	27.09.2021
DA0054	Demolition - Roof Plan	5	27.09.2021
DA0055	Demolition - Elevations	5	27.09.2021
DA0056	Demolition - Elevations	5	27.09.2021
DA0057	Demolition - Sections	5	27.09.2021
DA0101	Proposed - Ground Floor Plan	6	27.09.2021
DA0102	Proposed - First Floor Plan	6	27.09.2021
DA0103	Proposed - Second Floor Plan	6	27.09.2021
DA0104	Proposed - Roof Plan	5	27.09.2021
DA0201	Proposed - Drill Hall Elevations	5	27.09.2021
DA0202	Proposed - Production Hall Elevations	5	27.09.2021
DA0203	Proposed - Cross Elevations	5	27.09.2021
DA0301	Proposed - Drill Hall Sections	5	27.09.2021
DA0302	Proposed - Cross Sections	5	27.09.2021
DA0501	Artist Impression - Exterior View 01, Site Arrival	3	27.09.2021
DA0502	Artist Impression - Exterior View 02, Main Entry	3	27.09.2021
DA0503	Artist Impression - Interior View 03, Drill Hall facing West	3	27.09.2021
DA0504	Artist Impression - Interior View 04, Drill Hall facing East	3	27.09.2021
DA0505	Artist Impression - Interior View 05, Drill Hall facing South	3	27.09.2021
DA0601	Visual Impact Study - View from Cycle Path	4	27.09.2021
DA0602	Visual Impact Study - View from Rail Corridor	4	27.09.2021
DA0603	Visual Impact Study - View from Eastern Entry Road	3	27.09.2021
DA0604	Visual Impact Study - View from River Derwent	3	27.09.2021
DA0605	Visual Impact Study - View from Cenotaph	1	27.09.2021
DA0701	Proposed Palette of Materials and Finishes	2	27.09.2021
DA0801	Shadow Studies	4	27.09.2021



THIS DRAWING IS AN UNCONTROLLED COPY UNLESS STAMPED OTHERWISE

General Notes

Rev.	Issue	Revision Description	Date	Appr by
1	All	Issue for Consultant Input (80% SD)	27.08.2021	BH
2	All	Issue for 95 % Schematic Design (DA)	10.09.2021	BH
3	All	Issue for Development Approval	27.09.2021	BH



Level 2, 25 Rokeby Street, Collingwood, Vic 3066 Australia
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© 2019 John Wardle Architects ABN 83 006 814 261

Client	Sullivans Cove Distillery 1/10 Lamb Place, Cambridge TAS 7110
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Project **Sullivans Cove Distillery**
17 McVilly Drive, Queens Domain, TAS 7000

Title	Cover Sheet
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Project No.	2109
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Date _____

Oran	BH
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DEVELOPMENT APPLICATION

DA0000	Revision
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Scale ③ A1

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Check	DB
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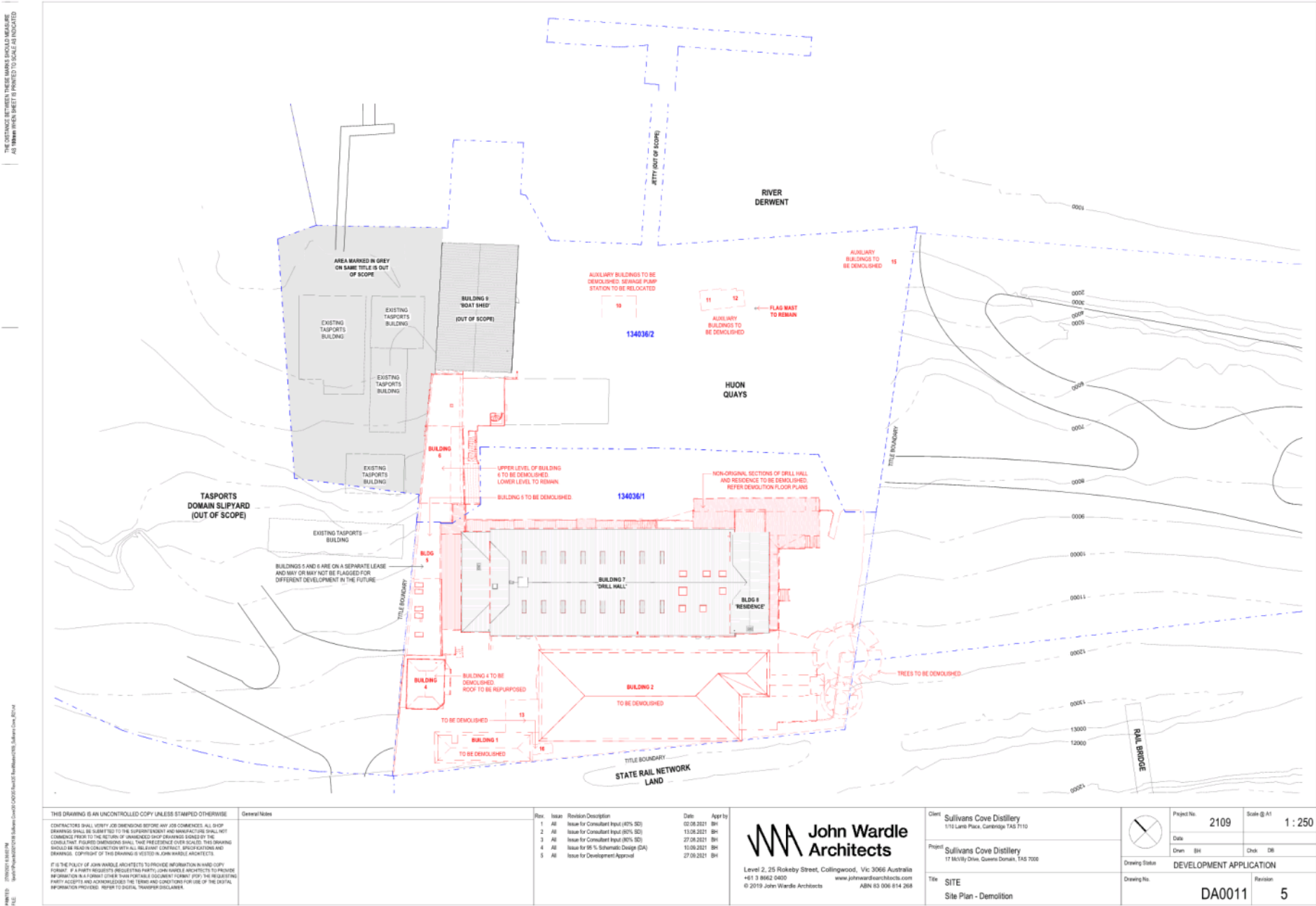
APPLICATION

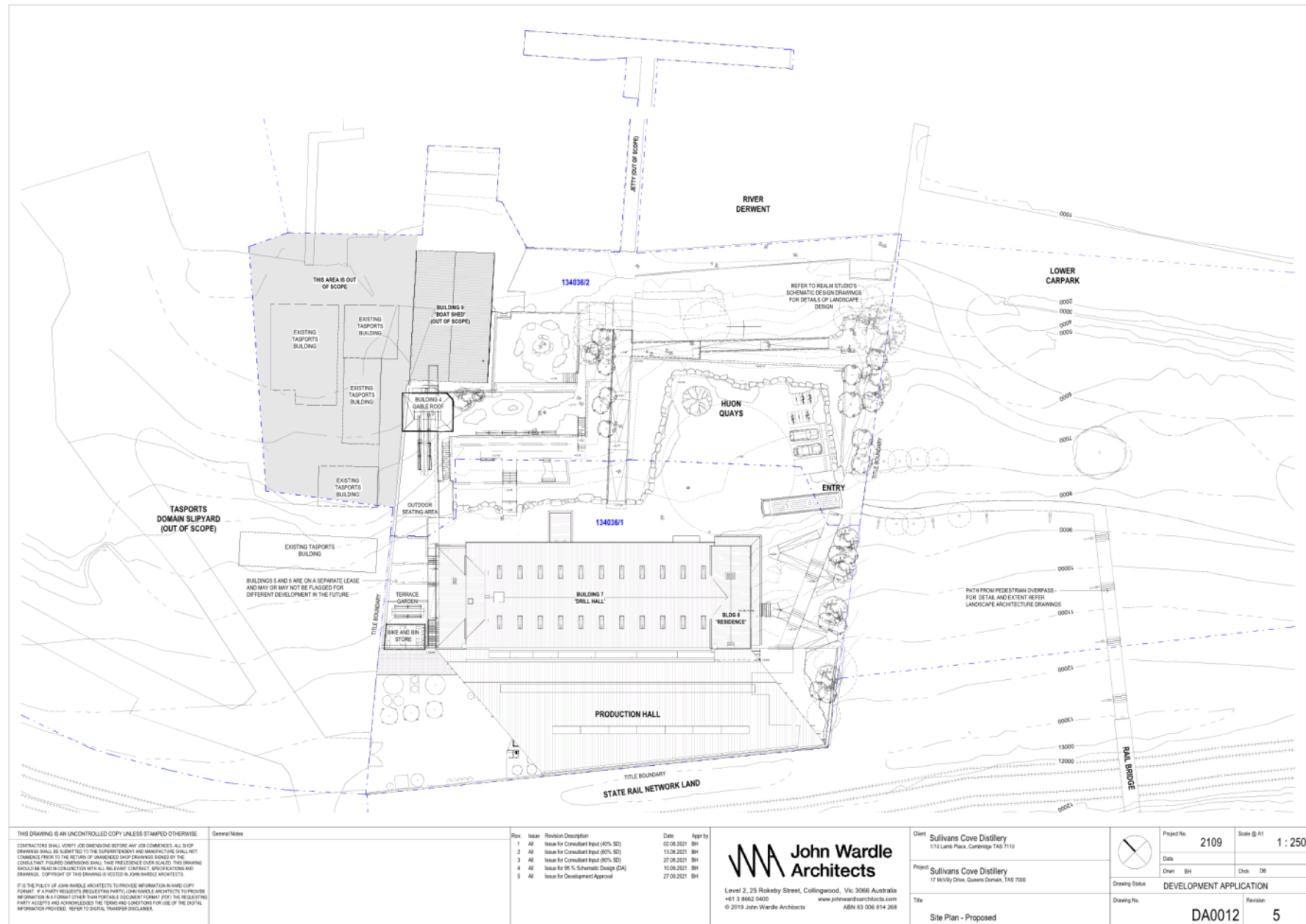
00	Revision
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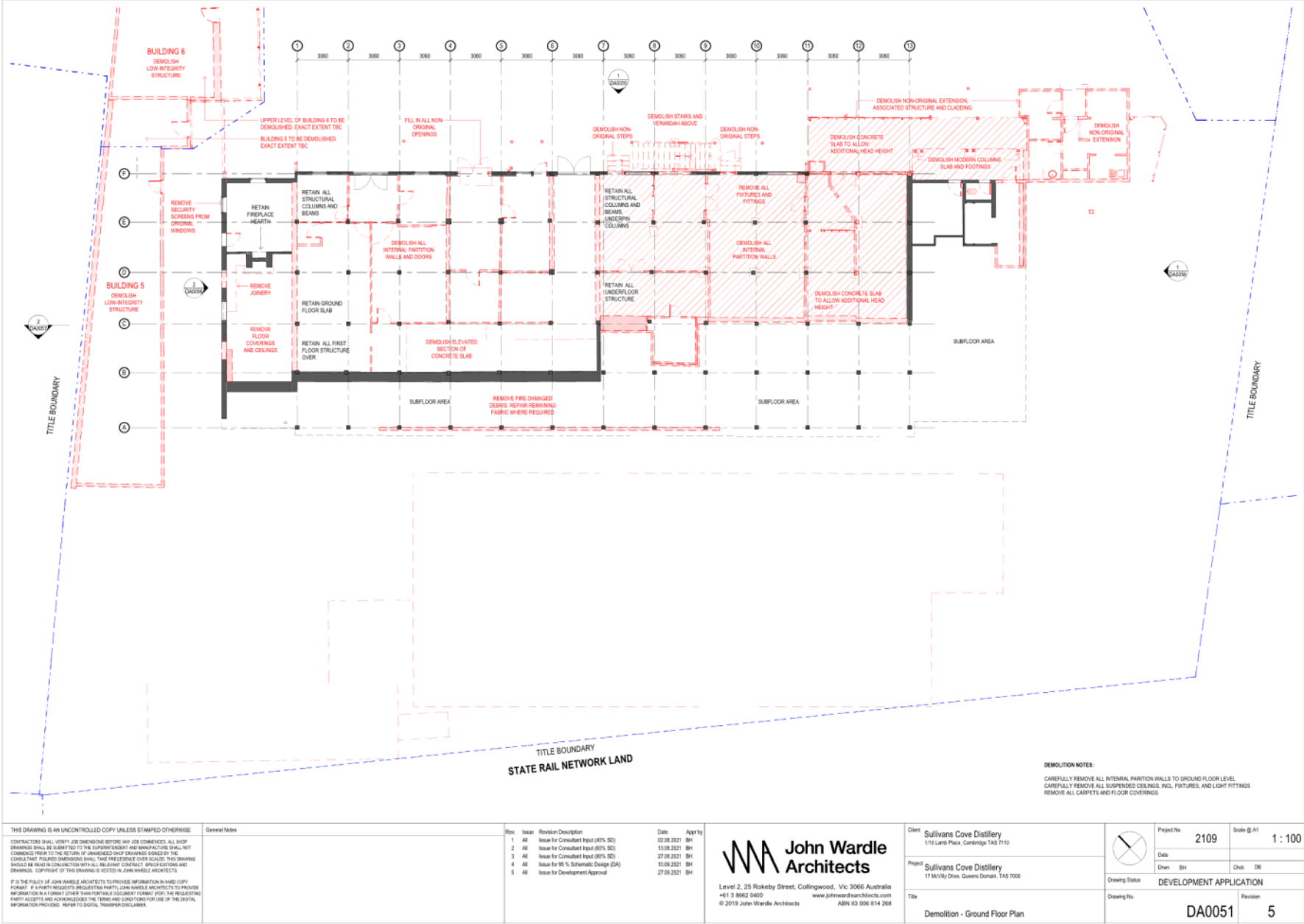
THE DISTANCE BETWEEN THESE MARKS SHOULD MEASURE AS 100mm WHEN SHEET IS PRINTED TO SCALE AS INDICATED

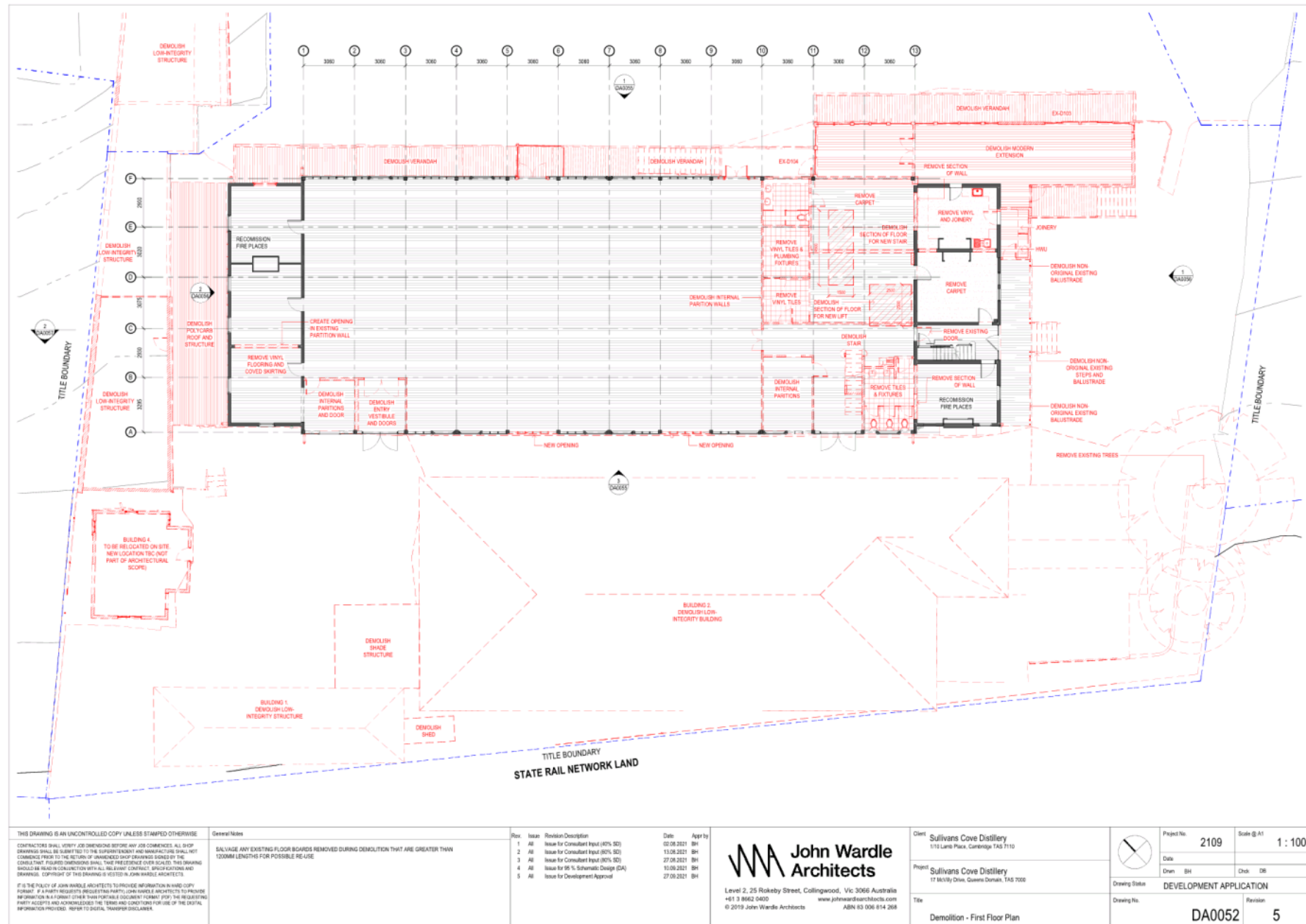
ACCEPTED 27/09/2023 6:10:09 PM
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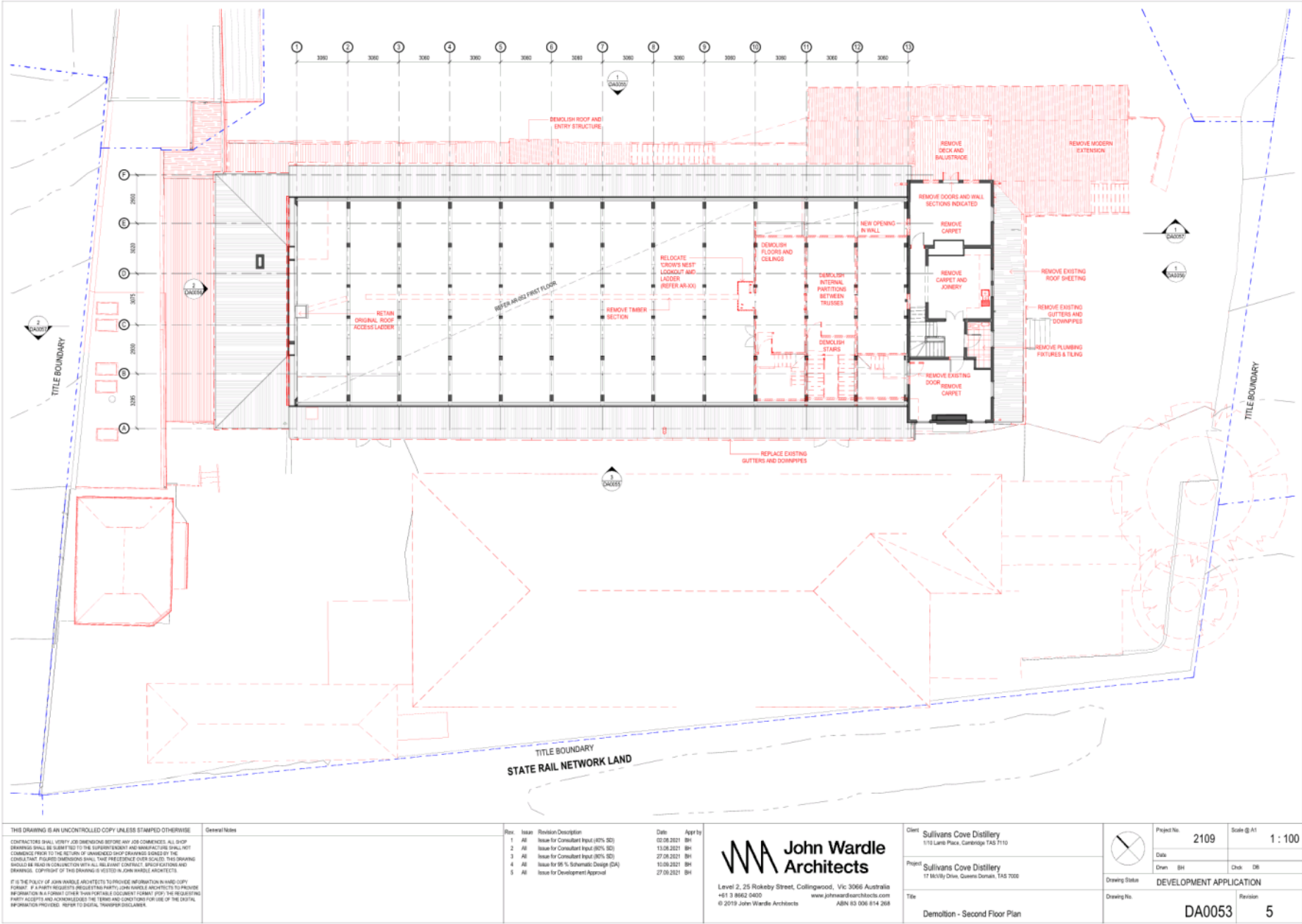


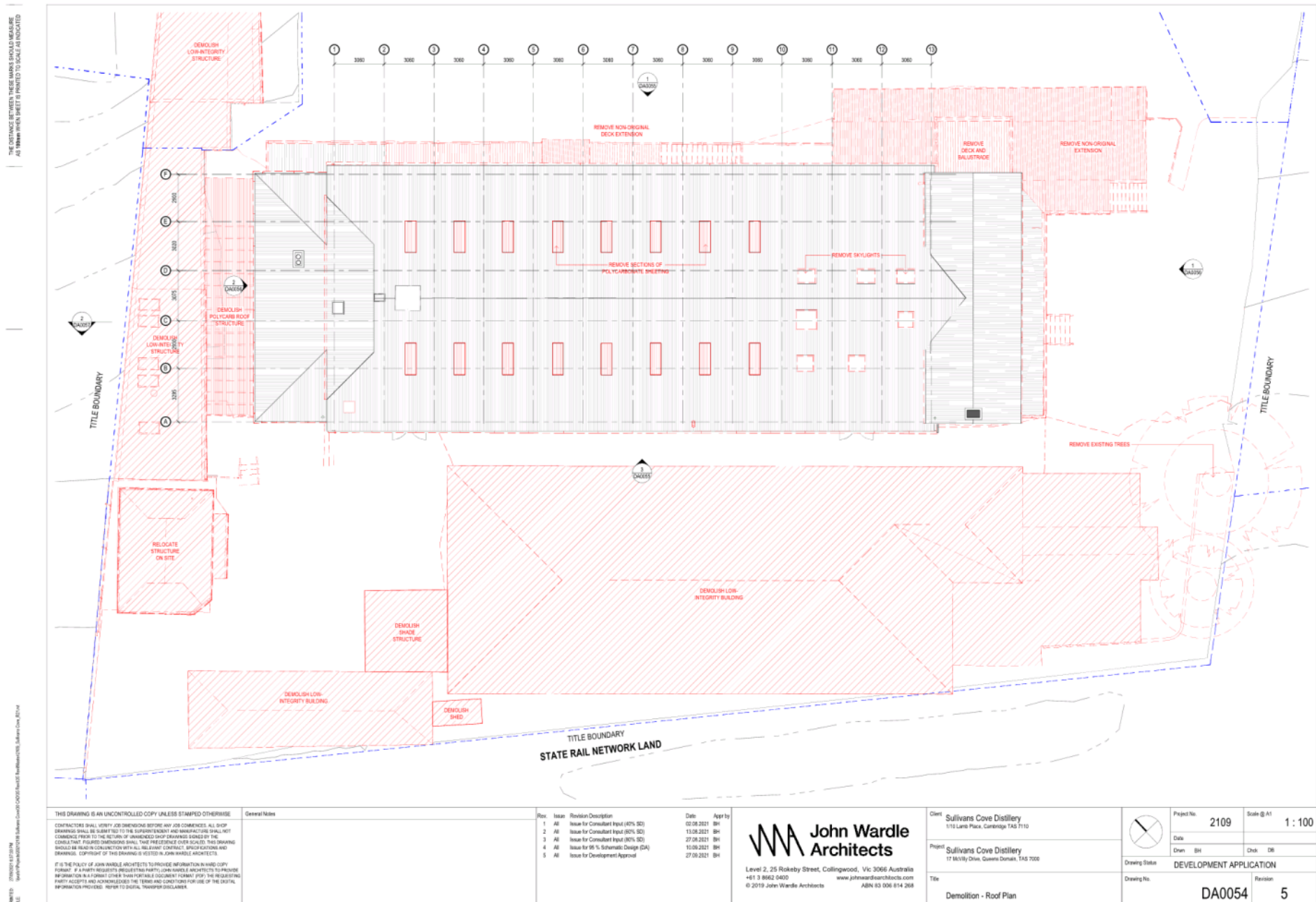


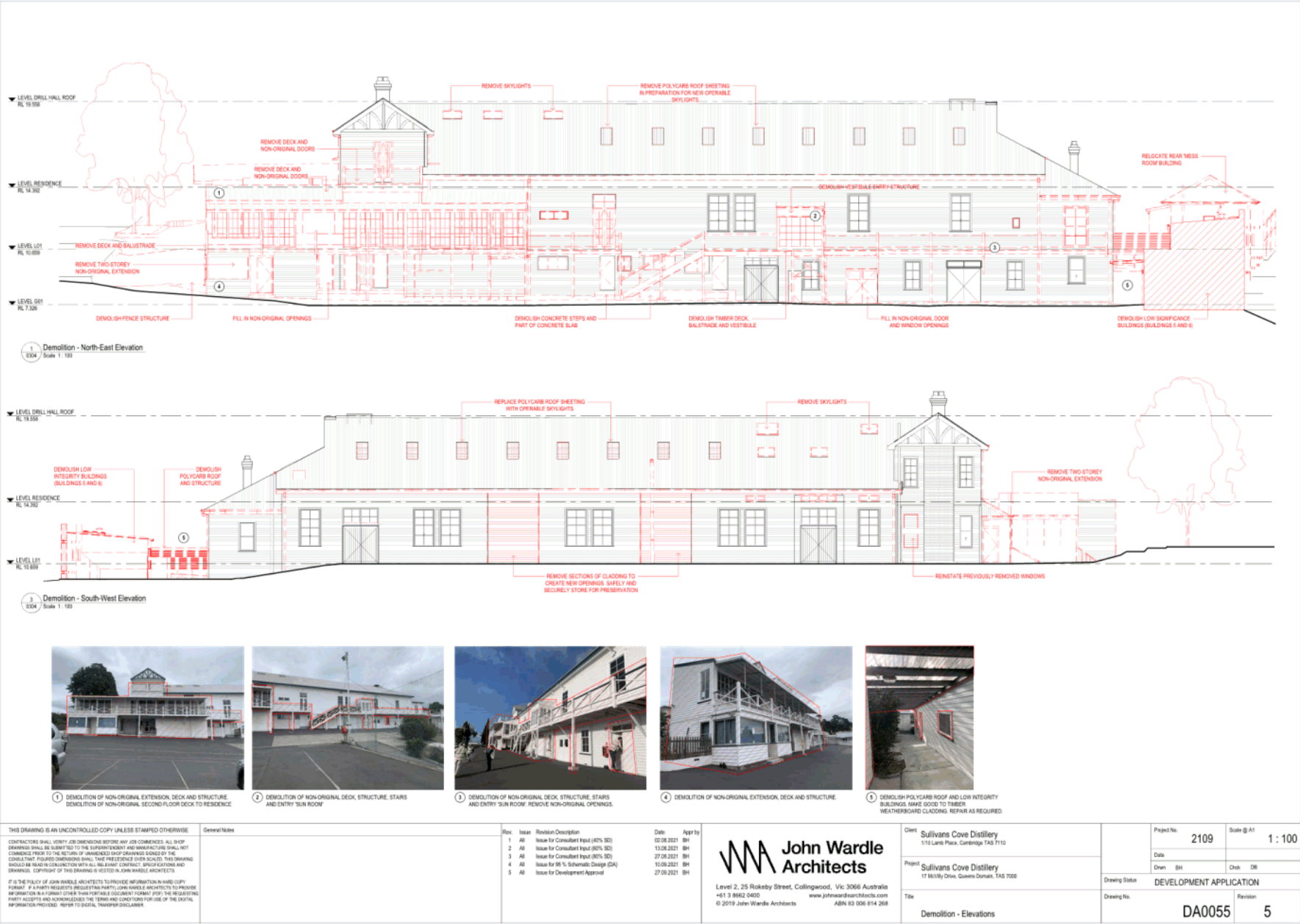


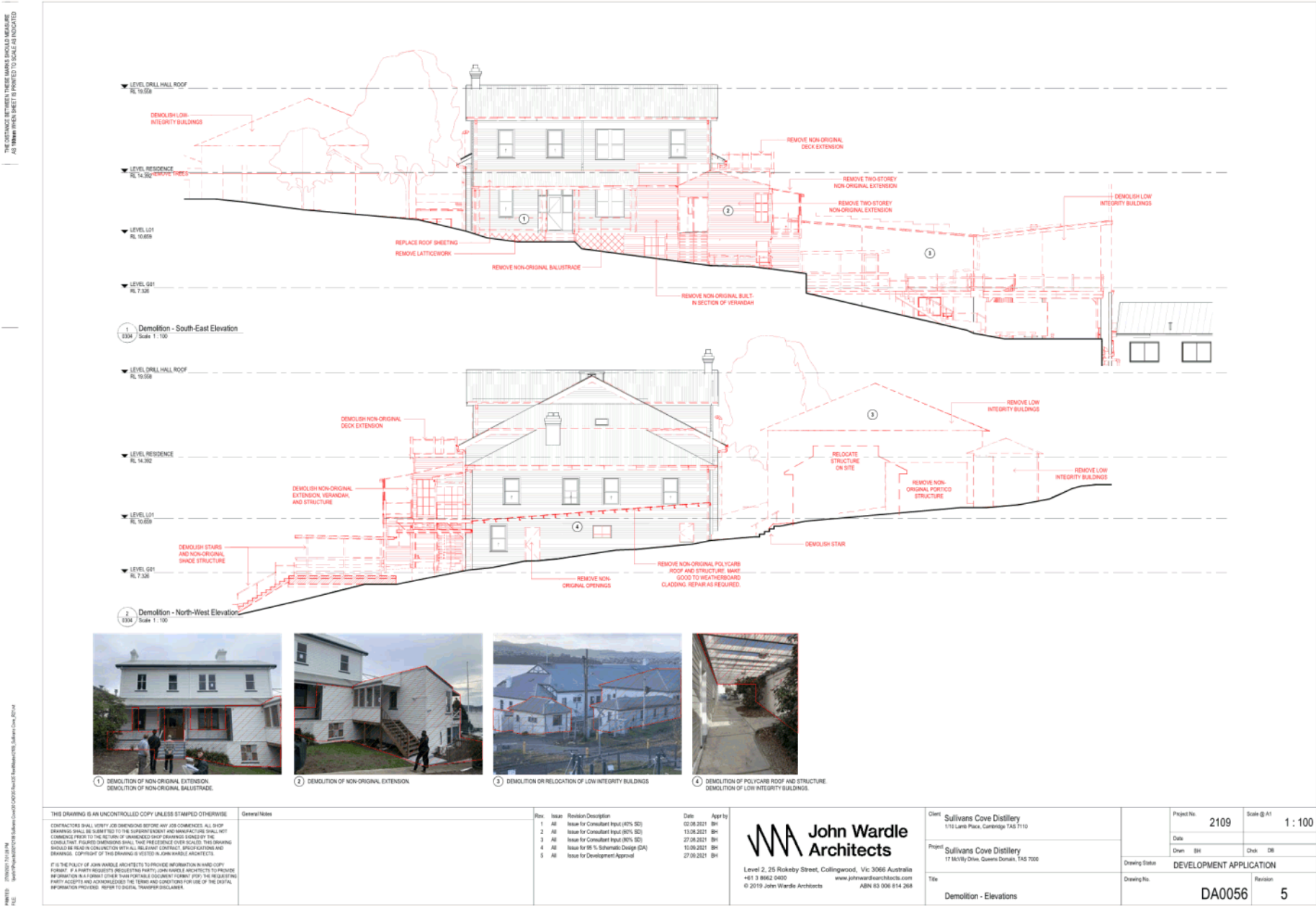


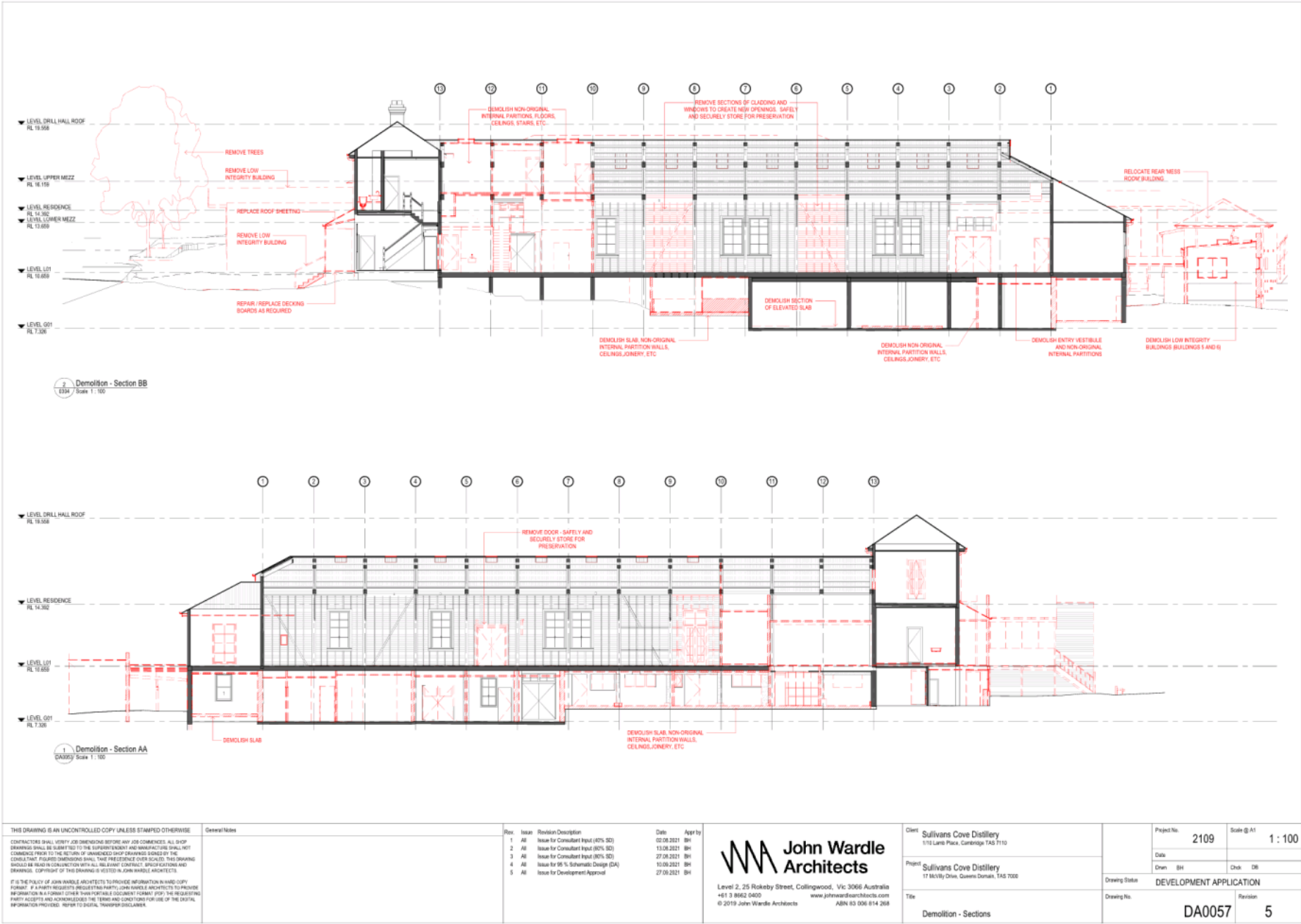


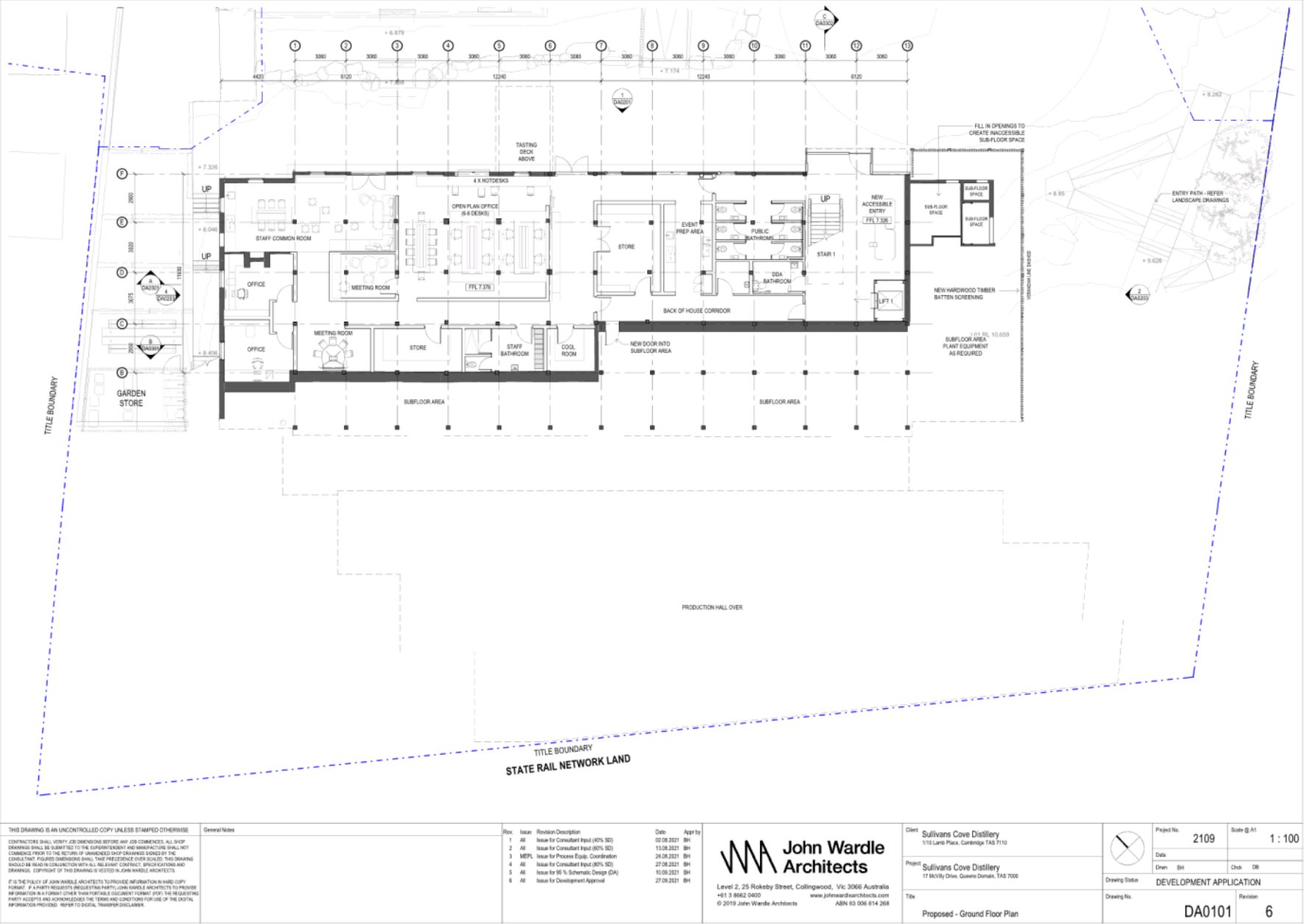


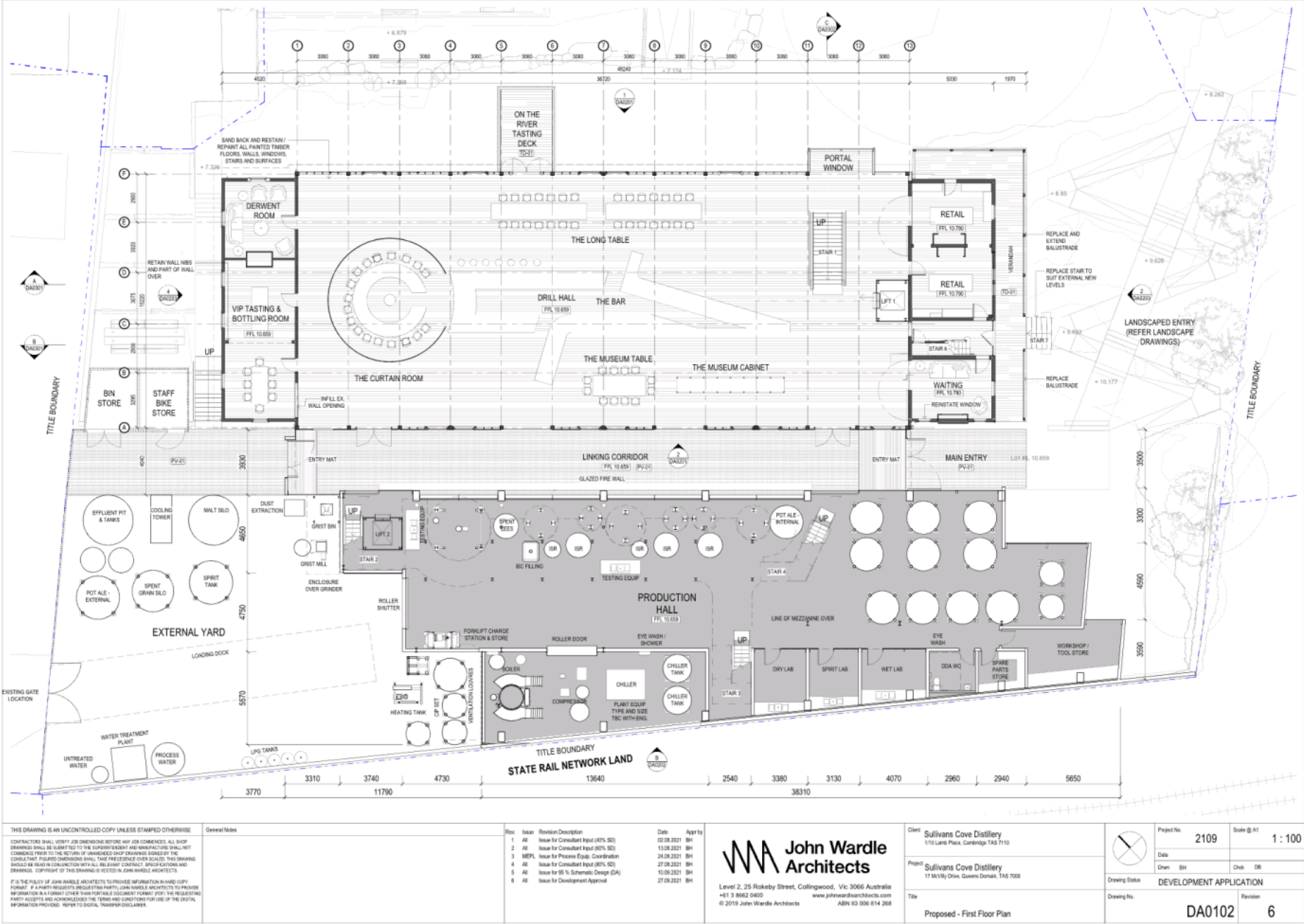


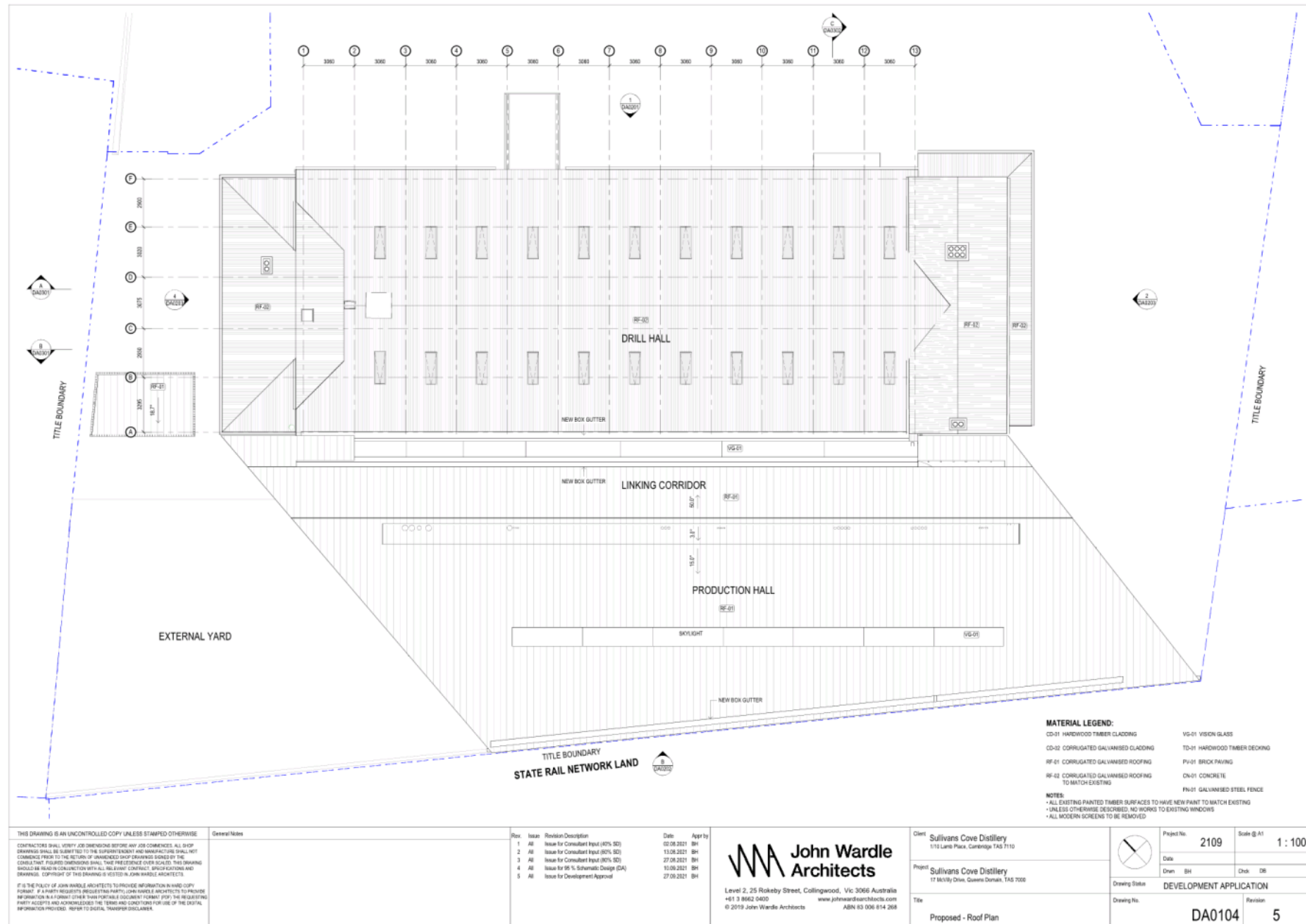








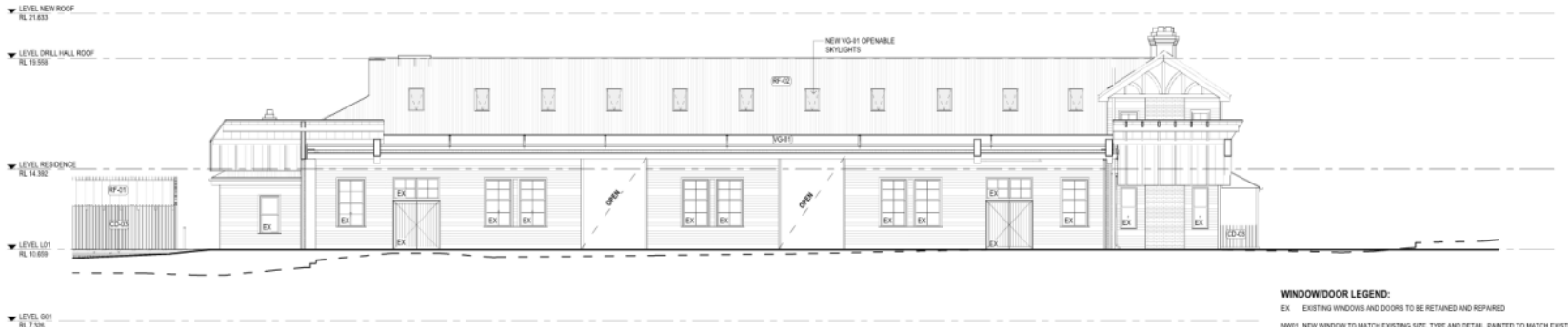




THE CONTAINER BETWEEN THESE BARS SHOULD BE MAINTAINED AS A WALL AND NOT BE REMOVED OR ALTERED

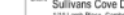


1 Proposed - North-East Elevation
Scale 1:100



2 Proposed - South-West Elevation
Scale 1:100

- WINDOW/DOOR LEGEND:**
- EX EXISTING WINDOWS AND DOORS TO BE RETAINED AND REPAIRED
 - NW01 NEW WINDOW TO MATCH EXISTING SIZE, TYPE AND DETAIL. PAINTED TO MATCH EXISTING
 - NW02 NEW WINDOW TO MATCH EXISTING SIZE WITH CONTEMPORARY DETAILING. UNPAINTED HARDWOOD
- MATERIAL LEGEND:**
- CD-01 CORRUGATED GALVANISED CLADDING
 - CD-02 CORRUGATED GALVANISED ROOFING
 - CD-03 CORRUGATED GALVANISED ROOFING TO MATCH EXISTING
 - VG-01 VISION GLASS
 - TD-01 HARDWOOD TIMBER DECKING
 - PV-01 BRICK PAVING
 - CN-01 CONCRETE
 - FW-01 GALVANISED STEEL FENCE
- NOTES:**
- ALL EXISTING PAINTED TIMBER SURFACES TO HAVE NEW PAINT TO MATCH EXISTING
 - UNLESS OTHERWISE DESCRIBED, NO WORKING TO EXISTING WINDOWS
 - ALL MODERN SCREENS TO BE REMOVED

THIS DRAWING IS AN UNCONTROLLED COPY UNLESS STAMPED OTHERWISE		General Notes		<div>Rev. Issue Revision Description Date Appr by</div> <div><div>1 All Issue for Consultant Input (10% SD) 02.08.2021 BH</div><div>2 All Issue for Consultant Input (10% SD) 11.08.2021 BH</div><div>3 All Issue for Consultant Input (10% SD) 27.08.2021 BH</div><div>4 All Issue for 10% Substantive Design (DA) 10.09.2021 BH</div><div>5 All Issue for Development Approval 27.09.2021 BH</div></div>		<div> John Wardle Architects</div> <div>Level 2, 25 Rokeby Street, Collierville, VIC 3066 Australia +61 3 8662 0450 www.johnwardlearchitects.com © 2019 John Wardle Architects ABN 63 006 614 268</div>		<div>Client Sullivans Cove Distillery 110 Lamb Place, Cambridge TAS 7110</div> <div>Project No. 2109 Scale @ A1 1 : 100</div> <div>Project Sullivans Cove Distillery 17 McVilly Drive, Queens Domain, TAS 7000</div> <div>Client Sullivans Cove Distillery 17 McVilly Drive, Queens Domain, TAS 7000</div>		<div>Project No. 2109 Scale @ A1 1 : 100</div> <div>Date</div> <div>Drawn BH Check DB</div> <div>Drawing Status DEVELOPMENT APPLICATION</div> <div>Drawing No. DA0201 Revision 5</div>	
CONTRACTORS SHALL VERIFY ALL DIMENSIONS BEFORE ANY WORK COMMENCES. ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE SUPERINTENDENT AND MANUFACTURER SHALL NOT COMMENCE PRIOR TO THE RETURN OF UNPAID SHOP DRAWINGS. THE CONSULTANT SHALL BE RESPONSIBLE FOR THE RETURN OF UNPAID SHOP DRAWINGS. THE CONSULTANT SHALL BE RESPONSIBLE FOR THE RETURN OF UNPAID SHOP DRAWINGS. THE CONSULTANT SHALL BE RESPONSIBLE FOR THE RETURN OF UNPAID SHOP DRAWINGS.		IT IS THE POLICY OF JOHN WARDLE ARCHITECTS TO PROVIDE INFORMATION IN A COPY FORMAT. IF A PARTY REQUESTS INFORMATION IN A COPY FORMAT, THE REQUESTING PARTY MUST ACCEPT AND ACKNOWLEDGE THE TERMS AND CONDITIONS FOR THE USE OF THE DIGITAL INFORMATION PROVIDED. REPAIR TO DIGITAL, TYPING, DISSEMINATION.				Proposed - Drill Hall Elevations					

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THE CONTINUITY BETWEEN THESE BORDERS SHOULD MAINTAIN
AN APPROPRIATE SCALE TO THE BUILDING FOOTPRINTS




3 Proposed - South-East Elevation
(DA101) Scale 1:100



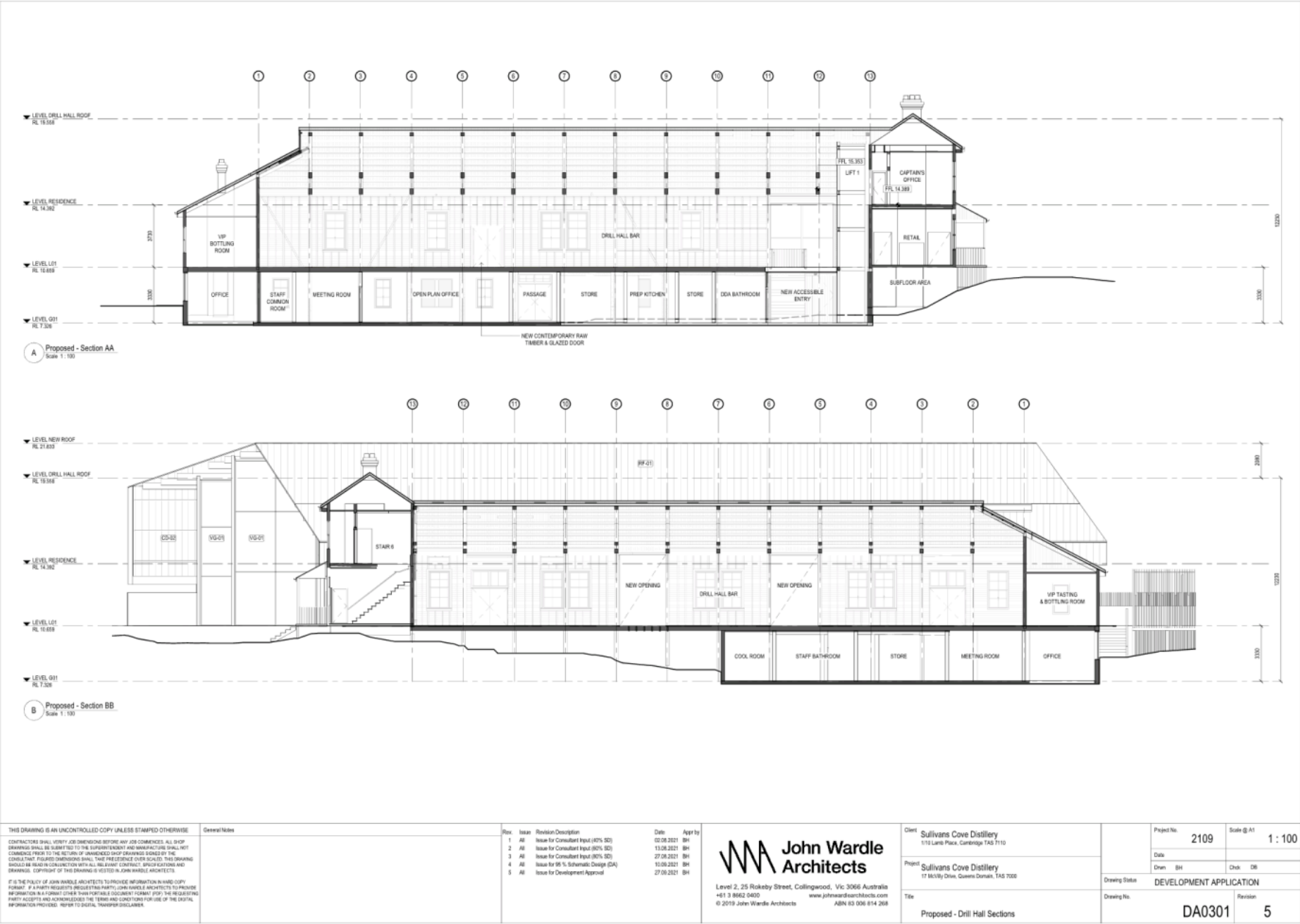
4 Proposed - North-West Elevation
(DA101) Scale 1:100

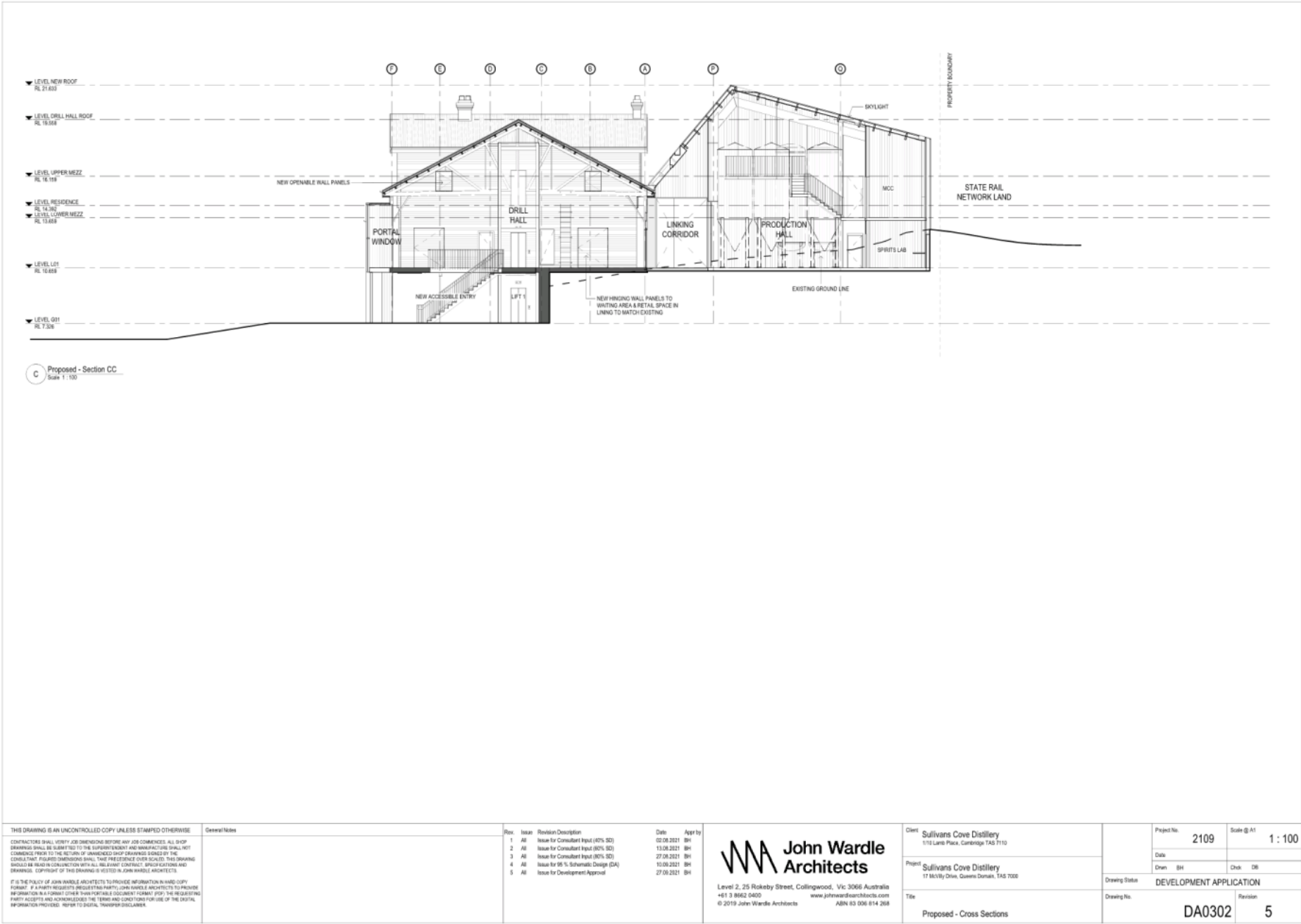
WINDOW/DOOR LEGEND:
EX EXISTING WINDOWS AND DOORS TO BE RETAINED AND REPAIRED
NW01 NEW WINDOW TO MATCH EXISTING SIZE, TYPE AND DETAIL, PAINTED TO MATCH EXISTING
NW02 NEW WINDOW TO MATCH EXISTING SIZE WITH CONTEMPORARY DETAILING, UNTREATED HARDWOOD
MATERIAL LEGEND:
CD-01 HARDWOOD TIMBER CLADDING VG-01 VISION GLASS
CD-02 CORRUGATED GALVANISED CLADDING TD-01 HARDWOOD TIMBER DECKING
RF-01 CORRUGATED GALVANISED ROOFING PV-01 BRICK PAVING
RF-02 CORRUGATED GALVANISED ROOFING TO MATCH EXISTING CN-01 CONCRETE
FN-01 GALVANISED STEEL FENCE
NOTES:
• ALL EXISTING PAINTED TIMBER SURFACES TO HAVE NEW PAINT TO MATCH EXISTING
• UNLESS OTHERWISE DESCRIBED, NO WORKS TO EXISTING WINDOWS
• ALL MODERN SCULPTURE TO BE REMOVED

THIS DRAWING IS AN UNCONTROLLED COPY UNLESS STAMPED OTHERWISE		General Notes		<table><tr><th>Rev.</th><th>Issue</th><th>Revision Description</th><th>Date</th><th>Appr by</th></tr><tr><td>1</td><td>All</td><td>Issue for Consultant Input (10% SD)</td><td>02.08.2021</td><td>BH</td></tr><tr><td>2</td><td>All</td><td>Issue for Consultant Input (10% SD)</td><td>11.08.2021</td><td>BH</td></tr><tr><td>3</td><td>All</td><td>Issue for Consultant Input (10% SD)</td><td>27.08.2021</td><td>BH</td></tr><tr><td>4</td><td>All</td><td>Issue for 10% Submission Design (DA)</td><td>10.09.2021</td><td>BH</td></tr><tr><td>5</td><td>All</td><td>Issue for Development Approval</td><td>27.09.2021</td><td>BH</td></tr></table>		Rev.	Issue	Revision Description	Date	Appr by	1	All	Issue for Consultant Input (10% SD)	02.08.2021	BH	2	All	Issue for Consultant Input (10% SD)	11.08.2021	BH	3	All	Issue for Consultant Input (10% SD)	27.08.2021	BH	4	All	Issue for 10% Submission Design (DA)	10.09.2021	BH	5	All	Issue for Development Approval	27.09.2021	BH	<div>John Wardle Architects</div> <div>Level 2, 25 Rokeby Street, Collingwood, Vic 3066 Australia +61 3 8662 0400 www.johnwardlearchitects.com © 2019 John Wardle Architects ABN 63 006 814 268</div>		<table><tr><td>Client</td><td colspan="2">Sullivans Cove Distillery 1110 Lamb Place, Cambridge TAS 7110</td></tr><tr><td>Project</td><td colspan="2">Sullivans Cove Distillery 17 McVilly Drive, Queens Domain, TAS 7000</td></tr><tr><td>Title</td><td colspan="2">Proposed - Cross Elevations</td></tr></table>	Client	Sullivans Cove Distillery 1110 Lamb Place, Cambridge TAS 7110		Project	Sullivans Cove Distillery 17 McVilly Drive, Queens Domain, TAS 7000		Title	Proposed - Cross Elevations		<table><tr><td>Project No.</td><td>2109</td><td>Scale @ A1</td><td>1:100</td></tr><tr><td>Date</td><td colspan="3"></td></tr><tr><td>Drawn</td><td>BH</td><td>Check</td><td>DB</td></tr><tr><td>Drawing Status</td><td colspan="3">DEVELOPMENT APPLICATION</td></tr><tr><td>Drawing No.</td><td>DA0203</td><td>Revision</td><td>5</td></tr></table>	Project No.	2109	Scale @ A1	1:100	Date				Drawn	BH	Check	DB	Drawing Status	DEVELOPMENT APPLICATION			Drawing No.	DA0203	Revision	5
Rev.	Issue	Revision Description	Date	Appr by																																																																
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THE CONTAINER BETWEEN THESE BORDERS SHOULD REMAIN AS AN OPEN SPACE, SUBJECT TO PLANNING REQUIREMENTS

PROJECT: SUDBURY DISTILLERY
DRAWING: PROPOSED MATERIALS AND FINISHES
DATE: 27/09/2021
BY: JWA

MATERIALS BOARD

EXTERIOR FINISHES



PF-01
EXISTING TIMBER WEATHERBOARD
- WALL CLADDING (DRILL HALL)
- REPAINTED TO MATCH EXISTING



RF-01, RF-02, CD-02
CORRUGATED SHEET (GALVANISED FINISH)
- ROOF LINING
- WALL CLADDING



CD-01
HARDWOOD TIMBER BOARDS
- WALL CLADDING (PRODUCTION HALL)



TD-01
HARDWOOD TIMBER DECKING



FN-01
STEEL FLAT FENCING
IN GALVANISED FINISH



PV-01
BRICK PAVING
- LINKING CORRIDOR FLOOR

NOTE: EXTERNAL GROUND COVER MATERIALS HAVE BEEN SHOWN AS INDICATIVE IN THE ARTIST'S IMPRESSION IMAGES. REFER LANDSCAPE ARCHITECT'S DRAWINGS FOR MORE DETAIL ON EXTERNAL GROUND COVER MATERIALS.

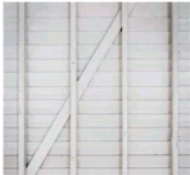


CN-01
GREY CONCRETE

INTERIOR FINISHES



CD-03
HARDWOOD TIMBER BATTENS
- EXISTING RESIDENCE VERANDAH



EXISTING TIMBER WEATHERBOARD
- WALL CLADDING (DRILL HALL)
- REPAINTED TO MATCH EXISTING

THIS DRAWING IS AN UNCONTROLLED COPY UNLESS STAMPED OTHERWISE		General Notes		Rev		Issue		Revision Description		Date		Appr by		 John Wardle Architects		Client		Sulivans Cove Distillery		Project No.		2109		Scale @ A1		1 : 10			
CONTRACTORS SHALL VERIFY JOB DIMENSIONS BEFORE ANY JOB COMMENCES. ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE SUPPLIER/END USER AND MANUFACTURER SHALL NOT COMMENCE PRIOR TO THE RETURN OF UNREVISED SHOP DRAWINGS SIGNED BY THE CONSULTANT FIGURED COMMISSIONER SHALL TAKE PRECEDENCE OVER ISSUES. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACT, SPECIFICATIONS AND DRAWINGS. COPYRIGHT OF THIS DRAWING IS VESTED IN JOHN WARDLE ARCHITECTS.		IT IS THE POLICY OF JOHN WARDLE ARCHITECTS TO PROVIDE INFORMATION IN HARD COPY FORMAT. IF A PARTY REQUESTS INFORMATION IN HARD COPY FORMAT, THE REQUESTING PARTY ACCEPTS AND ACKNOWLEDGES THE TERMS AND CONDITIONS FOR USE OF THE DIGITAL INFORMATION PROVIDED. REFER TO DIGITAL SHARING DISCLAIMER.		1		AI		Issue for 95% Substantive Design (DA)		15/09/2021		BH		<div>Level 2, 25 Rokeby Street, Collingwood, VIC 3066 Australia +61 3 8962 0400 © 2019 John Wardle Architects www.johnwardlearchitects.com ABN 63 006 814 268</div>		Project		Sulivans Cove Distillery 17 McVilly Drive, Queens Domain, TAS 7000		Date		Drawn		BH		Check		DB	
				2		AI		Issue for Development Approval		27/09/2021		BH								Drawing Status		DEVELOPMENT APPLICATION		Revision		2			
Title		Drawing No.		DA0701		2		Proposed Palette of Materials and Finishes																					

THE CONTAINERS BETWEEN THESE BARRS SHOULD MAINTAIN AN OPEN SPACE, UNLESS IT PROVIDES TO PLANTAIN INDICATED

1 Shadow Study - June 21 10.00
Scale 1:300

2 Shadow Study - June 21 12.00
Scale 1:300

3 Shadow Study - June 21 15.00
Scale 1:300

4 Shadow Study - Dec 22 10.00
Scale 1:300

5 Shadow Study - Dec 22 12.00
Scale 1:300

6 Shadow Study - Dec 22 15.00
Scale 1:300

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General Notes

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Rev

Issue

Revision Description

Date

Appr by

1

AI

Issue for Consultant Input (70% SD)

23.08.2021

BH

2

AI

Issue for 5% Scheme Design (DA)

10.09.2021

BH

3

AI

Issue for Development Approval

27.09.2021

BH

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Client

Sullivans Cove Distillery

110 Lamb Place, Cambridge TAS 7110

Project

Sullivans Cove Distillery

17 McVilly Drive, Queens Domain, TAS 7000

Title

Shadow Studies

Project No.

2109

Scale @ A1

1:500

Date

Drawn

BH

Check

DB

Drawing Status

DEVELOPMENT APPLICATION

Drawing No.

DA0801

Revision

3

THE CONTAINER BETWEEN THESE MARKS SHOULD REMAIN
AS SHOWN HEREIN, UNLESS IT APPEARS TO BE A MISTAKE



APPROVED FOR THE CITY OF CAMBRIDGE
BY THE CITY ENGINEER
DATE: 10/10/2021

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DRAWINGS SHALL BE SUBMITTED TO THE SUPERINTENDENT AND MANUFACTURER SHALL NOT
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General Notes

Rev.	Issue	Revision Description	Date	Appr. by
1	AI	Issue for Consultant Input (90% SD)	13/08/2021	BH
2	AI	Issue for Consultant Input (90% SD)	27/08/2021	BH
3	AI	Issue for 95 % Schematic Design (DA)	10/09/2021	BH
4	AI	Issue for Development Approval	27/09/2021	BH

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Architects**

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Client	Sullivans Cove Distillery 1/10 Lamb Place, Cambridge TAS 7110
Project	Sullivans Cove Distillery 17 MacVilly Drive, Queens Domain, TAS 7000
Title	Visual Impact Study - View from Cycle Path

Project No.	2109	Scale	Q: A1
Date		Drawn	BH
Checked	DB	Drawing Status	DEVELOPMENT APPLICATION
Drawing No.	DA0601	Revision	4

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<p>CONTRACTORS SHALL VERIFY JOB DIMENSIONS BEFORE ANY JOB COMMENCED. ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE SUPERINTENDENT AND MANUFACTURER FOR APPROVAL PRIOR TO THE RETURN OF UNANDED SHOP DRAWINGS. THE CONSULTANT FURNISHES DIMENSIONS ONLY. THE PRESENCE OF DIMENSIONS DOES NOT IMPLY THAT THE DIMENSIONS ARE CORRECT. THE PRESENCE OF DIMENSIONS DOES NOT IMPLY THAT THE DIMENSIONS ARE CORRECT. THE PRESENCE OF DIMENSIONS DOES NOT IMPLY THAT THE DIMENSIONS ARE CORRECT.</p> <p>IT IS THE POLICY OF JOHN WARDLE ARCHITECTS TO PROVIDE INFORMATION IN WRITING. IF A PARTY REQUESTS INFORMATION FROM JOHN WARDLE ARCHITECTS TO PROVIDE INFORMATION IN A FORM OTHER THAN WRITING, THE REQUESTING PARTY ACCEPTS AND ACKNOWLEDGES THE TERMS AND CONDITIONS FOR USE OF THE DIGITAL INFORMATION PROVIDED. REFER TO DIGITAL SIGNATURE DISCLOSURE.</p>		<div>Project Sullivans Cove Distillery 17 McIlroy Drive, Queens Domain, TAS 7005</div> <div>Date</div> <div>Drawn BH Check DB</div>		<div>Drawing Status DEVELOPMENT APPLICATION</div> <div>Drawing No. DA0602 Revision 4</div>					
		<div>Title Visual Impact Study - View from Rail Corridor</div>							

THE CONTRACTOR SHALL VERIFY THE DIMENSIONS BEFORE ANY JOB COMMENCES. ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE SUPERINTENDENT AND MANUFACTURER SHALL NOT COMMENCE PRIOR TO THE RETURN OF GRANTED SHOP DRAWINGS SIGNED BY THE CONSULTANT. FIGURES DIMENSIONS SHALL TAKE PRECEDENCE OVER LOCATES. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACT, SPECIFICATIONS AND DRAWINGS. COPYRIGHT OF THIS DRAWING IS RESERVED BY JOHN WARDLE ARCHITECTS.




PROJECT: SULLIVANS COVE DISTILLERY
DRAWING: DA0603
DATE: 27/08/2021
BY: JWA
CHECKED: JWA
DATE: 27/08/2021
BY: JWA
APPROVED: JWA
DATE: 27/08/2021
BY: JWA

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Client		Sullivans Cove Distillery		1/10 Lamb Place, Cambridge TAS 7110		Project No.		2109		Scale @ A1			
Project		Sullivans Cove Distillery		17 MacVicar Drive, Queens Domain, TAS 7000		Drawing Status		DEVELOPMENT APPLICATION		Drawing No.			
Title		Visual Impact Study - View from Eastern Entry Road				Revision		3		DA0603			

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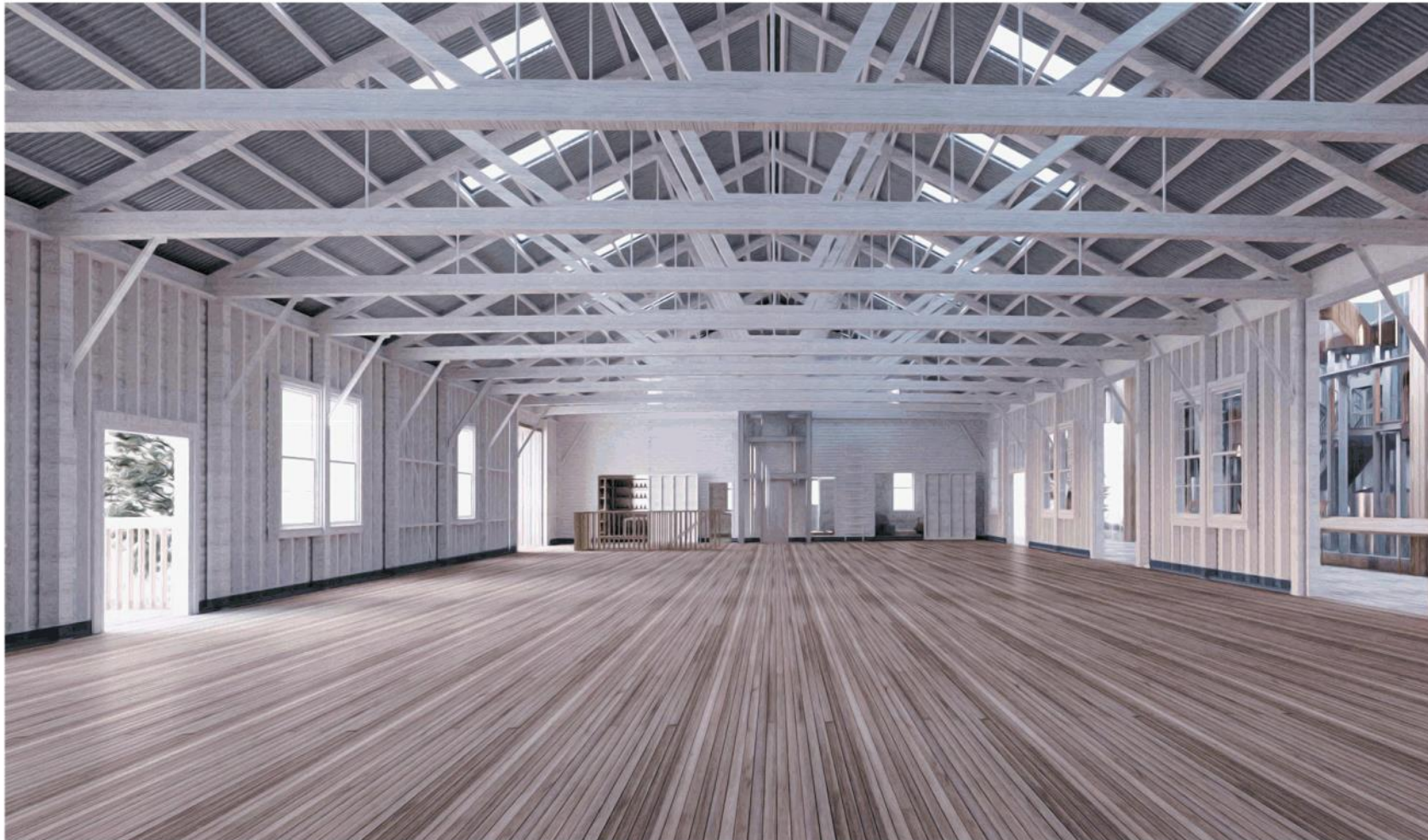
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				Title: Artist Impression - Exterior View 01, Site Arrival		Drawing Status: DEVELOPMENT APPLICATION																							
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PROJECT: SULLIVANS COVE DISTILLERY
DRAWING NO: DA0502
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PROJECT: SULLIVANS COVE DISTILLERY
DRAWING: INTERIOR VIEW 05, DRILL HALL FACING SOUTH
DATE: 27/09/2021
BY: JOHN WARDLE ARCHITECTS



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2109 – Sullivans Cove Distillery
Document Number: DA9101
Architect's Design Statement for DA (Rev.1)
Issued 10.09.2021

Design Intent:

A new home for the makers of the world's finest whisky

Our proposal seeks to provide a new home for Sullivans Cove Distillery - makers of the world's best single malt whisky.

Sullivans Cove whisky is made by a passionate family of people who care about their craft, using minimal intervention techniques and with locally sourced raw ingredients. It is a uniquely Tasmanian product that is receiving attention from a wide audience in Tasmania, interstate, and across the globe. Inspired by the Sullivans way of making whisky, the architecture has been approached in a similar manner – with care, authenticity, and careful intervention into the precious Derwent River precinct.

The new Sullivans Cove Distillery will consist of two structures.

The historic Drill Hall and adjoining Caretaker's Residence will house the main 'visitor experience' areas, including the world's first speciality single malt whiskey bar and a series of warm and comfortable tasting spaces. As a part of these works, the Drill Hall and Residence structure will be carefully restored to its 1912 state.

The second structure, the Shadow Building, is designed to sit reverently behind the historic dwelling. The building derives its name from its footprint; it looks as if it has been created from the shadow cast by the Drill Hall. The building is a long, linear structure with an intentionally restrained material palette. It is clad in corrugated galvanised steel and supported by a mass timber structure grown in plantations south of Hobart and manufactured within Tasmania. It houses the production and processing equipment for the distillery and will have an elevated mezzanine for visitors on guided tours to learn about the process of whisky making.

These two structures will sit within a beautiful landscape filled with native Australian flora and meandering paths, as well as an outdoor events area and gathering spaces for visitors to enjoy. The site will also be used to complement the already celebrated events calendar Hobart has growing recognition for. The restoration of these historic dwellings and its gardens will enliven Huon Quays, creating a new cultural destination in Hobart for local and international visitors alike to enjoy.

Exploration:

Historical Research Process

The Huon Quays site has a rich pre- and post-colonial settlement history. Before starting the design process, John Wardle Architects undertook an intensive research process in order to establish a heritage methodology. This process involved the reading of historical documents and ongoing consultation with a team of experts, including the project planner and heritage consultant. A working version of the scheme was presented to the Hobart City Council and Hobart State Heritage department with the intent of providing an opportunity for early feedback. Adjacent landholders and stakeholders were also involved throughout the early phases of design for their input. The project team has also consulted with NITA education and the Traditional Owners of the land to better understand the cultural significance of the site. Each of these steps has helped to inform the proposal for the architecture and landscape.

Creating a Campus:

Treatment of the Huon Quays Site

The proposal sets out a number of alterations to the Huon Quays site. The broad approach has followed the recommendations of the CMP as further discussed in the 2020 Praxis Preliminary Heritage Impact Assessment. After extensive consultation and research, it was concluded that the most appropriate way to treat the site is to remove any 'low integrity' structures, 'later-addition' extensions, so that the 1912 condition of the Huon Quays is reinstated and legible. The Drill Hall, adjoining residence, and the shadow building, along with the addition of the small, relocated roof, and the adjacent boat club, creates a sense of campus on this important site. For clarity, we have numbered the buildings as per the CMP, refer opposite page. Key alterations to the site include the following -

Building 1



The small timber-framed building, identified in the CMP as 'low internal integrity' is proposed for demolition. This is required to create sufficient space for the new Distillery to operate effectively.

Building 2

The large timber-framed 1940s era building, last used as a backpackers hostel, and identified in the CMP as 'low internal integrity' is proposed for demolition, to create space for the new distillery production area. Converting the existing building into a distillery is neither possible nor desirable for several reasons. The elevation above ground level poses challenges for the delivery of goods and accessibility. The distillery equipment required to make the building fit for purpose, would not fit within the envelope of the existing building. In order to manufacture hazardous goods within this building, the fabric would need to be significantly altered to meet regulatory requirements such as Australian Standard AS1940. This area of the site is also the only feasible area to construct a building large enough to house the functional requirements of a distillery on the site without disrupting sight lines from the Drill Hall to the River Derwent.

By removing the existing building and excavating down into the embankment as much as practicable, the new distillery can sit as low in the landscape as possible, reducing the individual prominence of the new building. We also note that building 2 is in poor condition, with leaks and damage throughout that would require extensive repair if it were to be repurposed.

Building 4

A small timber-framed 'mess room' building located beside the upper gate. This structure comprises of a single room, kitchenette and toilet. The CMP notes that "the gable roof form is the only feature of interest" and it otherwise has "little integrity". Efforts have been made to reuse the building on site, however the integrity of the structure and presence of asbestos makes this difficult. To retain the most significant part of the building, the gable roof form will be relocated and repurposed to provide shelter over a long table designed for Sullivans Cove family, staff and members of the public to enjoy. The rest of the building will be demolished to make space for plant equipment associated with the distillery and to allow for safe vehicle access to the site.

Buildings 5 and 6

The two masonry amenities buildings and the adjoining polycarbonate roof structure over the adjoining path are indicated in the CMP as 'internally and externally compromised'. Building 5 is proposed for demolition. Building 6 is proposed for partial demolition. We would like to retain the ground floor level for use as a storage area for site maintenance equipment, and the first floor level as a lookout point and area for staff with a small terraced garden. The recently installed polycarbonate roof is proposed for demolition. Note: Buildings 5 and 6 are on a separate lease and may or may not be flagged for different development in the future.

Buildings 7 and 8

The original 1912 Drill Hall and adjoining Caretaker's Residence building, identified in the CMP as being of 'high integrity' is proposed for retention and restoration. More detail on the proposed works for these buildings are in the following sections.

Building 9

The boat shed is out of scope for this project. None of the proposed works will affect this building or its use.

Building 10

Small masonry structure which was last used for storage of ammunition for patrol boat stationed at HMAS Huon. Now used as a sewage pump station. This structure will remain and be incorporated into the landscape scheme.

Buildings 11 and 12

Two masonry structures, which abut each other, originally used as a "ready use ammo locker" and identified in the CMP as 'low internal integrity' are proposed for demolition.

Buildings 13, 15 and 16

Modern structures with 'no aesthetic or historical associations' are proposed for demolition. Structures include an open carport and two colorbond garden sheds.

Flag Mast

Historic significance noted. Flag mast to be retained in its existing location.

Jetty

Out of scope. Provision for equitable access will be made in the landscape proposal in case required in future.

Landscape

Refer Landscape Architect's proposal for more detail.



Single Barrel, Minimal Intervention: A Minimal Intervention approach to the existing building

Restoration Works to Drill Hall and Caretaker's Residence Facade

As can be seen from the Conservation Management Plan, the original architectural drawings, and our own observations on site, there have been many alterations and additions made to the Drill Hall over the last century. While we acknowledge that later additions can help to tell the story of a structure's evolution, we feel that in this instance the ad-hoc additions and modifications detract from the original Edwardian-era building and inhibit its legibility. We therefore believe the most favourable conservation outcome would arise from removing these later additions to uncover the original utilitarian form of the building.

With this in mind, we propose to restore the Drill Hall and Caretaker's Residence to its original 1912 state as much as practicable. We propose to do this by uncovering previously hidden parts of the original structure and fabric, reinstating previously removed fabric and constructing several contemporary interventions which will seek to make the building compliant to current building codes and fit for purpose. All new works to the Drill Hall and Residence are proposed to be complementary, appropriate in scale, and respectful of the site's history. These additions will be designed and detailed to read as clearly 'modern' and associated with the next phase of the building's use. We appreciate the utilitarian nature of the buildings and wish to celebrate these aspects of the structure.

As part of these works, we propose to remove the non-original extension to the north-east corner of the Drill Hall and Residence (refer demolition plans), the non-original verandahs on the northern façade of the Drill Hall, and any non-original window and door openings.

To the Residence, we propose to reinstate a previously removed section of verandah and roof on the northern façade, where the non-original extension on the north-eastern corner currently sits. A new contemporary balustrade is proposed to the full extent of the verandah. The balustrade's language will be complementary to the original building fabric and will read as part of the residential section of the building in contrast to the Drill Hall. We hope this will help to improve the legibility of the original façade.

Restoration Works to Drill Hall and Caretaker's Residence Interior

Inside the Drill Hall, a key attribute of the building is its large and open main space. To reinstate the original extent and condition of the main Drill Hall volume, we propose a number of restoration works. This includes the removal of non-original internal partition walls, rooms, and staircases on the first floor level between grid lines 10 -13 (refer demolition plans). We also propose the removal of non-original wall and ceiling linings that enclose and/or cover up the original roof structure, wall framing, and timber weatherboard cladding. The entry vestibule and auxiliary rooms that sit on the southwest side of the Drill Hall main space are also proposed for removal to match the original architectural plans.

We acknowledge that the inside of the Residence building was originally residential. However, it has not been used for this purpose for many decades, and there is no foreseeable opportunity to reinstate that use. We hope that through careful restoration and adaptive reuse the residence will instead have a new future as part of the distillery, where visitors can appreciate its history and understand its previous function.

In the two northernmost rooms of the Residence, the non-original kitchen will be removed. Any non-original floor coverings will also be removed, and the fireplaces will be recommissioned. New openings will be created between the two northernmost rooms to create a cellar door space for the distillery. Nibs either side of the wall and a section of wall over will be retained in order to retain legibility that it was once two rooms.

Authentic Experience: Contemporary Interventions

Our proposal seeks to provide Sullivans Cove with a range of spaces, unique experiences, and special moments to offer their visitors. We have done so while keeping in mind Sullivans Cove's values of authenticity. The alterations and additions in the following section are proposed to bring the building up to code, make the building fit for purpose, or to create moments of enjoyment for visitors. Our approach has been for these alterations have as minimal an impact on the heritage dwellings as possible. Contemporary alterations are focussed in areas which have already undergone alteration.

Drill Hall Main Space

The main space of the Drill Hall will house a new whisky bar and will include a series of accompanying tasting spaces. The bar will be made up of a series of ~1100mm high objects that seem to 'float' in space – the bar, the museum table, and the long table. Above this datum, there will be a museum cabinet; this will be mostly



transparent with the purpose of housing historical artefacts and information about the site and the distillery. The objects will be highly detailed and delicate. They are intended to sit respectfully within the Drill Hall and to complement rather than compete with it. There will also be a circular curtain rail suspended below the truss line. The curtain will have two modes; completely open or completely closed. The curtain allows for flexibility of seating and the potential for larger private functions without permanent division of the space.

At the eastern end of the Hall, a new glass and steel lift is proposed to offer equitable access to the ground, first and second floors of the Drill Hall and Residence. As the floor level of the residence is lower than the bottom chord of the Drill Hall trusses, a section of the bottom chord of the truss adjoining the residence will need to be cut out to provide access.

Western End Rooms

The proposal imagines one of these domestic-scale rooms to the western end of the Drill Hall as a tasting room with a view to the River Derwent, and the other two as a visitor experience bottling room. The tasting room will be warm and comfortable with bespoke display shelving. Although the production bottling facility sits off-site, the bottling room would allow some visitors to hand bottle their own whisky. To create sufficient space for the bottling room, a new opening is proposed between the middle and southern-most room. Nibs on either side and a section of wall over will be kept in order to retain legibility that it was once two rooms. If permitted, the existing fireplaces will be recommissioned.

New Accessible Entry

Once the north-east corner addition is demolished, a hole in the building fabric on the northern facade will remain. In this space, we propose to construct a new, double height 'portal' window. This will hold a new entry door, and a large window with a view towards the River Derwent. The language of this portal will be contemporary but complementary, clearly delineating the original from the new building fabric. This entry will provide access to a lift, new public amenities, and a staircase that connects the gardens and events space to the first floor of the Drill Hall.

Ground Floor Works

As part of the proposal, the currently derelict and poorly architecturally resolved spaces on the ground floor of the Drill Hall building will be reinvigorated. All non-original internal partitions are proposed for removal. The existing structure will be refurbished, and the junctions between new and existing fabric will be carefully considered. Fire damaged building fabric in the undercroft will be repaired and sympathetically replaced to match existing where required.

The scale and layout of this area makes it appropriate for office spaces. It will also house staff amenities, administration and support spaces required to operate the distillery, key plant and storage spaces, and a staging / preparation area to support events. Locating these functions on the ground floor ensures as minimal an impact as possible to the original fabric. New openings are proposed to bring natural light into the staff areas. These new openings will, where possible, be created where original windows were previously removed. The new window openings will be of the same proportion as the adjacent original openings. However, their detailing will be clearly contemporary.

Caretaker's Residence

In the proposal, the Residence houses the cellar door retail space, a greeting / information area for visitors, and, upstairs, three tasting areas: the captain's post, the captain's library, and the captain's den. The intimate scale of these domestic rooms makes them ideal for hosting small groups.

On the lower floor of the Residence, new openings are proposed for the western wall of southernmost and northernmost rooms. These are required to connect what will be the cellar door and the greeting space to the Drill Hall. These openings will have large-hinged wall panels made from the existing wall fabric so that, when in the closed position, they will read as part of the wall. This will allow the fabric to be preserved in place, for the Drill Hall and Residence to remain separate buildings, and for the provision of access requirements for distillery staff. Additionally, it will allow the Caretaker's Residence rooms to remain legible as previously domestic spaces.

The cellar door will be located in the two northernmost rooms on the ground floor. These are proposed to be joined by creating doorway openings either side of the existing fireplace. Similar to the Bottling Room, nibs on either side and a section of wall overhead will remain to retain legibility that the space was once two rooms.

On the upper floor of the Residence, the rooms will be converted into tasting spaces and an office for the head distiller. The non-original deck and balustrade on the northern-most room of the Residence will be removed, and windows will be reinstated where the non-original doors leading out to the deck are currently. New openings will be made on the west wall to offer visitors a view towards the roof trusses. Similar to the lower floor, these openings will have hinged wall panels, and will be made from the existing wall fabric so that they read as part of the existing wall when in the shut position.



To provide equitable access to the top floor of the residence, an infill section of timber batten flooring will be placed over the top flight of the stairs leading into the corridor to create a landing for the lift. The stairs below this infill panel will be preserved and visible to those walking up the flight of stairs, connecting the landing to the room outside the existing bathroom. The bathroom will be refurbished and more appropriately fitted out to complement the heritage fabric it sits within.

New Insulated Roof and Thermal Comfort

In order to achieve thermal comfort in the Drill Hall, an additional insulated roof layer of minimal thickness is proposed. This will be aided by hydronic heaters as well as underfloor hydronic heating throughout the Drill Hall and Residence. All external walls will remain unlined and uninsulated to leave visible the existing timber structure and weatherboard cladding. This will help to preserve the simple, utilitarian detailing of the Drill Hall. The new roof will be clad in corrugated, galvanised steel-roofing to match the existing profile. New, operable skylights will be installed between the roof's trusses to achieve fresh air requirements and provide additional natural light. These replace the existing polycarbonate infill sheets which are now well degraded.

Linking Corridor

The Shadow Building sits just over three metres from the external wall of the Drill Hall building. Its roof lightly touches the Drill Hall roof, and a section of glazing overhead provides visual and practical separation. This detail ensures that the Shadow Building reads as separate to, rather than an extension of, the Drill Hall. The space between the two structures creates an alleyway similar in size to the existing one and retains legibility of the existing condition. The eaves and barge board of the Drill Hall are retained and visible. A shared box gutter above the line of the Drill Hall roof allows for roof drainage.

The Shadow Building

The Shadow Building sits behind the Drill Hall building. Its asymmetrical form is almost completely hidden when viewed from the River Derwent, revealing only a single angled facade, that signifies the main entry to the eastern side. This allows the distillery building to sit quietly, ensuring that the Drill Hall and Residence remain the visually dominant element on the site.

The form of the distillery building envelope has been dictated by the equipment and production requirements as well as the shape of the site. The roofline, a distorted gable roof, is complementary to the existing roof forms of the site, but does not seek to replicate any of the heritage structures. The building has been kept as low and as small as possible without creating any workflow or efficiency barriers for the production processes. The stills and fermenters (the tallest pieces of equipment) have been located underneath the roof ridge and have set the roof height accordingly with minimum clearance requirements. Working closely with the process engineer, the equipment heights have been reduced as much as practical, and the building has been excavated down into the landscape to the level of the Drill Hall main space so that the building sits as low as possible.

Materials and Finishes

The material palette for both structures is intentionally simple. In the Drill Hall and Caretaker's Residence, all previously painted surfaces will be sanded back and repainted to match their original colour. All previously unpainted surfaces will remain unpainted. The timber floorboards in the Drill Hall will be sanded back and re-sealed. Any surfaces in the Drill Hall that were previously covered up by carpet, tiles, or the like will be uncovered and repaired or restored as required.

In the Drill Hall and Residence, all contemporary interventions will be made from or clad in unpainted, locally sourced, hardwood timber. Externally, this will be left to weather over time to a silver grey. All contemporary furniture and joinery items within the Drill Hall and Caretaker's Residence will be made from or clad in a material such as zinc.

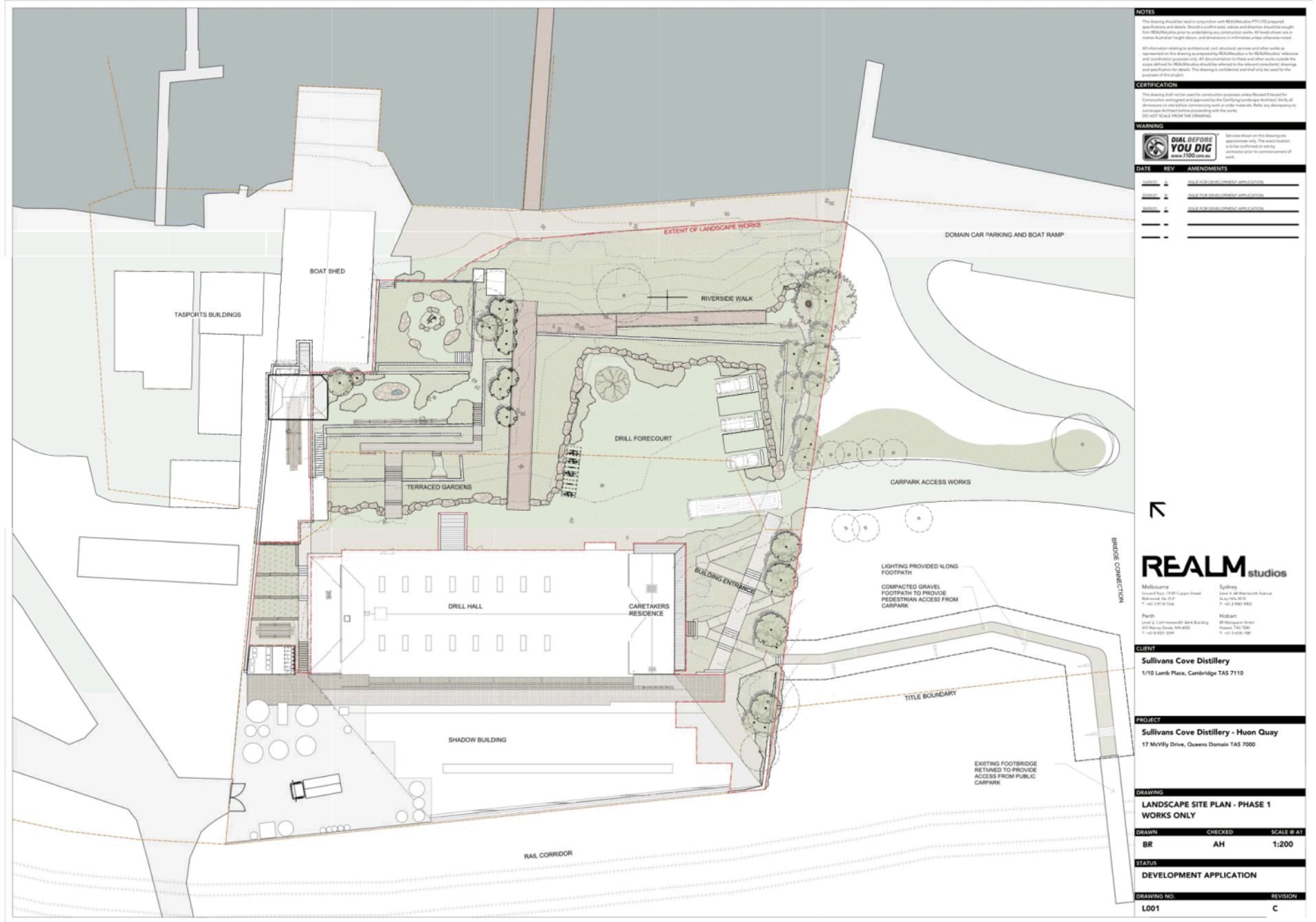
The new distillery building will be clad internally and externally in hot dip galvanised sheets; a traditional building material that has a beautiful, muted grey colour. The external walls, roof, and ceiling will be clad in corrugated sheets, while the internal walls will be clad with flat sheets. The roof structure will be made from locally manufactured, mass-timber glulam beams and columns. This is a material that sits comfortably on a site already home to some unique timber structures. The purlins, wall framing, and external walls to the east and west will be made from or clad in locally sourced hardwood timber. These surfaces will be left unpainted and will weather over time to a silver grey.

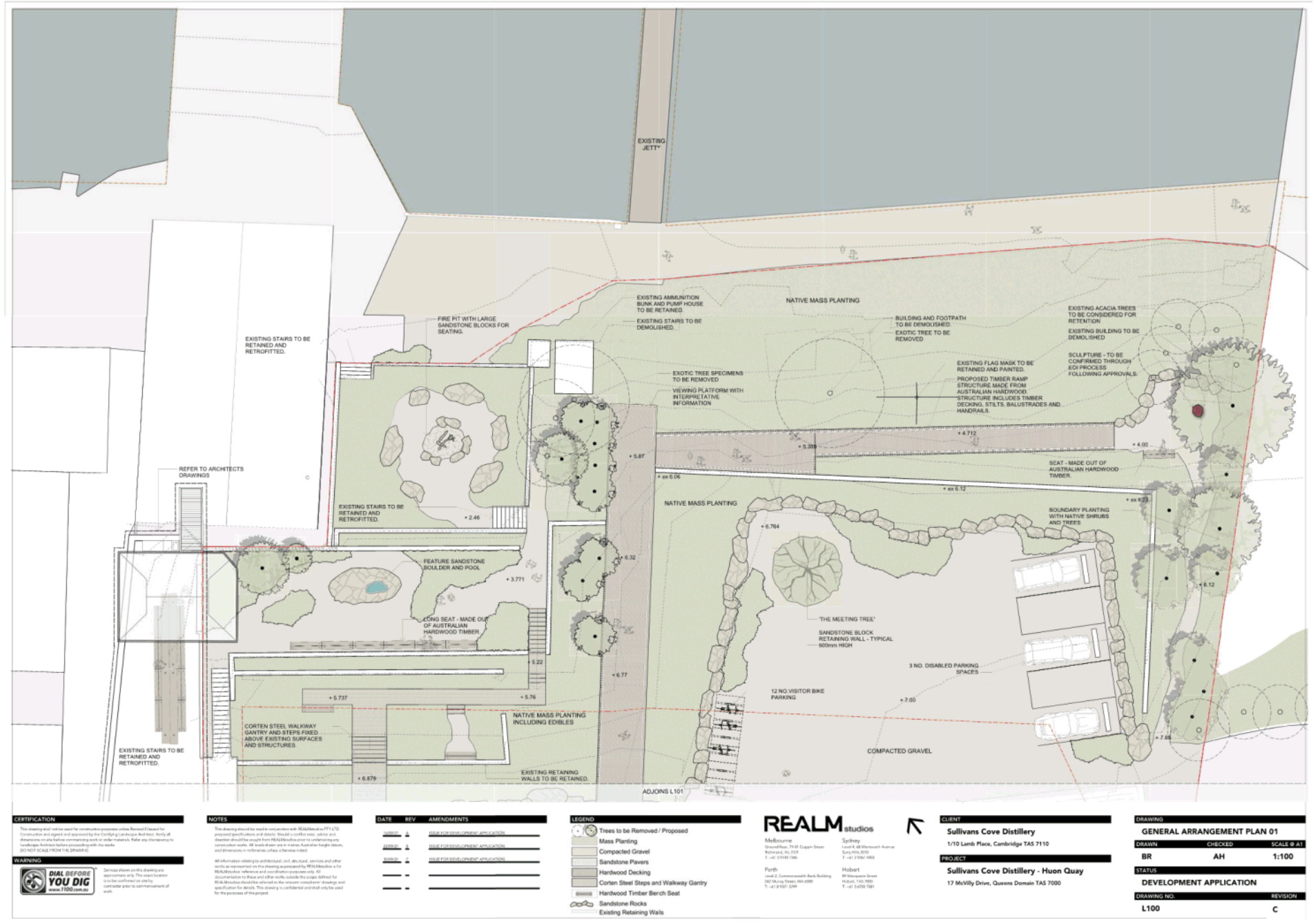
All materials have been chosen for their natural qualities, muted appearance, and connection to Hobart's major waterfront heritage industries. These materials were also commonly used during the early 20th century when the HMAS Huon site was first constructed. Wherever possible, materials will be locally sourced.

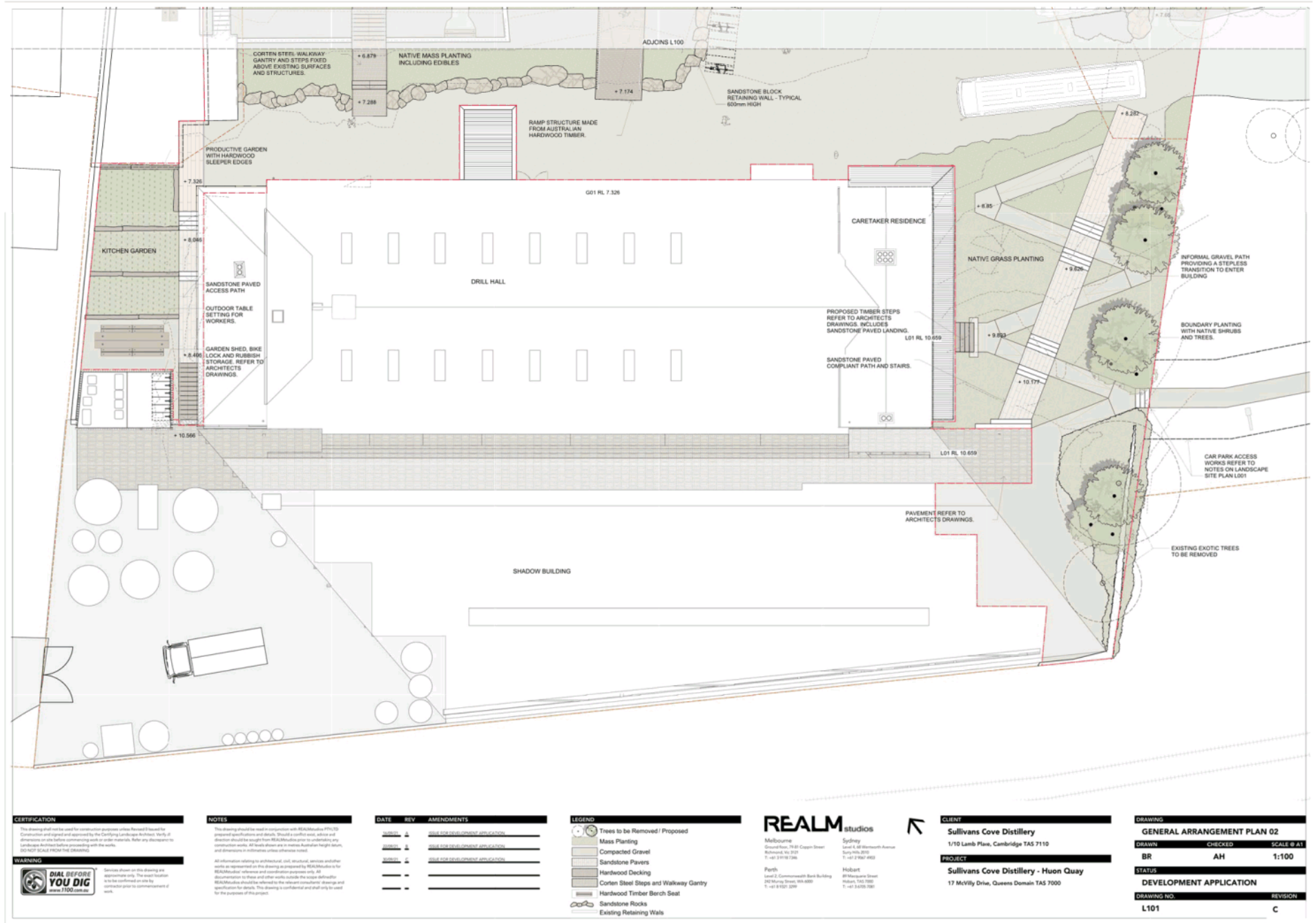
The Future of Huon Quays

We believe this proposal to adaptively reuse the site as a whisky distillery, bar, gardens, and event space will create a new and exciting destination for Hobart, and will bring life back to Huon Quays, providing enjoyment to visitors from many places for years to come.













Huon Quays

Planning Assessment

Sullivans Cove Distillery Pty Ltd

29 October 2021

→ **The Power of Commitment**

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Contents

1. Introduction	1
1.1 Purpose of this report	1
1.2 Scope and limitations	1
1.3 Project Documentation	1
2. Site Analysis	2
2.1 Subject Site	2
2.2 Title	2
2.3 Existing use and development	3
2.3.1 Outlooks	6
2.4 Surrounding land use and development	7
2.5 Aboriginal Heritage	10
2.6 Services	11
2.7 Historic Cultural Heritage	11
2.8 Natural values	11
2.9 Movement Network	12
2.10 Domain Masterplan	12
3. Proposed Development	15
3.1 Consultation	15
4. Scheme Assessment	16
4.1 Strategic Framework	16
4.2 Activity Area	17
4.2.1 Objectives and Performance Criteria	17
4.2.2 Use of Land	19
4.3 Conservation of Cultural Heritage Values	20
4.3.1 Conservation of Places of Cultural Significance	21
4.4 Urban Form	22
4.4.1 Building Form	23
4.4.2 Building Surfaces	24
4.5 Public Urban Space	25
4.6 Signs	25
4.7 Traffic, access and parking	25
4.7.1 Development Control	26
4.8 Subdivision	28
4.9 Demolition	29
4.10 Environmental Management	30
4.11 Key Sites	30
5. Conclusion	32

Table index

Table 1	Project titles	2
Table 2	Proposed Uses	19

Figure index

Figure 1	Site location	2
Figure 2	Aerial image	3
Figure 3	Site Layout	4
Figure 4	Building 7 – Drill Hall interior	5
Figure 5	Building 7 – northern elevation (left), and eastern elevation of former residence (right)	5
Figure 6	Building 4 (left) and Building 6 (right) and eastern elevations	6
Figure 7	Building 2 western elevation	6
Figure 8	Outlook from Building 7 towards the Tasman Bridge	7
Figure 9	View east from former dwelling to regatta grounds (left), neighboring slip yard at 1 McVilly Drive (right)	8
Figure 10	Former south rail line (left), western entrance to Subject site (right)	8
Figure 11	View from Cenotaph (left), view from base of Memorial Bridge (right)	9
Figure 12	View from end of cycleway	9
Figure 13	View from Council land to the south east (left) and McVilly Drive in the Regatta Grounds (right)	10
Figure 14	View from jetty at Regatta grounds	10
Figure 15	Site services	11
Figure 16	Regatta Ground Car Park and cycleway upgrades	13
Figure 17	Domain Masterplan	14
Figure 18	Activity area	17
Figure 19	Places of Cultural Significance (left), Places of archaeological sensitivity (right)	21
Figure 20	Deemed to Comply Heights	23

Appendices

Appendix A	Certificates of Title
Appendix B	Wastewater treatment plan

1. Introduction

1.1 Purpose of this report

This report has been prepared for Sullivans Cove Distillery by GHD Pty Ltd to assess the proposed development at Huon Quays in relation to the relevant provisions of the *Sullivans Cove Planning Scheme 1997*. This report is to accompany a permit application to City of Hobart Council for assessment in accordance with the *Land Use Planning and Approvals Act 1993* (LUPAA).

1.2 Scope and limitations

This report: has been prepared by GHD for Sullivans Cove Distillery and may only be used and relied on by Sullivans Cove Distillery for the purpose agreed between GHD and Sullivans Cove Distillery as set out in section 1.1 of this report.

GHD otherwise disclaims responsibility to any person other than Sullivans Cove Distillery arising in connection with this report. GHD also excludes implied warranties and conditions, to the extent legally permissible.

The services undertaken by GHD in connection with preparing this report were limited to those specifically detailed in the report and are subject to the scope limitations set out in the report.

The opinions, conclusions and any recommendations in this report are based on conditions encountered and information reviewed at the date of preparation of the report. GHD has no responsibility or obligation to update this report to account for events or changes occurring subsequent to the date that the report was prepared.

The opinions, conclusions and any recommendations in this report are based on assumptions made by GHD described in this report. GHD disclaims liability arising from any of the assumptions being incorrect.

Any reports, drawings, memos, or other deliverables produced by GHD shall be produced in a traditional and generally accepted format. Accessible reports, drawings, memos, or other deliverables can be provided by GHD at an additional cost if necessary.

1.3 Project Documentation

This assessment has been prepared in relation to accompanying documentation included in the following:

- Architectural Drawings, JWA, 27/09/2021
- Architect's Design Statement, JWA, 10/09/2021
- Concept Services, GHD, 10/2021
- Traffic Impact Assessment, GHD, 13/10/2021
- Heritage Impact Assessment, Praxis Environments, 10/2021
- Landscape Site Plan, Realm Studios, 11/10/2021

2. Site Analysis

The following is an analysis of the features of the 'Subject site', it is based on a desktop assessment and a number of site visits.

2.1 Subject Site

The Subject site includes land at 'Huon Quays', in the Queens Domain, situated to the north of the Cenotaph with frontage to the River Derwent. The development is primarily located on the Huon Quays title with a number of adjoining titles to be included for access to the site.

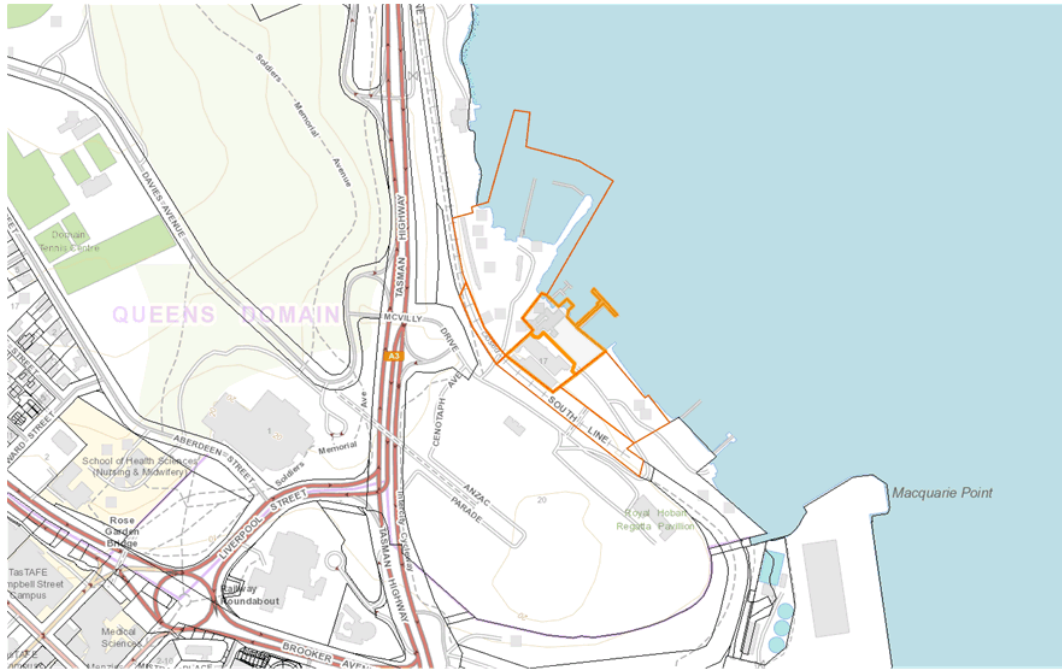


Figure 1 Site location

Base image and data from theLIST (www.thelist.tas.gov.au). © State of Tasmania.

2.2 Title

The Subject Site includes the titles as listed in Table 1 and illustrated in Figure 2. The titles are included as an appendices to this report. The proponent for this development is leasing only part of the land at 13 and 17 McVilly Drive from TasPorts.

Table 1 Project titles

Address	Certificate of Title/ Folio	Landowner/Authority
'Huon Quays', 17 McVilly Drive	134036/2	Tasmanian Ports Corporation Pty Ltd (TasPorts)
13 McVilly Drive	134036/1	
1 McVilly Drive	134037/1	
	118002/1	Crown Land / State Rail Network

Address	Certificate of Title/ Folio	Landowner/Authority
McVilly Drive	26915/3	
Cenotaph and Regatta Grounds', 20 McVilly Drive	135056/3	Local Government Authority (City of Hobart)



Figure 2 Aerial image

Base image and data from theLIST (www.thelist.tas.gov.au). © State of Tasmania.

2.3 Existing use and development

For the purposes of consistency, the layout of the 1995 *Conservation Management Plan* (CMP) has been referenced as per the image below.

Buildings on 17 McVilly site are currently vacant, gated and locked with no public access. The HMAS Huon was originally built in 1913 for the compulsory training of naval cadets and operated until 1994. TasPorts have advised the most recently Building 7, the main Drill Hall, was (informally) a function and meeting area and that there was a caretaker in residence to approximately 2019. Building 7 is the main Drill Hall and Building 8 the original residence, with recent extensions in the eastern corner including a meeting room and occasional operation as a night club.

Building 2, to the south was built for accommodation, has been used as a school room, then as a hostel. The building is currently vacant as are buildings 1, 4, 5, 13 and 16. Building 6 has been used as an office.

The site has been largely under utilised since the Drill Hall ceased operation. Limited maintenance of buildings has resulted in them falling into disrepair. On the north east edge of the site is the existing jetty. Buildings include sheds for the sea scouts and navy divers. There is an existing mast on the site in front of the tennis court.

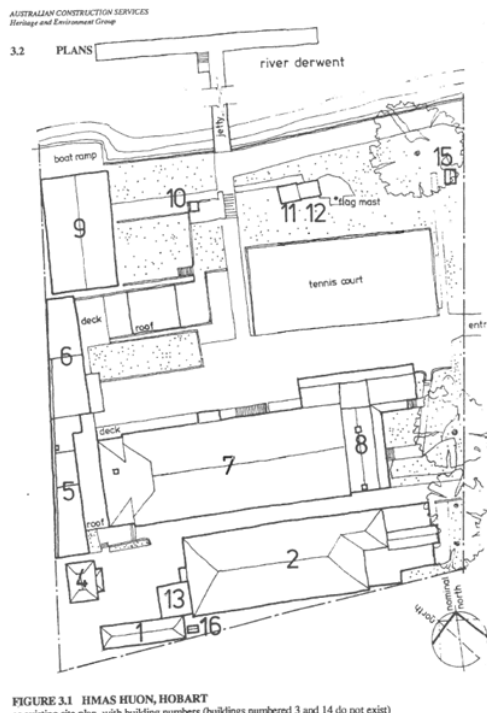


FIGURE 3.1 HMAS HUON, HOBART
as existing site plan, with building numbers (buildings numbered 3 and 14 do not exist)

Figure 3 Site Layout

Source: CMP¹

¹ Conservation Management Plan, HMAS Huon Hobart Tasmania, 1995, I. Nelsen, T. Sawyer and L. Scripps.



Figure 4 Building 7 – Drill Hall interior



Figure 5 Building 7 – northern elevation (left), and eastern elevation of former residence (right)



Figure 6 Building 4 (left) and Building 6 (right) and eastern elevations



Figure 7 Building 2 western elevation

2.3.1 Outlooks

The Subject Site has lookouts to numerous features that define Hobart and its landscape setting. The grade of the site naturally orientates northwards towards the River Derwent and the Tasman Bridge as the focal point for the land, and views towards the hills of the eastern shore. The Domain parklands are visible to the north west. To the south east are views of the Regatta Grounds. Views to the Cenotaph to the south are obscured by the rise of the land, however there are outlooks to kunanyi/Mount Wellington to the south west.



Figure 8 Outlook from Building 7 towards the Tasman Bridge

2.4 Surrounding land use and development

To the south east of the site is land that forms part of the Regatta Grounds, which is predominantly vacant grass with vehicle access and parking that is used for occasional events for the regatta, or as commuter parking and access to the Domain Boat ramp. The north eastern boundary is formed by the River Derwent.

To the south west is the former South line rail corridor, the railway has been removed and largely replaced with shared cycleway infrastructure. There is a pedestrian and services infrastructure bridge located to the south of the site, and a vehicle crossing as part of McVilly Drive to access the Regatta Grounds and the development area. To the west of the development area is a level crossing providing access to the TasPorts land.

On the other side of the rail corridor is the Cenotaph, and the associated car park and amenity building located near that part of the title located near the Subject Site. Council is currently undertaking upgrades to the car park area, inclusive of additional cycling infrastructure. The Cenotaph is an important focal point for the city as a soldiers war memorial, it is also used as overflow parking for the city at peak times such as the Saturday Salamanca Market and around the Christmas period.

To the north west of the Subject site is the Domain Slip, which is operated by TasPorts. In addition to slip infrastructure the land includes a single storey freestanding weatherboard building, which appears to be a former dwelling and is now an administrative building for the facilities.



Figure 9 View east from former dwelling to regatta grounds (left), neighboring slip yard at 1 McVilly Drive (right)



Figure 10 Former south rail line (left), western entrance to Subject site (right)



Figure 11 View from Cenotaph (left), view from base of Memorial Bridge (right)



Figure 12 View from end of cycleway



Figure 13 View from Council land to the south east (left) and McVilly Drive in the Regatta Grounds (right)



Figure 14 View from jetty at Regatta grounds

2.5 Aboriginal Heritage

A Desktop Assessment with Aboriginal Heritage Tasmania has been undertaken for 13 and 17 McVilly Drive and found that there is no known aboriginal heritage recorded within or close to the proposed works area and property, as such the AHT has no objection to the project proceeding in accordance with the requirements of an unanticipated Discovery Plan. Any Aboriginal heritage that may be found during works is subject to the provisions of the *Aboriginal Heritage Act 1975*.

2.6 Services

The Subject site has existing water and sewer connections, for which TasWater is the Authority and the application is to be referred for their consideration. A separate concept services plan for infrastructure accompanies this assessment.

City of Hobart is the Authority for stormwater, however, the stormwater infrastructure servicing the Subject site is privately owned by TasPorts and includes a 300 dia. and 275 dia. reinforced concrete pipe that discharge to the river. These services are expected to be reused as part of the development along with the inclusion of stormwater quality control measures.

There is existing infrastructure for electrical infrastructure on the western side of the Subject site.

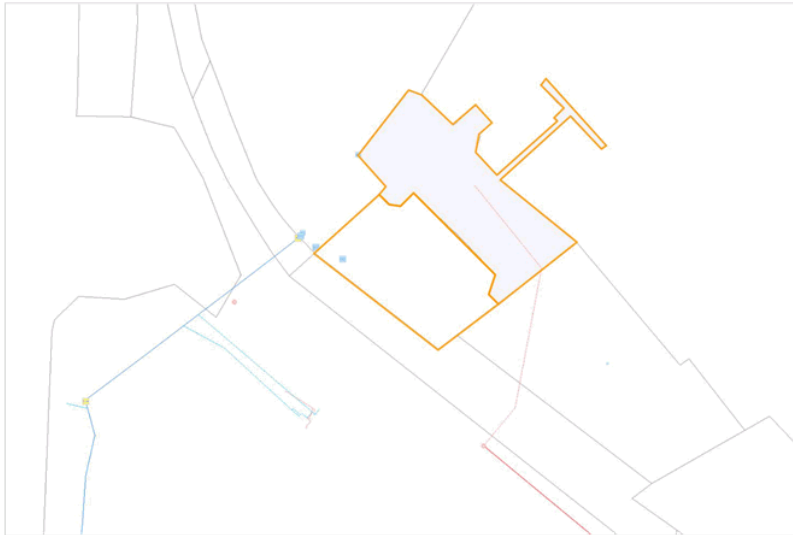


Figure 15 Site services

Base image and data from theLIST (www.thelist.tas.gov.au). © State of Tasmania.

2.7 Historic Cultural Heritage

The Subject site includes two places that are listed in the *Tasmanian Heritage Register* (as at 30 August 2019) as follows:

- ID 2,932; Former HMAS Huon Naval Depot (Huon Quays), 13 McVilly Drive Queens Domain
- ID 2,932; Former HMAS Huon Naval Depot (Huon Quays), 17 McVilly Drive Queens Domain

Approval for works on the Subject site will be required in accordance with the requirements of the *Historic Cultural Heritage Act 1995*.

The Subject site directly adjoins the listed heritage place, while this has the same address as the Regatta Grounds, it is on a separate title.

- ID 7,137 Cenotaph, Anzac Parade and Queen's Battery 20 McVilly Drive Queens Domain.

2.8 Natural values

The Subject site is largely developed and predominantly clear of vegetation. A review of threatened flora, fauna and vegetation communities only indicates one maritime species within proximity to the site, the Queens Domain, on the other side of the Tasman Highway is the location of multiple threatened flora. From a visit to the Subject site

the majority of plantings are introduced species and it is not considered necessary for further investigations into natural values.

2.9 Movement Network

Pedestrian access to the Subject site is limited with fencing restricting movement around the proposed development area. Access to the sea scouts is along the waters edge. There is an existing footbridge over the rail corridor. In the western corner the level crossing provides shared pedestrian access through the right of way over the Domain slip land with potential connections to the Intercity Cycleway. Further connections are facilitated to Domain across the Bridge of Remembrance.

Cycle infrastructure directly adjoins the Subject site with connections to the Intercity cycleway and to the Macquarie Point cycleway with linking infrastructure currently under construction at the Cenotaph car park.

The closest public transport infrastructure is located around 300 m away on the Tasman Highway servicing south bound traffic only. Prior to COVID a Salamanca Shuttle would operate that provided transport from Cenotaph car park to the Salamanca markets on Saturday. There have been investigations into the potential for the former rail corridor to be adapted to be used as a passenger rail servicing the northern suburbs.

There is on-site parking on the former tennis court on Subject site, which is fenced off from public use. This is accessed from the extension of McVilly Drive which accesses the boat ramp at the regatta ground. There is currently public car parking on the waterfront at the regatta ground, and upgrades to the Cenotaph car park. The existing road to the regatta ground is single lane.

There is vehicle access to the western side of the Subject site across the TasPorts land with a small area for on-site parking, which is currently fenced off.

2.10 Domain Masterplan

Council is currently constructing upgrades to the adjoining car park on the Cenotaph as shown in Figure 16. These are part of broader plans for the adjoining area identified as part of the Domain Masterplan. The Domain Masterplan (refer Figure 17) encourages transit facilities to the south east of the Subject site, with greater connectivity between alternative modes of transport, including connecting to future ferry infrastructure. The foreshore of the Regatta Grounds is intended as a public promenade with potential for future 'Mixed maritime commercial / cafe / boating facilities and provision for club facilities and equipment storage' or 'Mixed commercial / public toilets / cafe / information' on the vacant land on the hill behind.



Figure 16 Regatta Ground Car Park and cycleway upgrades²

² Hobart City Council, accessed 04/08/2021: < <https://www.hobartcity.com.au/Projects/Current-projects/Regatta-Ground-car-park-and-cycleway-upgrade>>

Figure 17 Domain Masterplan³³ Queens Domain Masterplan 2013-2033, 2013, Hobart City Council

3. Proposed Development

The "Project" requires demolition of a number of buildings on the Subject site, and internal works within the former Drill Hall. The focus of the new development is for the production of whisky and associated tasting and visitor experience. The design layout reflects this with the bulk of the building works including the tasting venue in the historic Drill Hall, and the distillery on its south western side in the Production Hall. The ground floor of the Drill Hall is proposed to be reconfigured for access, amenities, kitchen and office facilities. Smaller rooms of the former residence and former Drill Hall are to be used as private tasting rooms, office, retail and library of former whisky releases.

The arrangement of the ground floor provides connectivity to accessible parking to the north east of the building. A new staircase and lift provides a visual and physical connection between the different programs of the Subject site and varying floor levels.

The Drill Hall has been configured to enable a flexible arrangement for large groups attending for tastings and for display of previous releases. A substantial corridor is located on the south west of the building that distinguishes the tasting venue from the Production Hall and its activities by providing a physical separation, while still enable a visual connection between the spaces. The spatial configuration is designed to contribute to the visitor experience and understanding of the whisky process and final product.

The Production Hall is configured for the practical and regulatory requirements to produce whisky and also to enable tours of the facility. The overall height of the new building has been determined by the spatial requirements of the stills and fermenters. To the north east is an external yard to be used for loading facilities and as part of the production facilities and is primarily a service area that will be fenced from the adjoining land.

Materials and finishes are as described in the accompanying Architect's Design Statement (p 5). The existing Drill Hall is to be repainted externally in the original colour. The Production Hall is to be constructed of galvanised sheet metal; external walls to the east and west are locally unfinished timber with areas of glazing; and roofing is to be in corrugated sheet.

A landscaping plan has been prepared that manages the level changes and improves accessibility to buildings. The design has been prepared with respect to the cultural heritage of the Subject site for both Indigenous and European heritage. Building 4 has been partially re-used as a feature within the landscape and to retain the architectural character. Accessible parking has been located in the Drill Hall Forecourt. A pathway with lighting has been proposed to connect the frontage of the building to the existing pedestrian bridge over the rail corridor.

Vehicle access to the Subject site has been designed to meet the operational requirements of the distillery, with loading facilities in the service area meeting the manoeuvring requirements of the anticipated vehicles; and accessible requirements for visitors through accessible spaces in the Drill Hall Forecourt and the turning circles for drop off facilities for buses for 50 visitors to the Subject site. A bike shed has been provided on site to encourage modal share for transport.

Works are proposed on land owned by Crown and Council to improve pedestrian accessibility to the Subject site. On-site parking has been minimised to improve landscape and recreation outcomes, and appreciation of the cultural heritage setting.

3.1 Consultation

Pre-lodgement consultations have been undertaken with the following agencies, recommendations and suggestions have been adopted where possible: Heritage Tasmania; Hobart City Council planning, heritage, and parks and city amenity officers, TasRail; and Naval Cadets, as occupants of neighbouring boat sheds.

Appendix B includes information relevant to the anticipated wastewater to be generated by the development. This has been prepared as part of early consultation with TasWater.

4. Scheme Assessment

The following is an assessment of the Project with respect to the provisions of the *Sullivans Cove Planning Scheme 1997* (SCPS).

4.1 Strategic Framework

Section 4.11 Key Sites requires that development must reflect the provisions of the Strategic Framework (Part B) and the Objectives and Performance Criteria of the relevant Activity Area (Part D) and satisfy the Objectives and provisions of the Schedules (Part E) of the Scheme.

Summary of provisions	Development Response
Part B – Sullivans Cove Strategic Framework	
5 Values and Strengths of the Cove Sullivan's Cove is <ul style="list-style-type: none"> – is one of the world's finest city landscape settings – Australia's most intact historic waterfront – True dynamic and evolving working port – A place for people – A cultural, artistic and festive focus – Excellent transport links 	The Project is consistent with the description of the values and strengths of the Cove. The reuse of the long term vacant historic building will provide much needed maintenance and care of the building and significantly contribute to the revitalisation of this part of the cove. The Project will activate this part of the Subject site with employment, activities for visitors and residents alike, without compromising the existing port activities occurring on the coastal edge. Development and use are consistent with the scheme and is also complementary to implementing the Council's endorsed vision for the adjacent Domain Masterplan (refer section 2.10)
6 A preferred future for the cove	
6.1 An Economic Base for the Future	The Project utilises an existing developed site that is no longer needed for the historic purpose of training cadets and utilises this for the purposes of commercial industry and visitor experience. The established Sullivans Cove Whisky brand is synonymous with the location and is complementary to the commercial identity of the Cove more broadly.
6.2 Designing the Future Urban Form	The Project has been designed with a core focus on the historic values of the existing buildings and connection to the landscape setting of the Derwent River.
6.3 An Ecologically Sustainable Future	New buildings and works to the heritage building are to be constructed to or above contemporary standards for environmental performance and services infrastructure
6.4 Creating a Place for People	The Subject site is currently largely vacant and has previously been used for anti-social and illegal activities. The activation of the land will contribute to the revitalisation of the northern part of the Cove and options for the visitor experience. The project edge will improve the continuity of visitor and local experience along the recent connection of the cycleway and improve safety with greater number of activities.
6.5 Arts/Culture/Education/Recreation	Tasmania is recognised for its high value and grade produce. Whisky and learning about its production will contribute to the opportunities and experiences available in the Cove.
6.6 Identification of 'Key Sites'	The Subject site is a key site. The Project is consistent with realising the 'potential to further develop the economic base, pedestrian environment and cultural/recreational and educational profile of the Cove.'
7 PLANNING PRINCIPLES FOR MANAGEMENT OF ACTIVITIES IN THE COVE	The principles of the scheme are intended to achieve the 'future of the cove' and are implemented through the provisions of the Scheme. The proposal is consistent with the principles of Cultural resource, Natural resource, Efficiency, Economic development, and People in the cove, as demonstrated in the responses in the table above and in response to applicable provisions.

Summary of provisions	Development Response
	The accompanying Heritage Impact Assessment (HIA) has specifically responded to 7.3.2 Cultural Resource Principles (page 93).

4.2 Activity Area

The Subject site is located within Activity Area 4.2 'Regatta Point'. Land on the other side of the rail line is within the Activity Area 2.1 'Domain Open Space'

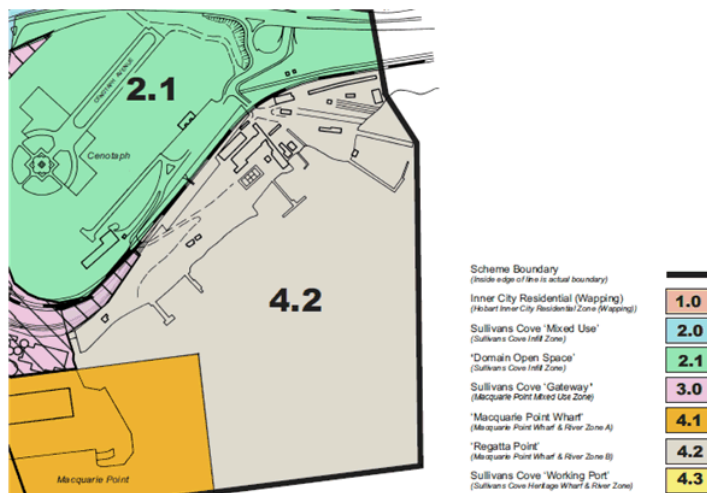


Figure 18 Activity area

Source: Sullivans Cove Planning Scheme

4.2.1 Objectives and Performance Criteria

The following is a response to section 20.2, which are the objectives and performance criteria for the Activity Area.

Scheme Objective	Development Response
Primary Objective: To provide for port facilities for ships, shipping and incidental uses taking into account the cultural and visual aspects of the areas as a main road entry to the City of Hobart, the need to accommodate appropriate public access, water-based activities and Regattas.	
The use and development of the area for ships, shipping and incidental uses may be supported only if the use and development, including the development of structures (wharves, piers, moorings etc) is in keeping with the existing character, scale and context of the existing built and spatial form of the area.	The proposed uses are not for shipping related uses. TasPorts is to retain management over those parts of the Subject site that are used for shipping related purposes and no change is proposed to their associated development.
Such use and development must also demonstrate its compatibility with the following: <ul style="list-style-type: none"> - Cultural heritage of the area. - Landscape and aesthetic values of the area. - Visual identity of the area, as viewed from other parts of the Cove Planning Area, and across the Derwent Estuary. - Existing and potential water-based activities. - The need to maintain public access to land and water. 	As per response above the configuration of the Subject site enables waterfront to be retained that is adequate for future use and development compatible with the proposed use and development. The proposed use and development provide an adaptive reuse of the land that transitions between the industrial activities of the adjacent shipyard, the active community uses of the Cenotaph and Regatta Grounds, while preserving the integrity of the heritage values of existing buildings.

Scheme Objective	Development Response
<ul style="list-style-type: none"> - Other activities, including recreation and Regatta Day and other festivals. - 'Best practice' environmental design and environmental management. - The management objectives of vehicle movement and access. - The provisions of any Outline Development Plan for the Activity Area that may be considered necessary to resolve potential land use and management issues. 	
The objectives and recommendations of the Queens Domain Management Plan (1996) must also be taken into consideration in determining the appropriateness of such use.	The findings of the Management Plan have been adopted in the more recently endorsed Domain Masterplan outlined in section 2.10. The proposed use and development are complementary to the intentions for the neighbouring land within the Masterplan.
Supporting Objectives	
(a) To protect the aesthetic and landscape values of the area, particularly as viewed from the Derwent Estuary.	
All use and development must be consistent with, and where possible, enhance the character of the area, including the character and form of existing buildings and structures, topography and landscape features, particularly as viewed from across the Derwent Estuary and Regatta Grounds.	<p>The Subject site was formerly used as a cadet Drill Hall and visitor accommodation and has been largely vacant for a substantial amount of time with the exception of occasional events. The proposed uses will significantly improve the character of the area.</p> <p>The Project is not significantly different from the form of existing buildings, other than to improve the appreciation of the historic heritage of the Drill Hall.</p> <p>Montages (refer architectural drawing DA0603) have been prepared of the proposal from the Regatta Ground demonstrating how changes have enhanced and improved the historic frontage of the former residence.</p> <p>The proposal will improve the visibility and qualities of the heritage buildings when viewed across the Derwent Estuary (refer architectural drawing DA0604).</p>
Development must be compatible with the scale of existing built form (height, bulk and volume).	<p>The maximum height of the existing Drill Hall is: 12.45 m</p> <p>The maximum height of the proposed Production Hall is: 11.0 m</p> <p>The height of the new building works for the Production Hall have been kept at the minimum height possible to enable circulation with the existing Drill Hall and to accommodate the functional requirements of the necessary equipment.</p> <p>The bulk and volume of the new building is comparatively similar with the existing Drill Hall and, taking into account the topography of the land site the designed to focus attention to the historic facades of the Drill Hall.</p>
Development must retain any buildings, structures, spaces or landscape features of identified significance.	As discussed, section 4.3.1 in relation to the Places of Cultural Significance and section 4.9 for Demolition, investigations have been undertaken to ensure that features of significance are retained.
Development must not significantly alter the topography of the area.	Some cutting is required to accommodate the Production Hall. This is not considered to be a significant change to the topography.
(b) To conserve and enhance the cultural heritage values of the area, including building, spaces, cultural events (e.g., Regatta Day) and archaeological heritage.	
All use and development must be compatible with, conserve, and where possible, enhance the cultural heritage of the area.	In the accompanying Heritage Impact Assessment historic heritage values of the Subject site have been assessed in relation to the provisions of Schedule 1 – Conservation of Cultural Heritage Values.

Scheme Objective	Development Response
Where possible, use and development must incorporate appropriate interpretation of such heritage.	The Project has been prepared to facilitate the restoration of the building form and Subject site to its 1912's form in accordance with the provisions of Schedule 1.
Activities in the area must not restrict or adversely impact upon culturally important events in the area.	The Subject site adjoins the Regatta Grounds and the Cenotaph, which hold culturally important events. The Project will not adversely impact on these events, rather they will provide greater activation in the area.
(c) To maintain and improve public access to the area, particularly the Regatta grounds and water's edge.	
Activities must not restrict public access to the area, particularly to the water and the Regatta Grounds.	The Project will not restrict public access, pedestrian circulation will be encouraged, and the occupation of the building will increase the activation of the area.
Activities must incorporate features to facilitate public access to, and use of, the area.	
Activities which will promote public access to the area are encouraged.	
Commercial activities such as restaurants and function centres will generally only be supported where it is clearly demonstrated that such use will allow and encourage unrestricted public use of the area.	<p>The proposal is primarily as a distillery and associated visitor experience.</p> <p>The scale of the Drill Hall and associated forecourt lends itself to entertainment and events and flexibility to enable functions and dining to occur would improve the activation of the area complimentary to public events occurring in the area.</p>
(d) To encourage a range of cultural and community activities, including festivals and Civic functions.	
Cultural and community uses will be encouraged in the area.	Improvements to the building would enable it to be used for occasional events complimentary to those that occur in the area, including the Regatta, Dark Mofo, ANZAC Day service etc.
(e) To encourage small scale maritime activities and compatible port facilities.	
Enhancement of port facilities and use of the area for maritime activities which do not adversely impact upon public access, amenity, cultural heritage and other identified values are encouraged. Such uses might include slip-yards, boat repair and other small scale maritime industries, and limited mooring of recreational and commercial fishing craft.	This Project does not include any alterations to existing maritime and port activities.
(f) To ensure sound environmental planning and management for all activities.	
All use and development to demonstrate the minimisation of on and off-site energy requirements resulting from the proposed activity.	The proposed works will result in the building and associated services infrastructure are upgraded to meet contemporary building design standards. The viability of production facilities will be dependent on ensuring energy is managed as efficiently as possible.
All use and development must minimise direct and indirect environmental risk or effects, and where possible, provide a net environmental gain for the wider environment.	

4.2.2 Use of Land

Those parts of the Subject site that form part of the Project are predominantly vacant, with historic buildings being originally constructed as a Drill Hall and residence and Building 2 most recently used as a backpackers. The Drill Hall has fallen variously into the definitions of a 'Community Centre' and 'Education Centre' over time, which would be permitted uses. Residential and visitor accommodation would be discretionary uses.

Table 2 Proposed Uses

Proposed Use	Definition	Status and Condition
Whisky Distillery	<p>Light Industry; Means the use of buildings or spaces for manufacturing, processing or storage of activities which do not:</p> <p>(a) cause a nuisance as defined in s.180 of the Local Government (Building and Miscellaneous Provisions) Act 1993; and</p>	Discretionary. S 180 of LGBMP is repealed.

Proposed Use	Definition	Status and Condition
	(b) cause "environmental harm" as defined in s.5 of the Environmental Management and Pollution Control Act 1994; and (c) create noise at the boundary of a residential zone which exceeds 40 dBA between the hours of 7:00 am and 8:00 pm, and 35 dBA between the hours of 8:00 pm and 7:00 am; and (d) require the on-street parking, loading, unloading or maneuvering of vehicles associated with the operation; and (e) adversely affect the amenity of nearby dwellings through vibration, dust, grit, smoke fumes, smell, bright lights, electrical interference, traffic generation or unsightly appearance; and (f) require the erection of any buildings or structures that are not in accordance with the character of the Planning Area; and (g) require the storage of goods or materials in any location visible from any residence, public street or public place.	It is not anticipated that the industry will result in Environmental Harm.
Hospitality – including tours, sales, function facilities	Eating Establishment , Land used to prepare and sell food for consumption on, or off, the premises. It also includes a Restaurant/Cafe, Take Away Food Premises and Convenience Restaurant.	Discretionary – Maximum gross leasable floor area of 200 square metres.
	Function Centre : land used, by arrangement, to cater for private functions, and in which food and drink may be served, such as a conference centre or reception centre. It may include entertainment and dancing.	Discretionary
	Hotel : Land used primarily to sell liquor for consumption on and off the premises. Additionally It may include accommodation, food for consumption on the premises, entertainment, dancing, amusement machines, and gambling.	Discretionary
	Shop Land used to sell goods or services, or to hire goods, but does not include Convenience Shop, Manufacturing Sales, Market, Landscape gardening supplies, Motor vehicle, boat, or caravan sales, Postal agency, Primary produce sales, or Trade supplies.	Discretionary – Maximum gross leasable floor area of 200 square metres.
Ancillary – Administration, amenities	Office	Discretionary

As demonstrated in discussions in relation to the Strategic Framework of the Scheme (refer section 4.1); to the response to the Objectives and Performance Criteria of the Activity Area (refer section 4.2.1); and the following responses in relation to the Schedules of the Scheme, the proposed discretionary use is an appropriate use for the Activity Area in accordance with 13.4 of the Scheme. Particularly as the Subject site is identified in Part F – Key Sites as a 'strategically important and presently under-utilised site within the Cove'.

4.3 Conservation of Cultural Heritage Values

Schedule 1 – Conservation of Cultural Heritage Values includes the Subject site as Place of Cultural Significance no 122. The land on the western side of the Subject site is partially shown as within the Place of Archaeology Sensitivity, no 88 as shown in the following figure.

The accompanying Heritage Impact Assessment (HIA) includes an assessment of the Project with respect to the Planning Principles of the Cove.

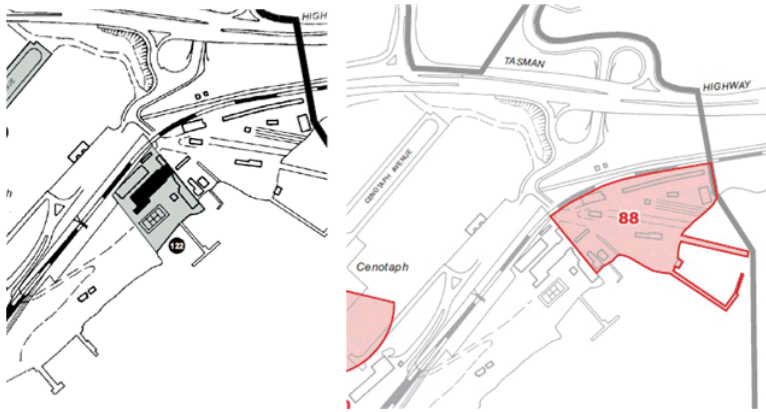


Figure 19 Places of Cultural Significance (left), Places of archaeological sensitivity (right)

Source: figure 5 & 5a SCPS.

4.3.1 Conservation of Places of Cultural Significance

A Conservation Management Plan exists for the Subject site and has been referred to in the accompanying HIA. The Project was prepared with respect to findings of an initial preliminary heritage assessment and ongoing consultation with the project heritage consultant. Responses in the table below are included in Section 7 of the HIA.

Scheme Provision	Development Response
<p>22.4.4 'Permitted' 'Building or Works'</p> <p>'Building or works' on places of cultural significance is 'permitted' in respect to this Schedule where it can be demonstrated that the following 'deemed to comply' provisions have been satisfied:</p> <p>The 'building or works' are related to the conservation of a place of cultural significance and are to be undertaken in accordance with a Conservation Plan accepted by the Planning Authority as satisfying the submission requirements for an application.</p>	<p>Proposed works are not wholly for the conservation of the heritage place and therefore the proposal is required to be assessed as discretionary in accordance with 22.4.5.</p>
<p>22.4.5 'Discretionary' 'Building or Works'</p> <p>'Building or works' on places of cultural significance which cannot satisfy the 'deemed to comply' provisions of Clause 22.4.4 may be approved at the discretion of the Planning Authority.</p> <p>The following criteria must be taken into consideration in the assessment of all proposals to undertake 'building or works' on places of cultural significance:</p>	
<p>'Building or works' must complement and contribute to the cultural significance, character and appearance of the place and its setting;</p>	<p>The accompanying HIA finds that the proposal complies with this part, with further discussion to specific policies in Section 6.1-6.3.</p>
<p>'Building or works' must be in compliance with the conservation strategy of an approved Conservation Plan, where required and/or provided;</p>	<p>The Subject site is the subject of a Conservation Management Plan (CMP). The HIA finds that the project complies with the policies of the CMP and is discussed further in sections 6.4-6.7</p>
<p>The location, bulk and appearance of 'building or works' must not adversely affect the heritage values of any place of cultural significance;</p>	<p>The accompanying HIA finds that the proposal complies with this part, with further discussion to specific policies in Section 6.1-6.7.</p> <p>Section 6.3 finds <i>'that the overall mass and form of the proposed production hall allows the (restored) Drill Hall and residence to remain as the dominant site feature, assisted particularly through the removal of other non-significant site features and vegetation and that the new building would be a suitably discrete backdrop element.'</i></p>

Scheme Provision	Development Response
'Building or works' must not reduce the apparent authenticity of places of cultural significance by mimicking historic forms;	The accompanying HIA finds that the proposal complies with this part, with further discussion to specific policies in Section 6.2-6.7.
'Building or works' may be recognisable as new but must not be individually prominent;	
The painting of previously unpainted surfaces is discouraged.	The Proposal does not include painting of any previously unpainted surfaces.

4.4 Urban Form

The schedule applies to all new buildings and extensions. The following also applies:

The provisions of Schedule 1 - Conservation of Cultural Heritage Values, take precedence over provisions of this Schedule. Height and scale of detailing of development adjacent to Places of Cultural Significance must be such that the place is not compromised by height discrepancies, scale discrepancies or a superficially "historic" or similar appearance.

The site is listed in Schedule 1 and responses in section 4.3, take precedence.

Scheme Provision	Development Response
23.2 Objectives	
The following objectives apply to the application of this Schedule:	
The traditional urban pattern of Sullivans Cove is to be conserved. A contemporary adaptation is to be created in development/redevelopment areas.	The Project does not alter the pattern of the development of the Subject site with the Drill Hall remaining the key feature of development. The alignment of the frontage of the Production Hall providing a contemporary response to the historic setting.
Views to Sullivans Cove along primary spaces are to be retained, especially to the River Derwent.	Not applicable. The Subject site does not include any defined primary spaces.
Views over the land bounded by Tasman Highway, Brooker Avenue and Liverpool Street from the City and Wapping to the Domain and from the Domain and Tasman Highway to the City are to be retained.	Not applicable. The land is not within the described area.
Expression of the Wall of the Cove is to be encouraged where possible.	Not applicable. The Subject site is not located within proximity to the Wall of the Cove.
The bulk and height of buildings must reflect the natural topography of the Sullivans Cove Planning Area, the amphitheatre sloping down to the Cove and the Macquarie Street and Regatta Point Ridges.	While not located within the amphitheatre of Sullivans Cove the Project reinforces the topography of the land with the height of the buildings stepping down towards the water's edge.
A diversity of building heights and volumes will be encouraged within this over-riding pattern, but buildings must have a respectful relationship to each other and to buildings of identified cultural significance within a street.	A new building is proposed with a maximum height that is less than that of the maximum height of the historic building on the Subject site. The Production Hall is set into the topography of the Subject site and as discussed in relation to section 4.3 has been prepared with regard to ensuring a respectful relationship with the former Drill Hall. The Project is located within proximity to the Cenotaph, the profile of the building is cut into the hill side and is located where it will not impact on the cultural significance. As can be seen in Figure 11, the Subject site is largely obscured when seen from the Cenotaph.
New buildings must not be individually prominent in terms of contrast with neighbouring buildings by being significantly higher or having a larger apparent size when viewed in street elevation.	The new building is located where the former accommodation building was situated and will not result in significantly greater impact than the building it replaces.
New buildings should facilitate the creation of 'secondary spaces' on lots in the Cove.	The Project will provide landscaped treatment of the Subject site to reinvigorate existing areas that could be considered

Scheme Provision	Development Response
	as 'secondary space' and will provide greater activation of this part of Sullivans Cove.
Such spaces should be encouraged where they demonstrably create useable pedestrian environments and facilitate pedestrian movement and views.	The Project will facilitate improvements for the pedestrian environment that are integrated with landscaping and the new and historic building elements.
New urban gardens are to be encouraged in secondary spaces only.	The Project will enable improvements to existing urban gardens.
On the land bounded by the Tasman Highway, Brooker Avenue and Liverpool Street the landscaping should reflect the variety of garden areas and parkland styles that exist in the immediate surrounding area and that mark the transition to the Domain.	Not applicable. The Subject site is not located within the defined area.

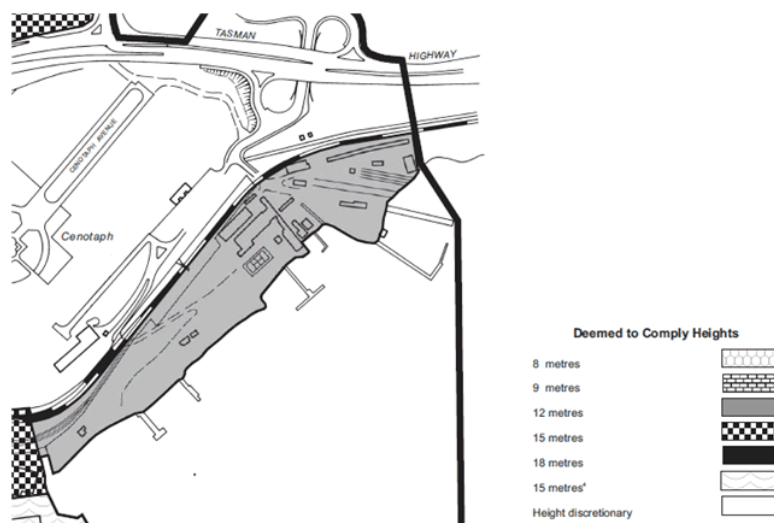


Figure 20 Deemed to Comply Heights

4.4.1 Building Form

Scheme Provision	Development Response
23.6.1 'Permitted' Buildings Development is 'permitted' where it can be demonstrated that the following 'deemed to comply' provisions have been satisfied:	
23.6.1A New Buildings	
Height: Buildings to the maximum height for the area shown on Figure 8. Parts of buildings above building eaves or parapets must reinforce the Objectives of this Schedule.	The new Production Hall building is 11.0 m, which is less than the specified height of 12 m, and the height of the existing Drill Hall. The Project meets with the Permitted requirements for Height.
Alignment – Primary Space: Buildings must be built to the street line of all primary street frontages, with walls located on the front property boundary and extending across no less than 90% of the primary street frontage.	Not applicable. The Subject site is not located on a primary street frontage or adjacent to a primary space.

Scheme Provision	Development Response
Where a new building is located on a corner with two primary street frontages, this requirement must be satisfied for each frontage. New buildings must not step back adjacent to a Primary Space. The only permissible exceptions to this is in situations where the stepping back is less than 1:20 relative to the height of the wall on the property boundary and where there is a wall to the boundary at least 12 m high.	
Alignment – Secondary Space: Where applicable, buildings must also be built to, or align with, the secondary street frontage.	Not applicable. The Subject site is not located on or near a secondary space.
Plot Ratio: Buildings must comply with the plot ratio standards referred to in Table A to this Schedule.	The applicable plot ratio is 2.5. Demolition ~ 325 m ² Additional floor area ~570 m ² Total floor area as part of this Project ~ 1,330 m ² Total site area = 6,871 m ² Inclusive of existing site sheds the plot ratio is still substantially less than as per Table A The Project meets with the Permitted requirements for Plot Ratio.
Apparent Size: The length of buildings in street edge elevation must not be more than twice the width of the abutting street.	Street is not defined within the Scheme. The proposal only has a frontage to the rail corridor to the south and to Council land to the east. The proposal does not substantially change the eastern elevation from existing and complies with requirements for development to a Place of Cultural Significance. The Project meets with the Permitted requirements for Apparent Size.
Urban Gardens: Where the construction of a building results in the creation of secondary spaces with public access. These spaces must be fenced at street frontage, landscaped and include facilities for pedestrians as appropriate, such as seating	The Subject site will still be managed privately. Landscaping will be improved on the south eastern boundary and improve amenity for visitors. The Project meets with the Permitted requirements for Urban Gardens.
23.6.1B Extensions to Existing Buildings The provisions of Clause 23.6.1.A also apply to the extension of existing buildings. The provisions in respect to Alignment-Primary Space, Alignment-Secondary Space and Apparent Size only apply where extensions are proposed on or near a lot boundary which abuts a street or other public space.	
	The changes to the existing building will not impact on the responses to alignment or apparent sizes.

4.4.2 Building Surfaces

Scheme Provision	Development Response
23.7.1 'Permitted' Buildings Development is 'permitted' where it can be demonstrated that the following 'deemed to comply' provisions have been satisfied:	
Building façade to a Primary Space	Not Applicable
Building Façade to a Secondary Space	Not Applicable
Night-Lighting Must accentuate the wall of the building when illuminated, and where appropriate also highlight the landscaping.	Lighting has been proposed for pedestrian access to the Subject site to improve safety and highlight the entrance to the buildings. Any further external lighting will be finalised during detailed design to meet the permitted standards.
Building surface to Brooker Avenue and Tasman Highway in Activity Area 2.0	Not Applicable

Scheme Provision	Development Response
<p>Surfaces adjacent to nectar bearing native flora (Tasmanian and exotic eucalyptus, etc).</p> <p>The configuration of buildings and any adjacent nectar bearing native flora must be designed to prevent bird collisions caused by the reflection of such vegetation or sky in glazing and/or unobstructed views through a surface to an outdoor space. Glazing must be consistent with the DPIWE Threatened Species Unit publication "Prevent window collisions: Save our native birds" or achieve comparable performance.</p>	<p>The Subject site is not situated in proximity to native flora community.</p>

4.5 Public Urban Space

This schedule applies to the proposed works to provide civic works for pedestrian connections across public land.

The Subject site is located within the Derwent Shore and Footpath works are 'as of right'.

4.6 Signs

No signage is proposed as part of this permit application.

4.7 Traffic, access and parking

A detailed Traffic Impact Assessment (TIA) has been prepared for the Project, the responses which are also included below.

Scheme Provision	Development Response
<p>26.2 Principles</p> <p>The following Principles of development control shall apply to the assessment of traffic, access and parking within Sullivans Cove:</p>	
<p>(a) It will facilitate the mutual compatibility of public and private transport;</p>	<p>The Project provides for careful consideration of essential private vehicles required on the Subject site and safe and efficient access. Where possible alternative transport options are recommended to improve transport outcomes for the Subject site and surrounds.</p> <p>From the TIA:</p> <p><i>Clause considered to be satisfied based on the following:</i></p> <ul style="list-style-type: none"> – No loss of connection to bus stops via the Intercity Cycleway. – Limited on-site parking discourages private car use and encourages use of active transport modes. <p><i>Travel by walking and cycling has a smaller footprint and greater compatibility with public transport.</i></p>
<p>(b) It can demonstrate either:</p> <p>(c) that it will not create traffic flows and movements that are detrimental to safety or amenity; or</p> <p>(d) that any such flows or movements can be accommodated through engineering works or traffic management arrangements – on or off site – that are satisfactory to the Planning Authority;</p>	<p>From the TIA:</p> <p><i>Clause considered to be satisfied based on the following:</i></p> <ul style="list-style-type: none"> – Proposed on-site car parking is limited to three accessible parking spaces, therefore limiting the number of vehicle movements accessing the Subject site and their impacts. – The remainder of the development site's car parking needs to be accommodated within existing adjacent car parks with existing parking movements.
<p>(e) It can make adequate provisions for the direction, access, turning and parking of all vehicular traffic;</p>	<p>Vehicle selection and layout design have been determined to allow for sufficient access, turning and parking. Swept paths for service/delivery vehicles and coach access to the Drill Forecourt are included as an appendices to the TIA.</p>

Scheme Provision	Development Response
(f) It can provide for pedestrian movement, in accordance with the requirements of the Planning Authority.	From the TIA: <ul style="list-style-type: none"> - Reduced vehicle access within Drill Forecourt due to limited parking provisions. - ~1.6-metre-wide footpath provided along main entry access road (to Drill Forecourt) and connecting existing pedestrian bridge across decommissioned rail line. - New footpath links to the site will be developed as part of the Cenotaph car park upgrade.
Unless particularly specified as a policy objective or requirement for all or part of an Activity Area or approved as part of the guidelines for the redevelopment of a Key Site, developments will not be expected to incorporate on-site vehicle parking and it will only be approved to the extent that the car parking does not direct the design of the development and does not have a detrimental effect on the form and character of Sullivans Cove or on its pedestrian amenity and activity. Any development necessitating an exercise of discretion under the Scheme in relation to height to facilitate the provision of on site parking would not be considered to be in keeping with the planning principles for the Cove.	The Subject site is a Key Site. Provision of extensive parking on-site is not a possibility without compromising the heritage values of the place or impacting on the character of Sullivans Cove. The parking configuration is also consistent with the Precinct Plan endorsed by Council in the Domain Masterplan. From the TIA: Clause considered to be satisfied based on the following: <ul style="list-style-type: none"> - The Activity Area allows for parking demands for employees, visitors, and maritime related activities. - Proposed on-site car parking is limited to three accessible parking spaces with minor impacts on the surrounding area.
Any provision for parking will aim to: (g) Encourage pedestrian and cycle access, amenity and safety; and (h) Address the essential and short-term parking demands of customers/visitors in the Cove; and (i) Discourage long term commuter parking within the Cove.	From the TIA: Clause considered to be satisfied based on the following: <ul style="list-style-type: none"> - Proposed on-site car parking is limited to three accessible parking spaces, therefore limiting the number of vehicle movements accessing the site and their interactions with pedestrians and cyclists. - On-site bicycle parking and development site proximity to the Intercity Cycleway supports bicycle and pedestrian access. - Pick-up/drop-off area for coach access encourages high occupancy vehicles and reduces single-occupancy parking demand. - The development will not attract commuter parking due to no proposed staff parking on-site.
26.3 Objectives for Traffic, Access and Parking in Activity Areas	
'Regatta Point' Activity Area 4.2 This Activity Area is characterised by low volumes of traffic. Parking shall be provided in this Activity Area to cater for the parking demands of employees, visitors and other maritime related activities. Land within this Activity Area will not be used for the parking demands of activities outside the Activity Area.	Clause considered to be satisfied based on the following from the TIA: <ul style="list-style-type: none"> - Low volume of vehicular traffic accessing the development driven by the deliberate limitations put on on-site parking provisions. - Parking demands of employees and visitors reduced by the proposal's encouragement of alternative transport modes. - Accessible (DDA compliant) parking is provided to meet the mobility needs of visitors. - No parking is provided that might be utilised for parking demands outside of the activity area.

4.7.1 Development Control

As per the 26.1 Introduction, car parking is not required to be provided on-site for use or development. The Subject site has access to ample car parking at the Regatta Grounds Car Park on the Cenotaph (refer Figure 16), which provides turning circles and occasional parking to the area. Connectivity for access to the Subject site is provided to the cycleway and pedestrian infrastructure. On-site car parking is a reduction of existing numbers and is to meet the practical requirements for loading areas and for accessibility requirements.

Scheme Provision	Development Response												
26.4.1 Traffic Generation													
<p>Where a development (including subdivision) is approved that involves the provision of onsite car parking, servicing or emergency access facilities that will result in a material increase in the volume of vehicular traffic entering or leaving a public road, then provision shall be made to accommodate such additional vehicles and their movement to the satisfaction of the Planning Authority, having regard to traffic safety or amenity as appropriate. Similar provision shall be made for pedestrians.</p> <p>Any development needing or expected to generate a demand for the delivery of people to the site in relatively significant volumes, will be expected to make appropriate provision for accommodating the associated vehicular movement - whether generated by private, public or tourist transport, in a manner consistent with the principles of this schedule.</p> <p>The location and form of any proposed access to an existing public road shall ensure that adequate sight distance in relation to the speed of through traffic is available.</p> <p>The Planning Authority may require certain measures to be carried out at the developer's expense in order to accommodate additional vehicles or pedestrians resulting from an approved development.</p> <p>All access, parking and traffic management works shall be constructed to the Planning Authority's current standards and in accordance with plans approved by the Planning Authority.</p>	<p>Current parking on site is informal and has not been designed to meet with any applicable standards, with most parking situated on a vacated tennis court.</p> <p>From the TIA:</p> <p><i>Clause considered to be satisfied based on the following:</i></p> <ul style="list-style-type: none">On-site car parking is limited to three accessible parking spaces, therefore limiting vehicular interaction with pedestrians in the surrounds.Pedestrian access is proposed via the main entry to Drill Forecourt. A footpath is provided at this access to support the volume increase in pedestrians.Distribution of generated people/vehicle trips amongst the various modes of travel, particularly walking, and cycling, reduces the impact of trips by any given mode of travel.Accesses to the proposed development site are existing, and no new vehicular accesses are proposed.												
26.4.2 Access Requirements													
<p>Unless existing buildings, topography, the requirements of the Disabilities Discrimination Act 1995 or heritage considerations dictate otherwise the maximum vehicle access widths and minimum footway widths shall be as set out below:</p> <table><tr><th>Number of Parking Spaces to be Provided on Site</th><th>Maximum Allowable Vehicular Access Width</th><th>Minimum Number & Width of Footways</th></tr><tr><td>1-5 inclusive</td><td>One 3.0m lane</td><td>One 0.6m</td></tr><tr><td>6-20 inclusive</td><td>Two 2.5m lanes</td><td>One 1.0m</td></tr><tr><td>21 and over</td><td>Two 3.0m lanes</td><td>Two 1.5m</td></tr></table>	Number of Parking Spaces to be Provided on Site	Maximum Allowable Vehicular Access Width	Minimum Number & Width of Footways	1-5 inclusive	One 3.0m lane	One 0.6m	6-20 inclusive	Two 2.5m lanes	One 1.0m	21 and over	Two 3.0m lanes	Two 1.5m	<p>From the TIA:</p> <p><i>Clause considered to be satisfied based on the following:</i></p> <ul style="list-style-type: none">A ~1.6-metre-wide footpath is provided at Access 2.A ~7.2-metre-wide access is provided at Access 1 and a ~4.5-metre-wide access is provided at Access 2. Wider access width required to accommodate larger vehicles including buses/coaches and MRVs.
Number of Parking Spaces to be Provided on Site	Maximum Allowable Vehicular Access Width	Minimum Number & Width of Footways											
1-5 inclusive	One 3.0m lane	One 0.6m											
6-20 inclusive	Two 2.5m lanes	One 1.0m											
21 and over	Two 3.0m lanes	Two 1.5m											
<p>The Planning Authority, in pursuance of the intent of the Planning Scheme, and in particular Clause 26.1 and the Principles of this schedule (Clause 26.2), may for any development require:</p> <p>The deletion of any vehicular access or otherwise limit or accept a reduction in the number of accesses or require different dimensions of access widths; and</p> <p>The provision or deletion of any-footway and/or a variation in their dimensions.</p>	Not Applicable.												
<p>Where appropriate, access and parking areas shall enable vehicles to enter and leave the site in a forward direction</p>	<p>As demonstrated in the swept paths and response included in the TIA the clause considered to be satisfied as the manoeuvring areas are designed to accommodate proposed vehicles.</p>												
26.4.3 Parking Standards													
<p>The number of parking spaces which can be accommodated in a car parking area and the aisle widths within it, shall comply with the relevant Australian Standard, unless otherwise determined by the Planning Authority having regard to the impact on the site and its environs.</p>	<p>Parking has been designed in accordance with Australian Standards.</p>												
26.4.4 Nature of Parking													

Scheme Provision	Development Response
<p>The Planning Authority may require the parking spaces approved as part of any development to be of a particular size, type, proportion and location to be suited to the likely nature of demand including turnover of use, type of use and user or visitor vehicles to be accommodated, and servicing needs. This will be determined by the Planning Authority after taking into consideration matters such as:</p> <ul style="list-style-type: none"> – Location of the site; – The nature of the site; – The nature of the surrounding area; – Existing traffic and parking patterns; – The nature of the operation and future growth plans or opportunities; – Existing on street restrictions; – Future on street or off street public parking proposals. <p>The Planning Authority may require specific parking spaces to be reserved for exclusive use by visitors to the development or for people with disabilities.</p> <p>Any car parking spaces sought to be provided on site will only be approved if located in a manner appropriate to the character and appearance of development in the vicinity and/or with the Principles of this Schedule and the parking and other Objectives for the relevant Activity Area. The siting of parking spaces between the building line and the street boundary line shall generally not be 'permitted'.</p>	<p>No parking is required on the Subject site.</p> <p>On-site car parking is limited to accessible parking designed in accordance with AS/NZS 2890.6:2009 <i>Parking facilities, Part 6: Off-street parking for people with disabilities</i>. These parking spaces are located near the ramp to the building entrance.</p>
26.5 Matters to be considered	
<p>The Planning Authority shall ensure that proposed development:</p> <ul style="list-style-type: none"> – Will not interrupt the efficient passage of port related vehicular movements; – Will not generate vehicular traffic which is detrimental to other vehicle movements in Sullivans Cove; – Will not utilise a key site identified in Part F, or other land considered appropriate, in a manner that would prevent its use for the provision of public parking purposes. The Planning Authority shall ensure the scope for the provision of significant public parking generated by the development - whether on-site or off-site is explored and evaluated, including the use of part Five Agreements; – Minimises footpath crossovers and disruption to footpath movements; – Providing parking, shall do so in accordance with requirements for the design and location of spaces and access as specified in this schedule; – Providing parking, is designed to complement the form and detail of adjacent buildings and have a continuity of street level activity and/or visual interest; – Providing parking, the vehicle access shall be discrete and where possible located so as to not disrupt traffic flows along streets through queuing; and – Incorporates through-site and cross-block pedestrian links and access to the waterfront unless it can be demonstrated that there is no benefit to the enhancement of pedestrian movement and/or amenity in the Cove. 	<p>From the TIA:</p> <p><i>Clause considered to be satisfied based on the following:</i></p> <ul style="list-style-type: none"> – All proposed parking will be provided on-site, therefore limiting disruptions to the external street network. – All additional car parking movements will be accommodated within the existing car parks which facilitate existing parking movements. – The proposed number of on-site parking spaces limits the number of vehicular movements accessing the site. Impact on the surrounding environment is therefore also limited. – Private vehicles (parking) represent only a portion of the travel demand shared across a variety of modes including walking, cycling, bus and coach. Considering the close proximity of the Intercity Cycleway connection to Hobart CBD, the Cenotaph and Regatta Grounds car parks will be used by only a portion of the development site's visitors and staff. <p><i>Pedestrian access to the development site utilises an existing pedestrian bridge across the decommissioned rail line.</i></p>

4.8 Subdivision

No subdivision is proposed as part of the application.

4.9 Demolition

Demolition works are detailed in the accompanying architectural documentation and the Heritage Impact Assessment (HIA).

Scheme Provision	Development Response
28.2 Objectives	
The protection and promotion of the Cultural Heritage and Urban Character of the Cove is of primary concern in the consideration of proposals to demolish the built fabric of the Cove.	Proposed demolition has been prepared with respect to the Cultural heritage of the Cove.

All demolition within the Cove is discretionary.

Scheme Provision	Development Response
28.5 Guidelines for Development Control	
The demolition of any building, or works on land shall not be 'permitted' unless; a replacement development has been approved, or such demolition is required by statutory order or is authorised by the Building Surveyor as essential to public safety.	No statutory order has been provided for the demolition of works on the Subject site.
Any application for demolition: (a) Shall be refused if the building is included as a cultural heritage place in Table 1 of the Conservation of Cultural Heritage Values Schedule of this Scheme, unless; (i) The demolition is approved as part of a Conservation Plan approved by the Planning Authority or otherwise in its discretion under the Conservation of Cultural Heritage Values Schedule of this Scheme; (ii) The building clearly detracts from the cultural values or significance of the place; or (iii) There are overriding environmental, economic considerations in terms of the building or practical considerations for its removal, either wholly or in part.	The Subject site is identified as a cultural heritage place. The proposed demolition works have been discussed in detail in the accompanying Heritage Impact Assessment, which provides the following statements in response to the scheme (p 90): (i) <i>The CMP for the site is not formally 'approved' by the Planning Authority. Discretion is used in this instance to consider the application alongside the recommendations of the CMP (and other heritage planning documents as cited in Section 3).</i> (ii) <i>Although buildings proposed to be removed don't necessarily detract from the cultural values or significance of the place, the CMP concludes that they are of no or low heritage significance. These are proposed to be removed in favour restoring the setting and assisting in providing a suitable adaptive reuse for the significant buildings (i.e. 7 and 8).</i> (iii) <i>As per the Architects design statement, it is not considered feasible to retain the buildings proposed for removal. They are not fit for purpose in the proposed scheme which seeks to reinvigorate the more important heritage buildings with the support of a new building. The practical consideration of retaining any of the buildings proposed for demolition concludes that their retention is neither necessary nor feasible in the overall scheme – the benefits of which for the adaptive reuse of the more significant buildings are considered to be an overriding priority.</i>
(b) May be refused if in the opinion of the Planning Authority the building contributes to the cultural heritage or urban character of the Activity Area and the building is capable of continued beneficial use.	From the HIA (p 90). <i>As per above, the buildings proposed for demolition do not contribute in any appreciable way to the cultural heritage or urban character of the area. Whilst these may be retained for beneficial use, all are in a degraded condition that would require substantial upgrades to make them fit for purpose for any use. They are not capable of being used for any beneficial use in the proposed scheme and their retention would inhibit the realisation of that scheme which has overriding benefits for the more significant building(s).</i>

Scheme Provision	Development Response
28.6 Matters to be Considered	
In considering any proposal for demolition, the Planning Authority shall give regard to the following matters: <ul style="list-style-type: none"> • The impact of the proposed demolition on the character of the Activity Area; • The impact of the proposed demolition on the cultural heritage values of the Cove; • The need to avoid creation of vacant sites and 'lost space' in the Cove. 	The Project is an opportunity to improve and activate the Subject site in a way that is consistent with the heritage values and the intents of the Scheme and Activity Area. The demolition will enable the repair and restoration of those parts of the Subject site that have been identified as contributing to the heritage values of the place. Redevelopment will minimise the extents of vacant area on the Subject site and will make more efficient use of the land.
28.7 Demolition – Submission Requirements	
An application for the demolition of a building shall satisfy the requirements of Clause 9.2 for the development proposed as a replacement.	The application includes a proposal for development of the land.
28.8 Demolition on a Place of Cultural Significance	
28.8.1 Where the application involves the demolition of a building on a Place included in Table 1 of Schedule 1 – Conservation of Cultural Heritage Values then the application must satisfy the following: <ul style="list-style-type: none"> • The requirement of Clause 22.4.3 for the submission of a Conservation Plan, and • The provision of street elevations or 'true perspectives' to show the scale and impact of the demolition on places of cultural significance and the streetscape. 	From the HIA: <i>A conservation management plan has been submitted as part of the application. Note that this CMP has been reviewed recently and considered to remain valid.</i> Photo montages have been prepared with the project documentation that illustrated the visual impact that the Project will have.
28.8.2 Where the application involves the demolition of any other building then the application must satisfy the following: <ul style="list-style-type: none"> • The requirement of Clause 22.5.3 for the submission of a report identifying that the building does not have heritage value, and • The provision of street elevations or 'true perspectives' to show the scale and impact of the demolition on places of cultural significance and the streetscape. 	From the HIA: <i>Not applicable. Whilst technically the buildings proposed for demolition are on a place listed in Table 1 of Schedule 1 (Conservation of Cultural Heritage Values) these buildings are considered by the CMP to be of no/low heritage value.</i>
28.8.3 Where the proposed demolition is required by statutory order or is authorised by the Building Surveyor as essential to public safety, and no replacement building is proposed, a plan shall be submitted showing how the site is to be landscaped and fenced for public urban space in a manner consistent with the intent of the Scheme, pending the granting of a permit for a new building.	Not applicable.

4.10 Environmental Management

The Project does not propose any Level 1 or Level 2 activities, the Environmental Management Schedule is not applicable to the proposal.

4.11 Key Sites

The Subject site is identified as Key Site 14. HMAS Huon. An amendment to the scheme is not required for the proposed use and development. It is recommended that, subject to approval of this Project, Council consider amending the Draft Local Provisions Schedule.

As demonstrated through the architectural documentation, landscape plan and this assessment the proposal satisfies the requirements of 31.4, with the inclusion of discussions relative to the Strategic Framework (Section 4.1), Activity Area (Section 4.2), and the provisions of applicable schedules:

- conservation of cultural heritage values (section 4.3)
- urban form (section 4.4)

- public urban space (section 4.5)
- traffic access and parking (section 4.7)
- demolition (section 4.9)

5. Conclusion

This assessment has been prepared for Sullivans Cove Distillery to review the proposed redevelopment of 'Huron Quays' at 13-17 McVilly Drive against the provisions of the *Sullivans Cove Planning Scheme 1997*. The application has been prepared in accordance with the requirements of the *Land Use Planning and Approvals Act 1993*. The application is also inclusive of access and footpath works on the adjoining land.

The Subject site is within the Activity Area 4.2 'Regatta Point' and is for redevelopment and reuse of the vacant historic building as a Whisky Distillery and associated visitor experiences related to the distillery. The Distillery requires approval for Light Industrial use and for ancillary uses that are variously defined as eating establishment, function centre, hotel, shop, warehouse and office. The uses are discretionary but have demonstrated that they are consistent with the various applicable parts of the scheme.

The Project includes retention of significant fabric that contributes to the cultural heritage values of the place and new development has been prepared with respect to the existing building and setting. The application requires referral as a listed heritage place in accordance with the *Historic Cultural Heritage Act 1985*.

Assessment has been prepared in accordance with applicable schedules of the scheme for urban form; public urban space; traffic, access and parking; and demolition and found to satisfy the objectives and provisions. As a Key site, the 'HMAS Huron' is recognised as a strategically important and under-utilised site.

The Project is considered to meet with the Strategic Intent of the Scheme and it is recommended that a permit is issued for development.



23 Paterson Street,
Launceston, Tasmania 7250
Australia
www.ghd.com

Your ref: PLN-21-734
Our ref: 12533276

16 February 2022

Cameron Sherriff
City of Hobart
GPO Box 503
Hobart TAS 7001

Additional Information – Permit application for McVilly Drive, Queens Domain

Dear Cameron,

The following is additional information in relation to the proposed development at Huon Quays (your ref: PLN-21-734). This information is provided in response to your letter requesting information in response to the existing draft Section 54 request from the 15/02/2022.

1. Planning

1.1 PLN Fi1 – 1

Sullivan's Cove Distillery's intent in redeveloping the Subject site is to return to the place that is its namesake, to primarily manufacture whisky and to provide a visitor experience to appreciate whisky.

The distillery, including the activity within the Production Hall on the first floor falls within the definition Light Industry, which is discussed further in relation to PLN Fi1 – 2 below.

The operation of the Drill Hall does not fit as comfortably into the definitions as determined by the scheme, as the layout has been designed for private tasting rooms and room for group tastings. This is intended as a visitor experience for tasting whisky, with limited other food and beverage options. Food will be prepared off site by 3rd parties for sale and consumption, with prep areas on site in the ground floor event prep area. On occasion the Drill Hall and grounds may be used for functions, including integrating with broader events within the Cove like Dark MOFO. However, functions are considered to be ancillary to the tasting experience.

Office and staff facilities on the ground floor, and Captains Quarters on the first floor of the drill hall are likewise ancillary to the operation of the distillery and visitor experience.

Only two uses proposed have a qualifying floor area.

- Shop; proposed ~38m² as shown by retail on the ground floor.
- Eating Establishment, restaurant; no restaurant is proposed. Any serving of food would be as part of the Hotel activities on the site or as part of the occasional events forming part of the function centre use.

As discussed in the Planning Assessment the proposed uses are considered to be an appropriate use to activate this presently under-utilised site. The existing buildings on the listed heritage place are no longer fit for the purpose for which they were constructed and would no longer meet with the strategic framework of the scheme.

Tours of the site are to be pre-booked and it is estimated at this stage to be limited to daylight hours between 2-5pm. Whisky production will be generally between 8am-6pm, with the potential for the operation of stills process outside of these hours.

1.2 PLN Fi1 – 2

The following is a further description in response to Part G definition of the Scheme.

Table 1 Light Industrial Use

Definition	Development Response
Light Industry; Means the use of buildings or spaces for manufacturing, processing or storage of activities which do not:	The proposed distillery is predominantly for the processing of malt and water to manufacture high quality aged whisky, Sullivans Cove Distillery also produce brandy distilled from Tasmanian wines.
(a) cause a nuisance as defined in s.180 of the Local Government(Building and Miscellaneous Provisions) Act 1993; and	S 180 of LGBMP is repealed.
(b) cause "environmental harm" as defined in s.5 of the Environmental Management and Pollution Control Act 1994; and	s.5 (1) of EMPCA defines Environmental harm as follows: <i>(1) For the purposes of this Act, environmental harm is any adverse effect on the environment (of whatever degree or duration) and includes an environmental nuisance.</i> Where Environmental Nuisance is interpreted in s. 3 as: <i>(a) the emission, discharge, depositing or disturbance of a pollutant that unreasonably interferes with, or is likely to unreasonably interfere with, a person's enjoyment of the environment; and</i> <i>(b) any emission, discharge, depositing or disturbance specified in an environment protection policy to be an environmental nuisance;</i> Communications with Council's environmental officer (R. Moore, 1/02/22) confirm that the EPA has reviewed the proposed use and development and do not consider the application to require assessment in accordance with EMPCA. Further discussion in relation to objectives for environmental management from the scheme has been provided in Table 2 demonstrating that the project will be undertaken to ensure that there are not adverse effects to the environment from the activity.
(c) create noise at the boundary of a residential zone which exceeds 40 dBA between the hours of 7:00 am and 8:00 pm, and 35 dBA between the hours of 8:00 pm and 7:00 am; and	The closest residential zone to the Subject site is located 500m away in the Glebe, hours of operation of the distillery during the week are to generally be between 8am and 6pm with the potential for the operation of stills process outside of these hours. Activities that may generate noise are discussed further in Table 2. It is not considered necessary for further investigation to be undertaken in relation to noise as the operation of the site does not generally result in substantial noise.
(d) require the on-street parking, loading, unloading or maneuvering of vehicles associated with the operation; and	Parking, loading, unloading and manoeuvring of service vehicles is all capable of being contained on the Subject Site as illustrated in the turning paths in the appendix to the Traffic Impact Assessment (ref SK001 and SK002)
(e) adversely affect the amenity of nearby dwellings through vibration, dust, grit, smoke fumes, smell, bright lights, electrical interference, traffic generation or unsightly appearance; and	The Subject Site is located in excess of 400m from the closest dwellings and is not considered to have any impacts on their amenity.
(f) require the erection of any buildings or structures that are not in accordance with the character of the Planning Area; and	The Project has been prepared in accordance with the findings of the Conservation Management Plan and found in the Heritage Impact Assessment, and in the Planning Assessment to be consistent with the Cultural Heritage values of the place and the Activity Area 'Regatta Point'. The Project satisfies the requirements of a strategically appropriate development within a Key Site.

Definition	Development Response
(g) require the storage of goods or materials in any location visible from any residence, public street or public place.	Storage areas within the external yards will be screened and secured from the street, as illustrated in DA0202

2. Environmental Management

2.1 EM1

The proposed distillery will not exceed 10KL per day, the detail of effluent is contained within Appendix B of the Planning Assessment. This volume is substantially less than the 100 KL in an 8 hour working day required for a Level 2 assessment under the *Environmental Management and Pollution Control Act 1994*. Existing activities and operation of the Sullivans Cove Distillery at their Cambridge property were not subject to a permit for assessment by the Environmental Protection Agency (EPA), and it is understood that the EPA do not require this application to be referred.

2.2 EM2

The accompanying letter from Sullivans Cove Distillery outlines the process, equipment, inputs and outputs that they have designed the proposed distillery around.

As stated in Section 4.10 of the Planning Assessment the proposal does not require assessment by EPA and therefore as per Section 29.3 Scope of Controls of the *Sullivans Cove Planning Scheme 1997*, the Schedule 8: Environmental Management is not considered apply to the proposed development and it is not a requirement to provide an Environmental Impact Assessment.

The following demonstrates how the proposed use and development has been prepared in accordance with 29.5 Environmental Objectives of the scheme. These responses have been prepared with assistance of Sullivans Cove Distillery Pty Ltd (SCD) representative and relate existing activities at their premises in Cambridge, accompanying this letter is additional information describing the process and waste management.

Table 2 Development Response to SCPS 29.5 Environmental Objectives

Objective	Development Response
Air Quality Activities shall demonstrate 'Best Practice Environmental Management' in respect to the minimisation and mitigation of all discharges to the atmosphere.	<p>The manufacturing process does not produce any emissions of significance. The choice of heating medium, LPG fired boiler, has been designed as to allow the use of a vertical boiler to maximise efficiency and minimise emissions. These types of boilers are common through food manufacture operations.</p> <p>Grain delivery will arrive via an enclosed truck and will be delivered via pumping thus eliminating any dust emissions. All components used for grain milling and process transport are enclosed with active dust control equipment being used to capture any airborne particles.</p> <p>As discussed in the accompanying letter, the risk of whisky fungus has also been raised. Whisky fungus is a mould which can grow in the presence of ethanol and has become an issue in some places in Scotland and Kentucky. There is the potential for this to exist within Tasmania, however it is not an established nor common issue. With respect to the Huon Quays site the production of the alcohol is in effect a closed loop process with produced alcohol going to a steel tank prior to removal from site. Coupled with continuous air exchange within the production building and the location on the shores of a harbour with consistent natural air flow there is minimal to no risk of whisky fungus. All barrel filling, storage and bottling is done at existing sites and limited space at Huon Quays will dictate this remains the case.</p> <p>Odours may be present from the Sewer Pump Station (SPS), detailed design will review the existing infrastructure and determine whether upgrades are necessary in accordance with the EPAs <i>Sewage Pumping Station</i></p>

Objective	Development Response
	<p><i>Environmental Guidelines</i> (Oct 2019). Control measures to be considered will include increased storage capacity, standby pumps, power supply, and operation and management plan. Design and ongoing management of the sewer will need to be approved and monitored by TasWater.</p> <p>Once these mitigation measures are considered, the risk of spills, odour and noise impacts to the environment will be minimised such that the proposed SPS will likely present a significantly lower risk to the environment than the existing SPS.</p> <p>The nearest sensitive receptors include dwellings or accommodation to the Subject site are located in Wapping on the other side of the ridge line and exceed 400m in distance.</p>
<p>Energy Efficiency</p> <p>Use and development must demonstrate measures undertaken to improve energy efficiency in the design, layout and use of new and existing buildings.</p>	<p>Energy efficiency is an important component of ensuring the commercial feasibility of the ongoing activities of the Project, and SCD are actively investigating these as part of the design process.</p> <p>The proposed development and use will include the following measures to improve energy efficiency:</p> <ul style="list-style-type: none"> – Adaptive reuse and retrofit of an existing heritage building. – Site layout options considered in concept stage for the efficiency of process. – The addition of a second roof over the existing roof allows for insulation to be incorporated without compromising the Heritage structure. – The size and type of boiler has been chosen for maximum efficiency – Use of natural light possible. – The site is located within an urban area with minimal on-site parking to minimise energy for transport. Bike storage with recharging facilities will be provided for staff. <p>Further measures being considered and to be resolved as part of detailed design include the following:</p> <ul style="list-style-type: none"> – Full insulation of all heating pipes. – Efficient design and control of process cooling water equipment to match demand – Reclamation of waste heat generated in the manufacturing process to provide heating for the Drill Hall building – Design incorporates use of engineered timber where possible to reduce the energy footprint of construction materials – The use of variable speed drives on equipment where possible – Replacement and specification of energy efficiency lighting systems.
<p>Flora and Fauna</p> <p>Flora and fauna of significance within the Sullivans Cove Planning Area must be protected. In particular, the habitats of the Derwent Estuary and Sullivans Cove water environment must be protected from the adverse environmental impacts of activities.</p>	<p>Local Landscape Architects have been engaged to design the outside spaces which compliment not only the heritage aspects of the site but respect the pre-colonial conditions. This includes the removal of introduced, non-native plants and replanting with native shrubs, trees and grasses indigenous to the area.</p> <p>As discussed in Section 2.8 of the Planning Assessment a review of Natural Values for the site was undertaken and there was no threatened flora, fauna or vegetation communities within or adjoining the Subject site that warrants additional controls.</p> <p>As summarised in the Site services – Concept plan, and discussed in relation to objectives for Water Quality, the proposed stormwater system will have reduced areas of impermeable surfaces, upgrades to improve infrastructure; inclusion of litter baskets; and potentially include rain water harvesting. It is anticipated that the works will improve stormwater runoff above current standards to the benefit of the Derwent Estuary. Any further improvements are capable of being conditioned by Council, as required.</p>
<p>Hazard and Risk</p> <p>Land within the Cove must be used and developed in a manner which provides a safe working and living environment. In doing so, best practices must be employed in</p>	<p>The manufacturing process is a mature process used around the world and is a relocation of SCD's existing established manufacturing facilities in Cambridge. The industry is highly regulated for Workplace health and safety and requirements are clearly identified. SCD have engaged specialist process consultants to ensure compliance; and building surveyors and fire consultants during the conceptual design stage to make sure all relevant</p>

Objective	Development Response
respect to the handling of dangerous goods and all relevant dangerous goods and environmental laws complied with.	<p>statutory requirements are met. Additionally, they have worked with Lote Consulting, a specialist process risk consultant for early input, and have arranged for a Hazop, safety in design and specialist hazardous material risk review during the detailed design phase. It is also intended to consult with Tasmania Fire Services to review the site during the design phase and as construction progresses in association with the building permit process.</p> <p>Further to the description of the Proposed Development in Section 3 of the Planning Assessment the process facilities have had to be located in a new building to meet regulatory requirements for safety; as has the separation of activities between the Drill Hall from the Production Hall. Particular efforts have been focussed on containment and the production building and yard areas have been designed to contain and manage all areas dangerous or hazardous goods are active. This includes the need to safely capture and store any runoff or spillages for both areas to a single point. Cleaning regimes for production and yard areas will likewise be undertaken in accordance with relevant statutory controls.</p>
<p>Land Contamination</p> <p>New activities which are proposed on land with a known history of industrial or other use where potential for contamination exists shall be accompanied by an environmental audit including an assessment of site condition.</p> <p>Contaminated land shall be managed in a manner which is compatible with the intended future use of the area. New activities on known contaminated sites must only be 'permitted' to occur after appropriate clean-up of the site, or where it is clearly demonstrated that the proposed activity will not result in an immediate or likely long term hazard to human health or the environment.</p>	<p>The Huon Quays site has an active history and this has involved the storage of environmentally sensitive materials. In 2007 a fuel storage tank was removed from the site and the associated testing has shown no site contamination present.</p> <p>The proposed design utilises much of the existing ground infrastructure as is with the main building works being focussed within the footprint of existing buildings.</p> <p>With TasPorts as the owner there has been inspection and reporting on any Asbestos used within the site. The asbestos items are small in number and minor in nature. All site works and disposal will be carried out within statutory requirements as well as TasPorts safe work procedures.</p> <p>The proposed use is not for a sensitive use and it is not considered likely that there is likely to be hazards to human health.</p>
<p>Land Reclamation</p> <p>Land forming and reclamation activities, where required, shall be carried out in a manner which minimises adverse environmental consequences.</p>	No land reclamation is proposed as part of the development.
<p>Noise</p> <p>Buildings shall be sited and designed having regard to current noise levels in the area as well as their intended use.</p> <p>Where activities with the potential to generate significant noise are proposed in proximity to residential accommodation and other 'noise sensitive' activities, appropriate measures to mitigate and minimise noise emissions must be undertaken.</p> <p>New 'noise sensitive' activities such as residential accommodation shall be located and where necessary incorporate acoustic measures to minimise the potentially adverse impacts of existing or likely future activities on nearby land.</p>	<p>As described in Section 2.4 of the Planning Assessment the site adjoins an industrial area used by neighbouring TasPorts slipway where the sandblasting of ship hulls generates a baseline of noise for the area. Noise within the area is further contributed to by the substantial amount of traffic on Tasman Highway and nearby interchanges.</p> <p>The nearest sensitive receptors include dwellings or accommodation to the Subject site are located in Wapping on the other side of the ridge line and exceed 400m in distance.</p> <p>The manufacturing process used within the distillery is by its nature a very quiet one. There are some supporting activities which generate noise and the following design considerations are in place to accommodate this:</p> <ul style="list-style-type: none"> – Grain milling: The milling equipment is relatively small by industrial standards, operates irregularly (for example it may be 3 times a week for several hours at a time), and will be contained within a sealed, insulated for noise, room designed for this purpose. This containment also allows for effective dust control of the process. The room has been positioned at the Slipway end of the production building minimising any noise projection towards public or other neighbouring properties. – Air compressor and boiler: Both the air compressor and boiler are relatively small and have been selected to be the smallest possible for the

Objective	Development Response
	<p>site. These are located in a specifically designed room which will minimise noise projection from this equipment</p> <ul style="list-style-type: none"> – Traffic: The geography of the area restricts the size of delivery trucks to 18m and under. The process equipment has been designed around these restrictions to maximise the trucks used and to minimise trips. For example initial grain deliveries will only be needed once a month and bulk spirit can be transferred from Huon Quays to SCD warehousing in Cambridge once a week. The truck movements to the site are determined in the TIA to be low and are restricted to daytime operations. – No heavy equipment is required on site. <p>Further noise may be generated by the Sewer Pump Station, detailed design will review the existing infrastructure and determine whether upgrades are necessary in accordance with the EPAs <i>Sewage Pumping Station Environmental Guidelines</i> (Oct 2019). As discussed in relation to Air Quality, upgrades are likely to minimise any impacts from the existing SPS.</p> <p>The project includes a substantial component for visitors to experience the manufacture of whisky through guided tours including moving through the production building along a mezzanine walkway, as well as tastings. Every design effort has been employed to control noise levels to make this experience safe and comfortable for visitors.</p>
<p>Waste Minimisation</p> <p>Activities must demonstrate how the practices and process associated with the activity will reduce as much as possible the amount of waste generated or the amount which requires subsequent treatment, storage or disposal. Activities must address waste minimisation from the source (source reduction) and recycling. Where appropriate, applications for new activities must include a waste management plan. Activities within roads and other public spaces must incorporate where relevant suitable waste and litter management facilities.</p>	<p>The manufacturing process allows for the re-purposing of much of the 'waste' generated. The inputs are water and grain. Once the grain has been processed it retains a good nutritional value and is typically used as a source of animal feed by local farmers. The storage of the used grain has been designed to allow for efficient truck loading for delivery to farms. This is a similar process used nationally and internationally. Similarly the pot ale, the waste liquid following the fermentation process, retains a nutritional value and is typically used by farmers to increase the nutritional value of feed by spraying it on fields.</p> <p>Any remaining liquid waste, including wash down and cleaning outputs, are trade waste and consultation with TasWater around management of this has commenced as part of the conceptual design phase so as to ensure adequate effluent treatment processes are incorporated.</p> <p>With respect to the visitor centre operation much of the goods inwards will be first sent to the main warehouse in Cambridge for receipt and dispatch. This process will minimise not only the need for on-site storage but also minimise packaging delivered to site. SCD currently recycles all waste packaging cardboard via repurposing it into padding for our product delivery.</p> <p>The Huon Quays Project is fundamentally for the production of whisky and the sharing of the process with visitors. As such there are no plans for food preparation on site. In the case of events it is expected any food will be pre-prepared and waste will accordingly be off site.</p> <p>The design has allowances for a significant increase in recycling bins over and above what SCD currently utilise at the Cambridge site, these are to be located in the proposed screened loading area of the Subject site.</p>
<p>Water Quality</p> <p>Activities shall demonstrate 'Best Practice Environmental Management' in respect to water use and management. Water use and disposal shall be managed in a manner which seeks to minimise off site disposal and which seeks to protect and, where possible, improve ambient water quality. The principles of minimising water sewage and waste water generation and the re-use, recycling and pre-treatment of waste water prior to disposal must be encouraged.</p>	<p>The control of the quality of water is critical to SCD at their existing facilities and as such there is a very high level of knowledge of the make up of any trade waste or effluent generated, as demonstrated in the Effluent Feasibility Study accompanying the application. Early engagement with TasWater has taken place and the design has in place the collection and treatment of effluent prior to leaving site in accordance with the guidelines provided by TasWater.</p> <p>As discussed in relation to Air Quality and Noise, the existing SPS infrastructure will be reviewed as part of detailed design to ensure that it meets EPA Guidelines and determine any upgrades that may be required.</p> <p>In addition to the physical equipment to control and monitor water quality leaving site SCD are working through the design and automation package which will include the capture and storage of data for trending and historic review.</p> <p>As previously discussed the pot ale produced during the process has a valuable nutritional element and it is expected this will be utilised by local farmers.</p>

Objective	Development Response
	The capture of rain water from the buildings to storage for use maintaining the landscaped areas. Selection of plantings native to the area will also minimise the maintenance requirements of the landscaping. Litter baskets will be included in the upgraded stormwater management system to improve water quality.

The location and quality of the proposed distillery and the associated visitor experience is fundamental to the branding of Sullivans Cove Distillery, and use of sustainable processes that minimise waste and energy consumption are necessary for the viability of production. As such, environmental management is an important part of ensuring delivery of a successful project.

Furthermore, a review of the standards of the Attenuation Code of the State Planning Provisions finds that the application would meet permitted standards as the proposed distillery is not within 200m of a sensitive use of a zone that would support development of dwellings.

3. Traffic, Access and Parking

Further to information within the Traffic Impact Assessment and Planning Assessment the following comments are made in relation to the proposed parking and access.

3.1 Parking and Access

- Staff parking** - On-site staff parking during the day is not provided other than the provision of DDA parking. Staff Parking is not a requirement of the Scheme and has not been provided as there are various alternative transport modes to the site. After hours, if staff are required on-site there will be flexibility to park on site within lit, safe areas as security will be required for staff and for product.
- Coach** – The Drill Forecourt has been designed to accommodate buses to drop off visitors to the site (as shown in the swept paths as appendices to the Traffic Impact Assessment (TIA)), there is also a turning area on the public Regatta Ground Car park adjoining the Subject site. It is more likely that smaller vehicles will manoeuvre in the Drill Forecourt turning area, which has been designed as a traffic calmed shared multi-purpose area (TIA section 3.2).
- Alternative Transport** - As discussed in the TIA (section 4.1.2) and the Planning Assessment (section 4.7) the proposal has been prepared with regard to maximising mode share for transport to the site to discourage private vehicle use. Circulation areas have been designed to maximise accessibility, functional requirements for the operation of the distillery, to enhance the landscape, and to respect the historic heritage values of the place.
- Alternative Parking** – Regular visitor numbers to the site are not considered to require alternative parking options and is not a requirement of the Scheme. Should events exceeding the regular use of the distillery, visitor experience, sales and administration occur an event specific management plan is capable of being prepared to Council's satisfaction.
- Bike Storage** - The site has excellent access to cycle infrastructure with proximity to the intercity cycleway. Visitor bike storage is located in the Drill Forecourt. Staff bike store that is enclosed is proposed located adjoining the secured external yard, as shown on the first floor plan (DA0102). Fit out of the bike store is capable of meeting Australian Standards and providing charging facilities.
- End point facilities for staff** – These are not a requirement of the planning scheme, however the inclusion of these within the project are an important incentive to encourage active transport mode choices, including cycling. Staff change facilities are included on the ground floor level (DA0101), with possible additional facilities within the staff area of the Production Hall.
- DDA parking and access** – The configuration of the Drill Forecourt has been designed with consideration to improved accessibility to the building, connecting compliant parking to internal lifts, with respect to the heritage values of the place. It is agreed that an alternative to compacted gravel finish should be used in the Drill Forecourt. A condition on the permit for an alternative to the approval of Council's Development Engineer would be supported.

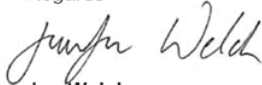
3.2 Waste and Storage

Waste management has been outlined further in the accompanying letter. Waste collection will be undertaken by a private operator, with access through the external yard to the secure bin storage as illustrated on the first floor plan (A0102). Storage will be required for around 6-8 wheely bins. Vehicles for waste management have been considered as part of the service vehicles

We would appreciate the opportunity to discuss for any potential concerns to be addressed by condition, where possible.

Should you wish to discuss this matter please do not hesitate to contact me.

Regards



Jen Welch
Senior Planner

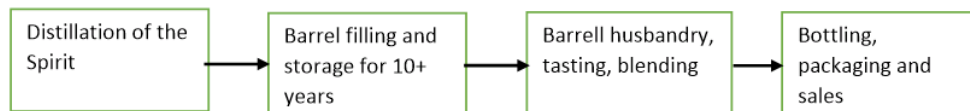
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Information on the Sullivan's Cove Distillery Huon Quays Operations

A high level understanding of how Sullivan's Cove Distillery operates provides a greater appreciation of the proposed works for the Huon Quays site. It is helpful to understand not only what is proposed for the Huon Quays site, but also to what is not required and why.

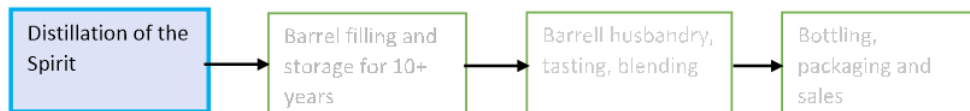
Production to Retail



The current operation is based in Cambridge. Distillation, barrel filling, barrel storage, barrel decanting, blending, bottle filling and dispatch is carried out in one location whilst in additional warehouses barrel storage and a dry goods store are managed. There is also a cellar door retail space and facilities for tours of the production process.

Sullivan's Cove Distillery excels at producing high quality single cask release whiskey. This means that each 'batch' of bottles for sale are matured in a single barrel and multiple barrels are not mixed together to achieve the final product. The barrel husbandry over the 10 to 30 year maturation process, the emptying and processing the spirit, the bottling, labelling and packaging are all carried out by hand.

Huon Quays Production Site Operation



Production

Only the production of the spirit will be carried out at the proposed Huon Quays site. The output from the site will be a 10,000L tanker truck (a mid-sized rigid truck) transporting the liquid to the existing Cambridge operation for processing. Initial production volumes will call for this to occur up to once a week.

Raw materials required for the production process include grain and water. Water will be treated and stored for use on site. Grain will be delivered by truck and pumped to a storage bin for use. Initial production volumes would call for a grain delivery once a month. Increased production volumes in the future would require more frequent deliveries.

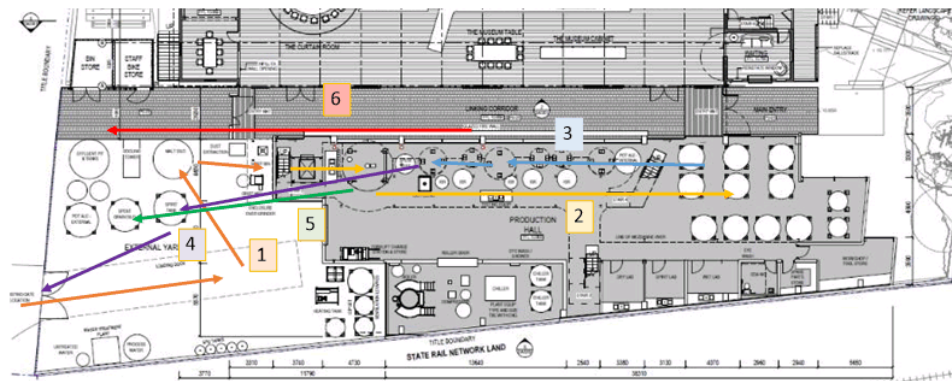
Once the grain is processed and the resulting liquid (wort) is extracted it is an established practice around the world that the spent grain is collected and used as livestock feed. It is our expectation

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that this will be the case for Huon Quays. Similarly there is a point in the process where there is a nutritionally rich liquid produced that is not required for further processing. Rather than disposing this as trade waste we have designed collection facilities for this to be removed from site and used by local farms on pasture to add nutrients. Again, this is standard practice in other distilleries around the world.

The end product of this process is whiskey coming from the liquid component of the raw materials. Any other liquid from the process or cleaning operations have a well know composition and details of these have been shared with TasWater at a very early engagement to ensure our operation will not adversely affect the local waste treatment facilities. Wastewater treatment facilities servicing the area have been established with industrial facilities in mind and easily accommodate the trade waste from the proposed facility.

A visual representation of the process and equipment is shown below.



Reference	Description
1	Malt is received via truck and stored in a malt tank before being milled for use. Grain will be pumped from the truck to minimise dust and spillage.
2	Milled malt (grist) enters the mash tun, is processed and the resulting liquid (wort) is moved to the fermenters.
3	Liquid (wash) from the fermenters is moved to the wash still for distillation. The resulting liquid (low wines) is moved to the spirit still for distillation.
4	Liquid from the spirit still (new make spirit) is moved to a spirit tank for bulk storage prior to being removed to Cambridge for barrel filling and storage. Regulations for the design of the spirit tank are very specific about site location and separation and bunding. The tank will be constructed from stainless steel.
5	Once the mash tun has operated the solid grain waste is moved to bulk storage prior to removal from site for use as livestock feed. Liquid waste is moved to the pot ale storage tank for removal from site for use on pasture.
6	Distillation waste, equipment cleaning outflow, and wash down outflow is moved to an effluent treatment plant prior to discharge to sewer system as trade waste. Continuous monitoring will ensure all discharge is within agreed ranges and historic data can be stored for trending analysis and future reference. Storage of treated waste prior to discharge will allow for control of discharge timing and volumes to assist sewer infrastructure if required.

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Fire regulations call for the mash tun, wash still and spirit still area of the production facility to be bunded to contain any potentially flammable liquid in the event of equipment failure. This bunding will enable all liquid waste produced during normal operations and also cleaning practices to be contained and directed to the on-site waste treatment equipment. Similarly regulations call for specific spacing and bunding around tanks and equipment located in the production yard. The design allows for the collection of run-off from this area for treatment prior to discharge from site.

Of significance is what will not take place at this site. Activities remaining at Cambridge include barrel filling, storage, bottling and retail dispatch. The main warehousing of dry goods will also reside at Cambridge with internal deliveries to Huon Quays of resources as required. This minimises staff, traffic, storage space, receipting activities and waste generation at Huon Quays. It also splits the home location of staff between the sites.

One potential environmental issue worth discussing is that of Whiskey Fungus. Major production centres such as Kentucky in the USA have experienced issues with a black mould that develops in conditions with an elevated and consistent amount of ethanol in the environment. This typically occurs around storage and maturation facilities where evaporation from barrels produce the required alcohol in the atmosphere. Huon quays is a closed production loop in a building that requires constant air exchanges (up to 6 air changes per hour within the building), located on a coastal position with prevailing windy conditions. There is no barrel storage at Huon Quays. As such the risk of whiskey fungus is extremely low and does not require further action.

Water usage

With reference to the production process described above the volumetric capacity of the wash still is 6000L and spirit stills is 2500L. Given the time requirements of the fermentation and distillation process the initial operation will call for up to 3 production runs a week. The Environment and Pollution Control Act 1994 deem production operations that use 100,000L of water in an 8 hour shift are deemed a Level 2 activity. Water use volumes per production run for all process and supporting equipment and activities are approximately 14,000L and this is split over multiple shifts, well under the 100,000L per shift limit.

Water from the mains is drawn from the water main, treated and stored for use in production meaning that the draw from the mains supply is spread over time rather than an instantaneous requirement.

Tours and Tasting

Although the primary function of Huon Quays will be the production of whiskey the heritage listed drill hall provides an outstanding backdrop to share the Sullivans Cove Distillery history, achievements and the production process. We can demonstrate the care, attention and skill that goes into making whiskey and the uniquely Tasmanian aspects of our operation.

The tours provided at Cambridge have shown it is the discussion and interaction with our guides that are the most valuable part and although not finalised, it is expected that tours at Huon Quays will involve groups of up to 12 visitors being guided by a tour guide through the production area and then to a tasting location within the Drill Hall. Tastings may be in one of the private rooms, in the hall space or at the bar and include discussion and tasting a number of our whiskeys.

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Tours will be staggered, for example every 15 or 20 minutes, and it is expected they will operate by prior online booking during designated hours, say 2pm to 5pm. This allows Sullivans Cove Distillery to manage the quality of the experience and the number of people on site at any one time. Future expansion of this service may include a morning period, say 10am to noon.

The production of whiskey relies heavily on the senses. The production team train their sense of smell and taste frequently and the building design of the production facility focusses on neutral areas where taste and smell can be used to guide decisions. Similarly when teaching visitors about the characteristics of whiskey these senses are important. As such the visitor facilities are focussed on this experience and do not include in-house food options nor coffee facilities. The site will also be designated as a no smoking area, inside and outside.

The tour and tasting by online booking only, and restricted times during the day helps manage staffing levels, public infrastructure such as parking and public transport, and the ability to adjust operations to suit conditions. For example in the hottest month of the year tours may be scheduled for the morning to avoid the higher afternoon temperatures.

General Operations at Huon Quays

Sullivans Cove Distillery operates with a group of very dedicated and skilled personnel and every effort has been made to facilitate their needs within the size and heritage dictated boundaries of the site. This includes:

- Secure and well-lit bicycle storage facilities for staff away from the public bicycle storage facilities.
- End of trip facilities including shower and dry change area with individual lockers for staff.
- Kitchen and lounge facilities including staff designated outdoor eating area.
- Storage areas specific to equipment individual staff need for their tasks
- The use of natural light at every opportunity.

The heritage standing of the site has a significant impact on design decisions. These include:

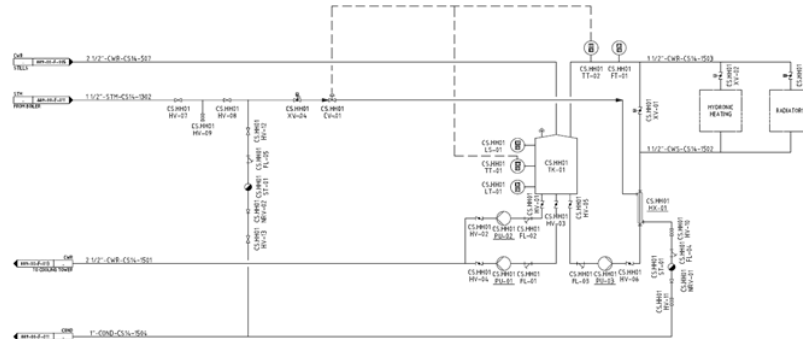
- Reusing the natural land features of the current site. Existing retaining walls are being utilised along with changes in ground levels. The main earthworks for the site are focussed on providing DDA access to the entrance.
- Demolition works to return the Drill Hall to its 1912 structure as much as possible and positioning of the new production building on the footprint of existing buildings.
- Restricting on-site parking.
- Guiding material choices such as roofing material.
- Driven design options around modification of the building to meet current standards. For example, the walls are a single skin weatherboard construction and building a second wall skin to accommodate insulation significantly affects the nature of the Drill Hall

The design process has incorporated as many energy saving options as the heritage site will allow for, including:

- Process equipment has been designed with modern control systems and energy management components such as variable speed drives.
- The distillation process requires heat. The excess heat will be used for hydronic heating (hot water radiators or underfloor heating) of the drill hall to overcome restrictions on insulating the heritage building.

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The Piping and Instrument Diagram for the part of the process to allow for the excess heat in the form of hot water to be used to heat the Drill Hall is shown below.



- ☐ The use of natural light and materials such as engineered timber where possible.
- ☐ Rainwater runoff from the building roofs will be collected and used to water the landscaping. Where possible gravity will be used for this activity to minimise energy input.

The current facilities at Cambridge produce a relatively small amount of operational waste requiring removal from the site. This is managed through the use of 1 general and 2 recycling wheelie bins for commercial collection. Waste management at the Huon Quays site will include:

- ☐ Commercial collection from within the property using between 6 and 8 wheelie bins. The design includes a bin storage positioned to allow for ease of use from the operation, ease of use during collection and also constructed to minimise opportunities for pest interaction including rodents, sea birds and possums. Please note that as mentioned above the food offering during normal operation is minimal and as such the food waste for disposal will also be minimal. General waste collection and recycling will be through a commercial provider with collection taking place fully within the site boundary. With twice the bin capacity currently used at Cambridge and the ability to use more frequent collections the arrangements provide sufficient capacity for now, increased future operations and any potential events.
- ☐ Any cardboard packaging currently received at Cambridge is shredded on site and repurposed as protective fill when packaging bottles of whiskey. Given the main store will remain at Cambridge this reuse of cardboard will continue.
- ☐ As discussed above the main waste from the grain process will be used for livestock feed or adding nutrients to pastures. The storage and handling equipment has been designed to include this activity.

As with many manufacturing processes security of the site is very important. The current Cambridge site is a closed site unless it is the designated times for tours. That is the site gates are locked so public can not enter out of the tour times. The Huon Quays site will include:

- ☐ As secure yard area for the production building utilising palisade fencing, electronic gates, CCTV and appropriate lighting.
- ☐ Gating to restrict public vehicular access to the river front entrance during non-tour times

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As discussed above the heritage nature of this site has a significant impact on the available space for parking at Huon Quays. The design includes the following:

- 3 number DDA parking spaces at the main building entrance.
- Coach turning circle at the front of the building for drop off and pick up only. No on-site coach parking. Please note that given the ideal size of a tour group is around 12 people, tour groups larger than a mini-bus are discouraged.
- The current design suggests a compacted crushed gravel may be used for the surface. It has been highlighted that this may not be consistent with DDA use and as such during the detailed design process alternative solid surfaces for this area, or for at least the parking and pathway areas will be explored.
- Public and staff bicycle parking spaces.
- The production yard has a driveway area designed for rigid truck delivery and suitable space for the safe operation of a forklift to unload if required. It is expected that production staff when working evenings, weekends or at times where there are no deliveries, may make use of this area for secure parking.
- Similar to the production yard use when not required, there is an opportunity to utilise the main building parking spaces for short term parking during time periods where there are no tours operating and the main gate is closed. For example, cleaners may take advantage of this of a morning or evening.

Appendices


Appendix A

Certificates of Title

Address	Certificate of Title/ Folio	Landowner/Authority	Document link
'Huon Quays', 17 McVilly Drive	134036/2	Tasmanian Ports Corporation Pty Ltd (TasPorts)	
13 McVilly Drive	134036/1		
1 McVilly Drive	134037/1		
	118002/1	Crown Land / State Rail Network	
McVilly Drive	26915/3		
Cenotaph and Regatta Grounds', 20 McVilly Drive	135056/3	Local Government Authority (City of Hobart)	

Appendix B

Wastewater treatment plan

 M.E. ENGINEERING PROCESS ENGINEERING CONSULTANTS	Effluent Feasibility Study	Rev	A
	889 – Sullivans Cove Distillery	Date	12/10/21


Revision History

Rev.	Description	By	Date
A	Initial issue	Gregor Warren	12/10/21

Overview

Sullivans Cove are considering building a distillery at Huon Quays in Hobart with a development plan including an existing Heritage listed building. The visitor centre is planned to use the Heritage building with the distillery in a newly built bespoke building that will combine with visitor centre to create a visitor experience.

The distillery is primarily to produce whisky and as part of the whisky making process there are various effluent streams that must be considered. This report summarises the initial effluent analysis and potential options for treatment.

 M.E. ENGINEERING <small>MECHANICAL ELECTRICAL ENGINEERING</small>	Effluent Feasibility Study 889 – Sullivans Cove Distillery	Rev A Date 12/10/21

1. Effluent Analysis

There were two analyses completed for the potential effluent discharge from the distillery. One based on literature and previous project experience and one based on the lab analysis of the existing smaller scale distilling operation. Please note, the plan is that the Pot Ale (Spent Wash) will be removed from site but the numbers are included for information.

1. Estimates based on literature and previous project experience:

Effluent			Temp degC	pH	BOD mg/L	COD mg/L	SS mg/L	Copper mg/L	Sulphates mg/L	Phosphates mg/L	Aluminium mg/L	Nitrogen mg/L
Pot ale ~	8,593	L per mash	70	4	25000	50000	4500	2	200	300.0	0.06	3000
Spent lees ~	2,198	L per mash	60	4	2000	4000	500	25	30	0.5	0.02	125
Washing waters ~	2,000	L per mash	70	13	500	1000	1000	0	0	0	0	0
Boiler blowdown ~	400	L per mash	100	9	50	100	0	0	0	0	0	0
Cooling tower blowdown ~	800	L per mash	20	7	50	100	0	0	0	0	0	0

For the analysis it is assumed that effluent is produced in proportion to production therefore the only considerations based on production are total volume of effluent and total copper production per day.

	Temp degC	pH	BOD mg/L	COD mg/L	SS mg/L	Copper mg/L	Sulphates mg/L	Phosphates mg/L	Aluminium mg/L	Nitrogen mg/L
Average	66	5	15,743	31,486	2,985	5	128	184	0	1,862
Ave (exc PA)	59	8	1,007	2,014	574	10.2	12	0	0	51
Acceptance	38	>6<10	600	1,500	600	10	1,500	50	100	150

Distillations Per Day	1	2	3	Acceptance
Total Volume (Exc PA)	2,699	5,398	8,096	100,000
Copper Per Day	27	55	82	30


2. Estimates based on lab analysis of the existing smaller scale distilling operation:

Effluent			Temp degC	pH	BOD mg/L	COD mg/L	SS mg/L	Copper mg/L	Sulphates mg/L	Phosphates mg/L	Aluminium mg/L	Nitrogen mg/L
Pot ale ~	8,593	L per mash	70	3.5	25000	50000	3800	32	200	280.0	0.06	1700
Spent lees ~	2,198	L per mash	60	3.4	3320	6000	61	0.5	30	1.3	0.02	9.1
Washing waters ~	2,000	L per mash	70	13	500	1000	1000	0	0	0	0	0
Boiler blowdown ~	400	L per mash	100	9	50	100	0	0	0	0	0	0
Cooling tower blowdown ~	800	L per mash	20	7	50	100	0	0	0	0	0	0

	Temp degC	pH	BOD mg/L	COD mg/L	SS mg/L	Copper mg/L	Sulphates mg/L	Phosphates mg/L	Aluminium mg/L	Nitrogen mg/L
Average	66	5	15,951	31,801	2,486	20	128	172	0	1,046
Ave (exc PA)	59	7	1,544	2,828	395	0.2	12	1	0	4
Acceptance	38	>6<10	600	1,500	600	10	1,500	50	100	150

Distillations Per Day	1	2	3	Acceptance
Total Volume (Exc PA)	2,699	5,398	8,096	100,000
Copper Per Day	1	1	2	30

The numbers for BOD from lab analysis are slightly higher than the estimates based on literature and previous project experience. However, the copper figures were significantly different. The lab samples had higher copper in the Pot Ale and significantly less in the Spent Lees.

 M.E. ENGINEERING <small>MECHANICAL ELECTRICAL ENGINEERING</small>	Effluent Feasibility Study 889 – Sullivans Cove Distillery	Rev A Date 12/10/21
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2. Effluent Acceptance Criteria


From the initial effluent analysis it would not be possible to put the Pot Ale (Spent Wash) to the normal effluent without significant treatment and the intention is it will be removed from site as it has value in spreading to land.

Excluding the pot ale from the analysis the following should be considered:

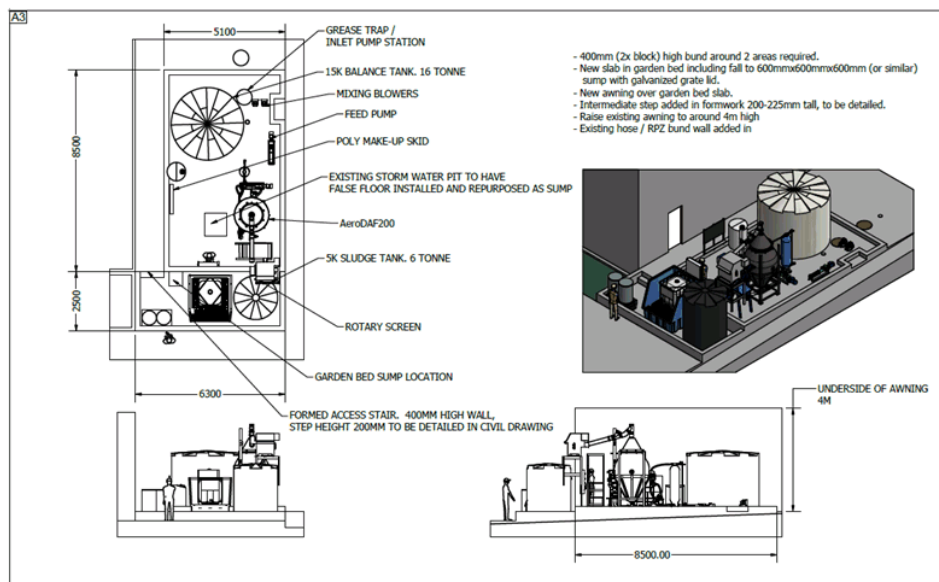
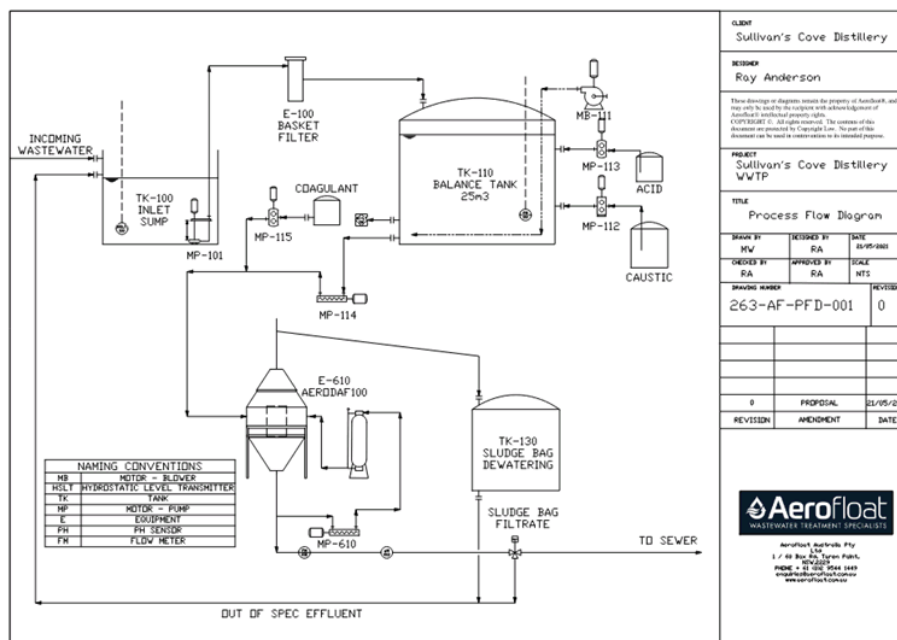
- Temperature – The temperature will be higher than discharge consent, however, it will cool in the external tank. A secondary method of cooling by heat exchanger or dilution will be required.
- pH – While the pH should normally be in acceptable limits it would be recommended to have a dosing systems to allow adjustments.
- BOD – This is estimated to be above the limit, this limit was open for discussion with TasWater, an increase should be discussed.
- COD – This is estimated to be above the limit, this limit was open for discussion with TasWater, an increase should be discussed.
- SS – While this is estimated to be below the limit, this limit was open for discussion with TasWater, an increase should be discussed.
- Copper – Based in the numbers in literature and from previous project experience the copper will be close to the limit on average and will be above the daily allowance once production is at two distillations per day. However, the copper in the lab samples for the spent lees was significantly less. Based on this uncertainty and the fact that Acceptance limits are unlikely to be exceeded at initial production it would be recommended to review once operational and consider treatment as required.

The total effluent volumes are relatively low and dilution could be considered as a treatment option. However, this would not remove potential concerns with the daily copper allowance. If this is the only concern of TasWater then specific copper removal methods should be investigated.

If the limits cannot be increased and dilution is not an option a DAF plant would typically be used. A typical setup is shown below from discussions with Aerofloat with an estimated cost of ~\$250,000.

 M.E. ENGINEERING <small>PROVIDING ENGINEERING SOLUTIONS</small>	Effluent Feasibility Study	Rev	A
	889 – Sullivans Cove Distillery	Date	12/10/21

3. Potential DAF Plant Solution (Only If Required)



ghd.com

→ The Power of Commitment



Huon Quays

Traffic Impact Assessment

Sullivans Cove Distillery Pty Ltd

13 October 2021

→ **The Power of Commitment**



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Author	Yishin Chen, Melissa Adikari
Project manager	Scott Rolls
Client name	Sullivans Cove Distillery Pty Ltd
Project name	Sullivans Cove Distillery
Document title	Huon Quays Traffic Impact Assessment
Revision version	Rev 0
Project number	12533276

Document status

Status Code	Revision	Author	Reviewer		Approved for issue		
			Name	Signature	Name	Signature	Date
S2	A	Y. Chen/ M. Adikari	M. Petrusma S. Barlow	On File	T. Beumer	On File	29 Sept 21
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Contents

1. Introduction	1
1.1 Purpose of this report	1
1.2 Scope and limitations	1
1.3 Assumptions	1
1.4 References	2
2. Existing conditions	3
2.1 Subject site	3
2.2 Planning and land use	3
2.3 Road network	5
2.4 Public transport network	7
2.5 Active transport network	8
2.6 Car parking	9
2.7 Crash history	11
3. Proposed development	13
3.1 Development design	13
3.2 Parking provisions	14
3.3 Access arrangement	15
4. Impact assessment	17
4.1 Trip generation	17
4.2 Trip distribution	18
4.3 Parking requirements	18
4.4 Impacts to traffic efficiency	21
4.5 Impacts to local amenity	21
4.6 Impacts to road safety	22
5. Planning Scheme assessment	23
5.1 Traffic, access, and parking	23
6. Summary	27

Table index

Table 2.1	Summary of recent crash history near Huon Quays	11
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Figure index

Figure 2.1	Subject site location	3
Figure 2.2	Current planning zones within Sullivans Cove	4
Figure 2.3	Excerpt of the Cenotaph / Regatta Ground Precinct Plan	5
Figure 2.4	Road network	6
Figure 2.5	Bus network	8

Figure 2.6	Shared path network connecting the Intercity Cycleway	9
Figure 2.7	Car parking	10
Figure 2.8	Recent crash locations near Huon Quays (April 2016 – March 2021)	12
Figure 3.1	Building plan	13
Figure 3.2	On-site parking	14
Figure 3.3	Accesses to development site	15
Figure 4.1	Proposed accessible parking	19
Figure 4.2	Dimensions of angle parking spaces	20
Figure 4.3	Bollard requirement	21

Appendices

Appendix A	Swept paths
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1. Introduction

1.1 Purpose of this report

This report has been prepared by GHD for Sullivans Cove Distillery Pty Ltd to assess the proposed development at Huon Quays in relation to the potential impacts on traffic and the local transport network, including the parking provisions of the *Sullivans Cove Planning Scheme 1997*.

1.2 Scope and limitations

This report has been prepared by GHD for Sullivans Cove Distillery Pty Ltd and may only be used and relied on by Sullivans Cove Distillery Pty Ltd for the purpose agreed between GHD and Sullivans Cove Distillery Pty Ltd as set out in this report.

GHD otherwise disclaims responsibility to any person other than Sullivans Cove Distillery Pty Ltd arising in connection with this report. GHD also excludes implied warranties and conditions, to the extent legally permissible.

The services undertaken by GHD in connection with preparing this report were limited to those specifically detailed in the report and are subject to the scope limitations set out in the report.

The opinions, conclusions and any recommendations in this report are based on conditions encountered and information reviewed at the date of preparation of the report. GHD has no responsibility or obligation to update this report to account for events or changes occurring subsequent to the date that the report was prepared.

The opinions, conclusions and any recommendations in this report are based on assumptions made by GHD described in this report. GHD disclaims liability arising from any of the assumptions being incorrect.

GHD has prepared this report on the basis of information provided by Sullivans Cove Distillery Pty Ltd and others who provided information to GHD (including Government authorities), which GHD has not independently verified or checked beyond the agreed scope of work. GHD does not accept liability in connection with such unverified information, including errors and omissions in the report which were caused by errors or omissions in that information.

1.3 Assumptions

This Traffic Impact Assessment was developed based on the following assumptions, as well as other assumptions documented in this report:

- The proposed development operates under normal conditions and event conditions. This report assesses its operation under normal conditions only. Event conditions will require a dedicated traffic management plan.
- The peak headcount of visitors under normal operations is around 75. This includes those attending the guided tours.
- The weekday headcount of staff is 20 under normal conditions; and this is consistent throughout the entire opening period between 8:00 am and 6:00 pm.
- The main entry point to the development site (Drill Forecourt) is intended for coach, private vehicle (accessible car parking), pedestrian and cyclist access.
- Service/delivery vehicle access is via the existing access from the Domain slip yard, 1 McVilly Drive.

1.4 References

The following sources have been used as referenced within this report:

- *Queens Domain Master Plan 2013-2033*
- *Sullivans Cove Planning Scheme 1997*
- AS/NZS 2890.6:2009 *Parking facilities, Part 6: Off-street parking for people with disabilities.*
- Austroads Design Vehicles and Turning Path Templates Guide, 2013.
- Five-year crash history, Department of State Growth.
- Traffic data on the Tasman Highway collected for the period between 27 June 2021 and 3 July 2021, Department of State Growth.
- 2109-DA0102 *Proposed – First Floor Plan*, Rev5, 10 September 2021, John Wardle Architects.
- L001, *Landscape Site Plan – Phase 1 Works Only*, Rev C by Realmstudios.
- L100, *General Arrangement Plan 01*, Rev C by Realmstudios.
- 0598.085-C111, RevB, *Regatta Ground Car Park Design – Civil Site Line Marking and Signage*, 29 October 2019, Cova Group.

2. Existing conditions

2.1 Subject site

The subject site is located at “Huon Quays” at 13 and 17 McVilly Drive, Queens Domain as shown in Figure 2.1. The site is located on the waterfront of the Derwent River between the TasPorts Domain slip yard to the west and the Royal Hobart Regatta Pavillion to the east. The Cenotaph memorial park is to the south.

The subject site spans two existing titles, where the northern site has an area of 3,804 m² and the southern site has an area of 3,067 m². Vehicular access to the southern site relies on 1 McVilly Drive to the west as a shared access with the adjacent slip yard, or via 20 McVilly Drive to the east which is accessed via Crown Land through the adjacent public car park access. The northern site has vehicular access from 20 McVilly Drive to the east only.

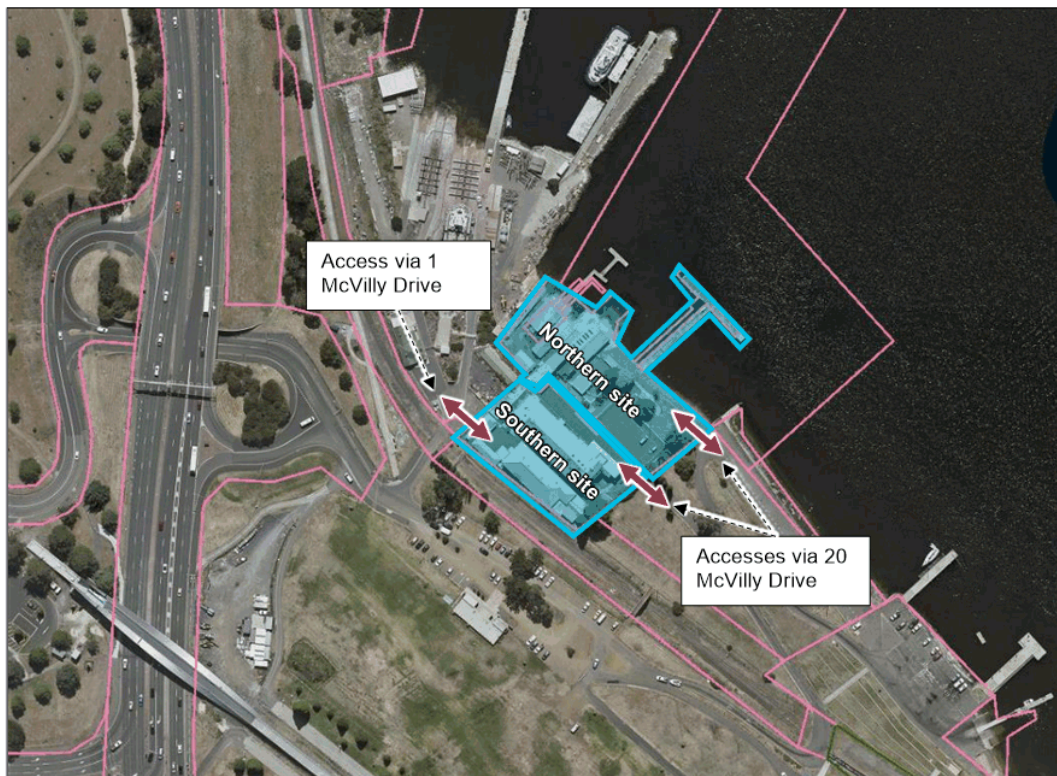


Figure 2.1 Subject site location

Data source: theLIST (www.thelist.tas.gov.au), Tasmanian Government (2021)

2.2 Planning and land use

The site is subject to the provisions of the *Sullivans Cove Planning Scheme 1997* (SCPS). As shown in Figure 2.2, the site is located within the Regatta Point planning zone and is immediately adjacent to the Domain Open Space Zone.

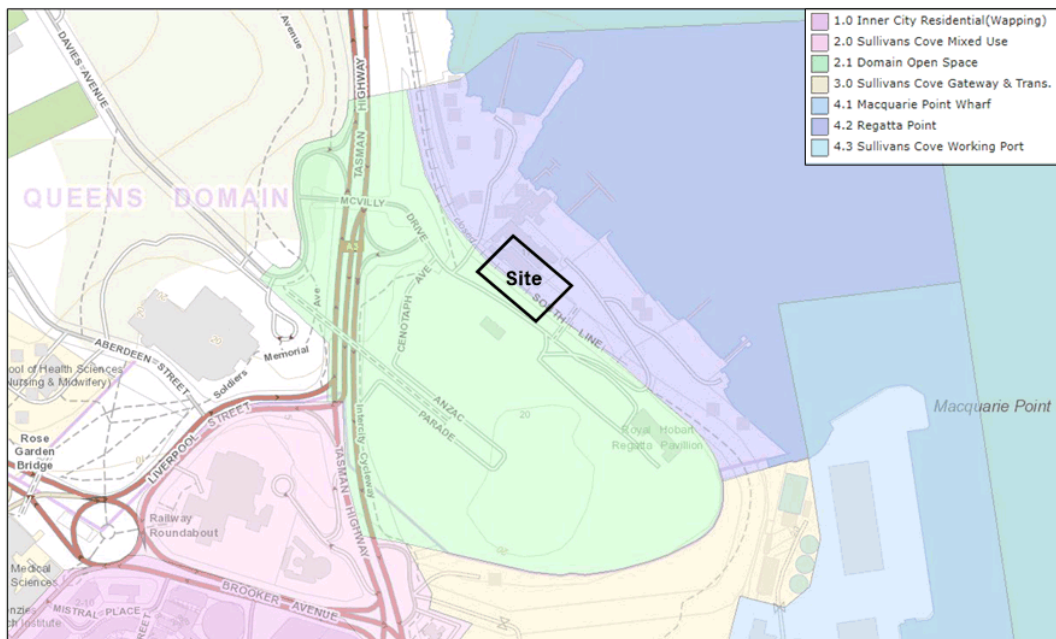


Figure 2.2 Current planning zones within Sullivans Cove

Data source: theLIST (www.thelist.tas.gov.au), Tasmanian Government (2021)

The majority of the buildings at the site have been vacant for a number of years. TasPorts has advised the most recent use of the main building, to the north, was (informally) a function and meeting area. The smaller buildings situated across the two site titles were historically used as a single-person office occupied by TasPorts and a block of toilets and staff change rooms used by TasPorts personnel. The site also has pedestrian access to a jetty.

The site historically had a tennis court which has been converted into a small car park with a supply of eight spaces with no accessible car parking spaces currently included. This car park is for private use by the site and is securely gated from public access.

The SCPS recognises that the Regatta Point activity area is characterised by low volumes of traffic and suggests that car parking should be provided to meet the needs of employees and visitors within the area, without providing excess parking to meet parking demand for activities outside of the area. The site is heritage listed which further limits the ability for additional parking to be provided on-site.

The Hobart City Council's *Queens Domain Master Plan 2013-2033* identifies a series of upgrades proposed in the Regatta Grounds and Cenotaph areas which are likely to interface with the Huon Quays subject site. An excerpt of the precinct plan is shown in Figure 2.3. The area has already been upgraded with the Bridge of Remembrance which provides pedestrian connectivity between Queens Domain and the Cenotaph over the Tasman Highway and an off-road cycle path for the Intercity Cycleway / Hobart Bike Track north of Huon Quays. The Cenotaph gravel car park is currently being upgraded to a formal facility, with opportunity for extension between the car park and the Royal Hobart Regatta Grounds. The following lists the potential upgrades in the future:

- Transit plaza for buses, taxis and a bicycle station between Huon Quays and the Cenotaph car park, with a new bridge across the rail corridor. This new bridge may replace the existing non-compliant pedestrian bridge across the decommissioned rail line.
- Shared Intercity Cycleway path to connect the existing facilities between the Hobart Bike Track and the Regatta Grounds alongside the rail corridor.
- Extended Cenotaph car parking area which would accommodate bus drop off/pick up and lay-by.
- Upgraded Regatta Grounds car park driveway with improved street and path lighting.



Figure 2.3 Excerpt of the Cenotaph / Regatta Ground Precinct Plan

Data source: "Queens Domain Master Plan 2013-2033", Hobart City Council (2013)

2.3 Road network

The road network in the vicinity of the subject site, shown in Figure 2.4, consists of the following roads:

- McVilly Drive
- Davies Avenue
- Unnamed road (Cenotaph Domain)
- Tasman Highway

These roads are discussed in more detail in the following sections.

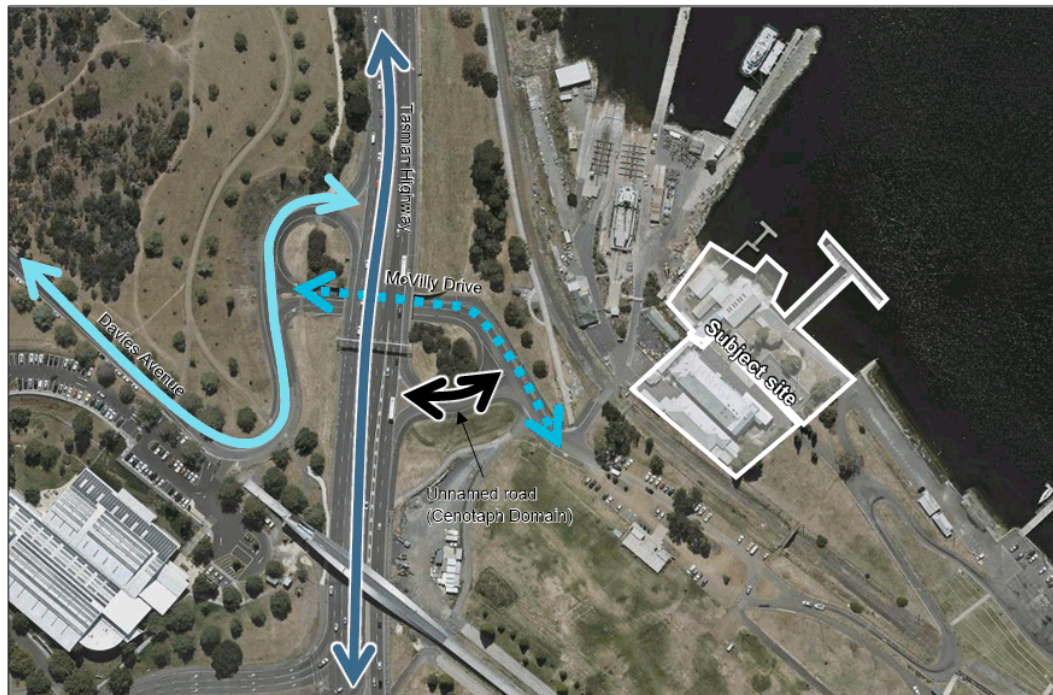


Figure 2.4 Road network

Data source: theLIST (www.thelist.tas.gov.au), Tasmanian Government (2021)

2.3.1 McVilly Drive

McVilly Drive is a local access road managed by the City of Hobart. It connects Davies Avenue to the west and the Regatta Grounds car park to the east, via an underpass below the Tasman Highway. McVilly Drive provides access to the subject site via existing accesses at 1 McVilly Drive and the Regatta Grounds car park.

The intersection of McVilly Drive and Davies Avenue is give-way controlled with McVilly Drive being the minor road. A pedestrian refuge crossing is provided across McVilly Drive adjacent to this intersection.

McVilly Drive is configured as a two-way, two-lane road with no on-street parking permitted. A shared path is provided along its eastbound traffic lane, connecting the Intercity Cycleway and Davies Avenue.

The default speed limit on McVilly Drive is 50 kilometres per hour (km/h).

2.3.2 Davies Avenue

Davies Avenue is a local collector road managed by the City of Hobart. It connects Upper Domain Road to the south-west and the Tasman Highway (northbound carriageway) to the north-east. The intersection of Davies Avenue and the Tasman Highway is left-in and left-out only with accelerating and decelerating slip lanes provided on and off the Tasman Highway.

Davies Street is configured as a two-way, two-lane road with on-street parking permitted in sections north of the Hobart Aquatic Centre car park access. Davies Street is a recognised cycling route connecting the Hobart Aquatic Centre/Soldiers Memorial Avenue to the Intercity Cycleway (via McVilly Drive and the Bridge of Remembrance). Signage is provided on Davies Avenue between the Tasman Highway and the Aquatic Centre to advise drivers of bicycle crossing movements in the area.

The posted speed limit on Davies Avenue is 50 km/h.

2.3.3 Unnamed road (Cenotaph Domain)

Unnamed road is a local collector road managed by the City of Hobart. It is a short, two-way, two-lane link road that connects the Tasman Highway (southbound carriageway) to the west and McVilly Drive to the east. The intersection of Unnamed Road and the Tasman Highway is left-in and left-out only with a slip lane provided on the Tasman Highway to Unnamed Road. Left-turn out movements to the Tasman Highway are give-way controlled.

The default speed limit on Unnamed Road is 50 km/h.

2.3.4 Tasman Highway

The Tasman Highway is a Category 1 state road managed by the Department of State Growth. It is a primary freight and passenger road that supports key inter-regional movements across Tasmania. Near the subject site, its northbound carriageway connects to Davies Avenue and its southbound carriageway connects to Unnamed Road (Cenotaph Domain).

Based on 2021 traffic data collected by the Department of State Growth at a location approximately 400 metres north of Lower Domain Road, the average mid-week peak period traffic volumes on the Tasman Highway are as follows:

- AM peak (8:00 am – 9:00 am) 4162 vehicles/hr (two-way)
- Midday peak (12:00 – 1:00 pm) 3647 vehicles/hr (two-way)
- PM peak (4:00 pm – 5:00 pm) 5452 vehicles/hr (two-way)

The posted speed limit on the Tasman Highway in the vicinity of the subject site is 70 km/h.

2.4 Public transport network

The public transport network in the vicinity of the subject site consists of Metro Tasmania and Tassielink bus routes as shown in Figure 2.5. Buses operate along both northbound and southbound carriageways of the Tasman Highway between Hobart CBD and the Tasman Bridge. The closest bus stop to the subject site is located adjacent to 40 Tasman Highway and the Tasman Highway/Lower Domain Road intersection, approximately 260 metres north-west of the subject site.

It is noted that there is a railway line along the southern boundary of the subject site. The railway line and railway crossing across the access road of 1 McVilly Drive (Domain slip yard) is not currently in use as part of the rail network, however, the railway crossing is utilised by vehicles crossing into the TasPorts slipway site.



Figure 2.5 Bus network

Data source: theLIST (www.thelist.tas.gov.au), Tasmanian Government (2021)

2.5 Active transport network

The active transport network in the vicinity of the subject site consists primarily of the Intercity Cycleway and a network of connected shared used paths and footpaths linking the Hobart Cenotaph, Hobart Aquatic Centre, The Soldiers Memorial Avenue, Tasmanian Botanical Gardens and Hobart CBD. The Tasman Highway acts as a barrier for active transport modes, however, access under and over the Tasman Highway is provided via McVilly Drive and the Bridge of Remembrance.

The Intercity Cycleway is a shared use corridor connecting Claremont to the north and the Hobart Regatta Grounds to the south. It is a key commuter and recreational route for pedestrians and cyclists travelling to and from the Hobart CBD. The Intercity Cycleway and the connecting shared use path network are shown in Figure 2.6.

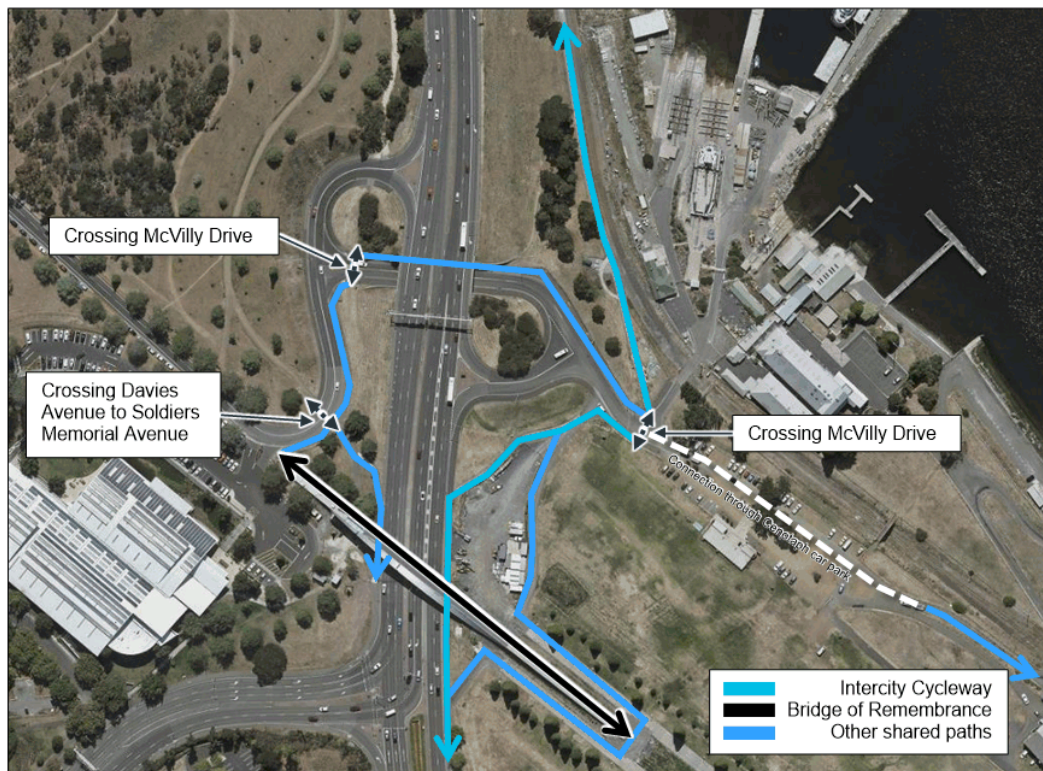


Figure 2.6 Shared path network connecting the Intercity Cycleway

Data source: theLIST (www.thelist.tas.gov.au), Tasmanian Government (2021)

Approximately 40 metres south-east of the subject site is a pedestrian overpass, which connects the Cenotaph car park across the decommissioned rail line. The active transport network immediately surrounding the subject site is otherwise limited to informal tracks and pathways.

The proposed upgrade of the Cenotaph car park will include new footpaths, thereby enhancing pedestrian connectivity in the immediate area with linkages to the external network.

2.6 Car parking

There are two car parks located near the subject site as shown in Figure 2.7. They comprise of the Cenotaph car park (96 spaces) located south of the subject site and the decommissioned rail line, and the Regatta Ground car park (50 spaces) located east of the subject site along the River Derwent. Parking restrictions apply within these car parks on weekdays between 8:30 AM and 6:00 PM.

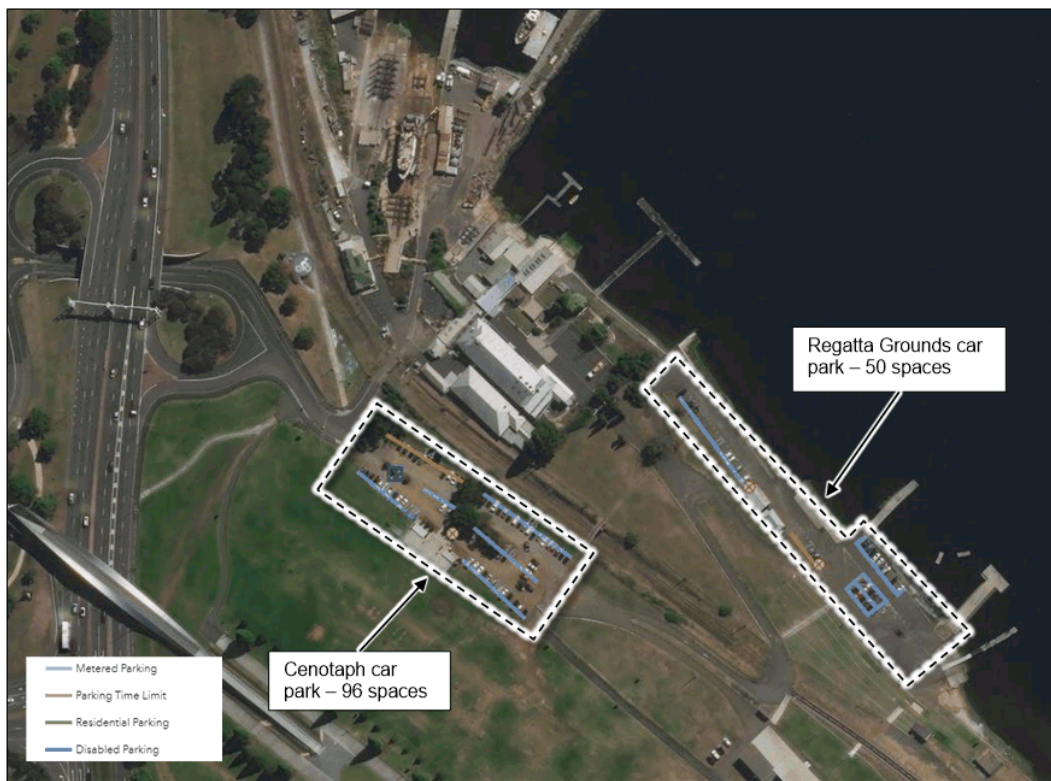


Figure 2.7 Car parking

Source: City of Hobart: Parking (accessed September 2021)

There is a combined total of 146 car parking spaces across the two car parks, including two accessible parking spaces. A breakdown of parking spaces by restriction type is outlined as follows:

- Cenotaph car park (existing)
 - 3-hour parking: 14 spaces
 - Metered parking: 80 spaces
 - Accessible parking (3P): 2 spaces
- Regatta Grounds car park
 - 3-hour parking: 7 spaces
 - Metered parking: 43 spaces

The Cenotaph and Regatta Grounds car parks are used by visitors of surrounding land uses within the Hobart Cenotaph and the Regatta Grounds. It is known that the Regatta Grounds car park is inundated during high tide events.

Council have used the Cenotaph area as an overflow car park for events including Christmas Shopping, Salamanca markets etc. Cyclists park at the Cenotaph and Regatta Grounds car parks before commencing rides along the Intercity Cycleway.

It is noted that the Queens Domain Master Plan recommends the future development of overflow car parking areas to accommodate parking demand.

2.6.1 Cenotaph car park – proposed upgrade

As mentioned in Section 2.2, the Cenotaph car park is currently being upgraded by the City of Hobart. Works will result in a net decrease of 19 parking spaces to 77 parking spaces. The breakdown of parking spaces in the upgraded car park will be as follows:

- 3-hour parking: 17 spaces
- Metered parking: 58 spaces
- Accessible parking (3P): 2 spaces

The proposed car park upgrade will improve safety and accessibility of car parking facilities in the area and provide enhanced pedestrian connections. A hatched area is located at the southern end of the car park, providing a turning circle for vehicles.

2.7 Crash history

Crash data for the study area covering the five-year period April 2016 to March 2021 was obtained from the Department of State Growth. Over this period, there were ten crashes recorded on the local, access and collector roads and their intersections with the Tasman Highway. A summary of this crash data is provided in Table 2.1. The locations of these crashes in the study area and their severities are shown in Figure 2.8, along with the locations of crashes on the Tasman Highway in the area.

Table 2.1 Summary of recent crash history near Huon Quays

Location	Number of crashes		Crash type(s)
	Total	Casualty	
Midblock			
Davies Avenue	1	0	Off path on curve (1)
McVilly Drive	1	0	Off path on straight (1)
Queens Domain car park	1	0	Pedestrian (1)
Intersection			
Davies Avenue / McVilly Drive	1	0	Intersection cross traffic (1)
Davies Avenue / Tasman Highway	2	0	Rear end / lane change (2)
Unnamed Road (Cenotaph Domain)/Tasman Highway	4	1	Rear end / lane change (3), head on (1)
TOTAL	10	1	

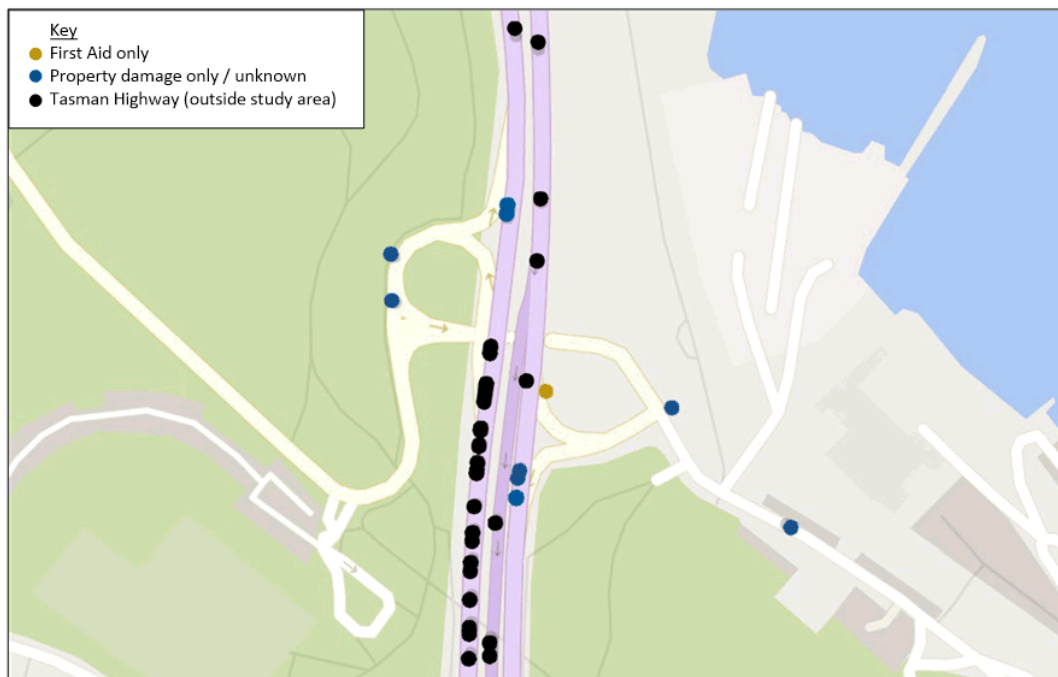


Figure 2.8 Recent crash locations near Huon Quays (April 2016 – March 2021)

Data source: Department of State Growth (2021)

There were 31 crashes on the Tasman Highway in the area, which were predominantly 'rear end / lane change' type crashes (94%). Three of the 31 resulted in minor injury, and the remaining resulted in property damage only. Thirteen percent of these crashes occurred in darkness (with streetlights), and the remaining 87% occurred in daylight conditions.

There were three midblock crashes in the study area, on Davies Avenue, McVilly Drive, and the Queens Domain car park. The crash on Davies Avenue was an 'off path on curve' type crash that occurred during daylight conditions and resulted in property damage only. The crash on McVilly Drive was an 'off path on straight' type crash that occurred in darkness (with streetlights) and resulted in property damage only. The crash in the Queens Domain car park was a 'pedestrian' type crash that occurred during daylight conditions, with an unknown severity.

There were seven intersection crashes in the study area, one at the intersection of Davies Avenue and McVilly Drive, two at the intersection of Davies Avenue and the Tasman Highway, and four at the intersection of Unnamed Road (Cenotaph Domain) and the Tasman Highway, all of which occurred during daylight conditions. The crash at the intersection of Davies Avenue and McVilly Drive was an 'intersection cross traffic' type crash that resulted in property damage only. Two crashes were recorded on the entry to the Tasman Highway from Davies Avenue, both were 'rear end / lane change' type crashes and resulted in property damage only. Four crashes were recorded at the intersection of McVilly Drive and the Tasman Highway, one at the exit from the Tasman Highway to McVilly Drive and the other three on the entry from McVilly Drive to the Tasman Highway. The crash at the exit from the Tasman Highway to McVilly Drive was a 'head on' type crash resulting in first aid. All three crashes at the entry from Unnamed Road (Cenotaph Domain) to the Tasman Highway were 'rear end / lane change' type crashes resulting in property damage only.

3. Proposed development

3.1 Development design

The proposed development comprises the new Sullivans Cove Distillery at 13 and 17 McVilly Drive. It consists of two main building structures amongst a curated landscape. These structures are situated in the southern section of the development site at 17 McVilly Drive, and include:

- The restored Drill Hall and Caretaker's Residence: the northern structure used for tastings and retail.
- The Production Hall: the southern structure used for production and processing. It also houses an elevated mezzanine used for hosting guided tours.

A plan of the proposed building area is shown in Figure 3.1.

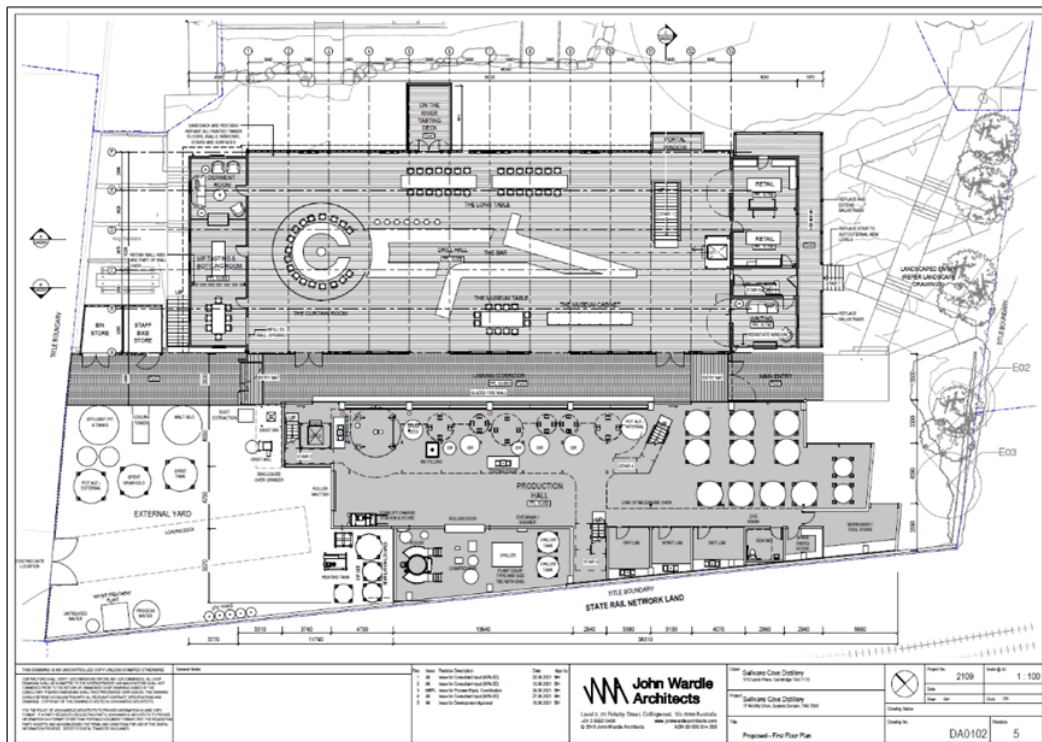


Figure 3.1 Building plan

Source: 2109-DA0102 Proposed – First Floor Plan, Rev5, 10 September 2021, John Wardle Architects.

3.2 Parking provisions

The following parking provisions, shown in Figure 3.2, are provided as part of proposed developments:

- 3 x accessible parking spaces in the Drill Forecourt.
- 12 x visitor bicycle parking spaces in the Drill Forecourt.
- Staff-only bike store.
- Turning area in Drill Forecourt for pick-up/drop-off.

The Drill Forecourt would be constructed of compacted gravel which provides a smooth surface that supports pedestrians, access for people with disabilities and bicycle access.

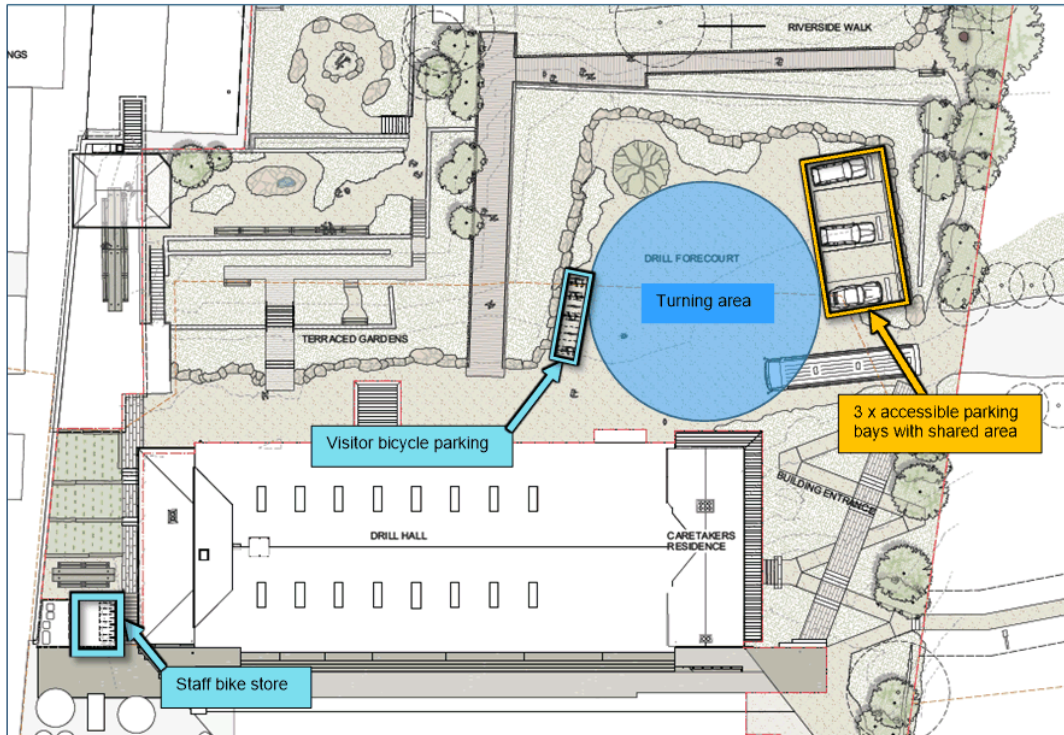


Figure 3.2 On-site parking

Source: L001, Landscape Site Plan – Phase 1 Works Only, Rev C by Realmstudios.

3.3 Access arrangement

The three existing vehicle accesses to the subject site, shown in Figure 3.3, are to be retained as part of the proposed developments.

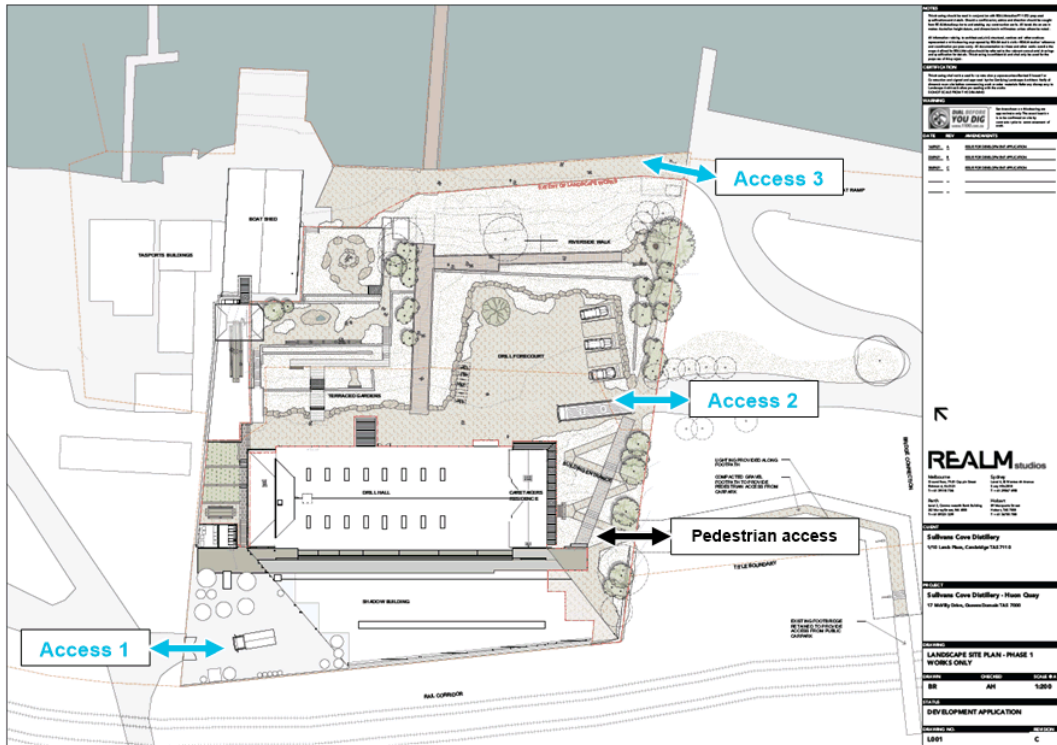


Figure 3.3 Accesses to development site

Source: L001, Landscape Site Plan – Phase 1 Works Only, Rev C by Realmstudios.

Access 1 connects to the existing Domain slip access at 1 McVilly Drive and is intended for use by staff and service and delivery vehicles. The maximum service vehicle size proposed for use is the equivalent of the MRV Design Vehicle as set out in Australian Standard AS 2890.2 (2018).

Access 2 is the main entry point to the Drill Forecourt connecting the pedestrian bridge over the rail line, and the existing access road through 20 McVilly Drive and Cenotaph car park. It provides access for coaches, cyclists, and private vehicles (3 x accessible parking spaces only). The maximum vehicle size proposed for use is the equivalent of a 12.5 M Bus as set out in the Austroads Guide (2013).

Access 3 is closed to public vehicles; primarily providing future access to the jetty and Building 9 'boat shed' (not part of this development).

The existing access road connecting Access 2 and 3 is currently used by vehicles to and from the Regatta Grounds car park. The access road is approximately 3.5-4.2 metres in width, except at a curve in the road across an inactive railway crossing (approximately 9-10 metres wide). The existing road width does not allow for passing vehicles, except for smaller vehicles at the railway crossing. A minimum width of 3.1 metres is required for bus access through this existing access road, and this requirement will be larger at the curve in the road to accommodate turning manoeuvres (refer Appendix A). This access road is proposed for upgrades as part of future works indicated in the Queens Domain Master Plan.

Pedestrian access to the site is provided via a dedicated footpath (approx. 1.3 metres wide) connecting the main access (Caretakers Residence) and the existing pedestrian railway bridge.

3.3.1 Vehicle swept paths

To ensure the development site accesses can adequately accommodate the proposed vehicle movements, swept paths have been undertaken at Access 1 and Access 2. The largest vehicles tested at these accesses are as follows:

- Access 1
 - Australian Standard AS 2890.2 (2018) MRV equivalent: 8.8 metres long
- Access 2
 - Austroads 2013 Bus equivalent: 12.5 metres long

The swept path assessment indicates that:

- The MRV Design Vehicle is able to enter Access 1 by undertaking a forward manoeuvre into the Domain slip yard area before reversing into the development site itself. Exit movements are executed via a forward motion into the Domain slip yard before a left turn is taken onto McVilly Drive. Refer to SK001 and SK002 in Appendix A.
- A standard 12.5 metre bus can undertake a three-point turn to manoeuvre in and out of the Drill Forecourt and Access 2. Refer to SK006 in Appendix A. Smaller vehicles up to a length of up to 10 metres can also complete a full circular movement within the Drill Forecourt.
- A standard 12.5 metre bus is able to undertake a turning manoeuvre within the existing access road (to Access 2/Drill Forecourt) across the inactive railway crossing. Refer to SK007 in Appendix A. This bus is of similar length to vehicles currently using the access road such as a passenger car towing boat. Given the width of this road (approx. 3.5-4.2 metres), passing opportunities along this access road are already limited to the wider railway crossing area. The swept path assessment indicates that passing opportunities at this location with the subject bus are likely to be limited to smaller vehicles such as an Austroads 2013 Passenger Car. This is considered acceptable based on low expected bus volumes to the development site (considering the peak on-site head count) and the comparable length of vehicles that currently use the road.

Refer to Appendix A for the swept paths.

Appropriate signage is recommended at the accesses to advise of the maximum vehicle sizes permitted. Should the volume of buses accessing the Drill Forecourt increase noticeably, treatments could be considered to manage two-way bus access along the existing road. This could include advisory signage of road-width limitations, or road widening at the railway crossing.

4. Impact assessment

4.1 Trip generation

4.1.1 Staff and visitors

The trip generation of the proposed development is based on a combined head count of staff members and visitors (members of the public), and the number of service/delivery vehicles accessing the site.

The hourly head count would increase linearly in the morning from opening at 8:00 am, peaking at lunchtime between 12:00 pm – 1:00 pm, before decreasing linearly in the afternoon to closing at 6:00 pm. The peak on-site head count is 75 people based on:

- 20 staff members from opening to closing (8:00 am – 6:00 pm) on weekdays. There are approximately 15 staff members on weekends.
- 55 visitors during peak hour at lunchtime (12:00 pm – 1:00 pm). The number of visitors is assumed to increase linearly in the morning and decreases linearly in the afternoon on either side of the midday peak. Patrons attending guided tours are included in this count.
- Total daily visitation is assumed to be around triple the midday peak headcount, equating to around 165 visitors daily.

Two trips, an inbound and outbound trip, are associated with each person using the site across the course of a day. On this basis, the estimated daily trip generation is as follows:

- Staff 40 trips per day (two-way)
- Visitors 370 trips per day (two-way)

Trip activity is likely to peak three times throughout the day, represented by the AM Commuter peak period, the Midday peak period, and the PM Commuter peak period. Anticipated trip generation during these times are outlined below:

- AM Peak ~30 trips per hour (two-way)
- Midday Peak ~60 trips per hour (two-way)
- PM Peak ~30 trips per hour (two-way)

Visitation throughout the remainder of the day would be in the range of 30-40 trips per hour (two-way), comprising mostly of visitors travelling to and from the site for tours or tastings.

It is noted that most of the trips generated would comprise of alternative modes of transport to private car. This is discussed in Section 4.1.2.

4.1.2 Mode share

The proposed development does not provide for on-site parking beyond three accessible car parking spaces and bicycle parking. The intention is to preserve the form and character of Sullivans Cove by discouraging the use of private vehicles in favour of alternative forms of transport. The bulk of vehicle and people trips to and from the development site are therefore distributed amongst the following travel modes:

- By private vehicle. Visitors/staff will need to be dropped-off/picked-up, or park at the Cenotaph and Regatta Grounds car parks located nearby. There are approximately 127 parking spaces in these locations.
- By active transport via the adjacent Intercity Cycleway, and McVilly Drive/Bridge of Remembrance from Liverpool Street. A bike store is to be provided for staff, and 12 bicycle parking spaces are proposed on-site for visitors. Travel by active transport is encouraged to/from land uses located in Sullivans Cove.
- By bus/coach (tourist transport) with drop-off/pick-up proposed within the Drill Forecourt. A typical coach contains approximately 40-50 seats. Minibuses may also be in use, and they typically accommodate approximately 12-30 seats. Limited on-site car parking provides for bus access and turning within the Forecourt.

- By public transport (bus). The closest bus stop (southbound) is located approximately 260 metres north-west of the development site on the Tasman Highway (refer back to Section 2.4). The northbound bus stop on the Tasman Highway is located even further north, and therefore a closer bus stop in the CBD will likely be more utilised.
- Travel by uber or taxi with drop-off/pick-up within the Drill Forecourt, or temporary parking sections provided within upgraded Cenotaph car park.
- Pedestrian arrival by foot utilising the existing network of pathways that service the site from Hobart and surrounding area.

4.1.3 Service vehicles

In addition to the trips outlined above, there would be approximately six service/delivery vehicles expected to access the development site on a weekly basis. This equates to an average of around two light truck trips each day on average (one entry movement and one exit movement).

4.2 Trip distribution

The directional split of daily traffic volumes on the Tasman Highway is approximately 50/50 in north and south directions based on traffic counter A0113110 located approximately 400 metres north of Lower Domain Road. The likely trip distribution of the development site is therefore similar, noting a potentially higher percentage of trips to and from the south due to proximity to the Hobart CBD and surrounds. City office/retail workers and CBD residents are within walking and cycling distances of the site. Tourist transport modes are also likely to originate in and around Hobart CBD due to the concentration of hotels and short-term stay accommodation in the area.

4.3 Parking requirements

The on-site parking provisions proposed as part of this development are limited to accessible car parking and bicycle parking. By providing limited parking on-site, visitors and staff members will be required to travel via alternative forms of transport, or park at the adjacent Cenotaph and Regatta Grounds car parks. This is considered to align with the objectives outlined in the *Sullivans Cove Planning Scheme 1997* based on the following points:

- Car parking is considered detrimental to the character and form of the Cove and site and should be limited as part of new developments (Section 26.1, Planning Scheme).
- The site is heritage listed which limits the ability for additional parking to be provided on site. The available space has been allocated to bus access and turning on-site, rather than private car parking, to maximise the number of guests that can travel to the site.
- The provision of on-site bicycle parking and the proximity of the development site to the shared path network (including the Intercity Cycleway) connecting the Hobart CBD encourages and facilitates travel by active transport modes.
- Travel by private vehicle is only one of several possible means of travel to the development site, therefore only a portion of the trips generated by the development will travel by private vehicle (parking). Parking demand is not expected to be significant as a result and will be accommodated within the adjacent Cenotaph and Regatta Grounds car parks.
- The three accessible car parking spaces will generate minor volumes of vehicle movements within the development site. This limits the exposure of pedestrians to vehicles and supports pedestrian safety.

To further support the use of alternative modes of transport, it is recommended that incentives be explored to encourage staff (and visitors where possible) to travel by active and public transport modes. Limitations to on-site and nearby parking provisions could also be communicated to inform visitors.

4.3.1 Car Park layout

The development proposes three angled accessible parking spaces on either side of a shared area as shown in Figure 4.1. Each of the dedicated parking spaces and the shared areas has dimensions of 2.4 m x 5.4 m (width x length).

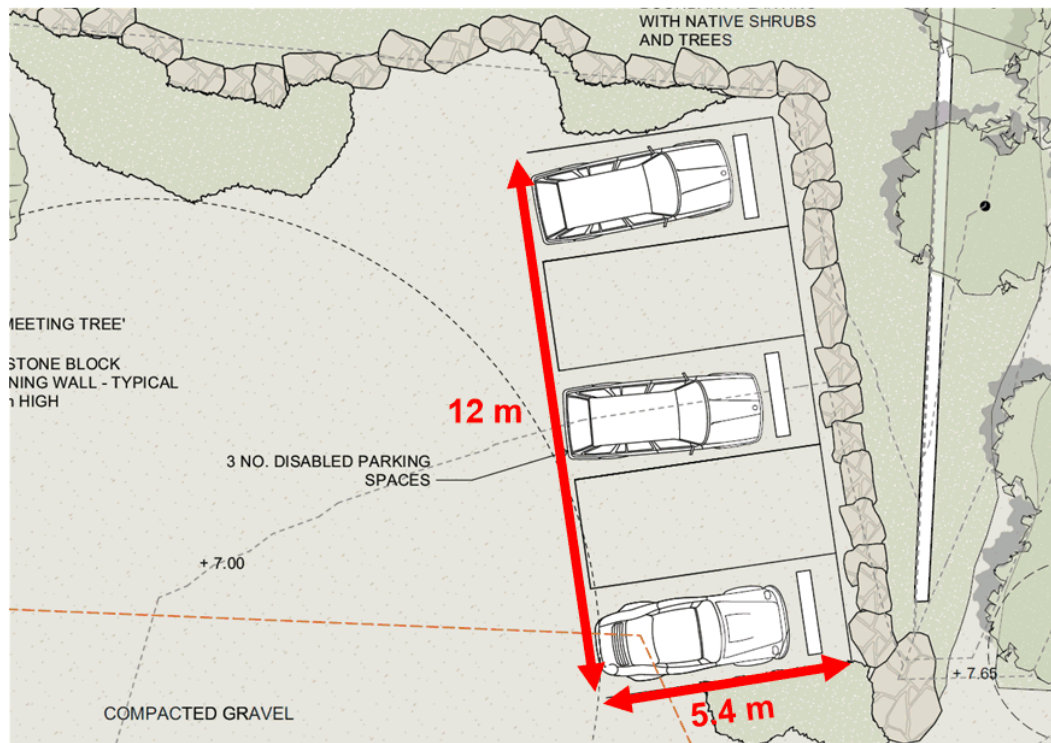


Figure 4.1 Proposed accessible parking

Source: L100, General Arrangement Plan 01, RevC, by Realmstudios.

Accessible parking spaces are required to comply with the parking dimensions specified in AS/NZS 2890.6:2009 *Parking facilities, Part 6: Off-street parking for people with disabilities*. Based on the standard dimensions shown below in Figure 4.2, the proposed car park layout is considered to comply with Australian Standards subject to the provision of bollards within the shared areas as shown in Figure 4.3.

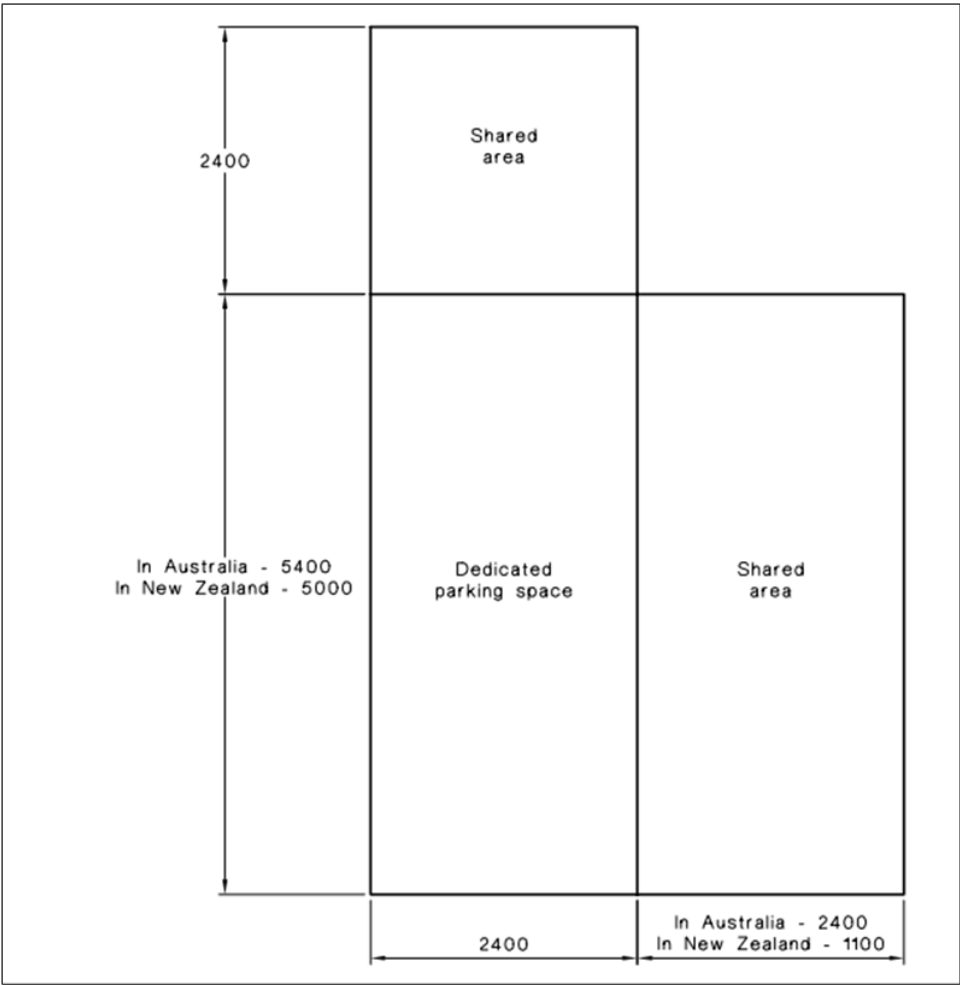


Figure 4.2 Dimensions of angle parking spaces

Source: AS/NZS 2890.6:2009 *Parking facilities, Part 6: Off-street parking for people with disabilities*

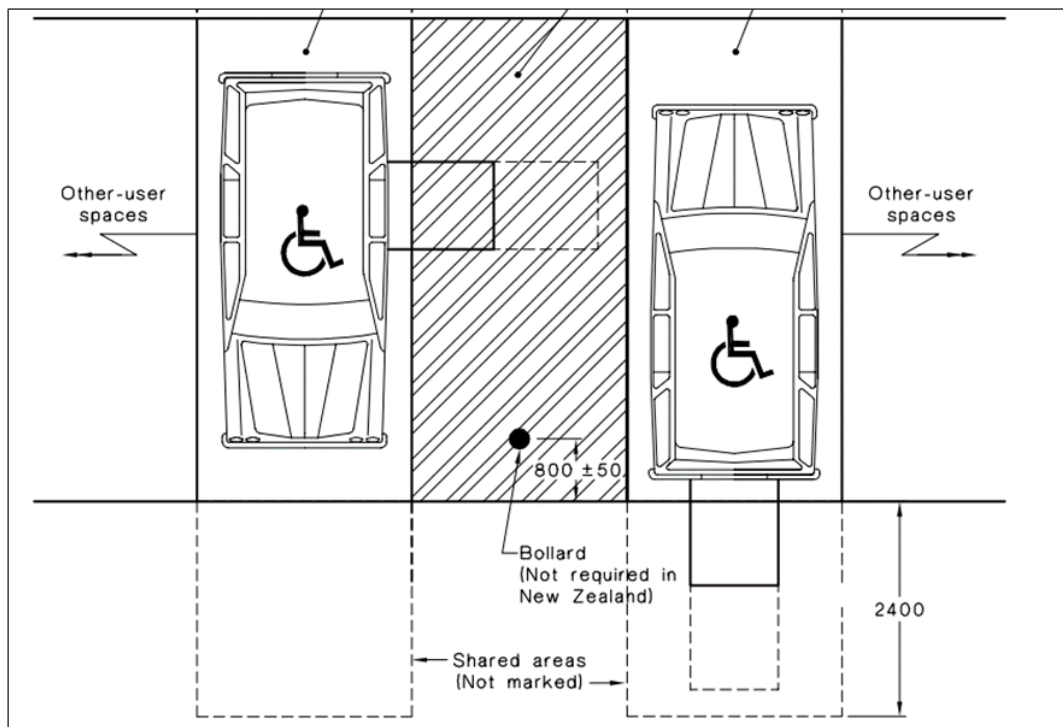


Figure 4.3 Bollard requirement

Source: AS/NZS 2890.6:2009 *Parking facilities, Part 6: Off-street parking for people with disabilities*

4.4 Impacts to traffic efficiency

The majority of the trips generated by the development will comprise of alternative transport modes to private vehicles. Any increase in traffic volume in the road network, due to the proposed development, is expected to peak around midday. This is outside of the AM and PM road peaks. Vehicle trips generated by the development during the road peaks will be approximately half of what it is during midday.

The Tasman Highway provides access to the development site via Davies Avenue and Unnamed Road (Cenotaph Domain). Considering the low-impact left-in/left-out only access arrangements at these intersections, any minor increase in vehicle trips will be absorbed by the road network without noticeable impacts.

4.5 Impacts to local amenity

No significant impacts to local amenity are foreseen for the proposed developments. This is based on the following points:

- Limited on-site parking encourages other modes of travel to the development site, limiting the impact of vehicle traffic on the form and character of the Cove.
- The development does not introduce any new accesses.
- The frequency of truck movements proposed across the Intercity Cycleway, to and from the Domain slip yard access, is low.

Furthermore, future upgrade of the adjacent Cenotaph car park will improve public amenities and the place value of the area.

4.6 Impacts to road safety

In reference to the crash history, it is acknowledged that there is a trend in rear end/lane change crashes at the Tasman Highway intersections with Davies Avenue and Unnamed Road (Cenotaph Domain). This may be due to a combination of factors, including high traffic volumes, high vehicle speeds and multi-lane carriageways on the Tasman Highway. This development is unlikely to significantly increase the crash risk at these junctions due to the following points:

- The majority of the trips generated by the development are likely to comprise of alternative transport modes to private vehicle.
- Peak trip generation of the development occurs during the midday period which is outside of the road peak periods on the Tasman Highway.
- Left-turn out movements per hour to the Tasman Highway would be dispersed across the Davies Avenue and Unnamed Road (Cenotaph Domain) intersections.
- Slip lanes are provided for vehicles turning left from the Tasman Highway onto Davies Avenue and Unnamed Road (Cenotaph Domain), providing a safer environment for decelerating vehicles.
- There is adequate sight distances at the Davies Avenue and Unnamed Road (Cenotaph Domain) intersections for vehicles merging onto the Tasman Highway.

No significant detrimental road safety impacts are foreseen for the proposed development. This is based on the following points:

- There is sufficient capacity in the road network generally to accommodate the proposed traffic volume with no major decrease in performance.
- The development does not introduce any new accesses.
- The development seeks to reduce trip generation within the Cove and along an existing access road by providing limited on-site parking in accordance with Section 26.1 of the Planning Scheme. Parking demand is to be accommodated by nearby public car parks with existing vehicle movements.
- The development proposes low frequency truck access to the site via Domain slip yard.
- Future upgrade of the adjacent Cenotaph car park will introduce new footpaths, improving pedestrian access to the development site.

It is noted that the Intercity Cycleway crosses McVilly Drive adjacent to accesses to Domain slip yard and Cenotaph car park, exposing cyclists and pedestrians to vehicles on McVilly Drive. This development is unlikely to significantly increase the pedestrian and cyclist crash risk here due to the above points, however, Council may consider upgrades to this crossing as part of the Cenotaph car park works.

5. Planning Scheme assessment

The development site is located within Activity Area 4.2 Regatta Point and therefore must comply with the requirements in the following sections of the *Sullivans Cove 1997 Planning Scheme*:

- Chapter 20 Activity Area 4.2 'Regatta Point'.
- Schedule 5 – Traffic, Access, and Parking.

Responses to these requirements are provided in the below sections.

5.1 Traffic, access, and parking

Scheme Provision	Development Response
26.2 Principles The following Principles of development control shall apply to the assessment of traffic, access, and parking within Sullivans Cove:	
It will facilitate the mutual compatibility of public and private transport;	Clause considered to be satisfied based on the following: <ul style="list-style-type: none"> – No loss of connection to bus stops via the Intercity Cycleway. – Limited on-site parking discourages private car use and encourages use of active transport modes. – Travel by walking and cycling has a smaller footprint and greater compatibility with public transport.
It can demonstrate either: <ul style="list-style-type: none"> – that it will not create traffic flows and movements that are detrimental to safety or amenity; or – that any such flows or movements can be accommodated through engineering works or traffic management arrangements – on or off site – that are satisfactory to the Planning Authority; 	Clause considered to be satisfied based on the following: <ul style="list-style-type: none"> – Proposed on-site car parking is limited to three accessible parking spaces, therefore limiting the number of vehicle movements accessing the site and their impacts. – The remainder of the development site's car parking needs to be accommodated within existing adjacent car parks with existing parking movements.
It can make adequate provisions for the direction, access, turning and parking of all vehicular traffic.	Clause considered to be satisfied based on the following: <ul style="list-style-type: none"> - Swept paths undertaken for service/delivery vehicles (refer to Appendix A). - Swept paths undertaken for coach access within Drill Forecourt (refer to Appendix A).
It can provide for pedestrian movement, in accordance with the requirements of the Planning Authority.	Clause considered to be satisfied based on the following: <ul style="list-style-type: none"> - Reduced vehicle access within Drill Forecourt due to limited parking provisions. - ~1.6-metre-wide footpath provided along main entry access road (to Drill Forecourt) and connecting existing pedestrian bridge across decommissioned rail line. - New footpath links to the site will be developed as part of the Cenotaph car park upgrade.
Unless particularly specified as a policy objective or requirement for all or part of an Activity Area or approved as part of the guidelines for the redevelopment of a Key Site, developments will not be expected to incorporate on-site vehicle parking and it will only be approved to the extent that the car parking does not direct the design of the development and does not have a detrimental effect on the form and character of Sullivans Cove or on its pedestrian amenity and activity. Any development necessitating an exercise of discretion under the Scheme in relation to height to facilitate the provision of onsite parking would not be considered to be in keeping with the planning principles for the Cove.	Clause considered to be satisfied based on the following: <ul style="list-style-type: none"> – The Activity Area allows for parking demands for employees, visitors, and maritime related activities. – Proposed on-site car parking is limited to three accessible parking spaces with minor impacts on the surrounding area.

Scheme Provision	Development Response
<p>Any provision for parking will aim to:</p> <ul style="list-style-type: none"> – Encourage pedestrian and cycle access, amenity, and safety; and – Address the essential and short-term parking demands of customers/visitors in the Cove; and – Discourage long term commuter parking within the Cove. 	<p>Clause considered to be satisfied based on the following:</p> <ul style="list-style-type: none"> – Proposed on-site car parking is limited to three accessible parking spaces, therefore limiting the number of vehicle movements accessing the site and their interactions with pedestrians and cyclists. – On-site bicycle parking and development site proximity to the Intercity Cycleway supports bicycle and pedestrian access. – Pick-up/drop-off area for coach access encourages high occupancy vehicles and reduces single-occupancy parking demand. – The development will not attract commuter parking due to no proposed staff parking on-site.
26.3 Objectives for Traffic, Access, and Parking in Activity Areas	
<p>'Regatta Point' Activity Area 4.2</p> <p>This Activity Area is characterised by low volumes of traffic. Parking shall be provided in this Activity Area to cater for the parking demands of employees, visitors, and other maritime related activities. Land within this Activity Area will not be used for the parking demands of activities outside the Activity Area.</p>	<p>Clause considered to be satisfied based on the following:</p> <ul style="list-style-type: none"> – Low volume of vehicular traffic accessing the development driven by the deliberate limitations put on on-site parking provisions. – Parking demands of employees and visitors reduced by the proposal's encouragement of alternative transport modes. – Accessible (DDA compliant) parking is provided to meet the mobility needs of visitors. – No parking is provided that might be utilised for parking demands outside of the activity area.

5.1.1 Development Control

Scheme Provision	Development Response
26.4.1 Traffic Generation	
<p>Where a development (including subdivision) is approved that involves the provision of on-site car parking, servicing or emergency access facilities that will result in a material increase in the volume of vehicular traffic entering or leaving a public road, then provision shall be made to accommodate such additional vehicles and their movement to the satisfaction of the Planning Authority, having regard to traffic safety or amenity as appropriate. Similar provision shall be made for pedestrians.</p> <p>Any development needing or expected to generate a demand for the delivery of people to the site in relatively significant volumes, will be expected to make appropriate provision for accommodating the associated vehicular movement - whether generated by private, public or tourist transport, in a manner consistent with the principles of this schedule.</p> <p>The location and form of any proposed access to an existing public road shall ensure that adequate sight distance in relation to the speed of through traffic is available.</p> <p>The Planning Authority may require certain measures to be carried out at the developer's expense in order to accommodate additional vehicles or pedestrians resulting from an approved development.</p> <p>All access, parking and traffic management works shall be constructed to the Planning Authority's current standards and in accordance with plans approved by the Planning Authority.</p>	<p>Clause considered to be satisfied based on the following:</p> <ul style="list-style-type: none"> – On-site car parking is limited to three accessible parking spaces, therefore limiting vehicular interaction with pedestrians in the surrounds. – Pedestrian access is proposed via the main entry to Drill Forecourt. A footpath is provided at this access to support the volume increase in pedestrians. – Distribution of generated people/vehicle trips amongst the various modes of travel, particularly walking, and cycling, reduces the impact of trips by any given mode of travel. – Accesses to the proposed development site are existing, and no new vehicular accesses are proposed.

Scheme Provision	Development Response												
26.4.2 Access Requirements													
<p>Unless existing buildings, topography, the requirements of the Disabilities Discrimination Act 1995 or heritage considerations dictate otherwise the maximum vehicle access widths and minimum footway widths shall be as set out below:</p> <table><tr><th>Number of Parking Spaces to be Provided on Site</th><th>Maximum Allowable Vehicular Access Width</th><th>Minimum Number & Width of Footways</th></tr><tr><td>1-5 inclusive</td><td>One 3.0m lane</td><td>One 0.6m</td></tr><tr><td>6-20 inclusive</td><td>Two 2.5m lanes</td><td>One 1.0m</td></tr><tr><td>21 and over</td><td>Two 3.0m lanes</td><td>Two 1.5m</td></tr></table>	Number of Parking Spaces to be Provided on Site	Maximum Allowable Vehicular Access Width	Minimum Number & Width of Footways	1-5 inclusive	One 3.0m lane	One 0.6m	6-20 inclusive	Two 2.5m lanes	One 1.0m	21 and over	Two 3.0m lanes	Two 1.5m	<p>Clause considered to be satisfied based on the following:</p> <ul style="list-style-type: none">– A ~1.6-metre-wide footpath is provided at Access 2.– A ~4.3-metre-wide access is provided at Access 1 and a ~5.0-metre-wide access is provided at Access 2. Wider access width required to accommodate larger vehicles including buses/coaches and MRVs.
Number of Parking Spaces to be Provided on Site	Maximum Allowable Vehicular Access Width	Minimum Number & Width of Footways											
1-5 inclusive	One 3.0m lane	One 0.6m											
6-20 inclusive	Two 2.5m lanes	One 1.0m											
21 and over	Two 3.0m lanes	Two 1.5m											
<p>The Planning Authority, in pursuance of the intent of the Planning Scheme, and in particular Clause 26.1 and the principles of this schedule (Clause 26.2), may for any development require:</p> <ul style="list-style-type: none">– The deletion of any vehicular access or otherwise limit or accept a reduction in the number of accesses or require different dimensions of access widths; and– The provision or deletion of any-footway and/or a variation in their dimensions.	<p>Not applicable.</p>												
<p>Where appropriate, access and parking areas shall enable vehicles to enter and leave the site in a forward direction.</p>	<p>Clause considered to be satisfied based on the following:</p> <ul style="list-style-type: none">– Access 1 provides access for delivery/service vehicles. The swept path undertaken at this location in Appendix A indicates that the reverse manoeuvre undertaken to enter this access is workable.– Access 2 allows for vehicles to enter and leave the site in a forward direction. An Austroads 2013 bus will be required to undertake a three-point turn, whilst smaller vehicles can complete a full-circle to enter and exit the site.												
26.4.3 Parking Standards													
<p>The number of parking spaces which can be accommodated in a car parking area and the aisle widths within it, shall comply with the relevant Australian Standard, unless otherwise determined by the Planning Authority having regard to the impact on the site and its environs.</p>	<p>On-site car parking is limited to accessible parking designed in accordance with AS/NZS 2890.6:2009 <i>Parking facilities, Part 6: Off-street parking for people with disabilities</i>. Refer to Section 4.3.1.</p>												
26.4.4 Nature of Parking													
<p>The Planning Authority may require the parking spaces approved as part of any development to be of a particular size, type, proportion, and location to be suited to the likely nature of demand including turnover of use, type of use and user or visitor vehicles to be accommodated, and servicing needs. This will be determined by the Planning Authority after taking into consideration matters such as:</p> <ul style="list-style-type: none">– Location of the site– The nature of the site– The nature of the surrounding area– Existing traffic and parking patterns– The nature of the operation and future growth plans or opportunities– Existing on street restrictions– Future on street or off-street public parking proposals. <p>The Planning Authority may require specific parking spaces to be reserved for exclusive use by visitors to the development or for people with disabilities.</p>	<p>On-site car parking is limited to accessible parking designed in accordance with AS/NZS 2890.6:2009 <i>Parking facilities, Part 6: Off-street parking for people with disabilities</i>. These parking spaces are located near the ramp to the building entrance.</p>												

Scheme Provision	Development Response
<p>Any car parking spaces sought to be provided on site will only be approved if located in a manner appropriate to the character and appearance of development in the vicinity and/or with the Principles of this Schedule and the parking and other Objectives for the relevant Activity Area. The siting of parking spaces between the building line and the street boundary line shall generally not be 'permitted'.</p>	
26.5 Matters to be considered	
<p>The Planning Authority shall ensure that proposed development:</p> <ul style="list-style-type: none"> – Will not interrupt the efficient passage of port related vehicular movements – Will not generate vehicular traffic which is detrimental to other vehicle movements in Sullivans Cove – Will not utilise a key site identified in Part F, or other land considered appropriate, in a manner that would prevent its use for the provision of public parking purposes. The Planning Authority shall ensure the scope for the provision of significant public parking generated by the development - whether on-site or off-site is explored and evaluated, including the use of part Five Agreements – Minimises footpath crossovers and disruption to footpath movements – Providing parking, shall do so in accordance with requirements for the design and location of spaces and access as specified in this schedule – Providing parking, is designed to complement the form and detail of adjacent buildings and have a continuity of street level activity and/or visual interest – Providing parking, the vehicle access shall be discrete and where possible located so as to not disrupt traffic flows along streets through queuing; and – Incorporates through-site and cross-block pedestrian links and access to the waterfront unless it can be demonstrated that there is no benefit to the enhancement of pedestrian movement and/or amenity in the Cove. 	<p>Clause considered to be satisfied based on the following:</p> <ul style="list-style-type: none"> – All proposed parking will be provided on-site, therefore limiting disruptions to the external street network. – All additional car parking movements will be accommodated within the existing car parks which facilitate existing parking movements. – The proposed number of on-site parking spaces limits the number of vehicular movements accessing the site. Impact on the surrounding environment is therefore also limited. – Private vehicles (parking) represent only a portion of the travel demand shared across a variety of modes including walking, cycling, bus, and coach. Considering the close proximity of the Intercity Cycleway connection to Hobart CBD, the Cenotaph and Regatta Grounds car parks will be used by only a portion of the development site's visitors and staff. – Pedestrian access to the development site utilises an existing pedestrian bridge across the decommissioned rail line.

6. Summary

This Traffic Impact Assessment report has investigated the potential traffic and transport related impacts associated with the new Sullivans Cove Distillery at 'Huon Quays' 13 and 17 McVilly Drive.

The key findings are as follows:

- The proposed development is anticipated to experience a two-way increase of approximately 60 staff and visitor trips per hour during its midday peak period. This is outside of the AM and PM road peaks – approximately 30 staff and visitor trips per hour will be generated during these peaks. The majority of these generated trips will comprise of alternative transport modes to private vehicles.
- Staff and visitor trips will largely originate from areas south and north of the development site. The proximity of the Hobart CBD south of the site and good connectivity makes for easy access via walking and cycling.
- Low volume service vehicle access is proposed to the development site via Domain slip yard. The swept path assessment indicates that the largest service vehicle that can be suitably accommodated at this access is an Australian Standard AS 2890.2 (2018) MRV. Appropriate signage is recommended at the access to advise of the maximum vehicle size permitted.
- Three accessible parking spaces and 12 bicycle parking spaces are provided within the Drill Forecourt for visitors. All other visitor parking demand will be accommodated within the adjacent Cenotaph and Regatta Grounds car parks. It is recommended that limitations to on-site parking be communicated as part of parking management measures to pre-inform visitors. This may also discourage travel by private car.

Note: Bollards are to be provided within the shared areas of the accessible parking area as per Australian Standard requirements.

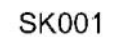
- A staff-only bike store is provided on-site to accommodate travel to work by bike. In addition to this, it is recommended that other incentives be considered to encourage staff travel by active and public transport.
- Bus/coach access is permitted within the Drill Forecourt for pick-up/drop-off. The swept path assessment indicates that a 12.5 metre Austroads 2013 Bus can undertake a three-point turn manoeuvre in and out of the Forecourt.
- Access to the Drill Forecourt (Access 2) from the external road network is via an existing access road crossing the decommissioned railway line, currently used by vehicles to and from the Regatta Grounds car park. The assessed maximum vehicle size to access the development site via this access road is an Austroads 2013 Bus (12.5 metres), which is of a comparable size to some boat towing vehicles. A minimum road width of 3.1 metres is required for an on-straight movement of this vehicle type. The swept path assessment at the curve in the road (across the railway crossing) indicated that singular bus access can be accommodated at this location. The Queens Domain Master Plan indicates that this access road will be upgraded as part of future works.
- Crash trends in the vicinity of the development site are not expected to be exacerbated by the proposed development.
- The proposed development is considered to comply with Planning Scheme requirements based on the following points:
 - The number of trips generated consist of both people and vehicle trips distributed across various possible modes of travel (including bicycle, walking, private vehicle – drop-off/pick-up, public transport – bus, tourist transport – coach). The proximity of the Intercity Cycleway and on-site bicycle parking provisions serves to encourage the use of active transport modes.
 - The development retains the use of three existing accesses to the site. The width of these accesses is required to accommodate service vehicles and bus/coach movements.
 - Car parking within the surrounding area is considered detrimental to the form and character of Sullivans Cove as per Section 26.1 of the Planning Scheme. The development proposes only three accessible parking spaces.
 - Pedestrian access to the development site is supported by a new ~1.3-metre-wide footpath connecting the existing pedestrian bridge across the decommissioned rail line. Additional pedestrian amenities are proposed as part of the future upgrade of the adjacent Cenotaph car park.

- On-site parking will only introduce minor volumes of vehicle movements to the site itself. The majority of the car parking movements generated will be accommodated within existing car parks nearby.
- The development does not compromise existing walking and cycling connections, and linkages to adjacent public transport nodes.

Based on the findings of this report, and subject to the recommendations outlined above, the proposed development can be supported on traffic grounds.

Appendix A

Swept paths





SWEPT PATH LEGEND

- VEHICLE BODY
- 500mm CLEARANCE
- TIRE TREADS

SWEPT PATH UNDERTAKEN AT 5km/hr
UNLESS OTHERWISE STATED

PRELIMINARY

C	MODIFIED SWEEP LAYOUT	09.06.21
B	MODIFIED SWEEP PATHS	31.05.21
rev	description	app'd date

John Wardle Architects
Sullivan's Cove Distillery
Tanker Service Bay
Swept Path Assessment

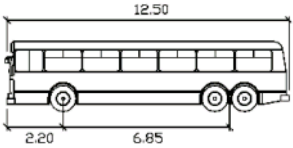
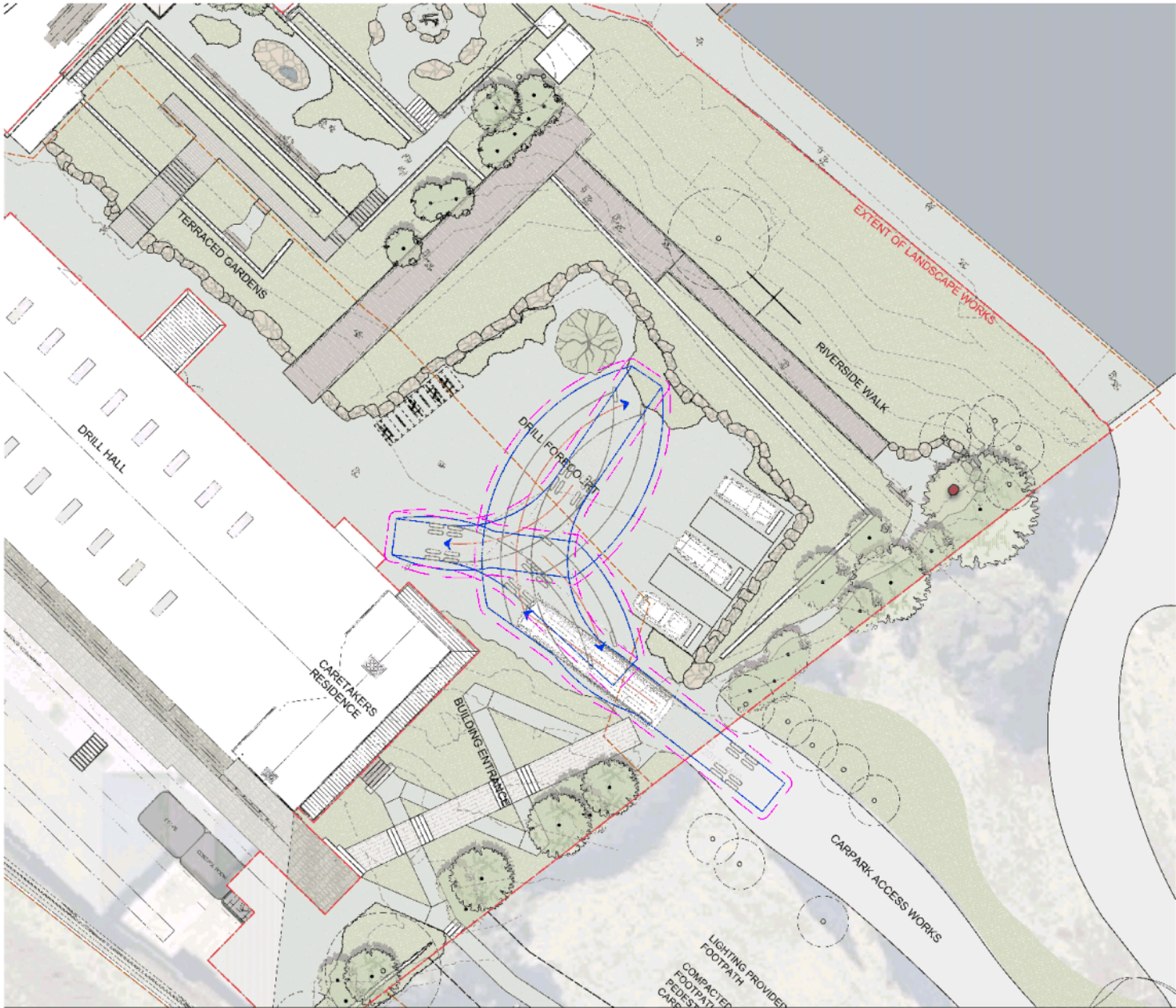


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scale 1:250 for A3 job no. 12533276
date September 2021 rev no. 1 C

approved (PD) SK002



BUS

meters

Width : 2.50

Track : 2.50

Lock to Lock Time: 6.0

Steering Angle : 36.6

SWEPT PATH LEGEND

- VEHICLE BODY
- 500mm CLEARANCE
- TIRE TREADS

SWEPT PATH UNDERTAKEN AT 5km/hr
UNLESS OTHERWISE STATED

PRELIMINARY

A	INITIAL ISSUE		29/09/21
rev	description	app'd	date

John Wardle Architects
Sullivan's Cove Distillery
Drill Forecourt
Swept Path Assessment

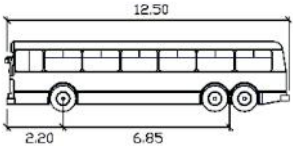
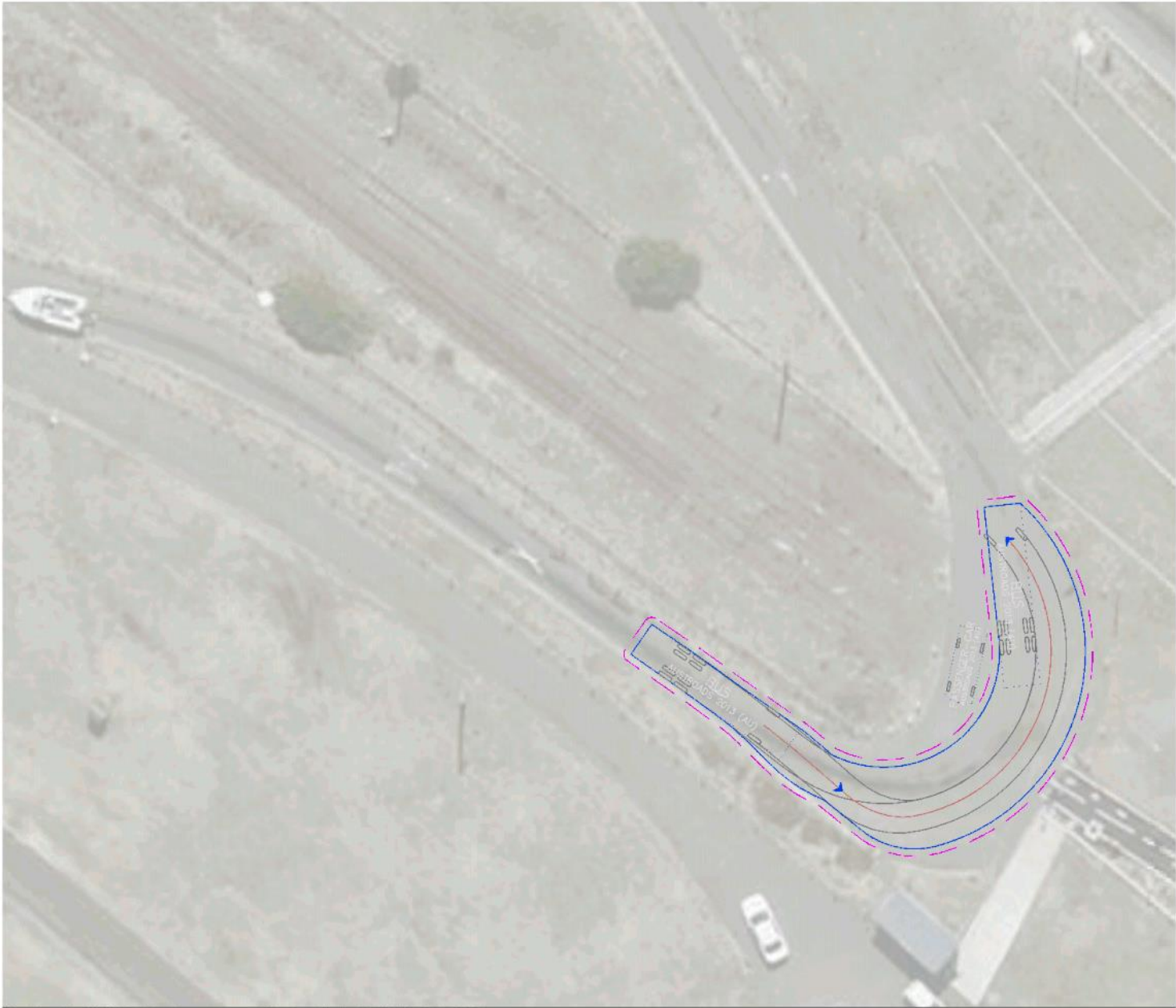


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BUS

meters

Width : 2.50

Track : 2.50

Lock to Lock Time: 6.0

Steering Angle : 36.6

SWEPT PATH LEGEND

- VEHICLE BODY
- 300mm CLEARANCE
- TIRE TREADS

SWEPT PATH UNDERTAKEN AT 5km/hr
UNLESS OTHERWISE STATED

PRELIMINARY

rev	description	app'd	date
A	INITIAL ISSUE		28/09/21

John Wardle Architects
Sullivan's Cove Distillery
Existing Access Road
Swept Path Assessment



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→ The Power of Commitment

Sullivans Cove Distillery Houn Quays

LANDSCAPE DEVELOPMENT APPLICATION

REALM studios

 NITA EDUCATION

 John Wardle
Architects



content



1. Introduction

Design Statement
City & Landscape
nipaluna country
Colonial history
Site conditions

3
5
6
7
8

2. Design Scheme

Conceptual evolution
Conceptual framework
Overview Axonometric
Layout Plan
Cameo Views
Materials + Finishes
Planting
Sections

9
10
13
14
15
19
20
22

Appendix

Site Plan (Phase 1 works only)
General Arrangement Plan 01
General Arrangement Plan 02

Acknowledgement of Country

As we develop conceptual thinking on lutruwita Aboriginal land, sea and waterways, we acknowledge, with deep respect the traditional owners of this land, the muwinina people. The muwinina people belong to the oldest continuing culture in the world. They cared for and protected Country for thousands of years. They knew this land, they lived on the land and they died on these lands. We honour them.

For the muwinina people, the area around nipaluna (Hobart) was their Country and they called Mount Wellington kunanyi. We acknowledge that it is a privilege to stand on Country and walk in the footsteps of those before us.

Along the river banks, among the gums and seas that continue to run through the veins of the Tasmanian Aboriginal community. We pay our respects to elders past and present and to the many Aboriginal people that did not make elder status and to the Tasmanian Aboriginal community that continue to care for Country. We recognise a history of truth which acknowledges the impacts of invasion and colonisation upon Aboriginal people resulting in the genocide and forcible removal from their lands.

Our Island is deeply unique, with spectacular landscapes with our cities and towns surrounded by bushland, wilderness, mountain ranges and beaches. We stand for a future that profoundly respects and acknowledges Aboriginal perspectives, culture, language and history. And a continued effort to fight for Aboriginal justice and rights paving the way for a strong future.

Project: Sullivans Cove Distillery - Houn Quays
Report Title: Development Application Report
Authors: Alaric Hellawell (REALMstudios)
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Review: Alaric Hellawell (REALMstudios)
Document Issue: 28/09/2021 - Issue C
Prepared by: REALMstudios



design statement

A new home for the makers of the world's finest whisky

The proposal seeks to provide a new home for Sullivans Cove Distillery - makers of the world's best single malt whisky.

Sullivans Cove whisky is made by a passionate family of people who care about their craft, using minimal intervention techniques and with locally sourced raw ingredients. It is a uniquely Tasmanian product that is receiving attention from a wide audience in Tasmania, interstate, and across the globe. Inspired by the Sullivans way of making whisky, the architecture has been approached in a similar manner - with care, authenticity, and careful intervention into the precious Derwent River - timtumili minanya precinct.

The new Sullivans Cove Distillery will see the careful restoration of the Drill Hall, Residence structure and introduction of the new Shadow Building (The Productive Distillery). Similarly, the landscape works will restore the cultural and ecological landscape of which the buildings sit within.

A landscape of repair and relics, filled with native Australian flora and meandering paths, as well as gathering and social spaces for visitors to enjoy. The site will also be used to complement the already celebrated events Hobart has growing recognition for. The restoration of these historic dwellings and its landscape will enliven Huon Quays, creating a new cultural destination in Hobart for local and international visitors alike to enjoy.

Stringent design principles and methodologies have been adopted, along with the site specific opportunities and constraints, to ensure that the sites development is conducted with due consideration of the sites significant sensitive cultural heritage, and to demonstrate best practice in environmental, cultural and social sustainability. The site has been adaptively reactivated to provide settings for an array of new activities. The place-based principles have been an important driver of the landscape design and is summarised as follows;

Country - Reveal the Aboriginal History of the Site, develop a landscape that reveals and repairs Country and provide spaces that enable the cultural practices and learnings of Country.

Naval Heritage - Interpret the sites Naval use which has left behind an impressive legacy of architecture and structure.

Industrial Change - Embrace the industrial character of the neighbouring context and working docks.

Geological Foundations - Express the riverine formations of sandstone outcrops and restore soils back to their rich fertile condition.

Hydrological Patterns - Reintegrate water as a crucial part of the sites history and future resilience.

Ecological Diversity - Re-establish pre-colonial endemic tree, shrubs and grassland plantings.

Connectivity and Experience - Provide multi-scaled gathering and social spaces.





Reanimating the landscape

The landscape proposal sets out a number of alterations to the Huon Quays site. On site observations and readings of Country show a landscape that has been altered significantly to that that would have existed pre-invasion and during the Naval operations.

The broad approach has an emphasis on ideas of 'revelation' more than those of 'creation', where minimal intervention and meaningful spatial relationships are established to evoke interpretations of site and place. These are framed around; reveal what has been lost; retain existing character; and introduce minimal interventions.

Reveal

Key aspects of the site have been masked by colonial settlement which will be revealed on site, including;

- Exotic trees and plantings will be removed to reinstate endemic species that reveal the Country that once thrived along the banks of timtumili minanya.
- Sandstone will once again become the dominant hard material found across the site in various forms.
- Water will become visible on the surface once again through ephemeral water flows against piped infrastructure.
- Gathering spaces and fire will become present on the banks of timtumili minanya once again.

Retain

Key aspects are retained on site to maintain the utilitarian form and uses of the site, including;

- Retaining walls and steps - Historic significance noted. Walls and steps to be retained in its existing location and rustic finish maintained.
- Building 10 - Small masonry structure which was last used for storage of ammunition for patrol boat stationed at HMAS Huon. Retained and incorporated into the landscape scheme for storage.
- Flag Mast - Historic significance noted. Flag mast to be retained in its existing location.
- Jetty - Out of scope. Provision for equitable access has been allowed for in the landscape proposal as part of a staged approach in case required in future.

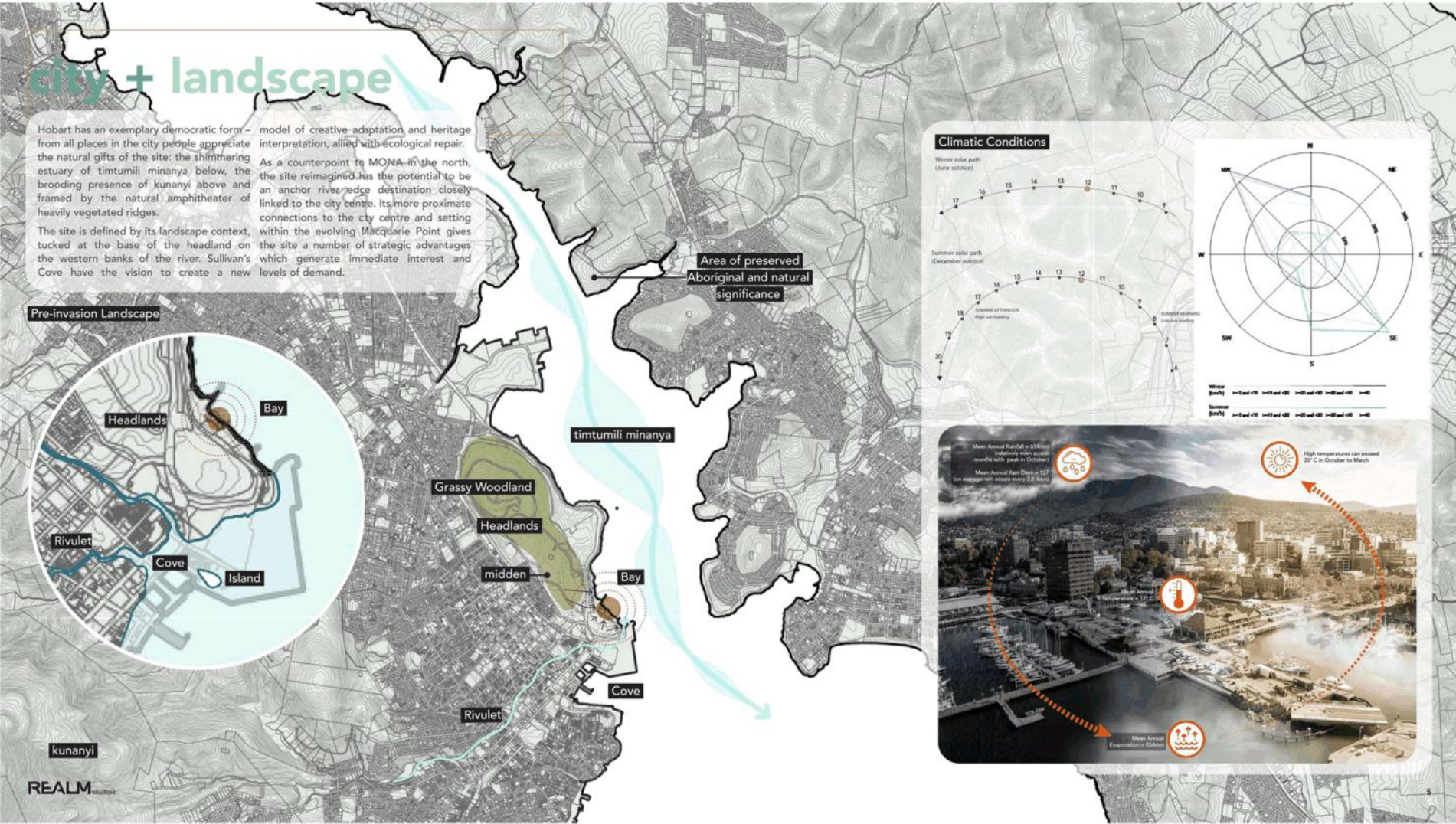
Minimal Intervention

Key contemporary interventions will be required to make the landscape compliant with current standards, fit for purpose and a place for visitors to enjoy.

Each intervention will be complementary, appropriate in scale, and respectful of the site's histories. These additions will be designed and detailed to read as clearly 'modern' and bring coherency with the contemporary aspects of the architectural works, including;

- Shadow Building Entry - a stepped sandstone path leads up to the new Shadow Building and existing residence building entries. The path is interwoven with an informal meandering gravel path.
- Terraced Garden steps and gantry - the existing terraces are unlocked by positioning a series of corten steel steps and gantry over the existing retaining walls. These provide a new way to orientate the varying levels and experience the newly established native gardens.
- Kitchen garden - to support the sites new daily occupants a productive food garden is created along the north edge of the Drill Hall taking advantage of the level change through terraced vegetable plots.





nipaluna country

Connection To Country

For Aboriginal people, cultural knowledge provides ways of understanding, protecting and enhancing the specific places that they/we know as Country.

While Country involves everything in the landscape, it is also about relationships – connections to Country.

These relationships, involving time, natural ecologies, cultural activities and all people, provide the frameworks by which people care for Country. They also establish the means by which Country in turn provides sustenance, support and meaning.

These relationships are a way of bringing together many different elements and operations, and strengthening the connections between people and the environment.

Many of the environmental actions and human operations that have informed Country for millennia remain in place - their re-awakening and reintegration contributes to the understanding and management of complex and sensitive places.

Aboriginal peoples' knowledge of Country, along with the knowledge held within Country, bring together all of these complex ecologies of place.

This comprehensive and holistic understanding, and a corresponding sharing of knowledge between peoples, becomes the beginning of our response to the site.



Robert Dowling, Group of Natives of Tasmania, 1859.



John Glover landscape artist (UK) 1767-1849 - Mt. Wellington and Hobart Town from Kangaroo Point. This painting represents the scenery of Hobart Town nestled by Mt. Wellington. In the foreground at the edge of Kangaroo Point there is a group of Aborigines dancing a Corroboree around the fire and some are swimming in the Derwent River.



The spread of artefacts indicates that while Tasmanian Aboriginal people occupied every type of habitat, they targeted areas at the edge of water such as site on the tintumilli minanya.

Designing With Country

There can be no sustainable development without the reintegration of natural and human systems. And there is no better model for human civilisation in balance with living systems than that of Australia's First Peoples.

Designing with Country, bringing Indigenous knowledge and thinkers into the process and consulting deeply with local Indigenous Communities is at the heart of the approach to the future of the site. Working with nita education and through conversations with Nunami Sculthorpe Green (City of Hobart) we aim to build and deepen an understanding of the site.

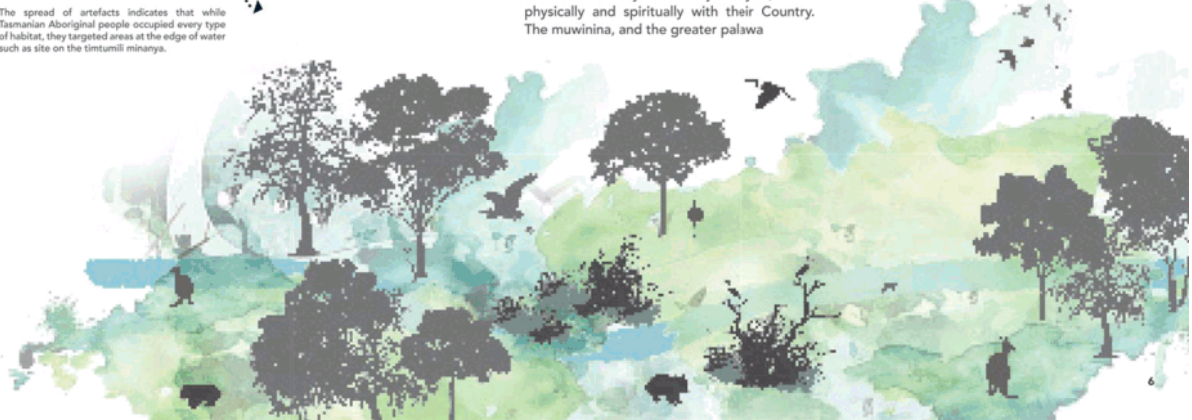
For millennia, this was the home of the muwinina people, who coexisted with their coastal lands, travelling along the waterways and ridgelines, benefitting from the area's fertile productivity, and harvesting food, clothing and resources from the estuarine ecosystems and adjoining woodlands. Campsites and shell middens acknowledge cultural inhabitation, production and meaning embedded within the landscape.

Indigenous communities were shaped by the land in which they lived and journeyed, linked physically and spiritually with their Country. The muwinina, and the greater palawa

nation, lived communal lives, resourceful and sustainable, distributing knowledge, materials and meaning through their interactions with the world and its ecologies. In all that they did, the muwinina balanced resource use, environmental management and cultural stewardship, instinctively and uncomplicatedly.

Their integration and interaction with Country also established land uses and operations connected with the broader environment, creating legacy uses extending across time with increased relevance today, especially in areas of climate, water, fire and ecosystem stewardship. This knowledge and these actions were shared regularly between neighbouring clans as the world around them changed across thousands of years.

This interdependent resourcefulness and resilience continues, and forms the core of much of today's Aboriginal cultural identity, not only in lutruwita, but across Australia. This legacy of lore passed down through time and shared today is a touchstone as we move forward in integrating Sullivans Cove with nipaluna.



colonial history

DOMAIN

The site is considered an extension of the Domain Headlands. During the early colonial period, what is the Domain was partly used for farming and as a source of timber. In 1811 areas granted were reacquired and the area soon after was referred to as the "Government Domain". Governors would lease parts of the land for timber, grazing and quarries to add to their income.

Public pressure to have the area confirmed as public space began in the 1830s. The Queen's Domain Vesting Act (1917) describes it as "a public reserve ... solely for the purposes of recreation, health, and enjoyment of the inhabitants of [Hobart] and others."

During the late nineteenth and early twentieth centuries, the Domain was planted with exotic and native trees, quarries were rehabilitated and a vast parkland was created with more manicured gardens with the botanical gardens and later the Beaumaris Zoo. Much of the Domain's natural environment is white-gum grasslands and bushland now dotted with a variety of introduced trees both exotic and native.

Cenotaph and Regatta Grounds

The area around the site was once the site of the Queen's Battery, dating from the 1840s. The area was also used for military exercises (including a rifle range) and drill throughout the colonial period.

The Hobart Regatta is the nation's second oldest and commenced in 1838 and has been every year since. The grandstand was built in 1877 and replaced by the John Colvin Stand in 1919-20. Since World War II, sideshows have added a carnival aspect to proceedings which include rowing, yachting, power boats and in some years swimming across the Derwent.

HMAS Huon

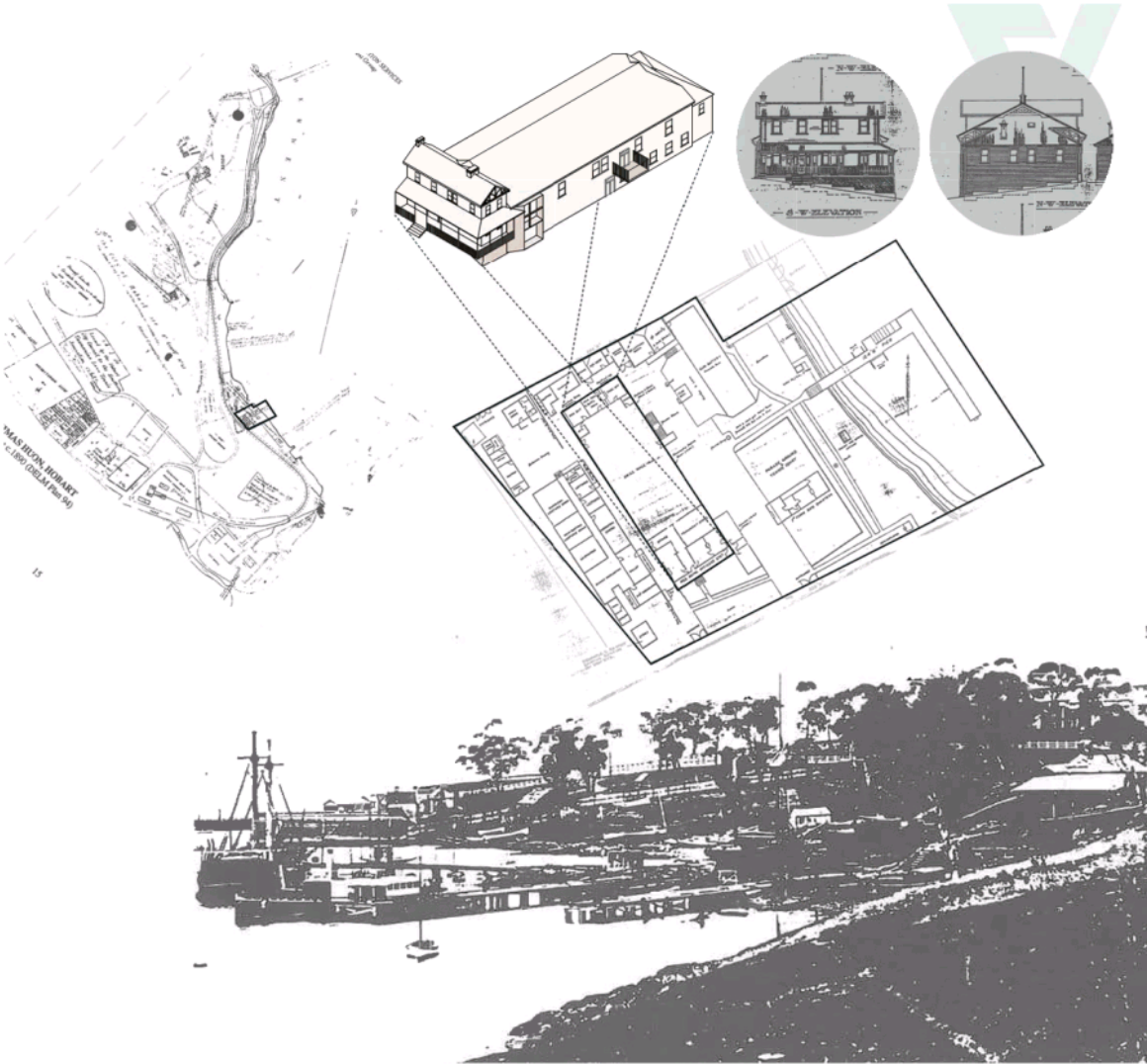
Originally this was the homeport of colonial Tasmania's navy. The drill hall was constructed for naval cadets after federation. It served as an important training centre and home dock for a number of small naval vessels until the base was de-commissioned in 1994. The oldest remaining building dates from 1912 and will become a prominent building of the precinct once again.



Early settlement, Hobart town from Domain 1800's



Sea Scouts at HMAS Huon in 1959



site conditions



1. Broken views to the water and poor quality surface pathway



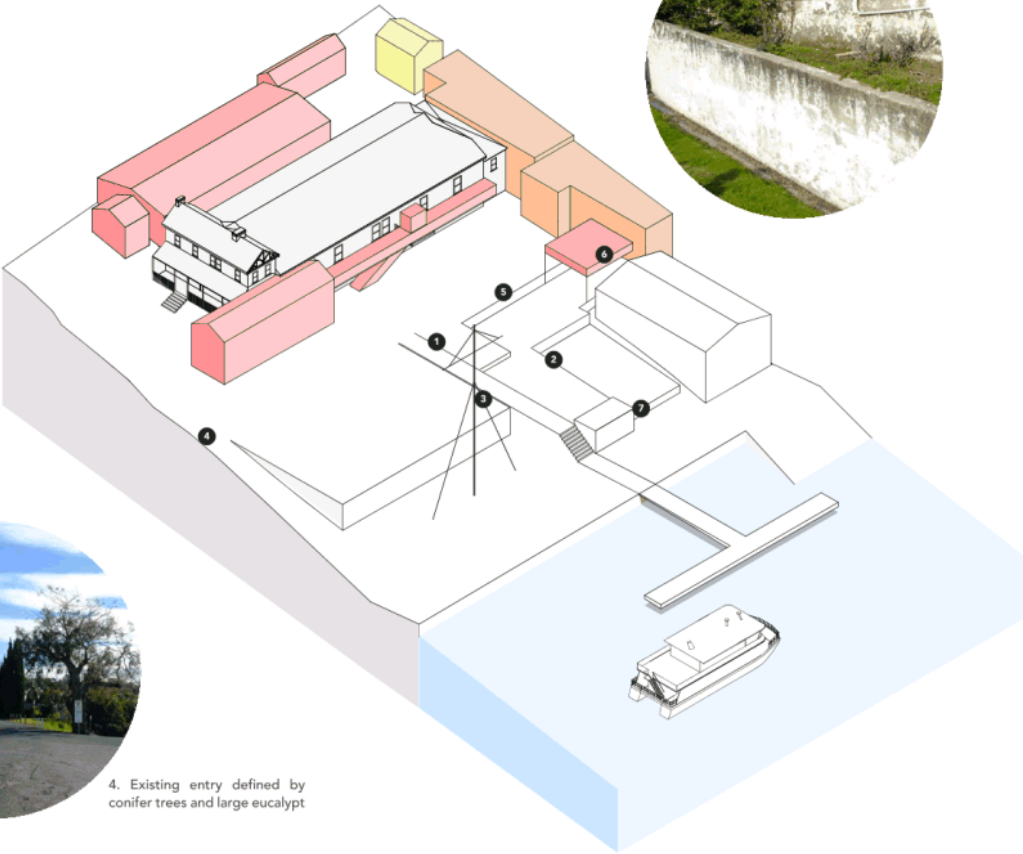
2. Existing concrete step access between terraces



3. Existing flag pole hidden amongst overgrown shrubs



4. Existing entry defined by conifer trees and large eucalypt



5. Existing retaining walls define structured level changes and a series of garden spaces



6. Existing view from raised deck across the river and working dock



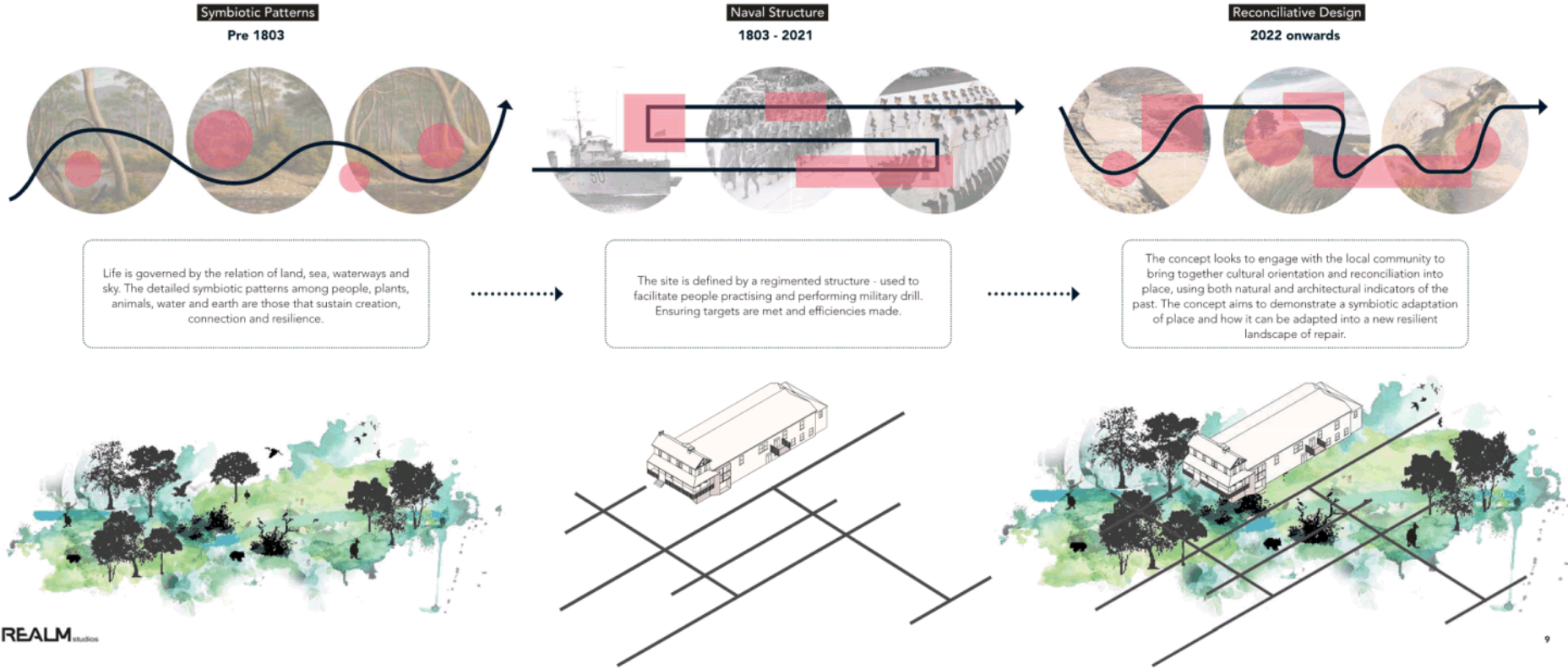
7. Overgrown exotic shrubs and trees define a colonial green character

conceptual evolution

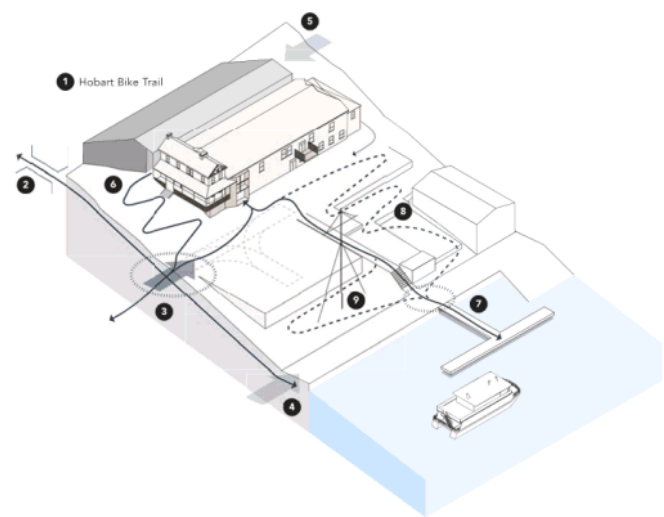
The site has experienced a significant evolution in recent time in both form, function and cultural association.

It is important to revive the significance of the landscape as a living indicator of place, revealing the history and living culture of Country. An integrative tapestry of spaces and connections will reflect the history and vision of the precinct.

A strong colonial structure remains - a hidden Country lays dormant.



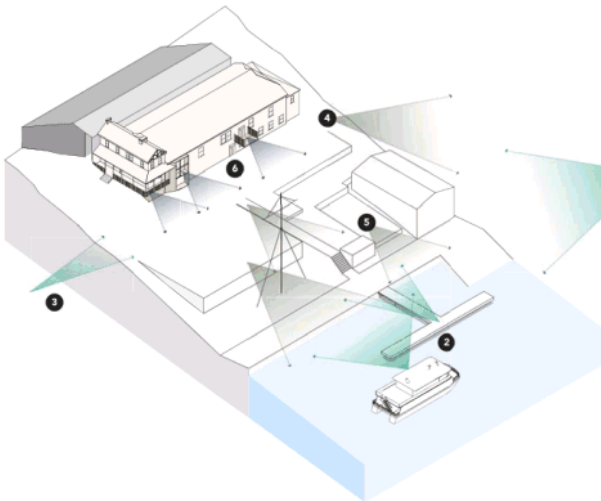
conceptual framework



ACCESS + MOVEMENT

- 1. Possible future link into Hobart Bike Trail
- 2. Pedestrian link and bridge retained as link to Public Car Park
- 3. Main gateway entry for pedestrians, coaches and disabled parking
- 4. Access maintained to 13 McVilly Drive and boat ramp
- 5. Proposed site service access
- 6. New DDA access to Shadow Building with potential additional pedestrian access link
- 7. Retained pedestrian spine and jetty link
- 8. Informal routes through terraced gardens
- 9. New DDA access from jetty and rivers edge as part of phase 2 works and separate DA process.

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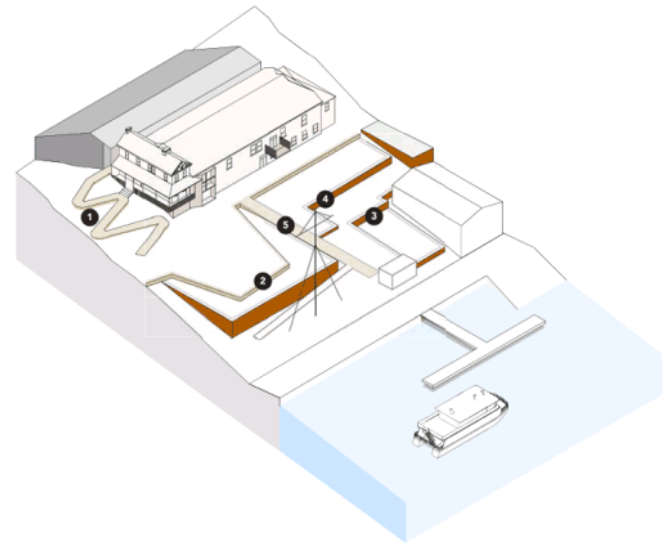


VIEWS

- 1. Views from the Tasman Bridge are strengthened by the sites overgrown character within an industrial edge condition - the design will maintain the green aesthetic whilst framing views to the architecture appropriately
- 2. Views from the jetty and water will be created to strengthen the buildings relationship with the water
- 3. Viewline into the site to define the arrival experience
- 4. Informed views will be created at strategic vantage points overlooking the working shipyards, river and horizon landscape
- 5. Gathering spaces will be orientated for views out across the water
- 6. Clear views from building vantage points will be maintained.



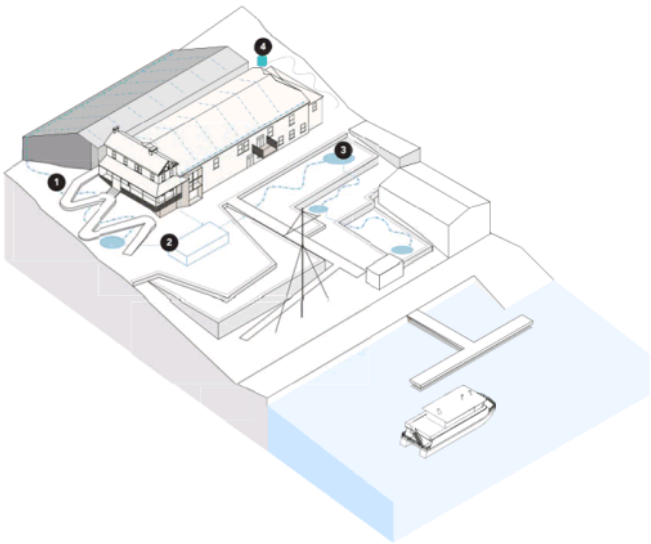
conceptual framework



LANDFORM + LEVELS

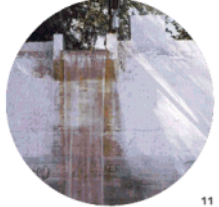
- 1. Ramped and stepped access provided for visitor experience and new Shadow Building entry
- 2. Sandstone retaining wall introduced to provide a level access and a forecourt to the Drill Hall
- 3. Existing retaining walls are retained for character and as a structural frame for the gardens
- 4. Steps are inserted to provide new access points across the terrace gardens
- 5. Central spine is redefined as an extension of the jetty board walk.

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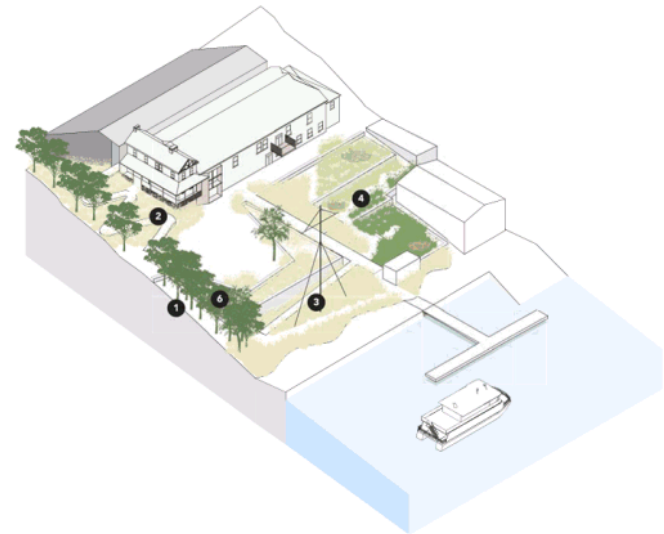


WATER

- 1. Divert roof water on surface to underground tank
- 2. Water is slowly released and filtered through linear ephemeral water line into underground tank
- 3. Water is piped to 'bubble-ups' which irrigates the terraced gardens and collects in soak gardens which provides opportunity to further cleanse water and charge soil.
- 4. A second tank provides a water source to support a staff kitchen garden.



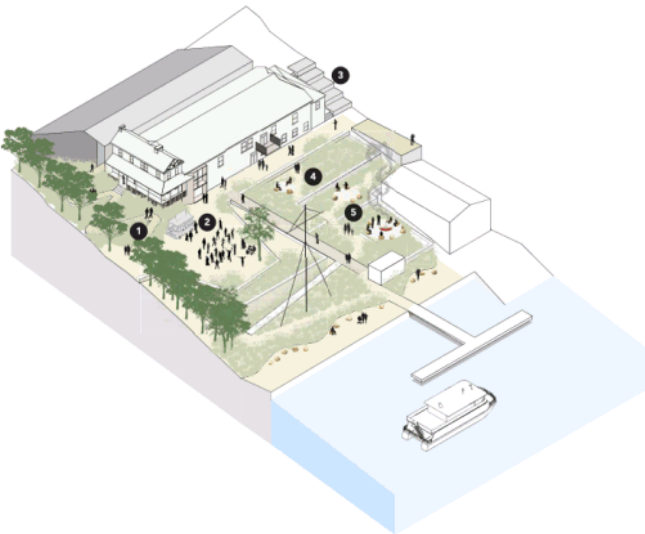
conceptual framework



ECOLOGY

- 1. Native tree stands including sheoaks and Eucalyptus globulus define the southerly boundary and protect the site against cold south easterly winds
- 2. Low textural mass grass planting define ramped entry points
- 3. Terraces are filled with endemic plantings that would originally be associated

- with the river environs and headlands including Themeda triandra
- 4. Native edibles and medicinals are introduced throughout the terrace gardens as a sensory exploration of Country



PROGRAM

- 1. Redefined entry and beginning of the distillery experience
- 2. Flexible courtyard and events space defined by new retaining edge
- 3. Staff kitchen garden positioned for sunlight access
- 4. Seating for a variety of groups sizes

- 5. Gathering spaces that bring fires back to the river banks and winter warmth





**Riverside Walk**

1. DDA access ramp (1:14)
2. Stepped access (inc. as part of phase 2)
3. Land jetty (1:14 ramp)
4. Ammunition bunker woodstore
5. Perimeter native buffer planting
6. Existing flagpole refurbished
7. Native grassland planting
8. View point and sculptural moment

Drill Forecourt

9. Site entry
10. Sandstone rock retaining wall
11. Existing concrete retaining wall retained
12. Multi-use forecourt
13. Feature meeting tree
14. 3 no. disabled parking spaces and 12 no. bike parking

Shadow Building Entry

15. Sandstone entry path
16. Informal gravel path
17. Native grassland planting
18. Perimeter buffer planting

Terraced Gardens

19. Firepit seating area
20. Oversized feature rock and pool
21. Bench seat (Hardwood)
22. Existing concrete retaining walls retained
23. Corten steel steps and gantry walkway
24. Native gardens and edibles
25. Pickers table (refer JWA drawings)
26. Viewing platform (refer JWA drawings)
27. Kitchen garden
28. Existing roof canopy structure (refer JWA drawings)
29. Bin store, garden shed and 6no. staff bike parking (refer JWA drawings)
30. Sandstone access path
31. Staff outdoor work/lunch space



riverside entry



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1. Existing flagpole refurbished
2. Hardwood timber ramp structure (1:14)
3. Native grassland planting
4. Compacted gravel
5. Sandstone feature boulders
6. Existing concrete retaining walls retained
7. View point and sculptural moment
8. Native tree planting and screening shrubs

drill forecourt



Stairs included in phase 2 as part of seperated DA process.

Phase 1 provides a river view lookout which is adapted in phase 2 to provided stepped access from the existing jetty.

- 1. Site entry
- 2. Sandstone rock retaining wall
- 3. Existing concrete retaining wall retained
- 4. Multi-use compacted gravel forecourt
- 5. Banksia meeting tree
- 6. 3 no. disabled parking spaces defined through parking blocks and parkings studs
- 7. 12 no. bike parking (corten steel finish)
- 8. Drill Hall entry
- 9. Native grassland planting
- 10. Hardwood timber ramp structure (1:14)



shadow building entry



1. Sandstone paved entry and steps into the Shadow Building
2. Informal compacted gravel path
3. Native grassland planting
4. Native tree planting and screening shrubs
5. Residence Building access
6. Shadow Building entry courtyard

terraced gardens



finishes palette

Materials

The material palette for the landscape is intentionally simple and are selected for their robustness, textural quality and historic reference to place.

- All previously painted walls will be retained as is providing rustic charm and character.
- All new timber structures will be unpainted, locally sourced, hardwood. This will be left to weather over time to a silver grey.
- Surface materials will be made of large scale sandstone slabs or crushed aggregates that seamless blend into planted areas and define paths, enhancing pedestrian comfort both informally and formally.
- Where required contrasting sandstone walling is selected for its robustness and natural quality.
- Australian hardwood timber and corten steel create ramps, steps and walkways that orientate the various challenging site levels.

Materials celebrate local selection and work in with the design language established in the Drill Hall interventions and new building.

All materials have been chosen for their natural qualities, muted appearance, connection to Hobart's major waterfront heritage industries and the pre-invasion landscape.

Surface



Informal paths - compacted gravel, southern gold dust



Crazy pave - sandstone



Planting Mulch - 40mm southern gold

Feature



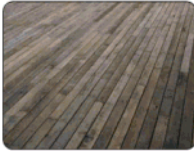
Landscape rocks and walls - sandstone



Retaining walls - existing retained as is



Stair plate and fixings - corten steel



Deck - Australian hardwood



Vegetable beds - Australian hardwood



Balustrade uprights - Australian hardwood

Furniture

Overall precinct furniture strategy includes custom site-specific pieces.

Objects are typically of a larger 'industrial' scale, as a response to the proportions of the precinct, multi-use needs and overall character of each space.

Relocated stones are proposed to be utilised for informal seating opportunities and marker points.

Australian hardwood timber is used to create industrial scale seating.

Seating



Feature seating - sandstone



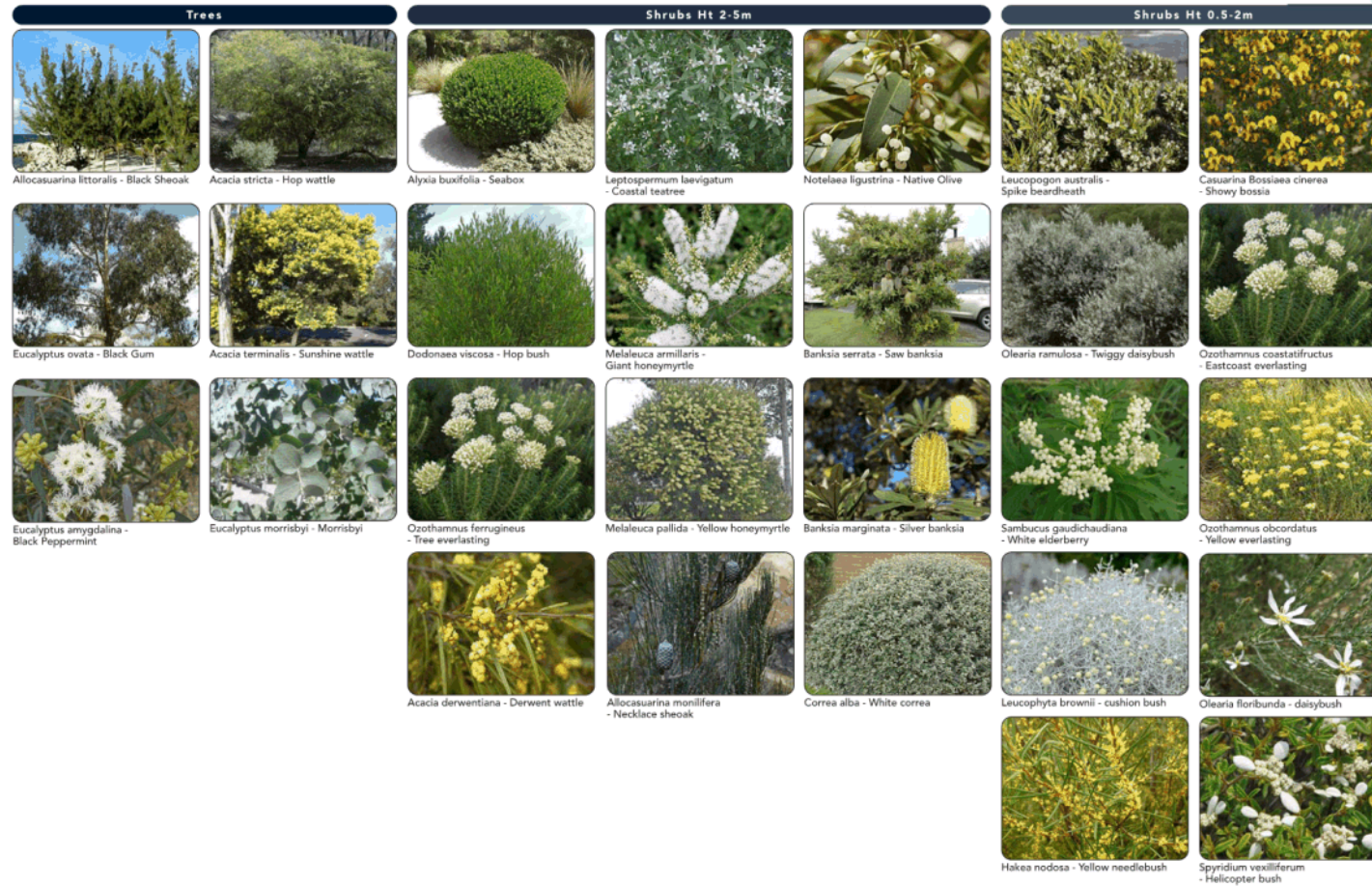
Long seat - Australian hardwood
Corten steel base plates surface fixed

planting palette

The planting palette has been considered against a backdrop of Country, site histories, sustainability, education and community and resolves around a regenerative landscape strategy. It was developed in consideration of river ecologies and Domain parklands native ecologies.

Strategies engaged to achieve principles:

- Establish forested edge conditions that define the site boundary removing the need for fenced edges.
- Develop endemic grasslands and ground covers to create refuge for native fauna and soil micro fauna as an act of landscape repair. Utilise native wetland species to establish swamp ecologies and cleanse water before entering the River.
- Introduce endemic edibles and cultural plantings.
- Seasonal colour provided through multiple flowering shrubs and groundcovers.
- Provide productive and recreational landscapes for people to occupy and enjoy.

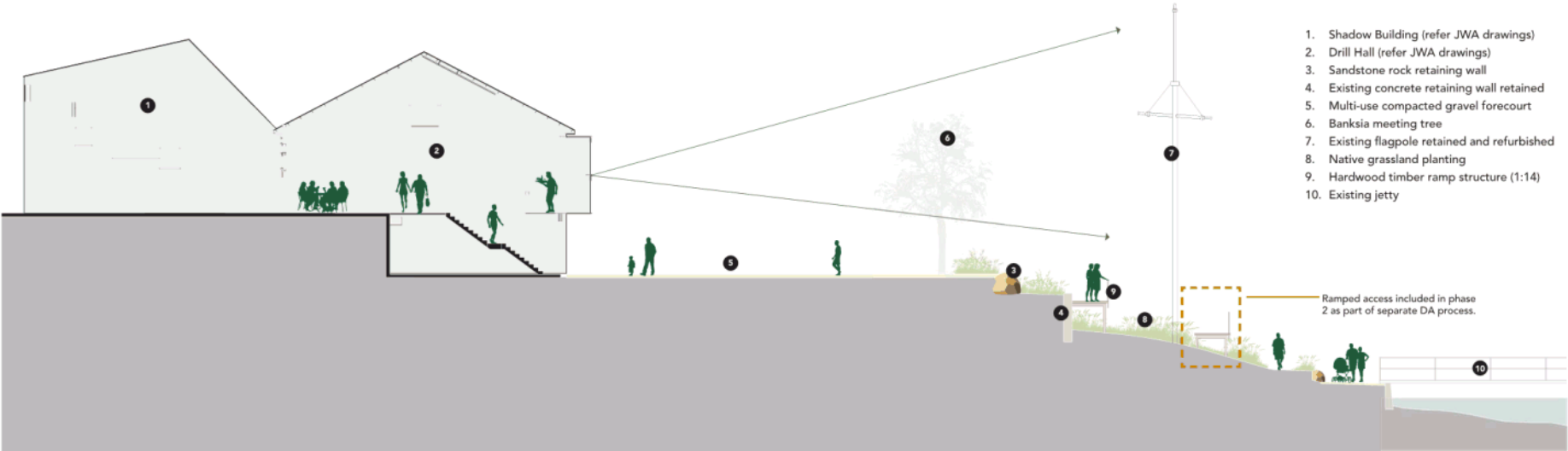
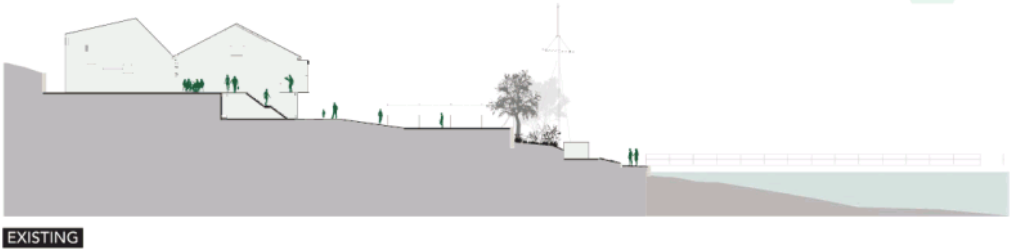




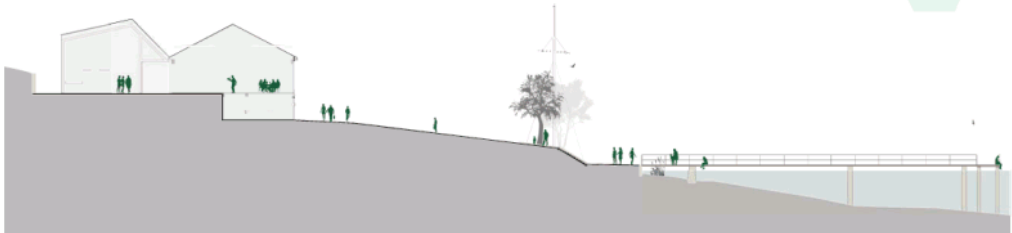
planting palette



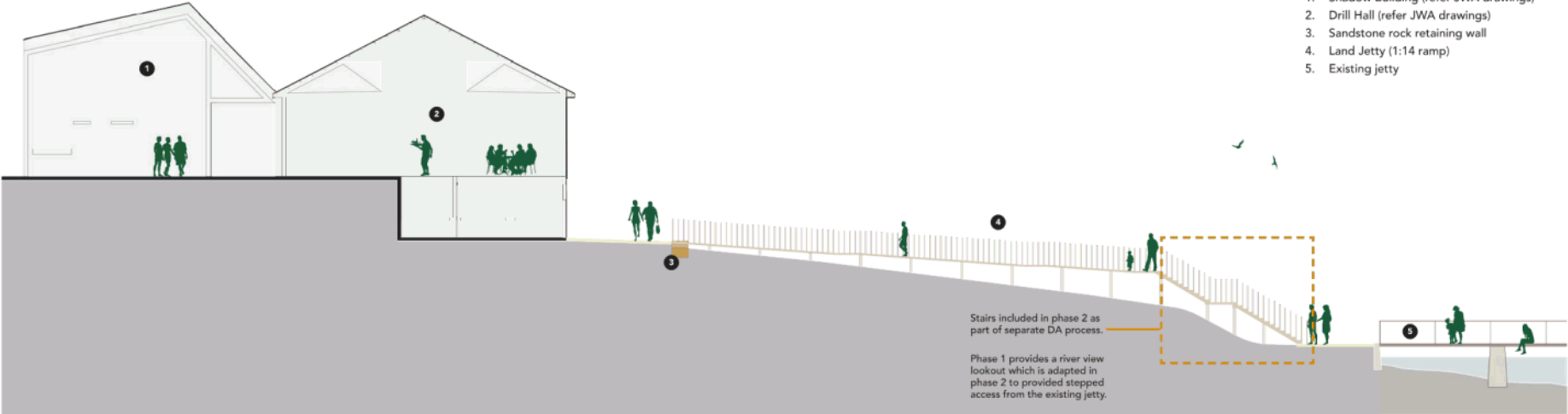
sectional composition



sectional composition

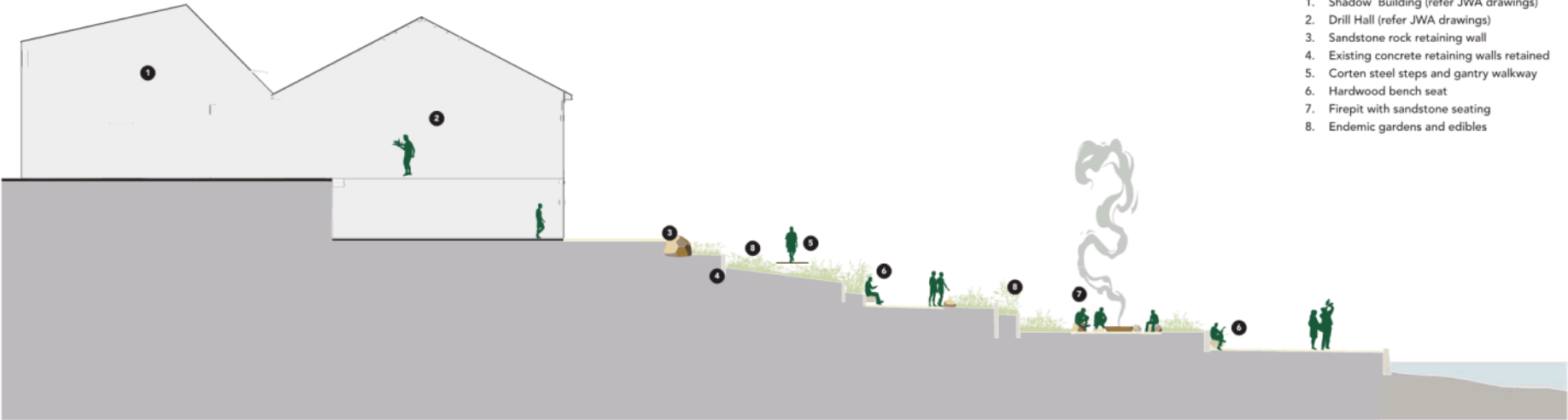
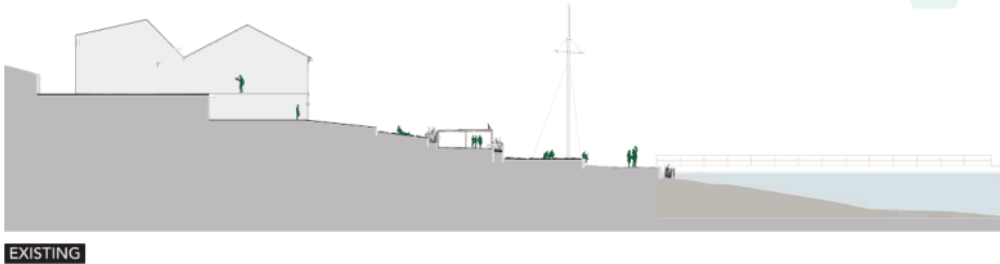


EXISTING



PROPOSED - SECTION A

sectional composition



- 1. Shadow Building (refer JWA drawings)
- 2. Drill Hall (refer JWA drawings)
- 3. Sandstone rock retaining wall
- 4. Existing concrete retaining walls retained
- 5. Corten steel steps and gantry walkway
- 6. Hardwood bench seat
- 7. Firepit with sandstone seating
- 8. Endemic gardens and edibles

PROPOSED - SECTION C



city making + feasibility

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Heritage Impact Assessment Huon Quays Redevelopment

Sullivans Cove Distillery

For Sullivans Cove Distillery

October 2021

Contents:

1. INTRODUCTION, RATIONALE AND BRIEF	1
2. STATUTORY HERITAGE REQUIREMENTS	3
SULLIVANS COVE PLANNING SCHEME 1997	3
HISTORIC CULTURAL HERITAGE ACT 1995	6
3. PREVIOUS HERITAGE STUDIES AND CONSERVATION PLANNING CONTEXT	8
3.1. CONSERVATION MANAGEMENT PLAN (1995)	8
3.2. PAUL DAVIES REPORTS (2016)	22
3.3. PRELIMINARY HERITAGE IMPACT ASSESSMENT, PRAXIS ENVIRONMENT (2020)	26
4. MORE DETAILED ASSESSMENT OF THE DRILL HALL AND RESIDENCE AND REFINED CONSERVATION POLICY	27
5. THE PROPOSED DEVELOPMENT	42
6. COMMENTARY ON HERITAGE IMPACT AND COMPLIANCE WITH CONSERVATION POLICIES.	43
6.1. DEMOLITION	44
6.2. DEVELOPMENT OF THE FORMER DRILL HALL AND RESIDENCE.	56
6.3. THE NEW PRODUCTION HALL IN RELATION TO THE FORMER DRILL HALL AND RESIDENCE	63
6.4. COMPLIANCE WITH THE GENERAL POLICIES OF THE CMP	69
6.5. COMPLIANCE WITH THE CMP POLICIES REGARDING DEMOLITION	71
6.6. COMPLIANCE WITH CMP POLICIES REGARDING THE FABRIC/FORM OF THE DRILL HALL/RESIDENCE.	77
6.7. COMPLIANCE WITH THE CMP POLICIES REGARDING SETTING AND CURTILAGE	81
6.8. COMPLIANCE WITH 2016 PAUL DAVIES RECOMMENDATIONS	84
7. ASSESSMENT AGAINST STATUTORY HERITAGE REQUIREMENTS	88

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Unless otherwise stated, the north point (or approximate) of maps and plans is to the top of the page – project north is designated as the waterfront of the site.

Cadastral information depicted in this document must not be relied upon without verification by a Surveyor. Rectified aerial imagery has not been used; therefore the actual location as depicted in aerial images may differ to that of actual survey.

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1. Introduction, rationale and brief

This report has been commissioned by GHD Pty. Ltd. (on behalf of Sullivans Cove Distillery), in order to assess the possible historic heritage impact of a concept proposal for a redevelopment of the Huon Quays site, 17 McVilly Drive, Hobart for a distillery operation. Figure 1.1 depicts the *subject site* - traditionally known as HMAS Huon/Huon Quays:



Figure 1.1 – The *subject site* as relevant to the current document. Adapted from www.thelist.tas.gov.au



Figure 1.2 – Aerial photographs of the *subject site* as relevant to the current document. Adapted from www.thelist.tas.gov.au

The subject site is subject to specific statutory heritage requirements (as detailed in Section 2) therefore there is the statutory obligation to appropriately manage the heritage values of the place in any forthcoming development.

2. Statutory heritage requirements

The following heritage listings and overarching legislative provisions are relevant to the management of the historic cultural heritage values of the subject site:

Sullivans Cove Planning Scheme 1997

The subject site listed as a Place of Cultural Significance (place 122) on Part 5(22) Schedule 1 (Table 1) of the Sullivans Cove Planning Scheme 1997, therefore is subject to the provisions of Schedule 22 of the scheme (Heritage).

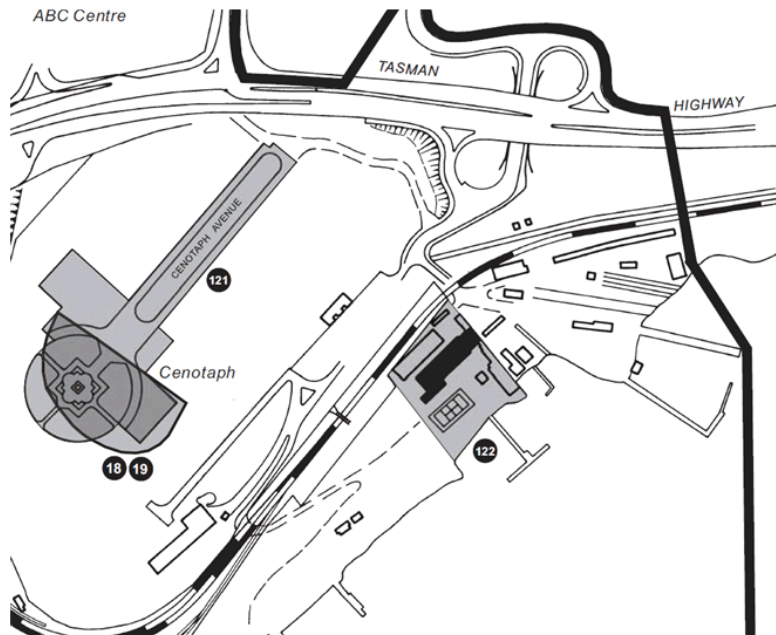


Figure 2.1 - Excerpt from Figure 5, Part E.22 of the SCP597 – the subject site marked as Place 122.

In particular, any proposed development of the place which is not 'deemed to comply' with the provisions of that Schedule (i.e. maintenance of cultural significance as guided by an endorsed conservation plan) will need to demonstrate compliance with Schedule 22.4.5 of the scheme via a *discretionary* development application, which must consider the following:

- ☐ 'Building or works' must complement and contribute to the cultural significance, character and appearance of the place and its setting;
- ☐ 'Building or works' must be in compliance with the conservation strategy of an approved Conservation Plan, where required and/or provided;

- ☐ *The location, bulk and appearance of 'building or works' must not adversely affect the heritage values of any place of cultural significance;*
- ☐ *'Building or works' must not reduce the apparent authenticity of places of cultural significance by mimicking historic forms;*
- ☐ *'Building or works' may be recognisable as new but must not be individually prominent;*
- ☐ *The painting of previously unpainted surfaces is discouraged.*

The *Submission Requirements* as per Part 22.4.3 state that the following *may* be required by the planning authority for a place listed on Table 1 of Schedule 1:

- ☐ A Conservation Plan as defined in Clause 22.3.
- ☐ Street elevations or 'true perspectives' to show the scale and impact of 'building or works' on places of cultural significance.

In addition to the specific heritage provisions of Parts 22 of the scheme, Section 7.3.2 of the scheme lists principles that apply to the retention of the 'historic character of the Cove':

- ☐ *To facilitate use and development which is compatible with conservation of the Cove's cultural heritage values.*
- ☐ *To encourage the recycling of existing buildings through the promotion of new uses, particularly in buildings of identified cultural heritage value.*
- ☐ *The recognisable historic character of Sullivans Cove is not to be compromised by new development which overwhelms the historic spaces and buildings, or, by new development which reduces the apparent authenticity of the historic places by mimicking historic forms*
- ☐ *Changes to urban character will be consistent with conservation of cultural significance and maritime and historic character of Sullivans Cove.*
- ☐ *No new development or part of a development is to be individually prominent particularly when viewed from Sullivans Cove or the River Derwent. Exceptions include cranes and similar development relating to the essential operating requirements of the Tasmanian Ports Corporation.*
- ☐ *The distinctive urban form of Sullivans Cove is to be reinforced in development areas.*

Part 28.5 of the scheme provides guidelines for development control regarding demolition of any Place of Cultural Significance:

The demolition of any building, or works on land shall not be 'permitted' unless; a replacement development has been approved, or such demolition is required by statutory order or is authorised by the Building Surveyor as essential to public safety. Any application for demolition:

(a) Shall be refused if the building is included as a cultural heritage place in Table 1 of the Conservation of Cultural Heritage Values Schedule of this Scheme, unless;

(i) The demolition is approved as part of a Conservation Plan approved by the Planning Authority or otherwise in its discretion under the Conservation of Cultural Heritage Values Schedule of this Scheme;

(ii) The building clearly detracts from the cultural values or significance of the place; or

(iii) There are overriding environmental, economic considerations in terms of the building or practical considerations for its removal, either wholly or in part.

(b) May be refused if in the opinion of the Planning Authority the building contributes to the cultural heritage or urban character of the Activity Area and the building is capable of continued beneficial use.

Part 2.8. specifically considers demolition on a Place of Cultural Significance:

28.8.1 *Where the application involves the demolition of a building on a Place included in Table 1 of Schedule 1 – Conservation of Cultural Heritage Values then the application must satisfy the following:*

The requirement of Clause 22.4.3 for the submission of a Conservation Plan, and

The provision of street elevations or ‘true perspectives’ to show the scale and impact of the demolition on places of cultural significance and the streetscape.

28.8.2 *Where the application involves the demolition of any other building then the application must satisfy the following:*

The requirement of Clause 22.5.3 for the submission of a report identifying that the building does not have heritage value, and

The provision of street elevations or ‘true perspectives’ to show the scale and impact of the demolition on places of cultural significance and the streetscape.

28.8.3 *Where the proposed demolition is required by statutory order or is authorised by the Building Surveyor as essential to public safety, and no replacement building is proposed, a plan shall be submitted showing how the site is to be landscaped and fenced for public urban space in a manner consistent with the intent of the Scheme, pending the granting of a permit for a new building.*

Note that the subject site is not included as a Place of Archaeological Sensitivity on Part 5(22) Schedule 1 (Table 1) of the scheme.

Historic Cultural Heritage Act 1995

The place is listed on the Tasmanian Heritage Register (Place # 2932 – listed as ‘Foreshore Properties’, which can be assumed to be both titles comprising the subject site). Accordingly, the place is subject to the provisions of the *Historic Cultural Heritage Act 1995* (HCHA). Part 6 of the HCHA (Heritage Works) sets the process by which approvals for works may be gained from the Tasmanian Heritage Council (THC):

35. Heritage works require heritage approval

- (1) A person must not carry out any heritage works unless those heritage works have heritage approval.*
- (2) For the purposes of subsection (1), heritage works are taken to have heritage approval if, and only if –*
 - (a) in a case where a certificate of exemption has been issued, the heritage works are carried out in accordance with –*
 - (i) that certificate of exemption; and*
 - (ii) if a discretionary permit or other permit is required for the heritage works under the Planning Act, that discretionary permit or other permit; or*
 - (b) in a case where a certificate of exemption has not been issued, the heritage works are carried out in accordance with a discretionary permit.*
- (3) It is a defence in proceedings for an offence under subsection (1) if the defendant establishes that –*
 - (a) the heritage works were carried out in response to an emergency; and*
 - (b) the heritage works were, both as to nature and extent, reasonably necessary for the purposes of responding to the emergency; and*
 - (c) in the circumstances, it was not practicable to seek a certificate of exemption; and*
 - (d) the defendant, before, while or as soon as practicable after carrying out the heritage works, notified the Heritage Council, in writing, of the emergency and the details of the heritage works.*

Sections 36-41 set the process for the lodgment and assessment of applications for a heritage works permit, via a Discretionary Development Application under the Land Use Planning and Approvals Act 1993.

Section 42 describes the process whereby certain works may be exempt from the requirement of s.35:

42. Certificates of exemption for heritage works

- (1) A person may apply to the Heritage Council for a certificate of exemption for heritage works.*
- (2) The exemption certificate application –*
 - (a) is to be in a form provided or approved by the Heritage Council; and*
 - (b) is to be supported by such information as the Heritage Council requires, either at the time of lodgment or subsequently.*
- (3) The Heritage Council may –*
 - (a) approve the exemption certificate application; or*

(b) refuse the exemption certificate application.

(4) Without limiting its discretion, the Heritage Council must approve the exemption certificate application if it is reasonably satisfied that the heritage works –

(a) are identified in the works guidelines as works that will have no impact or only negligible impact on the historic cultural heritage significance of the relevant registered place or heritage area; and

(b) are capable of being carried out in accordance with the works guidelines.

Whilst the HCHA provides no specific detail as to how particular proposals are considered, nor does it provide any indicative thresholds of what may be considered to have *no or negligible* heritage impact, the THC/Tasmanian Government publication *Works Guidelines for Historic Heritage Places* (November 2015)¹ provides further detail on the application process, guiding principles and the basis for decisions made by the THC. In addition, the THC has a series of practice notes and technical guides, available via www.heritage.tas.gov.au which provide useful guiding principles for how the THC are expected to assess and determine applications for heritage works.

Environment Protection & Biodiversity Conservation Act 1999

The place is not included on the National or Commonwealth Heritage Lists, therefore the historic cultural heritage provisions of the Environment Protection and Biodiversity Conservation Act 1999 are not applicable. Nor is the subject site on the World Heritage list or within any buffer zone arising from such.

Aboriginal Heritage Act 1975 (Amended 2015)

An assessment of any possible Aboriginal heritage values is not part of the brief for this report; nonetheless the provisions of the *Aboriginal Heritage Act 1975* are applicable to the place.

¹ http://heritage.tas.gov.au/Documents/Works_Guidelines_FINAL_Nov2015.pdf

3. Previous heritage studies and conservation planning context

3.1. Conservation Management Plan (1995)

The conservation planning context for the former HMAS Huon site is governed primarily by the *Conservation Management Plan, HMAS Huon, Hobart, Tasmania* (Australian Construction Services, September 1995) – hereinafter referred to as the *CMP*, which was commissioned prior to the Commonwealth Government's divestment of the site and intended to guide the best-practice management of the heritage values of the site going forward. That document (p75) defines the respective buildings of the site as follows (that numbering system has been adopted in further planning documents – see Section 5.2, and has been adopted here):

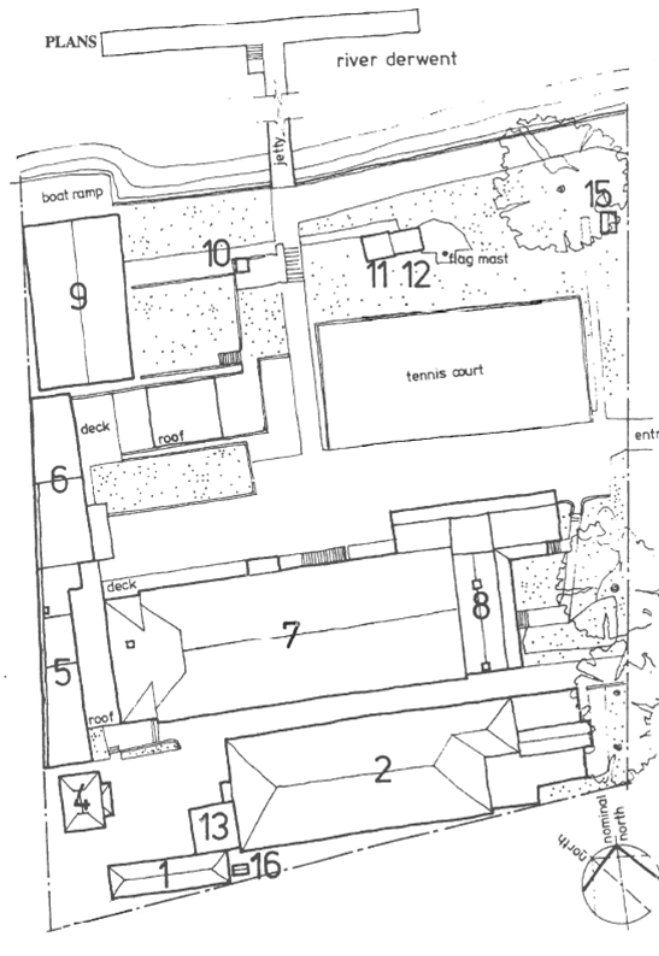


Figure 4.1 – Building numbers drawn from the CMP (and repeated in the current document). Note that building 9 is not within the current development site.

The history of the HMAS Huon site was detailed in Section 2 of the CMP, which has been used as the basis for the current assessment and this document is to be read in conjunction with that previous work and the citations therein. The scope of the preliminary heritage impact assessment (Nor the current document) did not allow for any further historical research, however several historic photographs which were not included in the CMP have been consulted here – particularly in the consideration of the exterior treatment of the drill hall.

As part of the brief for the preliminary heritage impact assessment, it was asked whether the CMP was considered to remain current and valid (noting it is almost 30 years old) and whether any detail was deficient. The conclusion was that the CMP is still valid and that it continues to provide a sound (albeit not necessarily comprehensive) basis for conservation planning for the site. The broad deficiencies in the CMP are considered to be:

- That internal detailing was not a key aspect of the CMP (although broad principles were introduced).
- Viewscapes were only dealt with briefly.
- An update to reflect the current statutory heritage/planning and ownership contexts is required (generally provided here). E.g. the CMP was written in the very early years of the Historic Cultural Heritage Act 1995, pre-dates the Sullivans Cove Planning Scheme 1997 and was during Commonwealth ownership of the site for military purposes.
- Whilst providing a sound basis for conservation planning, it is likely that a broader and more up-to-date master planning process would need to be undertaken to extend the scope of the CMP.

Nonetheless, it is considered that the CMP is sufficient to inform the scope of the current considerations and is relied upon here, with added analysis and detail where warranted (e.g. see Section 4).

Section 4.3 of that document defines the significance of the buildings comprising the site as:

Items categorised as crucial to the significance of the place:

- *7/8. The drill hall and residence are the first buildings at HMAS Huon and they are the most distinctive from an architectural aspect. They reflect the involvement of the Tasmanian public works department, training nature of the establishment and also have a relatively high integrity which permits the expression of their original character.*

Items associated with the general significance

- *1. This is an early building although it has no aesthetic qualities of merit or specific historical associations. The building has a low relative integrity.*
- *2. This building has no inherent aesthetic qualities or merit but can be associated with the development of HMAS Huon during WWII. This building has a relatively low integrity.*

- 4. This is an early building although it has little aesthetic qualities (except for its gable roof) and has no specific historical associations. This building has a relatively low integrity.
- 5/6 (central portions). These are remnants of early amenities buildings with no aesthetic qualities of merit or specific historical associations. These buildings have been dramatically extended and altered, particularly internally, but also with a complete change to the roofs to building 6.
- 9. This is the remnant of an early building (pre-WWII) which is the only building with a direct physical association to the river and the site's naval tradition.

Items with little or no reflection of the significance

- 5/6 (southern and northern portions). These are post-WWII buildings with no inherent aesthetic qualities or historical associations.
- 10. While this appears to be an early (pre-WWII) structure, it is minor with no inherent aesthetic qualities or historical associations.
- 11/12. These structures probably date from WWII but they are minor with no inherent aesthetic qualities or historical associations.
- 13. This is a modern structure with no aesthetic qualities or historical associations.
- 15/16. These are modern structures with no aesthetic qualities or historical associations.

The CMP provides a series of conservation policies for the site (policy numbers as per the CMP):

5.3.1	<i>It is recommended that the HMAS Huon be conserved to protect and enhance its cultural heritage significance, with controlled adaptation to encourage its occupation and viability and to make possible the maintenance of its significant fabric.</i>
5.3.2	<i>It is recommended that the naval associations and orientation of the HMAS Huon to the River Derwent be preserved and if possible enhanced.</i>
5.3.3	<p><i>It is recommended that the 'crucial' buildings of HMAS Huon (as identified in 4.3) are conserved and adapted with special attention to:</i></p> <ul style="list-style-type: none"> -□ <i>The restoration/reconstruction of buildings 7/8 to its 1912 extent allowing for adaptation for compatible uses.</i> -□ <i>The maintenance of building 9 to its present form or its replacement with a building of identical scale and form.</i> -□ <i>The restoration/reconstruction of the exterior of building 4 to an appropriate, or at least more sympathetic, earlier state.</i>

5.3.4	<i>It is recommended that the 'contributory' buildings of HMAS Huon (as identified in 4.3) are encouraged to be maintained and adapted, although the 'crucial' buildings should be given priority for conservation.</i>
5.3.5	<i>It is recommended that any new development required to make the property viable be respect [sic.] the significance, scale and existing general character of HMAS Huon.</i>
5.3.6	<p><i>It is recommended that the Commonwealth ensure statutory protection of HMAS Huon as identified in this document by appropriate state and/or local government level prior to its disposal.</i></p> <p><i>It is further recommended that HMAS Huon be nominated by the Department of Defence for inclusion of the Register of the National Estate based upon this document prior to its disposal.</i></p> <p><i>It is further recommended that all future owners agree as a condition of disposal not to oppose the inclusion of the HMAS Huon property on any heritage registers or lists.</i></p>

Section 6.2 of the CMP provides the following guidelines for individual items on the site:

- *Building 7/8 should remain the dominant building on the site, particularly as seen from the river.*
- *A jetty should be maintained in its present location although the replacement of alteration/extension/reduction of the existing jetty should be permitted. Any additional structures or enclosures or enclosures associated with the jetty should be minimal in number, extent and visual treatment.*
- *None of the existing landscaping – lawns, flower beds, shrubs, paving – need be retained. Any replacement landscaping should:*
 - *Retain the open space between buildings 7/8 and the river and views to and from that building and the water;*
 - *Simplify the landscaping to enhance and reinforce buildings 7/8;*
 - *Seek to create more uniformity in vegetation selection and materials/construction for paving and retaining walls.*
 - *Rationalise the terracing of the site.*
- *It is encouraged that the four large trees on the eastern boundary of the site be retained and maintained although this is not mandatory.*
- *The retention of the tennis court is not mandatory and neither is its removal. It should however not be greatly enlarged or modified. Any structures associated with the courts or any subsequent uses of the area should be minimal in number, extent and visual treatment.*
- *The retention of the existing covered barbeque area and the timber deck off building 6 is not required and in fact should be considered for removal and/or replacement in a more sympathetic and relevant design.*
- *The modern covering on the added on the west side of building 7 should be removed and not replaced.*

- *Where hard paving is required, bitumen should dominate although concrete and controlled acres of other finishes – such as brick paving – should also be permitted.*
- *A naval flag mast, as adjacent to buildings 11/12, should be maintained although its location may be changed so long as it remains on the river side of buildings 7/8 and preferably in a prominent location on the site.*

In terms of specific buildings, the CMP recommends the following (note that the following also includes buildings not contained within the current subject site – those within the current subject site are highlighted):

Building	Recommendation to retain	If retained (paraphrased for brevity in some instances).	If not retained
1	<i>Encouraged but not mandatory</i>	<ul style="list-style-type: none"> • <input type="checkbox"/> <i>Would need to be restored/reconstructed.</i> • <input type="checkbox"/> <i>Should not be enlarged</i> • <input type="checkbox"/> <i>Openings may be retained or altered but if altered should generally reflect a traditional approach to those openings in regards to proportions and materials.</i> • <input type="checkbox"/> <i>No restrictions on interior that don't impact exterior.</i> • <input type="checkbox"/> <i>Paint in a scheme that unifies the buildings on the site.</i> • <input type="checkbox"/> <i>No works are to compete with or detract from buildings 7/8.</i> 	<i>Appropriately documented and archived.</i>
2	<i>Encouraged but not mandatory</i>	<ul style="list-style-type: none"> • <input type="checkbox"/> <i>It need not be restored/reconstructed unless desired.</i> • <input type="checkbox"/> <i>Should not be enlarged and its external form and cladding should be retained.</i> • <input type="checkbox"/> <i>Openings may be retained or altered, but if altered should generally reflect a traditional approach to openings in regards to proportions and materials.</i> • <input type="checkbox"/> <i>More recent additions to the east may be removed or modified within their existing plan envelope. Roof forms may be modified but should remain simple and in keeping with the main form of the building.</i> • <input type="checkbox"/> <i>Small porch may be modified to be more traditional. No restrictions on interior if these don't impact</i> • <input type="checkbox"/> <i>Exterior, although v-groove walling and fibrous plaster should be retained and expressed.</i> • <input type="checkbox"/> <i>No works are to compete with or detract from buildings 7/8.</i> 	<i>No action required as the building as original drawings exist.</i>
4	<i>Encouraged but not mandatory</i>	<ul style="list-style-type: none"> • <input type="checkbox"/> <i>It need not be restored/reconstructed unless desired.</i> • <input type="checkbox"/> <i>Should not be enlarged. External cladding to remain.</i> • <input type="checkbox"/> <i>Openings may be retained or altered, but if altered should generally reflect a traditional approach to openings in regards to proportions and materials.</i> • <input type="checkbox"/> <i>Retain distinctive gable roof and vents.</i> 	<i>Appropriately documented and archived.</i>

		<ul style="list-style-type: none"> • <input type="checkbox"/> Covered entry on east side may be removed or adapted. • <input type="checkbox"/> Retain four panel doors on east elevation. • <input type="checkbox"/> No restrictions on interior that don't impact exterior. • <input type="checkbox"/> No works are to compete with or detract from buildings 7/8. 	
5	Northern portion: Encouraged but not mandatory	<ul style="list-style-type: none"> • <input type="checkbox"/> It need not be restored/reconstructed unless desired. • <input type="checkbox"/> It should not be enlarged and its external form, openings and treatment should be retained including the large v-groove boarded door. • <input type="checkbox"/> Skillion roof should be retained. • <input type="checkbox"/> Paint may be removed if desired. • <input type="checkbox"/> No restrictions on interior that don't impact exterior. • <input type="checkbox"/> No works are to compete with or detract from buildings 7/8. 	<p>Appropriately documented and archived.</p> <p>The modern roof between this building and building 7 should be removed.</p>
	Southern portion: Not required.	<ul style="list-style-type: none"> • <input type="checkbox"/> Should not be enlarged. • <input type="checkbox"/> Openings may be altered. • <input type="checkbox"/> Skillion roof should be maintained. • <input type="checkbox"/> External treatment should match the northern portion. • <input type="checkbox"/> External privacy screen may be removed or altered. • <input type="checkbox"/> No restrictions on interior that don't impact exterior. • <input type="checkbox"/> No works are to compete with or detract from buildings 7/8. 	No action required as the building as original drawings exist.
6	Northern portion: Not required.	<ul style="list-style-type: none"> • <input type="checkbox"/> Should not be enlarged. • <input type="checkbox"/> Openings may be altered. • <input type="checkbox"/> Although not mandatory, the skillion roof should be modified to be less dominant from the river. • <input type="checkbox"/> External treatment should match the southern portion. • <input type="checkbox"/> External deck should be removed or modified to be smaller and less 'mock colonial'. 	<p>No action required as the building as original drawings exist.</p> <p>If replaced with another building, this should only be single storey and with a</p>

		<ul style="list-style-type: none"> • <input type="checkbox"/> No restrictions on interior that don't impact exterior. • <input type="checkbox"/> No works are to compete with or detract from buildings 7/8. 	greater traditional character than the existing.
	Southern portion: Encouraged but not mandatory	<ul style="list-style-type: none"> • <input type="checkbox"/> It need not be restored/reconstructed unless desired. • <input type="checkbox"/> It should not be enlarged and its external form, openings and treatment should be retained including the large v-groove boarded door. • <input type="checkbox"/> Although not mandatory, the modification of the roof to its earlier gable form is encouraged. • <input type="checkbox"/> Remove the flat roof porch and replaced with a more traditional form. • <input type="checkbox"/> Paint may be removed if desired. • <input type="checkbox"/> No restrictions on interior that don't impact exterior. • <input type="checkbox"/> Retain fireplaces (need not be operable). • <input type="checkbox"/> No works are to compete with or detract from buildings 7/8. 	Appropriately documented and archived.

		For both buildings	Individual retained buildings (non-retention is not an option).
7	<p>These two buildings were designed as one building although the residence portion – building 8 – is distinctly different in its external detailing and internal treatment.</p> <p>These buildings should be retained and their traditional qualities restored/reconstructed wherever possible including an appropriate colour scheme. The important era for this building and the basis for any restoration/reconstruction is its original 1912 date. Later additions and/or alterations do not contribute to its significance.</p>	<ul style="list-style-type: none"> • <input type="checkbox"/> The repair of existing fabric is preferred to its replacement. • <input type="checkbox"/> Exposed services should be removed and/or rationalised wherever possible and new services concealed either entirely in the residence portion or within the structure of the drill portion. • <input type="checkbox"/> Reversible works are preferred wherever possible. • <input type="checkbox"/> A maintenance and inspection program should be developed for the long term care of the building. • <input type="checkbox"/> The existing building envelope of both buildings should be retained including its cladding, original openings, timber detailing, chimneys etc. except as may be noted elsewhere. • <input type="checkbox"/> Any alterations required to the existing fabric should reflect the construction, materials, scale and proportions of the original building. 	<ul style="list-style-type: none"> • <input type="checkbox"/> The balcony/veranda added to the north elevation may be retained, removed and/or altered but should not be increased in size or further enclosed. It would be desirable that they be removed and the original staircase be reinstated, but this should not be mandatory. • <input type="checkbox"/> The original openings on the building's perimeter wall should be retained and no further openings or modifications permitted. It is desirable to reinstate the double doors on the southeast corner if the opportunity arises. Non-original openings at the lower level of the north elevation may be retained, closed or altered so long as they are treated in a traditional manner in keeping with the original openings of the building. • <input type="checkbox"/> There should be no restrictions on the interior of the lower level, except retaining the original chamfered timber columns, arched fireplace supports, not impacting the drill hall space above, no compromise structural integrity and no further excavation. • <input type="checkbox"/> The existing offices at the western end of the drill hall space should be retained including the existing doors, fireplaces, pressed metal ceilings, painted lining boards and other traditional detailing. The reinstatement of missing details is encouraged but not mandatory. The retention of the internal enclosure around the double doors on the southwest corner of the main room is not required, but neither its removal mandatory. The reinstatement of the one missing room in the northwest corner of the main room should not be required but may be undertaken if the room is necessary for a function. • <input type="checkbox"/> The retention of the modern varnished timber lining boards in the drill hall space is not required but neither is their removal. A lining more appropriate to the original would be preferred (e.g. painted horizontal boards).

			<ul style="list-style-type: none"> • <input type="checkbox"/> <i>The existing timber floor should be retained along with the exposed structure of the walls and roof. Limited lining of the walls and ceilings should be permitted but must be limited and controlled to maximise the expression of the structure. The roof trusses should remain exposed but the space between may be lined.</i> • <input type="checkbox"/> <i>The enclosure of rooms and offices at the eastern end of the drill hall space may remain but their removal along with the associated stairs and partitions is encouraged. Further full height enclosures and the enclosure of the roof trusses should not be permitted.</i> • <input type="checkbox"/> <i>Only after critical assessment of accommodation requirements should additional partition of the drill hall space be considered and only then if the rooms are only a single storey (<2800mm high).</i> • <input type="checkbox"/> <i>Further works and decoration to the interior of the drill halls space should recognise the original approach to the use of materials, the simplicity of its treatment and the dominance of painted finishes. New works should create a restrained character rather than an inflated or flamboyant appearance.</i>
8			<ul style="list-style-type: none"> • <input type="checkbox"/> <i>All the remaining traditional fabric such as the pressed metal ceilings, fireplace surrounds, joinery and staircase should be retained. Where elements are missing it is encouraged, although not mandatory, that they be appropriately reinstated.</i> • <input type="checkbox"/> <i>The scale of the rooms should remain the same but interconnections between rooms may be introduced in an appropriately sympathetic manner.</i> • <input type="checkbox"/> <i>It would be desirable to remove the additions to the north and to reinstate the original perimeter of the building. This however is not mandatory. The extent of the present additions should not be increased and the shallow pitched</i>

			<p>awning over the northernmost deck addition should be removed. As well, the glazed rooms at the lower level under that deck should also be removed.</p> <ul style="list-style-type: none"> • <input type="checkbox"/> The original timber veranda post decoration should be reinstated to the east elevation. • <input type="checkbox"/> Additional accommodation requiring significant changes should either be located within rooms or areas already altered or the added portions of the buildings, or possible in the areas adjoining the two portions of buildings 7-8.
9	Encouraged but not mandatory. The removal of this building is dependent upon its replacement.	<ul style="list-style-type: none"> • <input type="checkbox"/> The building need not be restored/ reconstructed. • <input type="checkbox"/> It should not be enlarged and its external form and cladding should be retained. • <input type="checkbox"/> Its opening may be retained or altered, but if altered should generally reflect a traditional approach in regards to proportions and materials (allowance should be made for the river side doors where functional requirements may require modern roller type doors). • <input type="checkbox"/> No restrictions on interior that don't impact exterior. • <input type="checkbox"/> No works are to compete with or detract from buildings 7/8. 	<ul style="list-style-type: none"> • <input type="checkbox"/> The removal or replacement of this building should only be permitted if it is replaced with a similar building on the same plan utilising the remnant early footings. Any replacement should reflect the existing form and use of weatherboards.
10	These structures may be removed or retained, but if retained they should not be enlarged.	<ul style="list-style-type: none"> • <input type="checkbox"/> The retention of these structures and any proposed works such as painting, should consider their visual impact upon the appreciation of the property. 	<ul style="list-style-type: none"> • <input type="checkbox"/> Appropriately documented and archived. • <input type="checkbox"/> The adjacent concrete tanks need not be retained.
11-12			<ul style="list-style-type: none"> • <input type="checkbox"/> Appropriately documented and archived. • <input type="checkbox"/> The adjacent portions of concrete pads associated with the flag mast need not be retained if the mast is relocated.

13	<i>This structure should be removed and not replaced.</i>		
15-16	<i>These structures may be removed or retained, but if retained they should not be enlarged.</i>	<ul style="list-style-type: none">• <input type="checkbox"/> <i>The retention of this structures and any proposed works such as painting, should consider their visual impact upon the appreciation of the property.</i>	<ul style="list-style-type: none">• <input type="checkbox"/> <i>It would be desirable to transfer the function of these sheds to one or part of the 'crucial' or 'contributory' buildings.</i>

The CMP also sets guidelines for any new development:

6.2.3	<i>New Development</i>
a	<i>New building developments should only be considered after it is critically assessed whether the required accommodation can be provided within the 'crucial' or 'associated' buildings having regards for the previous guideline statements.</i>
b	<i>New building development should only be sited where 'associated' buildings currently exist so long as the abovementioned accommodation assessment is formally undertaken and the buildings to be removed have been recorded as noted.</i>
c	<i>New building development should preferably utilise weatherboard construction with gable roof forms. Building developments should be single storey, have a traditional form and character and new developments should avoid monolith solutions.</i>
d	<i>New buildings should stop short of literally replicating traditional buildings and only draw upon them in a general sense for inspiration. New buildings should be understated compared with buildings 7/8.</i>
e	<i>New building development should not adversely impact upon the appreciation of the buildings 7/8 nor make them redundant in a functional sense.</i>
f	<i>New development should endeavour to maintain the site with its own identity rather than be orientated outside the existing property boundaries. This does not imply that it can not be functionally part of a larger development or that it must be fenced off.</i>
g	<i>Refer to Figure 6.2 for areas of potential development.</i>

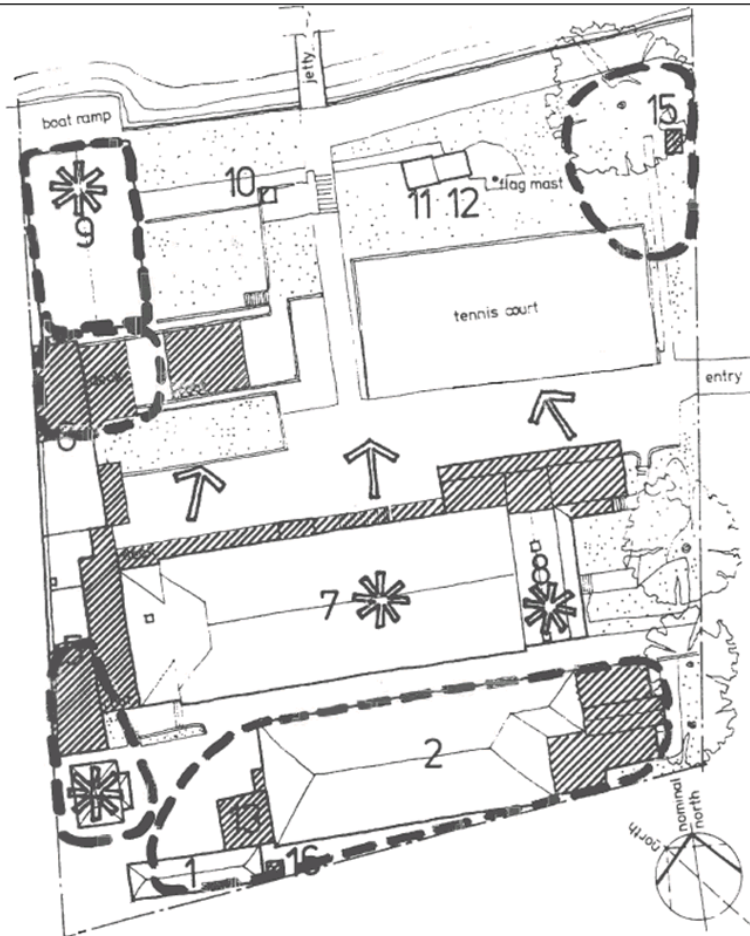


Figure 6.2 HMAS HUON, HOBART
development guides: arrows are important views, stars are focal points, hatched building portions are modern and may/should be removed, dashed areas may be re-developed with new buildings

3.2. Paul Davies Reports (2016)

In 2016, Paul Davies Pty. Ltd. Architects and Heritage Consultants were commissioned by Tasports to undertake a review of the heritage issues surrounding the site, as well as to formulate development guidelines arising from a previous site Master Plan. Specifically, these documents are:

- *Review of Heritage Issues, Former HMAS Huon Site, Hobart* (July 2016).
- *Huon Quays. Detailed Design Considerations for Future Site Development Arising from the Site Master Plan* (November 2016).

Whilst the Davies documents were not intended to replace the CMP, the first of these plans aimed to assess the currency of the CMP (then over 20 years old) and to revisit the broad recommendations for each building (as detailed in 3.1 above). That document was also intended to consider the possibility of national heritage significance of the site (alluded to in the CMP, but not largely explored) as well as consider the wider setting and landscape context of the place (also not largely dealt with in the CMP). In this document, Davies concludes that:

The parameters that would govern development on the site from a heritage perspective are:

1. *Retain building 7/8, undertake conservation work, remove some of the additions and accretions and develop an integrated use for it within the overall development.*
2. *Retain a setting between building 7/8 and the waterfront, that is without new built form occupying that area.*
3. *Undertake development within the areas marked on the attached site plan (page 6) [reproduced below] noting that the proposed area extends across the boundary with the slipway.*
4. *Integrate development with any proposals from City Council for the Regatta Ground so that the whole area is integrated and strengthened by any works that take place.*
5. *Undertake a detailed urban design and heritage assessment to establish guidelines for new development of the site. This should include a new CMP for the combined Slipway Huon site. This plan can set out with more authority the relative heritage values of all the component parts of the site and will allow development areas to be clearly defined. It would appear that there is considerable development potential available.*



[Reproduced from Davies, July 2016:6]. Indicative site plan showing areas that may be possible to undertake development. It is noted that this is conceptual and needs to be developed through a more detailed site study that considers heritage, views, setting, adjacent uses and proposals, etc. The building coloured red is the Drill Hall/Residence (7/8), the boat shed is building 9 and the areas coloured yellow are potential areas for development. It is noted that this does not necessarily suggest that the whole of the identified area can be developed but, on a preliminary assessment these areas appear capable of development.

Davies briefly reviewed the whole-building recommendations of the CMP and concluded the following:

Building	Name	Review
1	Office (pre-1940)	<i>This structure may have some significance, this will need to be assessed. However it is a small building that if significant may be able to be relocated or adapted.</i>
2	Mess rooms (1940 & 1954)	<i>Quite low significance.</i>
3 & 14*	Sea scouts	<i>On adjacent site – not assessed</i>
4	Mess room, lavatories and later office (1925)	<i>May have modest significance.</i>
5-6	Amenities (c1920, 1960, 1972)	<i>Unlikely to have more than low significance.</i>
7/8	Drill Hall HMAS Huon/residence 1912 (extended 1928).	<i>This is the core and original building that has triggered the National Heritage listing². It has State level heritage significance and possibly National significance.</i>
9*	Navy Divers building, former boatshed (1961).	<i>The CMP is not clear on its significance or history, it may have moderate significance but will need to be further investigated.</i>
10, 11, 12, 13, 15, 16	Ammunition store, stores, carport, garden sheds.	<i>Not significant.</i>

The second Davies document was formulated partly in response to Recommendation 5 which sought to further analyse the wider site and context and in particular earmark possible sites for further development. This document generally assumed that the drill hall/residence building would be retained, and that no other buildings need necessarily be retained (with the possible exception of the boatshed – i.e. Building 9). This assessment also included a portion of the adjacent slipyards site (as per the subject site definition in Section 1 of the current document).

The key point of the second Davies document (i.e. detailed design considerations) was to formulate broad development guidelines/building envelopes which assumed that further development of the site is to occur (as foreshadowed by the precinct plan as per 5.2 above). The precinct plan had identified four nominal building envelopes on the site – however as discussed above, that report did not include detailed consideration of heritage and in particular wider viewscape/curtilage issues for the drill hall in particular. The Davies report provided a more rigorous assessment of such (and did not repeat the previous assessment of the significance of other buildings). In particular, key points which the report sought to address were (including):

² Note that Davies erroneously states that the place is on the National Heritage List – it is not on that list.

- ☐ *creating a good setting around the Drill Hall Building that links it (visually and potentially functionally) to the water, the Cenotaph area and the Regatta Grounds*
- ☐ *retaining the dominance of the Drill Hall building as part of the gateway to Hobart on the approach from Tasman Bridge*
- ☐ *the scale and potential massing of new structures with regard to views to the site from key locations and views out from the Drill Hall Building*

That document was also prefaced with the following:

The Drill Hall as noted is the key structure of significance, over time its setting in relation to the water, while maintained has been compromised by a range of works that have served to separate the building to some extent from its setting.

The Drill Hall is a fine building that has a dominance in the locality achieved through its scale and design, its colour (white) and its location and orientation that allows it to be seen from a range of vantage points. It is important in terms of retaining significance for the building to be seen and not obscured or overwhelmed by new development.

However not all parts of that building have high significance and the later changes and fitout are capable of adaptation, removal or being replaced with sympathetic elements.

Davies undertook a series of viewscape analyses, which established key viewlines which he suggested be maintained in any form of future development in order to maintain the prominence and curtilage of the drill hall building. Davies further analysed the broad possible development locations as identified in the precinct plan, and further articulated the possible floor area and spatial arrangement of such buildings, as well as considering general roof forms, the natural topography of the site etc. Davies was careful to articulate that the proposed locations in the precinct plan need not necessarily represent a 'maximum capacity' arrangement but was more an 'options' discussion of where *some* development may occur.

Whilst the report did not set firm 'conservation policy' as such, it provided a rigorous assessment of the notional building envelopes for such development which will be utilised in Section 6 of the current document.

3.3. Preliminary Heritage Impact Assessment, Praxis Environment (2020)

In 2020 Praxis Environment was engaged to broadly consider a schematic proposed development for a potential 'fit' with the conservation planning background of the site and to 'red flag' any areas of possible non-compliance which may prevent the implementation of such a proposal. That report was used in the initial feasibility and concept design phase and has provided the basis for the current (expanded and more specific) document.

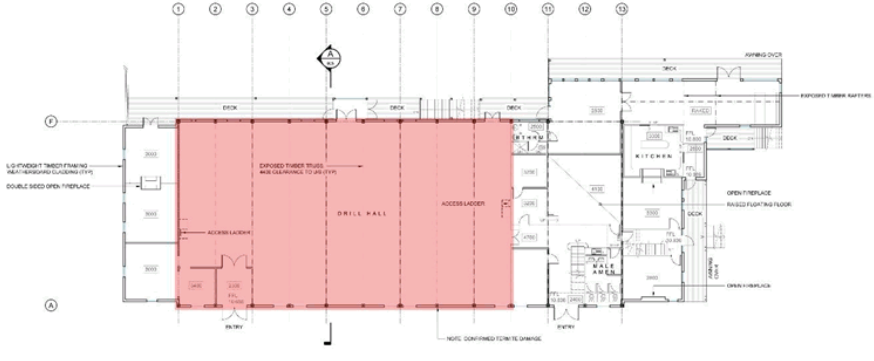
Broadly, that concept involved the conversion of buildings 7/8 to the 'front face' of a major distillery operation, with those buildings to house barrel storage, a museum, offices and a bar and brand experience area. The ground floor area would be redeveloped as publicly accessible spaces and generally the volume of the rooms on the first and second floors would be retained. Buildings 1,2,13 and 16 would be demolished to make way for a large new building at rear, attached to buildings 7/8 by a glazed linkway which would house the more 'industrial' aspects of the distillery (e.g. wash, distilling, bottling etc.).

That preliminary heritage impact assessment concluded the following:

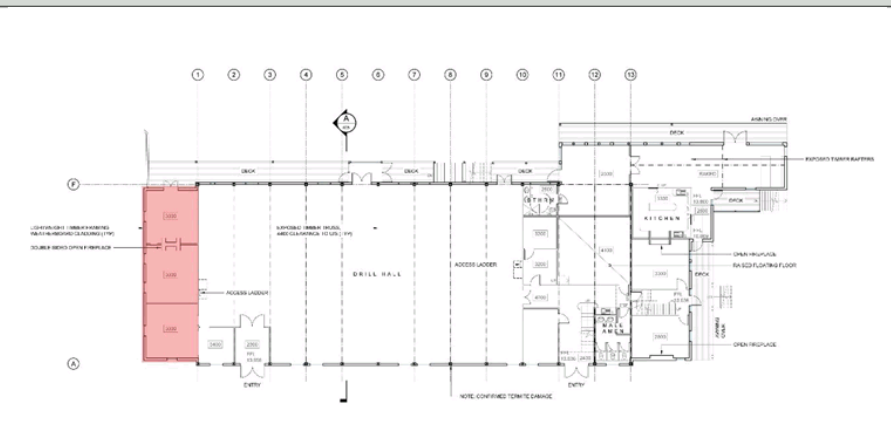
- That a detailed fabric survey be undertaken of the drill hall/residence (i.e. buildings 7/8) which will consider the conservation policies of the CMP in specifically identifying and ranking significant fabric to inform the detail design process **[see Section 4 of the current document]**.
- Initially (subject to the above) it is likely that the following will need to be further considered and articulated in the detail design process:
 - Retention/restoration of the volume of the drill hall.
 - Retention/restoration of the rooms on the western end of the drill hall.
 - Removal of modern partitioning of the drill hall unless there is a functional need to retain
 - Retention of the smaller spaces of the residence.
 - Minimisation of the need for openings in the rear wall of the drill hall/residence.
 - Retention of key structural features in the ground floor (basement).
 - These points will need to feed into a further refined heritage impact assessment to support any future development application **[the purpose of the current document]**.
- Further resolution on how the excavation of the basement void will be achieved to link with the basement of the proposed building. **[note that this was omitted from the conceptual scheme in the current proposal]**.
- That if it is not considered feasible/desirable to retain building 4 in its current location (and restored/converted for an adaptive reuse, then consideration should be given to its relocation to a nearby site (e.g. other Tasports land, or in consultation with administrators of other public land).
- Further clarity on the intention to retain or remove buildings 5,10,11,12 is required (noting that their removal is likely to be justifiable if required).

4. More detailed assessment of the drill hall and residence and refined conservation policy

Further to the recommendation of the preliminary heritage assessment (Praxis 2020), a further detailed assessment of the drill hall and residence was undertaken, which sought to set more specific policies for the approach to planning works on those buildings (i.e. in a more detailed and direct manner than that of the CMP). The following advice arise from that analysis which was provided to the design team in the early stages of resolving the project:

The body of the drill hall	
	
Remove	<ul style="list-style-type: none"> • <input type="checkbox"/> More recent vestibule and room in N/W corner.
Retain	<ul style="list-style-type: none"> • <input type="checkbox"/> The ability to discern the large open space as a recreation area. • <input type="checkbox"/> Exposed trusses and open roofspace. • <input type="checkbox"/> Ideally limit intervention to wall linings to retain the ability to see framing and back of weatherboards. • <input type="checkbox"/> Timber floors. • <input type="checkbox"/> Lined wall and joinery to the three northern end rooms (lower added portion of lining may be adapted/removed).
Adapt	<ul style="list-style-type: none"> • <input type="checkbox"/> Any necessary partitioning should be lightweight and ephemeral and allow discernment of the larger space. • <input type="checkbox"/> Roof insulation should be achieved under any new cladding, rather than lining the ceiling if possible. • <input type="checkbox"/> Installation of new services, furnishings, fittings etc. should be as reversible as possible. • <input type="checkbox"/> Paint finishes etc. • <input type="checkbox"/> Stair/lift access to the upper floor and basement is likely to be desirable. Whilst this may result in the loss of some significant/original fabric, it is likely that an acceptable balance can be achieved if it reinvigorates use of this space and/or overall viability of the building.

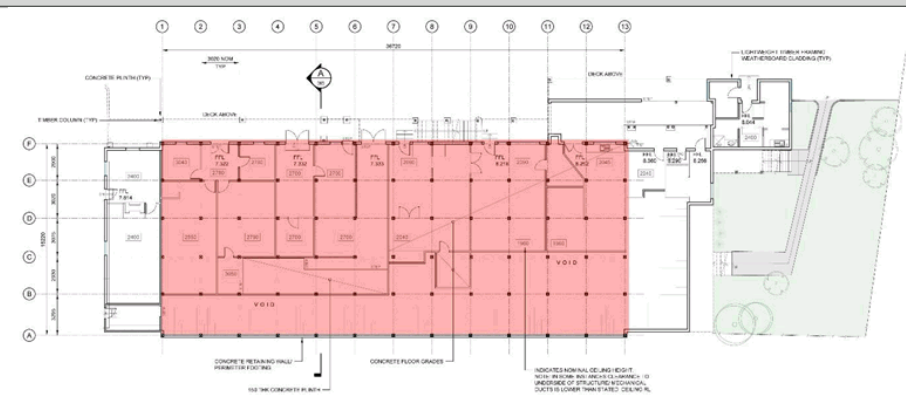
The southern end of the drill hall	
Remove	<ul style="list-style-type: none"> ❑ Ideally remove modern insertions that have intruded into the space of the drill hall, including the dining room, toilets, hallways, stairs, plant room and upper rooms/mezzanine.
Retain	<ul style="list-style-type: none"> ❑ Whilst removal would be a better option, if these areas provide critical support to the ongoing use of the building, they may be retained. ❑ Reinstate the ability to discern the large open space as a recreation area. ❑ Exposed trusses and open roofspace.
Adapt	<ul style="list-style-type: none"> ❑ Although not ideal, a better resolved partitioning of this area may be acceptable if this provides critical support to the ongoing use of the building, they may be retained. ❑ This area may provide a logical and minimal impact area for vertical circulation infrastructure from the basement to the upper floor of the residence through the drill hall.

Northern end rooms of the drill hall	
	
Remove	<ul style="list-style-type: none"> ❑ Modern floor coverings.
Retain	<ul style="list-style-type: none"> ❑ The ability to discern the three separate and smaller-scale rooms. ❑ Lining boards ❑ Skirtings, architraves etc. ❑ Fireplaces.
Adapt	<ul style="list-style-type: none"> ❑ Connection may be achieved by domestic-scale doors between if necessary. ❑ New doors may be replica Edwardian or notably modern as desired. ❑ Installation of new services, furnishings, fittings etc. should be as reversible as possible. ❑ Paint finishes, floorcoverings etc. ❑ Reinstate opening in wall to small room on NW corner of the drill hall.

31

The residence (upper floor)	
Remove	<ul style="list-style-type: none"> ❑ Modern fittings as desired (e.g. kitchen and bathroom fitout).
Retain	<ul style="list-style-type: none"> ❑ It is critical that the smaller domestic feel of this area be retained. ❑ Joinery such as skirtings, architraves, fire surrounds, doors etc. as these are of a heightened level of detailing when compared to the more utilitarian drill hall fitout. ❑ Wall linings. ❑ Stairs. ❑ Fireplaces, chimney breasts etc.
Adapt	<ul style="list-style-type: none"> ❑ Installation of new services, furnishings, fittings etc. should be as reversible as possible. ❑ Paint finishes etc. ❑ Fill later opening to mezzanine rooms if desired. ❑ Some floor modification may be required to achieve a level area, depending on future use. ❑ Partitioning around bathroom may be modified to suit an adaptive reuse.


33

The basement (main body)	
	
Remove	<ul style="list-style-type: none"> <input type="checkbox"/> If desired, removal all modern partitioning to reinstate the larger space. <input type="checkbox"/> All fittings, fixtures etc. may be removed as desired.
Retain	<ul style="list-style-type: none"> <input type="checkbox"/> Structural columns, beams etc. <input type="checkbox"/> Ideally retain the ability to see the underside of the upper floorboards, although subject to use lining the ceilings may be acceptable.
Adapt	<ul style="list-style-type: none"> <input type="checkbox"/> Front wall apertures offer scope for adaptation to suit internal layout. <input type="checkbox"/> Flooring may be modified and possibly excavated to provide more headspace if needed (particularly in the void). <input type="checkbox"/> Void area may offer potential for additional floorspace. <input type="checkbox"/> Stair/lift access to the upper floor(s) is likely to be desirable. Whilst this may result in the loss of some significant/original fabric, it is likely that an acceptable balance can be achieved if it reinvigorates use of this space and/or overall viability of the building.





The basement (residential)	
Remove	<ul style="list-style-type: none">❑ All fittings, fixtures etc. may be removed as desired.❑ Wall linings and joinery.
Retain	
Adapt	<ul style="list-style-type: none">❑ Generally adapt as desired within existing spaces.❑ Front wall apertures offer scope for adaptation to suit internal layout.❑ Possibility of lowering ground level in void area to create additional space.❑ May be an ideal plant room area?

37

Eastern (riverside) elevation	
	
Remove	<ul style="list-style-type: none"> ❑ Ideally remove the clubroom additions to reinstate the original Edwardian front of the building and to provide a clearer differentiation between the drill hall and residence particularly from the Derwent. ❑ Remove the modern balconies if desired (see notes below).
Retain	<ul style="list-style-type: none"> ❑ Original door and window openings and associated original joinery etc. ❑ Original detailing such as weatherboards, fretwork, bargeboards/fascias etc. ❑ Roof form, chimneys etc.
Adapt	<ul style="list-style-type: none"> ❑ Basement level apertures offer scope for modification to suit any rationalised internal layout – but ideally retain original apertures (3 northern windows and double doors with toplight). ❑ If the clubrooms are removed, ideally reinstate the lost front detailing of the residence by reinstatement of two windows into the openings that have been converted to doors. ❑ If the clubrooms are removed, ideally reinstate the front wraparound portion of the residential veranda. ❑ Remove the modern balconies if desired, however interpretation of a balcony (or at least a large stair landing) from the northern end of the drill hall is desirable – this may be modelled on the original, or a contemporary solution (which also allows ongoing use of the original doors). ❑ If the clubrooms are removed, there will be a large area of the drill hall façade that will need to be reinstated. This may be done as a 'restoration' of known earlier detailing, however a contemporary approach may be acceptable (subject to end-use requirements).

Northern (slipways) elevation	
Remove	
Retain	<ul style="list-style-type: none">• <input type="checkbox"/> Generally retain this elevation as existing unless connectivity is required to any future new development to the north (at which time further consideration would be required).
Adapt	

Southern (regatta grounds) elevation	
	
Remove	<ul style="list-style-type: none"> ❑ Modern veranda detailing, lattice etc. if desired. ❑ Clubrooms additions as discussed above.
Retain	<ul style="list-style-type: none"> ❑ Upper floor elevation. ❑ Original door and window openings and associated original joinery etc. ❑ Original detailing such as weatherboards, fretwork, barges/fascias etc. ❑ Veranda form. ❑ Rear two bays of the ground floor elevation ❑ Roof form, chimneys etc
Adapt	<ul style="list-style-type: none"> ❑ Ideally if clubrooms additions are removed, reinstate the original veranda detailing from original elevations. Alternatively, a more contemporary but sympathetic approach to veranda detailing may be acceptable. ❑ Replacement of the steps with new steps or a ramp is likely to be acceptable.

Eastern (railway) elevation	
	
Remove	
Retain	<ul style="list-style-type: none">• <input type="checkbox"/> Generally retain this elevation as existing unless connectivity is required to any future new development to the east (at which time further consideration would be required).• <input type="checkbox"/> Roof form, chimneys etc
Adapt	<ul style="list-style-type: none">- <input type="checkbox"/> If adaptation is necessary for connectivity, this would best be through blank sections of wall or the existing doorways. Retention of the windows is desirable to interpret this as the rear wall of the building.

5. The proposed development

As per the methodology detailed in Section 3, Section 2 has detailed the statutory heritage responsibilities applicable to the site and Section 5 has detailed the various conservation policies that have been formulated further to the significance assessed in Section 6. The following documentation reflecting the proposed development has been considered in this assessment (provided by John Wardle Architects). The Architects Statement

DA - DRAWING LIST

Sheet No.	Sheet Name	Rev.	Date
DA0000	Cover Sheet	3	27.09.2021
DA0001	Locality Plan - Existing	4	27.09.2021
DA0011	Site Plan - Demolition	5	27.09.2021
DA0012	Site Plan - Proposed	5	27.09.2021
DA0051	Demolition - Ground Floor Plan	5	27.09.2021
DA0052	Demolition - First Floor Plan	5	27.09.2021
DA0053	Demolition - Second Floor Plan	5	27.09.2021
DA0054	Demolition - Roof Plan	5	27.09.2021
DA0055	Demolition - Elevations	5	27.09.2021
DA0056	Demolition - Elevations	5	27.09.2021
DA0057	Demolition - Sections	5	27.09.2021
DA0101	Proposed - Ground Floor Plan	6	27.09.2021
DA0102	Proposed - First Floor Plan	6	27.09.2021
DA0103	Proposed - Second Floor Plan	6	27.09.2021
DA0104	Proposed - Roof Plan	5	27.09.2021
DA0201	Proposed - Drill Hall Elevations	5	27.09.2021
DA0202	Proposed - Production Hall Elevations	5	27.09.2021
DA0203	Proposed - Cross Elevations	5	27.09.2021
DA0301	Proposed - Drill Hall Sections	5	27.09.2021
DA0302	Proposed - Cross Sections	5	27.09.2021
DA0501	Artist Impression - Exterior View 01, Site Arrival	3	27.09.2021
DA0502	Artist Impression - Exterior View 02, Main Entry	3	27.09.2021
DA0503	Artist Impression - Interior View 03, Drill Hall facing West	3	27.09.2021
DA0504	Artist Impression - Interior View 04, Drill Hall facing East	3	27.09.2021
DA0505	Artist Impression - Interior View 05, Drill Hall facing South	3	27.09.2021
DA0601	Visual Impact Study - View from Cycle Path	4	27.09.2021
DA0602	Visual Impact Study - View from Rail Corridor	4	27.09.2021
DA0603	Visual Impact Study - View from Eastern Entry Road	3	27.09.2021
DA0604	Visual Impact Study - View from River Derwent	3	27.09.2021
DA0605	Visual Impact Study - View from Cenotaph	1	27.09.2021
DA0701	Proposed Palette of Materials and Finishes	2	27.09.2021
DA0801	Shadow Studies	4	27.09.2021

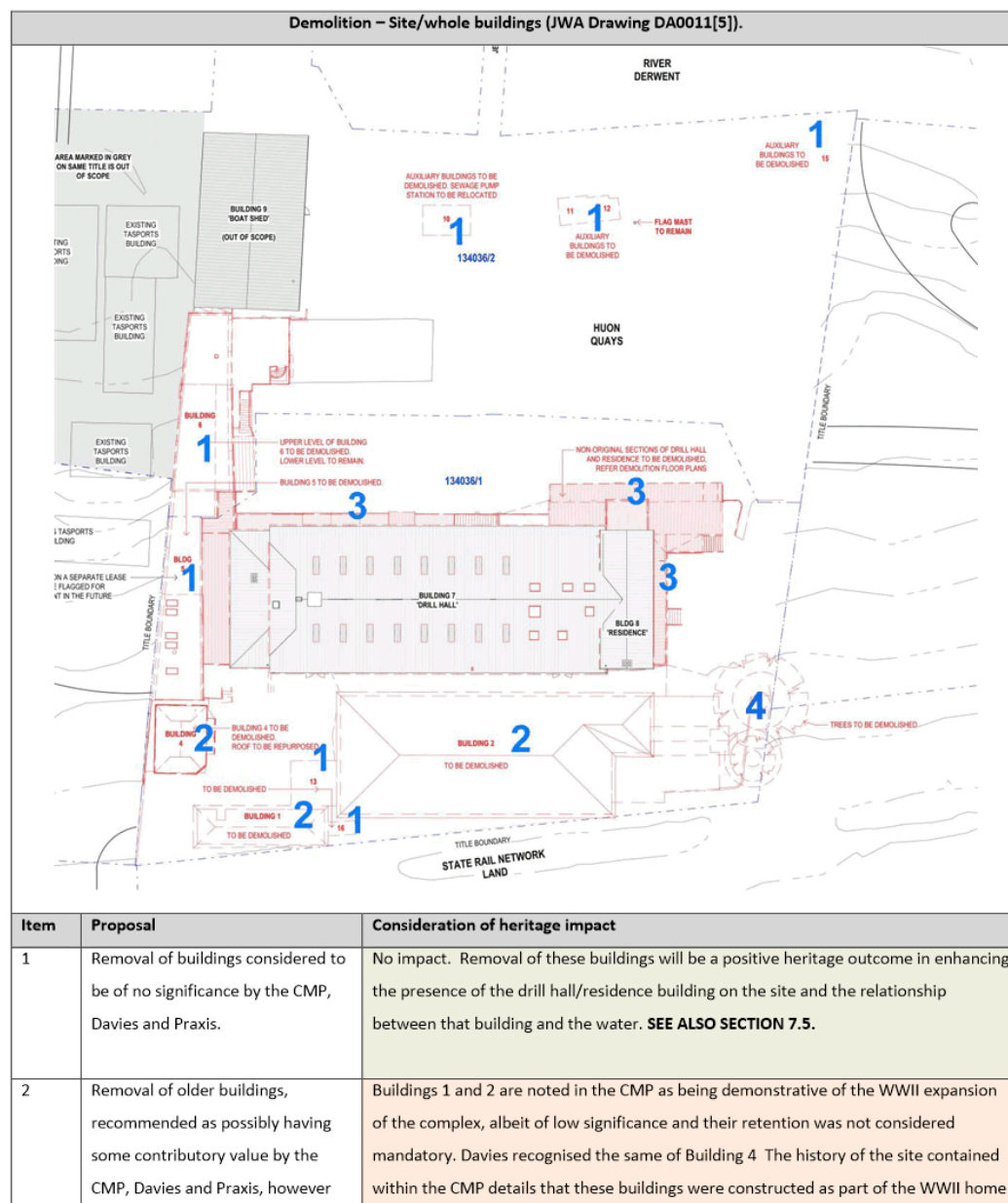
6. Commentary on heritage impact and compliance with conservation policies.

The following discussion analyses the proposed development along a number of lines, including demolition, development, viewscape/setting etc. The discussion against specific parts of the proposal are then considered against the various policies and recommendations of the documents cited in Section 3, mostly the CMP. Section 7 will then condense that assessment to specifically address the statutory heritage requirements as introduced in Section 2.

The following colour coding has been used as a 'quick' means of understanding the heritage impact:

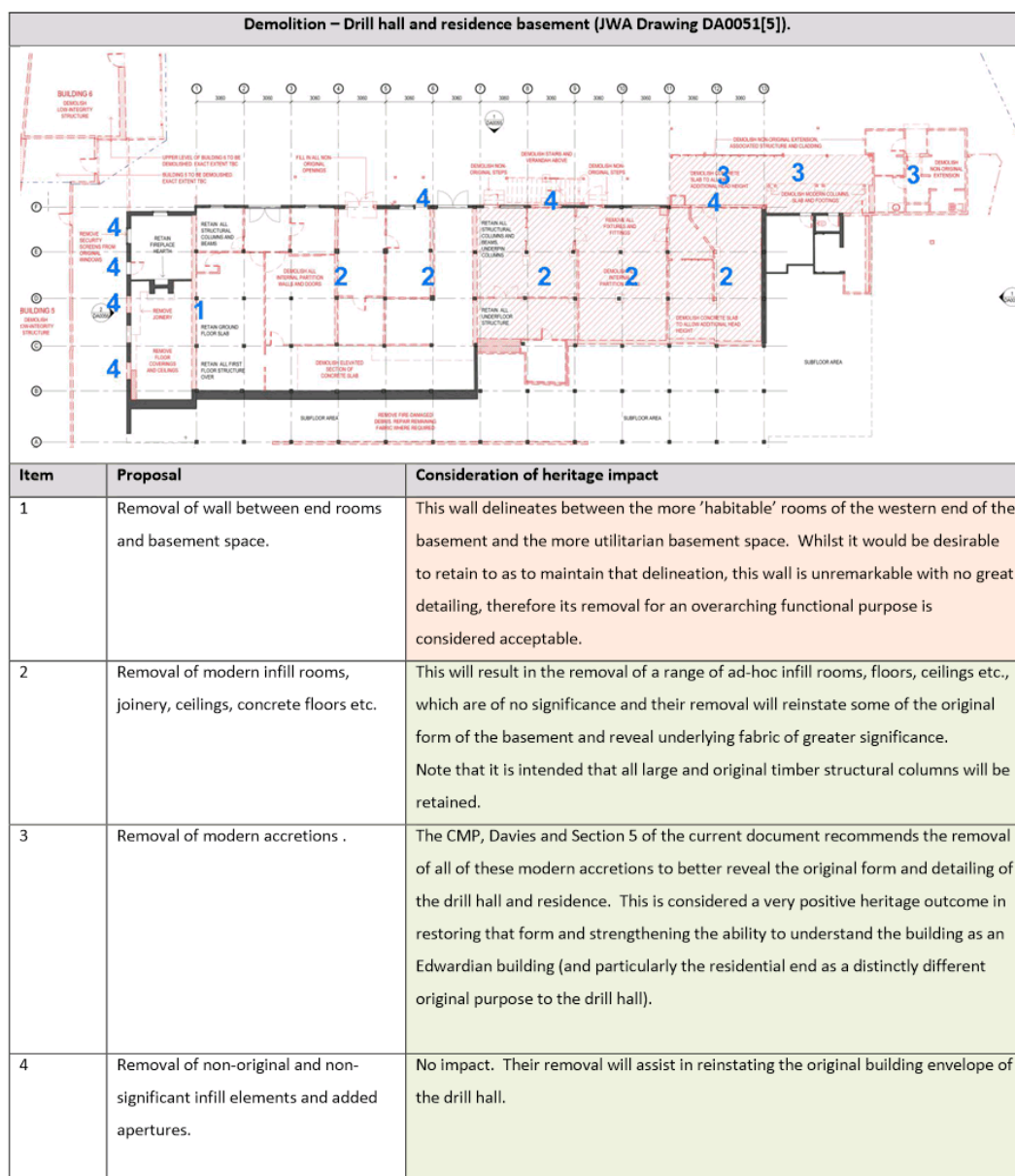
Colour	Impact
	No heritage impact and complies with conservation policies.
	Minor heritage impact which may not wholly comply with conservation policies, that can be justified within the wider context of adaptive reuse of the building, meeting necessary standards or where there is no prudent/feasible alternative to that impact.
	An unacceptable heritage impact.

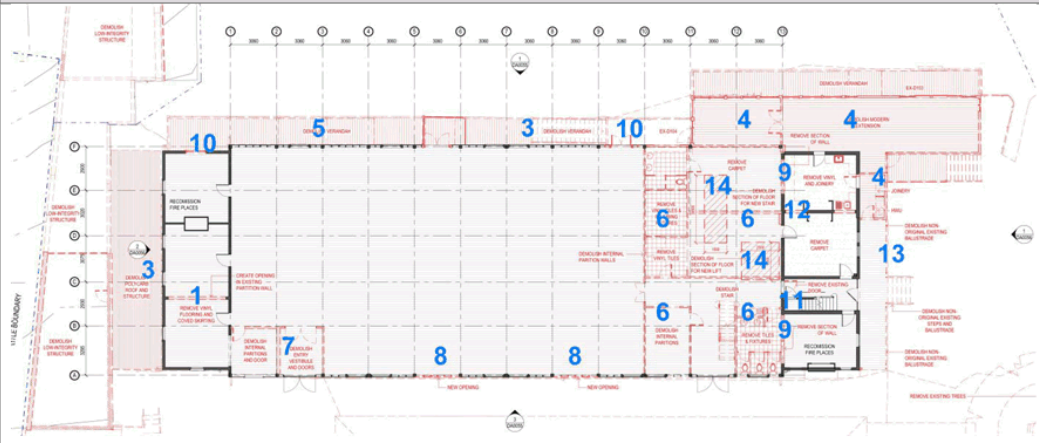
6.1. Demolition



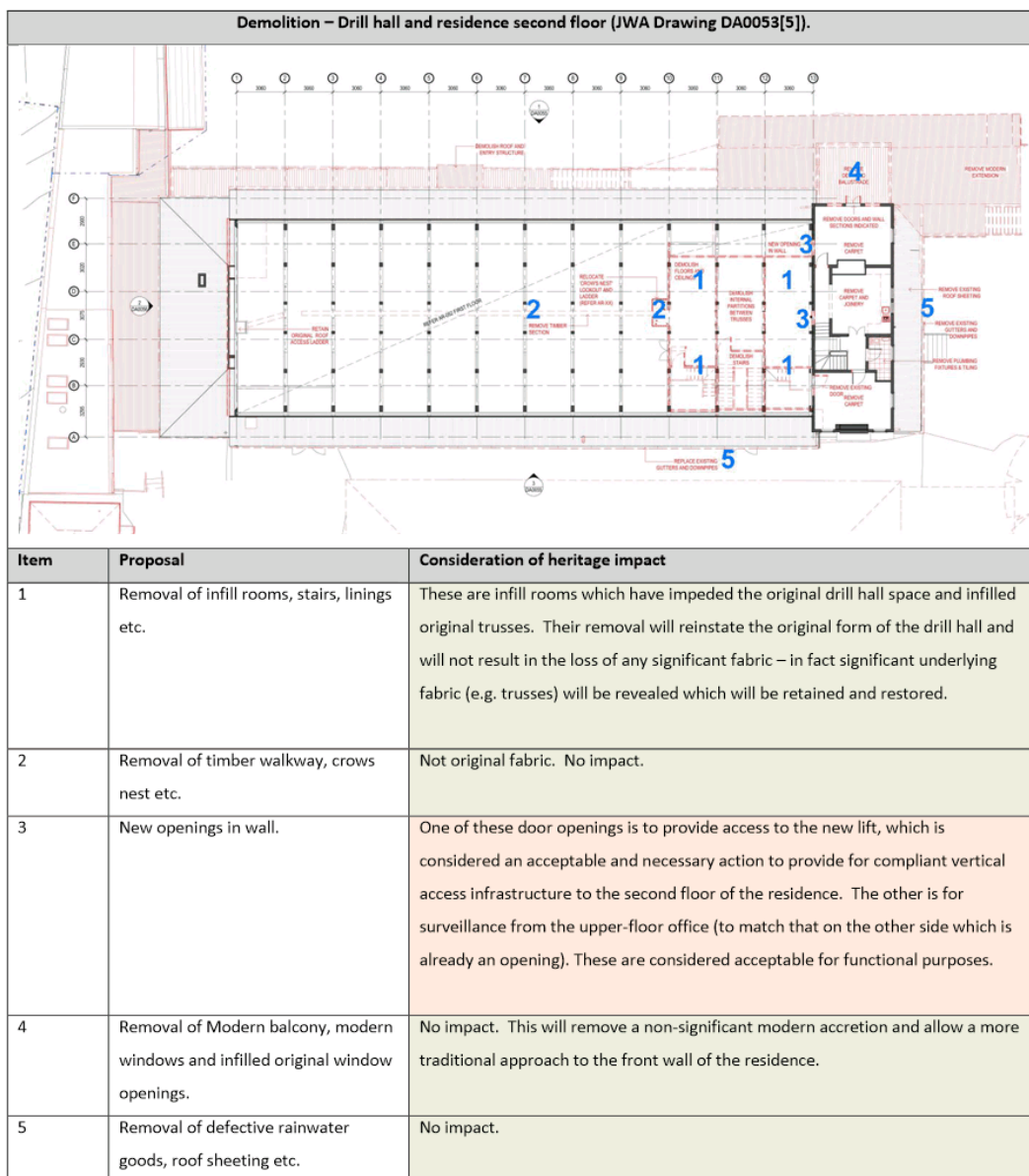
<p>were not recommended for mandatory retention.</p>	<p>effort to expand the HMAS Huon facility – building 2 being built as a mess room and sleeping quarters in 1940 and converted to classrooms and a medical officer’s residence in 1954 – the interior of the building was largely modified at that time and has been substantially modified since. Building 1 appears to be contemporary with that building (and appears on a 1940 site plan – see p22 CMP). Building 4 appears to be a highly modified building possibly contemporary with the drill hall – note that it is proposed to reuse parts of this building elsewhere on the site which is a good heritage outcome.</p> <p>Whilst the CMP proposes that the retention of these buildings is not mandatory, it sets a range of conservation policies/recommendations for the treatment of these buildings should they be retained. This need not be interpreted as mandatory retention (given the overarching option of demolition) but is concerned more with ensuring that whatever modifications to, or replacement of, these buildings is sympathetic to buildings 7/8 and does not impede the setting, curtilage of views/capes to/from those buildings (further discussed below).</p> <p>The Burra Charter (Article 15) requires that: <i>The contributions of all aspects of cultural significance of a place should be respected. If a place includes fabric, uses, associations or meanings of different periods, or different aspects of cultural significance, emphasising or interpreting one period or aspect at the expense of another can only be justified when what is left out, removed or diminished is of slight cultural significance and that which is emphasised or interpreted is of much greater cultural significance.</i></p> <p>This therefore requires consideration as to whether Buildings 1/2, built some 27 years later than the original buildings 7/8 might be of some significance in demonstrating the ongoing evolution of the overall place. The CMP has approached this discussion on the scenarios of both demolition and in the event that the buildings are modified, and has concluded (in regard to demolition) that if demolished (as allowable by the CMP), any new (i.e. replacement) development must not impact more crucial buildings (i.e. in this case buildings 7/8). The context of any new development must therefore be considered within the wider context of what is aimed to be achieved by a particular development and more specifically what benefit that may bring to the overall complex and in particular the appropriate adaptive reuse of the more crucial buildings.</p> <p>In this case, the proposed development requires an additional large space in order to facilitate a feasible and large distillery proposal. As per the Architects Design Statement, the installation of distilling operations into buildings 7/8 was considered – however it was concluded that the installation of such ‘intrusive’ manufacturing facilities into that building was likely to have a larger range of adverse heritage impacts than if it were to be installed in another (i.e. less significant) or a new building.</p>
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		<p>Similarly, Building 2 was considered for conversion however simply does not provide the required spatial considerations and would be a very difficult building to achieve compliance within for distilling purposes. The spatial requirements for such an operation were considered too large for any existing building on the site, therefore a new build was considered to be the only viable option. A range of factors influenced the decision to locate such a building at the rear of buildings 7/8 – including the requirement that views to/from that building not be impeded (in particular the relationship between buildings 7/8 and the river and the desire to maintain key viewlines to/from the building (e.g. from the Regatta Grounds). Further, connectivity to building 7/8 was a key driver so as to be able to adaptively reuse those buildings as part of the overall development <u>which is considered by the CMP as a critical part of their ongoing maintenance</u>. In this instance, the placement of a new building at the rear of buildings 7/8 which appropriately connects to those buildings (see discussion below) and which promotes the ongoing use of those buildings (in this case in a more passive manner than the installation of any industrial processes into those buildings) is considered to be an appropriate heritage outcome which overrides the benefits that may arise from the retention of buildings which are not necessarily of the highest heritage value (consistent with the <i>Burra Charter</i> article as cited above). SEE ALSO SECTION 7.5.</p>
3	Removal of modern accretions to the drill hall/residence (i.e. buildings 7/8).	<p>The CMP, Davies and Section 5 of the current document recommends the removal of all of these modern accretions to better reveal the original form and detailing of the drill hall and residence. This is considered a very positive heritage outcome in restoring that form and strengthening the ability to understand the building as an Edwardian building (and particularly the residential end as a distinctly different original purpose to the drill hall).</p>
4	Removal of two trees.	<p>A row of small trees is shown on the 1946 aerial photograph in this location, running from the southern boundary to the foreshore. These trees may be remnants of that row, which has elsewhere been removed and replanted. As a remnant of a former linear planting along a remaining boundary, these trees aren't considered to have a high level of value. Their removal will allow an enhanced view to the residence from the cenotaph area.</p>



Demolition – Drill hall and residence first floor (JWA Drawing DA0052[5]).		
		
Item	Proposal	Consideration of heritage impact
1	Removal of wall between two end rooms.	Ideally this wall would be retained to delineate these three smaller-scale rooms on the western end of the building, however for functional reasons this removal is desired. This will retain the general feel of these smaller rooms with a bulkhead and ribs retained to delineate the line of wall. This is considered acceptable.
3	Removal of modern accretions .	The CMP, Davies and Section 5 of the current document recommends the removal of all of these modern accretions to better reveal the original form and detailing of the drill hall and residence. This is considered a very positive heritage outcome in restoring that form and strengthening the ability to understand the building as an Edwardian building (and particularly the residential end as a distinctly different original purpose to the drill hall).
4	Demolition of bar and restaurant additions.	
5	Demolition of balcony.	
6	Demolition of dining area infill, modern toilets, hallway, stairs and partitions.	These are infill rooms which have impeded the original drill hall floorspace. Their removal will reinstate the original form of the drill hall and will not result in the loss of any significant fabric – in fact significant underlying fabric (e.g. flooring) will be revealed which will be retained and restored.
7	Removal of room, non original doors and vestibule.	

8	Removal of sections of blank wall to provide connectivity to new development at rear.	Whilst this will result in the loss of original fabric, it is essential that the drill hall have adequate connectivity to the proposed new development at rear. A number of options were discussed between the heritage consultant and design team, including removal of windows to provide that connectivity. It was concluded that the loss of blank wall was more favorable than the loss of windows and that the retention of the windows also retained the ability to discern this as an external wall. On balance to respond to the practical needs of connectivity this action is considered acceptable.
9	Opening of walls to form 'operable walls'.	Whilst Section 5.1 suggests that the drill hall and residence need to remain distinctly separate spaces, with the maintenance of the more intimate rooms of the residence, it is desired to provide connection between these two areas to stimulate use of those smaller spaces. The operable walls are considered an acceptable outcome, as they will retain most of the original fabric, and allow those rooms to be closed off again to retain their domestic feel.
10	Removal of non-original doors/windows.	No impact.
11	Removal of non-original door.	No impact.
12	Removal of a small section of wall on the western side the fireplace in residence	Ideally this wall should be retained to delineate the smaller-scale rooms of the residence, however for functional reasons this removal is desired. Note that the opening to the east will reinstate a former opening hence reinstating some of the original circulation pattern of the building. This will retain the general feel of these smaller rooms with a bulkhead and nibs retained to delineate the line of wall. This is considered acceptable.
13	Removal of modern veranda detailing, modern steps and balustrade.	No impact. This is being removed in favour of reinstating more traditional veranda detailing.
14	Removal of sections of floorboards and framing for lift and stairs.	This is considered acceptable as a means of providing compliant vertical circulation infrastructure between all floors which will enhance the use of the building.







		wall. On balance to respond to the practical needs of connectivity this action is considered acceptable.
6	Re-open earlier blocked window.	A positive outcome in reinstating an earlier aperture.

Demolition – Drill hall and residence eastern and western elevations (JWA Drawing DA0056[5]).		
Item	Proposal	Consideration of heritage impact
1	Removal of non-original and non-significant added apertures.	No impact. Their removal will assist in reinstating the original building envelope of the drill hall.
2	Removal of modern accretions.	The CMP, Davies and Section 5 of the current document recommends the removal of all of these modern accretions to better reveal the original form and detailing of the drill hall and residence. This is considered a very positive heritage outcome in restoring that form and strengthening the ability to understand the building as an Edwardian building (and particularly the residential end as a distinctly different original purpose to the drill hall).
3	Removal of modern veranda detailing, modern steps and balustrade.	No impact. This is being removed in favour of reinstating more traditional veranda detailing.

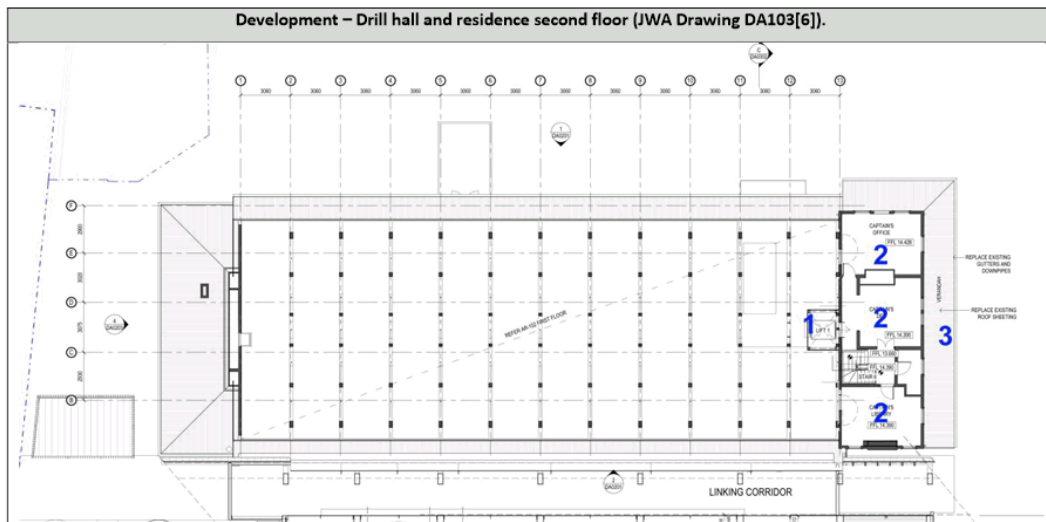
4	Removal of two trees.	A row of small trees is shown on the 1946 aerial photograph in this location, running from the southern boundary to the foreshore. These trees may be remnants of that row, which has elsewhere been removed and replanted. As a remnant of a former linear planting along a remaining boundary, these trees aren't considered to have a high level of value. Their removal will allow an enhanced view to the residence from the cenotaph area.
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6.2. Development of the former drill hall and residence.

Item	Proposal	Consideration of heritage impact
	<p>Insertion of a series of partition walls within the retained existing timber column system to include public bathrooms, office spaces, staff facilities, meeting rooms etc. A new stair will be installed to the first floor via a new accessible entry on the eastern end of the drill hall basement which also serves as a lift. The fenestration of the northern elevation will be restored to a similar arrangement as original with non-original apertures infilled.</p>	<p>No impact. The conversion involves the removal of a substantial amount of ad-hoc infill and provides a better resolved arrangement of functional spaces. The new entrance lobby will provide compliant and logical access via stairs and a lift to all parts of the building which has never been achieved in this building. This is considered an excellent use of space in locating higher-impact (e.g. toilets) and necessarily smaller rooms in an area which has less sensitive fabric and where partitioning will not impede understanding of the building (i.e. rather than partitioning the drill hall itself). Overall this is considered an excellent heritage outcome.</p>

Development – Drill hall and residence first floor (JWA Drawing DA102[6]).		
Item	Proposal	Consideration of heritage impact
Note that the new building and associated exterior elements will be discussed below.		
1	Fitout of western rooms for small-scale bottling and tasting.	This is an appropriate use which retains the smaller and more intimate spaces of these rooms as a contrast to the larger drill hall.
2	Fitout of the curtain room.	The use of a curtain in lieu of walls is a suitable approach to retaining the larger space of the drill hall but providing the ability to create a smaller space with an ephemeral addition.
3	Bar area.	The open-plan bar area maintains the tenor of the large drill hall. Whilst minor impacts may arise from plumbing penetrations, waterproofing etc, if these are done in as unobtrusive manner as possible then no this impact will be minor and acceptable.
4	Tasting deck.	This deck will effectively interpret the original lookout balcony of this area in a contemporary way.
5	Portal window.	This window will be a clearly modern element which although is not consistent with the original fenestration of the building, occupies a space currently occupied by a larger and unsympathetic extension and glazing. This will provide a better resolved element that will also act as part of the entrance statement (discussed below).

6	Reuse of removed wall panels for operable walls.	These panels will allow the smaller rooms of the former residence to be re-closed from the larger drill hall space which will maintain the contrast of their original use.
7	Stairs and lift	The installation of clearly modern stairs and lift with minimal removal of significant fabric required is considered to be a positive outcome in providing unprecedented universal access to all parts of the building.
8	Infill non-original opening.	A positive outcome in reinstating lost form of the end room.
9	Fitout of former residential rooms for retail spaces and waiting room.	This is an appropriate use which retains the smaller and more intimate spaces of these rooms as a contrast to the larger drill hall. The significant stairs and joinery will be retained.
10	Reinstatement of a more traditional veranda configuration.	The ad-hoc additions and unsympathetic repairs to the veranda will be removed and missing elements reinstated to provide a more traditional appearance to the veranda.

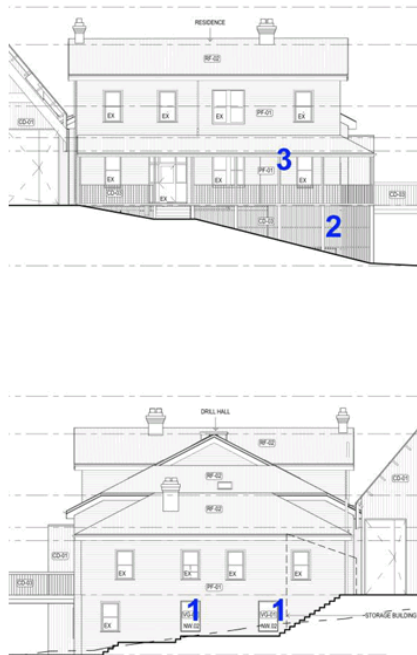


Item	Proposal	Consideration of heritage impact
Note that the new building and associated exterior elements will be discussed below.		
1	Lift	The installation of a clearly modern lift with minimal removal of significant fabric required is considered to be a positive outcome in providing unprecedented universal access to all parts of the building.
2	Fitout of former residential rooms for office and library.	This is an appropriate use which retains the smaller and more intimate spaces of these rooms as a contrast to the larger drill hall. The significant stairs and joinery will be retained. Note that there will need to be some modification of the top stairs to make way for the lift landing. This will involve encapsulation of the steps rather than demolition or removal.
3	Repair and reinstatement of original veranda form and replacement of roof sheeting and rainwater goods.	The ad-hoc additions and unsympathetic repairs to the veranda will be removed and missing elements reinstated to provide a more traditional appearance to the veranda.

Development – Drill hall and residence northern and southern elevations (JWA Drawing DA201[5]).		
Item	Proposal	Consideration of heritage impact
Note that the new building and associated exterior elements will be discussed below.		
1	Tasting deck.	This deck will effectively interpret the original lookout balcony of this area in a contemporary way.
2	New windows	These windows will be reinstated as per the original elevations in order to reinstate the original fenestrative pattern of the building. This provides a considerable heritage benefit in removing ad-hoc fenestration and restoring the original façade.
3	New operable skylights	These will replace the current non-original skylights and be operable for ventilation functionality.
4	Reinstatement of previously removed front wall.	This wall has been removed to make way for the dining room and bar extension. It will be replaced on the original line using traditional materials to reinstate that area of the façade following the removal of an intrusive element.
5	New infill portal window and entrance.	This entrance and window will be a clearly modern element which although is not consistent with the original fenestration of the building, occupies a space currently occupied by a larger and unsympathetic extension and glazing. This will provide a better resolved element that will also act as part of the entrance statement (discussed below).

6	Slat infill to ground floor under veranda	This provides a more traditional approach to the undercroft area following the removal of non-significant extensions.
7	Reinstatement of a more traditional veranda configuration.	The ad-hoc additions and unsympathetic repairs to the veranda will be removed and missing elements reinstated to provide a more traditional appearance to the veranda.

Development – Drill hall and residence eastern and western elevations (JWA Drawing DA203[5]).



Item	Proposal	Consideration of heritage impact
Note that the new building and associated exterior elements will be discussed below.		
1	New windows	These windows will be installed in locations that currently have added apertures (i.e. not original). These new windows will be better proportioned to the general tenor of windows and offer a better heritage outcome than existing.
2	Slat infill to ground floor under veranda	This provides a more traditional approach to the undercroft area following the removal of non-significant extensions.
3	Reinstatement of a more traditional veranda configuration.	The ad-hoc additions and unsympathetic repairs to the veranda will be removed and missing elements reinstated to provide a more traditional appearance to the veranda.

6.3. The new production hall in relation to the former drill hall and residence

View from the water and north-east (JWA Drawings DA501[3] and DA0604[3]).

**Discussion**

The proposed production hall is higher than the drill hall, and occupies a wider frontage than that building, however it has been designed to read as a 'shadow' building which provides a discreet backdropping element, with only a small portion of blank roof visible above the drill hall roof, which is only visible from more distant vantage points where its presence is further diminished (the sloping topography of nearer environs give the drill hall greater presence which would block all views to the new roof). The eastern end of the production hall which projects further than the residence has been designed to read as a much more ephemeral 'shed', with a splayed end, glazing and exposed framing to reduce what could be a solid appearance and materiality designed to read as subservient to the more robust drill hall and residence. The materiality of this building, with exposed timber framing is intended to tie in with the proposed slatted infill

underneath the veranda to provide some materiality cohesion between these buildings. The distorted gable roofline is complimentary to the existing (and varied) roof forms of site, but does not seek to replicate any of the heritage structures.

As stated in the architect's design statement:

The form of the distillery building envelope has been dictated by the equipment and production requirements as well as the shape of the site. We have tried to be mindful in keeping the building as low and small as possible, whilst also not creating any workflow or efficiency barriers for production processes. We have located the stills (the tallest pieces of equipment) underneath the roof ridge and have set the roof height accordingly with minimum clearance requirements. We have worked closely with the process engineer to bring these heights down as much as possible.

From these vantage points, is concluded that the overall mass and form of the proposed production hall allows the (restored) drill hall and residence to remain as the dominant site feature, assisted particularly through the removal of other non-significant site features and vegetation and that the new building would be a suitably discrete backdropping element.

View from south-east and approach from the Regatta Grounds (JWA Drawings DA502[3] and DA603[3]).

**Discussion**

A key to the success of the proposed rear building is to provide functional attachment to the rear wall of the drill hall, but to retain the independence of the drill hall as an apparent standalone building – this was a key part of the initial heritage advice provided to the design team. The key driver for this building form is to promote it as a ‘back shed’ which is subservient in form and detailing to the more articulated existing buildings with a lightweight and transparent connection to maintain some apparent independence of those buildings. It was not desirable to allow the new building to read as an extension to the drill hall – but more as a ‘shadowing’ building designed to sit behind – and although larger, for it to be recessive with the drill hall remaining the dominant building when viewed from a range of vantage points and particularly from the water, Regatta Grounds approach and the Tasman Highway approach (from the bridge).



From these vantage points, there is a clear and distinct break between the buildings, with light attachment achieved through glazing, however the space between reads as a laneway (similar to the existing conditions). Again, the materiality of the building is intended to interpret the traditional timber and galvanised iron palette of the drill hall, but to use this in a more contemporary way to avoid mimicry. From the more distant approaches from the Regatta Grounds the existing rear wall of the drill hall remains as a distinct element which separates the old and new.

View from south and approach from McVilly Drive (JWA Drawings DA604[4] and DA605[1]).



Discussion

The proposed production hall substantially blocks views to the rear (southern) and western walls of the drill hall from these vantage points. This viewline however is not considered as critical as others discussed here as these walls of the building are less articulated than others and there is the precedent of other buildings having obscured these views for the last 80 years. This view has also been previously inhibited by the higher topography of this area further accentuated by the build-up of the railway line.

View from south east approach from the Regatta Grounds carpark (JWA Drawing DA602[4]).	
	
<p>Discussion</p> <p>The proposed production hall, although higher, would have a similar presence in relation to the drill hall/residence and the overall landscape from this vantage point – given that the current Building 2 is a prominent element in that viewline, as well as the vegetation proposed for removal. The better-resolved ‘neighbour’ provided by the proposed building will enhance this viewline and the removal of other clutter (i.e. non-significant buildings, landscape elements and additions) will enhance the dominance of the residence from this vantage point.</p>	

6.4. Compliance with the general policies of the CMP

Note that the colour coding introduced in Section 6.1 applies here –

Colour	Compliance with policy
	Fully complies.
	Largely complies
	Does not comply

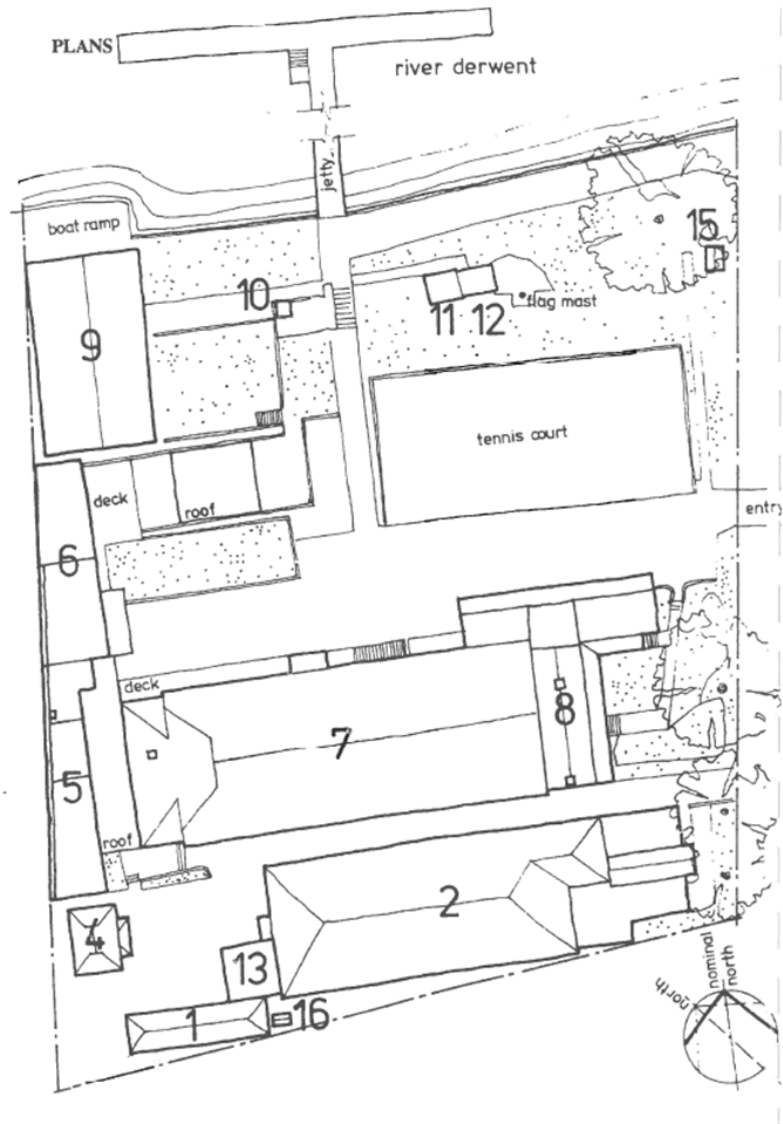
Policy #	Policy	Commentary on proposed concept
5.3.1	<i>It is recommended that the HMAS Huon be conserved to protect and enhance its cultural heritage significance, with controlled adaptation to encourage its occupation and viability and to make possible the maintenance of its significant fabric.</i>	The proposal intends to retain the key identified heritage buildings and to provide a vibrant new use which will be largely publicly accessible and to assure their ongoing viable maintenance, use and development.
5.3.2	<i>It is recommended that the naval associations and orientation of the HMAS Huon to the River Derwent be preserved and if possible enhanced.</i>	Whilst the CMP has no control/influence over tenancies particularly given the 1995 divestment of the site, a key driver of the proposal is to maintain the connection of the drill hall/residence to the river and to maintain key viewfields to and from the building. The maintenance/enlivenment of the area between the building and the river and enhanced public use and access will be beneficial to understanding and appreciating that connection.
5.3.3	<i>It is recommended that the 'crucial' buildings of HMAS Huon (as identified in 4.3) are conserved and adapted with special attention to:</i> <ul style="list-style-type: none"> -□ <i>The restoration/reconstruction of buildings 7/8 to its 1912 extent allowing for adaptation for compatible uses.</i> -□ <i>The maintenance of building 9 to its present form or its replacement with a building of identical scale and form.</i> -□ <i>The restoration/reconstruction of the exterior of building 4 to an appropriate, or at least more sympathetic, earlier state.</i> 	<p>The 'crucial' buildings (i.e. 7-8) will be conserved and adapted (subject to further and finer detail heritage planning).</p> <p>Building 9 is outside the current subject site.</p> <p>Building 4 will be partially retained on the site – with substantial elements of this building reincorporated into a new structure nearer to the waterfront.</p>
5.3.4	<i>It is recommended that the 'contributory' buildings of HMAS Huon (as identified in 4.3) are encouraged to be maintained and adapted, although the 'crucial' buildings should be given priority for conservation.</i>	'Contributory' buildings (i.e. 1, 2) are to be demolished. The CMP recommends that these be recorded (except 2 which is well documented already). The demolition of buildings 1 and 2 is essential to facilitate the viable redevelopment of the site which is

		the essential key driver to the whole scheme – without which the future of the entire site is less certain and leaves the ‘crucial’ buildings in a vulnerable state.
5.3.5	<i>It is recommended that any new development required to make the property viable be respect [sic.] the significance, scale and existing general character of HMAS Huon.</i>	The proposed development whilst of a similar (slightly smaller) scale to the key building 7/8 maintains its subservience as a ‘rear’; building lightly attached to that building via a highly transparent linkage. That proposed building will not dominate the existing building and maintain the legibility of the heritage building as a standalone structure. The character of the existing building will be retained as will its relationship to the river with all major development proposed to be at the rear of the building. It is considered that the proposal is a scheme which is both viable and does not ‘over-develop’ the site in terms of impacting the setting, curtilage and views to/from the HMAS Huon building.
5.3.6	<p><i>It is recommended that the Commonwealth ensure statutory protection of HMAS Huon as identified in this document by appropriate state and/or local government level prior to its disposal.</i></p> <p><i>It is further recommended that HMAS Huon be nominated by the Department of Defence for inclusion of the Register of the National Estate based upon this document prior to its disposal.</i></p> <p><i>It is further recommended that all future owners agree as a condition of disposal not to oppose the inclusion of the HMAS Huon property on any heritage registers or lists.</i></p>	<p>As per Section 2, the place is subject to a range of statutory heritage listings which are considered adequate to scrutinise any possible development and to manage the heritage values of the place.</p> <p>The Register of the National Estate is now defunct, however the Tasmanian Heritage Register (via the <i>Historic Cultural Heritage Act 1995</i>) has been enacted since the CMP was written which is considered to give adequate protection to the place.</p>
	<i>Building 7/8 should remain the dominant building on the site, particularly as seen from the river.</i>	The major portion of the proposed development is at the rear of buildings 7/8 and therefore wholly maintains its legibility from the river. The massing and peak-roof height of the proposed building has been designed to be subservient and merely a backdropping element when viewed from the river.
	<i>A jetty should be maintained in its present location although the replacement of alteration/extension/reduction of the existing jetty should be permitted. Any additional structures or enclosures or enclosures associated with the jetty should be minimal in number, extent and visual treatment.</i>	The proposal does not include any changes to the jetty.
	<p><i>None of the existing landscaping – lawns, flower beds, shrubs, paving – need be retained. Any replacement landscaping should:</i></p> <ul style="list-style-type: none"> • <input type="checkbox"/> <i>Retain the open space between buildings 7/8 and the river and views to and from that building and the water;</i> 	The proposed landscaping achieves all of these objectives.

	<ul style="list-style-type: none"> • <input type="checkbox"/> Simplify the landscaping to enhance and reinforce buildings 7/8; • <input type="checkbox"/> Seek to create more uniformity in vegetation selection and materials/construction for paving and retaining walls. • <input type="checkbox"/> Rationalise the terracing of the site. 	
	<i>It is encouraged that the four large trees on the eastern boundary of the site be retained and maintained although this is not mandatory.</i>	These trees are intended to be removed, which is not inconsistent with this policy.
	<i>The retention of the tennis court is not mandatory and neither is its removal. It should however not be greatly enlarged or modified. Any structures associated with the courts or any subsequent uses of the area should be minimal in number, extent and visual treatment.</i>	The tennis court has already been converted to parking and will be removed in favour of more sympathetic and functional landscaping (and disabled parking).
	<i>The retention of the existing covered barbeque area and the timber deck off building 6 is not required and in fact should be considered for removal and/or replacement in a more sympathetic and relevant design.</i>	These elements will be removed.
	<i>The modern covering on the added on the west side of building 7 should be removed and not replaced.</i>	The proposal will remove and not replace this.
	<i>Where hard paving is required, bitumen should dominate although concrete and controlled areas of other finishes – such as brick paving – should also be permitted.</i>	Whilst specific ground treatments have not yet been definitively designed, this can be resolved at detail design stage.
	<i>A naval flag mast, as adjacent to buildings 11/12, should be maintained although its location may be changed so long as it remains on the river side of buildings 7/8 and preferably in a prominent location on the site.</i>	The existing flagpole will be retained.

6.5. Compliance with the CMP policies regarding demolition

The concept involves the demolition of several buildings to make way for the proposed development. The following table considers the significance of these buildings in relation to the CMP and makes a conclusion/recommendation within the context of the current concept proposal;



Building	Proposal	Recommendation as per CMP (if retained)	Recommendation as per CMP (if not retained)	Conclusion/recommendation on heritage impact
1	Demolition	<ul style="list-style-type: none"> • Would need to be restored/ reconstructed. • Should not be enlarged • Openings may be retained or altered but if altered should generally reflect a traditional approach to those openings in regards to proportions and materials. • No restrictions on interior that don't impact exterior. • Paint in a scheme that unifies the buildings on the site. • No works are to compete with or detract from buildings 7/8. 	Retention is encouraged but not mandatory. If not retained, should be appropriately documented (except 2 for which sufficient documentation exists).	<p>It is proposed to demolish this building to provide access and curtilage to the new building.</p> <p>The demolition of this building is not inconsistent with the recommendations of the CMP, even though it may have some contributory value.</p>
2	Demolition	<ul style="list-style-type: none"> • It need not be restored/reconstructed unless desired. • Should not be enlarged and its external form and cladding should be retained. • Openings may be retained or altered, but if altered should generally reflect a traditional approach to openings in regards to proportions and materials. • More recent additions to the east may be removed or modified within their existing plan envelope. Roof forms may be modified but should remain simple and in keeping with the main form of the building. • Small porch may be modified to be more traditional. No restrictions on interior if these don't impact • Exterior, although v-groove walling and fibrous plaster should be retained and expressed. • No works are to compete with or detract from buildings 7/8. 		<p>The proposal seeks to demolish this building and provide a new and larger building in this area. Whilst the retention of the building is encouraged by the CMP, it is not considered mandatory therefore demolition may be justified.</p> <p>The proposal seeks to build a large purpose-built distillery on this part of the site, which would be linked to building 7/8 via a wide glazed linkway with building 7/8 being used as more 'passive' spaces such as museum, offices, barrel store etc. (see below). The main production facility (e.g. wash, distilling, filtering, etc.) would occur in the new building, allowing for a more passive use of the space in the heritage building – which is considered to be a good principle in terms of heritage outcomes.</p> <p>The architects design statement states: .</p> <p><i>Converting this building into a distillery is neither possible nor desirable for several reasons. The elevation above ground level poses challenges for the delivery of goods. The distillery equipment required to make the building fit for purpose, would not fit within the envelope of the existing building. In order to manufacture hazardous goods within this building, the fabric would need to be significantly altered to meet regulatory requirements such as Australian Standard AS1940. This area of the site is also the only feasible area to construct</i></p>

				<p>a building large enough to house the functional requirements of a distillery on the site without disrupting view lines from the Drill Hall to the River Derwent. We also note that building 2 is in poor condition, with leaks and damage throughout that would require extensive repair if it were to be repurposed</p>
4	Relocation.	<ul style="list-style-type: none"> • It need not be restored/reconstructed unless desired. • Should not be enlarged. External cladding to remain. • Openings may be retained or altered, but if altered should generally reflect a traditional approach to openings in regards to proportions and materials. • Retain distinctive gable roof and vents. • Covered entry on east side may be removed or adapted. • Retain four panel doors on east elevation. • No restrictions on interior that don't impact exterior. • No works are to compete with or detract from buildings 7/8. 		<p>The proposal will relocate substantial elements of this building to elsewhere on the site.</p>
5	Demolition.	<p>Retention of the southern portion not required.</p> <p>Retention of the northern portion encouraged but not mandatory.</p> <ul style="list-style-type: none"> • <input type="checkbox"/> It need not be restored/reconstructed unless desired. • <input type="checkbox"/> It should not be enlarged and its external form, openings and treatment should be retained including the large v-groove boarded door. • <input type="checkbox"/> Skillion roof should be retained. • <input type="checkbox"/> Paint may be removed if desired. • <input type="checkbox"/> No restrictions on interior that don't impact exterior. • <input type="checkbox"/> No works are to compete with or detract from buildings 7/8. 	<p>Appropriately documented and archived.</p> <p>The modern roof between this building and building 7 should be removed.</p>	<p>It is proposed to demolish this building to provide access and curtilage to the new building.</p> <p>The demolition of this building is not inconsistent with the recommendations of the CMP, even though it may have some contributory value.</p>
6	Partially demolished.	<p>Northern portion</p> <ul style="list-style-type: none"> • <input type="checkbox"/> Should not be enlarged. 	Northern portion	<p>This building is proposed for partial demolition, retaining the ground floor level for use as a storage area, and the first floor level as a lookout point.</p>

		<ul style="list-style-type: none"> • <input type="checkbox"/> <i>Openings may be altered.</i> • <input type="checkbox"/> <i>Although not mandatory, the skillion roof should be modified to be less dominant from the river.</i> • <input type="checkbox"/> <i>External treatment should match the southern portion.</i> • <input type="checkbox"/> <i>External deck should be removed or modified to be smaller and less 'mock colonial'.</i> • <input type="checkbox"/> <i>No restrictions on interior that don't impact exterior.</i> • <input type="checkbox"/> <i>No works are to compete with or detract from buildings 7/8.</i> <p>Southern portion</p> <ul style="list-style-type: none"> • <input type="checkbox"/> <i>It need not be restored/reconstructed unless desired.</i> • <input type="checkbox"/> <i>It should not be enlarged and its external form, openings and treatment should be retained including the large v-groove boarded door.</i> • <input type="checkbox"/> <i>Although not mandatory, the modification of the roof to its earlier gable form is encouraged.</i> • <input type="checkbox"/> <i>Remove the flat roof porch and replaced with a more traditional form.</i> • <input type="checkbox"/> <i>Paint may be removed if desired.</i> • <input type="checkbox"/> <i>No restrictions on interior that don't impact exterior.</i> • <input type="checkbox"/> <i>Retain fireplaces (need not be operable).</i> • <input type="checkbox"/> <i>No works are to compete with or detract from buildings 7/8.</i> 	<p>No action required as the building as original drawings exist.</p> <p>If replaced with another building, this should only be single storey and with a greater traditional character than the existing.</p> <p>Southern portion Appropriately documented and archived.</p>	
7	Retention and refurbishment		See Section 4.1.	See specific discussion in Sections 7.1 -3 and 7.6 These buildings will be retained, restored and enhanced as the key heritage focal point of the site.

8	for new distillery supporting infrastructure			
9	Retained – outside the subject site.			
10	Demolition. .	The retention of these structures and any proposed works such as painting, should consider their visual impact upon the appreciation of the property.	Appropriately documented.	The removal of these buildings will have no impact.
11				
12				
15				
13	Demolition	This structure should be removed and not replaced.		
16		This structure may be removed or retained, but if retained should not be enlarged.	It would be desirable to transfer the function of these sheds to one or part of the 'crucial' or 'contributory' buildings.	

6.6. Compliance with CMP policies regarding the fabric/form of the drill hall/residence.

Policy statement as per Section 6.2 of the CMP.	Commentary on proposed concept.
<i>The repair of existing fabric is preferred to its replacement.</i>	Section 4 has expanded the broader fabric analysis of the CMP to provide specific guidance early in the design process to understand the sensitivity of building fabric.
<i>Exposed services should be removed and/or rationalised wherever possible and new services concealed either entirely in the residence portion of within the structure of the drill hall portion.</i>	Detail on how this will be achieved will be required to be further articulated in the detail design process to be submitted as part of the building application process.
<i>Reversible works are preferred wherever possible</i>	
<i>A maintenance and inspection program should be developed for the long term care of the building.</i>	This can be formulated in follow-up to the development process.
<i>The existing building envelope of both buildings should be retained including its cladding, original openings, timber detailing, chimneys etc. except as may be noted elsewhere.</i>	The proposal reinstates much of the lost form of the building resulting from later non-significant and intrusive additions.
<i>Any alterations required to the existing fabric should reflect the construction, materials, scale and proportions of the original building.</i>	Where practicable, significant fabric will be repaired like-for-like as necessary, new insertions, additions etc. may be clearly modern and demonstrate the evolution of the building (in a reversible manner where possible). The materials palette 'borrows' materials, scale, proportion (etc.) from the original building but has been careful not to distort the appreciation of the evolution of the building (as consistent with <i>Burra Charter</i> principles).
<i>The balcony/veranda added to the north elevation may be retained, removed and/or altered but should not be increased in size or further enclosed. It would be desirable that they be removed and the</i>	The proposal removes this non-significant balcony and veranda and reinstates an interpretive form of the original lookout balcony.

<i>original staircase be reinstated, but this should not be mandatory.</i>	
<i>The original openings on the building's perimeter wall should be retained and no further openings or modifications permitted. It is desirable to reinstate the double doors on the southeast corner if the opportunity arises. Non-original openings at the lower level of the north elevation may be retained, closed or altered so long as they are treated in a traditional manner in keeping with the original openings of the building.</i>	A key driver of this proposal has been to remove later apertures and to reinstate as much as practicable the original fenestrative pattern of the building. New/contemporary fenestrative forms are only included where larger sections of walls have been previously removed and where there is a practical reason to undertake a more modern approach (i.e the entrance area on the northern elevation). Wall removal on the rear wall for the necessary connectivity will maintain all significant window and door detailing with openings limited to blank walls.
<i>There should be no restrictions on the interior of the lower level, except retaining the original chamfered timber columns, arched fireplace supports, not impacting the drill hall space above, no compromise of structural integrity and no further excavation.</i>	The proposal seeks to invigorate this space as supporting infrastructure for the wider building. It will retain the timber columns, arched fireplace supports and will not compromise the integrity of the drill hall. Very minor impact arises from the insertion of stairs and a lift, which is considered acceptable as a means of providing unprecedented universal access to all parts of the building to stimulate the adaptive reuse.
<i>The existing offices at the western end of the drill hall space should be retained including the existing doors, fireplaces, pressed metal ceilings, painted lining boards and other traditional detailing. The reinstatement of missing details is encouraged but not mandatory. The retention of the internal enclosure around the double doors on the southwest corner of the main room is not required, but neither its removal mandatory. The reinstatement of the one missing room in the northwest corner of the main room should not be</i>	The proposal retains these rooms, although part of one wall will be removed to make a larger room from two smaller rooms. Later non-significant partitioning will be removed and key features such as the tin ceilings and fireplaces will be retained.

<i>required but may be undertaken if the room is necessary for a function.</i>	
<i>The retention of the modern varnished timber lining boards in the drill hall space is not required but neither is their removal. A lining more appropriate to the original would be preferred (e.g. painted horizontal boards).</i>	These boards are to be removed.
<i>The enclosure of rooms and offices at the eastern end of the drill hall space may remain but their removal along with the associated stairs and partitions is encouraged. Further full height enclosures and the enclosure of the roof trusses should not be permitted.</i>	These elements will be removed to reinstate the original form of the drill hall. The currently hidden trusses will be re-exposed.
<i>Only after critical assessment of accommodation requirements should additional partition of the drill hall space be considered and only then if the rooms are only a single storey (<2800mm high).</i>	The schematic proposal indicates that the drill hall area will be used as a museum, sales area and brand experience area with a void maintained above. Whilst not explicitly stating this, it is assumed that a large open space is desired for these functions (excluding the need to maintain the rooms on the western end as per above) therefore the detail design process should further articulate this consistent with this recommendation.
<i>Further works and decoration to the interior of the drill halls space should recognise the original approach to the use of materials, the simplicity of its treatment and the dominance of painted finishes. New works should create a restrained character rather than an inflated or flamboyant appearance.</i>	The proposal seeks to allow the building fabric to 'speak for itself' without unnecessary adornment. The fitout detail will interpret early and local building materials to compliment this approach.
<i>All the remaining traditional fabric such as the pressed metal ceilings, fireplace surrounds, joinery</i>	This detailing will be almost wholly retained – it will only be removed where new openings are required (e.g. between

<i>and staircase should be retained. Where elements are missing it is encouraged, although not mandatory, that they be appropriately reinstated.</i>	the residence and drill hall) and even in that case it may be reused in reinstating lost elements of that room (together with reproduction detailing) or retained in elements such as the operable walls etc.
<i>The scale of the rooms should remain the same but interconnections between rooms may be introduced in an appropriately sympathetic manner.</i>	The proposal achieves this and also utilises a restrained approach to new openings so as to retain the tenor of original room sizes.
<i>It would be desirable to remove the additions to the north and to reinstate the original perimeter of the building. This however is not mandatory. The extent of the present additions should not be increased and the shallow pitched awning over the northernmost deck addition should be removed. As well, the glazed rooms at the lower level under that deck should also be removed.</i>	The proposal will remove a series of ad-hoc, non-significant and intrusive additions to reinstate the original form of the drill hall and residence.
<i>The original timber veranda post decoration should be reinstated to the east elevation.</i>	The schematic montages indicate that these will be retained, and the new entrance situated nearby to reinforce that entrance approach.
<i>Additional accommodation requiring significant changes should either be located within rooms or areas already altered or the added portions of the buildings, or possible in the areas adjoining the two portions of buildings 7-8.</i>	Rooms requiring significant changes (e.g. toilets, kitchen, and indeed the distillery operation itself) are limited to the less-sensitive basement space and to a proposed new building respectively.

6.7. Compliance with the CMP policies regarding setting and curtilage

The CMP sets policy for 'new development' and in particular the impact that such may have upon the demolition of buildings (as discussed in 7.1) as well as the general setting of the significant buildings on the site. The following table considers the proposal in terms of these policies (further to the viewscape analysis above).

Policy (as per Section 6.2.3 of the CMP.)	Commentary on proposal
<i>New building developments should only be considered after it is critically assessed whether the required accommodation can be provided within the 'crucial' or 'associated' buildings having regards for the previous guideline statements.</i>	<p>The proposal clearly articulates that the new building development has been designed to carry the higher-impact processes of the overall distillery operation which takes development pressure and intervention needs off the more crucial heritage building(s). Installation of such infrastructure into the heritage building(s) was considered and dismissed as being too high-impact and restrictive and the proposal as it stands is considered to be a good heritage outcome in using the heritage building(s) for more passive associated uses with improved public access and appreciation.</p> <p>It is considered imperative for the long-term future of the 'crucial' buildings that the overall use of the site be feasible and that adequate facilities are provided for improved and ongoing public use and appreciation of parts of the site. All previous conservation policies and guidelines have been rigorously assessed and utilised to inform the current design process in accordance with ICOMOS Burra Charter and industry conservation planning standards.</p>
<i>New building development should only be sited where 'associated' buildings currently exist so long as the abovementioned accommodation assessment is formally undertaken and the buildings to be removed have been recorded as noted.</i>	<p>The proposed development is situated where buildings of low/no significance are currently situated. This is considered acceptable given the overall heritage benefit of the proposed scheme in support of the 'crucial' heritage building(s) that this demolition is an acceptable outcome. This is consistent with the 'new development areas' of the CMP as further articulated by Davies.</p>
<i>New building development should preferably utilise weatherboard construction with</i>	<p>The proposed building is intended to read as a large yet subservient 'shed' which is consistent with the tenor or maritime-related activities along the Hobart waterfront. The form of the building is simple and traditional and the</p>

<p><i>gable roof forms. Building developments should be single storey, have a traditional form and character and new developments should avoid monolith solutions.</i></p>	<p>cladding, fenestration and other detailing (some yet to be resolved) seeks to provide a subservience to the existing heritage buildings.</p> <p>Overall, the proposed building is considered to provide an overall heritage benefit, in that it:</p> <ul style="list-style-type: none"> -□ Has been designed and sited with the primary objective of retaining/restoring the prominence of the drill hall/residence as the key heritage feature on the site and to maintain an appropriately accessible and functional curtilage to that building. -□ Will provide amenity, functionality and assist in the overall viability of the site which is critical to the maintenance and adaptive reuse of the drill hall/residence. -□ Will 'borrow' forms or materials from traditional forms and materials of the site (and wider Hobart waterfront) and allow an interpretive continuity without literally 'mimicking' heritage and confusing the evolution of the site.
<p><i>New buildings should stop short of literally replicating traditional buildings and only draw upon them in a general sense for inspiration. New buildings should be understated compared with buildings 7/8.</i></p>	
<p><i>New building development should not adversely impact upon the appreciation of the buildings 7/8 nor make them redundant in a functional sense.</i></p>	<p>A key driver of this scheme is to adaptively reuse and invigorate the significant buildings on the site as part of a wider scheme which will invigorate the site and improve public access and appreciation. This clearly improves the function of the significant buildings and installs a vibrant, yet comparatively passive use that requires minimal intervention.</p> <p>The siting of the new development has considered the wider viewfields to and from the heritage building(s) and to maintain their relationship with the foreshore and the legibility of their place in the landscape.</p> <p>Overall it is concluded that this thoughtful and well-planned new development is imperative in stimulating the preservation and use of the site and assuring its viable future.</p>
<p><i>New development should endeavour to maintain the site with its own identity rather than be orientated outside the</i></p>	<p>The siting of the proposed buildings is within the current site boundaries therefore will maintain (and in fact improve) the identity of the key heritage building(s).</p>

<p><i>existing property boundaries. This does not imply that it can not be functionally part of a larger development or that it must be fenced off.</i></p>	<p>Whilst the proposal separates some key heritage buildings (e.g. 9) from the core of the site in terms of use/function, these will remain legible as part of the original precinct and allow their continued use or consideration of future adaptive reuses.</p> <p>The proposal is designed to be permeable to the important public recreation spaces of the Regatta Grounds with possible future linkages to any public development of Macquarie Point and thereby have the considerable advantage of the potential to expand the wider Hobart waterfront area to include this currently underutilised and meaningful heritage space and enhance its identity.</p>
<p><i>Refer to Figure 6.2 for areas of potential development.</i></p>	<p>The proposal is largely within the areas proposed for possible future development by the CMP.</p>

6.8. Compliance with 2016 Paul Davies recommendations

Recommendation	Commentary on proposal
<i>1. Retain building 7/8, undertake conservation work, remove some of the additions and accretions and develop an integrated use for it within the overall development.</i>	The proposal retains and restores buildings 7 and 8 and removes substantial non-significant modern accretions to reinstate the earlier form of the building.
<i>2. Retain a setting between building 7/8 and the waterfront, that is without new built form occupying that area.</i>	The proposal retains the setting of the drill hall and residence in relation to the waterfront with no major development planned in the front area that would disrupt viewlines or provide visual clutter.
<i>3. Undertake development within the areas marked on the attached site plan (page 6) [reproduced below] noting that the proposed area extends across the boundary with the slipway.</i>	The proposed new building is contained wholly within one of the two broad areas proposed by Davies as being possible future development sites.
<i>4. Integrate development with any proposals from City Council for the Regatta Ground so that the whole area is integrated and strengthened by any works that take place.</i>	Whilst the proposal does not explicitly seek to achieve this, the invigoration of the site will certainly open opportunities for collaboration with adjacent site administrators and the proposal will not preclude that happening.
<i>5. Undertake a detailed urban design and heritage assessment to establish guidelines for new development of the site. This should include a new CMP for the combined Slipway Huon site. This plan can set out with more authority the relative heritage values of all the component parts of the site and will allow development areas to be</i>	This recommendation anticipated a possible combined redevelopment of the former HMAS Huon site and the adjacent slipyards (i.e. further to recommendation 3 above). Note that the current proposal does not extend into the slipyards site and that the possibility of future complimentary development is not precluded by the current proposal. The second Davies document (see below) did extend this notion into some broad recommendations.

<i>clearly defined. It would appear that there is considerable development potential available.</i>	
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Building	Name	Review	Commentary on proposal
1	<i>Office (pre-1940)</i>	<i>This structure may have some significance, this will need to be assessed. However it is a small building that if significant may be able to be relocated or adapted.</i>	The proposal seeks to remove this building. Note that the 2020 Praxis review of the significance of the building has not assigned any notable significance to this building. The second Davies report assumes the ability to remove/relocate this building.
2	<i>Mess rooms (1940 & 1954)</i>	<i>Quite low significance.</i>	This building is proposed to be removed.
3 & 14*	<i>Sea scouts</i>	<i>On adjacent site – not assessed</i>	Not applicable – adjacent site.
4	<i>Mess room, lavatories and later office (1925)</i>	<i>May have modest significance.</i>	The proposal seeks to relocate substantial structural elements of this building elsewhere on the site.
5-6	<i>Amenities (c1920, 1960, 1972)</i>	<i>Unlikely to have more than low significance.</i>	The proposal seeks to remove these buildings.
7/8	<i>Drill Hall HMAS Huon/residence 1912 (extended 1928).</i>	<i>This is the core and original building that has triggered the National Heritage listing³. It has State level</i>	The proposal will retain, restore and enhance these buildings as the key heritage feature of the site.

³ Note that Davies erroneously states that the place is on the National Heritage List – it is not on that list, despite the CMP suggesting that the place is of national significance.

		<i>heritage significance and possibly National significance.</i>	
9*	<i>Navy Divers building, former boatshed (1961).</i>	<i>The CMP is not clear on its significance or history, it may have moderate significance but will need to be further investigated.</i>	Not applicable - this building has been excluded from the current development site so its current use can be maintained.
10, 11, 12, 13, 15, 16	<i>Ammunition store, stores, carport, garden sheds.</i>	<i>Not significant.</i>	The proposal seeks to remove these buildings.

The second Davies report considered the broader site character and townscape setting with a series of recommendations:

Recommendation	Commentary on proposal
<i>creating a good setting around the Drill Hall Building that links it (visually and potentially functionally) to the water, the Cenotaph area and the Regatta Grounds</i>	<p>The proposal retains and enhances the relationship of the drill hall to the water in that it removes more recent accretions to the waterfront façade of the building as well as removing redundant and non-significant elements between the building and the waterfront. Further, a more sympathetic landscape treatment will enhance that relationship.</p> <p>The proposal will emphasise the entry to the site from adjacent to the Regatta Grounds and via the Cenotaph area which will enhance that functional linkage.</p>
<i>retaining the dominance of the Drill Hall building as part of the gateway to Hobart on the approach from Tasman Bridge</i>	<p>The proposal will remove less and non-significant elements cluttering the northern and western edges of the site which will enhance the dominance of the building from the approach from the Tasman Bridge. Further, the removal of modern accretions from the waterfront side of the building will enhance the dominance of the original 1912 building form when viewed from the bridge, approaches and water.</p>

<i>the scale and potential massing of new structures with regard to views to the site from key locations and views out from the Drill Hall Building</i>	Davies had made suggestions of possible building envelopes, including footprints, possible size, height etc. based on an analysis of a range of desired viewlines. The proposed new structure is within one of these locations and is generally in accordance with the foreshadowed dimensions discussed by Davies. Whilst the proposal does perpetuate the blocking of views to the rear of the drill hall (precedented by the current building 2, which has blocked that view for some 70 years) the overall views to the drill hall from a 360-degress context will be improved with the removal of other buildings along the western edge of the site (as discussed above).
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7. Assessment against statutory heritage requirements

Schedule 1 – Conservation of Cultural Heritage Values	
Provision	Commentary in relation to the proposed development.
22.4. Conservation of Places of Cultural Significance	
<p>22.4.3. Submission requirements. All applications for ‘building or works’ must satisfy the relevant submission requirements of clause 9.2 of the Scheme. In addition, pursuant to s.54 of the Land Use Planning and Approvals Act 1993, the following information may be required:</p> <ul style="list-style-type: none"> • <input type="checkbox"/> A Conservation Plan as defined in Clause 22.3. • <input type="checkbox"/> Street elevations or ‘true perspectives’ to show the scale and impact of ‘building or works’ on places of cultural significance. 	<p>The proposed development complies with this part. The development application documentation includes a conservation management plan, and the current document has been formulated by a heritage consultant (Brad Williams, Praxis Environment) and has also utilised reports by heritage consultant Paul Davies.</p> <p>Together, these documents are considered as a sufficient basis to understand the history, significance and characteristics of the place upon which a range of policies have been developed and implemented to guide future management and development of the place consistent with <i>Burra Charter</i> and J.S. Kerr principles. The design rationale provided in the development application documentation demonstrates how this process has been implemented in the design process.</p>
<p>22.4.5. ‘Building or works’ on places of cultural significance which cannot satisfy the ‘deemed to comply’ provisions of Clause 22.4.4 may be approved at the discretion of the Planning Authority.</p>	<p>This proposal is not considered to wholly be ‘conservation’ of a place of cultural significance – therefore cannot be treated as a ‘permitted’ development. Whilst the proposal will conserve the drill hall and residence and a range of other site attributes, it does include a new and substantial building, therefore Part 22.4.4 is not invoked (permitted works), and accordingly this application is discretionary.</p>
The following criteria must be taken into consideration in the assessment of all proposals to undertake ‘building or works’ on places of cultural significance:	
<p>‘Building or works’ must complement and contribute to the cultural significance, character and appearance of the place and its setting;</p>	<p>The proposed development complies with this part. See the general discussion and compliance with other parts of the scheme (as well as compliance with other policies, provisions and outcomes as discussed in this assessment – in particular Sections 6.1-6.3).</p>

'Building or works' must be in compliance with the conservation strategy of an approved Conservation Plan, where required and/or provided;	See 22.4.3 above. The proposed development substantially complies with the policies, recommendations and provisions of the documents cited above – see the specific discussion Sections 6.4-6.7.
The location, bulk and appearance of 'building or works' must not adversely affect the heritage values of any place of cultural significance;	The proposed development complies with this part. See discussion in Sections 6.1-6.7.
'Building or works' must not reduce the apparent authenticity of places of cultural significance by mimicking historic forms; 'Building or works' may be recognisable as new but must not be individually prominent;	The proposed development complies with these parts. See discussion in Sections 6.2-6.7.
The painting of previously unpainted surfaces is discouraged.	The proposal does not include painting of any previously unpainted surfaces.

22.2 Objectives of Schedule 1	
To provide the mechanisms to allow the conservation of heritage values	The process by which this application is to be assessed by the planning authority meets this objective. By this application being discretionary, the planning authority can scrutinise the proposal to ensure that it gives due consideration to heritage values.
To provide an incentive for 'building or works' to be carried out in a manner which is compatible with conservation of cultural heritage values.	The proponent and designers have gone to great lengths to both meet obligations arising from statutory heritage requirements as well as going a step further to embrace the heritage values of the site and to capitalise from public engagement in that heritage. The development strives to provide a building and outdoor spaces which respond to the heritage themes of the area which is to be supplemented with interpretation. Heritage is seen to make the development more appealing to the public, strengthen community 'ownership' of the building and is aimed to encourage people to re-engage with this currently rarely accessible building and site.
To ensure that the recognisable historic character of Sullivans Cove is not compromised by new development which overwhelms the places of cultural significance, or, by new development which reduces the apparent authenticity of the historic places by mimicking historic forms.	The proposed development meets this objective. The proposed development has regard to the values of adjacent and nearby (and wider) heritage places and provides a sympathetic response to the heritage themes of the area by incorporating interpretive elements into the design which provide reference to, but not mimic, historic forms and fabric.
To encourage new development to be recognisable as new, but not individually prominent. Such development must reflect a "good neighbour" relationship to places of identified cultural value.	The proposed development meets this objective. As discussed above, the building will be clearly recognisable as new, and its prominence has been minimised as much as practicable by the design responses to the various conservation/urban form policy as detailed above. Insofar as it neighbouring the drill hall/residence building (on the same site) it provides necessary space for the achievement of the overall scheme to utilise the heritage building as the 'flagship' of the development and to allow the installation of higher-impact equipment within the modern building – hence minimising the issues relating to sensitive heritage fabric.

Schedule 7 – Demolition	
28.5 Guidelines for development control	
Any application for demolition:	
(a) Shall be refused if the building is included as a cultural heritage place in Table 1 of the Conservation of Cultural Heritage Values Schedule of this Scheme, unless;	
(i) The demolition is approved as part of a Conservation Plan approved by the Planning Authority or otherwise in its discretion under the Conservation of Cultural Heritage Values Schedule of this Scheme;	The CMP for the site is not formally 'approved' by the Planning Authority. Discretion is used in this instance to consider the application alongside the recommendations of the CMP (and other heritage planning documents as cited in Section 3).
(ii) The building clearly detracts from the cultural values or significance of the place; or	Although buildings proposed to be removed don't necessarily detract from the cultural values or significance of the place, the CMP concludes that they are of no or low heritage significance. These are proposed to be removed in favour restoring the setting and assisting in providing a suitable adaptive reuse for the significant buildings (i.e. 7 and 8).
(iii) There are overriding environmental, economic considerations in terms of the building or practical considerations for its removal, either wholly or in part.	As per the Architects design statement, it is not considered feasible to retain the buildings proposed for removal. They are not fit for purpose in the proposed scheme which seeks to reinvigorate the more important heritage buildings with the support of a new building. The practical consideration of retaining any of the buildings proposed for demolition concludes that their retention is neither necessary nor feasible in the overall scheme – the benefits of which for the adaptive reuse of the more significant buildings are considered to be an overriding priority.
(b) May be refused if in the opinion of the Planning Authority the building contributes to the cultural heritage or urban character of the Activity Area and the building is capable of continued beneficial use.	
As per above, the buildings proposed for demolition do not contribute in any appreciable way to the cultural heritage or urban character of the area. Whilst these may be retained for beneficial use, all are in a degraded condition that would require substantial upgrades to make them fit for purpose for <i>any</i> use. They are not capable of being used for any beneficial use in the proposed scheme and their retention would inhibit the realisation of that scheme which has overriding benefits for the more significant building(s).	

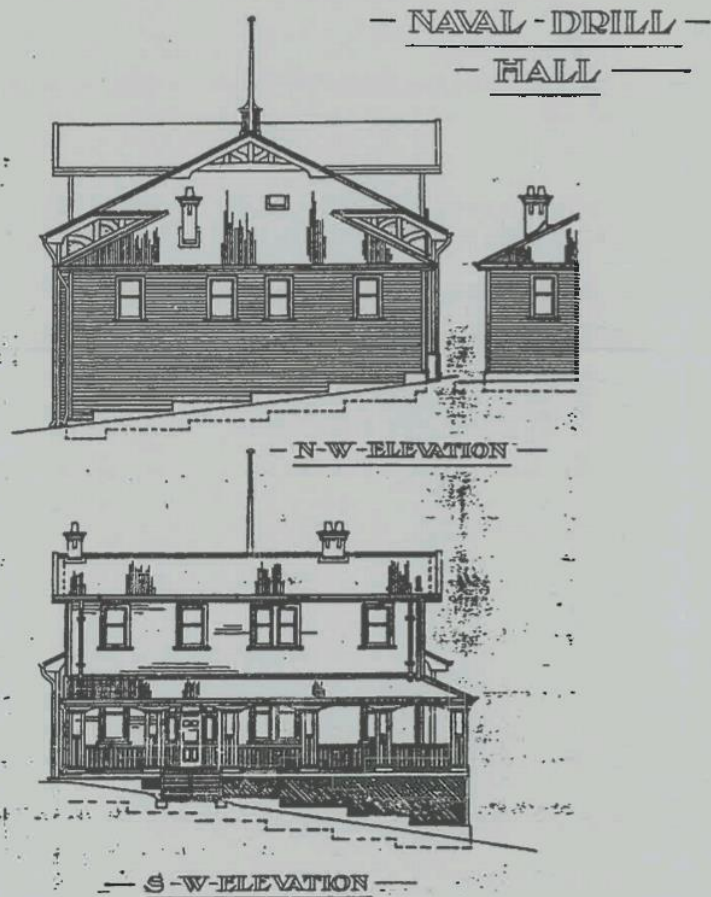
28.8 Demolition of a Place of Cultural Significance	
28.8.1. Where the application involves the demolition of a building on a Place included in Table 1 of Schedule 1 – Conservation of Cultural Heritage Values then the application must satisfy the following:	
The requirement of Clause 22.4.3 for the submission of a Conservation Plan, and	A conservation management plan has been submitted as part of the application. Note that this CMP has been reviewed recently and considered to remain valid.
The provision of street elevations or ‘true perspectives’ to show the scale and impact of the demolition on places of cultural significance and the streetscape.	The application package includes elevations and ‘true perspectives’.
28.8.2 Where the application involves the demolition of any other building then the application must satisfy the following:	
The requirement of Clause 22.5.3 for the submission of a report identifying that the building does not have heritage value, and	Not applicable. Whilst technically the buildings proposed for demolition are on a place listed in Table 1 of Schedule 1 (Conservation of Cultural Heritage Values) these buildings are considered by the CMP to be of no/low heritage value.
The provision of street elevations or ‘true perspectives’ to show the scale and impact of the demolition on places of cultural significance and the streetscape.	
28.8.3 Where the proposed demolition is required by statutory order or is authorised by the Building Surveyor as essential to public safety, and no replacement building is proposed, a plan shall be submitted showing how the site is to be landscaped and fenced for public urban space in a manner consistent with the intent of the Scheme, pending the granting of a permit for a new building.	
Not applicable.	

Planning Principles for Activities in the Cove – 7.3.2 Cultural Resource Principles	
To facilitate use and development which is compatible with conservation of the Cove's cultural heritage values.	The statutory listings to which the place is subject have resulted in a rigorous process of site analysis, understanding values and responding to policy aimed at ensuring that the historic heritage values of the place, vicinity and wider cove have been taken into consideration in the proposal. The process has followed <i>Burra Charter</i> principles, as well as the conservation planning principles of J.S. Kerr to best understand, consider and address historic heritage values in the development process.
To encourage the recycling of existing buildings through the promotion of new uses, particularly in buildings of identified cultural heritage value.	The former HMAS Huon Drill Hall/Residence are buildings of identified cultural heritage value and have been largely unused for over a decade. The buildings have no public access and no use that provides an impetus for maintenance nor promotes sustainability. The current disuse of the building has resulted in evidence of squatters, ingress of vermin and weather which threaten the integrity of the building. The proposal seeks to provide an adaptive reuse into the restored building and to allow ongoing use, maintenance, public access and appreciation of the building to ensure its future.
The recognisable historic character of Sullivans Cove is not to be compromised by new development which overwhelms the historic spaces and buildings, or, by new development which reduces the apparent authenticity of the historic places by mimicking historic forms	The proposed development, although large in scale, has utilised a number of design principles and features (as discussed in this assessment) to minimise the apparent bulk and break the overall form of the development and to retain the prominence of the drill hall and residence from most vantage points. The building would occupy an enlarged, but not dissimilar footprint to the existing 1940s building on that site. Whilst the proposed building would be higher, it takes the theme of a waterfront warehouse which is preceded along the Sullivans Cove waterfront and utilises modern materials and finishes to provide the functional space required to support the adaptive reuse of the drill hall and for the higher impact infrastructure to be enclosed in a new building (thereby reducing stress on heritage fabric).
Changes to urban character will be consistent with conservation of cultural significance and maritime and historic character of Sullivans Cove.	The proposed development will change the urban character of the site and vicinity in a positive manner. It will invigorate a public use for the site and has the potential for future linkages with other important public spaces of the area. The discussion within this assessment concludes that the proposed development will not have an adverse heritage impact upon the historic character of the area, therefore it is considered that the proposed development will reinstate and reinforce the character of the cove.
No new development or part of a development is to be individually prominent particularly when viewed from Sullivans Cove	The proposal has responded to various viewscape analyses which seek to retain the prominence of and relationship between the drill hall to the waterfront and overall enhances that relationship through the removal of non-significant intervening elements. This will enhance the prominence of the drill hall and

or the River Derwent. Exceptions include cranes and similar development relating to the essential operating requirements of the Tasmanian Ports Corporation.	residence from the Tasman Bridge, approach along the Tasman Highway, from within the water and Regatta ground. Whilst the proposed building will backdrop the existing building, it will not be substantially higher and will not provide major visual competition to the prominence of the existing heritage building.
The distinctive urban form of Sullivans Cove is to be reinforced in development areas.	The proposed development makes a positive contribution to the urban form of Sullivans Cove. Currently, the site is a relatively inactive and underutilised space which has evolved in a somewhat ad-hoc manner between an industrial complex and an important waterfront recreation area. Whilst the proposal will be a commercial use, it will reinstate public access to the site and potentially provide a catalyst for complimentary surrounding development.

KATE LONEDAY

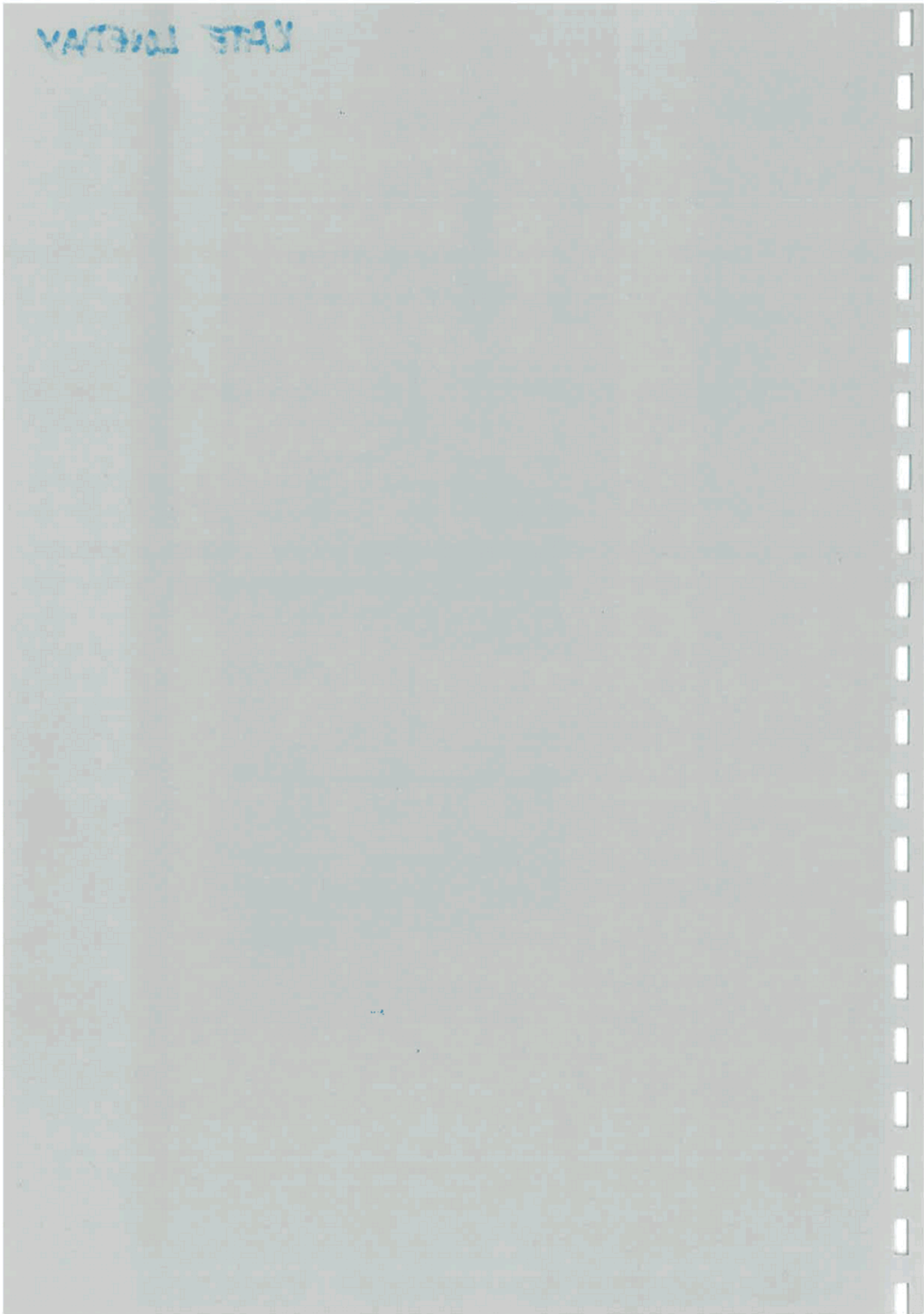
CONSERVATION MANAGEMENT PLAN

HMAS HUON
HOBART, TASMANIA

portion of original drill hall drawing
c.1912

363.69 NEL

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HOBART, TASMANIA**

Prepared for:

DEPARTMENT OF DEFENCE
Campbell Park Offices
Canberra, ACT

Prepared by:

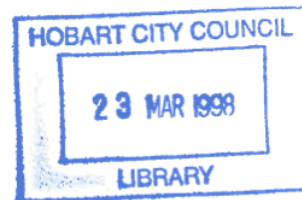
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TABLE OF CONTENTS		Page
PART ONE - THE CONSERVATION ANALYSIS		
1.0	INTRODUCTION	1
1.1	Background to the Plan	
1.2	Heritage Listings	
1.3	Definition of the Property	
2.0	DOCUMENTARY EVIDENCE	5
2.1	Historical Summary	
2.2	Traditional Plans and Photographs	
3.0	PHYSICAL EVIDENCE	26
3.1	Brief Summary	
3.2	Plans	
3.3	Photographs	
4.0	STATEMENT OF CULTURAL SIGNIFICANCE	59
4.1	Definitions	
4.2	Statements	
4.3	Associated Fabric	
4.4	Areas for Further Research	
PART TWO - THE CONSERVATION PLAN		
5.0	CONSERVATION POLICIES	62
5.1	Influences on Policy Development	
	<ul style="list-style-type: none"> User Requirements Building Conditions Defence Policy (DI(G) 40-1 	
5.2	Definitions	
5.3	Policy Recommendations	
5.4	Illustrations (as required)	
6.0	IMPLEMENTATION	66
6.1	Statutory Obligations (heritage)	
	<ul style="list-style-type: none"> Commonwealth State City of Hobart 	
6.2	Guidelines	
	<ul style="list-style-type: none"> Site Individual Buildings New Development 	
7.0	APPENDICES	77

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PART ONE -THE CONSERVATION ANALYSIS

1.0 INTRODUCTION

1.1 BACKGROUND TO THE PLAN

- 1.1.1 HMAS HUON was declared surplus to Defence requirements and naval activities ceased on the site in 17 June 1994. Naval activities in Tasmania are now conducted and housed at the Defence Centre at Anglesea Barracks in Hobart.

The Department of Defence has been discussing the disposal of the site with the State Marine Board who own the adjacent site to the west. More recently the Hobart City Council has expressed an interest although neither of these organisations has confirmed a specific use or commitment to the site as of the preparation of this report.

The Department of Defence has recognised that the National Trust listing of the site indicates its potential significance and has commissioned this conservation management plan to confirm that significance and provide guidelines for its possible conservation as a result of any new use.

- 1.1.2 The Department of Defence stated in its brief :

"The CMP is an urgent requirement pending imminent disposal of the place and the emphasis of the Plan should be with providing innovative suggestions for adaptive reuse of the buildings and general guidelines for the future management of the place."

and;

"Historical research is not seen as the main thrust of the document and a comprehensive history of the site can be completed at a later date." (It should be noted that ACS recommended that as there appeared to be no coordinated historical material an Historical Summary should be part of the plan.)

- 1.1.3 In response, ACS stated in its proposal:

"The entire document will be streamlined to the minimum required for credibility to meet your time requirements." (It should be noted that approval to proceed was received on 5 July 1995 and the draft was required by 11 August 1995.)

- 1.1.4 Therefore, the reader will note that this conservation management plan is not as extensive as some others prepared by the Heritage and Environment Group but still follows the steps and principals expressed by the Australia ICOMOS *Burra Charter* and Dr. Jim Kerr's *The Conservation Plan*.

1.2 HERITAGE LISTINGS

1.2.1 Commonwealth

HMAS HUON is not included on the Register of the National Estate. This however does not imply that the property is of no potential significance as the property has not been considered for inclusion on that register.

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1.2.2 State

At the present time the State of Tasmania has no heritage legislation and therefore HMAS HUON is not listed on any State based heritage register or listing.

1.2.3 City of Hobart

At the present time the City of Hobart has not included HMAS HUON in Appendix 1, Schedule F of the Hobart Planning Scheme which lists heritage properties. The property however is included in an amendment to the Planning Scheme which is presently being considered by the State Government.

1.2.4 National Trust of Australia (Tasmania)

HMAS HUON is classified by the National Trust. The classification occurred in September 1993. The following is the statement of significance prepared by the Trust.

"The naval depot, HMAS HUON, located near the Queen's Domain overlooking the River Derwent, includes several significant elements. The original Commander's Residence and attached Drill Hall were constructed in 1912, within a year of the formation of the Royal Australian Navy, and the base has been occupied by navy personnel continuously since that date. Major additions to the complex include the 1938 classroom wing and the 1960s single storey recreational block ("Club HUON"). The listing also includes the Parade Ground, jetty structure and associated elements. The boundary of the listing is the current title boundary."

"The Hobart Naval Depot has also had the names "Cerberus VI" (1939-1942) and "HMAS Derwent" (1940-1942). It was renamed "HMAS HUON" on 1 March 1942. Tasmania has had the highest per capita enlistment in the Royal Australian Navy, and the depot has very strong historical and social associations with Tasmanian naval personnel and their families, since before the First World War."

"The aesthetic significance of the depot complex and its setting contributes to its importance. It has a strong relationship with the adjacent shipyards and the nearby Queen's Domain and Royal Hobart Regatta grounds."

"The main building (incorporating the former residence and the drill hall) has architectural significance as an example of a substantial timber building, with typical Federation construction technology and detailing, with is generally intact and has been well maintained."

1.3 DEFINITION OF THE PROPERTY

1.3.1 HMAS HUON is located approximately one kilometre north of the Hobart GPO on the foreshore of the River Derwent. The property is located on the northern slope of the Cenotaph portion of the Queens Domain between the railway line into Hobart and the foreshore.

The property slopes steeply from south to north in a series of man made terraces. Between the buildings these terraces are paved in a variety of materials but mainly bitumen and grass.

1.3.2 For the purpose of this report north is nominally identified as the along the axis of the jetty as its projects into the river. Refer to **Figure 3.1**.

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- 1.3.3** The property has 14 numbered buildings, numbers 1 to 16 (building numbers 3 and 14 do not appear to exist). Refer to **Figure 3.1** for the building numbers and section **3.0** for a brief recording of these buildings as they exist today.

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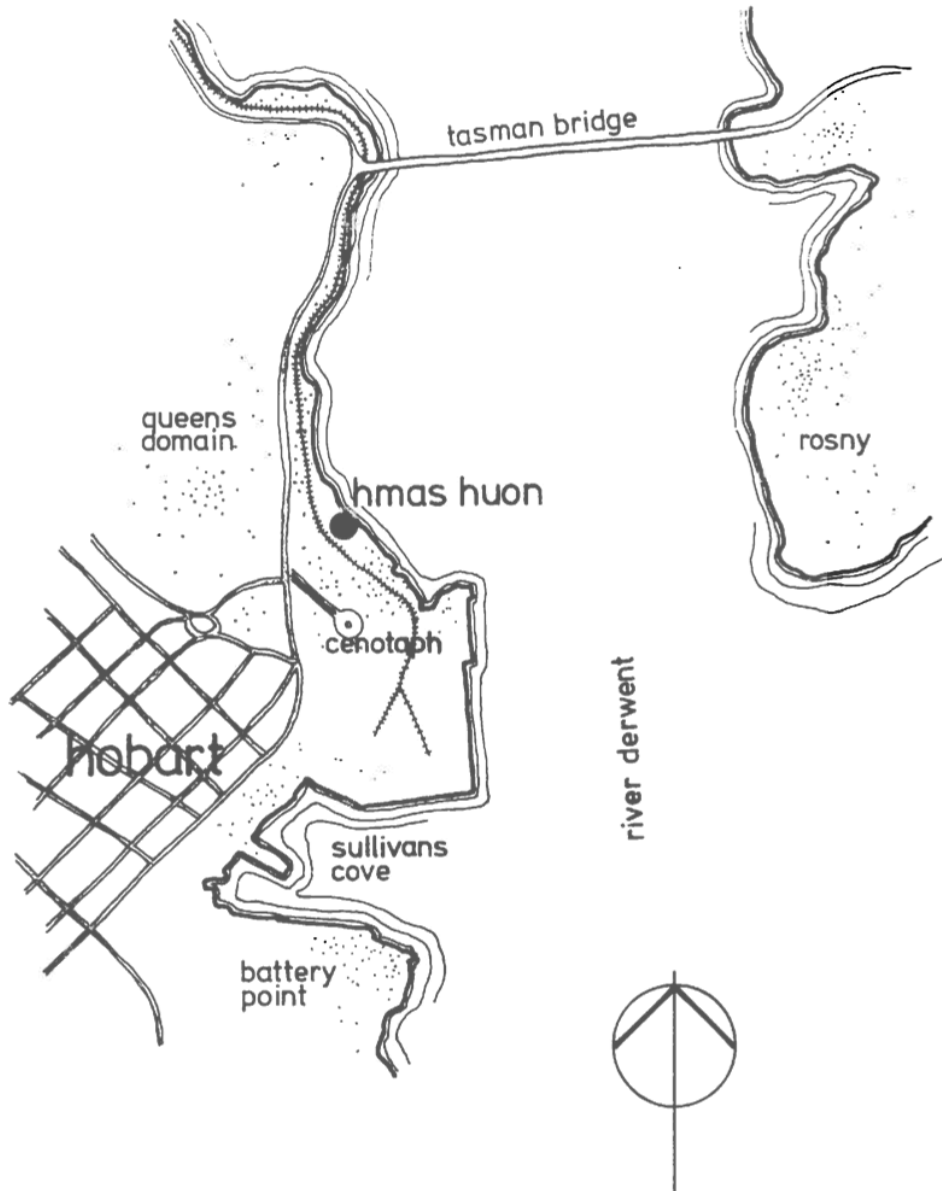


FIGURE 1.1 HMAS HUON, HOBART
location plan - refer to **Figure 3.1** for a site plan

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2.0 DOCUMENTARY EVIDENCE

2.1 HISTORICAL SUMMARY

2.2.1 Macquarie Point

- a. HMAS Huon is situated on the northern shore of Macquarie Point, part of the Queen's Domain in Hobart. The Domain was reserved for public purposes following Governor Macquarie's instruction of 1812 although it was several years before the grants already located there were resumed by the Crown.
- b. Ever since, Macquarie Point, as a result of its proximity to the town, has been used for recreational, commercial and defence purposes. During the 1820s to 1840s cricket matches were regularly held on a somewhat stony pitch on the site now occupied by the Cenotaph. The Point is the venue of the Royal Hobart Regatta, established by Governor Franklin in 1838, and the oldest such event in Australia.
- c. Tasmania was then Van Diemen's Land and still a penal colony. Imperial troops manned the colony's garrisons and on field days they were reviewed on Macquarie Point. As early as the 1820s Macquarie Point was selected as the site for a coastal battery although work did not begin until 1854 when Britain's participation in the Crimean War added a temporary urgency to defence plans. The Queens Battery was finally completed and armed in 1865. It was demolished in 1925-6 when the site was chosen for the Cenotaph.

2.2.2 The Royal Navy

- a. The early naval history of Hobart is really a history of the activities of the Royal Navy. Although Tasmania never had its own navy, the Royal Navy was often in Tasmanian waters. Tasmania, in common with the other Australian colonies, was founded by naval and military men. The first settlement, at Risdon Cove on the Derwent, was established by a naval officer, Lieutenant John Bowen.
- b. The visits of ships of the Royal Navy were important to Hobart from an economic and social as well as a defence point of view. Many of the ships that visited the port were on scientific or exploratory expeditions - Charles Darwin visited the port as a member of the scientific staff of HMS *Beagle* in 1836, for instance. During 1839-40 a naval expedition under the command of Captain James Ross came to Hobart with HMS *Erebus* and *Terror* while carrying out a magnetic survey of the southern Hemisphere. While in Hobart, and with the co-operation of the Governor, Sir John Franklin, himself a naval man, Ross supervised the erection of a magnetic observatory on the Domain. This was one of a series erected around the world on this expedition. The Rossbank Observatory was under the control of the Admiralty and personnel were drawn from the Royal Navy until 1852 when the facility was handed over to the Colonial Government.¹
- c. As well as carrying out the scientific objects of their expedition, the officers of *Erebus* and *Terror* enjoyed a round of social engagements and gave a memorable ball on board the two

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ships which were lashed together for the occasion. The re-stocking of supplies and the constant employment of watermen taking visitors to and from these and other Royal Navy ships added significantly to the port's economy.

- d. In 1870, the Imperial troops were withdrawn and the colonies became responsible for their own defence although the conduct of war at sea was still the concern of the British Government in the form of the Royal Navy which maintained an Australian Squadron in colonial waters. Victoria developed its own navy but the other colonies generally did not. The Royal Navy was accorded great respect and admiration, and the regular summer visits to Hobart brought general excitement, a dazzling social whirl and profit for shopkeepers. So important were the naval visits that the commanders were even able to dictate local legislation. The threat of an end to naval visits following complaints that crew of HMS *Wolverine* had caught a particularly virulent form of syphilis in Hobart was enough to force the local government to enact the draconian Contagious Diseases Act of 1879.²

2.2.3 The Defences of the Colony

- a. Prior to the withdrawal of British troops, a number of small batteries had been built close to Hobart. originally the focus had been on the protection of Sullivans Cove with sunken batteries built at Macquarie Point and Battery Point. At first the threat was perceived to be the random privateer who might hold the settlement to ransom - during the 1830s, before Melbourne was even thought of, Hobart was a prosperous whaling port. Elaborate plans for a massive fort at Macquarie Point were drawn up by the local Commanding Officer of the Royal Engineers. Lack of finance and support from the Home Government soon put a stop to such flights of fancy, however.
- b. Later, more specific threats were recognised. The Russians were thought to be the main danger both in the 1850s during the Crimean War and during the 1880s when widespread hysteria gripped the city and local citizens made plans to take to the hills. The French and the Americans also came under suspicion at various times. The paranoia was no doubt related to the extreme isolation felt by many Tasmanians who still clung to thoughts of "Home." This also explains why the visits of the Australian Squadron were such a comfort and so warmly welcomed. The local response to the panic and hysteria, which was to some extent, whipped up by the press, was more pragmatic. When fear was at its peak defensive works were begun but when the danger subsided projects faltered and funds were directed elsewhere.
- c. In 1877 Sir William F. Drummond Jervois arrived in the colonies to take up the dual position of Governor of South Australia and Defence Adviser to the Australian Colonies. He, along with other military men of the time, saw Hobart as a vulnerable spot from which an enemy could launch attacks on British shipping as well as the other Australian capitals. He considered that Australia's naval defence would be better served by the colonies acting together - an argument later used in the promotion of Federation. Jervois' report therefore concentrated on shore-based defences. He recommended the upgrading of the Queen's Battery at Macquarie Point, the completion of batteries at Sandy Bay and on the eastern shore, and the dismantling of the works at Battery Point.³

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2.2.4 The Torpedo Boat

- a. Jervois' scheme also included torpedo defences in the form of two lines of electro-contact mines laid between Macquarie Point and Montague Point on the other side of the river. The first attempt by the Engineer Corps to lay the mines was unsuccessful, the local ferry boats proving unsuitable for the purpose. In 1884, the colony acquired a torpedo boat, for which a shed and slip were built off Macquarie Point just along the foreshore from HMAS Huon.⁴ The boat was initially equipped with a spar torpedo but later, Whitehead torpedos were fitted. Although the boat's capabilities were admired by the Commandant of Local Forces and W. Pitfield, the Hobart engineer who re-fitted the engines, the boat was frequently on the slips undergoing repair. Minor damage was caused on several occasions by the slip itself while the use of unqualified crew was a continuing hazard. For more than two years from March 1896, the boatways were so badly damaged that the boat could not be got on the water. The boat seems not to have been used after this, the Torpedo Corps becoming once more the Engineer Corps to concentrate on field engineering.
- b. The defences of the colony were manned by volunteer artillerymen and a small company of Permanent Artillery. Suitably qualified officers and instructors were sometimes difficult to find although the Torpedo/Engineer Corps did sometimes have the advantage of a naval instructor. The single torpedo boat was the closest Tasmania ever came to having a navy.

2.2.5 The Domain Slip

- a. In the meantime other activities were taking place on the site now occupied by HMAS Huon. Boats had occasionally been built on temporary slips on the beach below - notably by Charles Williamson - although the permanent shipyards were at Battery Point. However, in 1849, Captain Goldsmith of the ship *Rattler* approached the Government with a proposal to erect a patent slip for the repair of merchant vessels of 600-700 tons trading to Hobart Town.
- b. The Lieut Governor of the day recognised the advantages of such a facility and agreed with the proposal subject to the slip's being made available to ships of the British Navy and the Convict department for half the normal charge. Goldsmith asked for a site within Sullivan's Cove but in the event he was granted a lease on the site now occupied by the Marine Board of Hobart's slipyard and HMAS Huon.⁵ Unfortunately the delay in settling on the site and the terms of the lease coincided with the discovery of gold in Victoria. The male population of Hobart crossed the Strait in droves and by the time Goldsmith was given the go-ahead, he was unable to obtain either the labour or the necessary materials.⁶
- c. The slip was finally completed in 1855 and the same year Goldsmith sold out to John Macgregor, one of the town's most well-known ship-builders. The site was leased by a number of individuals over the next 60 years but by the early years of this century, the slip was proving inadequate for the needs of the shipping industry and the lessees could not make it pay. The Government wanted the slip off its hands and offered the lease to the

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Marine Board of Hobart on the condition that a new and larger slip be laid down. The Board reluctantly agreed and formally took over the slipyards in April 1914.

- d. In 1900 a small area of 25 roods was marked off from the slip yards "for the benefit of yachting clubs."⁷ Early photographs suggest there were already slips and boatsheds on this site and the 1900 survey may have been merely formalising a situation which already existed. It also seems likely that in the early years of the naval depot these sheds and ways were used.

2.2.6 The Naval Jetty

The Australian Squadron had for many years landed at Macquarie Point, using the Cattle Jetties which were originally built to serve the Government Slaughterhouse. The commanders were always offered berthing facilities at Princes Wharf within Sullivans Cove but preferred the Cattle Jetties, probably because the latter's more isolated position allowed better control over the crew than the open wharves of the Cove. In 1905 the jetties were renamed the Naval Jetties in recognition of the prevailing practice and facilities were upgraded at one of the jetties which became known as Naval Pier. These jetties were a short distance along the foreshore from HMAS Huon and during the Second World War they temporarily became part of the state's naval establishment.⁸

2.2.7 Commonwealth Defence

Following Federation, the Commonwealth took over responsibility for both naval and military defence. There was considerable concern that the volunteer system inherited from colonial days was not sufficient to ensure the security of Australia, particularly in the light of ongoing conflicts in other parts of the world and the perceived rise of German militarism. Between 1909 and 1911 a number of Defence Acts were passed to introduce compulsory military training for junior cadets (12-14 years of age), cadets (14-18) and militia forces (18-26). A small number of these were accepted for naval training. In 1911 the Commonwealth Navy became the Royal Australian Navy and the naval militia became the Royal Australian Naval Reserve.⁹

2.2.8 HMAS Huon

- a. Compulsory training involved a much larger scale of operations than the former voluntary system and accordingly drill halls and rifle ranges were built all over the state to accommodate the trainees. One of these drill halls was at Macquarie Point on part of the land originally reserved for the slipyard. It appears to have been the only naval drill hall in the state. The other drill halls built in the state's country towns at this time were generally very much smaller and simpler in design. A brick drill hall dating from 1824 survives at Anglesea Barracks although no longer used for the purpose.¹⁰
- b. The authorities visited Launceston to select a site for a naval drill hall but any subsequent history is unknown. However, during the 1939-45 war the only naval establishment in Launceston was a recruiting office. There were also plans to build a "sub-naval" base at Dowsing's Point near Hobart to accommodate six destroyers and four submarines but this also did not eventuate.

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- c. In 1911 there were 101 cadets on the roll and 8 naval militia. The senior officer was Captain Milford MacArthur, Hobart's Harbour Master. Their training comprised both drilling and theoretical subjects but some nights were spent repairing or painting the boat or carrying out other routine maintenance.¹¹
- d. In 1913 Captain Tickell, the Director of Naval Reserves, visited the state on a tour of inspection. In a public statement, he outlined the role of the Reserve:
*"One of its objects was to supply the wastage of the seagoing force, and, in the event of war, it would relieve the seagoing men from duty in the dockyards and those places. It would supply the entire personnel for the examination of ships coming into harbours and for signal stations. As much seamanship was taught to the members as possibly could be in the time allowed. It was estimated that, under fair conditions, trainees could, in five years, be made equal to fill junior positions in a ship and senior positions on shore"*¹²
- e. Numbers of cadets remained during the war but the Reserve diminished to two or three men - presumably because others of suitable age were on active service.¹³
- f. During the 1920s some buildings were erected near the drill hall and extensions were made to the residence. In 1924, the District Naval Officer sought clarification of the seaward boundary of the site before extending the side boundary fence to prevent the foreshore being used as a right of way to the slipyards. It was planned to build a new boatshed [Building 9] and, material being already on the ground, the situation had to be regularised. Subsequently the naval authorities obtained a lease, for a nominal rent, of the ground between high and low water marks. The following year the wardroom [Building 4] was built.¹⁴
- g. During this period the naval cadets and reserves distinguished themselves at a number of regattas around the state, principally the Tamar and Royal Hobart Regattas. The Naval Supply Office at Derwent Hall in Anglesea Barracks still retains dozens of trophies won during this period.¹⁵
- h. In 1929 compulsory training in peacetime was discontinued, but in 1939, with the outbreak of war, the compulsory clauses of the Defence Act were brought into force. Reservists went into training at the Naval Depot which was now called HMAS Derwent. A mess room and sleeping quarters was built in 1940 to accommodate 76 men [Building 2].
- i. The Naval establishment increased in 1941 with the construction of the Naval Oil Fuel Installation on the Domain at the site of the old Beaumaris Zoo.¹⁶ Four tanks were built at the site together with caretakers and guard accommodation, and pump houses. Pipes were laid to the wharves to supply shipping. During the war Hobart be¹⁷came a port of call for Liberty ships and other warships, for rest and recreation, re-provisioning and repairs. A Port War Signal Station was established at South Arm as part of Fort Direction but under naval authority. Naval personnel also manned the coast-watching stations at Tasman Island and South Bruny, and carried out harbour and swept channel patrols, the latter with back up from the Naval Auxiliary Patrol.¹⁸

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- j. On 1 March 1942 the naval depot became HMAS Huon.
- k. In 1954, accommodation was re-arranged. Building 2 was converted into classrooms. Correspondence of the Resident Naval Officer refers in the late 1950s to an increase in Reserve Personnel. In 1958 the complement was 3 +23, of which ten of the ratings lived in. A 1950s statement of the duties of the Resident Naval Officer (Tasmania) indicates that most of his duties were ceremonial and administrative. He was responsible for providing the requirements of visiting warships and for representing the Navy at official functions. The day to day administration of HMAS Huon and of recruiting and reserve training was left to the Executive and Staff Officers.¹⁹
- l. An Attack class patrol boat *HMAS Ardent* was commissioned in November 1968 - to be manned by Reserves of HMAS Huon. She was decommissioned in 1994, by which time she was the last of the Attack class in service.
- m. HMAS Huon closed on 17 June 1994 and Naval personnel transferred to Anglesea Barracks.

2.2.9 Footnotes

- 1 Savours, Ann and McConnell, Anita, "The History of Rossbank Observatory, Tasmania", in *Annals of Science*, 39 1982, Passim.
- 2 Scripps, Lindy and Hudspeth, Audrey, *The Female Factory Historic Site, Cascades: Historical Report*, 1992, p 34ff
- 3 JPPT HA 1878 No.37 Report of Sir William Drummond Jervois "Defence, Tasmania"
- 4 JPPT 1885 No.93 Report of the Commandant of Local Forces
- 5 See Figures 2.1 and 2.2
- 6 CSO 24/94/2852 and CSO 24/164/2852 Colonial Secretary's correspondence re the Domain Slip
- 7 DELM Plan Buckingham 61/14 November 1900
- 8 Mercury 7 June 1905
- 9 Cyclopaedia of Tasmania 1931 pp 86-7 and Odgers p 235
- 10 Dollery in Rowntree p 123
- 11 Australian Archives P1552 Compulsory trainees muster roll and drill record
- 12 Mercury 26 April 1913
- 13 Australian Archives P1552 Compulsory trainees muster roll and drill record
- 14 See 2.2.10 Chronology of the Site's Development
- 15 Inventory of historical material - NSO TAS
- 16 This site is currently being rehabilitated
- 17 M.O'Brian, "Tasmania's War Effort 1939-1945", Launceston, 1946, p.20-1

2.2.10 Chronology of the Site's Development

April 1912	Tenders called for construction of Naval Drill Hall and Offices [Buildings 7 and 8]. R.S. Spiers was successful contractor with a tender of £2285/13/6.	<i>Gazette</i> 23 April 1912 p. 1067 and 18 June
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	It was one of a number of works carried out by the Tasmanian Public Works Department for the Commonwealth at this time.	1912 p. 1301 PWD Plan 518 [ACS]
May 1913	Drill Hall completed by May 1913	<i>Tasmanian Mail</i> 22 May 1913
April 1925	New mess room and lavatories. This appears to be Building 4.	PWD Plan 7519 17 April 1925 [AA]
May 1927	New concrete boatways were constructed with a 6-inch concrete floor over rock fill.	PWD Plan 7834 17 May 1927 [AA]
March 1928	Alterations to residence [Building 8]. The room shown on the original plan as the ground floor bedroom was effectively doubled in size by an encroachment into the drill hall. The intervening wall was removed and a new fibro-plaster wall built to enclose the new room. A new window was built in the external wall.	PWD Plan 7972 26 March 1928 [AA]
August 1928	This plan for a new retaining wall shows that the area now occupied by the classroom block [Building 2] was then the site of the parade ground which was surrounded on two sides by an existing retaining wall. Building 4 is also shown on this plan, designated "Officer."	PWD Plan 8054 7 August 1929 [AA]
June 1929	A plan for a boat shed. [Building 9]. A note on the plan reads "Demolished and replaced 1961"	PWD Plan 8212 6 June 1929 [AA]
August 1930	Alterations to Naval Drill Hall including enclosing the balcony on the residence [Building 8].	PWD Plan 8451 4 August 1930 [AA]
June 1940	Plan of new mess and sleeping quarters [Building 2], Naval Depot shows sleeping and messing quarters for 76 and sleeping and messing quarters for Petty Officers.	E.S.B./6MD Plan 592 28 March 1940 [AA]

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1 March 1942	The Depot was renamed "HMAS Huon" after a brief period as "Cerberus VI" and "HMAS Derwent."	National Trust Statement of Significance
October 1951	<p>The reconstruction of the jetty was proposed. It was then an L-shaped structure and incorporated facilities for reserve training -</p> <p>a) whaler's davits b) sounding machine and sounding machine boom c) wooden tower to simulate chains for sounding practice d) berthage for training motor boat "Huon" and work boat.</p> <p>The jetty had been badly damaged by a gale and by teredo worm. An extension to the jetty was also proposed to provide for the anticipated R.P.V although the latter was considered a "luxury" for a depot the size of HMAS Huon. In the event the renewal of the jetty was put off - apparently until 1959.</p>	<p>P1221 33/31/1 23 October 1951 RNO to Sec. Naval Board & ff [AA]</p>
1954	Building 2 was altered to provide lecture rooms, armoury and Medical Officer's accommodation.	Works Plan VT? [ACS]
May 1955	Plan showing boatshed and slipway repairs and extensions. There appear to have been several proposals for alterations to or renewal of the boatshed, the work finally being carried out in 1961.	PWD Plan 9551 10 May 1955 [ACS]
1958	A new toilet and ablutions block for ratings was proposed since the existing ones were in such a poor state. However funding was not available and the work was postponed.	<p>P1221 24 January 1958 Secretary, Dept of Navy to Director of Works, Vic. [AA]and Plan VTA 2764 [ACS]</p>
June 1960	Plan of alterations to boat shed [Building 9].	Work Plan

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TA60/108/C
3 June 1960
[ACS]

2.2.11 Bibliography

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JPPT HA 1878 No. 37 Report of Sir William Drummond Jervois "Defences, Tasmania"
JPPT 1885 No. 93 Report of the Commandant of Local Forces

Archives Offices of Tasmania

CSO 24/94/2852 and CSO 24/164/2852 Colonial Secretary's correspondence re the Domain slip

PWD 47 Correspondence concerning works carried out for the Defence Department by the Public Works Department of Tasmania was searched but was found to contain nothing relating to HMAS Huon

Australian Archives

AA(Tas) P1221 Correspondence files

AA(Tas) P1548 Naval district orders

AA(Tas) P1552 Compulsory trainees muster roll and drill record

AA(Tas) P1715 Drawings of Commonwealth buildings

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2.2 TRADITIONAL PLANS AND PHOTOGRAPHS

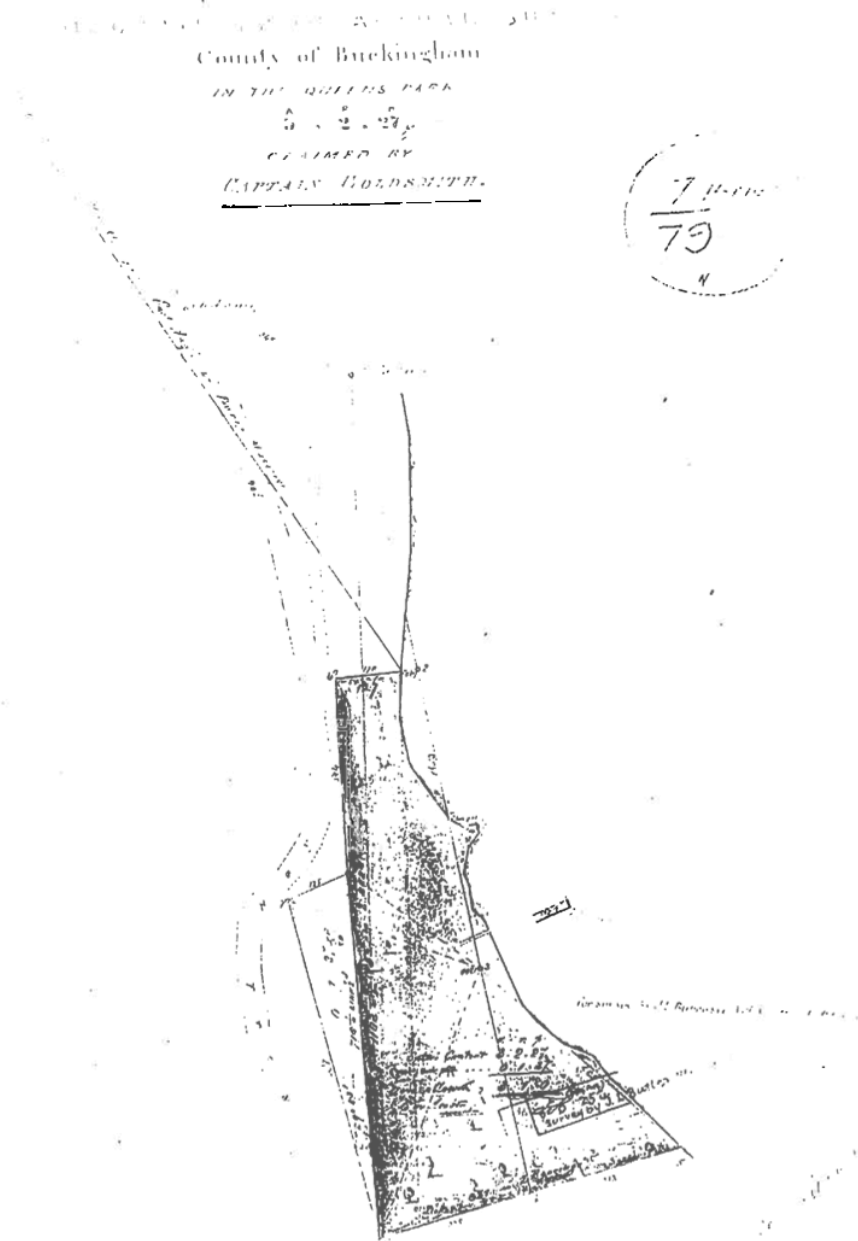


Figure 2.1 HMAS HUON, HOBART
survey of Captain Goldsmith's slipyard, 1851 (DELM Plan Hobart 7/79)

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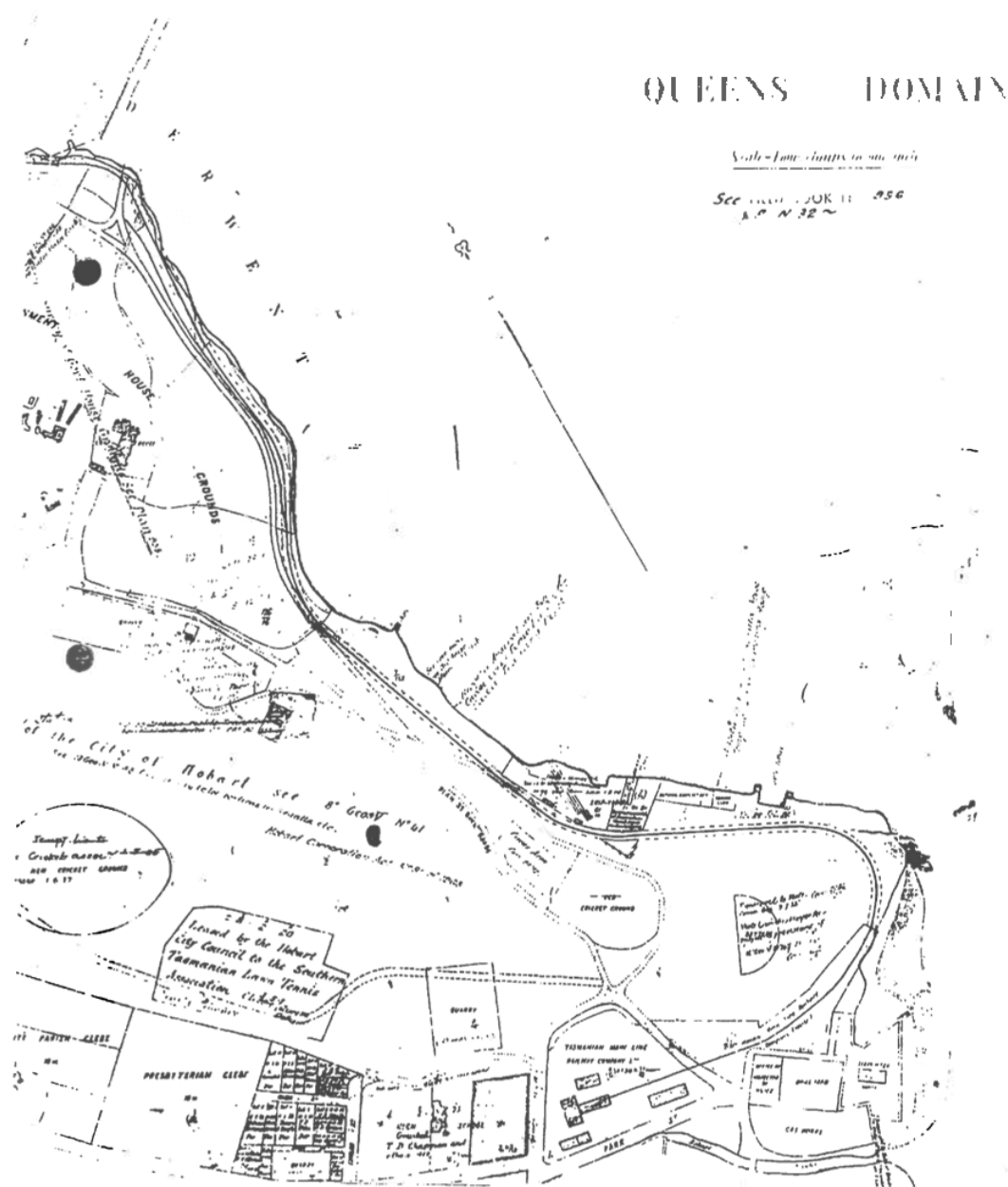


Figure 2.2 HMAS HUON, HOBART
the Queen's Domain c.1890 (DELM Plan 94)

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Figure 2.3 HMAS HUON, HOBART

top: site of HMAS HUON c.1890, beyond the slipyard - at middle left is the enclosure of the Domain Baths (AOT photo 30/6862)

bottom: site of HMAS HUON at the extreme left, from the water during a Regatta rowing event c.1910 (AOT photo NS 1231/68)

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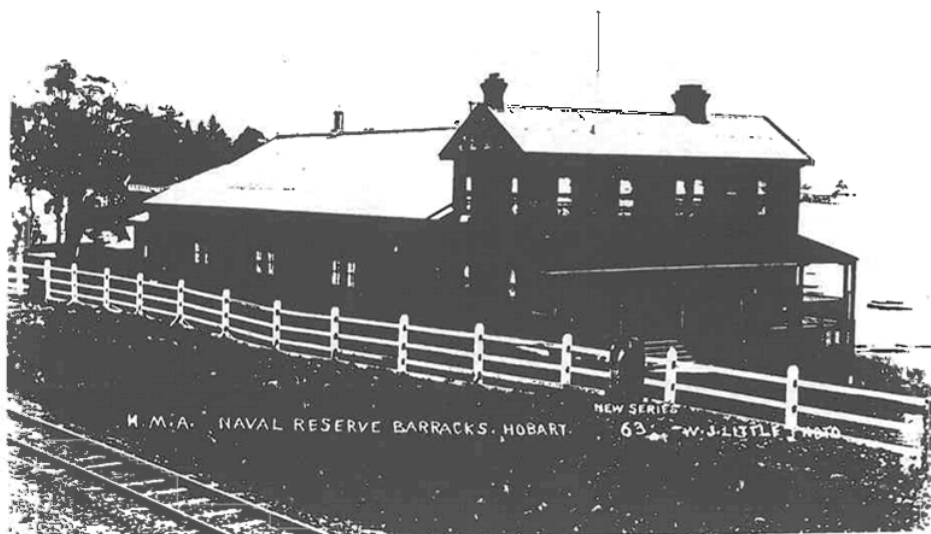
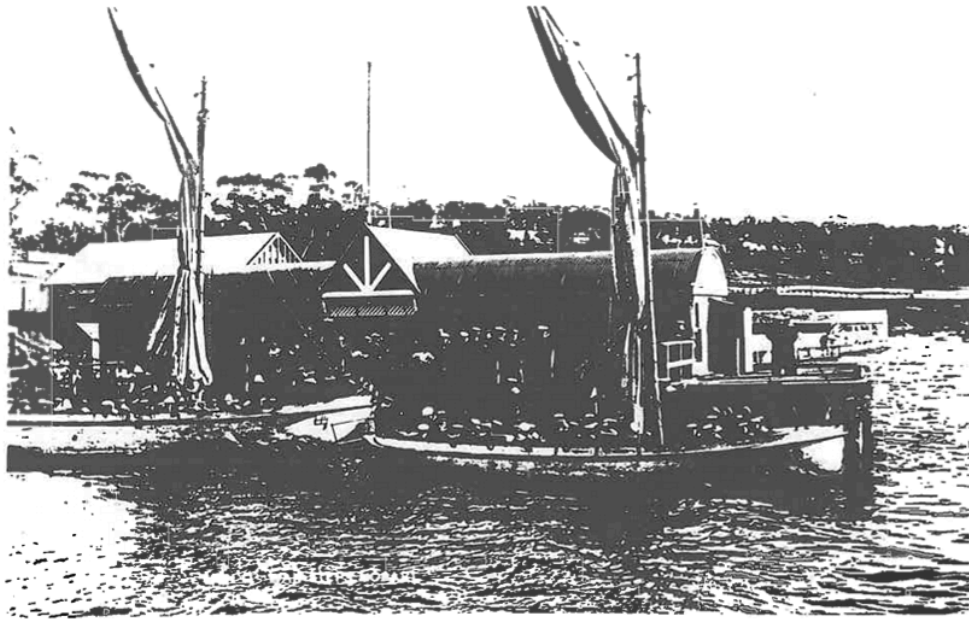


Figure 2.4 HMAS HUON, HOBART

top: cadets at Naval Pier, Hobart c.1912 (AOT photo NS 1013/946)

bottom: the drill hall shortly after completion, may 1913 (AOT photo NS 526/9)

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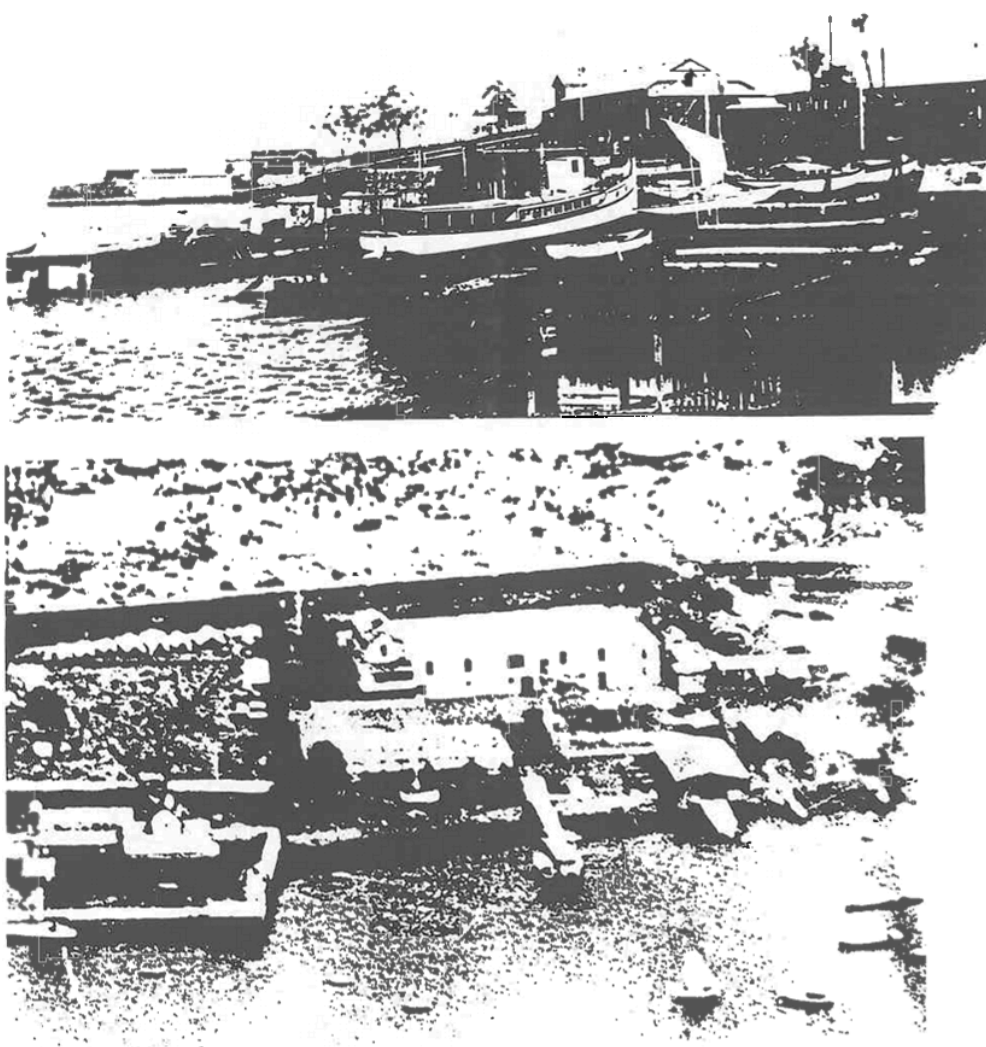


Figure 2.5 HMAS HUON, HOBART
top: the drill hall from the slipyards c.1923 (AOT photo NS 942/141)
bottom: detail from an aerial view of the Regatta ground showing HMAS HUON and the Domain Baths on the left c.1930 (AAOT photo NS 892/41)

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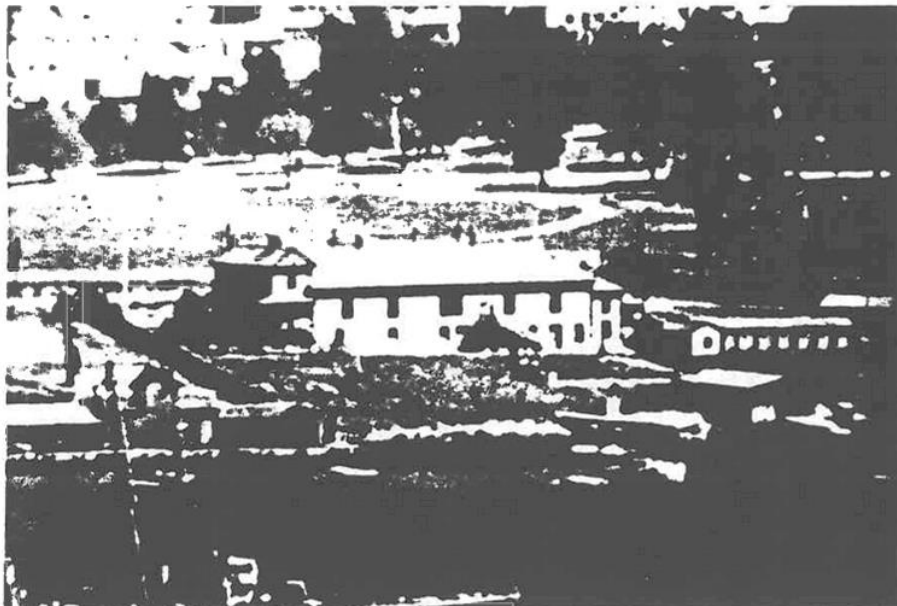
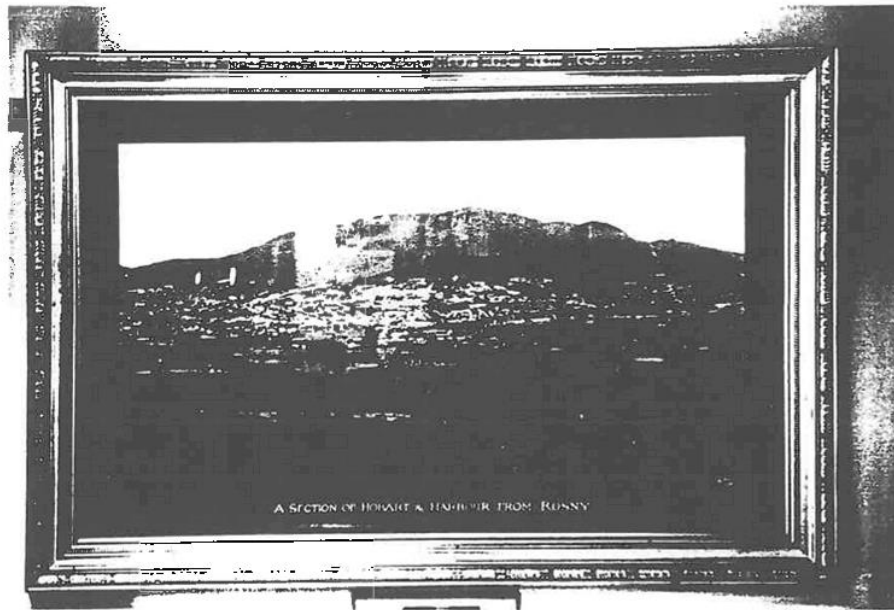


Figure 2.6 **HMAS HUON, HOBART**

top: Hobart from the eastern shore, c.1920s (late?) (Department of Defence - Anglesea Barracks)

bottom: detail from above

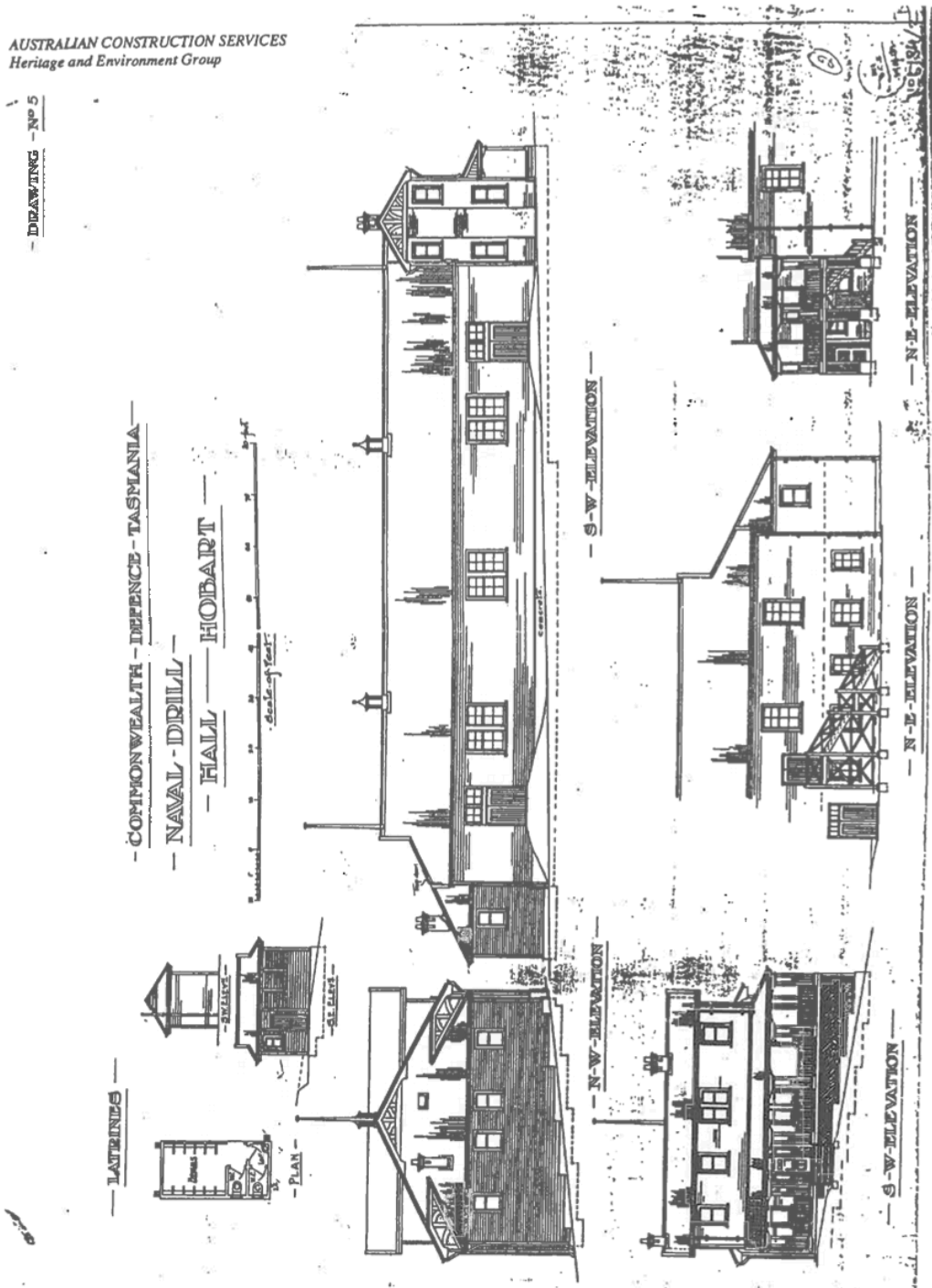


Figure 2.10 HMAS HUON, HOBART
Naval Drill Hall drawings, c.1912 (ACS microfilm) (copy also held by RAN at Anglesea Barracks)

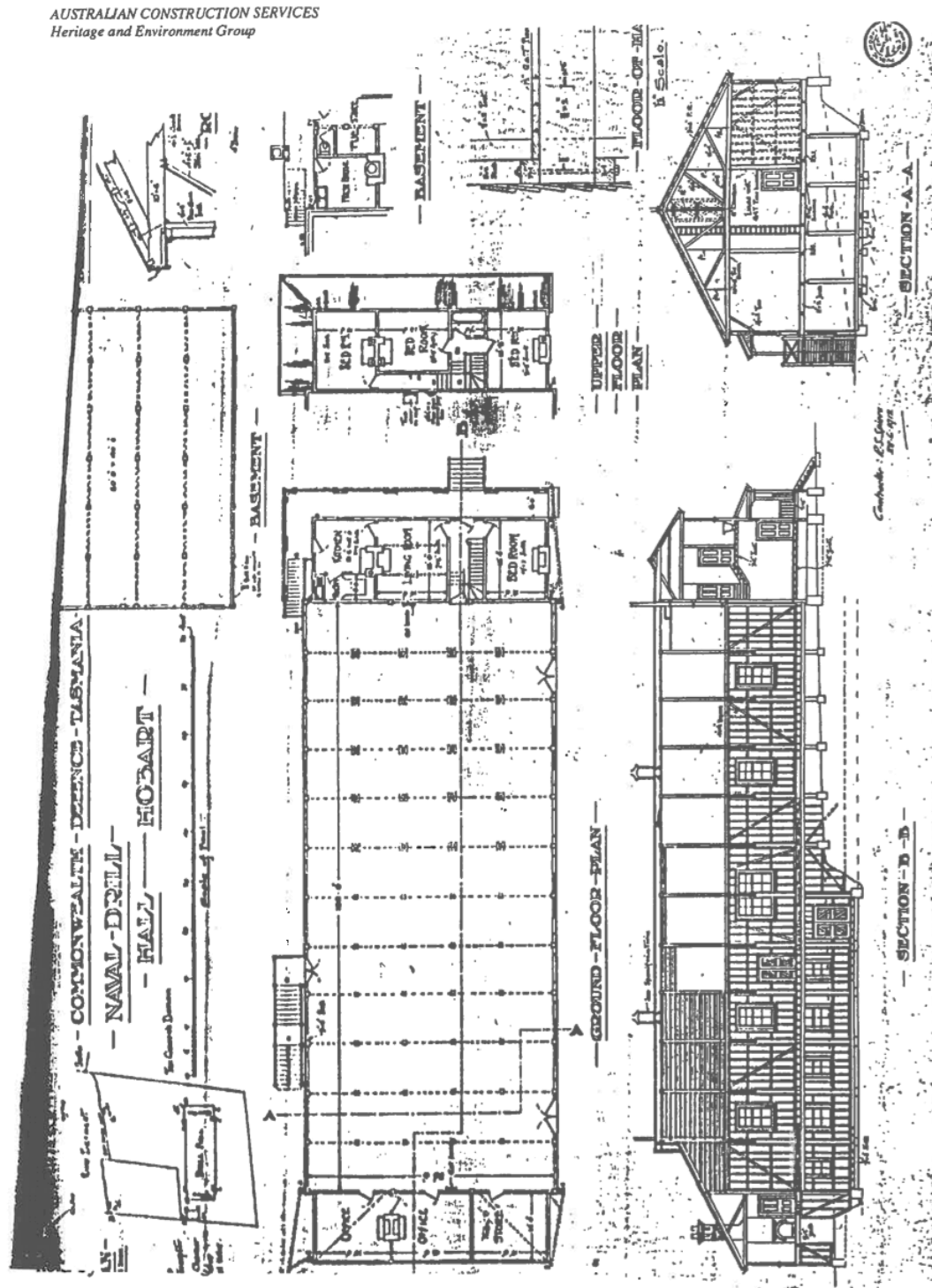


Figure 2.11 HMAS HUON, HOBART

Naval Drill Hall drawings, c.1912 (ACS microfilm - poor quality is on the microfilm copy) (copy also held by RAN at Anglesea Barracks)

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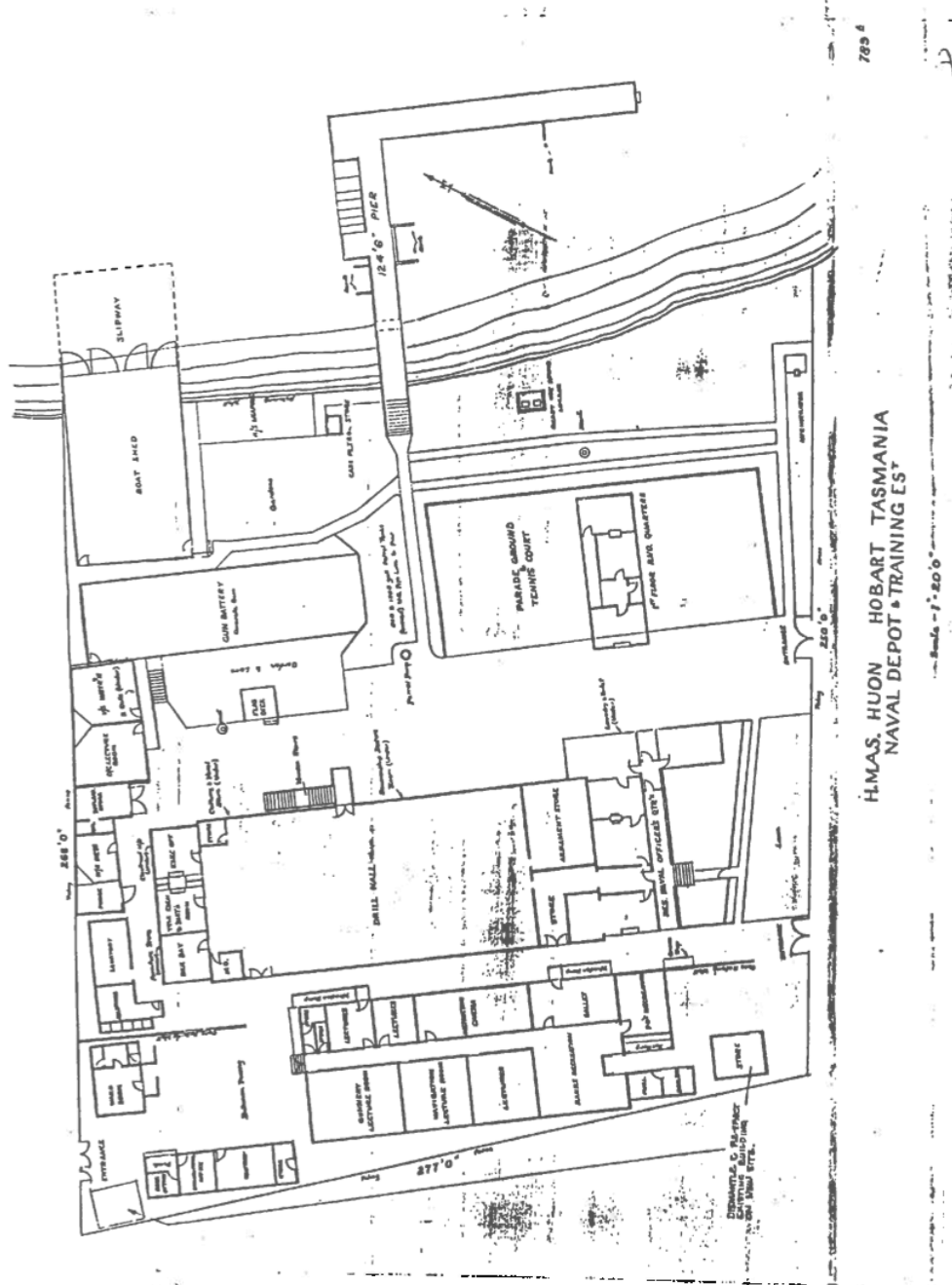


Figure 2.12 HMAS HUON, HOBART
site plan, c.1940s (ACS microfilm)

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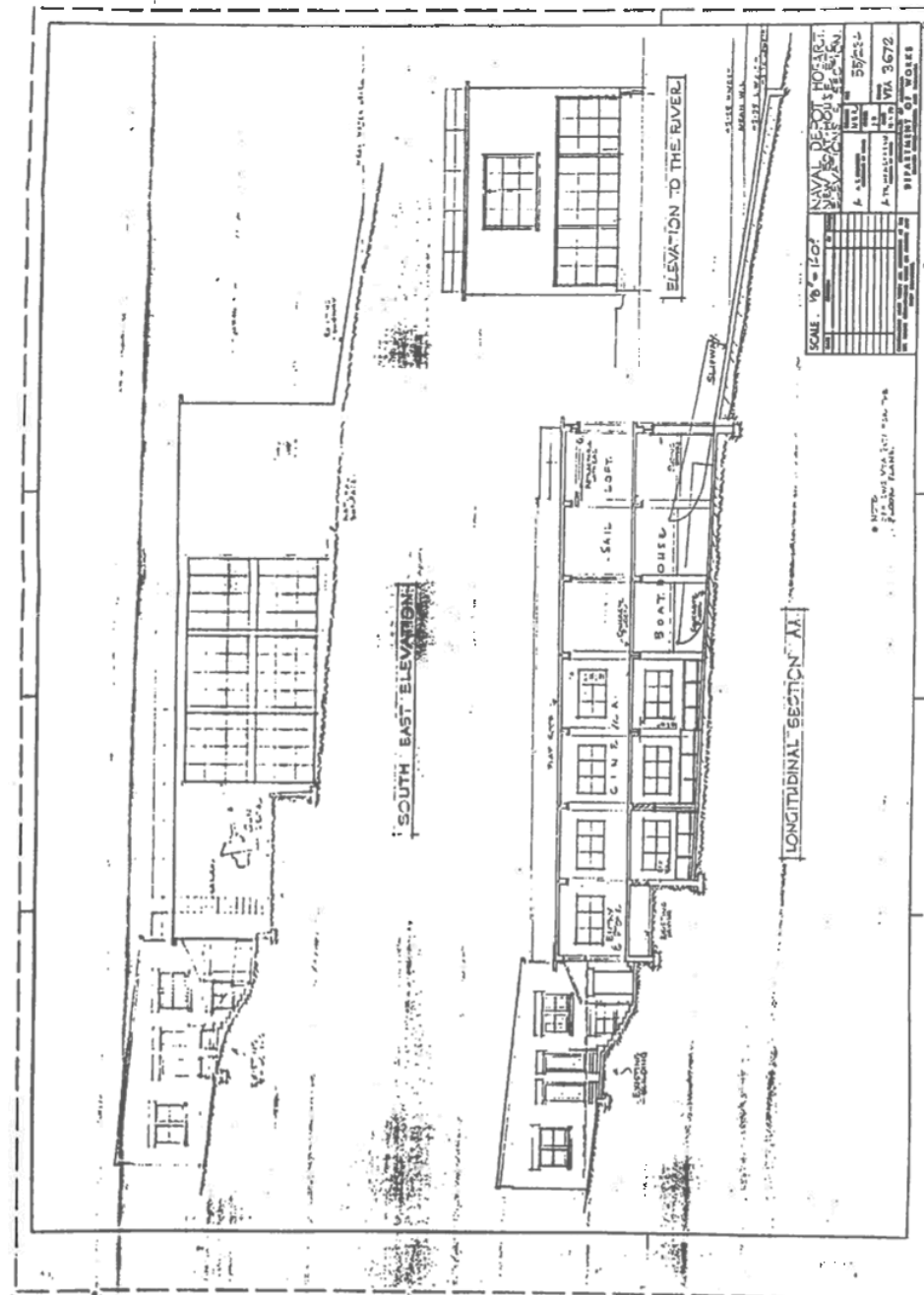


Figure 2.13 HMAS HUON, HOBART
proposed new boat shed - never built, building 6 shown on left, c.1950s (ACS microfilm)

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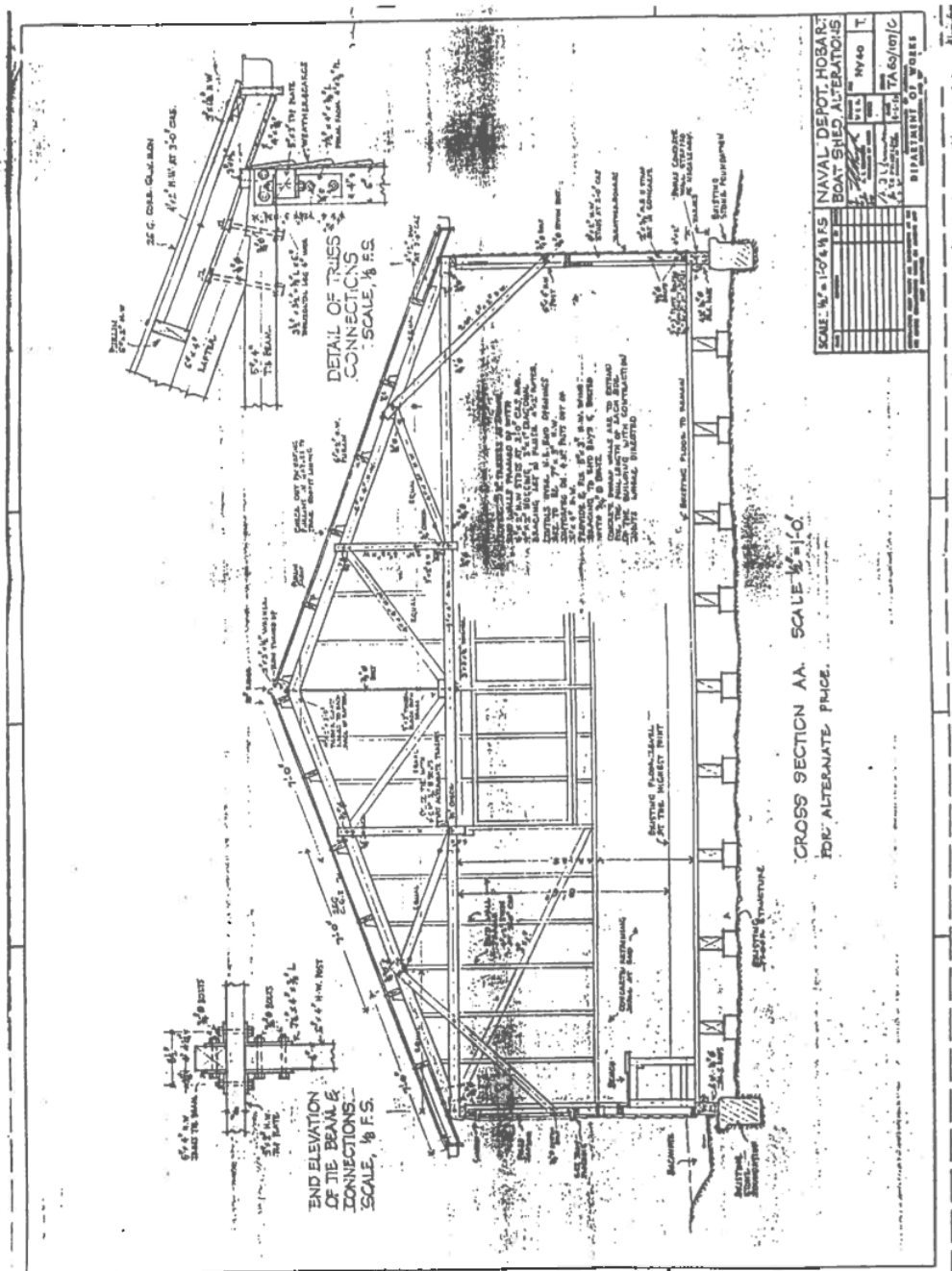


Figure 2.14 HMAS HUON, HOBART
alteration to the Naval Depot boat shed, c.1960s (ASC microfilm)

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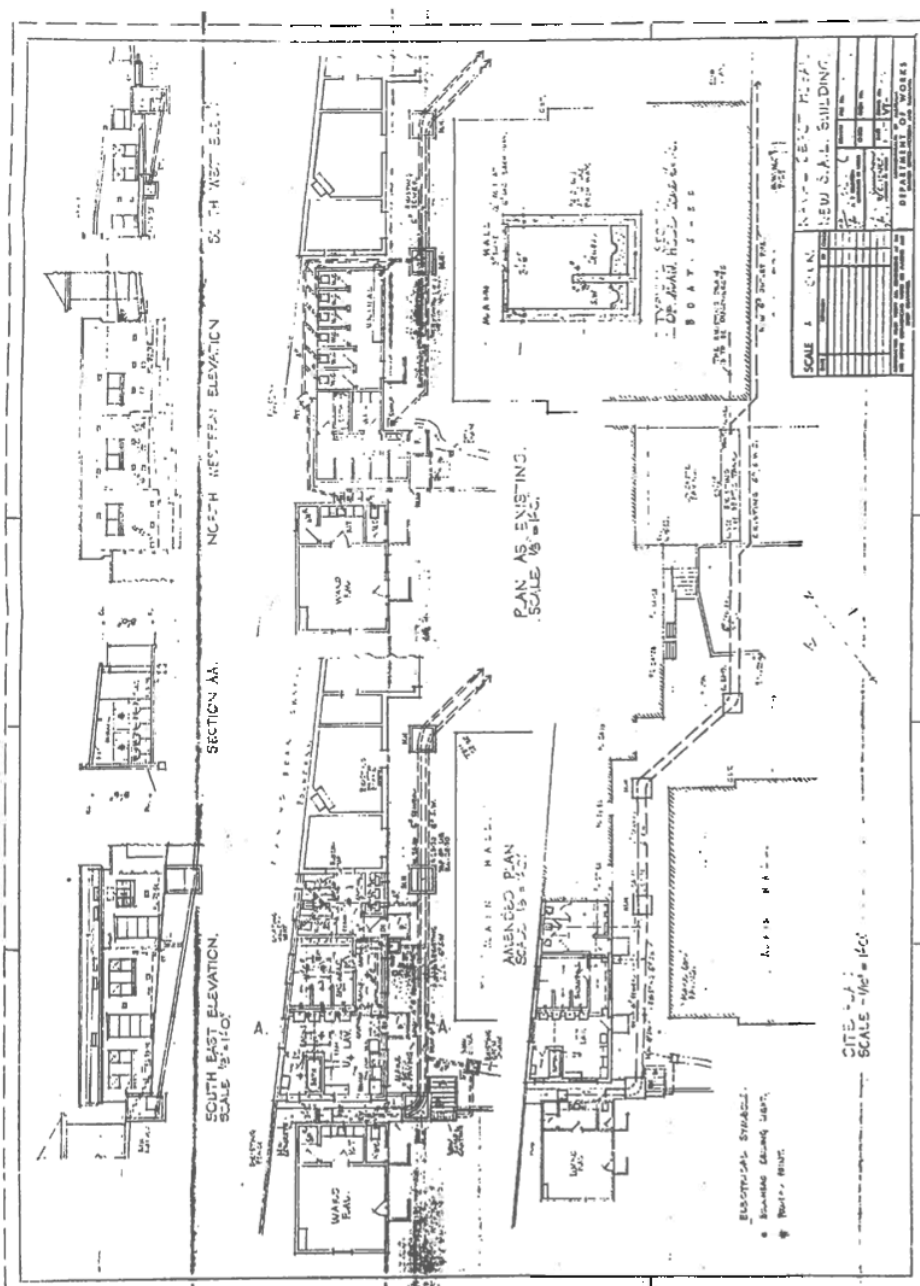


Figure 2.15 HMAS HUON, HOBART
new SAL building - southern portion of building 5, c.1960s (ACS microfilm - poor quality is on the microfilm copy)

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3.0 PHYSICAL EVIDENCE

3.1 BRIEF SUMMARY

3.1.1 Buildings

a. Building 1 (refer to Figures 3.8 and 3.9)

This is a 'small' timber framed, weatherboard clad building of an undetermined date. There is no indication of the building on the 1912 site plan of the original drill hall - building 7/8 - and the first reference to it is on a WWII era site plan. The building details which survive indicate however that is older than WWII. The building is spartan in its design and treatment and it has been completely stripped out internally of partitions and finishes.

The building has no aesthetic interest and its historical contribution is in a general sense only demonstrating the development of HMAS HUON over the years. The building's low internal integrity adversely impacts upon its potential importance.

b. Building 2 (refer to Figures 3.9 - 3.11)

This is a large timber framed building clad in shadow line boards with substantial, modern additions at its eastern end. The building dates from 1940 and was originally built for accommodation purposes and then altered for class rooms in the 1950s. The form, plan and external materials of the building appear largely intact although the interior walls and ceilings have been largely lined with new materials and all the internal doors have been replaced. Limited portions of early vertical 'v' groove lining boards are still exposed.

The building has no aesthetic interest and its historical contribution is in a general sense demonstrating the development of HMAS HUON over the years and particularly during WWII. The building's relatively low internal integrity adversely impacts upon its potential importance.

c. Building 4 (refer to Figure 3.12)

This is a small timber framed, weatherboard clad building built as a mess room in 1925. The gabled roof form is the only feature of interest and aligns it with the drill hall/residence (buildings 7/8) in having any such features. A small enclosed entry lobby has been modified and windows added to the main room. With the exception of two minor window frames, all of the windows have been added/altered. The interior of the building has been fully lined with new fittings and features and except for its form and external cladding the building has little integrity.

The building has a modest aesthetic interest (if only in comparison to the other spartan buildings of the establishment) and its historical contribution is in a general sense demonstrating the development of HMAS HUON over the years. The building's low internal and external integrity adversely impacts upon its potential importance.

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d. Building 5 (refer to Figures 3.13 and 3.14)

Buildings 5 and 6 are joined and effectively make up one building. Building 5 is a painted masonry amenities structure with a corrugated iron skillion roof and is made up of two separate buildings abutting each other. The southern portion was added in 1972 (Department of Works drawing). The northern portion appears early and is probably the one seen as a skillion roof building in the c. late 1920's photo of Hobart which is at Anglesea Barracks. Rendered or concrete lintels indicate original openings although the present doors and windows appear added. Internally, the northern portion has been upgraded at various stages and virtually no original fabric exists.

The southern portion of the building has no heritage significance. The northern portion of the building has no aesthetic interest and its historical contribution is only in a general sense demonstrating the overall development of HMAS HUON over the years. The building's low integrity adversely impacts upon its potential importance.

e. Building 6 (refer to Figures 3.15 - 3.17)

Building 6 is a painted masonry office/amenities building with unusual corrugated iron roofs sloping to the centre. The northern, two storey portion of this building was added in 1960 (Department of Works drawing). The southern portion is an earlier building and appears to be the building with a gable roof in the c.1920s photographs of Hobart which is in Anglesea Barracks. The two sets of windows and one door opening are original but one other door has been filled in. The interior has been had all walls removed and the perimeter walls and one new partition are lined with wood paneling. While the building is early, its integrity both externally and internally has been compromised except for basic elements.

The building has no aesthetic interest and its historical contribution is only in the general sense demonstrating the overall development of HMAS HUON over the years.

f. Building 7/8 (refer to Figures 3.18 - 3.25)

This is the major and earliest building on the site. Building 7 is a drill hall and building 8 is an attached residence. Building 7 is a large timber framed structure clad in weatherboards with a lower level area which has been excavated out below the main hall. The western portion of this underfloor area was excavated as part of the drill hall but the eastern portion was excavated at various stages since. A series of decks have been added to the north elevation at the main floor level. The eastern end of the drill hall's internal space has been filled in at various stages as well to create new rooms and offices. The overall integrity of the building is good with generally only additions rather than changes. The building (with its attached building 8) is the only one on the property which demonstrates any architectural pretence and in its design is readily appreciated for its 1912 date.

Building 8 is a two storey weatherboard residence also clad in weatherboards and attached to the eastern end of the drill hall (building 7). While portions of the interior have been upgraded, they demonstrate a high integrity with the joinery, fireplaces and pressed metal ceiling intact. Minor alterations have been made to accommodate shelving and an enlarged toilet. Several additions and alterations have been made to the northern portion to accommodate an Officers Mess.

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The building has a considerable and unusual aesthetic importance for a building of its type and age. Its historical contribution is also greater as the first building on the site and pivotal to all subsequent uses and activities on the site. The relative expression of this building's aesthetic qualities as compared with the other buildings on the site also enhance its importance.

g. Building 9 (refer to Figure 3.26)

This is a large timber framed, weatherboard clad boat shed of an undetermined date. It does appear that a boat shed occupied this site for many years although it is not shown on the original 1912 site plan. Drawings exist to indicate a construction date of 1929 but these same drawings indicate it was "demolished and replaced in 1961". Other drawings from the Department of Works indicate considerable works including the replacement of the roof trusses and gable end structure as well as the entire wall and roof cladding and new windows. The entire interior has been entirely lined and the floor timber floor shown on the above mentioned drawings has been replaced with concrete. The drawings make reference to earlier footings but it certainly appears that in itself it has little integrity to an earlier era.

The building has no aesthetic interest and its historical contribution is only in the general sense demonstrating the overall development of HMAS HUON over the years. It is unfortunate that as the only building on the site with a direct and obvious naval and maritime association its integrity is so low.

h. Building 10 (refer to Figure 3.27)

This is a small masonry structure which was last used for the storage of ammunition for the patrol boat when it was stationed at HMAS HUON. The exact date of the structure or its original use is unknown but a similar structure is shown in its location on the c.1920s photo at of Hobart in Anglesea Barracks.

The structure has no aesthetic interest and its historical contribution is only in general sense demonstrating the overall development of HMAS HUON over the years. As an ammunition locker, it has a closer function association to the site's military function, but the structure's purpose is not self-evident nor typical of an explosives store.

i. Buildings 11/12 (refer to Figure 3.17)

These are two masonry structures which abut each other. Their exact date is unknown but both are indicated on the WWII era site plan where they are referred to as a "ready use ammo locker".

The structure has no aesthetic interest and its historical contribution is only in a general sense demonstrating the overall development of HMAS HUON over the years. As an ammunition locker, it has a closer functional association to the site's military function, but the structure's purpose is not self-evident nor typical of an explosives store.

j. Building 13 (refer to Figure 3.28)

This is a modern, open carport which has been installed recently against building 2.

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The structure has no aesthetic or historical associations.

k. Buildings 15/16 (refer to Figures 3.28 and 3.29)

These are modern, prefabricated garden sheds which have been installed relatively recently.

The structures have no aesthetic or historical associations.

3.1.2 The Site (refer to Figures 3.1 - 3.7)

The site contains many non building features which are mostly of a relatively recent date. Some of these are attached to the buildings and others are free standing.

The landscaping generally is recent or of little interest. The c. late 1920s photograph at Anglesea Barracks only indicates gently sloping lawns down to the water. While the site contains numerous planting beds and shrubs, these are of no heritage interest. There are four large trees, one willow and three conifers, along the eastern boundary which while of no heritage interest, are dominant visual elements.

The tennis court is also used as a parade ground but its existence can only be dated to the c. 1943 drawing of the site. The flag mast below the tennis court is also indicated on that drawing but the c. late 1920s photograph at Anglesea Barracks indicates a ceremonial one just outside the western end of building 7.

The existing jetty can not be dated but there are indications that it has been extensively repaired and certainly extended.

Some buildings indicated on earlier drawings have been removed at unknown dates, particularly along the western boundary.

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3.2 PLANS

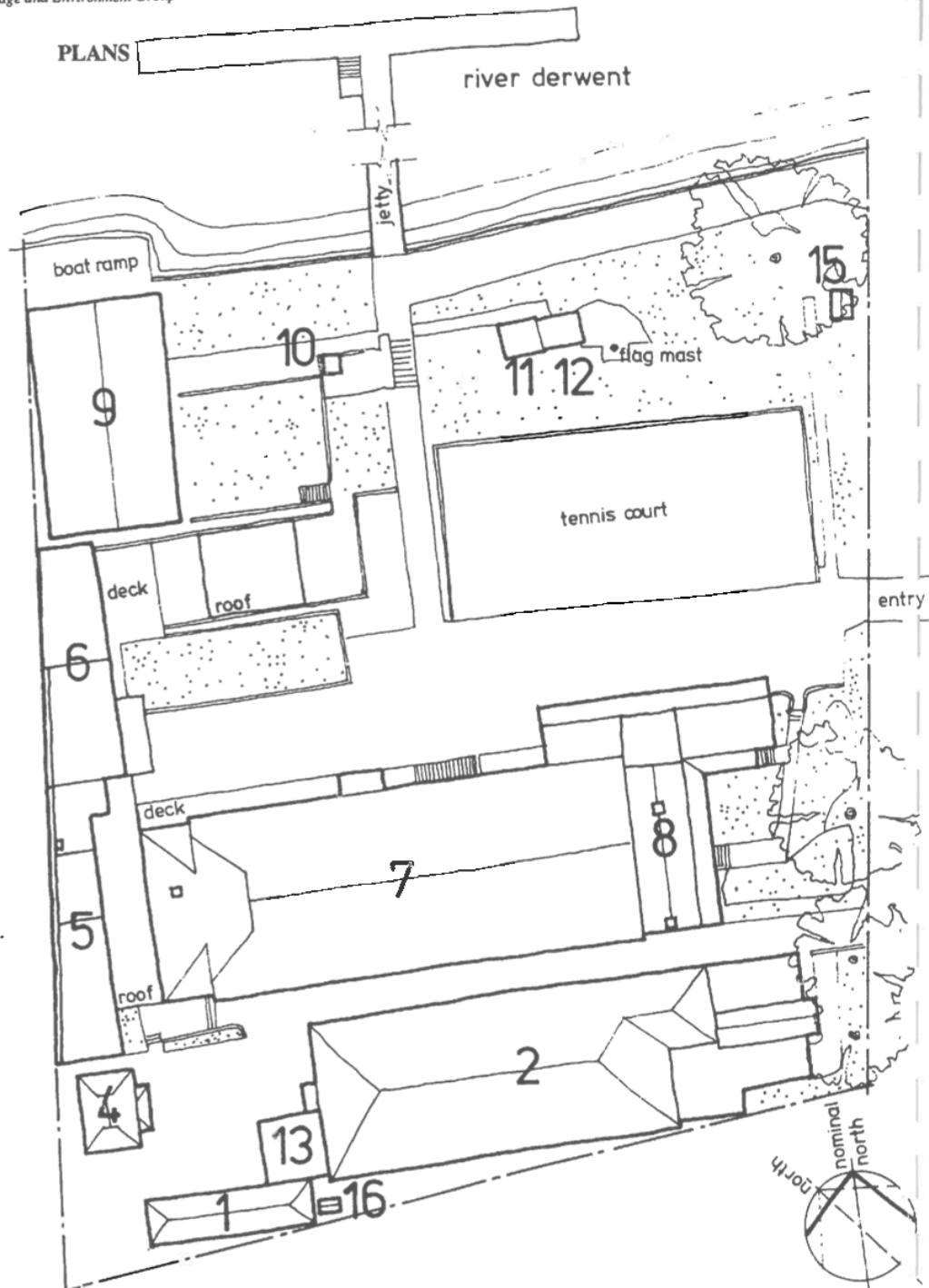


FIGURE 3.1 HMAS HUON, HOBART
as existing site plan, with building numbers (buildings numbered 3 and 14 do not exist)

WESTERLYN INTERSECTION STUDIES
The City of Hobart Council

3.2 PHOTOGRAPHS

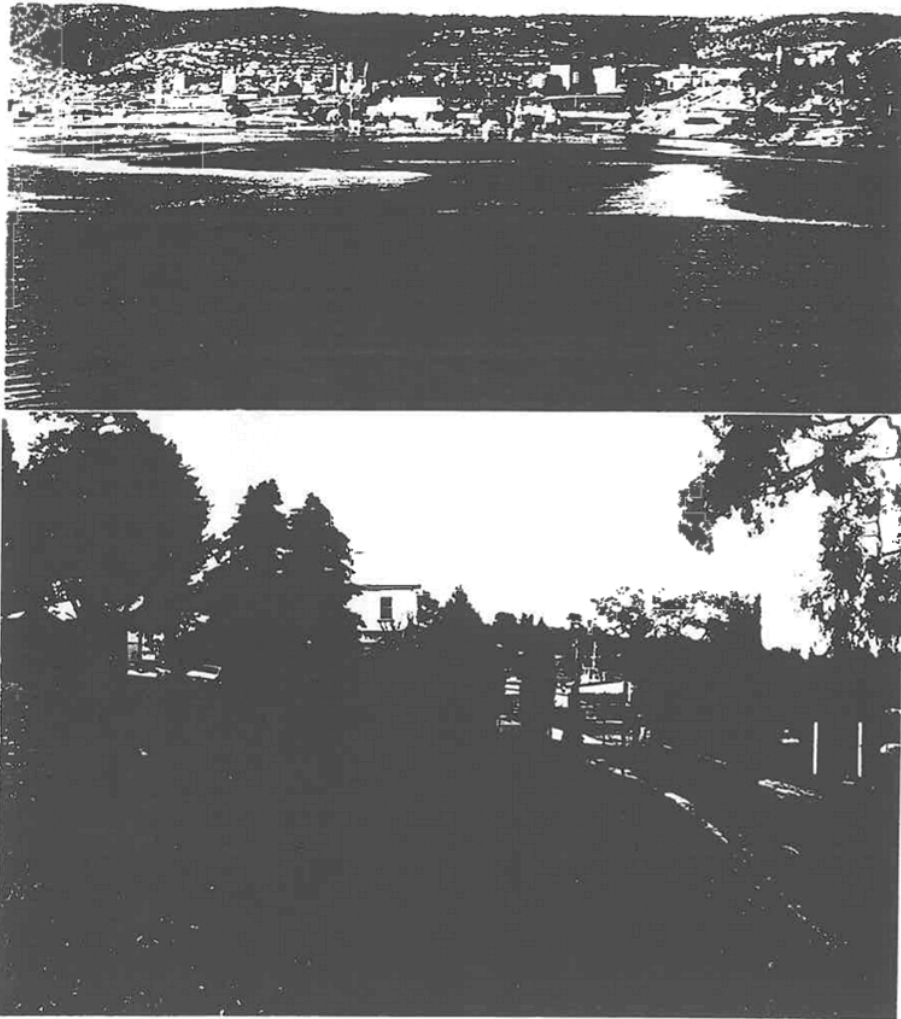


FIGURE 3.2 HMAS HUON, HOBART

top: HMAS HUON from the Tasman Bridge, main white building is the Drill Hall and the Hobart Marine Board slip ways are to the right
bottom: entry from the east

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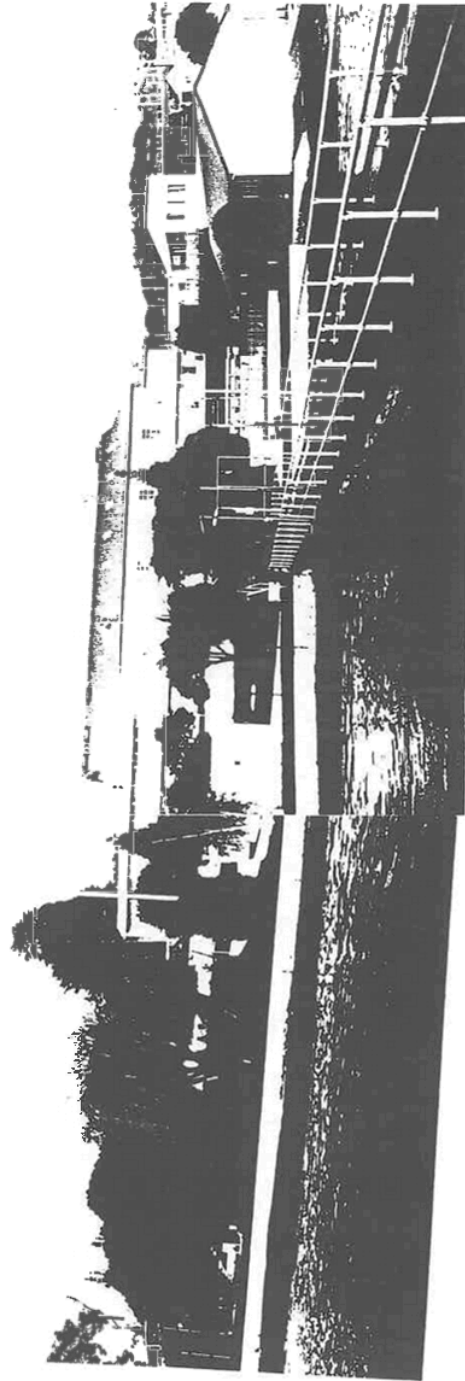


FIGURE 3.3 HMAS HUON, HOBART
composite photograph from the jetty

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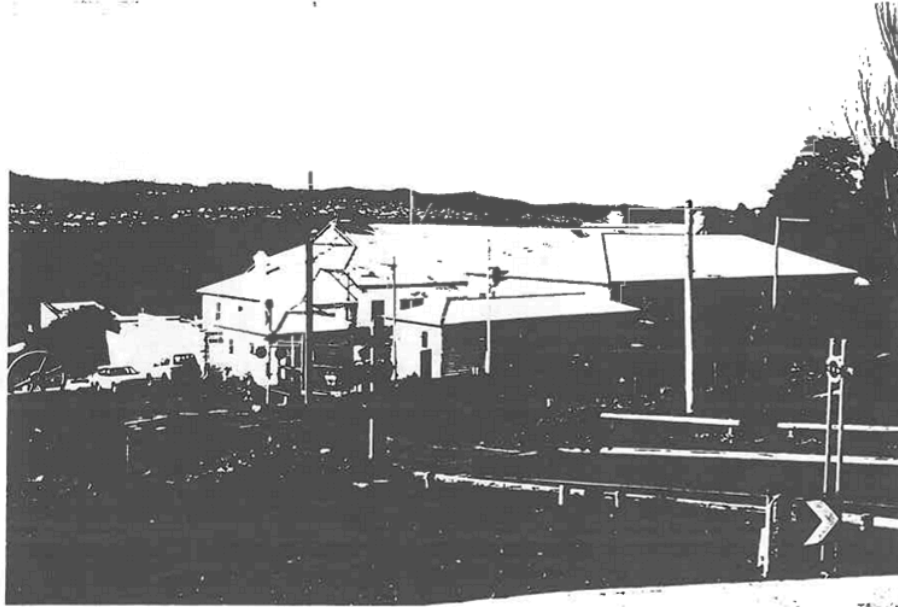


FIGURE 3.4 HMAS HUON, HOBART
top: view looking north-east
bottom: jetty

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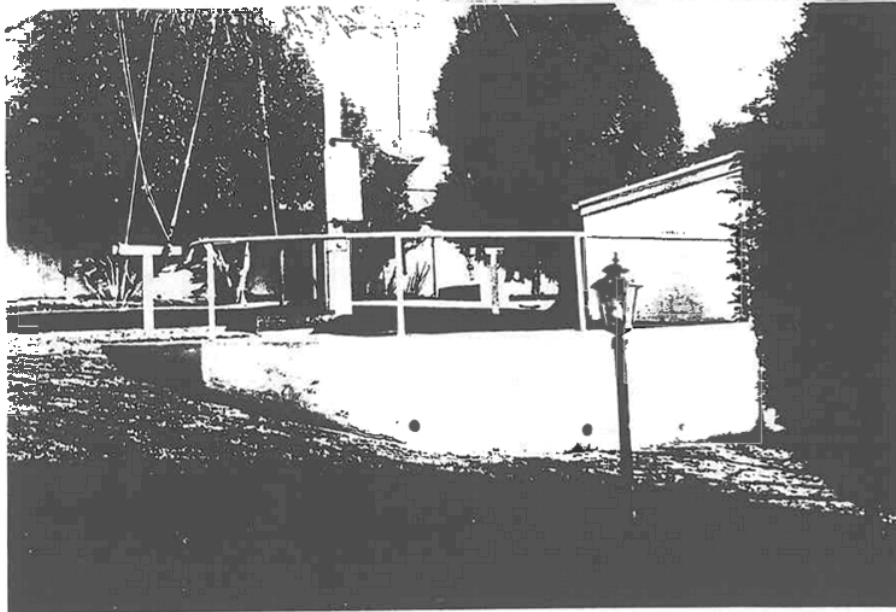


FIGURE 3.5 HMAS HUON, HOBART
top: flag mast
bottom: flag mast base (buildings 11/12 adjacent)

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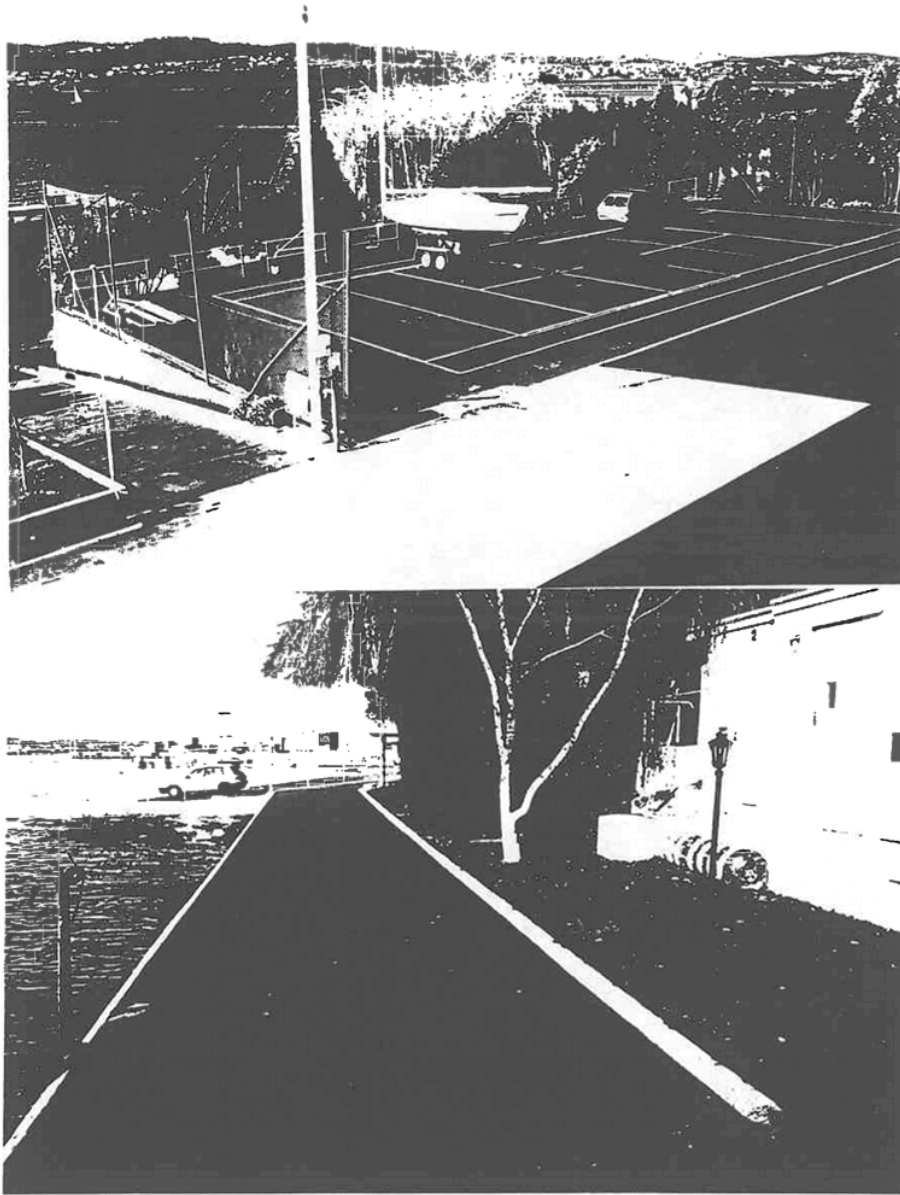


FIGURE 3.6 HMAS HUON, HOBART
top: tennis court
bottom: foreshore paving (buildings 11/12 on the right)

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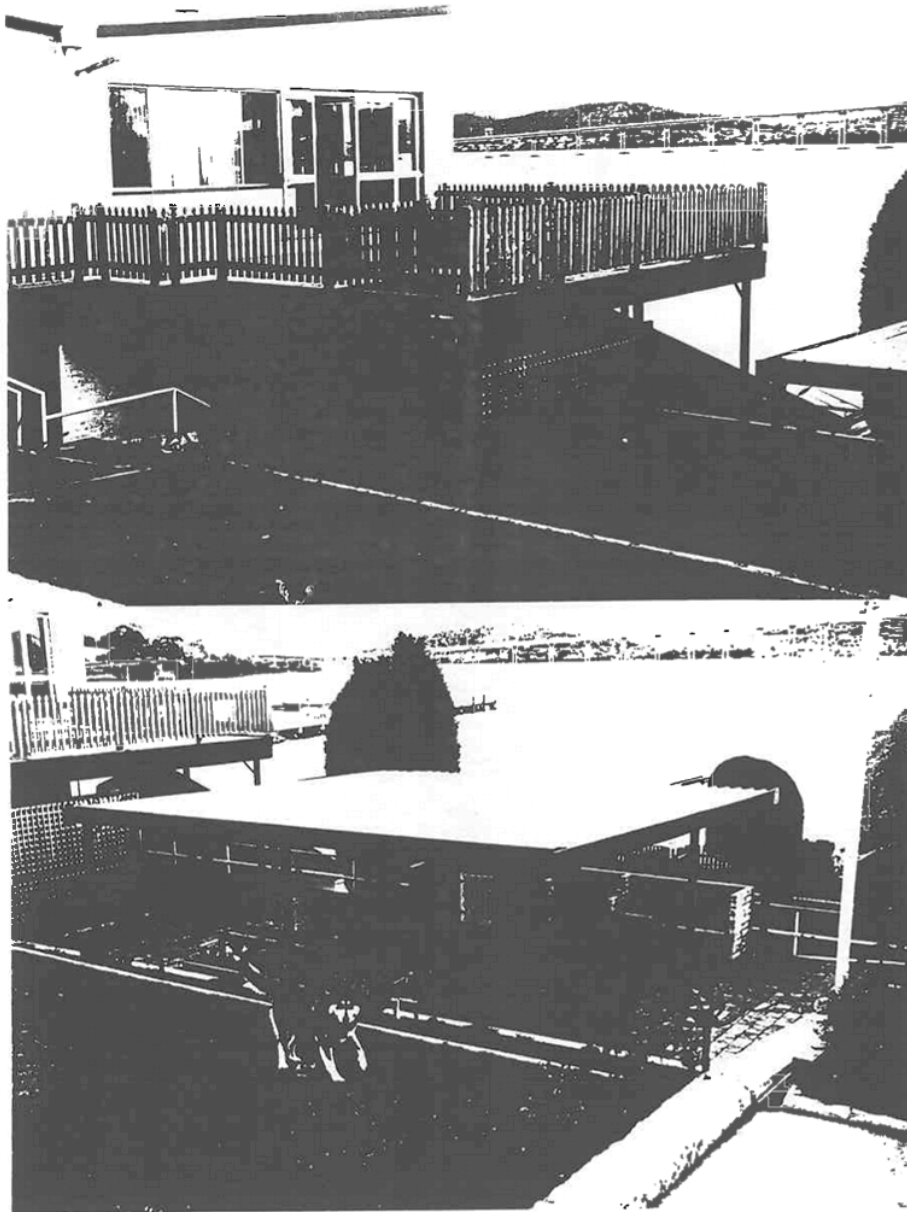


FIGURE 3.7 HMAS HUON, HOBART
top: deck attached to building 6
bottom: roofed barbecue area adjacent to building 6

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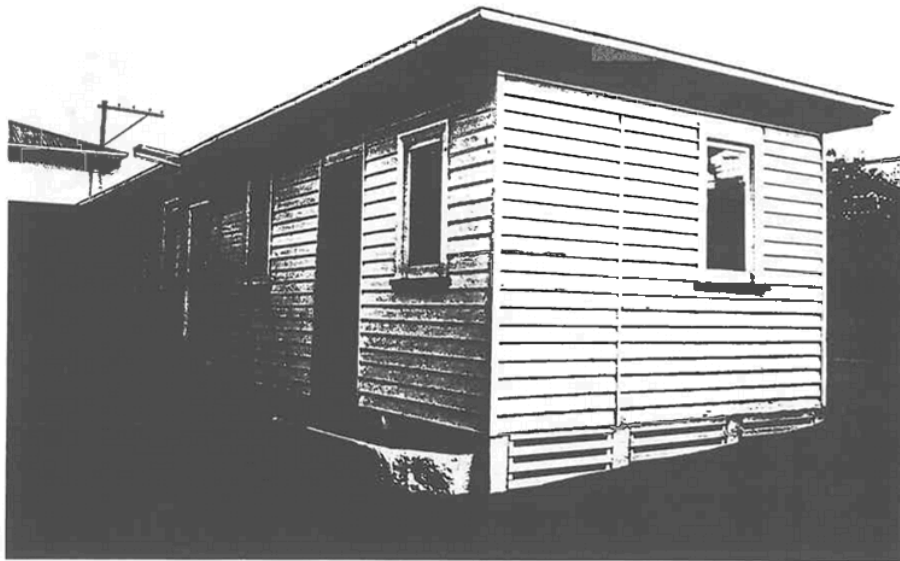
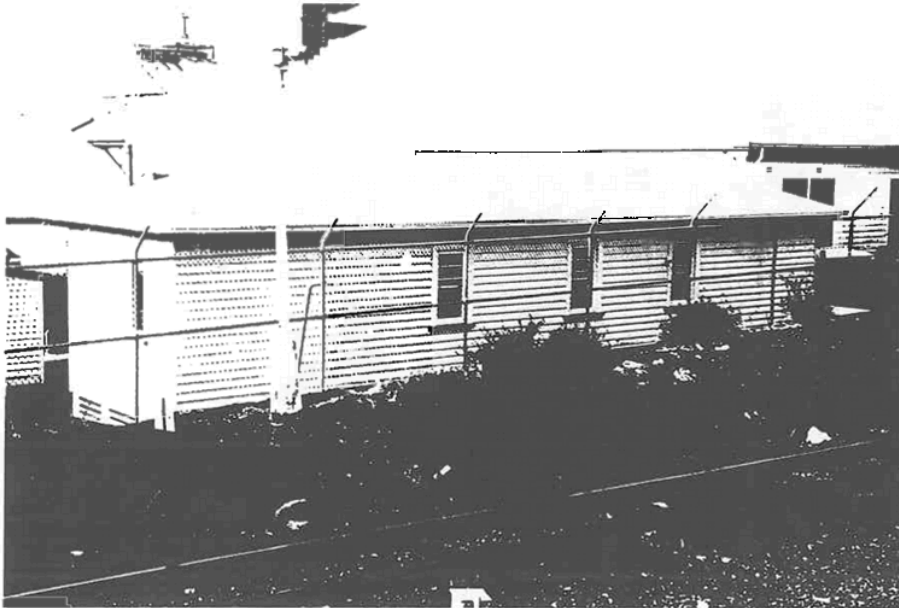


FIGURE 3.8 **HMAS HUON, HOBART**
top: building 1, south elevation
bottom: building 1, west elevation

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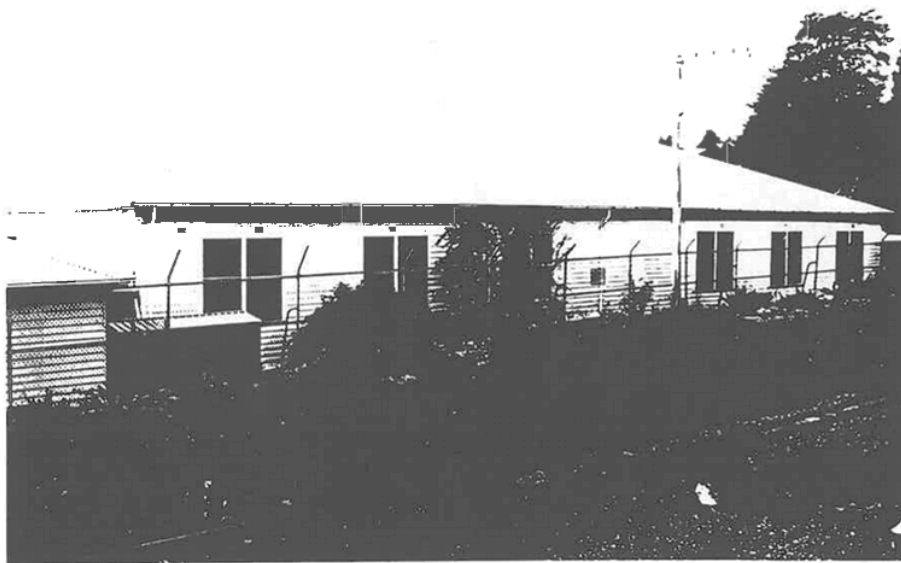
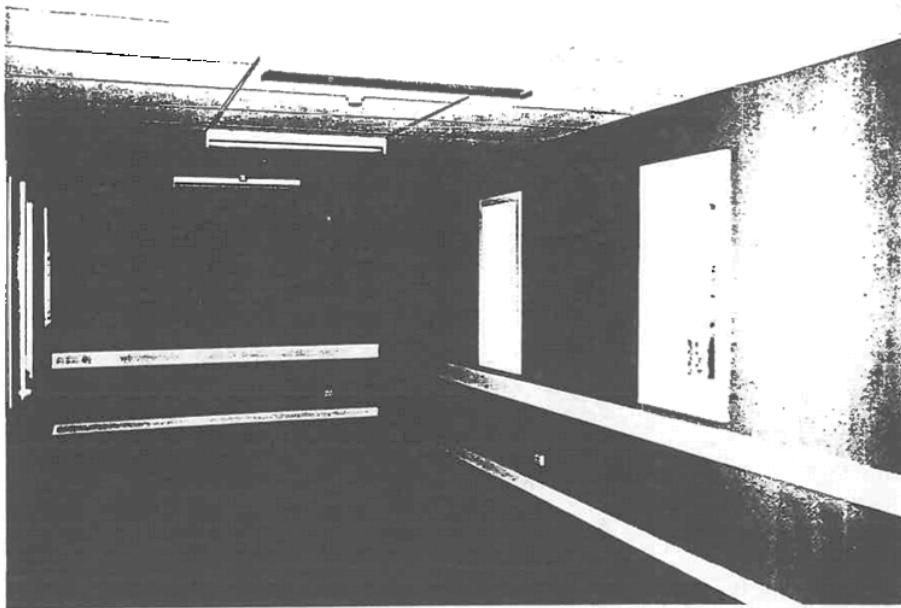


FIGURE 3.9 HMAS HUON, HOBART

top: building 1, interior

bottom: building 2, part of south elevation

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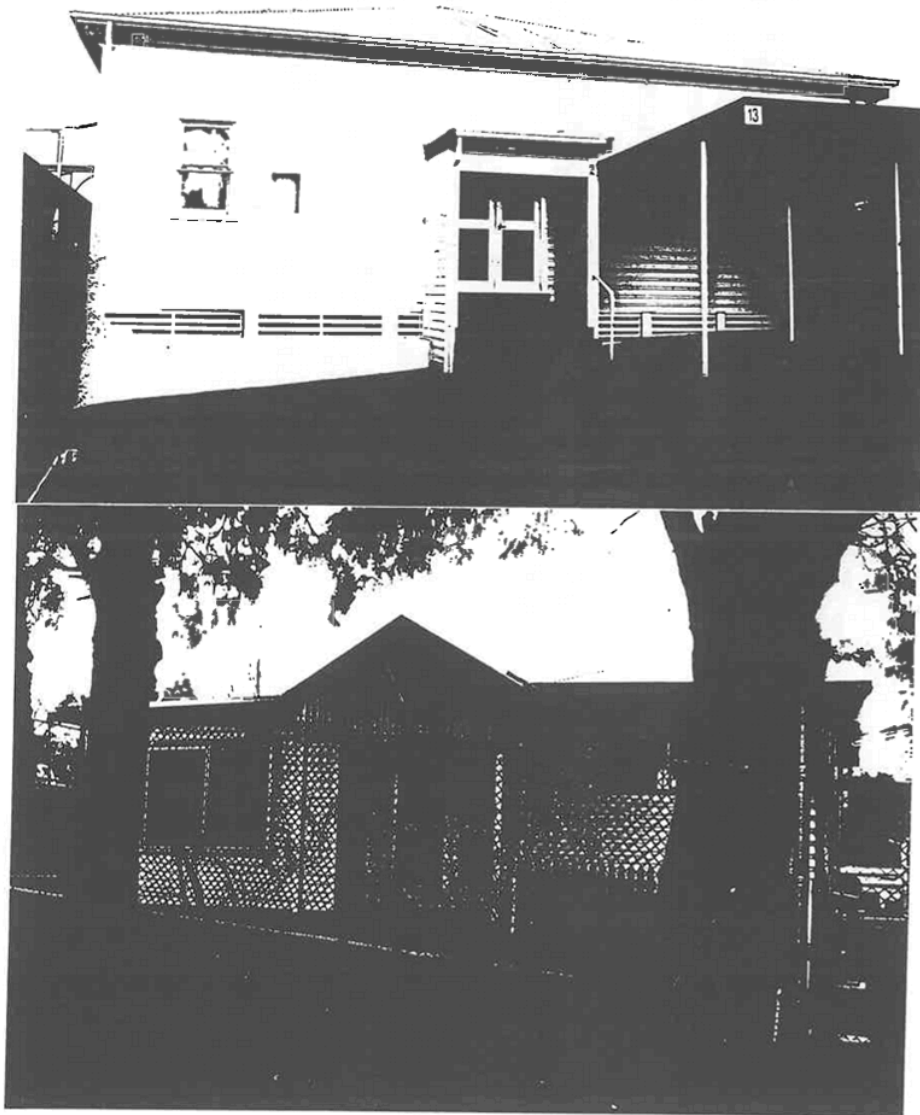


FIGURE 3.10 HMAS HUON, HOBART

top: building 2, west elevation (building 13 - carport - adjacent)

bottom: building 2, east elevation

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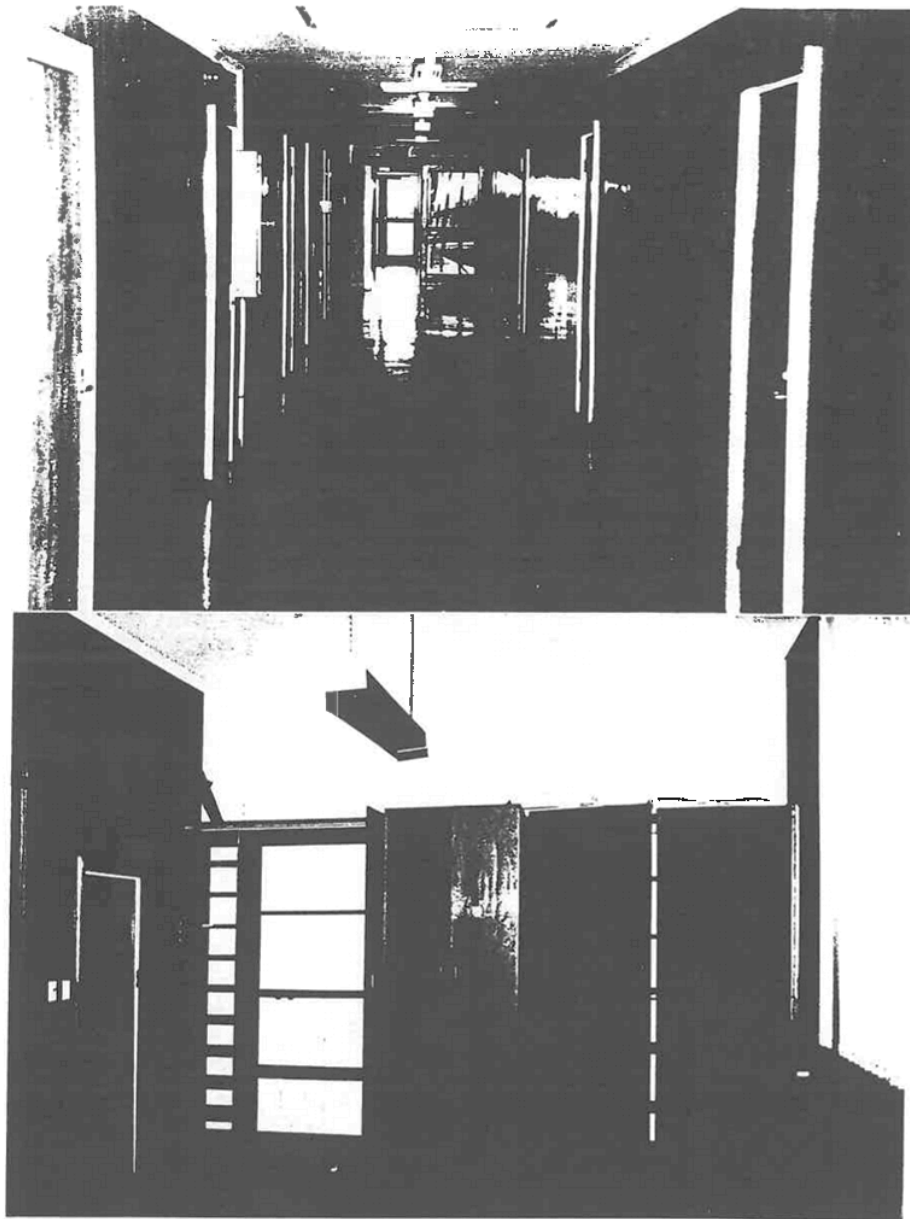


FIGURE 3.11 HMAS HUON, HOBART

top: building 2, central corridor

bottom: building 2, latter wall paneling on left and earlier boards on right

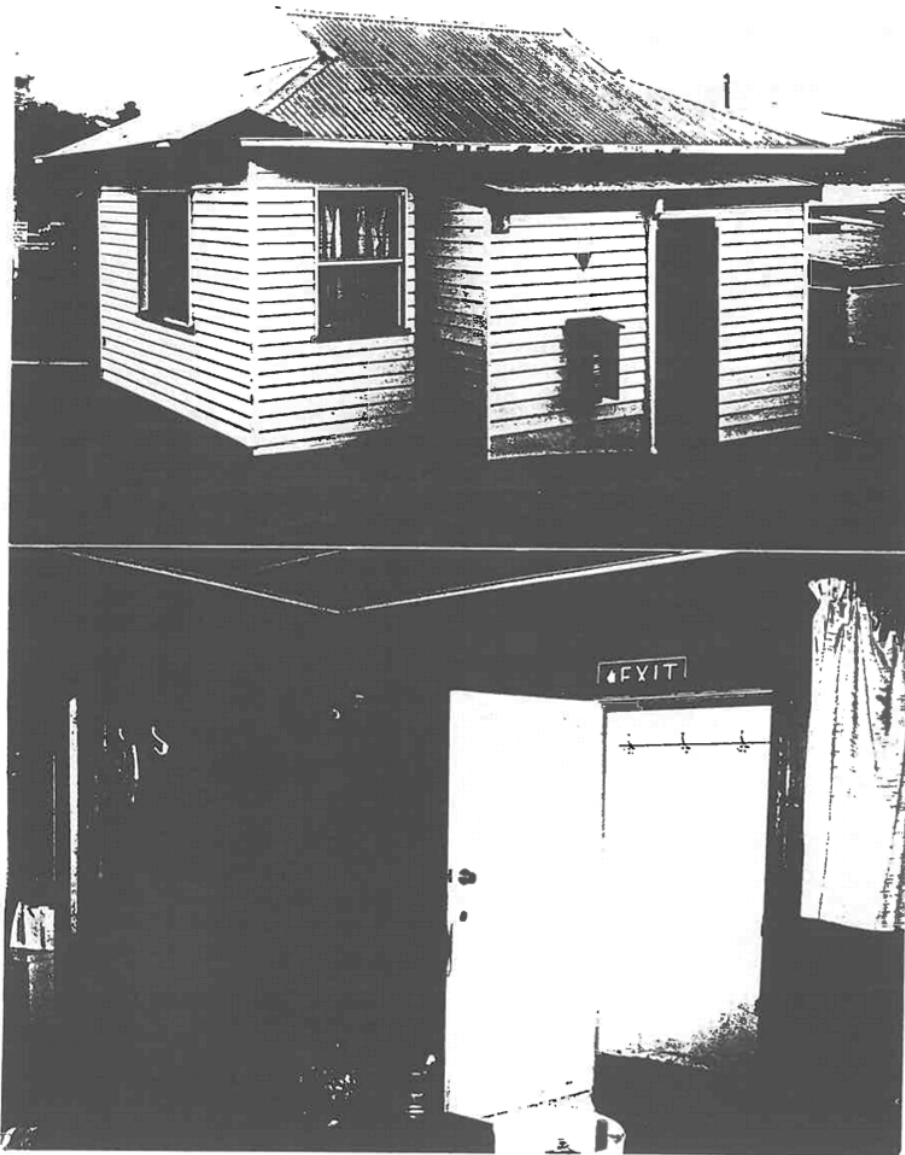


FIGURE 3.12 HMAS HUON, HOBART

top: building 4, south-east corner

bottom: building 4, interior of main room

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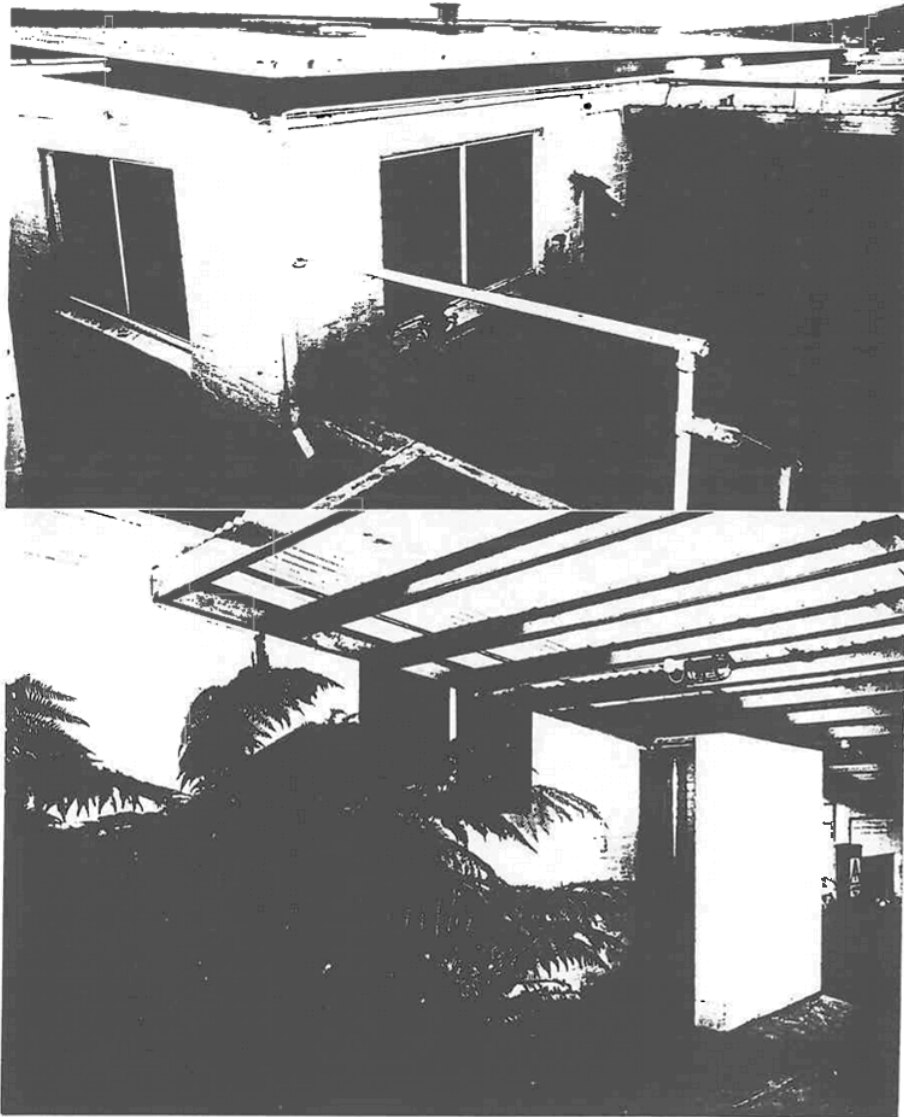


FIGURE 3.13 HMAS HUON, HOBART
top: building 5, southern portion
bottom: as above, under adjacent roof

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FIGURE 3.14 HMAS HUON, HOBART

top: building 5, northern portion under adjacent roof
bottom: as above outside the roofed area

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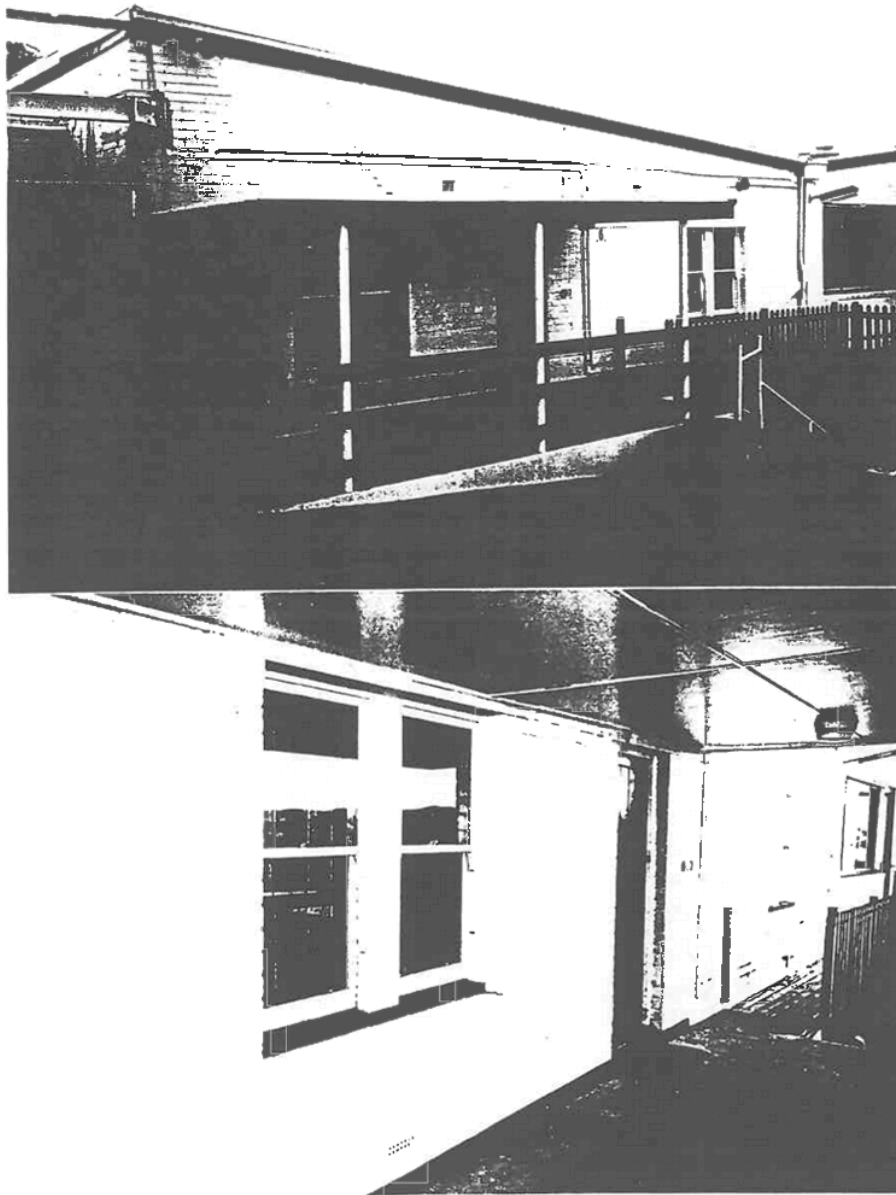


FIGURE 3.15 HMAS HUON, HOBART

top: building 6, southern portion with adjacent roofed area
bottom: as above under the roofed area

AUSTRALIAN CONSTRUCTION SOCIETY
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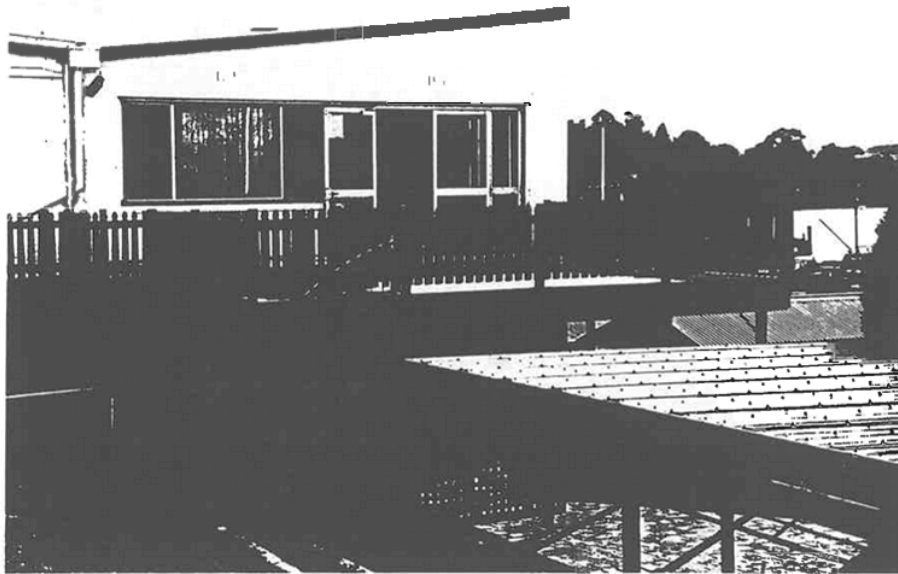
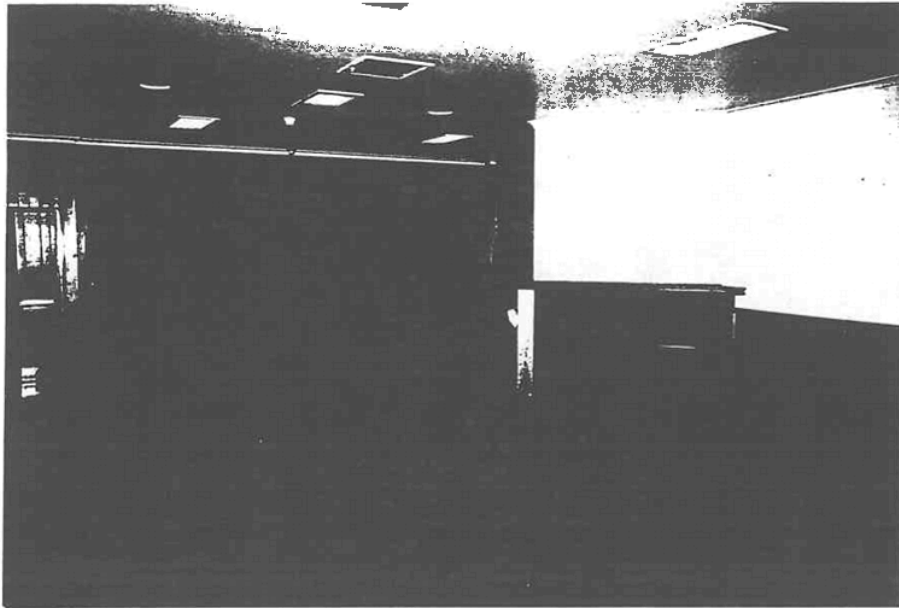


FIGURE 3.16 HMAS HUON, HOBART
top: building 6, southern portion interior
bottom: building 6, northern portion

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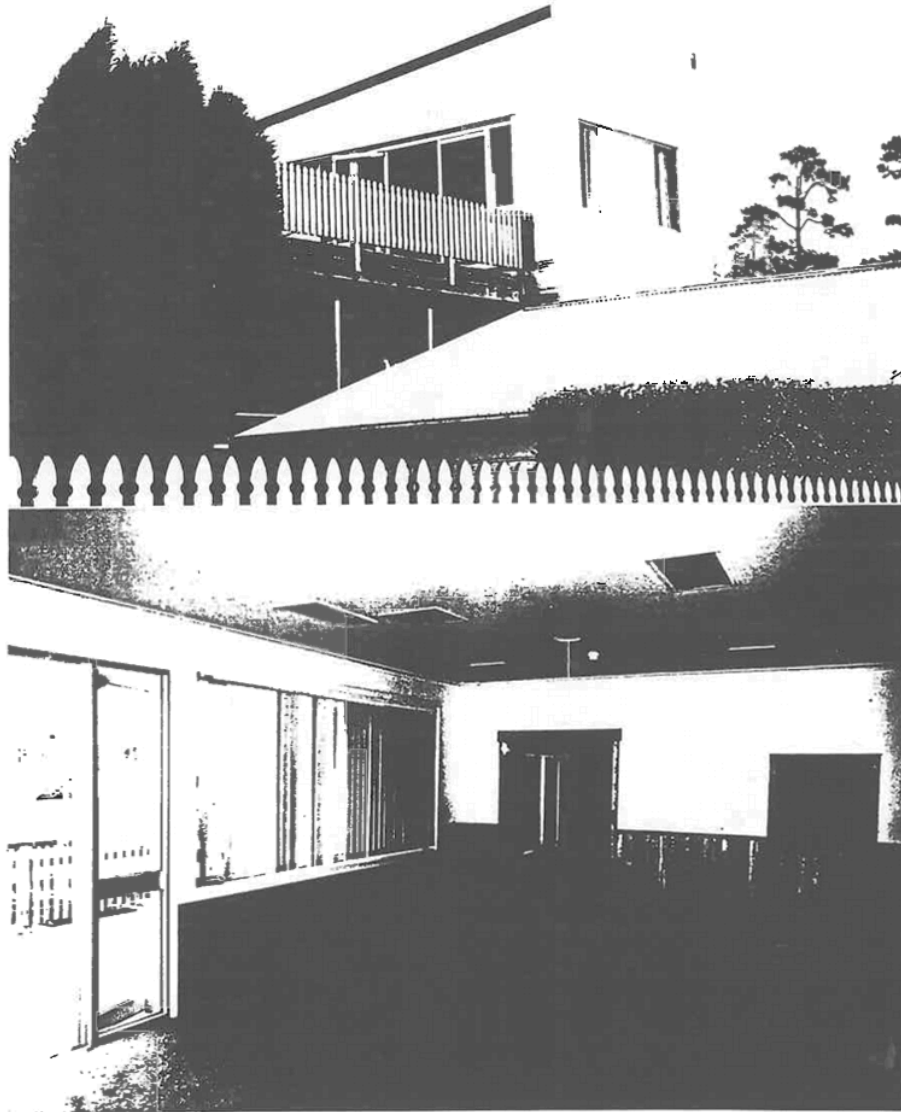


FIGURE 3.17 HMAS HUON, HOBART

top: building 6, northern portion over the roof of building 9
bottom: building 6 interior

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FIGURE 3.18 HMAS HUON, HOBART
building 7, composite photograph of north elevation

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FIGURE 3.19 HMAS HUON, HOBART

top: building 7, south elevation

bottom: building 7, original double door and adjacent original double window

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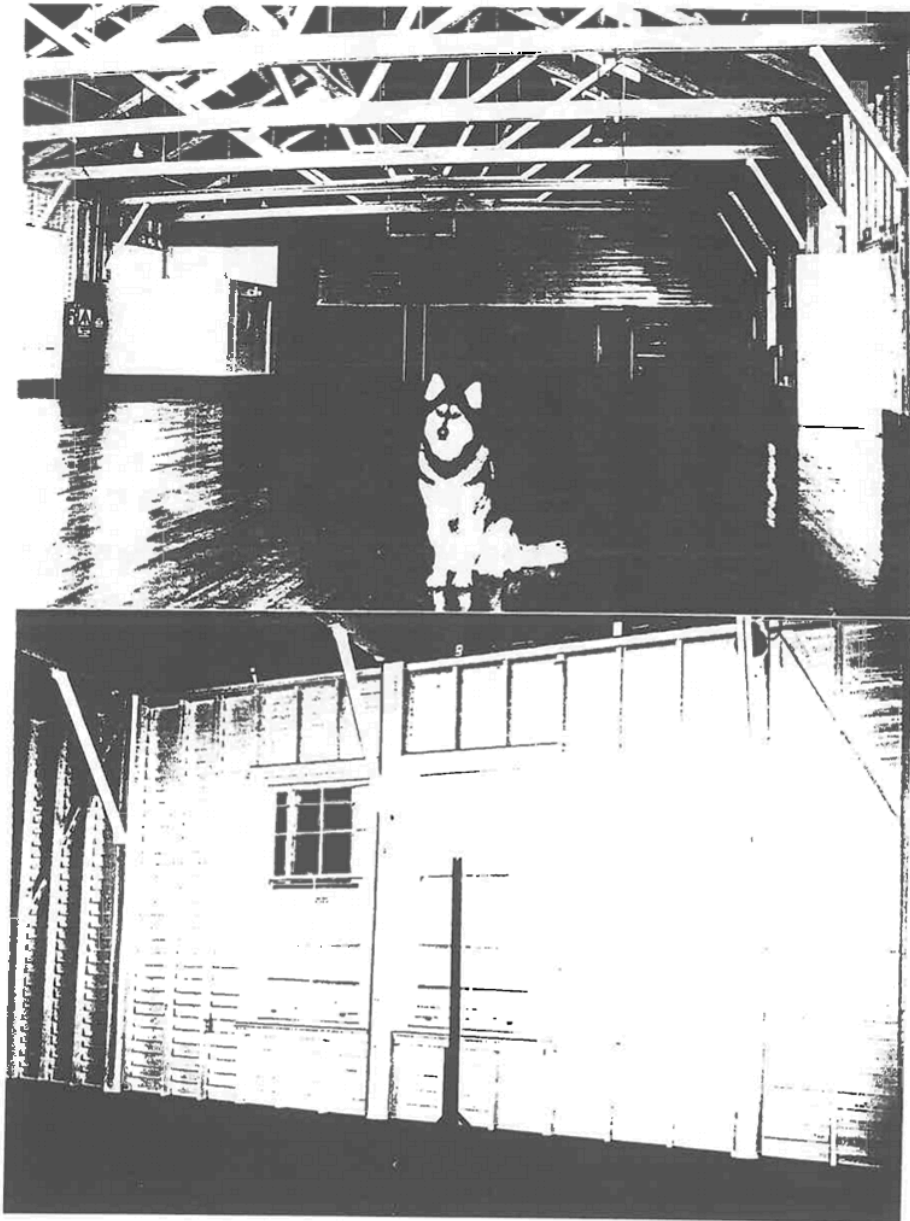


FIGURE 3.20 HMAS HUON, HOBART

top: building 7, general view of interior looking west
bottom: building 7, internal wall of south elevation

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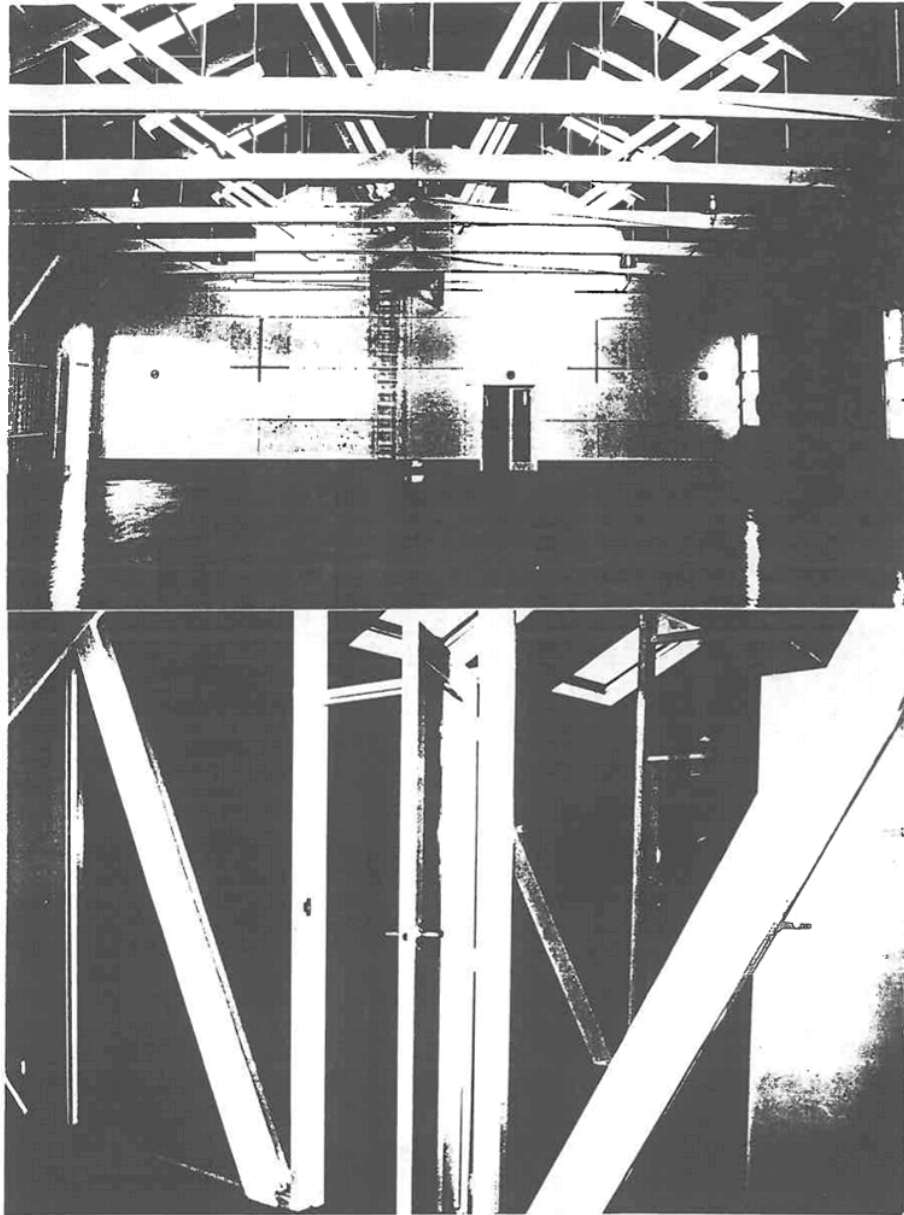


FIGURE 3.21 HMAS HUON, HOBART

top: building 7, general view of interior looking east

bottom: building 7, glazed partitions within roof trusses at east end

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FIGURE 3.22 HMAS HUON, HOBART

top: building 7, interior of lower area excavated later

bottom: building 7, interior of lower area originally excavated but fitted out later

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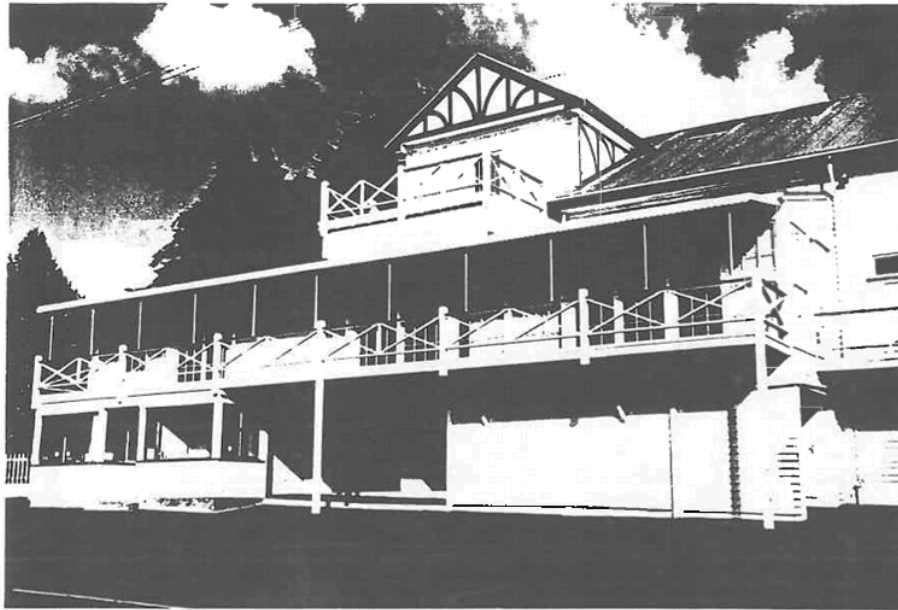


FIGURE 3.23 HMAS HUON, HOBART

top: building 8, north elevation

bottom: building 8, upper portion of south elevation over the roof of building 2

ANZAC AND ASSOCIATION STREETS
Heritage and Conservation Group

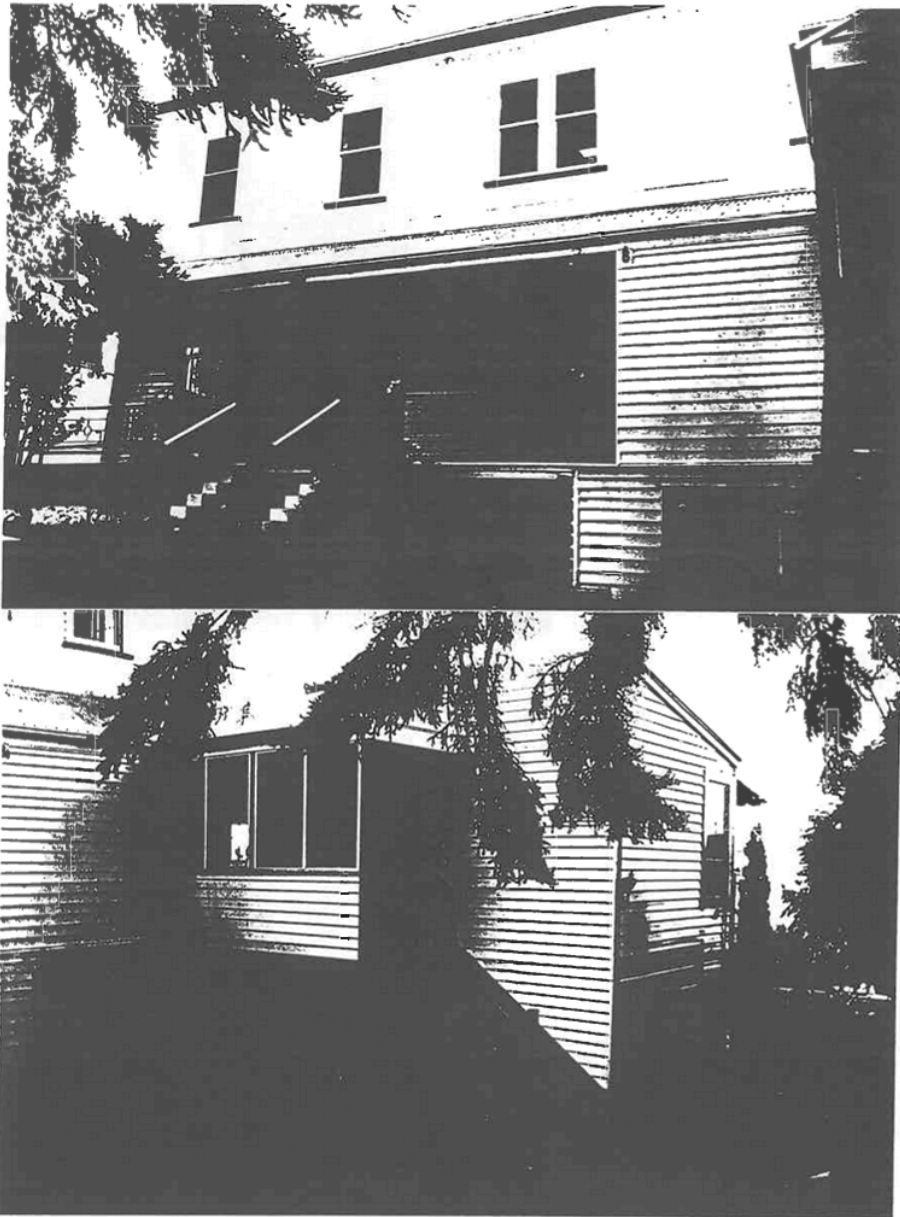


FIGURE 3.24 HMAS HUON, HOBART

top: building 8, east elevation

bottom: building 8, added projection on north-east corner

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Heritage and Environment Group

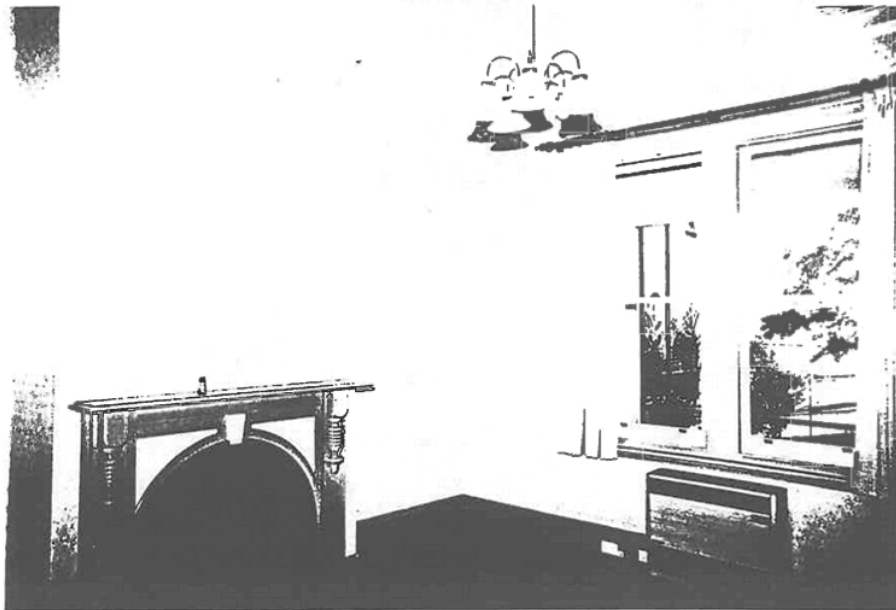
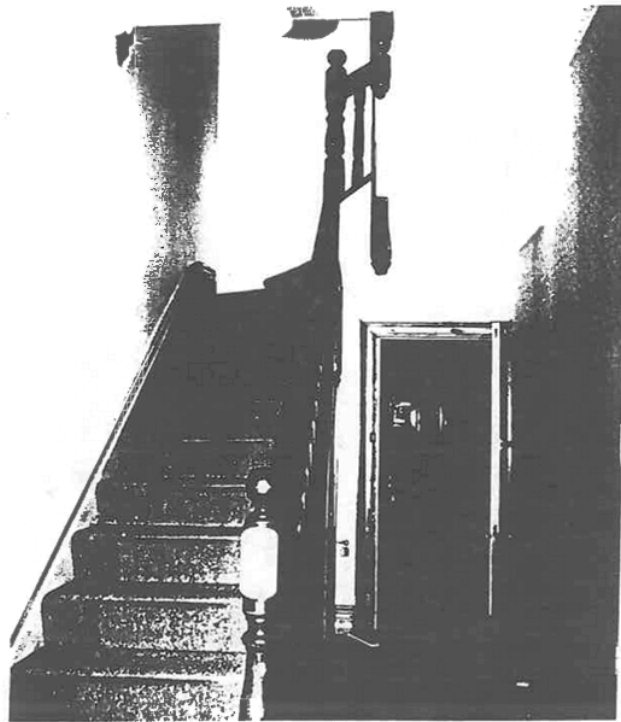


FIGURE 3.25 HMAS HUON, HOBART

top: building 8, staircase at ground floor level

bottom: building 8, main room on ground floor, others similar

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FIGURE 3.26 HMAS HUON, HOBART

top: building 9, from the jetty

bottom: building 9, interior of office area

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FIGURE 3.27 HMAS HUON, HOBART

top: building 10 - on right, lower structure to the left is an underground tank
bottom: buildings 11/12, north-west corner

AUSTRALIAN CONSTRUCTION
Heritage and Environment

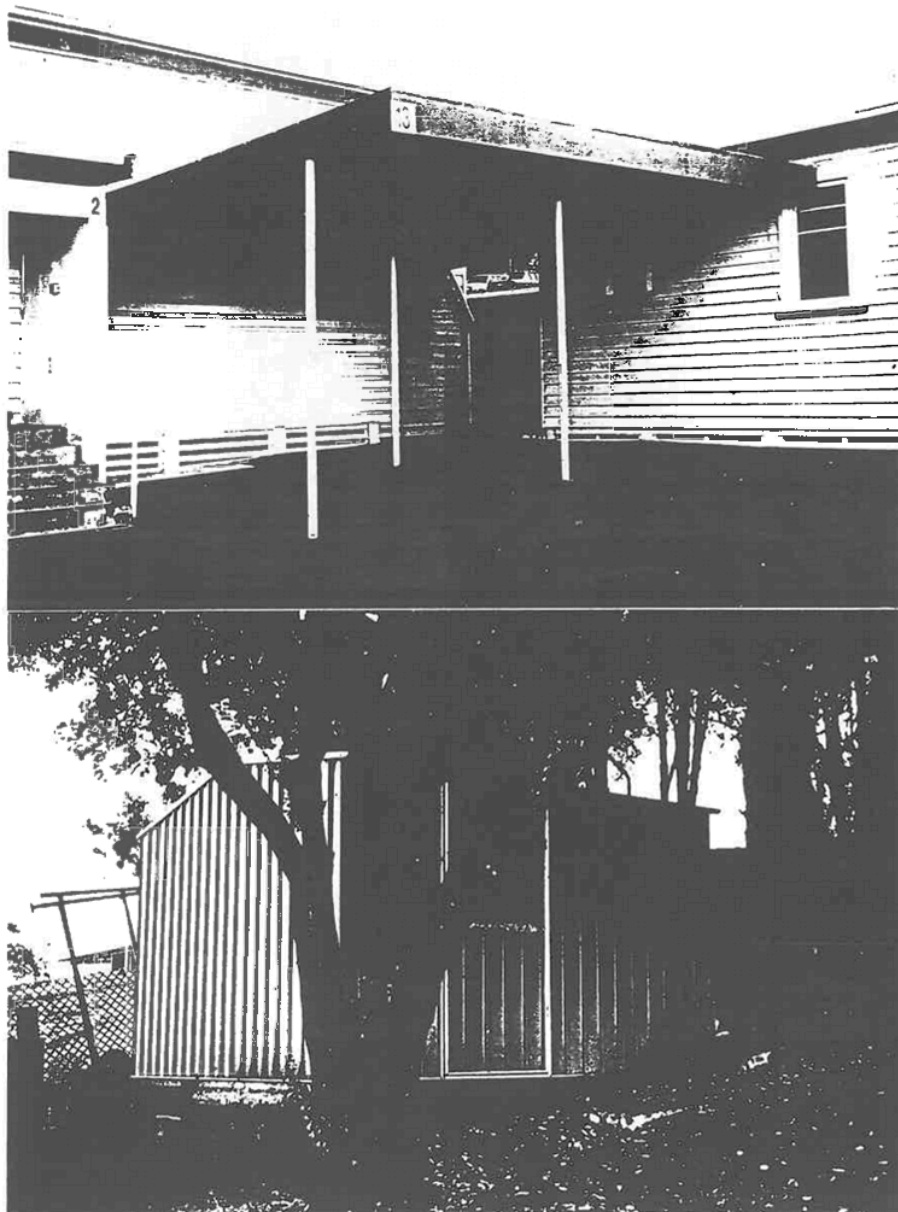


FIGURE 3.28 HMAS HUON, HOBART

top: building 13, carport

bottom: building 15, garden shed

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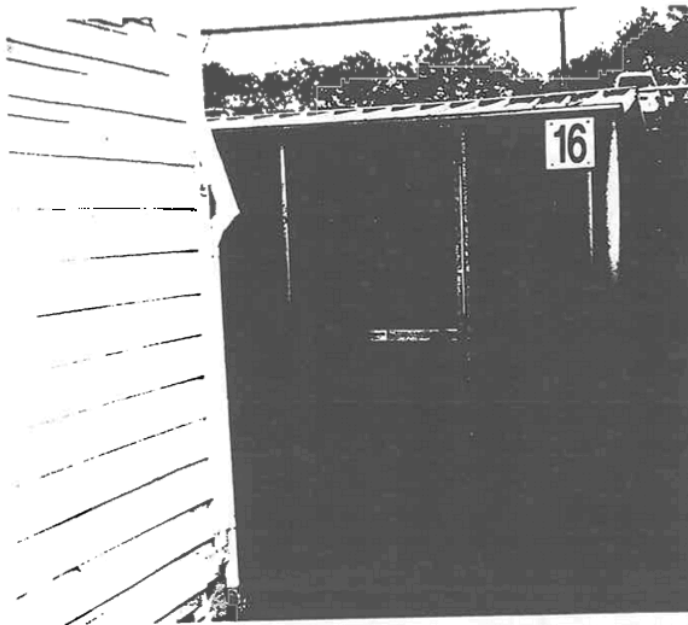


FIGURE 3.29 HMAS HUON, HOBART

top: building 16 garden shed
bottom:

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4.0 STATEMENT OF CULTURAL SIGNIFICANCE

4.1 DEFINITIONS

- a. The Australia ICOMOS *Burra Charter* defines 'cultural significance' as:
"... aesthetic, historic, scientific or social value for past, present and future generations."
- b. The following categories of significance are based upon the Australian Heritage Commission's criteria for inclusion on the Register of the National Estate.
- c. In the following Statement of Significance, the use of the terms 'significant', 'important' and 'of interest' is deliberate and intended to imply a hierarchy of merit relative to the respective criteria being considered.

4.2 STATEMENTS

4.2.1 *"its importance in the course or pattern of Australia's natural or cultural history"*

HMAS HUON is *important* as a reflection of the Commonwealth's compulsory military training scheme after Federation and in establishing a military (and particularly naval) presence in Tasmania.

4.2.2 *"its possession of uncommon, rare or endangered aspects of Australia's natural or cultural history"*

HMAS HUON is *important* as the only naval establishment (Commonwealth or State) in Tasmania.

4.2.3 *"its potential to yield information that will contribute to an understanding of Australian's natural or cultural history"*

HMAS HUON is *of interest* as an expression of the Commonwealth's compulsory military training schemes and also as an expression of the use of the respective state public works departments to undertake Commonwealth public works projects during the early years of the Commonwealth's evolution. These aspects are reflected in other states as well but in Tasmania HMAS HUON is a notable expression of these historical aspects.

4.2.4 *"its importance in demonstrating the principal characteristics of:*

- . a class of Australia's natural or cultural places;*
- . a class of Australia's natural or cultural environment"*

This category is not directly relevant to the significance of HMAS HUON.

4.2.5 *"its importance in exhibiting particular aesthetic characteristics valued by a community or cultural group"*

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HMAS HUON is *of interest* for the design of the drill hall and residence (buildings 7/8) in comparison with other Tasmanian drill halls or similar buildings.

4.2.6 *“its importance in demonstrating a high degree of creative or technical achievement at a particular period”*

This category is not directly relevant to the significance of HMAS HUON.

4.2.7 *“its strong or special associations with a particular community or cultural group for social, cultural or spiritual reasons”*

This category is not directly relevant to the significance of HMAS HUON. (This is not to imply any disassociation of the site and the people of Tasmania but it is felt this association is implied elsewhere in these statements.)

4.2.8 *“its special association with the life or works of a person, or group of persons, of particular importance in Australia’s natural or cultural history”*

This category is not directly relevant to the significance of HMAS HUON.

4.3 ASSOCIATED FABRIC

- a. The following physical elements are categorised according to their contribution to the significance of the property. In a philosophical sense, all of the elements on the site contribute to its significance but a judgement has been made to critically consider the relevant contributions. The contribution is not solely based upon age, but also includes their inherent historical and physical qualities.

b. **Crucial** to the significance:

- . **7/8** The drill hall and residence are the first buildings at HMAS HUON and they are also the most distinctive from an architectural aspect. They reflect the involvement of the Tasmanian public works department, training nature of the establishment and also have a relatively high integrity which permits the expression of their original character

c. **Associated** with the general significance:

- . **1** This is early building although it has no aesthetic qualities of merit or specific historical associations. The building has a low relative integrity.
- . **2** This building has no inherent aesthetic qualities of merit but can be associated with the development of HMAS HUON during WWII. This building has a low relative integrity.
- . **4** This is an early building although it has little aesthetic qualities (except for its gabled roof) and has no specific historical associations. This building has a relatively low integrity.
- . **5/6 (central portions)** These are remnants of early amenities buildings with no aesthetic qualities of merit or specific historical associations. These buildings have been

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dramatically extended and altered, particularly internally, but also with a complete change to the roofs to building 6.

- . 9 This is the remnant of an early building (pre-WWII) which is the only building with a direct physical association to the river and the site's naval tradition.

d. **Little or no reflection of the significance:**

- . 5/6 (southern and northern portions) These are post-WWII buildings with no inherent aesthetic qualities or historical associations.
- . 10 While this appears to be an early (pre-WWII) structure, it is minor with no inherent aesthetic qualities or historical associations.
- . 11/12 These structures probably date from WWII but they are minor with no inherent aesthetic qualities or historical associations.
- . 13 This is a modern structure with no aesthetic qualities or historical associations.
- . 15/16 These are modern structures with no aesthetic qualities or historical associations.

4.4 AREAS FOR FURTHER RESEARCH

4.4.1 The following aspects could be considered in more detail. It is not felt that these aspects will significantly alter the above statements but could enhance and/or clarify them.

- . Why was this site chosen for the naval drill hall - were other sites considered?
- . What was the effect of the end of compulsory training on the operation of HMAS HUON?
- . What was the wartime role of HMAS HUON - the existing information only provides inferences
- . What kind of training was provided at HMAS HUON?

4.4.2 A more comprehensive view of the Commonwealth's post-Federation naval and military history nationally could place HMAS HUON in a more definitive context.

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PART TWO - THE CONSERVATION PLAN

5.0 CONSERVATION POLICIES

5.1 INFLUENCES ON POLICY DEVELOPMENT

5.1.1 User Requirements

- a. As stated previously, the Navy has vacated the property although they maintain a small presence in building 9 - the boat shed - by a diving team. It is understood that a condition of the disposal of the property will require that presence to be maintained.
- b. The Department of Defence has had discussions with the Hobart Marine Board in reference to their purchase of the site. The Marine Board own the adjacent slip ways where boat maintenance is undertaken. The Marine Board has stated that its intention is to lease the majority of the site to the Water Police although it anticipates using some of the accommodation available for its own office uses. The Marine Board has stated that the Water Police are intending to vacate their premises in Sullivan's Cove to free up that area for commercial enterprise. It is understood that an architect has been engaged to address the requirements of the Water Police and that it is proposed to construct a new building in front of buildings 6 and 7. The authors have not seen the proposal or a brief upon which it may be based.
- c. The City of Hobart has also expressed informally an interest in the site but has no proposals upon which to base any uses or offers. Officers informally expressed concern that Marine Board ownership of the HMAS HUON site may result in the relocation of the container facilities currently in Sullivan's Cove and the extensive redevelopment of that shoreline. It should be noted in fairness that there is no evidence of this intent.
- d. The Australia ICOMOS *Burra Charter* defines a compatible use as:
"...a use which involves no change to the culturally significant fabric, changes which are substantially reversible, or changes which require a minimal impact."
 Section 6.0 Implementation will provide more details as to the culturally significant fabric and the types of works which would be considered appropriate.
- e. A use which maintains an association with the Royal Australian Navy would be desirable but a use which maintains an association with the water would be equally appropriate and encouraged. The proposal to use the site for the Water Police would certainly appear appropriate from this point of view as would any similar water oriented activities such as a rowing club, sailing or power boat club, or even boat repairs.

The large internal space of the drill hall and the magnificent views from it across the River Derwent to the Tasman Bridge would also seem to be ideal for a function venue, although the properties somewhat isolated location may work against this. As a large space though the main building at least may also lend itself to sports or fitness related functions.

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- e. None of the above comments are a comment on the viability of such functions, only their compatibility. The issue of viability concerns other issues as well as conservation and is beyond the scope of this report.

In the detail consideration of uses for the property, it may be necessary to consider the demolition of buildings or structures of lesser significance in order to focus limited funds or activities into those of greater significance. The hierarchy of buildings in section 4.3 **Associated Fabric** provides an indication for that purpose.

Viability may also require multiple users of the property but if such a scenario eventuates, the integrity, visual treatment and appreciation of the site as a whole should dominate any considerations of individual identity or desires. The property should in such a situation remain under the ownership and control of one group/person/authority with leases or possibly strata titles to accommodate multiple uses.

Viability may also require new structures to be built. The location and appropriateness of such structures should none the less respect the significance and visual amenity of the property. The following Guidelines address new buildings. Before any new buildings are initiated however, the existing buildings should be critically considered to determine whether they can provide the necessary accommodation, even with alteration to the 'associated' buildings.

5.1.2 Building Conditions

- a. The buildings are in remarkably good condition and in most instances the maintenance required is only minor such as painting. The only serious building fault which was noticed was vertical cracking in the southern portion of building 6 which appears relatively recent.
- b. The condition of the buildings and the other elements on the site indicate that they are suitable for continued use and do not present any dangers to safety.

5.1.3 Defence Policy DI(G) 40-1

- a. DI(G) 40-1 is not a statutory document but is an internal Department of Defence document which highlights the statutory obligations under Commonwealth legislation. DI(G) 40-1 identifies that Defence has heritage obligations under both the *Australian Heritage Commission Act 1975* and the *Environment Protection (Impact of Proposals) Act 1974* as well as minor addition legislation which would not apply to the HMAS HUON situation.
- b. Crucial to the application of both of the Acts mentioned above is the inclusion of the property on the Register of the National Estate. As stated previously, HMAS HUON is not currently included. DI(G) 40-1 does state in section 5 however,
"It should also be noted that, like other Commonwealth departments and authorities, Defence also endeavours to comply with relevant State legislation and local government ordinances..."

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In this regards the fact that the City of Hobart is amending its planning scheme to include HMAS HUON as a heritage property is applicable.

- c. The above mentioned action by the City of Hobart and the listing of the property by the National Trust of Australia (Tasmania) implies a need (not a statutory obligations) for care to be taken in regards actions likely to affect HMAS HUON. The disposal of the HMAS HUON is recognised by the Australian Heritage Commission as such an action. The commissioning of this conservation management plan is a recognition of the factor and is a demonstration by the Department of Defence that care is being taken even if not strictly required from a statutory viewpoint.
- d. Department of Defence officers should make themselves familiar with DI(G) 40-1 and its requirements.

5.2 DEFINITIONS

The following definitions are from the Australia ICOMOS *Burra Charter* and are used in the **Policy Recommendations**.

Conservation means all the processes of looking after a place so as to retain its cultural significance. It includes maintenance and may according to circumstance include preservation, restoration, reconstruction and adaptation and will be commonly a combination of more than one of these.

Maintenance means the continuous protective care of the fabric, contents and setting of a place, and is to be distinguished from repair. Repair involves restoration or reconstruction and should be treated accordingly.

Preservation means maintaining the fabric of a place in its existing state and retarding deterioration.

Restoration means returning the EXISTING fabric of a place to a known earlier state by removing accretions or by reassembling existing components without the introduction of new material.

Reconstruction means returning a place as nearly as possible to a known earlier state and is distinguished by the introduction of materials (new or old) into the fabric. This is not to be confused with either re-creation or conjectural reconstruction which are outside the scope of this Charter.

Adaptation means modifying a place to suit proposed compatible uses.

Compatible use means a use which involves no change to the culturally significant fabric, changes which are substantially reversible, or changes which require minimal impact.

5.3 POLICY RECOMMENDATIONS

The following recommendations provide policies for the conservation of HMAS HUON. Further information for achieving these policies are provided in **6.0 IMPLEMENTATION**. The words in italics are defined above.

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- 5.3.1 It is **recommended** that HMAS HUON be *conserved* to protect and enhance its cultural significance, with controlled *adaptation* to encourage its occupation and viability and to make possible the maintenance of its significant fabric.
- 5.3.2 It is **recommended** that the naval associations and orientation of HMAS HUON to the River Derwent be *preserved* and if possible enhanced.
- 5.3.3 It is **recommended** that the 'crucial' buildings of HMAS HUON (as identified in 4.3) are *conserved* and *adapted* with special attention to:
- the *restoration/reconstruction* of buildings 7/8 to its 1912 extent allowing for *adaptation* for *compatible uses*;
 - the maintenance of building 9 to its present form or its replacement with a building of identical scale and form;
 - the *restoration/reconstruction* of the exterior of building 4 to an appropriate, or at least more sympathetic, earlier state;
- 5.3.4 It is **recommended** that the 'contributory' buildings of HMAS HUON (as identified in 4.3) are encouraged to be *maintained* and *adapted*, although the 'crucial' buildings should be given priority for *conservation*.
- 5.3.5 It is **recommended** that any new development required to make the property viable be respect the significance, scale and general existing character of HMAS HUON.
- 5.3.6 It is **recommended** that the Commonwealth ensure statutory protection of HMAS HUON as identified in this document by appropriate state and/or local government level prior to its disposal.

It is further **recommended** that HMAS HUON be nominated by the Department of Defence for inclusion on the Register of the National Estate based upon this document prior to its disposal.

It is further **recommended** that all future owners agree as a condition of disposal not to oppose the inclusion of the HMAS HUON property on any heritage registers or lists.

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6.0 IMPLEMENTATION

6.1 STATUTORY OBLIGATIONS (HERITAGE)

6.1.1 Commonwealth

Because HMAS HUON is not on the Register of the National Estate, there are no Commonwealth statutory heritage obligations towards the property. If the property should be added to the RNE then there will be obligations under Section 30 of the *Australian Heritage Commission Act 1975*. Refer to **Appendix 7.2** for an extract of Section 30.

It is **recommended** however that the Department of Defence formally advise the Australian Heritage Commission of its intent to dispose of HMAS HUON and a copy of this conservation management plan should be provided for their information. This is a courtesy action rather than a statutory referral.

6.1.2 State

Because there is no State heritage legislation in Tasmania, the Commonwealth has no State statutory heritage obligations towards HMAS HUON. If this legislation should eventuate, there may be obligations if this property is identified by the State - although the Commonwealth is exempt from State and local government legislation.

The above does not include other statutory obligations relating to issues such as foreshore easements, etc. While the Commonwealth is exempt from state legislation, it has been the practice of the Commonwealth to conform with that legislation in spirit.

It is **recommended** that the Department of Defence formally advise the Tasmanian Department of Planning of their intent to dispose of HMAS HUON and a copy of this conservation management plan provided for their information. This is a courtesy action rather than a statutory referral.

Potential purchasers and/or users should be made aware of the situation regarding the state of Tasmania and advised to fully investigate any implications for themselves.

6.1.3 City of Hobart

As stated previously, HMAS HUON is not identified in the heritage portion of the present Hobart Planning Scheme. It is however part of a proposed amendment and it can be anticipated that it will be included shortly.

While the Commonwealth is exempt from local government legislation, it has been the practice of the Commonwealth to conform with that legislation in spirit. There is nothing in the Planning Scheme to prevent the disposal of HMAS HUON, although there are implications with any re-zoning from its present 'recreational' use. Potential purchasers and/or users should be made aware of the situation regarding the City of Hobart and advised to fully investigate any implications for themselves.

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It is **recommended** that the Department of Defence formally advise the City of Hobart of their intent to dispose of HMAS HUON and a copy of this conservation management plan provided for their information. This is a courtesy action rather than a statutory referral.

6.1.4 National Trust of Australia (Tasmania)

There are no statutory obligations resulting from the National Trust classification of HMAS HUON.

It is **recommended** however that the Department of Defence formally advise the Trust of their intent to dispose of HMAS HUON and provide a copy of this conservation management plan for their information. This is a courtesy action rather than a statutory referral.

6.1.5 Conditions of Sale/Disposal

It is **recommended** that the Department of Defence ensure in its disposal documentation that any future owners of the property:

- recognise the property's heritage significance and agree to abide by the policies and guidelines identified in this conservation management plan;
- agree not to object to the inclusion of the property on any heritage registers/schedules;
- agree to liaise with the Hobart City Council and others as though the property is listed as a heritage property under the Planning Scheme.

6.2 GUIDELINES

6.2.1 Site

- Building 7/8 should remain the dominant building on the site, particularly as seen from the river.
- A jetty should be maintained in its present location although the replacement or alteration/extension/reduction of the existing jetty should be permitted. Any additional structures or enclosures associated with the jetty should be minimal in number, extent and visual treatment.
- None of the existing landscaping - lawns, flower beds, shrubs, paving, etc - need be retained. Any replacement landscaping should:
 - retain the open space between buildings 7/8 and the river and views to and from that building and the water;
 - simplify the landscaping to enhance and reinforce buildings 7/8;
 - seek to create more uniformity in vegetation selection and materials/construction for paving and retaining walls;
 - rationalise the terracing of the site.
- It is encouraged that the four large trees on the eastern boundary be retained and maintained although this is not mandatory.
- The retention of the tennis court is not mandatory although neither is its removal. It should however not be enlarged or greatly modified. Any structures associated with the courts or any subsequent uses of the area should be minimal in number, extent and visual treatment.
- The retention of the existing covered barbecue area and the timber deck off building 6 is not required and in fact should be considered for removal and/or replacement in a more sympathetic and relevant design.

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- . The modern covering added on the west side of building 7 should be removed and not replaced. If weatherproofing is required it should be provided with a verandah/arcade fixed to buildings 5/6.
- . Where hard paving is required, bitumen should dominate although concrete and controlled areas of other finishes - such as brick paving - should also be permitted.
- . A naval flag mast, as existing adjacent to buildings 11/12, should be maintained although its location may be changed so long as it remains on the river side of buildings 7/8 and preferably in a prominent location on the site.

6.2.2 Individual Buildings**a. Building 1**

- . The retention of this building is encouraged but is not mandatory.
- . If the building is retained:
 - . it need not be restored/reconstructed, although this may be undertaken if desired;
 - . it should not be enlarged and its external form and cladding should be retained;
 - . its openings may be retained or altered, but if altered should generally reflect a traditional approach to those openings in regards to proportions and materials;
 - . there should be no restrictions on the interior except that any proposed works should not adversely impact upon its exterior;
 - . it should be painted in a scheme unify the many buildings on the site;
 - . no works should compete with, or detract from, the appreciation of building 7/8.
- . If the building is not retained, it should be appropriately documented with measured drawings and photographs and the documentation lodged with the Tasmanian Archives as no original drawings are known to exist.

b. Building 2

- . The retention of this building is encouraged but is not mandatory.
- . If the building is retained:
 - . it need not be restored/reconstructed, although this may be undertaken if desired;
 - . it should not be enlarged and its external form and cladding should be retained;
 - . its openings may be retained or altered, but if altered should generally reflect a traditional approach to those openings in regards to proportions and materials;
 - . the more recent additions to the east may be removed or further modified within their plan envelope. Their roof forms may be modified but should remain simple in keeping with the main portion of the building;
 - . the small porch on the west elevation should be modified to be more traditional although this is not mandatory;
 - . there should be no restrictions on the interior except that any proposed works should not adversely impact upon its exterior and wherever possible the remaining 'v' groove wall lining and fibrous plaster ceilings should be retained and expressed;
 - . no works should compete with, or detract from, the appreciation of building 7/8.
- . If the building is not retained, it is not necessary to document the building as original drawings exist.

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- . The retention of this building is encouraged but is not mandatory.
- . If the building is retained:
 - . it need not be restored/reconstructed, although this may be undertaken if desired;
 - . it should not be enlarged and its external form and cladding should be retained;
 - . its openings may be retained or altered, but if altered should generally reflect a traditional approach to those openings in regards to proportions and materials;
 - . its distinctive gabled roof should be retained with appropriately detailed vents;
 - . the covered entry on the east side may be removed or appropriately altered.
 - . The two four panel doors on the east elevation should be retained, although not necessarily with both operable.
 - . There should be no restrictions on the interior except that proposed works should not adversely impact upon the exterior of the building.
 - . no works should compete with, or detract from, the appreciation of building 7/8.
- . If the building is not retained, it should be appropriately documented with measured drawings and photographs and the documentation lodged with the Tasmanian Archives as no original drawings are known to exist.

c. Building 5

- . This building consists of two separate but adjoining buildings. The northern portion is earlier (pre-WWII) and the southern half is modern (1960s).
- . The retention of the northern portion is encouraged but not mandatory but the retention of the southern portions is not required.
- . If the southern portion is retained:
 - . it should not be enlarged;
 - . the openings may be altered;
 - . the skillion roof should be maintained;
 - . its external treatment should reflect the adjacent northern portion in colour and proportions;
 - . the external privacy screen into the toilet may be removed or altered;
 - . there should be no restrictions on the interior except that proposed works should not adversely impact upon the exterior of the building;
 - . no works should compete with, or detract from, the appreciation of building 7/8.
- . If the southern portion is not retained it is not necessary to record it as the original drawings are known to exist.
- . If the northern portion is retained:
 - . it need not be *restored/reconstructed*, although this may be undertaken if desired;
 - . it should not be enlarged and its external form, openings and treatment should be retained including the large 'v' groove boarded door;
 - . its skillion roof should be retained;
 - . the paint may be removed from the building although this is not mandatory;
 - . there should be no restrictions in the interior of the northern portion except that proposed works should not adversely impact upon the exterior of the building;
 - . no works should compete with, or detract from, the appreciation of building 7/8.

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- . If the building is not retained, it should be appropriately documented with measured drawings and photographs and the documentation lodged with the Tasmanian Archives as no original drawings are known to exist.
- . The modern roof between this building and building 7 should be removed.

d. Building 6

- . This building consists of two separate but adjoining buildings. The southern portion is earlier (pre-WWII) and the northern half is modern (1960s).
- . The retention of the southern portion is encouraged but is not mandatory, but the retention of the northern portion is not required.
- . If the northern portion is retained:
 - . it should not be enlarged;
 - . the openings may be altered;
 - . although not mandatory, the modification of the skillion roof should be encouraged to be less dominant from the river, possibly in conjunction with the modification of the roof to the southern portion;
 - . its external treatment should reflect the adjacent southern portion in colour and proportions;
 - . the external deck should be removed or modified to be smaller and less 'mock colonial';
 - . there should be no restrictions on the interior except that proposed works should not adversely impact upon the exterior of the building;
 - . no works should compete with, or detract from, the appreciation of building 7/8.
- . If the northern portion of the building is not retained it should only be replaced with a single storey building with a greater traditional character than the existing.
- . If the northern portion is not retained, it is not necessary to record the building as original drawings are known to exist.
- . If the southern portion of the building is retained:
 - . it need not be restored/reconstructed, although this may be undertaken if desired
 - . it should not be enlarged and its external form, openings and treatment should be retained including the timber windows and 'v' groove boarded door;
 - . although not mandatory, the modification of the roof to its earlier gable form should be encouraged;
 - . the existing flat roofed porch on the east elevation should be removed and if required, should be modified to be more traditional in appearance;
 - . there should be no restrictions in the interior of the northern portion except that proposed works should not adversely impact upon the exterior of the building and the existing fireplaces should be retained (although need not be operable).
 - . no works should compete with, or detract from, the appreciation of building 7/8.
- . If the southern portion of the building is not retained, it should be appropriately documented with measured drawings and photographs and the documentation lodged with the Tasmanian Archives as no original drawings are known to exist.

e. Building 7/8

- . These two buildings were designed as one building although the residence portion - building 8 - is distinctly different in its external detailing and internal treatment.

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- These buildings should be retained and their traditional qualities *restored/reconstructed* wherever possible including an appropriate traditional colour scheme. The important era for this building (7/8), and the basis for any *restoration/reconstruction* is its original 1912 date. Later existing additions and/or alterations do not contribute to its significance.
- For buildings 7/8:
 - the repair of existing fabric is preferred to its replacement;
 - exposed services should be removed and/or rationalised wherever possible and new services concealed either entirely in the residence portion or within the structure of the drill portion;
 - reversible works are preferred wherever possible;
 - a maintenance and inspection program should be developed for the long term care of the building;
 - the existing external envelope of both buildings should be retained including its cladding, original openings, timber detailing, chimneys, etc except as may be noted elsewhere;
 - any alterations required to the existing fabric should reflect the construction, materials, scale, and proportions of the original building.
- For building 7 (the drill hall):
 - the balcony/verandah added to the north elevation may be retained, removed and/or altered but should not be increased in size or further enclosed. It would be desirable that they be removed and the original staircase be reinstated, but this should not be mandatory;
 - the original openings on the building's perimeter should be retained and no further openings or modifications should be permitted. It is desirable to reinstate the double doors on the southeast corner if the opportunity arises;
 - non original openings at the lower level of the north elevation may be retained, closed or altered so long as they are treated in a traditional manner in keeping with the original openings of the building;
 - there should be no restrictions on the interior of the lower level except:
 - that remaining chamfered timber columns should be retained;
 - the brick arched supports for the fireplaces over in the western end of the building should be retained intact;
 - no proposed works must adversely impact upon the main space of the drill hall over;
 - no proposed works must compromise the structural integrity of the building;
 - no excavation further under the building should be undertaken.
 - the existing offices at the western end of the drill hall space should be retained including the existing doors, fireplaces, pressed metal ceilings, painted lining boards and other traditional detailing. The reinstatement of missing details (including plaster to the fireplace breasts) is encouraged but is not mandatory. The retention of the internal enclosure around the double doors on the southwest corner of the main room is not required, but neither is its removal mandatory. The reinstatement of the one missing room in the northwest corner of the main room (projecting east from the existing offices) should not be required but may be undertaken if the rooms is necessary for a function;
 - the retention of the modern varnished timber lining boards in the drill hall space generally is not required but neither is their removal. A lining more appropriate to the original would be preferred such as the painted horizontal boards;
 - the existing timber floor should be retained along with the exposed structure of the walls and roof. Limited lining of the walls and ceilings should be permitted but must be limited and

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- controlled to maximise the expression of the structure. The roof trusses should remain exposed but the space between may be lined;
- the enclosure of rooms and offices at the eastern end of the drill hall space may remain but their removal along with the associated stairs and partitions (and the reinstatement of the original building extent and fabric) is encouraged. Further full height enclosures and the enclosure of the roof trusses should not be permitted;
- only after critical assessment of accommodation requirements should additional partitioning of the drill hall space be considered and only then if the rooms are only single storey and the partitions limited to the minimum height necessary and not to exceed 2800 mm±.
- further works and decoration to the interior of the drill hall space should recognise the original approach to the use of materials, the simplicity of its treatment, and the dominance of painted timber finishes. New works should create a restrained character rather than an inflated or flamboyant appearance.
- For building 8 (the residence):
 - all of the remaining traditional fabric, such as the pressed metal ceilings, fireplace surrounds, joinery and staircase should be retained. Where elements are missing it is encouraged, although not mandatory, that they be appropriately reinstated;
 - the scale of the rooms should remain the same but interconnections between rooms may be introduced in an appropriately sympathetic manner;
 - it would be desirable to remove the additions to the north and northeast of the original residence and to reinstate the original perimeter of the building. This however is not mandatory. The extent of the present additions but should not be increased and the shallow pitched awning over the northern most deck addition should be removed. As well, the glazed rooms at the lower level, under the above mentioned deck should also be removed;
 - the original timber verandah posts and decoration should be reinstated to the east elevation.
 - Additional accommodation requiring significant changes should either be located within rooms or areas already altered or the added portions of the building, or possible in the areas adjoining the two portions of buildings 7 and 8.

f. Building 9

- The retention of this building is encouraged but is not mandatory. The removal of this building is dependent upon its replacement.
- If the building is retained:
 - it need not be *restored/reconstructed*;
 - it should not be enlarged and its external form and cladding should be retained;
 - its opening may be retained or altered, but if alter should generally reflect a traditional approach to those openings in regards to proportions and materials (allowance should be made for the river side doors where functional requirements may require modern roller type doors);
 - There should be no restrictions on the interior except that proposed works should not adversely impact upon the exterior of the building;
 - no works should compete with, or detract from, the appreciation of buildings 7/8.
- The removal or replacement of this building should only be permitted if it is replaced with a similar building on the same plan utilising the remnant early footings. Any replacement should reflect the existing form and use of weatherboards.

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- The use of this building (or any replacement) is strongly encouraged to have a maritime/naval function, and preferably used as a boat shed.

g. Building 10

- This structure may be removed or retained, but if retained it should not be enlarged.
- The retention of this structure, and any proposed works such as painting, should consider their visual impact upon the appreciation of the property.
- If removed, the structure should be appropriately documented with measured drawings and photographs and the documentation lodged with the Tasmanian Archives.
- The adjacent exposed concrete tanks need not be retained.

h. Building 11/12

- These structures may be removed or retained, but if retained they should not be enlarged.
- The retention of these structures, and any proposed works such as painting, should consider their visual impact upon the appreciation of the property.
- If removed, the structures should be appropriately documented with measured drawings and photographs and the documentation lodged with the Tasmanian Archives.
- The adjacent portions of concrete pads associated with the flag mast need not be retained if the mast is relocated.

i. Building 13

- This structure should be removed and not replaced.

j. Building 15 and 16

- These buildings may be removed or retained, but if retained they should not be enlarged.
- The retention of the buildings, and any proposed works such as painting, should consider their visual impact upon the appreciation of the property.
- It would be desirable to transfer the function of these sheds to one or part of the 'crucial' or 'contributory' buildings.

6.2.3 New Development

- a. ~~No~~ new building developments should only be considered after it is critically assessed whether the required accommodation can be provided within the 'crucial' or 'associated' buildings having regards for the previous guideline statements.
- b. New building development should only be sited where 'associated' buildings currently exist so long as the above mentioned accommodation assessment is formally undertaken and the buildings to be removed have been recorded as noted.

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- c. New building development should preferably utilise weatherboard construction with gable roof forms. Building developments should be single storey, have a traditional form and character and new developments should avoid large monolith solutions.
- d. New buildings should stop short of literally replicating traditional buildings and only draw upon them in a general sense for inspiration. New buildings should be 'understated' compared with buildings 7/8.
- e. New building development should not adversely impact upon the appreciation of the buildings 7/8 nor make them redundant in a functional sense.
- f. New development should endeavour to maintain the site with its own identity rather than be oriented outside the existing property boundaries. This does not imply that it can not be functionally part of a larger development or that it must be fenced off.
- g. Refer to **Figure 6.2** for areas of potential development.

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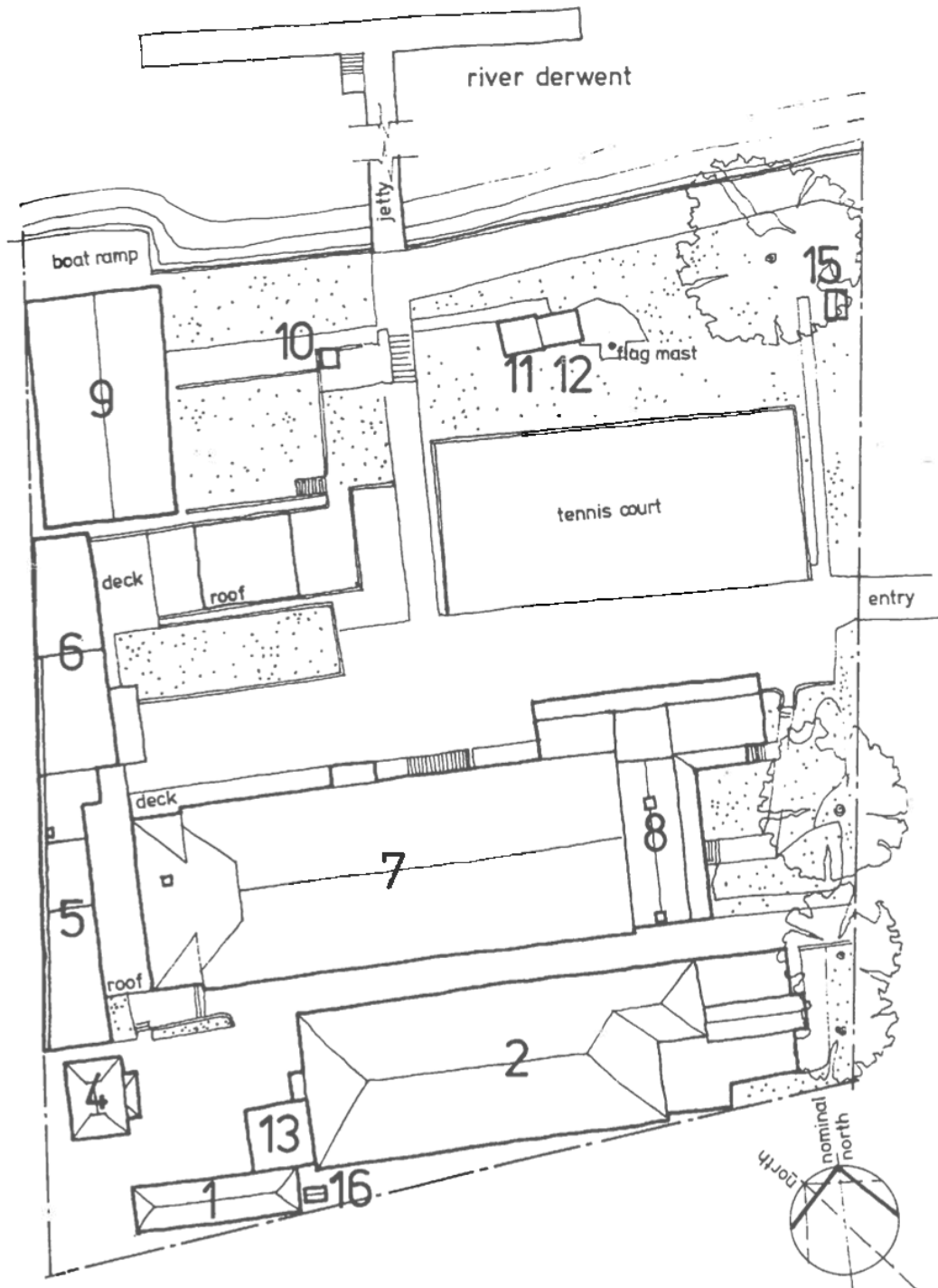


Figure 6.1 HMAS HUON, HOBART
as existing site plan for reference

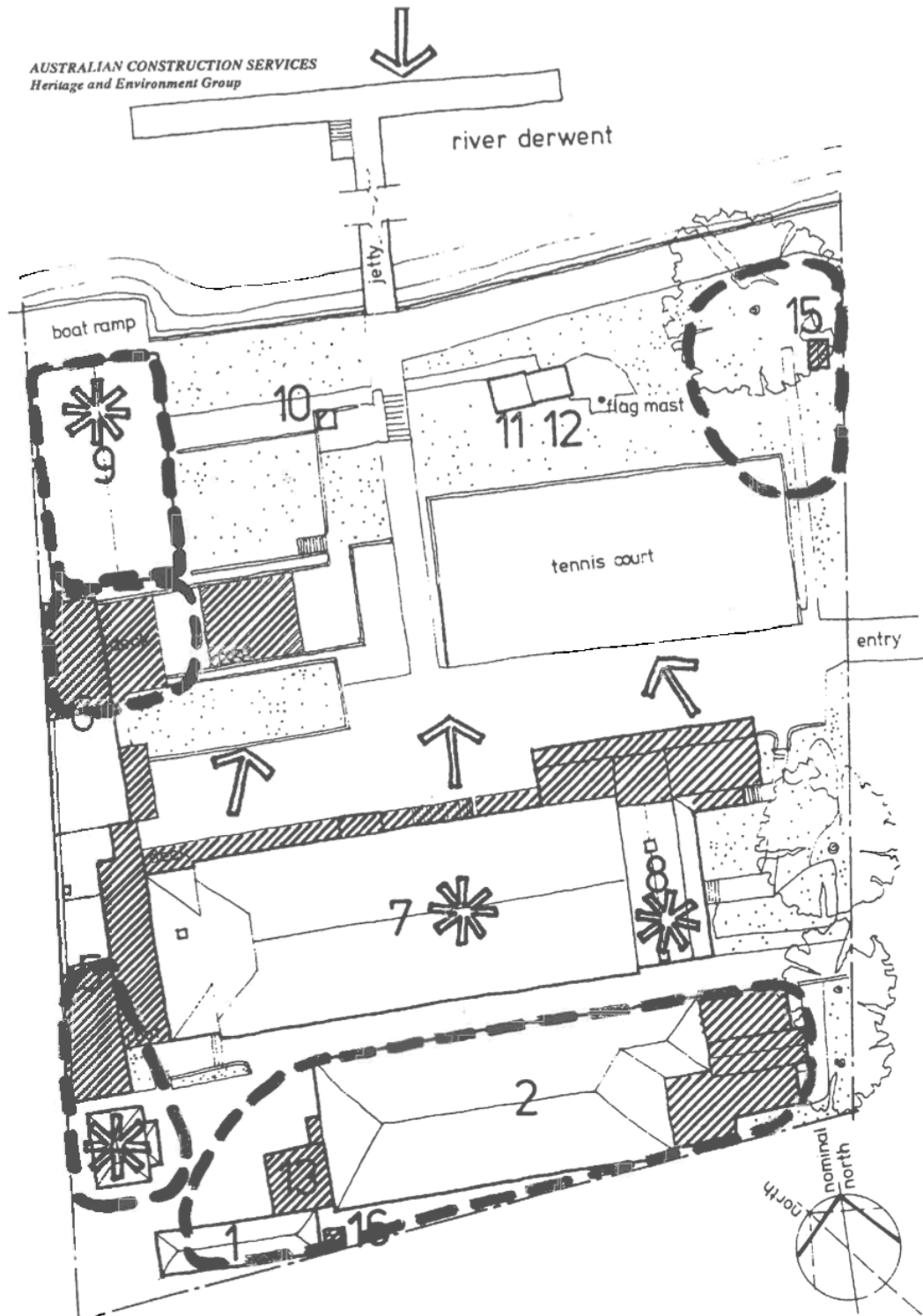


Figure 6.2 HMAS HUON, HOBART

development guides: arrows are important views, stars are focal points, hatched building portions are modern and may/should be removed, dashed areas may be re-developed with new buildings

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7.0 APPENDICES

7.1 ACKNOWLEDGMENTS

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- . Dick Knoop

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7.2 AUSTRALIAN HERITAGE COMMISSION ACT 1975, SECTION 30

- 30.(1) Each Minister shall give all such directions and do all such things as, consistently with relevant laws, can be given or done by him for ensuring that the Department administered by him or any authority of the Commonwealth in respect of which he has ministerial responsibilities does not take any action that adversely affects, as part of the national estate, a place that is in the Register unless he is satisfied that there is no feasible and prudent alternative to the taking of that action and that all measures that can reasonably be taken to minimise the adverse effect will be taken and shall not himself take any such action unless he is so satisfied.
- (2) Without prejudice to the application of sub-section (1) in relation to action to be taken by an authority of the Commonwealth, an authority of the Commonwealth shall not take any action that adversely affects, as part of the national estate, a place that is in the Register unless the authority is satisfied that there is no feasible and prudent alternative, consistent with any relevant laws, to the taking of that action and that all measures that can reasonably be taken to minimise the adverse effect will be taken.
- (3) Before a Minister, a Department or an authority of the Commonwealth takes any action that might affect to a significant extent, as part of the national estate, a place that is in the Register, the Minister, Department or authority, as the case may be, shall inform the Commission of the proposed action and give the Commission a reasonable opportunity to consider it.
- (3A) Where the Commission is informed of a proposed action by a Minister, Department or authority, the Commission shall, as soon as practicable, provide its comments on the proposed action to the Minister, Department or authority (as the case may be).
- (4) For the purposes of this section, the making of a decision or recommendation (including a recommendation in relation to direct financial assistance granted, or proposed to be granted to a State) the approval of a program, the issue of a license or the granting of a permission shall be deemed to be the taking of action and in the case of a recommendation, if the adoption of the recommendation would adversely affect a place, the making of the recommendation shall be deemed to affect the place adversely.

The following text does not form part of the AHC Act but is provided to assist with the interpretation of the Act.

In Section 30 of the AHC Act, the term '*to a significant extent*' is used to describe actions which should be referred to the Commission for comment. Although not officially part of Section 30, the following list has been provided by the Commission to clarify actions which it considers may constitute an action affecting of a property '*to a significant extent*'.

- disposal of all or parts of the property by lease, sale or transfer of ownership from the Commonwealth;

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- . restoring a building;
- . altering the fabric of a building or replacing the fabric by another material;
- . additions to, or new construction nearby, existing building elements;
- . redesign and reconstruction of parts of a building;
- . painting unpainted surfaces;
- . cleaning the building fabric by mechanical, chemical or blasting methods;
- . removing paint by mechanical, chemical or blasting methods;
- . applying masonry preservatives;
- . excavating or removing old soil or footings;
- . road or path making;
- . earthworks or landscaping;
- . removing or planting vegetation;
- . removing, changing or replacing machinery, drains, etc.;
- . removing the contents of a building;
- . installing or improving services (eg. air conditioning, sewerage, elevators); and
- . discontinuing repairs and maintenance.

The Commission also points out that there are indirect actions which could have adverse impact on a heritage property, such as:

- . new construction which may remove the function of a heritage property and thus render it redundant and vulnerable to demolition;
- . new construction which may be adjacent to a heritage property and have an impact on it or its appreciation (the heritage property effected may not be Commonwealth owned but still included in the Register of the National Estate).

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7.3 AUSTRALIA ICOMOS 'BURRA CHARTER'

THE AUSTRALIA ICOMOS CHARTER FOR THE CONSERVATION OF PLACES OF CULTURAL SIGNIFICANCE (*The Burra Charter*)

(The explanatory notes which appear in smaller typeface do not form part of the Charter and may be added to by Australia ICOMOS)

Preamble

Having regard to the International Charter for the Conservation and Restoration of Monuments and Sites (Venice 1966), and the Resolutions of 5th General Assembly of ICOMOS (Moscow 1978), the following Charter was adopted by Australia ICOMOS on 19th August 1979 at Burra Burra. Revisions were adopted on 23rd February 1981 and on 23 April 1988.

Definitions

Article 1. For the purpose of this Charter:

- 1.1** *Place* means site, area, building or other work, group of buildings or other works together with associated contents and surroundings.

Article 1.1

Place includes structures, ruins, archaeological sites and landscapes modified by human activity.

- 1.2** *Cultural significance* means aesthetic, historic, scientific or social value for past, present or future generations.

- 1.3** *Fabric* means all the physical material of the *place*.

- 1.4** *Conservation* means all the processes of looking after a *place* so as to retain its *cultural significance*. It includes *maintenance* and may according to circumstance include *preservation, restoration, reconstruction* and *adaptation* and will be commonly a combination of more than one of these.

- 1.5** *Maintenance* means the continuous protective care of the *fabric*, contents and setting of a *place*, and is to be distinguished from repair. Repair involves *restoration* or *reconstruction* and should be treated accordingly.

Article 1.5

The distinctions referred to in Article 1.5, for example in relation to roof gutters, are:

maintenance - regular inspection and cleaning of gutters

repair involving restoration - returning of dislodged gutters to their place

repair involving reconstruction - replacing decayed gutters.

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- 1.6 *Preservation* means maintaining the *fabric* of a *place* in its existing state and retarding deterioration.
- 1.7 *Restoration* means returning the EXISTING *fabric* of a *place* to a known earlier state by removing accretions or by reassembling existing components without the introduction of new material.
- 1.8 *Reconstruction* means returning a *place* as nearly as possible to a known earlier state and is distinguished by the introduction of materials (new or old) into the *fabric*. This is not to be confused with either re-creation or conjectural reconstruction which are outside the scope of this Charter.
- 1.9 *Adaptation* means modifying a *place* to suit proposed compatible uses.
- 1.10 *Compatible use* means a use which involves no change to the culturally significant fabric, changes which are substantially reversible, or changes which require a minimal impact.

Conservation Principles

Article 2. The aim of *conservation* is to retain or recover the *cultural significance* of a *place* and must include provision for its security, its *maintenance* and its future.

Article 2

Conservation should not be undertaken unless adequate resources are available to ensure that the fabric is not left in a vulnerable state and that the cultural significance of the place is not impaired. However, it must be emphasised that the best conservation often involves the least work and can be inexpensive.

Article 3. *Conservation* is based on a respect for the existing *fabric* and should involve the least possible physical intervention. It should not distort the evidence provided by the *fabric*.

Article 3

The traces of additions, alterations and earlier treatments on the fabric of a place are evidence of its history and uses. Conservation action should tend to assist rather than to impede their interpretation.

Article 4. *Conservation* should make use of all the disciplines which can contribute to the study and safeguarding of a *place*. Techniques employed should be traditional but in some circumstances they may be modern ones for which a firm scientific basis exists and which have been supported by a body of experience.

Article 5. *Conservation* of a *place* should take into consideration all aspects of its *cultural significance* without unwarranted emphasis on any one at the expense of others.

Article 6. The conservation policy appropriate to a *place* must first be determined by an understanding of its *cultural significance*.

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Article 6

An understanding of the cultural significance of a place is essential to its proper conservation. This should be achieved by means of a thorough investigation resulting in a report embodying a statement of cultural significance. The formal adoption of a statement of cultural significance is an essential prerequisite to the preparation of a conservation policy.

Article 7. The conservation policy will determine which uses are compatible.

Article 7

Continuity of the use of a place in a particular way may be significant and therefore desirable.

Article 8. *Conservation* requires the maintenance of an appropriate visual setting: eg., form, scale, colour, texture and materials. No new construction, demolition or modification which would adversely affect the settings should be allowed. Environmental intrusions which adversely affect appreciation or enjoyment of the *place* should be excluded.

Article 8

New construction work, including infill and additions, may be acceptable provided:

it does not reduce or obscure the cultural significance of the place

it is in keeping with Article 8.

Article 9. A building or work should remain in its historical location. The moving of all or part of a building or work is unacceptable unless this is the sole means of ensuring its survival.

Article 9

Some structures were designed to be readily removable or already have a history of previous moves, eg. prefabricated dwellings and poppet-heads. Provided such a structure does not have a strong association with its present site its removal may be considered. If any structure is moved, it should be moved to an appropriate setting and given an appropriate use. Such action should not be to the detriment of any place of cultural significance.

Article 10. The removal of contents which form part of the *cultural significance* of the *place* is unacceptable unless it is the sole means of ensuring their security and *preservation*. Such contents must be returned should changed circumstances make this practicable.

Conservation Processes

Preservation

Article 11. *Preservation* is appropriate where the existing state of the *fabric* itself constitutes evidence of specific *cultural significance*, or where insufficient evidence is available to allow other conservation processes to be carried out.

Article 11

Preservation protects fabric without obscuring the evidence of its construction and use. The process should always be applied:

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where the evidence of the fabric is of such significance that it must not be altered. This is an unusual case and likely to be appropriate for archaeological remains of national importance;

where insufficient investigation has been carried out to permit conservation policy decisions to be taken in accord with Articles 23 to 25.

New construction may be carried out in association with preservation when its purpose is the physical protection of the fabric and when it is consistent with Article 8.

Article 12. *Preservation is limited to the protection, maintenance and, where necessary, the stabilisation of the existing fabric but without the distortion of its cultural significance.*

Article 12

Stabilisation is a process which helps keep fabric intact and in a fixed position. When carried out as part of preservation work it does not introduce new materials into the fabric. However, when necessary for the survival of the fabric, stabilisation may be effected as part of a reconstruction process and new materials introduced. For example, grouting or the insertion of a reinforcing rod into a masonry wall.

Restoration

Article 13. *Restoration is appropriate only if there is sufficient evidence of an earlier state of the fabric and only if returning the fabric to that state reveals the cultural significance of the place.*

Article 13

See explanatory note for Article 2.

Article 14. **Restoration** should reveal anew culturally significant aspects of the *place*. It is based on respect for all the physical, documentary and other evidence and stops at the point where conjecture begins.

Article 15. *Restoration* is limited to the reassembling of displaced components or removal of accretions in accordance with Article 16.

Article 16. The contributions of all periods to the *place* must be respected. If a *place* includes the *fabric* of different periods, revealing the *fabric* of one period at the expense of another can only be justified when what is removed is of slight *cultural significance* and the *fabric* which is to be revealed is of much greater *cultural significance*.

Reconstruction

Article 17. *Reconstruction* is appropriate only where a *place* is incomplete through damage or alteration and where it is necessary for its survival, or where it reveals the *cultural significance* of the *place* as a whole.

Article 18. *Reconstruction* is limited to the completion of a depleted entity and should not constitute the majority of the *fabric* of a *place*.

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Article 19. *Reconstruction* is limited to the reproduction of *fabric*, the form of which is known from physical and/or documentary evidence. It should be identifiable on close inspection as being new work.

Adaptation

Article 20. *Adaptation* is acceptable where the *conservation* of the *place* cannot otherwise be achieved, and where the *adaptation* does not substantially detract from its *cultural significance*.

Article 21. *Adaptation* must be limited to that which is essential to a use for the *place* determined in accordance with Articles 6 and 7.

Article 22. *Fabric* of *cultural significance* unavoidably removed in the process of *adaptation* must be kept safely to enable its future reinstatement.

Conservation Practice

Article 23. Work on a *place* must be preceded by professionally prepared studies of the physical, documentary and other evidence, and the existing *fabric* recorded before any intervention in the *place*.

Article 24. Study of a *place* by any intervention in the *fabric* or by archaeological excavation should be undertaken where necessary to provide data essential for decisions on the *conservation* of the place and/or to secure evidence about to be lost or made inaccessible through necessary *conservation* or other unavoidable action. Investigation of a *place* for any other reason which requires physical disturbance and which adds substantially to a scientific body of knowledge may be permitted, provided that it is consistent with the conservation policy for the *place*.

Article 25. A written statement of conservation policy must be professionally prepared setting out the *cultural significance* and proposed conservation procedure together with justification and supporting evidence, including photographs, drawings and all appropriate samples.

Article 25

The procedure will include the conservation processes referred to in Article 1.4 and other matters described in Guidelines to the Burra Charter: Conservation Policy.

Article 26. The organisation and individuals responsible for policy decisions must be named and specific responsibility taken for each such decision.

Article 27. Appropriate professional direction and supervision must be maintained at all stages of the work and a log kept of new evidence and additional decisions recorded as in Article 25 above.

Article 28. The records required by Articles 23, 25, 26 and 27 should be placed in a permanent archive and made publicly available.

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Article 29. The items referred to in Article 10 and Article 22 should be professionally catalogued and protected.

Application Referral Development Engineering - Response

From:	KB and DM
Recommendation:	Proposal is acceptable subject to conditions.
Date Completed:	
Address:	17 MCVILLY DRIVE, HOBART 13 MCVILLY DRIVE, HOBART 20 MCVILLY DRIVE, HOBART 1 MCVILLY DRIVE, HOBART 10 EVANS STREET, HOBART ADJACENT CROWN LAND
Proposal:	Partial Demolition, Extension, Alterations and Associated Works for Partial Change of Use to Light Industry (Whisky Distillery), Eating Establishment, Function Centre, Hotel and Shop
Application No:	PLN-21-734
Assessment Officer:	Cameron Sherriff,

Referral Officer comments:

Summary:

PLN-21-734 - 17 MCVILLY DRIVE, 13 MCVILLY DRIVE, 20 MCVILLY DRIVE, 1 MCVILLY DRIVE, AND 10 EVANS STREET
Partial Demolition, Extension, Alterations and Associated works for partial change of use to light industry (Whiskey Distillery), eating establishment, function Centre, Hotel and shop.

Detailed Assessment

The proposed development for located at 17 McVilly Drive and adjacent titles, is denoted by the *Sullivans Cove Planning Scheme 1997* as Activity Area 4.2, "Regatta Point". The *Sullivans Cove Planning Scheme 1997* States that Activity Area 4.2 is characterised by low volumes of traffic. with parking provided in this activity area to cater for the parking demands of employees, visitors and other maritime related activities. The Scheme also states that land within this Activity area will not be used for the parking demands of activities outside of the activity area.

Submitted documentation for the development includes a Traffic Impact Assessment (TIA) has been completed by GHD Pty Ltd, and finds the parking and access provisions proposed for the development align with the *Sullivans Cove Planning Scheme 1997*.

Proposed Parking

- The proposal totals 3 DDA parking spaces.
- 12 visitor bicycle parking
- staff only bike store and accompanying facilities
- Turning area in Drill Forecourt for pick-up/drop off
- Existing cenotpath car park 77 spaces
- Existing regatta grounds car park 50 spaces

Proposed Accesses

4 accesses proposed

1. Existing Domain slip access at 1 McVilly Drive, intended use staff and delivery vehicles. Designed to accommodate for MRV.
2. Existing access road through 20 McVilly Drive. Provides access for coaches, cyclists, and private vehicles. (3 accessible parking spaces only.)
3. Existing access, closed to public vehicles. to accommodate for future development that does not form part of this permit.
4. Pedestrian Access - for pedestrians accessing the site from the cenotaph carpark/CBD. Overpass over railway.

Vehicles to Access

Access 1 MRV swept path (Satisfied)

Access 2 Bus Equivalent (Satisfied)

Schedule 5 - Traffic, Access and Parking Assessment Guidelines for Development Control

Traffic generation - *Sullivans Cove Planning scheme 26.4.1*

Any development needing or expected to generate demand for the delivery of people to the site in relatively significant volumes, will be expected to make appropriate provisions for accommodating the associated vehicular movement - whether generated private, public or tourist transport, in a manner consistent with the principles of this schedule.

The proposal satisfies clause 26.4.1, accommodating for the increase in associated vehicle movements by enhancing the already existing accesses to a standard where amenity to Regatta Point should not be impacted. It is also outlined in the TIA that the proposal aims to maximise mode share for transport to discourage private vehicle use to site, this minimises the impact on the traffic conditions.

Access Requirements - *Sullivans Cove Planning scheme 26.4.2*

The proposal satisfies clause 26.4.2, maintaining all of the existing accesses and adhering to the specifications provided in the *Sullivans Cove Planning Scheme*. Pedestrian footways are provided at 1.3m, and is below the maximum two 2.5m lanes.

Parking Standards - *Sullivans Cove Planning scheme 26.4.3*

The disability parking spaces provided comply with the Australian Standard AU/NZS 2890.1:2004. The number of carparking spaces provided aligns with the *Sullivans Cove Planning Scheme 1997* which states that the activity area of 4.2 Regatta Point parking provided in this activity area is to cater for the parking demands of employees, visitors and other maritime related activities. In the activity area of Regatta Point, there are over 150 metered parking spaces, with an added area for overflow parking. This will be sufficient in accommodating for the parking demand of the development and aligns with the *Sullivans Cove Planning Scheme 1997*.

Nature of parking - *Sullivans Cove Planning scheme 26.4.4*

As outlined in the TIA (section 4.1.2) and the Planning Assessment (section 4.7) the proposal has been prepared with regard to maximising mode share for transport to the site to discourage private vehicle use. Circulation areas have been designed to maximise accessibility, functional requirements for the operation of the distillery, to enhance the landscape, and to respect the historic heritage values of the place. Regular visitor numbers to the site are not considered to require alternative parking options and is not a requirement of the Scheme. The site has access to cycle infrastructure with proximity to the intercity cycleway. Visitor bike storage is located in the Drill Forecourt. Staff bike store that is enclosed is proposed located adjoining the secured external yard. Having regard to the type of development, and the consideration of matters such as the location of the site, nature of the site, nature of the surrounding area, existing traffic and parking

patterns, the nature of the operation and future growth, existing on street restrictions, and future on street or off street public parking proposals, given the likely demand of the development, the nature of the parking proposed is satisfies the conditions outlined in the Scheme.

Summary:

The Traffic, Parking and Access conditions proposed under PLN-21-734, adhere and align to the requirements of the Sullivans Cove Planning Scheme 1997 for the activity area 4.2 Regatta Point. Traffic generation has been considered with the proposal aiming to minimise private travel with its patrons mainly arriving via tour bus and mode share. The proposal aligns with the access requirements outlined in the Scheme. Parking Standards have been adhered to with the parking provided complying with the Australian Standard and hence satisfying the Scheme. And the Nature of parking aligns with the activity area requirements and the Scheme.

Hence, in a Council related engineering context, the proposal can be supported in principal subject to the following conditions and advice.

Conditions:

ENG 1 - General Accordance

ENG 4 - Surface Treatment

ENG 5 - guest and staff parking zero approved spaces

ENG 7 - Bike parking must be in accordance with Australian standards

ENG 9 - number and Delineation of DDA spaces

ENG sw1 - stormwater drained via gravity to approved outlet

Advice:

- Dial before you dig
- Fees and charges
- Building Permit
- Plumbing Permit
- Occupation of the Public Highway

Application Referral Cultural Heritage - Response

From:	Sarah Waight
Recommendation:	Proposal is acceptable subject to conditions.
Date Completed:	
Address:	17 MCVILLY DRIVE, HOBART 13 MCVILLY DRIVE, HOBART 20 MCVILLY DRIVE, HOBART 1 MCVILLY DRIVE, HOBART 10 EVANS STREET, HOBART ADJACENT CROWN LAND
Proposal:	Partial Demolition, Extension, Alterations and Associated Works for Partial Change of Use to Light Industry (Whisky Distillery), Eating Establishment, Function Centre, Hotel and Shop
Application No:	PLN-21-734
Assessment Officer:	Cameron Sherriff,

Referral Officer comments:

The heritage site:

This application is for demolition and new work to a place listed in Table 1 Places of Cultural Significance (site 122 Former HMAS Huon Naval Depot) of Schedule 1 - Conservation of Cultural Heritage Values in the *Sullivans Cove Planning Scheme 1997*. It has two site addresses of 13 McVilly Drive and 19 McVilly Drive.

The listing applies to the entire site and consists of a number of elements - some of which are of heritage significance and others less significant. Heritage assessments/documentation has been undertaken to assign heritage values and degrees of significance.

History of site:

In 1855 an area including this site was leased for the construction of a patent slip. A patent slip is an inclined place extending from the shore into the water, featuring a cradle into which the ship is floated and secured and then hauled out of the water. It was based on a design by a Scottish shipwright who patented the original design in 1818. For the following 50 years maritime activities were at the centre of this site.

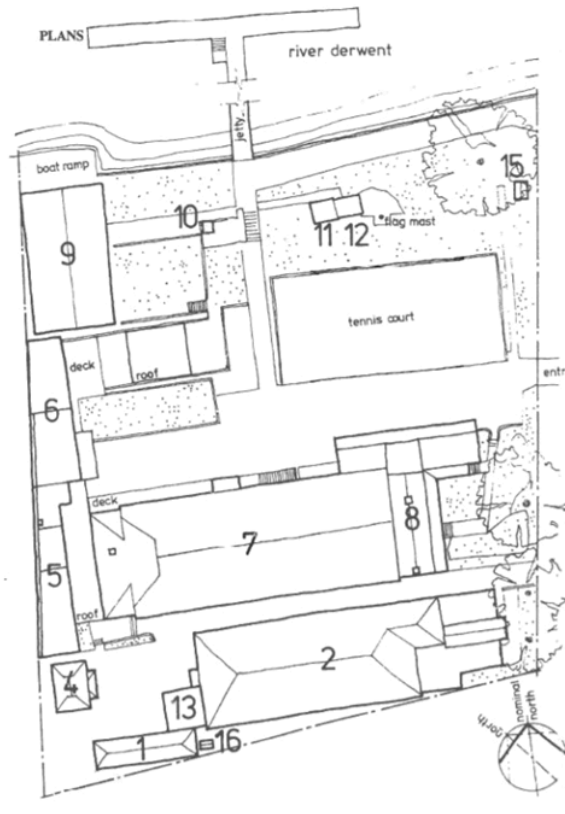
Following the passage of the 1909-1911 *Defence Act*, parts of the former slipyard lease were made over to the Commonwealth Government for a Naval Depot and a Drill Hall was built for naval cadets in 1912-1913. Compulsory military training followed the disbanding of volunteers after Federation, infrastructure for the force was required throughout the State. The Drill Hall was Tasmania's only naval facility of this type. Numerous additional buildings were building in the 1920s and 1940s, including a new mess and barrack, most of which remain, some of which have been altered. The base was named HMAS Derwent in 1939 and renamed HMAS Huon on 1 March 1942. After the end of WWII activity at the base abated and the HMAS Argent, an Attack class patrol boat was commissioned and stationed at *HMAS Huon* and it was manned by naval reserves. In 1994 when both vessel and base were decommissioned.

Supporting heritage reports:

Following the decommissioning of the site, the Australian Construction Services prepared a

Conservation and Management Plan: HMAS Huon in 1995.

The following image is from the CMP and the building numbering system from that report is used in the planning report and other documentation associated with this application.



Site and building numbers from the 1995 CMP; Source CMP

In summary, the CMP concludes that the above buildings have the following significance and recommended policy approach.

Building numbers:

- 1 - building - of general significance - if not kept it should be documented
- 2 - building - of general significance - encouraged to be maintained and adapted, but not mandatory.
- 4 - building - of general significance - encouraged to be maintained and adapted, but not mandatory.
- 5 - building - of general significance - southern end - of little significance - recommended for documentation
- 6 - building and deck - of general significance - southern end - of little significance - recommended for documentation
- 7 - Drill hall - crucial to significance - recommended for conservation - remain the dominant building on the site
- 8 - two storey building - crucial to significance - recommended for conservation - remain the dominant building on the site.
- 9 - building - of general significance, retention is encouraged, but not mandatory, but not

enlarged.

10 - concrete structure - of little significance - recommended for documentation

11/ & 12 - flag mast and flag mast base - of little significance - recommended for documentation

13 - carport - of little significance

15 - garden shed - of little significance

16 - garden shed - of little significance

In addition to the above document, the application is supported by a Heritage Impact Assessment by Praxis Environment, dated October 2021 and includes a review of the heritage issues surrounding the site by Paul Davies Pty Ltd Architects and Heritage Consultants commissioned in 2016 by Tasports.

Relevant assessment provisions:

Internal works are exempt under clause 8.4 of the Scheme.

Under Schedule 1, the works proposal must be assessed against clause 22.4.5 and all criteria (dot points) must be taken into consideration in the assessment of all proposals for 'buildings or works'.

22.4.5 'Discretionary' 'Building or Works'

'Building or works' on places of cultural significance which cannot satisfy the 'deemed to comply' provisions of Clause 22.4.4 may be approved at the discretion of the Planning Authority.

The following criteria must be taken into consideration in the assessment of all proposals to undertake 'building or works' on places of cultural significance:

- *'Building or works' must complement and contribute to the cultural significance, character and appearance of the place and its setting;*
- *'Building or works' must be in compliance with the conservation strategy of an approved Conservation Plan, where required and/or provided;*
- *The location, bulk and appearance of 'building or works' must not adversely affect the heritage values of any place of cultural significance;*
- *'Building or works' must not reduce the apparent authenticity of places of cultural significance by mimicking historic forms;*
- *'Building or works' may be recognisable as new but must not be individually prominent;*
- *The painting of previously unpainted surfaces is discouraged.*

The demolition must be assessed against 28 Schedule 7 Demolition. Under clause 28.3.1 all demolition is discretionary to ensure the protection and promotion of cultural heritage values. A Conservation Management Plan (1995), review of the 1995 Plan from 2016 and Heritage Impact Assessment are submitted as a mandatory requirement under clause 28.8.1 dot point 1.

28.5 Guidelines for Development Control (Demolition)

The demolition of any building, or works on land shall not be 'permitted' unless; a replacement development has been approved, or such demolition is required by statutory order or is authorised by the Building Surveyor as essential to public safety.

Any application for demolition:

(a) Shall be refused if the building is included as a cultural heritage place in Table 1 of the Conservation of Cultural Heritage Values Schedule of this Scheme, unless;

- (i) *The demolition is approved as part of a Conservation Plan approved by the Planning Authority or otherwise in its discretion under the Conservation of Cultural Heritage Values Schedule of this Scheme;*
- (ii) *The building clearly detracts from the cultural values or significance of the place; or*
- (iii) *There are overriding environmental, economic considerations in terms of the building or practical considerations for its removal, either wholly or in part.*
- (b) *May be refused if in the opinion of the Planning Authority the building contributes to the cultural heritage or urban character of the Activity Area and the building is capable of continued beneficial use.*

Further to this the Scheme states:

28.8 Demolition on a Place of Cultural Significance

28.8.1 *Where the application involves the demolition of a building on a Place included in Table 1 of Schedule 1 – Conservation of Cultural Heritage Values then the application must satisfy the following:*

- *The requirement of Clause 22.4.3 for the submission of a Conservation Plan, and*
- *The provision of street elevations or 'true perspectives' to show the scale and impact of the demolition on places of cultural significance and the streetscape.*

28.8.2 *Where the application involves the demolition of any other building then the application must satisfy the following:*

- *The requirement of Clause 22.5.3 for the submission of a report identifying that the building does not have heritage value, and*
- *The provision of street elevations or 'true perspectives' to show the scale and impact of the demolition on places of cultural significance and the streetscape.*

Heritage values:

The Queens Domain Cultural Heritage Management Plan identified a number of sites for inclusion in the *Sullivans Cove Planning Scheme 1997* including this site. It ascribed the following statements of significance:

The former HMAS Huon Naval Depot is of historic heritage significance for the important role it has played in the development of Tasmania's defence system.

The former HMAS Huon Naval Depot is of historic heritage significance because of its ability to demonstrate the principal characteristics of a Federation period naval base.

Demolition:

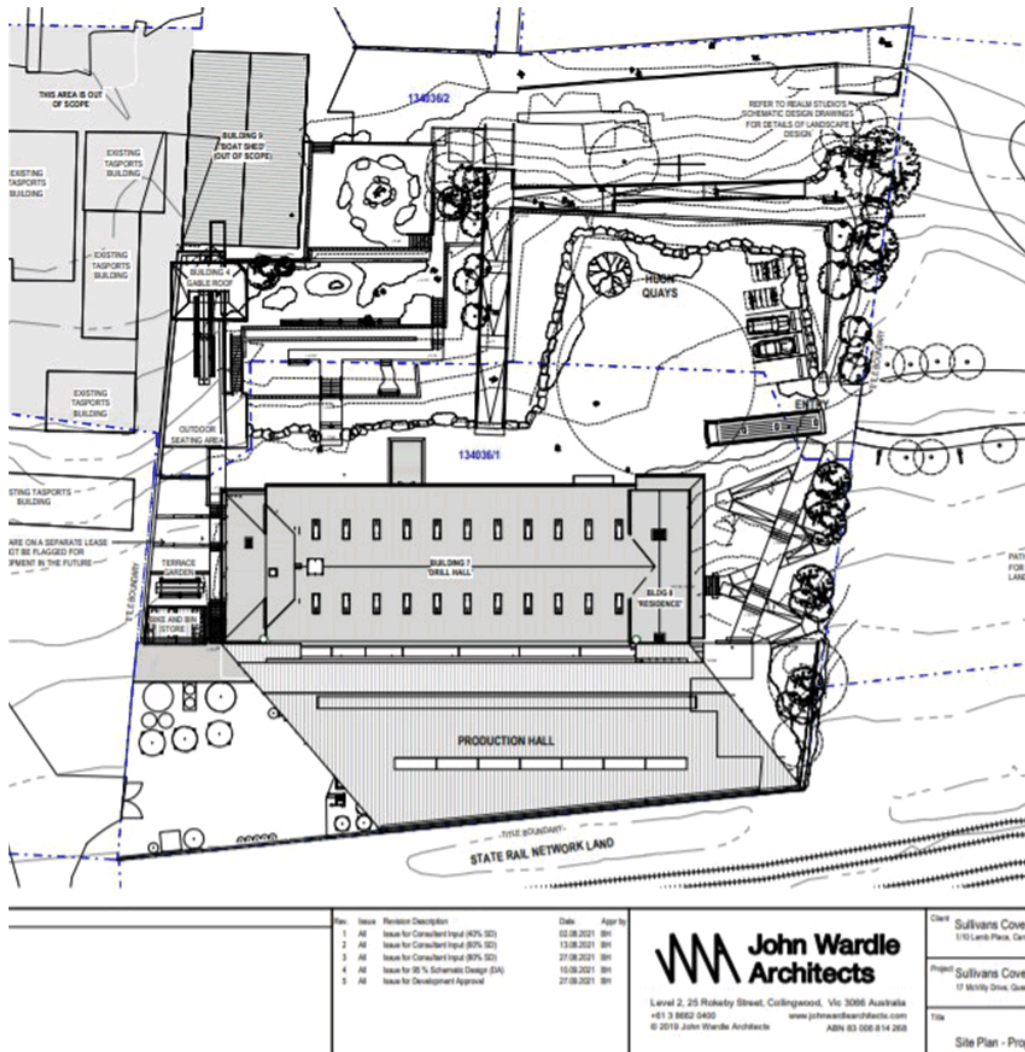
The proposal is for the demolition of the following buildings identified in the above site plan: 1, 2, 4 (although it is shown to be relocated), 5, 6, 13 and 16. Partial demolition of buildings 7 and 8 are also proposed, both internally and externally. Trees are also proposed to be removed.

The demolition is assessed within the supporting heritage documents and summarised above. Where demolition is to occur, the CMP recommends the documentation of the following buildings: 1, 4, 5, 6, 10, 11, 12. This can be achieved by a condition of permit.

The proposal satisfies the guidelines for development control in Schedule 7 - Demolition (cl 28.5) and cl 28.8 Demolition on a Place of Cultural Significance.

New work:

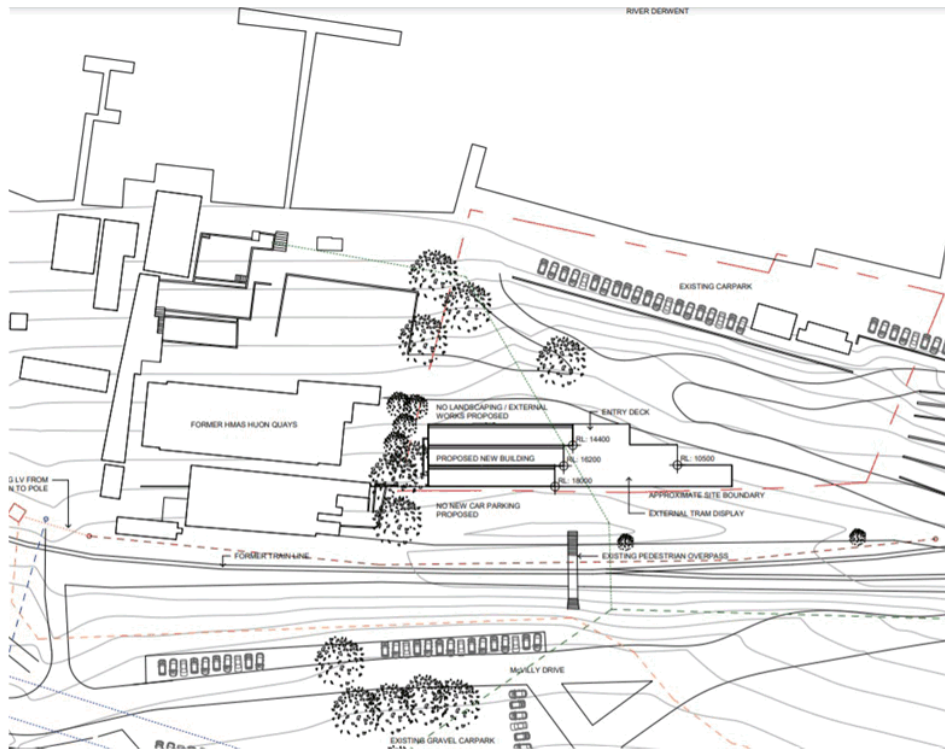
The proposed new production hall connects to the the Drill Hall via a linking corridor. The site plan is shown below.



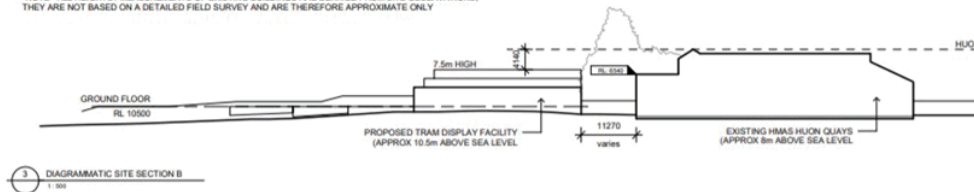
Proposed site plan: Source: Applicant's documentation.

The new building (Production Hall) to the rear of the Drill Hall is a large building, taller than the Drill Hall. It has a RL 21.633 with an asymmetrical angled roof. In comparison, the Drill Hall has a RL 19.558.

The following images shows the relationship and an understanding of comparative heights between the two HMAS Huon site buildings (Drill Hall and proposed) and the recently approved new building for the Tram Depot and Display Facility (PLN-20-182) and show a difference in height of approximately 3.6 metres between the proposed Production Hall and the Tram Sheds and 1.6 metres difference between the Drill Hall and the Tram sheds.



*NOTE - ALL HEIGHTS / MEASUREMENTS OF EXISTING BUILDINGS ARE DERIVED FROM SITE OBSERVATIONS. THEY ARE NOT BASED ON A DETAILED FIELD SURVEY AND ARE THEREFORE APPROXIMATE ONLY



The visual studies show the new structure and associated production tanks obscuring the Drill Hall from the cycle path and oblique angles. This is mitigated by topography of the site and in this regard, a slightly taller building may be considered acceptable in this situation.



Building 2 in the foreground (to be demolished) and the Drill Hall (with chimney and gable ends) which is currently visible. The proposal will obscure the Drill Hall from this angle. Source: Council image



Building 2 in the foreground (to be demolished) and the Drill Hall (with chimney and gable ends) which is currently visible. The proposal will obscure the Drill Hall from this angle. Source: Council image



Building 2 in the foreground (to be demolished) and the Drill Hall (with chimney and gable ends) which is currently visible. The proposal, including tanks, will obscure the Drill Hall from this angle. Source: Council image.

The design of the Production Hall is a reinterpretation of the long buildings on the site with an asymmetrical gable, while underneath the roof the building footprint is stepped with large expanses of glazing to the ends and hardwood cladding. It does not mimic historic forms.

The contemporary interpretation of the weatherboard Drill Hall could be interpreted as being in compliance with the strategy of the approved Conservation Plan, while the location, bulk and appearance could be reconfigured to be more subservient and lesser in scale, the scheme only calls for the proposal **not** to be 'individually prominent'. Had it been sited elsewhere in the Cove, or been of another form and more extreme height differential, then the proposal might be considered to be individually prominent and of an location, bulk and appearance that does adversely affect the heritage values of the listed place. As such, on balance, the proposal considers the above criteria of 22.4.5

Conclusion:

The following conditions are recommended:

A condition of approval is required to finalise the location of building 4 and for it to be restored/reconstructed to an appropriate and more sympathetic earlier state.

A condition of approval will also require the submission of an interpretation plan to ensure the sites history and heritage values are prepared by the applicant and installed on the site. The interpretation on site must be to the approval of Council.

The landscaping plan be revised to retain the tennis court in scale. The tennis court should not be enlarged or greatly modified.

Documentation of buildings to be demolished must be undertaken.

The following advice clauses are recommended:

It is recommended that the trees on the eastern boundary of the site be retained.

It is recommended that revised plans are required to ensure that the rail bridge connection via a pathway takes into account the approved siting of the adjacent

(PLN-20-182 - 20 McVilly Drive - New Building for Museum (Tram Depot and Display Facility))

Sarah Waight
Senior Cultural Heritage Officer
26 April 2022

7.2 APPLICATIONS UNDER THE HOBART INTERIM PLANNING SCHEME 2015

7.2.1 19 BEACH ROAD, SANDY BAY - ALTERATIONS PLN-21-704 - FILE REF: F22/39228

Address:	19 Beach Road, Sandy Bay
Proposal:	Alterations
Expiry Date:	16 May 2022
Extension of Time:	Not applicable
Author:	Cameron Sherriff

RECOMMENDATION

That pursuant to the *Hobart Interim Planning Scheme 2015*, the Council approve the application for alterations, at 19 Beach Road, Sandy Bay 7005 for the reasons outlined in the officer's report and a permit containing the following conditions be issued:

GEN

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-21-704 - 19 BEACH ROAD SANDY BAY TAS 7005 - Final Planning Documents except where modified below.

Reason for condition

To clarify the scope of the permit.

ENG sw1

All stormwater from the proposed development (including but not limited to: roofed areas, ag drains, retaining wall ag drains and impervious surfaces such as driveways and paved areas) must be drained to the Council's stormwater infrastructure prior to first occupation or commencement of use (whichever occurs first).

All works under this permit must occur within the boundaries of the site (CT 156268/1001)

Advice:

Under section 23 of the Urban Drainage Act 2013 it is an offence for a property owner to direct stormwater onto a neighbouring property.

Reason for condition

To ensure that stormwater from the site will be discharged to a suitable Council approved outlet.

ENV 1

Sediment and erosion control measures sufficient to prevent sediment from leaving the site must be installed prior to any disturbance of the site, and maintained until all areas of disturbance have been stabilized or re-vegetated.

Advice:

For further guidance in preparing a Soil and Water Management Plan – in accordance with Fact sheet 3 Derwent Estuary Program click [here](#).

Reason for condition

To avoid the sedimentation of roads, drains, natural watercourses, Council land that could be caused by erosion and runoff from the development, and to comply with relevant State legislation.

ADVICE

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's [website](#) for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

BUILDING PERMIT

You may need building approval in accordance with the *Building Act 2016*. Click [here](#) for more information.

This is a Discretionary Planning Permit issued in accordance with section 57 of the *Land Use Planning and Approvals Act 1993*.

PLUMBING PERMIT

You may need plumbing approval in accordance with the *Building Act 2016*, *Building Regulations 2016* and the National Construction Code. Click [here](#) for more information.

COUNCIL RESERVES

A Public Spaces By-law permit will be required for excavation of the existing stormwater pipe/s and installation of the new stormwater sediment pit. Information about these permits, and the application form to undertake works in a public space, can be found [here](#).

FEES AND CHARGES


Click [here](#) for information on the Council's fees and charges.

DIAL BEFORE YOU DIG

Click [here](#) for dial before you dig information.

Attachment A: PLN-21-704 - 19 BEACH ROAD SANDY BAY TAS
7005 - Planning Committee or Delegated Report ↓



Attachment B: PLN-21-704 - 19 BEACH ROAD SANDY BAY TAS
7005 - CPC Agenda Documents ↓ 

**APPLICATION UNDER HOBART INTERIM PLANNING SCHEME 2015**

Type of Report: Committee
Council: 16 May 2022
Expiry Date: 16 May 2022
Application No: PLN-21-704
Address: 19 BEACH ROAD , SANDY BAY
Applicant: Jesse Hamill (Kings Outdoor Living)
39 Gordon Street
Proposal: Alterations
Representations: Nil
Performance criteria: Coastal Erosion Hazard Code

1. Executive Summary

- 1.1 Planning approval is sought for Alterations, at 19 Beach Road, Sandy Bay.
- 1.2 More specifically the proposal includes:
- Installation of a powdercoated aluminium-framed, gable-roofed shade/shelter awning over the existing first-floor external deck at the northern end of the existing building, currently known as the Seagrass Long Point cafe and restaurant.
 - The awning matches the peak height and profile of the roof of the existing building and covers an area of 8.5m x 3.93m.
 - The awning incorporates a sealed glass balustrade around the deck perimeter, pvc blinds, polycarbonate roofing and gable end infill.
 - No intensification of the existing use is proposed as a result of the installation of the awning. It is intended simply to provide weather protection and a more comfortable outdoor dining space for guests of the cafe/restaurant.
 - The awning will be connected to existing guttering and downpipes, to which a new in ground stormwater pit will be installed within the site.
- 1.3 The proposal relies on performance criteria to satisfy the following standards and codes:
- 1.3.1 E16.0 Coastal Erosion Hazard Code

- 1.4 No representations were received during the statutory advertising period between 07/04 and 26/04/2022.
- 1.5 The proposal is recommended for approval subject to conditions.
- 1.6 The final decision is delegated to the Council, because the subject site is Council-owned.

2. Site Detail

Image 1: Aerial view of the subject property and surrounds.

- 2.1 19 Beach Road, Sandy Bay (Image 1, above) is the 461m² Council-owned site of the Seagrass Long Point Cafe and Restaurant, which is housed within the two-storey building which has existed on the site since the 1950s (Plates 1, 2 and 3, below). The restaurant was once known as Prossers which was established in the building in the mid-1980s. The surrounding area consists of the shore of the River Derwent and the beach and park lands associated with and near to Long and Nutgrove Beaches, with the site situated on the point of land between the two known as Long Point.



Plate 1: The existing building on the subject site. The proposed awning covers a currently open deck on the far end of the building (Source: Planner's Photo).



Plate 2: The existing deck is attached to the northern end of the existing building (Planner's photo).



Plate 3: Wide angle view of the northern elevation of the existing building (Source: Planner's photo).

3. Proposal

3.1 Planning approval is sought for Alterations, at 19 Beach Road, Sandy Bay..

3.2 More specifically the proposal is for:

- Installation of a powdercoated aluminium-framed, gable-roofed shade/shelter awning over the existing first-floor external deck at the northern end of the existing building, currently known as the Seagrass Long Point cafe and restaurant.
- The awning matches the peak height and profile of the roof of the existing building and covers an area of 8.5m x 3.93m.
- The awning incorporates a sealed glass balustrade around the deck perimeter, pvc blinds, polycarbonate roofing and gable end infill.
- No intensification of the existing use is proposed as a result of the installation of the awning. It is intended simply to provide weather protection and a more comfortable outdoor dining space for guests of the cafe/restaurant.
- The awning will be connected to existing guttering and downpipes, to which a new in ground stormwater pit will be installed within the site.

4. Background

- 4.1 General Manager Consent was gained for this application in November 2021 under GMC-21-67.

5. Concerns raised by representors

- 5.1 No representations were received during the statutory advertising period between 07/04 and 26/04/2022.

6. Assessment

- 6.1 The *Hobart Interim Planning Scheme 2015* is a performance based planning scheme. To meet an applicable standard, a proposal must demonstrate compliance with either an acceptable solution or a performance criterion. Where a proposal complies with a standard by relying on one or more performance criteria, the Council may approve or refuse the proposal on that basis. The ability to approve or refuse the proposal relates only to the performance criteria relied on.
- 6.2 The site is located within the Open Space Zone of the *Hobart Interim Planning Scheme 2015*.
- 6.3 The existing use is Food Services. The proposal maintains and does not intensify this existing use. The existing use is a Discretionary use in the zone. Part C 9.2.1 of the *Hobart Interim Planning Scheme 2015* allows proposals for development (excluding subdivision), associated with a use class specified in an applicable Use Table, as a discretionary use, to be considered as if that use class had permitted status in that Use Table, where the proposal for development does not establish a new use, or substantially intensify the use.
- 6.4 The proposal has been assessed against:
- 6.4.1 Part D - 19 Open Space Zone
 - 6.4.2 E7.0 Stormwater Management Code
 - 6.4.3 E11.0 Waterway and Coastal Protection Code
 - 6.4.4 E16.0 Coastal Erosion Hazard Code
- 6.5 The proposal relies on the following performance criteria to comply with the

applicable standards:

6.5.1 Coastal Erosion Hazard Code:

Buildings and Works – Part E 16.7.1 P1

6.6 Each performance criterion is assessed below.

6.7 Buildings and Works (Coastal Erosion Hazard Areas) – Part E 16.7.1 P1

6.7.1 There is no acceptable solution for buildings and works within Coastal Erosion Hazard Area.

6.7.2 The proposal includes the proposed development upon an existing building within a mapped low Coastal Erosion Hazard Area.

6.7.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.

6.7.4 The performance criterion P1 at clause E16.7.1 provides as follows:

Buildings and works must satisfy all of the following:

- (a) not increase the level of risk to the life of the users of the site or of hazard for adjoining or nearby properties or public infrastructure;*
- (b) erosion risk arising from wave run-up, including impact and material suitability, may be mitigated to an acceptable level through structural or design methods used to avoid damage to, or loss of, buildings or works;*
- (c) erosion risk is mitigated to an acceptable level through measures to modify the hazard where these measures are designed and certified by an engineer with suitable experience in coastal, civil and/or hydraulic engineering;*
- (d) need for future remediation works is minimised;*
- (e) health and safety of people is not placed at risk;*
- (f) important natural features are adequately protected;*
- (g) public foreshore access is not obstructed where the managing public authority requires it to continue to exist;*
- (h) access to the site will not be lost or substantially compromised by expected future erosion whether on the proposed site or off-site;*
- (i) provision of a developer contribution for required mitigation works consistent with any adopted Council Policy, prior to commencement of works;*
- (j) not be located on an actively mobile landform.*

- 6.7.5 The Council's Environmental Development Planner has assessed this aspect of the development and provides the following comments:

Approval is sought to construct an awning above an existing concrete deck for a restaurant at 19 Beach Road, Sandy Bay.

The proposed awning would be made from powder-coated aluminium framework and twin-wall polycarbonate roofing with glass balustrades.

Coastal Erosion Hazard Code

The Code applies because development is proposed within a Coastal Erosion Hazard Area (low hazard band).

No exemptions apply.

The relevant standards are under clause E16.7.1. There is no acceptable solution for A1. Performance criterion P1 states the following:

Buildings and works must satisfy all of the following:

- (a) not increase the level of risk to the life of the users of the site or of hazard for adjoining or nearby properties or public infrastructure;*
- (b) erosion risk arising from wave run-up, including impact and material suitability, may be mitigated to an acceptable level through structural or design methods used to avoid damage to, or loss of, buildings or works;*
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- (g) public foreshore access is not obstructed where the managing public authority requires it to continue to exist;*
- (h) access to the site will not be lost or substantially compromised by expected future erosion whether on the proposed site or off-site;*
- (i) provision of a developer contribution for required mitigation works consistent with any adopted Council Policy, prior to*

commencement of works;

(j) not be located on an actively mobile landform.

The development would occur in an area already used by the restaurant so there would be no increase in risk compared with the existing situation. The building is partially protected by a sea wall.

Any erosion risk is more relevant to the existing building than the proposed works, which would be situated on top of the existing building. Wave runup would be an issue for the existing building, not the proposed development.

It is unclear whether future remedial works are likely to be necessary given the partial protection of the sea wall and the expected design life of the development. Remedial works would only be required if the existing building is subject to erosion. It is unlikely that the proposed development could be designed to significantly minimise risk further.

It is not conceivable that the development would be occupied during an erosion event that threatens the structural integrity of the building, so there should be no risk to the health and safety of people as a result of the development.

No natural features would be impacted.

Public access to the foreshore would not be obstructed by the proposed development.

It is considered unlikely that access to the site would be compromised during the expected life of the development given the road is protected by the sea wall and is largely outside the erosion hazard area.

Council does not have a policy on developer contributions.

The development would be located on an existing building.

The application is considered to be consistent with P1 and the exercise of discretion is recommended.

6.7.6 The proposal complies with the performance criterion.

7. Discussion

- 7.1 Planning approval is sought for Alterations, at 19 Beach Road, Sandy Bay.
- 7.2 The application was advertised and no representations were received.
- 7.3 The proposal has been assessed against the relevant provisions of the planning scheme and is considered to perform well.
- 7.4 The proposal has been assessed by other Council officers, including the Council's Stormwater Engineer, Environmental Development Planner, and Open Space Planner. The officers have raised no objection to the proposal, subject to conditions.
- 7.5 The proposal is recommended for approval.

8. Conclusion

- 8.1 The proposed Alterations, at 19 Beach Road, Sandy Bay satisfies the relevant provisions of the *Hobart Interim Planning Scheme 2015*, and as such is recommended for approval.

9. Recommendations

That: Pursuant to the *Hobart Interim Planning Scheme 2015*, the City Planning Committee, in accordance with the delegations contained in its terms of reference, approve the application for Alterations, at 19 Beach Road, Sandy Bay for the reasons outlined in the officer's report and a permit containing the following conditions be issued:

GEN

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-21-704 - 19 BEACH ROAD SANDY BAY TAS 7005 - Final Planning Documents except where modified below.

Reason for condition

To clarify the scope of the permit.

ENG sw1

All stormwater from the proposed development (including but not limited to: roofed areas, ag drains, retaining wall ag drains and impervious surfaces such as driveways and paved areas) must be drained to the Council's stormwater infrastructure prior to first occupation or commencement of use (whichever occurs first).

All works under this permit must occur within the boundaries of the site (CT 156268/1001)

Advice:

Under section 23 of the Urban Drainage Act 2013 it is an offence for a property owner to direct stormwater onto a neighbouring property.

Reason for condition

To ensure that stormwater from the site will be discharged to a suitable Council approved outlet.

ENV 1

Sediment and erosion control measures sufficient to prevent sediment from leaving the site must be installed prior to any disturbance of the site, and maintained until all areas of disturbance have been stabilized or re-vegetated.

Advice:

For further guidance in preparing a Soil and Water Management Plan – in accordance with Fact sheet 3 Derwent Estuary Program click [here](#).

Reason for condition

To avoid the sedimentation of roads, drains, natural watercourses, Council land that could be caused by erosion and runoff from the development, and to comply with relevant State legislation.

ADVICE

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's [website](#) for further information.

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(Cameron Sherriff)

Development Appraisal Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.



(Ben Ikin)

Senior Statutory Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

Date of Report: 27 April 2022

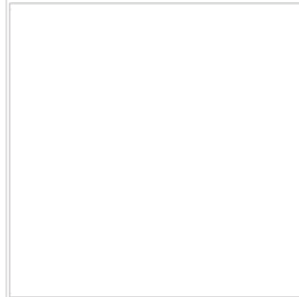
Attachment(s):

Attachment B - CPC Agenda Documents

Planning: #243756

Property

19 BEACH ROAD SANDY BAY TAS 7005

**People**

Applicant

*

Kings Outdoor Living
Jesse Hamill
39 Gordon Street
SORELL TAS 7172
0419628852
jesse@kingsoutdoorliving.com.au

Owner

*

Seagrass Long Point
Mark Escott
19 Beach Road
SANDY BAY TAS 7005
0418 279 118
markescott8@gmail.com

Entered By

JESSE HAMILL
0419 628 852
jesse@kingsoutdoorliving.com.au

Use

Restaurant

Details

Have you obtained pre application advice?

☒ No

If YES please provide the pre application advice number eg PAE-17-xx

Are you applying for permitted visitor accommodation as defined by the State Government Visitor Accommodation Standards? Click on help information button for definition. If you are not the owner of the property you MUST include signed confirmation from the owner that they are aware of this application.

*

☒ No

Is the application for SIGNAGE ONLY? If yes, please enter \$0 in the cost of development, and you must enter the number of signs under Other Details below.

*

<input checked="" type="radio"/> No	
If this application is related to an enforcement action please enter Enforcement Number <input type="text"/>	
Details What is the current approved use of the land / building(s)? *	
<input type="text" value="Commercial"/>	
Please provide a full description of the proposed use or development (i.e. demolition and new dwelling, swimming pool and garage) *	
<input type="text" value="Awning addition over existing concrete deck area"/>	
Estimated cost of development *	
<input type="text" value="35523.00"/>	
Existing floor area (m2)	Proposed floor area (m2) Site area (m2)
<input type="text"/>	<input type="text"/> <input type="text"/>
Carparking on Site	
<div style="text-align: right;">N/A</div> <div style="border: 1px solid black; padding: 2px; display: inline-block;"> <input type="checkbox"/> Other (no selection chosen) </div>	
Total parking spaces	Existing parking spaces
<input type="text"/>	<input type="text"/>
Hours of Business Are the proposed hours of business different from the existing? <input checked="" type="radio"/> No	
What days and hours of operation are proposed for the business?	
Existing	Proposed
From	To
Monday to Friday	Monday to Friday
From	To
Saturday	Saturday
From	To
Sunday	Sunday
From	To
Sunday	Sunday
From	To
Number of Employees List the total number of people who will be working on the site. Proposed number of employees Existing number of employees <input type="text"/> <input type="text"/>	
Goods Deliveries Will there be any commercial vehicles accessing the site? <input checked="" type="radio"/> No	
Type of Vehicle	Trips per Week
Very Large (Semi trailer)	<input type="text"/>
Large	<input type="text"/>
Medium	<input type="text"/>
Small	<input type="text"/>
Outdoor storage / seating / number of beds Is outdoor storage proposed? <input checked="" type="radio"/> No	
Other Details <input type="text"/>	

Does the application include signage?

*

☐ NoHow many signs, please enter 0 if there are none
involved in this application?

*

Tasmania Heritage RegisterIs this property on the Tasmanian Heritage
Register?• ☐ No**Documents****Required Documents**

Title (Folio text and Plan and Schedule of Easements)

*

FolioText-156268-1001.pdf

Plans (proposed, existing)

*

Optimo Awning Plan.jpg

Covering Letter

Cover Letter.pdf

Supporting Documents

Photos or Montages

Existing downpipes under deck.jpg

Folio Plan

FolioPlan-156268-1001.pdf

Schedule of Easements

ScheduleOfEasements-156268-1001.pdf

Boundary Setbacks

Boundary Setbacks.pdf

Engineering 1

Optimo awning engineering standards under 36m2 (1).pdf

Engineering 2

Optimo awning engineering standards under 36m2 (2).pdf

Engineering 3

Optimo awning engineering standards under 36m2 (3).pdf

Engineering 4

Optimo awning engineering standards under 36m2 (4).pdf

Engineering 5

Optimo awning engineering standards under 36m2 (5).pdf



To whom it may concern,

Please find the accompanying documents in relation to an awning addition at 19 Beach Road, Sandy Bay 7005 (Seagrass Long Point).

The proposed awning to be made from powder coated aluminium framework and twin wall polycarbonate roofing. The awning will be 33.405 square metres and the client will have the awning gutters connected into an existing downpipe under the deck by a qualified plumber.

If you require any more information please don't hesitate to ask.

Best regards,

Jesse Hamill

Kings Outdoor Living

39 Gordon Street, Sorell 7172



Enquiries to: City Planning
Phone: (03) 6238 2715
Email: coh@hobartcity.com.au

9 November 2021

Jesse Hamill (Kings Outdoor Living)
39 Gordon Street
SORELL TAS 7172

mailto: jesse@kingsoutdoorliving.com.au

Dear Sir/Madam

**19 BEACH ROAD, SANDY BAY - WORKS ON COUNCIL LAND NOTICE OF LAND
OWNER CONSENT TO LODGE A PLANNING APPLICATION - GMC-21-67**

Site Address:

19 Beach Road, Sandy Bay

Description of Proposal:

Works on Council Land – Awning Addition

Applicant Name:

Jesse Hamill
Kings Outdoor Living

PLN (if applicable):

N/A

I write to advise that pursuant to Section 52 of the *Land Use Planning and Approvals Act 1993*, I grant my consent on behalf of the Hobart City Council as the owner/administrator of the above land for you to make application to the City for a planning permit for the development described above and as per the attached documents. I granted consent pursuant to delegation, a copy of which is enclosed.

Please note that the granting of the consent is only for the making of the application and in no way should such consent be seen as prejudicing any decision the Council is required to make as the statutory planning authority.

Hobart Town Hall
50 Macquarie Street
Hobart TAS 7000

Hobart Council Centre
16 Elizabeth Street
Hobart TAS 7000

City of Hobart
GPO Box 503
Hobart TAS 7001

T 03 6238 2711
F 03 6234 7109
E coh@hobartcity.com.au
W hobartcity.com.au

CityofHobartOfficial
ABN 39 055 343 428
Hobart City Council

This consent does not constitute an approval to undertake any works and does not authorise the owner, developer or their agents any right to enter or conduct works on any Council managed land whether subject to this consent or not.

If planning approval is granted by the planning authority, you will be required to seek approvals and permits from the City as both landlord, land manager, or under other statutory powers (such as other legislation or City By-Laws) that are not granted with the issue of a planning permit under a planning scheme. This includes the requirement for you to reapply for a permit to occupy a public space under the City's Public Spaces By-law if the proposal relates to such an area.

Accordingly, I encourage you to continue to engage with the City about these potential requirements.

Yours faithfully



John Fisher
ACTING DIRECTOR CITY AMENITY

Relevant documents/plans:

Plans - Optimo Awnings - 1559



Approved - General
Manager Consent
GMC 21607 09/11/2021



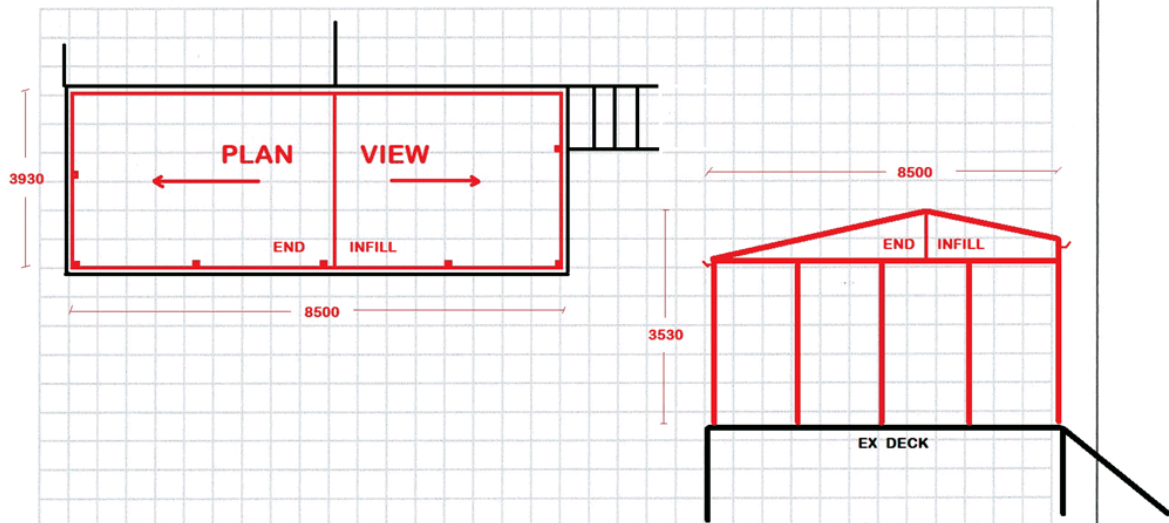
39 Gordon Street, Sorell, 7172
Mobile: 0418 531 393
ABN: 83145735333
Sam King PTY LTD

Quotation Inc. GST

Quotation No 1559
Purchaser: Seagrass Long Point
Address: 19 Beach Road, Sandy Bay TAS 7005
Site Address: AS ABOVE

Date: 29/04/2021
Phone (B) 6227 9229
Phone (H) 0418 279 118

STYLE	FRAME LENGTH	FRAME WIDTH	POST SIZE	FRAME COLOUR	TRIM COLOUR	SHEET COLOUR
Pitched	8500	3930	90	White	Monument	Silver 90%



PLANS/ ENGINEERING	N/A	COUNCIL FEES	No	INSTALLATION	Yes	ENGINEERING COSTS (WHERE APPLICABLE)
FOOTINGS	N/A	GUTTERING	Yes	STORM WATER	No	-

Aluminium powder coated frame
Twinwall polycarbonate roofing

NAME: Seagrass Long Point
ADDRESS: 19 Beach Road, Sandy Bay TAS 7005

Quotation No. 1559

Total \$	35523	Less Deposit Paid	8523	Balance Due \$	27000
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PLEASE NOTE: STORM WATER DRAINAGE CONNECTION IS NOT INCLUDED IN PRICE UNLESS OTHERWISE STATED. OPTIMO AWNINGS HOLDS NO RESPONSABILITY FOR PRE EXISTING LEAKS IN HOUSE GUTTERS. PLEASE CHECK AND REPAIR BEFORE INSTALLATION. COLORS ON QUOTATION ARE AN INDICATION ONLY AND IS THE SOLE RESPONSIBILITY OF THE OWNER/AGENT TO CHECK THE COLORS ABOVE ARE CORRECT.

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

SEARCH OF TORRENS TITLE

VOLUME 156268	FOLIO 1001
EDITION 1	DATE OF ISSUE 27-Jan-2009

SEARCH DATE : 19-Oct-2021

SEARCH TIME : 03.12 PM

DESCRIPTION OF LAND

City of HOBART

Lot 1001 on Sealed Plan 156268 (Section 27A of the Land Titles Act.)

Derivation : Part of 41A-2R-0P Gtd. to George Cartwright and Robert Pitcairn

SCHEDULE 1C871210 TRANSFER to HOBART CITY COUNCIL Registered
27-Jan-2009 at 12.01 PMSCHEDULE 2

Reservations and conditions in the Crown Grant if any

C871210 FENCING PROVISION in Transfer

C871210 REVERSIONARY CONDITIONS set forth in Transfer

UNREGISTERED DEALINGS AND NOTATIONS

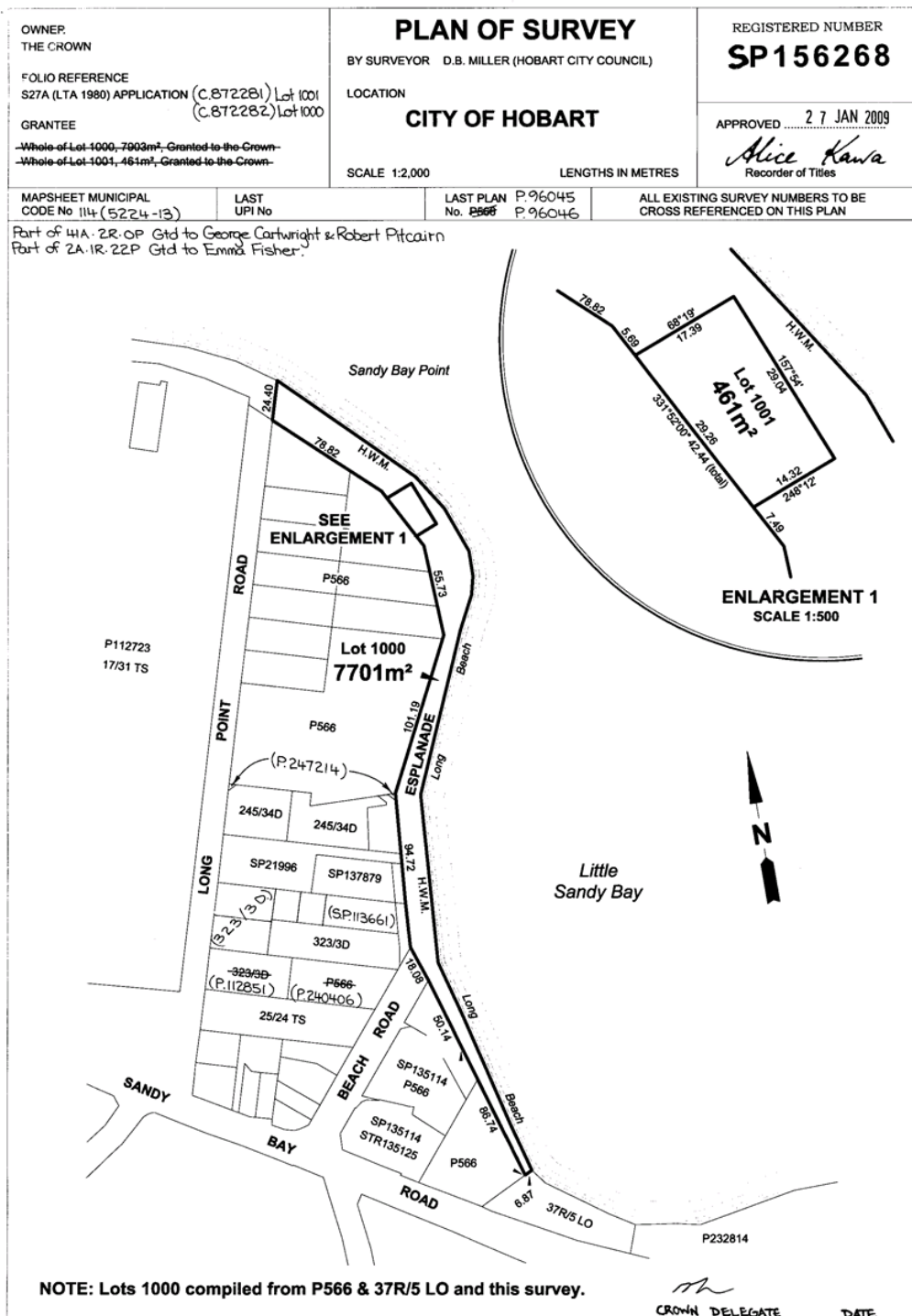
No unregistered dealings or other notations



FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



**SCHEDULE OF EASEMENTS**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



<p align="center">SCHEDULE OF EASEMENTS</p> <p>NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.</p>	<p align="center">Registered Number</p> <p align="center">SP 156268</p>
--	--

PAGE 1 OF 1 PAGE/S

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

No easements covenants or profits a prendre are intended to be created.

SIGNED by **MICHAEL DAVID JONES**)
 being and as the Manager Crown Land Services)
 prescribed in Statutory Rule No. 187 of 2001 and)
 pursuant to an Instrument of Delegation dated)
 25 November 2004 in the presence of:-)


Signature of witness:

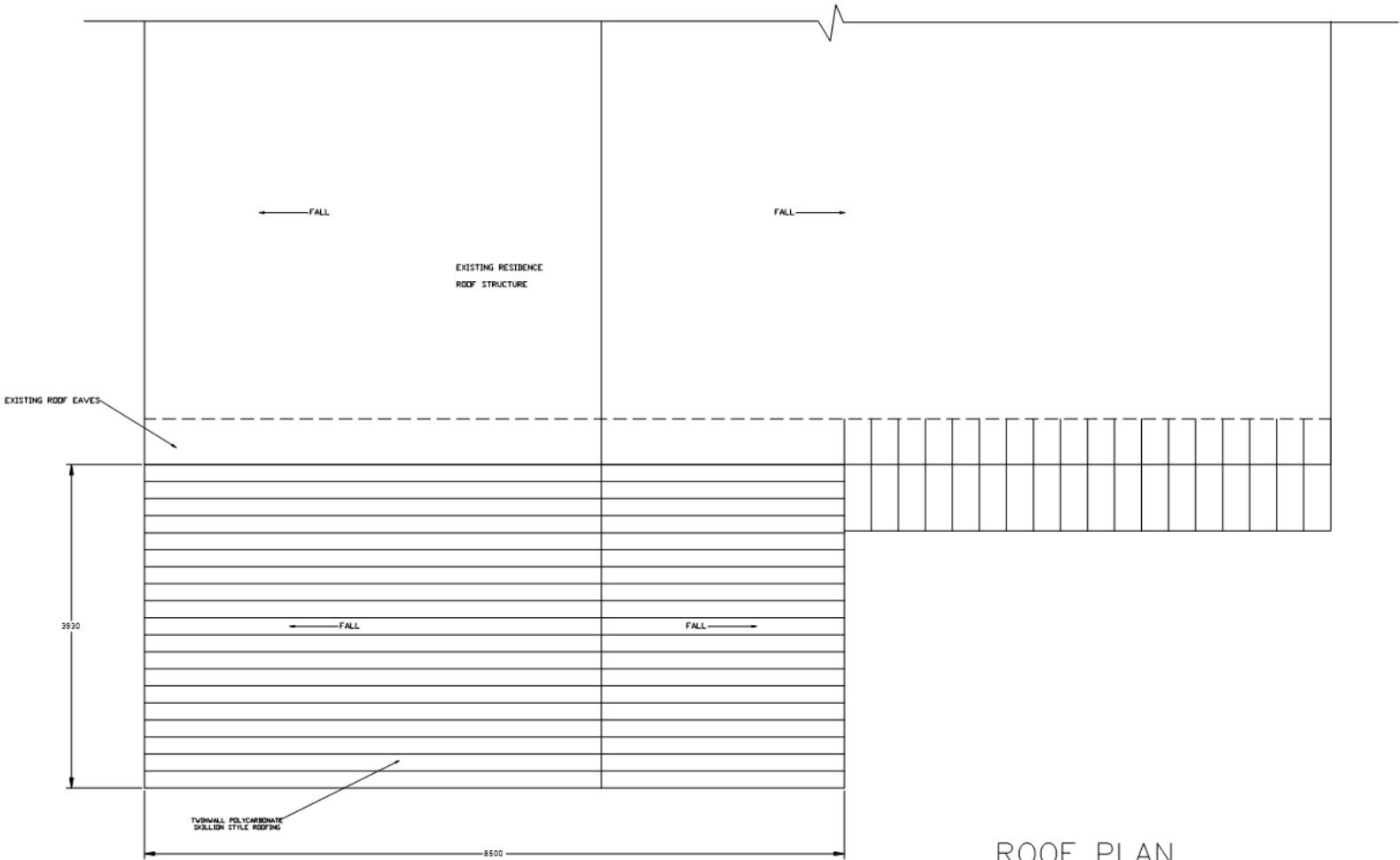
Name of witness: Kelvin Pelham
 Occupation: Property Officer
 Address: Crown Land Services
 134 Macquarie Street
 HOBART

(USE ANNEXURE PAGES FOR CONTINUATION)


<p>SUBDIVIDER: The Crown</p> <p>FOLIO REF: Section 27A</p> <p>SOLICITOR & REFERENCE: Crown Land Services, 20 48 70</p>	<p>PLAN SEALED BY: The Crown</p> <p>DATE: 17 December 2008</p> <p>20 48 70..... REF NO. Crown Delegate</p>
<p>NOTE: The Council Delegate must sign the Certificate for the purposes of identification.</p>	



 <div>Kings Outdoor Living M: 1300 912 195 A: 39 Gordon Street, Sorell, Tasmania, Australia, 7172 W: http://www.kingsoutdoorliving.com.au E: admin@kingsoutdoorliving.com.au ABN: 83 145 735 333</div>	PROJECT NAME PROPOSED AWNING					DRAWN BY Joshua Wing
	LOCATION 19 Beach Road, Sandy Bay, TAS, 7005					SCALE NTS
	DRAWING TITLE BOUNDARY SETBACKS					DRAWING STATUS Planning Approval
						DATE 29/11/2021
						DRAWING NO. KOL-J-3934-01
		Rev	Date	Drawn	Checked / Approved	Remarks



ROOF PLAN
SCALE 1:50



Kings Outdoor Living

M: 1300 912 195
A: 39 Gordon Street, Sorell, Tasmania, Australia, 7172
W: <http://www.kingsoutdoorliving.com.au>
E: admin@kingsoutdoorliving.com.au
ABN: 83 145 735 333

Outdoor Shade Sails, Awnings, Umbrellas, Sun Rooms, Blends, Screens, Glass Walkways and Decks

PROJECT NAME
PROPOSED AWNING

LOCATION
19 Beach Road, Sandy Bay, TAS, 7005

DRAWING TITLE
FLOOR AND ROOF PLAN

Rev	Date	Drawn	Checked / Approved	Remarks

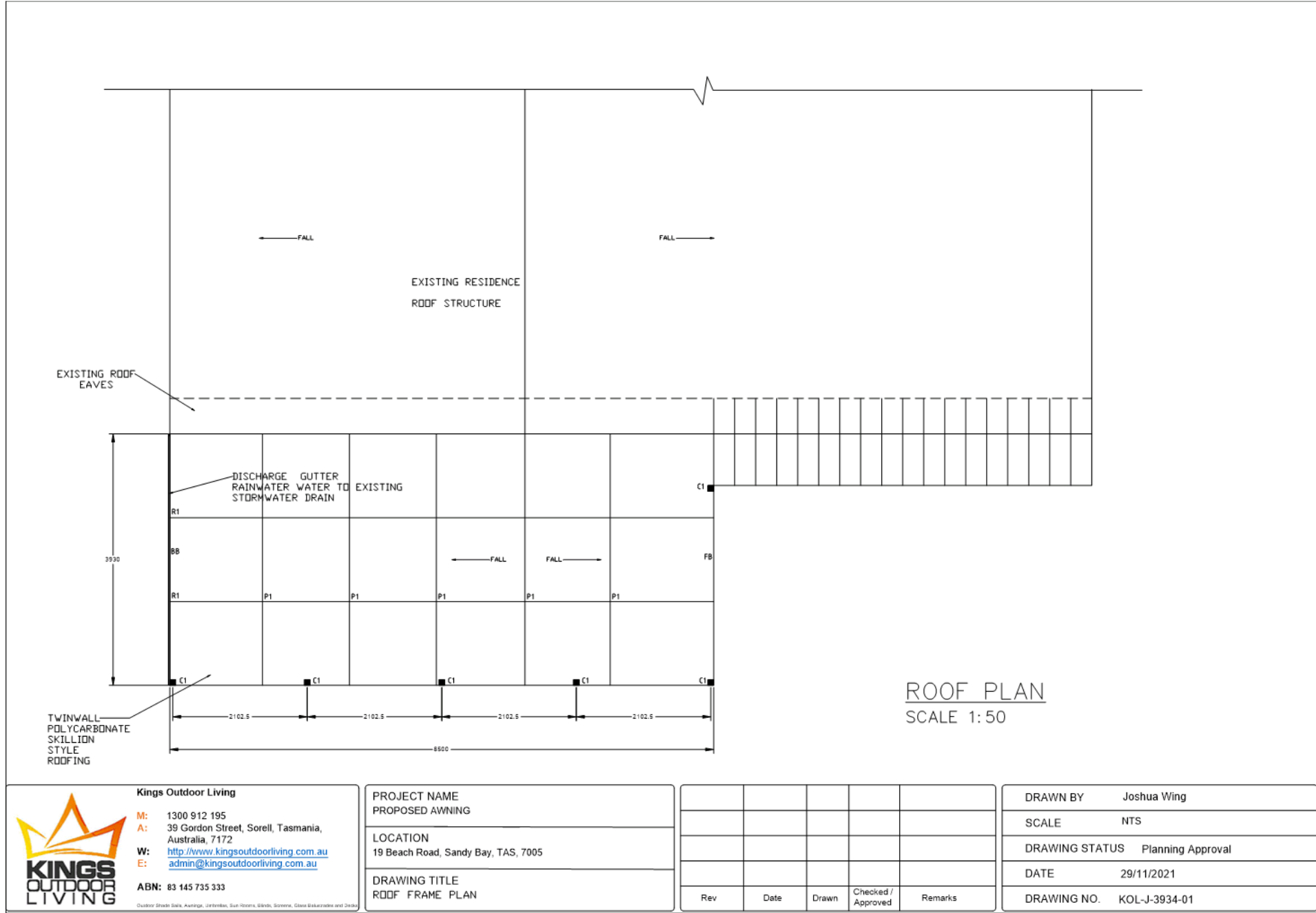
DRAWN BY Joshua Wing

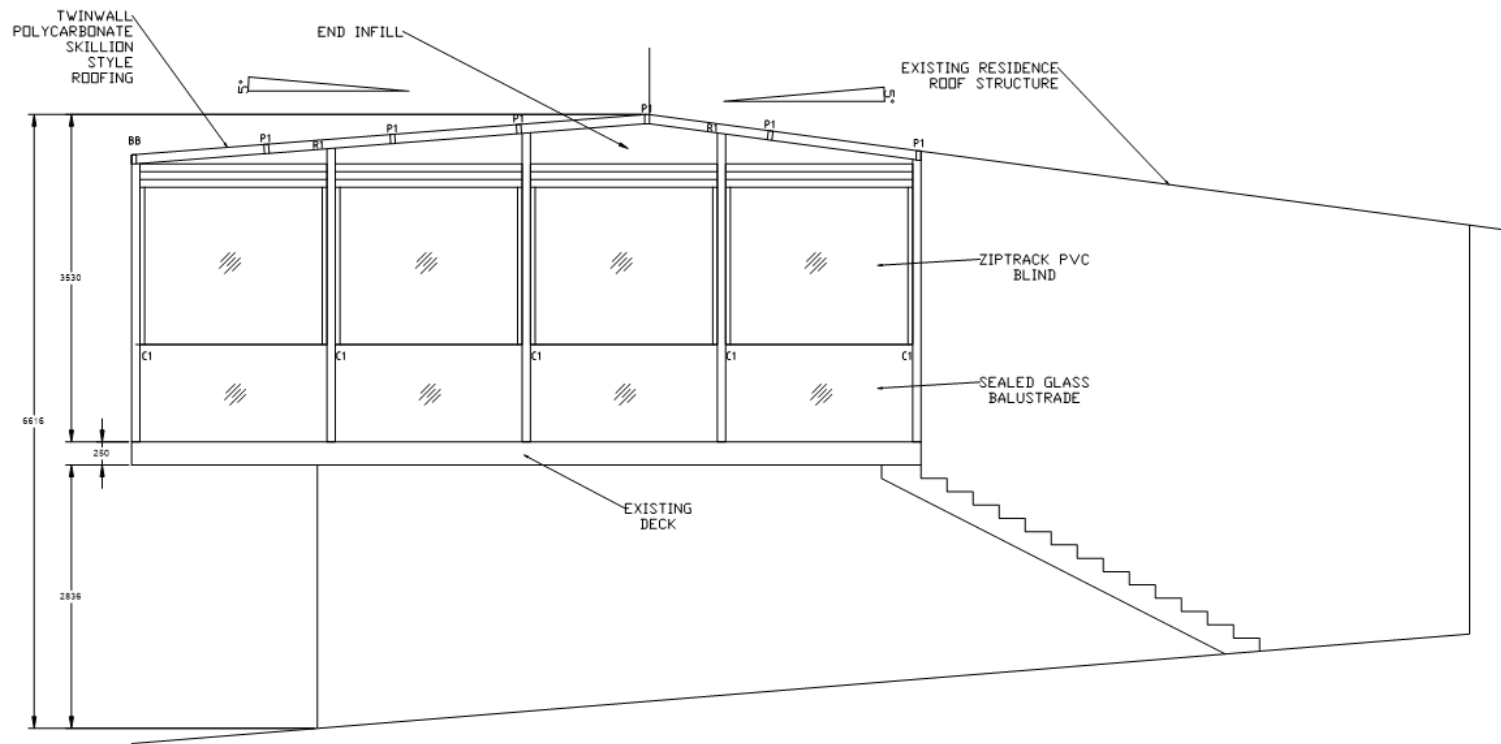
SCALE NTS

DRAWING STATUS Planning Approval

DATE 29/11/2021

DRAWING NO. KOL-J-3934-01





MEMBER SCHEDULE		
MARK	SIZE	REMARKS
BB	100 x 50 RHS ALUMINIUM	REFER DETAIL 10
C1	90 x 10 SHS ALUMINIUM	
R1	100 x 50 RHS ALUMINIUM RAFTER	REFER DETAIL 10
P1	100 x 50 RHS ALUMINIUM	REFER DETAIL 10

CONFIRM ALL DIMENSIONS ON SITE

GENERAL NOTES

1. ALL JOINTS MADE WITH INTERNAL BRACKET SYSTEM FASTENED WITH 4 TEK SCREWS PER JOINT.
2. ALL ALUMINIUM TO AS1664
3. ALL ALUMINIUM TO BE POWDER COATED TO OWNERS SPECIFICATION.

NOTE:
DURING INSTALLATION, CONTRACTOR IS TO CHECK & CONFIRM THAT FRAMING HAS BEEN INSTALLED AND TIED DOWN IN ACCORDANCE WITH AS1684-2010 RESIDENTIAL TIMBER FRAMED CONSTRUCTION

FRONT ELEVATION (NORTH WEST)

SCALE 1:50

**Kings Outdoor Living**

M: 1300 912 195
A: 39 Gordon Street, Sorell, Tasmania, Australia, 7172
W: <http://www.kingsoutdoorliving.com.au>
E: admin@kingsoutdoorliving.com.au

ABN: 83 145 735 333

Outdoor Shade Sails, Awnings, Umbrellas, Sun Rooms, Blends, Screens, Glass Balustrades and Decking

PROJECT NAME
PROPOSED AWNINGLOCATION
19 Beach Road, Sandy Bay, TAS, 7005DRAWING TITLE
FRONT ELEVATION AND DETAILS

Rev	Date	Drawn	Checked / Approved	Remarks

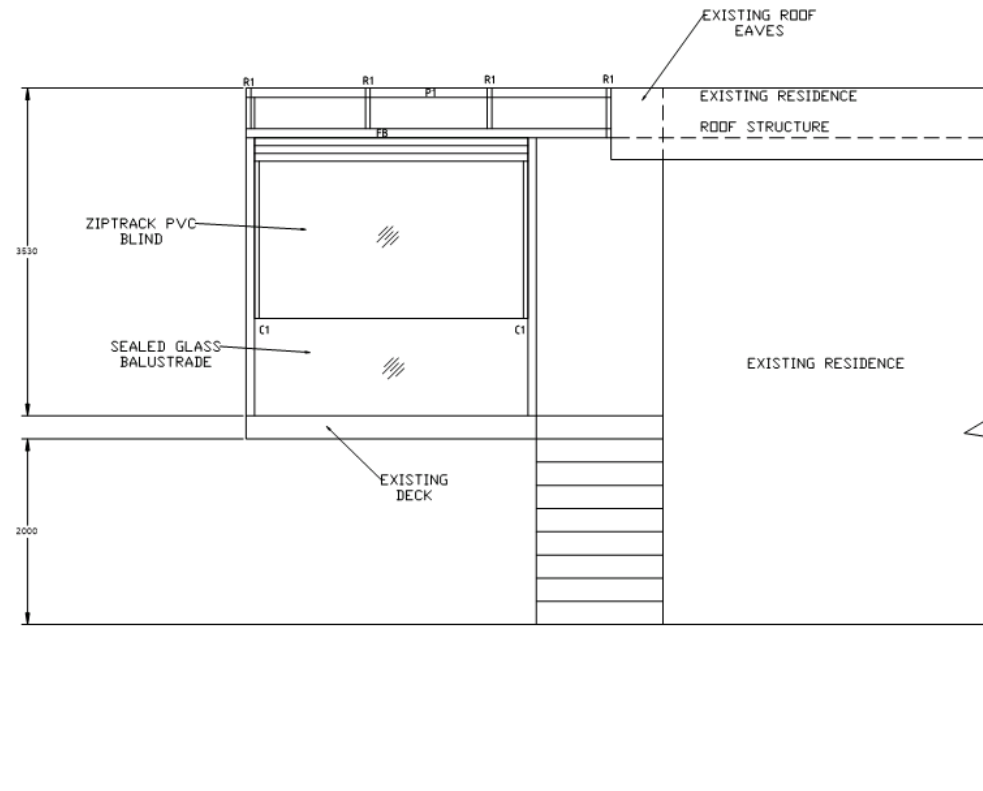
DRAWN BY Joshua Wing

SCALE NTS

DRAWING STATUS Planning Approval

DATE 29/11/2021

DRAWING NO. KOL-J-3934-01



SIDE ELEVATION (NORTH EAST)

SCALE 1:50

**Kings Outdoor Living**

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E: admin@kingsoutdoorliving.com.au

ABN: 83 145 735 333

Outdoor Shade Sails, Awnings, Umbrellas, Sun Rooms, Blinds, Screens, Glass Balustrades and Decking

PROJECT NAME
PROPOSED AWNINGLOCATION
19 Beach Road, Sandy Bay, TAS, 7005DRAWING TITLE
SIDE ELEVATION AND DETAILS

Rev	Date	Drawn	Checked / Approved	Remarks

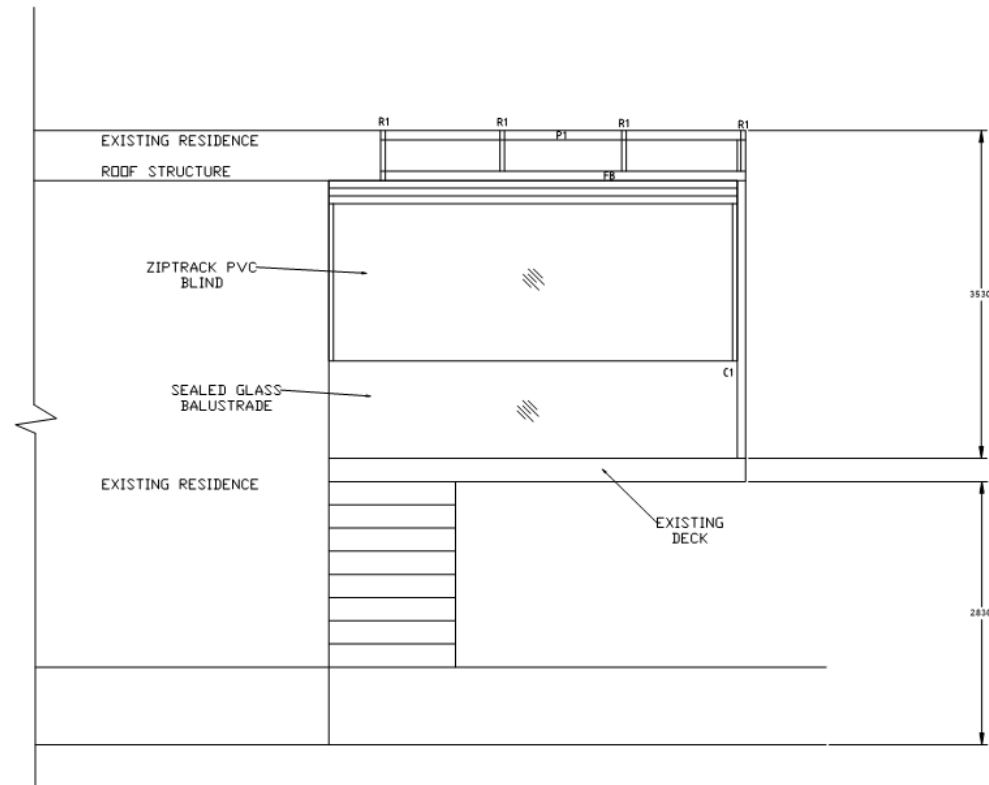
DRAWN BY Joshua Wing

SCALE NTS

DRAWING STATUS Planning Approval

DATE 29/11/2021

DRAWING NO. KOL-J-3934-01



SIDE ELEVATION (SOUTH WEST)

SCALE 1:50

**Kings Outdoor Living**

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 A: 39 Gordon Street, Sorell, Tasmania,
 Australia, 7172
 W: <http://www.kingsoutdoorliving.com.au>
 E: admin@kingsoutdoorliving.com.au

ABN: 83 145 735 333

Outdoor Shade Sails, Awnings, Umbrellas, Sun Rooms, Blinds, Screens, Glass Balustrades and Decks

PROJECT NAME
PROPOSED AWNINGLOCATION
19 Beach Road, Sandy Bay, TAS, 7005DRAWING TITLE
SIDE ELEVATION AND DETAILS

Rev	Date	Drawn	Checked / Approved	Remarks

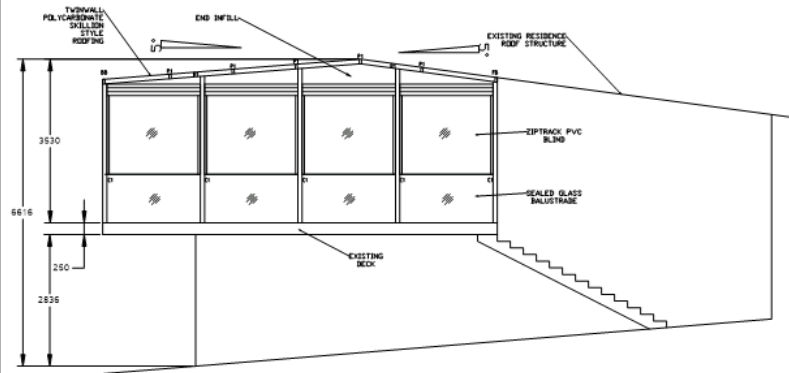
DRAWN BY Joshua Wing

SCALE NTS

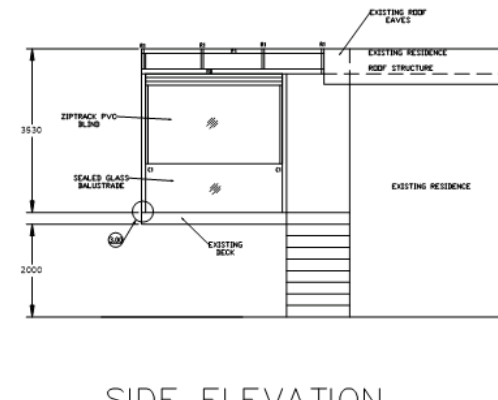
DRAWING STATUS Planning Approval

DATE 29/11/2021

DRAWING NO. KOL-J-3934-01



FRONT ELEVATION
SCALE 1:100



SIDE ELEVATION
SCALE 1:100

MEMBER SCHEDULE		
MARK	SIZE	REMARKS
BB	100 x 50 RHS ALUMINIUM	REFER DETAIL 1.0
C1	90 x 90 SHS ALUMINIUM	
R1	100 x 50 RHS ALUMINIUM RAFTER	REFER DETAIL 1.0
P1	100 x 50 RHS ALUMINIUM	REFER DETAIL 1.0

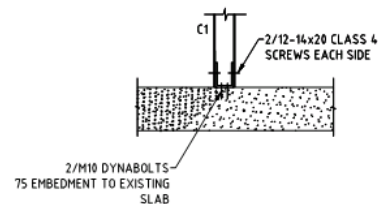
CONFIRM ALL DIMENSIONS ON SITE

GENERAL NOTES

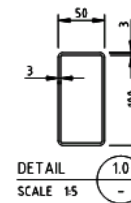
1. ALL JOINTS MADE WITH INTERNAL BRACKET SYSTEM FASTENED WITH 4 TEK SCREWS PER JOINT.
2. ALL ALUMINIUM TO AS1664
3. ALL ALUMINIUM TO BE POWDER COATED TO OWNERS SPECIFICATION.

NOTE:

DURING INSTALLATION, CONTRACTOR IS TO CHECK & CONFIRM THAT FRAMING HAS BEEN INSTALLED AND TIED DOWN IN ACCORDANCE WITH AS1684-2010 RESIDENTIAL TIMBER FRAMED CONSTRUCTION



DETAIL
SCALE 1:20



DETAIL
SCALE 1:5



Kings Outdoor Living

M: 1300 912 195
A: 39 Gordon Street, Sorell, Tasmania, Australia, 7172
W: <http://www.kingsoutdoorliving.com.au>
E: admin@kingsoutdoorliving.com.au

ABN: 83 145 735 333

Outdoor Shade Sails, Awnings, Verandas, Sun-Rooms, Blends, Screens, Glass Balustrades and Decking

PROJECT NAME
PROPOSED AWNING

LOCATION
19 Beach Road, Sandy Bay, TAS, 7005

DRAWING TITLE
ELEVATION AND DETAILS

Rev	Date	Drawn	Checked / Approved	Remarks

DRAWN BY Joshua Wing


SCALE NTS

DRAWING STATUS Planning Approval

DATE 29/11/2021

DRAWING NO. KOL-J-3934-01

RE: Council

 Bernadette Woods <bernadette@bwevents.com.au>
To: Jesse Hamill
Cc: 'Mark Escott Escott'

 You replied to this message on 17/11/2021 12:14 PM.

Hi Jess

It had no impact on how we operate , its primary purpose is to provide weather protection:
1. Summer to protect against sun.....quite often on sunny days guest can not sit out there as it is too hot and too much glare.
2. Winter again no protection from the cold rain and wind.
It just provides a more comfortable space for our guest all year round regardless of the weather.

I look forward to talking with you soon.

Kind Regards,

Bernadette Woods
Creative Director and Founder
P: (03) 6227 9229 E: bernadette@bwevents.com.au

Life's too short to eat bad food.



glenalbyn.com.au



seagrassstasmania.com



bwevents.com.au

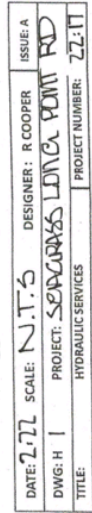


glenalbyn.com.au

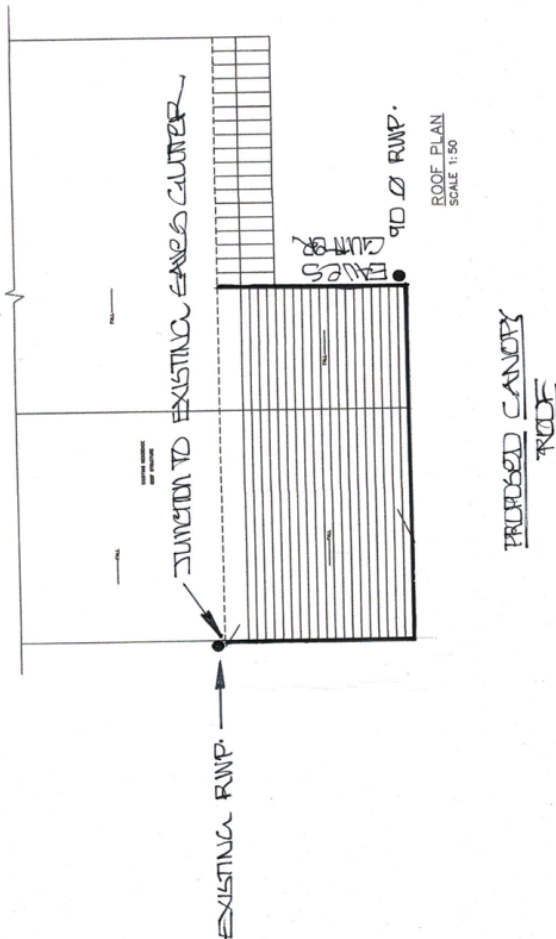
 Reply  Reply All  Forward 

Tue 16/11/2021 9:04 PM

From: Mark Escott <markescott@gmail.com>
Sent: Tuesday, 16 November 2021 9:00 PM
To: Bernadette Woods <bernadette@bwevents.com.au>
Subject: Fwd: Council



DEC LUNDAVILIR DESIGN CONSULTANTS CC963M



SCOPE AND NOTES- SEAGRASS RESTAURANT

THE PROJECT IS FOR THE ADDITIONAL OF A ROOFED AREA ABOVE AN EXISTING DECKED SPACE AT THE NORTH WESTERN END OF THE EXISTING RESTAURANT BUILDING. THE BUILDING IS CURRENTLY SERVED BY A PRIVATE STORMWATER SYSTEM WITH A DISCHARGE TO THE DERWENT RIVER VIA THE DRAINAGE SYSTEM BEHIND THE EXISTING SEA WALL. CURRENTLY ALL THE PAVED AREA AT THE TOP OF THE SEAWALL DISCHARGES DIRECTLY TO THE DERWENT RIVER WITHOUT ANY MEASURES TO PROVIDE SILT COLLECTION. THE PROPOSAL IS TO FORM A NEW CONNECTION INTO THE EXISTING STORMWATER DRAINAGE SYSTEM AS DESIGNED BUT INSERTING A SILT RETENTION PIT OVER THE EXISTING DRAINAGE OUTLET (WITHIN THE PROPERTY BOUNDARY). THE EXISTING STORMWATER DRAINAGE SYSTEM HAS BEEN SUBJECTED TO DYE AND FLOW TESTS TO DETERMINE ANY POINTS OF FAILURE - NO ISSUES WERE IDENTIFIED.

THE CONTRACTOR SHALL:

- LODGE A COMMENCEMENT OF WORK NOTICE WITH HOBART CITY COUNCIL (HCC) BEFORE COMMENCING ANY NEW WORK.
- WORK ONLY FROM APPROVED DRAINAGE PLANS (HCC STAMPED AND DATED 2022)
- ALLOW TO EXPOSE THE EXISTING STORMWATER DRAINAGE SYSTEM WITHIN THE PROPERTY BOUNDARY, CHECK FOR PIPE SIZE AND CONDITION AND CONSTRUCT A NEW SPEL OR EQUAL STORMSACK SILT RETENTION PIT WITH HEEL SAFE GRATE AS DETAILED.
- ARRANGE FOR HCC INSPECTIONS OF ANY NEW WORK PRIOR TO CONCEALMENT.
- ALLOW TO PROVIDE ANY NECESSARY SAFETY BARRIERS, LIGHTS AND WARNINGS TO THE PUBLIC FOR ANY WORK AREAS, EXCAVATIONS AND THE LIKE.
- SUPPLY AND FIT NEW RAINWATER PIPE DROPPERS FROM THE PROPOSED CANOPY ROOF TO DISCHARGE TO THE EXISTING STORMWATER DRAINAGE SYSTEM.
- AT COMPLETION PROVIDE THE PROPERTY OWNER AND HCC WITH A COPY OF AS CONSTRUCTED DRAWINGS OF ANY SUBSURFACE OR CONCEALED PIPE INSTALLATIONS.
- ENSURE THAT ALL NEW EAVES GUTTERS ARE SLOTTED FRONT TYPE.

BENCHMARK STANDARDS ARE AS/NZS3500, NCC AND ANY CONDITIONS IMPOSED BY HCC, THE SCOPE OF WORKS IS NOT EXHAUSTIVE, LATENT CONDITIONS MAY ARISE DURING THE CONSTRUCTION PHASE THAT REQUIRE AMENDMENT, ALL ALTERATIONS MUST BE APPROVED BY THE DESIGNER.

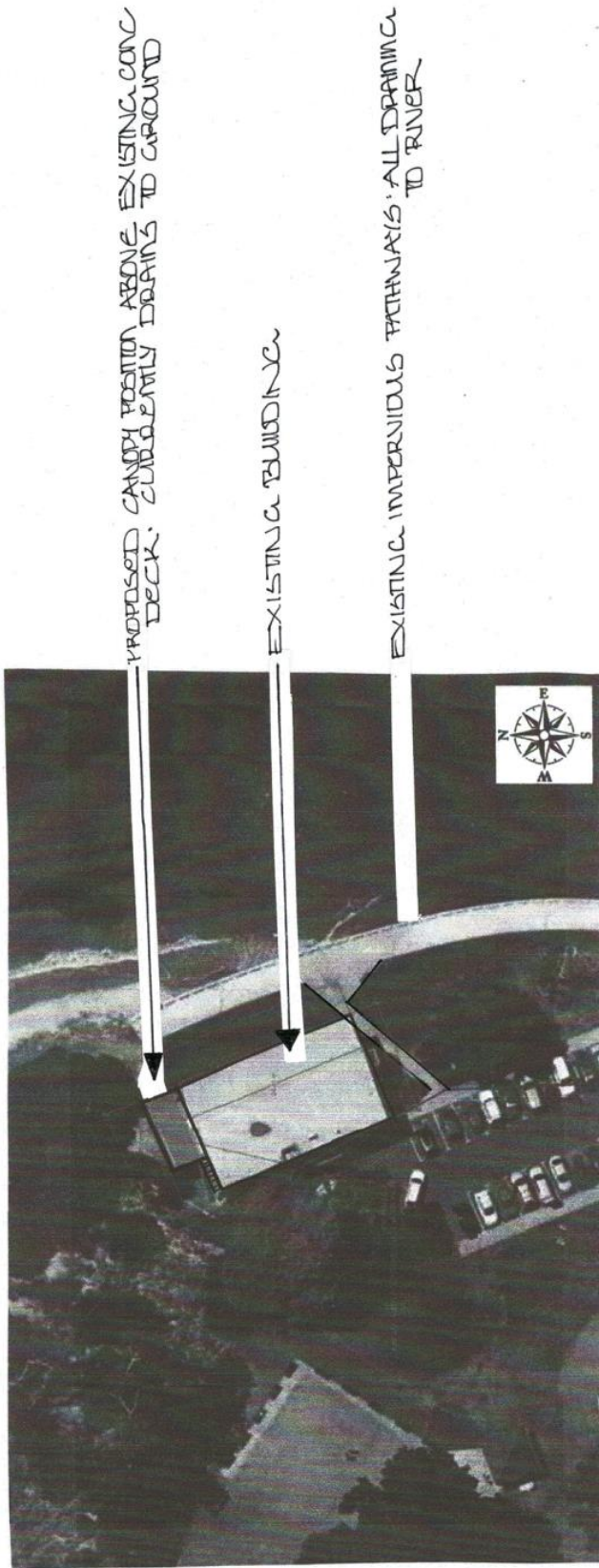
CLIMATE CHANGE IS AN UNKNOWN FACTOR AND IS THEREFORE UNQUANTIFIABLE. THIS DESIGN IS PREPARED BASED ON STANDARDS AND CODES RELEVANT AND CURRENT AT THE DATE OF THE DESIGN. REC HYDRAULIC P/L DESIGN DO NOT ACCEPT ANY RESPONSIBILITY FOR THE FUTURE EFFECT OF DROUGHT, UNSEASONAL RAINFALL EVENTS, LACK OF SYSTEM MAINTENANCE OR OTHER OCCURRENCES RESULTING FROM THE EFFECTS OF FUTURE CLIMATE CHANGE. DATED FEBRUARY 2022 ONGOING RESPONSIBILITY FOR MAINTENANCE OF THE SYSTEM REMAINS WITH THE PROPERTY OWNER.

19 DERWENT ROAD SANDY BAY

DATE: 1/12	SCALE: N.T.S	DESIGNER: R COOPER	ISSUE: A
DWG: H 2	PROJECT: SEAGRASS-LONG POINT RD		
TITLE:	HYDRAULIC SERVICES	PROJECT NUMBER:	17-0

EMAIL: info@rechvd.com.au Tel: 0418 597 741

P E C HYDRAULIC DESIGN CONSULTANTS CC963M



19 BEACH ROAD SANDY BAY

DATE: 2-22	SCALE: NTS	DESIGNER: R COOPER	ISSUE: A
DWG: H	3	PROJECT: SPA CROSSING FROM RD	
TITLE:		HYDRAULIC SERVICES	PROJECT NUMBER: 22-17

EMAIL: info@rechvd.com.au Tel: 0418 597 741

R F C HYDRAULIC DESIGN CONSULTANTS CC963M

144 Lewisham Scenic Drive
LEWISHAM 7173
0418 597 741
E: info@rechyd.com.au

**REC Hydraulic
Design P/L**

Designers Direction - 01

To:	Hobart City Council	From:	Rod Cooper
E	Moshen Motiei SW Engineer	Pages:	1
Tel		Date	March 31 st 2022
Re:	Stormwater Drain 19 Beach Road Sandy Bay	cc:	Jesse Hamill

☐ Urgent ☐ For Review ☐ Please Comment ☐ Please Reply ☒ Please Recycle

- Reference is made to proposed stormwater extension at 19 Beach Road Sandy Bay.
- From discussion today (31.3.22) with Moshen Motei we confirm that there is no intention to extend the stormwater drain serving the proposed awning outside the title to the building.
- The proposed surcharge pit must be retained within the title boundaries.

Rod Cooper

REC Hydraulic Design P/L

March 31st 2022



CERTIFICATE OF THE RESPONSIBLE DESIGNER

Section 94
Section 106
Section 129
Section 155Form **35**

To: Owner name
 Address
 Suburb/postcode

Designer details:

Name: Category:
 Business name: Phone No:
 Business address:
 Fax No:
 Licence No: Email address:

Details of the proposed work:

Owner Designer's project reference No.
 Lot No:

Type of work: Building work ☐ Plumbing work ☒ (X all applicable)

Description of work:

Description of the Design Work (Scope, limitations or exclusions): (X all applicable certificates)

Certificate Type:	Certificate	Responsible Practitioner
	<input type="checkbox"/> Building design	Architect or Building Designer
	<input type="checkbox"/> Structural design	Engineer or Civil Designer
	<input type="checkbox"/> Fire Safety design	Fire Engineer
	<input type="checkbox"/> Civil design	Civil Engineer or Civil Designer
	<input checked="" type="checkbox"/> Hydraulic design	Building Services Designer
	<input type="checkbox"/> Fire service design	Building Services Designer
	<input type="checkbox"/> Electrical design	Building Services Designer
	<input type="checkbox"/> Mechanical design	Building Service Designer
	<input checked="" type="checkbox"/> Plumbing design	Plumber-Certifier; Architect, Building Designer or Engineer
	<input type="checkbox"/> Other (specify)	
Deemed-to-Satisfy: <input checked="" type="checkbox"/> Performance Solution		
Other details: Does not require approval from Taswater.		

Design documents provided:			
The following documents are provided with this Certificate – <i>Document description:</i>			
Drawing numbers::22:17 H2(A) H3(A)	H1	Prepared by: REC Hydraulic Design P/L	Date 20.2.22
Schedules:		Prepared by:	Date
			Date:
Computations:		Prepared by	Date:
Performance solution proposals:		Prepared by:	Date:
Test reports:			
Standards, codes or guidelines relied on in design process:			
AS/NZS 3500, NCC, and specific requirements of Hobart City Council			
Any other relevant documentation:			
.REC Hydraulic Design Consultants have not been engaged to provide and site supervision.			
Attribution as designer:			
<p>I Rod Cooper am responsible for the design of that part of the work as described in this certificate;</p> <p>The documentation relating to the design includes sufficient information for the assessment of the work in accordance with the <i>Building Act 2016</i> and sufficient detail for the builder or plumber to carry out the work in accordance with the documents and the Act;</p> <p>This certificate confirms compliance and is evidence of suitability of this design with the requirements of the National Construction Code.</p>			
	<i>Name: (print)</i>	<i>Signed</i>	<i>Date</i>
Designer:	Rod Cooper	<i>R J Cooper</i>	20.02.22
Licence No:	1010321 & CC963M		
Assessment of Certifiable Works: (TasWater)		NOT A REQUIRED ITEM	

PLUMBING WORK

Use this form for:

- Notice of Work
- Application for a Certificate of Likely Compliance
- Application for a Plumbing Permit

Section 108
Section 156
Section 165

To: Permit Authority
 Address
 Suburb/postcode

Form **3**

Application for: Permit ☐ CLC ☒ Notice of Work ☐ (X ones applicable)

Certificate of Completion ☐ (X to grant approval for certificate to be issued following the final inspection)

NOTE: Standard of Work Certificate and applicable fees must be submitted prior to Certificate of Completion being issued, in accordance with section 178 or section 115 of the *Building Act 2016*

Applicant / details:

Note: Only an owner or agent of the owner may make an application

Owner: Contact person:
Address: Phone No:
 Fax No:
 Email address:
Agent:
Address: Phone No:
 Fax No:

Note: Agents to be authorised in writing by the owner

Details of plumbing work:

Type of work: Permit work ☐ Notifiable work ☒ Planning approval granted ☐
 (X one applicable) (if applicable)

Lot No:
 Certificate of title No:

The work: (water or sewerage reticulation / stormwater / roof plumbing / on-site waste water management system / backflow prevention device / other)

Type of plumbing installation:
Brand / model:

Use of building: (main use: dwelling, shop, food business, factory) Building class(es):

Plumber details:

Name: Category:
 Address: Phone No:
 Fax No:

Licence No. Email address:

Plumbing designer details:

Name:	<input type="text" value="REC Hydraulic Design P/L"/>	Category:	<input type="text" value="Services Design"/>
Address:	<input type="text" value="144 Lewisham Scenic Drive"/>	Phone No:	<input type="text" value="0418 597 741"/>
	<input type="text" value="Lewisham TAS"/>	<input type="text" value="7173"/>	Fax No: <input type="text" value="N/A"/>
Licence No.	<input type="text" value="1010321 & CC963M"/>	Email address:	<input type="text" value="info@rechyd.com.au"/>

Documents provided:

The following documents are provided with this application -

Document description:

Prepared by:

<p>Documents as specified in Schedule 2 of the Director's Specified List:</p> <p>Hydraulic design documentation and Form 35</p>	<p>REC HYDRAULIC DESIGN CONSULTANTS P/L</p> <p>ROD COOPER CC963M</p>
---	--

The plumbing work will be carried out in accordance with the *Building Act 2016*, the *Building Regulations 2016* and the National Construction Code.

	<i>Name: [print]</i>	<i>Signed</i>	<i>Date</i>
Owner to sign here	<input type="text"/>	<input type="text"/>	<input type="text" value="22.2.22"/>

PROJECT SAFETY REPORT

PROJECT ADDRESS: 19 BEACH ROAD SANDY BAY TASMANIA

REC Hydraulic Design Consultants P/L has been engaged to provide the following design services for this project. Our work is limited to the following:

- Design for hydraulic Roof drainage and stormwater management for a proposed canopy over an existing deck..
- The function of this Project Safety Report is to identify potential health and safety hazards associated with the project as a workplace during its construction until hand over to the client. Ongoing responsibility for site safety and the finished project lies with the property owner.

▪

Project description

The proposal is to achieve compliance for hydraulic installations as described above

Contractors Construction Sequence

- Establish site, identify & protect existing services;
- Excavate for drainage and various extensions.
- Installation of all pipework, trenching to meet WH & S requirements.
- Backfill and make good all existing surfaces disturbed elements and trenches.
- Make good to any damaged elements.

1.1 Existing site

The following site hazards may exist at the site in its existing state:

- Underground services/ power, comms,
- Overhead services: power/communication
- Road/access: site lines/construction access/geometry/strength
- Pedestrian Safety

1.2 Consultation

For this project we have consulted with the following people or organisations to understand the project and quantify any workplace health and safety risks in the areas covered by our work:

- Building Plans
- Site Assessment
- HCC Information

Risk identification by REC Hydraulic Design P/L begins during the site investigation phase, and ends with the issue of construction documentation. The process includes the identification of existing site hazards, a review for safe constructability of designed elements, in service and maintenance hazards for the completed building, and any special demolition issues. The process for managing identified risks remains the responsibility of the contractor. Information provided by REC Hydraulic Design is not exclusive – correct WH&S procedures must be followed by the contractor.

2 Hazard Risk Identification and Risk Management Recommendations.

REC hydraulic Design Consultants P/L have not been engaged to provide any contract administration input to the project at this stage. Contractors shall be mindful of the need to develop safety plans for potential public activity adjacent. All contractors are required to provide their own Safety Management Plans to the property owner prior to commencing any work inclusive of preliminary site assessment. The SMP is to be signed and dated by all parties and include a section for employees to sign off on their understanding of the safety and risk issues.

Contractors Shall:

- ***Allow to identify the location of all existing subsurface services prior to commencing any new work.***
- ***The building is of an age where material containing asbestos fibre may be present, if encountered the contractor is to observe all safety requirements.***

Note that Codes of Practice are available via the Worksafe Tasmania Website, the non exhaustive list below is a reference:

- Safe Design of Structures.
- Work Health and Safety Cooperation and Coordination.
- Managing the Work Environment and Facilities.
- Managing the Risks of Plant in The Workplace.
- Managing the Risk of Falls in The Workplace.
- Managing Noise and Preventing Hearing Loss at Work.
- Managing Electrical Risks in The Workplace.
- How to Manage Work Health and Safety.
- Hazardous Manual Tasks.

Contractors have an obligation to source the above information.

3 Protection Work Identification

Protection Work involves taking steps to protect adjoining premises, their occupants and members of the public from harm that may be caused as a result of plumbing work. Protection work is required when proposed works have the potential to damage an adjoining property or risk the safety of occupants of other people. The contractor is required to take into consideration aspects of the proposed works that may have this effect and to act accordingly, where there is any doubt, particularly where a latent condition has impacted on the situation the work area must be made safe and the relevant authority and qualified experts must be notified ***before proceeding***.

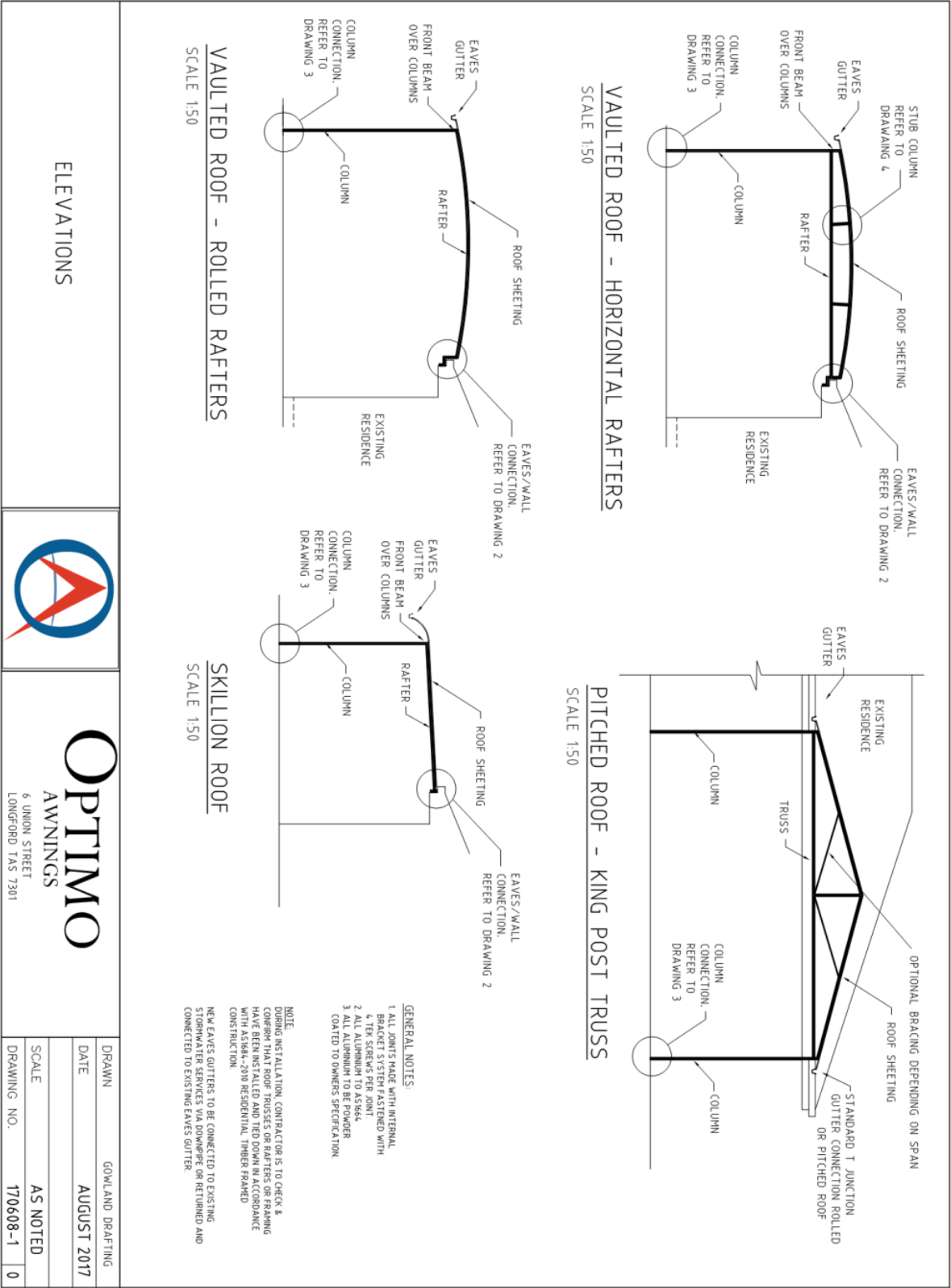
The information provided in this document is not exclusive, additional issues may arise during the construction phase and must be addressed.

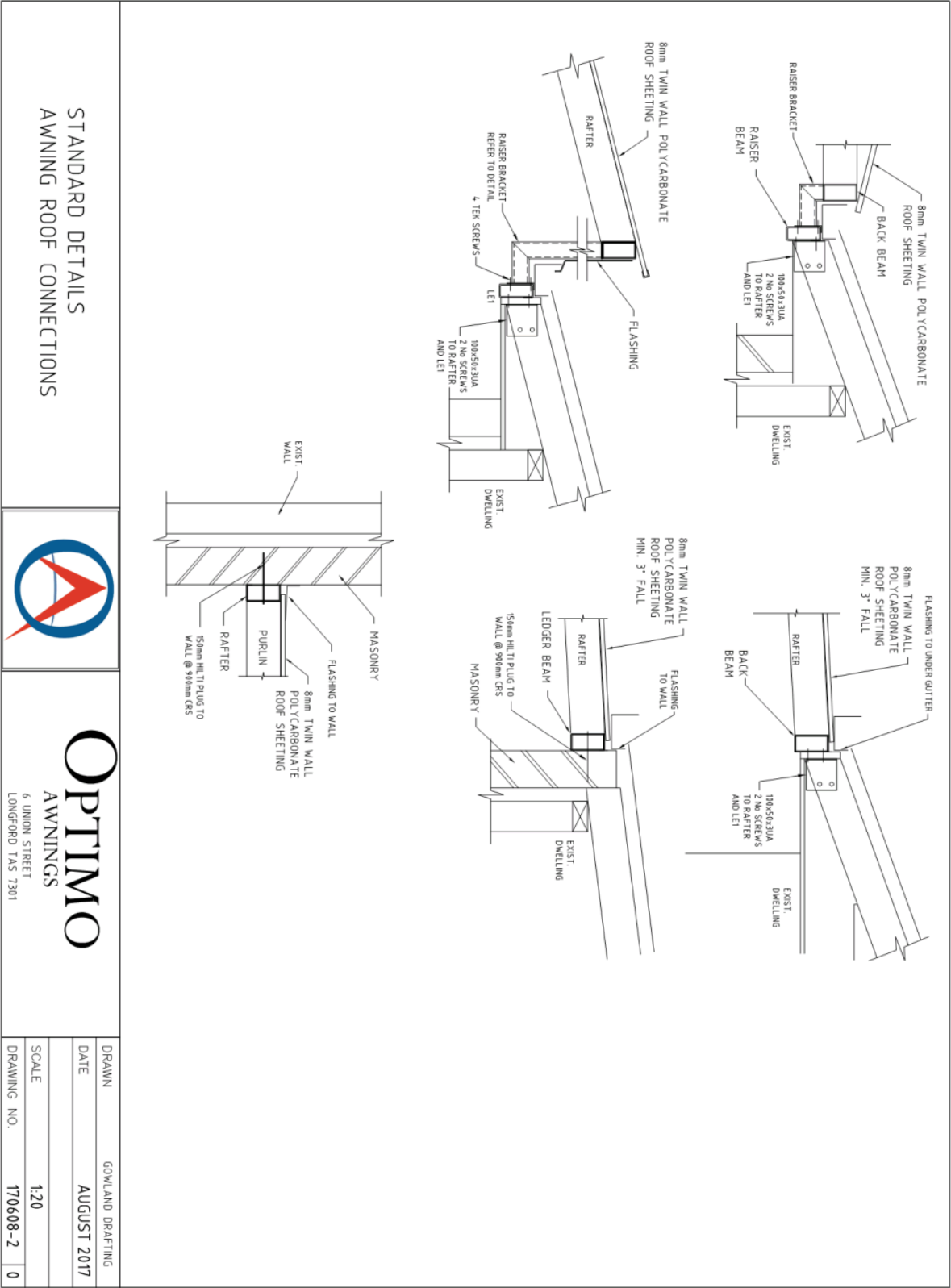
REC HYDRAULIC DESIGN CONSULTANTS P/L HAVE NOT BEEN ENGAGED TO PROVIDE ANY SITE SUPERVISION OF HYDRAULIC WORKS

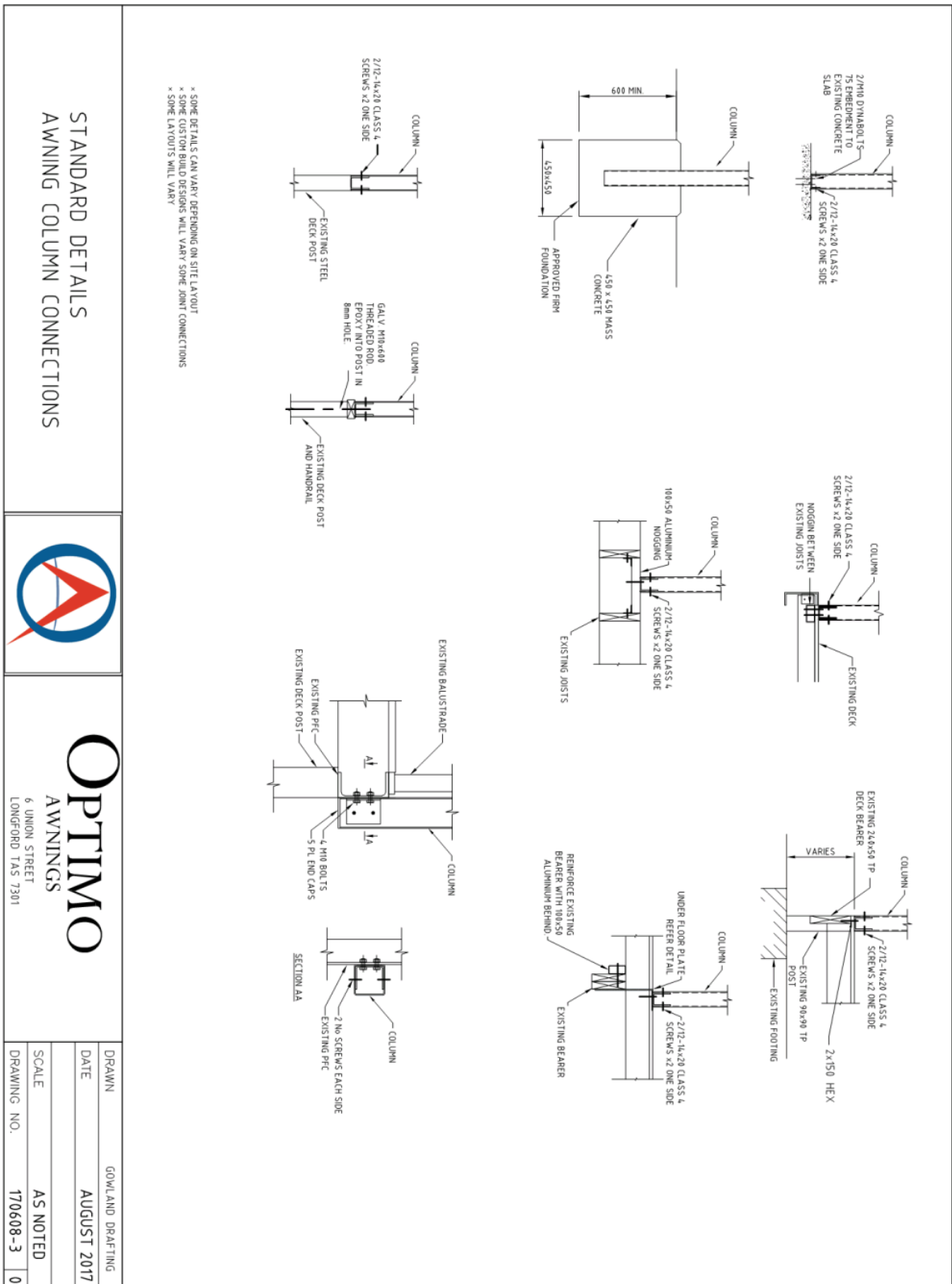
Provision of SMP's remains the responsibility of each contractor, subcontractor and their employees.

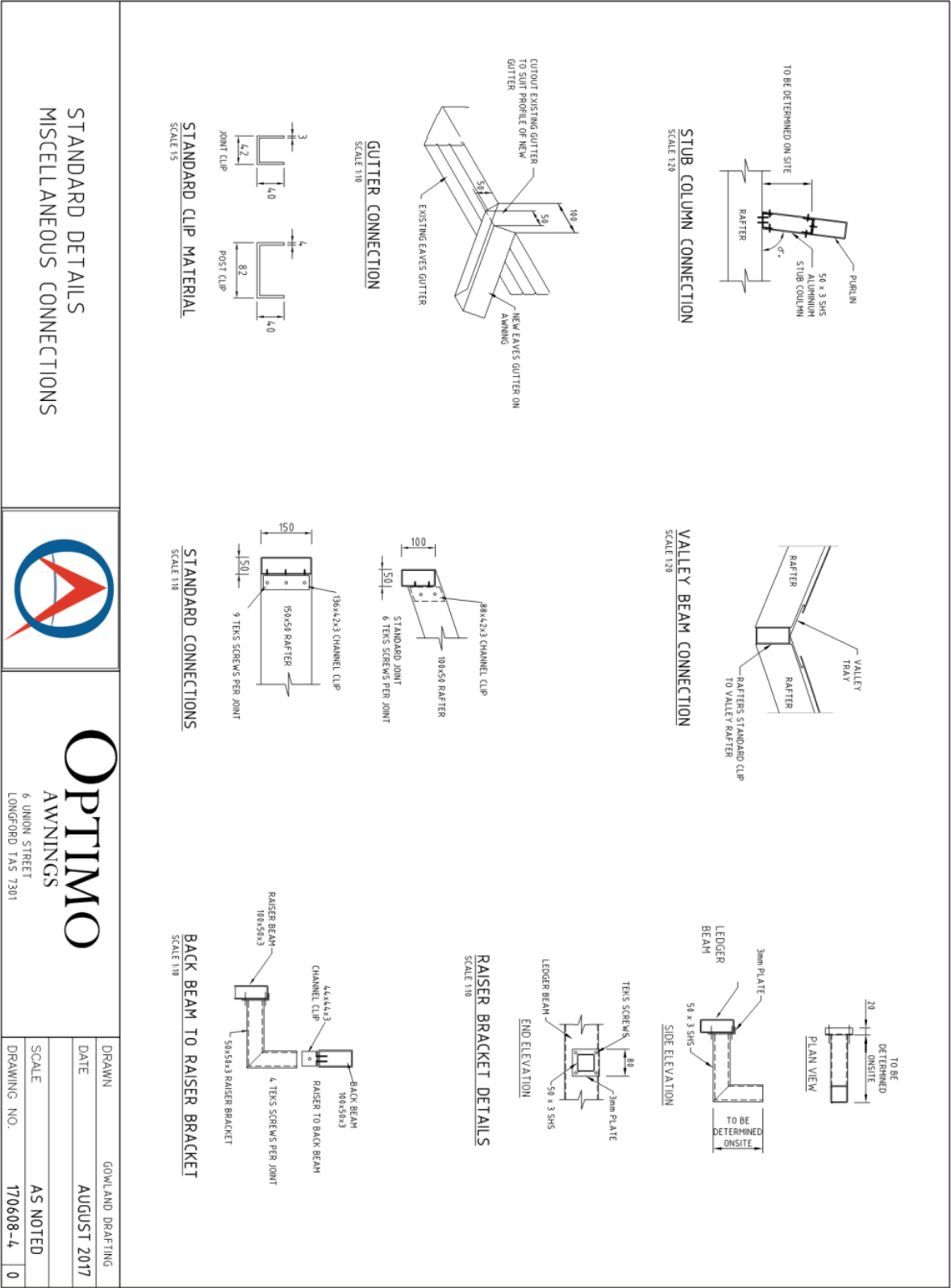
R J Cooper

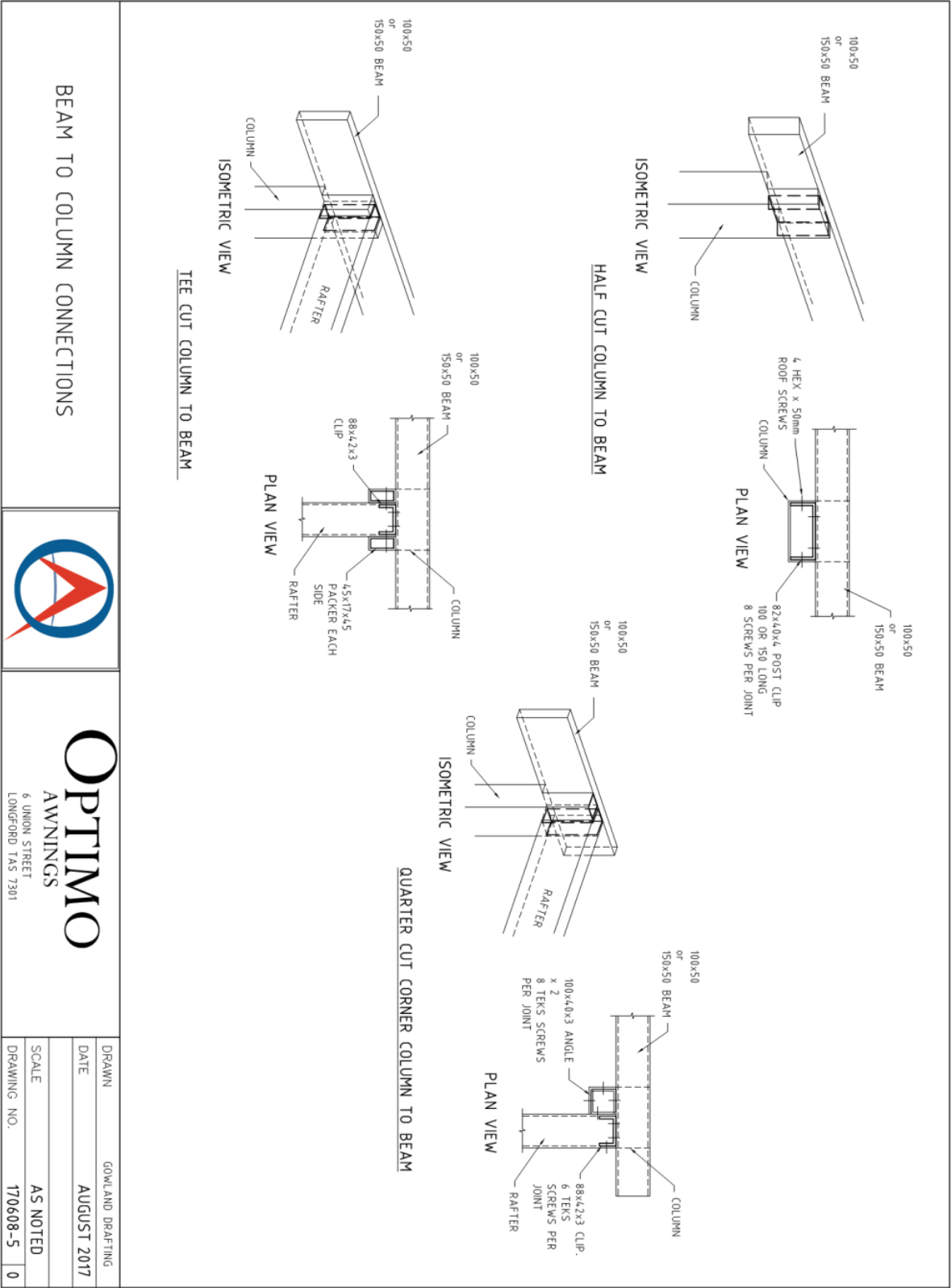
Rod Cooper- Director
REC Hydraulic Design Consultants P/L 144 Scenic Drive Lewisham Tas 7173
20th February 2022











BEAM TO COLUMN CONNECTIONS

Optimo
AWNINGS
6 UNION STREET
LONGFORD TAS 7301

DRAWN	GOWLAND DRAFTING
DATE	AUGUST 2017
SCALE	AS NOTED
DRAWING NO.	170608-5 0

**7.2.2 1 POWELL STEET, SANDY BAY - PARTIAL DEMOLITION,
ALTERATIONS AND EXTENSION
PLN-22-134 - FILE REF: F22/39239**

Address: 1 Powell Street, Sandy Bay
Proposal: Partial Demolition, Alterations and Extension
Expiry Date: 16 May 2022
Extension of Time: Not applicable
Author: Lucy Ockenden

RECOMMENDATION

That pursuant to the *Hobart Interim Planning Scheme 2015*, the City Planning Committee, in accordance with the delegations contained in its terms of reference, approve the application for partial demolition, alterations, and extension at 1 Powell Street Sandy Bay TAS 7005 for the reasons outlined in the officer's report and a permit containing the following conditions be issued:

GEN

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-22-134 - 1 POWELL STREET SANDY BAY TAS 7005 - Final Planning Documents except where modified below.

Reason for condition

To clarify the scope of the permit.

ENG sw1

All stormwater from the proposed development (including but not limited to: roofed areas, ag drains, retaining wall ag drains and impervious surfaces such as driveways and paved areas) must be drained to the Council's stormwater infrastructure prior to first occupation or commencement of use (whichever occurs first).

Advice:

Under section 23 of the Urban Drainage Act 2013 it is an offence for a property owner to direct stormwater onto a neighbouring property.

Reason for condition

To ensure that stormwater from the site will be discharged to a suitable Council approved outlet.

ENG 1

Any damage to council infrastructure resulting from the implementation of this permit, must, at the discretion of the Council:

1. Be met by the owner by way of reimbursement (cost of repair and reinstatement to be paid by the owner to the Council); or
2. Be repaired and reinstated by the owner to the satisfaction of the Council.

A photographic record of the Council's infrastructure adjacent to the subject site must be provided to the Council prior to any commencement of works.

A photographic record of the Council's infrastructure (e.g. existing property service connection points, roads, buildings, stormwater, footpaths, driveway crossovers and nature strips, including if any, pre-existing damage) will be relied upon to establish the extent of damage caused to the Council's infrastructure during construction. In the event that the owner/developer fails to provide to the Council a photographic record of the Council's infrastructure, then any damage to the Council's infrastructure found on completion of works will be deemed to be the responsibility of the owner.

Reason for condition

To ensure that any of the Council's infrastructure and/or site-related service connections affected by the proposal will be altered and/or reinstated at the owner's full cost.

ENV 1

Sediment and erosion control measures sufficient to prevent sediment from leaving the site must be installed prior to any disturbance of the site, and maintained until all areas of disturbance have been stabilized or re-vegetated.

Advice:

For further guidance in preparing a Soil and Water Management Plan – in accordance with Fact sheet 3 Derwent Estuary Program click [here](#).

Reason for condition

To avoid the sedimentation of roads, drains, natural watercourses, Council land that could be caused by erosion and runoff from the development, and to comply with relevant State legislation.

ADVICE

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's [website](#) for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

BUILDING PERMIT

You may need building approval in accordance with the *Building Act 2016*. Click [here](#) for more information.

This is a Discretionary Planning Permit issued in accordance with section 57 of the *Land Use Planning and Approvals Act 1993*.

PLUMBING PERMIT

You may need plumbing approval in accordance with the *Building Act 2016*, *Building Regulations 2016* and the National Construction Code. Click [here](#) for more information.

PLANNING

You are advised to undertake the construction works in a manner which keeps Powell Street free and unobstructed for residents and visitors.

STORMWATER




Please note that in addition to a building and/or plumbing permit, development must be in accordance with the Hobart City Council's Infrastructure By law. Click [here](#) for more information.

FEES AND CHARGES

Click [here](#) for information on the Council's fees and charges.

DIAL BEFORE YOU DIG

Click [here](#) for dial before you dig information.

- | | |
|---------------|--|
| Attachment A: | PLN-22-134 - 1 POWELL STREET SANDY BAY
TAS 7005 - Planning Committee or Delegated
Report ↓  |
| Attachment B: | PLN-22-134 - 1 POWELL STREET SANDY BAY
TAS 7005 - CPC Agenda Document ↓  |
| Attachment C: | PLN-22-134 - 1 POWELL STREET SANDY BAY
TAS 7005 - Planning Referral Officer Cultural
Heritage Report ↓  |

**APPLICATION UNDER HOBART INTERIM PLANNING SCHEME 2015**

Type of Report: Committee
Committee: 9 May 2022
Expiry Date: 16 May 2022
Application No: PLN-22-134
Address: 1 POWELL STREET , SANDY BAY
Applicant: GREGORY EADE
26 BROWNE STREET
WEST HOBART TAS 7000
Proposal: Partial Demolition, Alterations, and Extension
Representations: Three (3)
Performance criteria: Historic Heritage Code

1. Executive Summary

- 1.1 Planning approval is sought for Partial Demolition, Alterations, and Extension at 1 POWELL STREET SANDY BAY TAS 7005.
- 1.2 More specifically the proposal includes:
- Demolition of the carport, lean-to structure and roofed area at the rear of the dwelling, and storage shed
 - Construction of a rear extension, consisting of a new bathroom, kitchen, dining and living area
 - Construction of a new storage shed, located on the southern boundary and setback approximately 11m from the front boundary
 - Internal alterations to accommodate a WIR and ensuite
 - Two new timber decks
 - Extension cladding to be constructed from vertical timber shiplap boards, with a clear finish
 - Storage shed cladding to be constructed from vertical timber shiplap boards with a charcoal stain
- 1.3 The proposal relies on performance criteria to satisfy the following standards and codes:
- 1.3.1 Historic Heritage Code - Heritage Precinct

- 1.4 Three (3) representations objecting to the proposal were received within the statutory advertising period between 31 March 2022 - 14 April 2022.
- 1.5 The proposal is recommended for approval subject to conditions.
- 1.6 The final decision is delegated to the City Planning Committee because three representations were received objecting to the proposal.

2. Site Detail

- 2.1 The subject site is located on the east side of Powell Street, Sandy Bay, in the northern section of Powell Street. The site is located in a Heritage Precinct and contains a single-storey Victorian dwelling.

The surrounding land uses are predominantly residential, consisting of single detached dwellings on medium size urban lots. Princes Street Primary School is located in close proximity to the west of the site.



Figure 1: Aerial view of subject site (source: Geocortex, 2022)



Figure 2: Subject site viewed from Powell Street (source: site visit, April 2022)

3. Proposal

3.1 Planning approval is sought for Partial Demolition, Alterations, and Extension at 1 POWELL STREET SANDY BAY TAS 7005.

3.2 More specifically the proposal is for:

- Demolition of the carport, lean-to structure and roofed area at the rear of the dwelling, and storage shed
- Construction of a rear extension, consisting of a new bathroom, kitchen, dining and living area
- Construction of a new storage shed, located on the southern boundary and setback approximately 11m from the front boundary
- Internal alterations to accommodate a WIR and ensuite
- Two new timber deck
- Extension cladding to be constructed from vertical timber shiplap boards, with a clear finish
- Storage shed cladding to be constructed from vertical timber shiplap boards with a charcoal stain



Figure 3: Rear of dwelling and shed to be demolished (source: applicant)

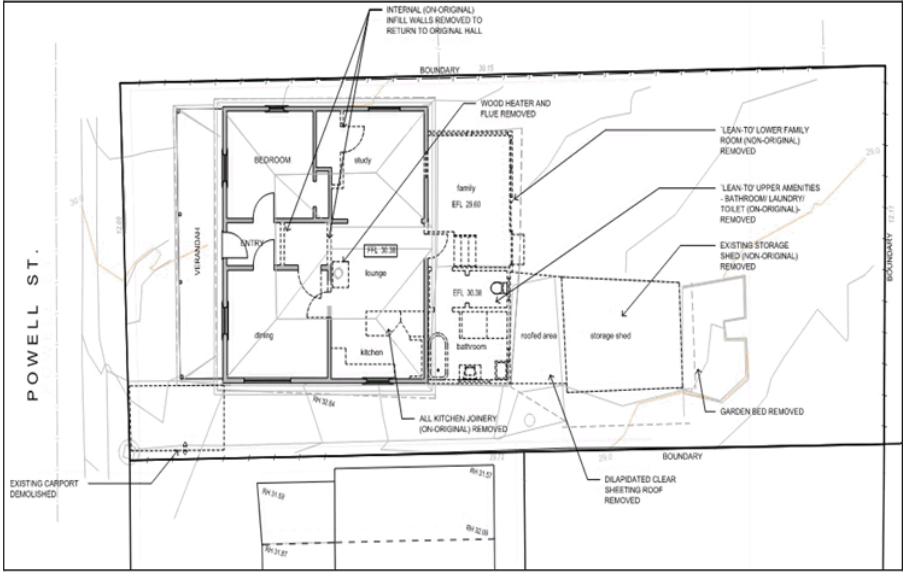


Figure 4: Demolition plan (source: applicant)

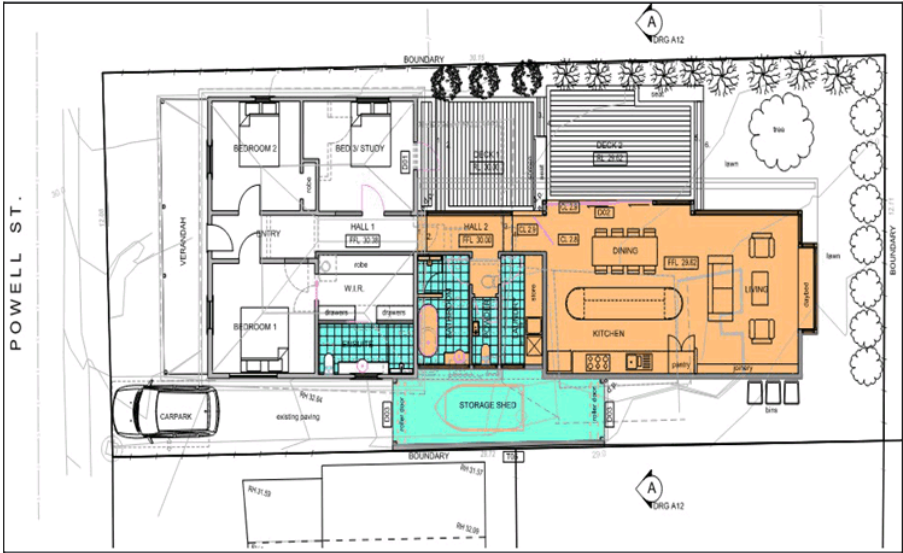


Figure 5: Proposed floor plan (source: applicant)

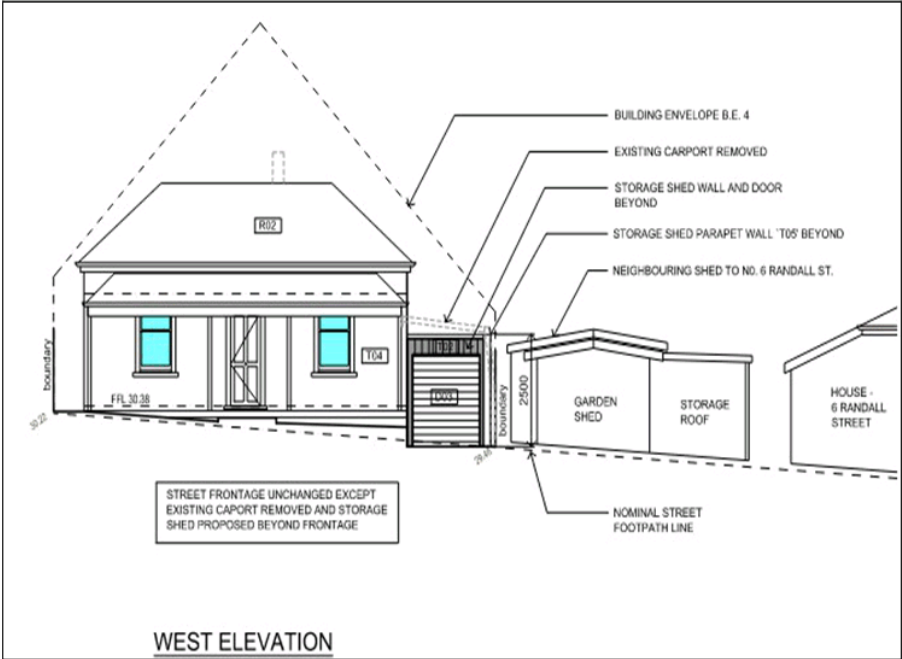


Figure 6: Proposed west elevation (source: applicant)

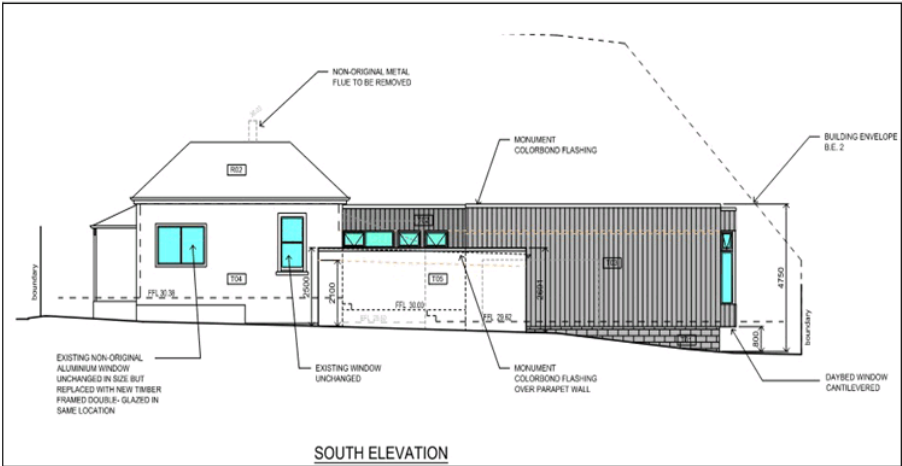


Figure 7: Proposed south elevation (source: applicant)

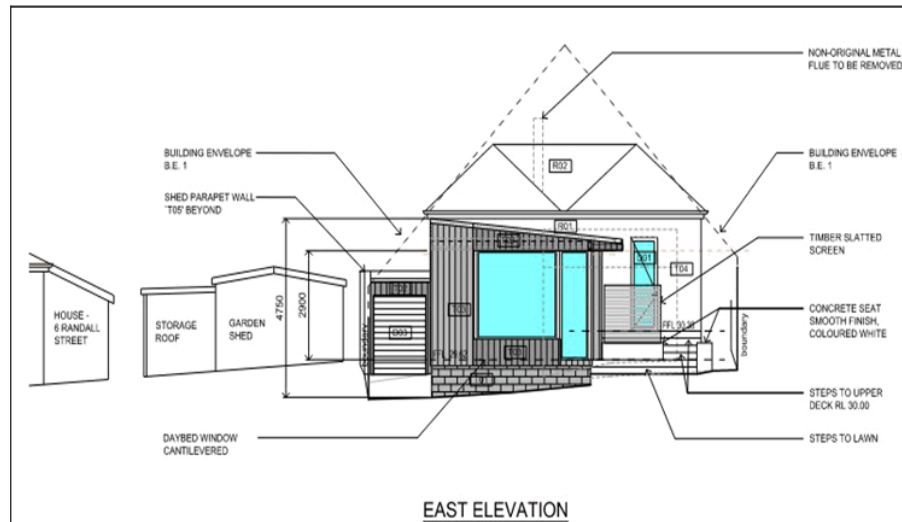


Figure 8: Proposed east elevation (source: applicant)

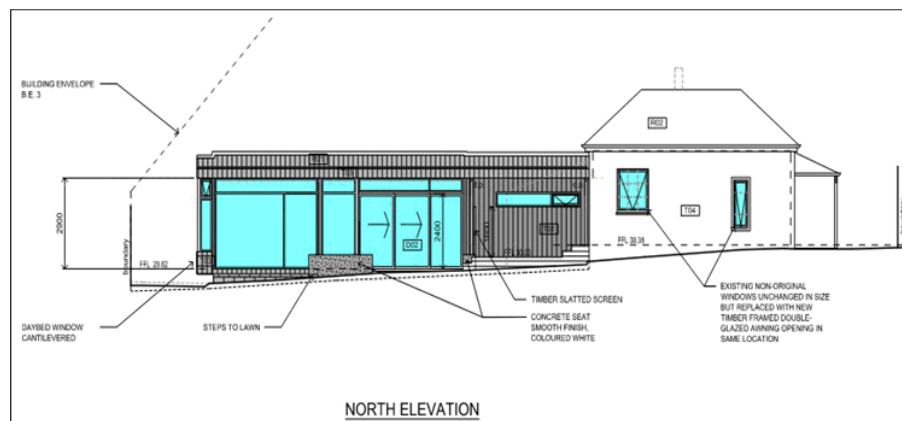


Figure 9: Proposed north elevation (source: applicant)

4. Background

4.1 There is no relevant background for this application.

5. Concerns raised by representors

5.1 Three (3) representations objecting to the proposal were received within the statutory advertising period between 31 March 2022 - 14 April 2022.

- 5.2 The following table outlines the concerns raised in the representations received. Those concerns which relate to a discretion invoked by the proposal are addressed in Section 6 of this report.

•	Building works will result in the parking situation at the end of Powell st where we live becoming congested and unliveable
•	The road cannot cope in its current state with having heavy trucks or trade vehicles parking in powell st for a long period of time
•	In order for the building works to take place, there will be a temporary (i.e. 1 hour maximum) loading and unloading, but our experience with the council and tradespeople is that they have no regard or concern for the existing residents in our end of powell st
•	If the council, can not provide any action or guarantees in writing, then the building itself cannot take place due to the unique size and dimensions and congestion of the end of powell st that we reside at
•	Recommend that no tradespeople can park their car/truck/vehicle for a purpose other than loading and unloading for a short period of time (no greater than an hour) and all trades vehicles must be parked on another road
•	Major previous problems with parking, driveway accessibility issues
•	There are no more parking slots on the street in front of my and my nieghbour's house. Even there isn't a place to park visitors car when they visit our houses. If any construction vehicle come to remove concrete or unload equipment to her house then I won't be able to enter the road because it blocks my gate. And neighbors are always concerned about parking.
•	The impact on access to our property if there are no restrictions placed on the movement of vehicles coming and going from the development property we realise that there will need to be loading and unloading of materials and tools but the vehicles will then need to park offsite and as such would need to be written in as a condition of the development application permit we invite you to attend the site in question and you will understand our concerns.

6. Assessment

- 6.1 The *Hobart Interim Planning Scheme 2015* is a performance based planning scheme. To meet an applicable standard, a proposal must demonstrate compliance with either an acceptable solution or a performance criterion. Where a

proposal complies with a standard by relying on one or more performance criteria, the Council may approve or refuse the proposal on that basis. The ability to approve or refuse the proposal relates only to the performance criteria relied on.

6.2 The site is located within the Inner Residential Zone of the *Hobart Interim Planning Scheme 2015*.

6.3 The existing use is residential - single dwelling, which is a NPR use in the zone.

6.4 The proposal has been assessed against:

6.4.1 D11.0 Inner Residential Zone

6.4.2 E7.0 Stormwater Code

6.4.3 E13.0 Historic Heritage Code

6.5 The proposal relies on the following performance criteria to comply with the applicable standards:

6.5.1 Historic Heritage Code:

*Development Standards for Heritage Precincts E13.8.1 Demolition P1;
E13.8.2 Buildings and Works other than Demolition P1, P3*

6.6 Each performance criterion is assessed below.

6.7 Development Standards for Heritage Precincts E13.8.1 Demolition P1; E13.8.2 Buildings and Works other than Demolition P1, P3

6.7.1 There are no acceptable solution for clauses 13.8.1 and 13.8.2.

6.7.2 The proposal includes demolition of the rear lean-to structure and existing roofed area, and building and works to construct a rear extension, storage shed, decks and internal alterations.

6.7.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.

6.7.4 The performance criterion at clauses 13.8.1 and 13.8.2 provides as follows:

13.8.1

P1 Demolition must not result in the loss of any of the following:

(a) buildings or works that contribute to the historic cultural heritage

significance of the precinct;

(b) fabric or landscape elements, including plants, trees, fences, paths, outbuildings and other items, that contribute to the historic cultural heritage significance of the precinct;

unless all of the following apply:

(i) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;

(ii) there are no prudent or feasible alternatives;

(iii) opportunity is created for a replacement building that will be more complementary to the heritage values of the precinct.

13.8.2

P1 Design and siting of buildings and works must not result in detriment to the historic cultural heritage significance of the precinct, as listed in Table E13.2

P2 Design and siting of buildings and works must comply with any relevant design criteria / conservation policy listed in Table E13.2, except if a heritage place of an architectural style different from that characterising the precinct.

P3 Extensions to existing buildings must not detract from the historic cultural heritage significance of the precinct.

P4 New front fences and gates must be sympathetic in design, (including height, form, scale and materials), and setback to the style, period and characteristics of the precinct.

P5 The removal of areas of landscaping between a dwelling and the street must not result in the loss of elements of landscaping that contribute to the historic cultural significance or the streetscape values and character of the precinct.

- 6.7.5 The application was referred to Council's Cultural Heritage Officer, who advised the following:

The proposal seeks the demolition of the rear extensions back to the core parts of the original cottage, to allow for the construction of an elongated rear extension that would occupy just over half the width of the original rear elevation and extend out almost to the rear boundary. Constructed with a low mono pitched roof hidden behind a low parapet, the rear addition

would be clad in vertical shiplap timber in a mixture of charcoal stain and clear finish. The front car port and rear shed would also be demolished and a new storage shed constructed on the return boundary with No.6 Randell Street.

With regard to the proposed demolition, E.13.8.1 states that demolition must not result in the loss of buildings or works that contribute to the historic cultural heritage significance of the precinct. The rear additions and garden structures highlighted for demolition could be described as being entirely in keeping with the character of the precinct, given that their form, scale, construction materials all speak of an adhoc approach to the extension of the cottage as befitting its status as a workers cottage. However, the extensions are themselves of little merit and almost entirely hidden from view and therefore make no discernable visual contribution to the character of the Precinct. As such, it is therefore considered that the extensions do not in themselves make a positive contribution to the Heritage Precinct.

With regards to the proposed rear and side extension, as with all applications relating to Heritage Precincts, the principal starting point is the degree to which development would be visible from the public realm. In this instance, due to the arrangement of sub-divisions, the rear of the site is not significantly enclosed by the built form of neighbouring properties and thus development within the rear garden would be partially visible.

The scale of the development is considered to be out of proportion with the original scale of the cottage to a degree. However, it is noted that it would sit entirely behind the main bulk of the cottage and its roof form would spring from below the original eave level, creating a pleasing and respectful transition between the old and new parts of the site. Whilst the side storage shed addition is considered unfortunate, it would stand some distance back from the original front building line and when coupled with the removal of the previous car port that stood onto the front boundary, it is considered that the overall streetscape appearance of the cottage would be improved. It is also considered that the intended use of timber as a cladding material would act as a suitable link between the old and new parts of the site and the intended colouration is considered to be relatively restrained in this instance.

As such, it is therefore considered that the impact of the proposal upon the characteristics of the wider Heritage Precinct would not result in detriment to the above as stated under Clauses E.13.8.1 P1 and E.13.8.2

P1 and P3 of the Hobart Interim Planning Scheme 2015

6.7.6 The proposal complies with the performance criterion.

7. Discussion

- 7.1 Planning approval is sought for Partial Demolition, Alterations, and Extension at 1 POWELL STREET SANDY BAY TAS 7005
- 7.2 The application was advertised and received three (3) representations. The representations raised concerns regarding the traffic and parking impact during construction. While this is not a relevant planning consideration, advice has been included addressing the issue.
- 7.3 The proposal has been assessed against the relevant provisions of the planning scheme and is considered to perform well.
- 7.4 The proposal has been assessed by other Council officers, including the Council's Development Engineer and Cultural Heritage Officer. The officers have raised no objection to the proposal, subject to conditions.
- 7.5 The proposal is recommended for approval.

8. Conclusion

- 8.1 The proposed Partial Demolition, Alterations, and Extension at 1 POWELL STREET SANDY BAY TAS 7005 satisfies the relevant provisions of the *Hobart Interim Planning Scheme 2015*, and as such is recommended for approval.

9. Recommendations

That: Pursuant to the *Hobart Interim Planning Scheme 2015*, the City Planning Committee, in accordance with the delegations contained in its terms of reference, approve the application for Partial Demolition, Alterations, and Extension at 1 POWELL STREET SANDY BAY TAS 7005 for the reasons outlined in the officer's report and a permit containing the following conditions be issued:

GEN

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-22-134 - 1 POWELL STREET SANDY BAY TAS 7005 - Final Planning Documents except where modified below.

Reason for condition

To clarify the scope of the permit.

ENG sw1

All stormwater from the proposed development (including but not limited to: roofed areas, ag drains, retaining wall ag drains and impervious surfaces such as driveways and paved areas) must be drained to the Council's stormwater infrastructure prior to first occupation or commencement of use (whichever occurs first).

Advice:

Under section 23 of the Urban Drainage Act 2013 it is an offence for a property owner to direct stormwater onto a neighbouring property.

Reason for condition

To ensure that stormwater from the site will be discharged to a suitable Council approved outlet.

ENG 1

Any damage to council infrastructure resulting from the implementation of this

permit, must, at the discretion of the Council:

1. **Be met by the owner by way of reimbursement (cost of repair and reinstatement to be paid by the owner to the Council); or**
2. **Be repaired and reinstated by the owner to the satisfaction of the Council.**

A photographic record of the Council's infrastructure adjacent to the subject site must be provided to the Council prior to any commencement of works.

A photographic record of the Council's infrastructure (e.g. existing property service connection points, roads, buildings, stormwater, footpaths, driveway crossovers and nature strips, including if any, pre-existing damage) will be relied upon to establish the extent of damage caused to the Council's infrastructure during construction. In the event that the owner/developer fails to provide to the Council a photographic record of the Council's infrastructure, then any damage to the Council's infrastructure found on completion of works will be deemed to be the responsibility of the owner.

Reason for condition

To ensure that any of the Council's infrastructure and/or site-related service connections affected by the proposal will be altered and/or reinstated at the owner's full cost.

ENV 1

Sediment and erosion control measures sufficient to prevent sediment from leaving the site must be installed prior to any disturbance of the site, and maintained until all areas of disturbance have been stabilized or re-vegetated.

Advice:

For further guidance in preparing a Soil and Water Management Plan – in accordance with Fact sheet 3 Derwent Estuary Program click [here](#).

Reason for condition

To avoid the sedimentation of roads, drains, natural watercourses, Council land that could be caused by erosion and runoff from the development, and to comply with relevant State legislation.

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(Lucy Ockenden)

Graduate Development Appraisal Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.



(Ben Ikin)

Senior Statutory Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

Date of Report: 27 April 2022

Attachment(s):

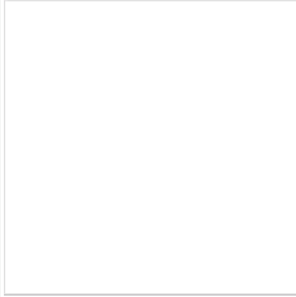
Attachment B - CPC Agenda Documents

Attachment C - Planning Referral Officer Cultural Heritage Report

Planning: #252640

Property

1 POWELL STREET SANDY BAY TAS 7005

**People****Applicant ***GREGORY EADE
0427871723
geade@bigpond.net.au**Owner ***LEWIS UTTING
1 POWELL STREET
SANDY BAY TAS 7005
0408509649
lutting@scidev.com.au**Owner ***HELENA LEHOS
1 POWELL STREET
SANDY BAY TAS 7005
0408509649
hlehos@yahoo.co.uk**Entered By**GREGORY EADE
0427871723
geade@bigpond.net.au**Use**

Single dwelling

Details

Have you obtained pre application advice?

☐ No

If YES please provide the pre application advice number eg PAE-17-xx

Are you applying for permitted visitor accommodation as defined by the State Government Visitor Accommodation Standards? Click on help information button for definition. If you are not the owner of the property you MUST include signed confirmation from the owner that they are aware of this application. *

☐ No

Is the application for SIGNAGE ONLY? If yes, please enter \$0 in the cost of development, and you must enter the number of signs under Other Details below. *

<div style="display: flex; align-items: center;"><div style="border: 1px solid #ccc; padding: 2px 5px; margin-right: 5px;"> No</div></div>		
<div style="border: 1px solid #ccc; height: 20px; margin-top: 5px;"></div>		

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

SEARCH OF TORRENS TITLE

VOLUME	FOLIO
15561	1
EDITION	DATE OF ISSUE
5	07-Dec-2021

SEARCH DATE : 10-Mar-2022

SEARCH TIME : 08.07 AM

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Diagram 15561

Being the land described in Conveyance No. 48/4502

Derivation : Part of 66A-2R-30ps. Gtd. to W.M. Orr.

Prior CT 3873/54

SCHEDULE 1

M926048 TRANSFER to LEWIS EDWARD UTTING and HELENA ELISABETH
LEHOS as tenants in common in equal shares
Registered 07-Dec-2021 at 12.02 PM

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

E285736 MORTGAGE to Macquarie Bank Limited Registered
07-Dec-2021 at 12.03 PM

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



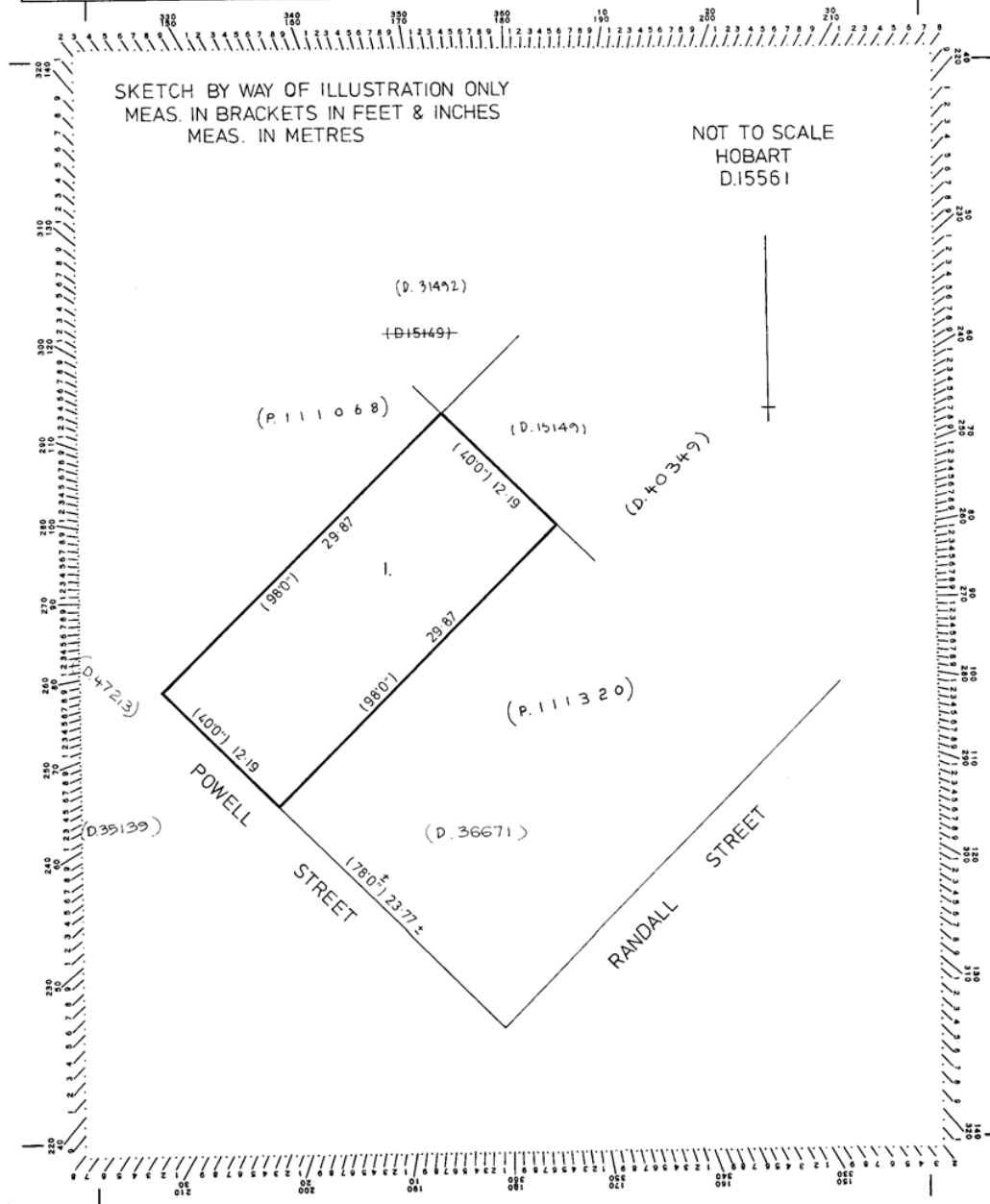
FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



Owner: DECEASED PERSONS ESTATE	PLAN OF SURVEY by Surveyor of land situated in the CITY OF HOBART	Registered Number: D.15561
Title Reference: Z 1028		APPROVED 19..... Effective from: <i>J. Sroude</i> Recorder of titles
Grantee: PART OF 66-2-30 GTD. TO WILLIAM MORGAN ORR	SCALE 1: MEASUREMENTS IN METRES	



e a d e s i g n

gregory eade building design

26 browne street

west hobart 7000

1 Powell Street, Sandy Bay

ZONED: INNER RESIDENTIAL

OVERLAY: HERITAGE PRECINCT SB2

Project

A proposed new living and bathroom pavilion to the rear backyard with a storage shed added to the southern boundary. The proposed extensions are within all setbacks and building envelopes of the current planning scheme. The building envelopes BE1 & BE4 (shown on site plan A03), show the storage shed within the building envelope and is located low to the ground to avoid overshadowing of the garden sheds at 6 Randall Street.

Services

Both stormwater and sewer have connections to the street. The stormwater has a connection in the south-west corner. The existing family room and the existing storage shed which are to be demolished, have downpipes connected to this stormwater drain to the street. The proposed extensions will utilize this connection.

The one-car parking will remain unchanged. The ugly carport, which is in poor condition, on the street frontage will be removed.

Summary

The low modern design is understated and does not compete with the original house. It is designed to have minimal impact on the house and the surrounding heritage area. The new pavilion is to be subservient to the main house and is largely hidden behind the existing house from the street. The pavilion is readily identifiable as modern and the timber wall cladding is to soften the impact on the heritage shapes of the area. The streetscape will not be affected.

The proposal is well within the building envelope and setbacks of the current Hobart Interim Planning Scheme. It has endeavoured to minimize the impact on the neighbouring properties and is sympathetic to the heritage values of the area.

e a d e s i g n

1 POWELL STREET, SANDY BAY
HOUSE EXTENSION

WIND CLASSIFICATION: - N2
TITLE REFERENCE: - CT - 15561/1
SOIL CLASSIFICATION: - M
CLIMATE ZONE: - 7
BAL RATING - low
ZONED - inner residential
OVERLAY - heritage precinct SB2

DRAWING LIST

A01 COVER SHEET
A02 NOTE SHEET
A03 SITE PLAN
A04 EXSTING & DEMOLITION PLAN
A05 FLOOR PLAN
A06 DIMENSIONAL LAYOUT
A07 ROOF PLAN
A08 SOUTH ELEVATION
A09 EAST ELEVATION
A10 NORTH ELEVATION
A11 WEST ELEVATION
A12 SECTION

gregory eade building design

26 browne street, west hobart 7000
mobile: 0427 871723
email: geade@bigpond.net.au
accreditation: CC1133F
ABN number: 34985528960
client: lewis utting & helena lehoss

NOTES

1. ALL WORK TO COMPLY WITH THE BUILDING CODE OF AUSTRALIA AND THE LOCAL COUNCIL REGULATIONS.
2. MATERIALS AND WORKMANSHIP SHALL COMPLY WITH THE RELEVANT S.A.A. CODES AND STANDARDS
3. ALL TIMBER TO BE NEW AND FREE FROM DEFECTS. ALL FRAMING SHALL COMPLY WITH A.S. 1684. TIMBER TO BE GRADE F17 AND NEW STUD WALLS TO BE 90 x 45 F17 @ 450 CRS. UNLESS NOTED OTHERWISE.
4. PLASTERBOARD TO BE 10mm TO INTERNAL WALLS AND 13mm TO CEILINGS. WATER RESISTANT VILBOARD TO ALL WET AREAS.
5. BUILDING TO COMPLY TO BCA ENERGY EFFICIENCIES PART 3.12. EXTERNAL DOORS & WINDOWS TO HAVE SUITABLE SEALS TO COMPLY WITH BCA PART 3.12.3.3 TO MAKE BUILDING AIRTIGHT. EXTERNAL DOORS TO HAVE RAVEN SEALS OR SIMILAR APPROVED:
RP20 TOP & SIDES
RP3 BOTTOM
RP2A SLIDING DOORS
6. PROVIDE IMPERVIOUS WALLS AND FLOOR FINISHES TO ALL WET AREAS. SUBSTRATES, FINISHES, SPLASHBACKS & SEALING TO COMPLY WITH NCC PART 3.8.1 & TABLE 3.8.1.1. AROUND AND UNDER SHOWERS & OTHER WET AREAS. SPLASHBACKS 200 MIN. ABOVE BASINS/ SINK. ENCLOSED SHOWER WITHOUT HOB TO BE WATERPROOFED OVER THE ENTIRE ENCLOSED SHOWER AREA INCLUDING WATERSTOP. WATERPROOF TO NOT LESS THAN 150mm ABOVE THE SHOWER FLOOR SUBSTRATE WITH THE REMAINDER BEING WATER RESISTANT TO A HEIGHT OF 1800mm ABOVE FINISHED FLOOR LEVEL. JOISTS TO BE 250 MAX CRS. SUBSTRATE TO BE 18mm CEMENT SHEET. ALL JOINTS SEALED. WATERPROOFING TO BE Sika OR APPROVED EQUIVALENT. ALL CORNERS - INTERNAL, EXTERNAL, HORIZONTAL - TO BE WATERPROOFED 40mm WIDE TO 1800mm ABOVE FLOOR LEVEL
7. STRUCTURAL DESIGN
ALL ENGINEERING DESIGN INCLUDING FOOTINGS, HOLD-DOWN DETAILS, BRACING, LINTELS & FRAMING BY JOHNSTONE, McGEE & GANDY.
8. SMOKE DETECTORS / ALARM ARE TO BE INSTALLED IN ACCORDANCE WITH BCA PART 3.7.5. SMOKE DETECTORS MUST BE INTER-CONNECTED & CONNECTED TO MAINS POWER AND INSTALLED IN CEILINGS IN LOCATIONS SHOWN THUS ON PLANS. WIRED IN SMOKE DETECTORS/ ALARMS TO COMPLY WITH A.S. 3786.
9. GLAZING TO BE TOUGHENED 5mm MIN. THICK. WHERE REQUIRED TO DECK AREA, TO COMPLY WITH AS. 12.88 - 2006 & A.S. 2047 & TO BCA PART 3.12.2. & TO BE DOUBLE GLAZED THROUGHOUT TO COMPLY WITH RED SOLUTIONS ENERGY 6 STAR MIN. RATING.

ALL WINDOWS CLEAR (C.) UNLESS SHOWN
'O.' = OPAQUE

10. INSULATION TO BE TO BCA PART 3.12.1
EXISTING INSULATION
 ceiling space - batts R2.5
 walls - timber stud weatherboard , no insulation
 floor - timber floor over timber joists, no insulation

NEW INSULATION

EXTENSION
ROOF - R4.1 ENVIROSEAL PROCTORWRAP HTR vapour permeable under timber battens & installed to manufacturer's specification to reduce condensation . R4.1 bulk insulation -BRADFORD GOLD CEILING BATTS HP - 215 THICK)
EXTERNAL WALLS (STUD) - PROCTORWRAP RW building sheet over studs/ 90 thick R2.5 BRADFORD GOLD HP BATTS with 19 thick vertical battens over sheet on stud and 35 thick horizontal batten. cladding fixed to 35 batten
INTERNAL WALLS including wall between laundry to living/ ensuite to bed 1/ ensuite to bathroom/ hallways to have R 2.0 BATTS IN STUD WALLS.
UNDER FLOOR
 new timber floor - R2.5 BATTS
 existing timber floor - R2.5 BATTS

11. ALL STAIRS TO HAVE NON-SLIP NOSING TO COMPLY WITH B.C.A. PART 3.9.1.4 - 2015. HANDRAIL 900 ABOVE NOSING - MIN 30 DIA AND 50 CLEAR OF SIDE WALL.
12. DRAINAGE / PLUMBING
 INSTALL ALL PLUMBING TO TASMANIA PLUMBING REGULATIONS A.S.3500 AND TO LOCAL COUNCIL APPROVAL.
 INSTALL 'RMC' TYPE TEMPERING VALVE TO HWC. TEMPERATURE FROM HWC OUTLET TO BE MINIMUM 60° C. TEMPERATURE AT SANITARY FIXTURE OUTLETS TO BE MAXIMUM 50° C.
13. NO CEILING PENETRATIONS FOR LIGHTING. ALL LIGHTING TO BE SURFACE MOUNTED. ALL EXHAUST FANS AND VENTILATING RANGE HOOD TO RUN TO OUTSIDE AND TO BE FITTED WITH SELF CLOSING DAMPER, FILTER OR THE LIKE TO A.S. PART 3.12.3.4
14. SUB FLOOR CROSS VENTILATION FOR SUSPENDED TIMBER FLOOR TO BE 6000 sq. mm per METRE LENGTH OF WALL AS PER BCA PART 3.4.1

LOT AREA	362 m ²
HOUSE 'LEAN-TO' DEMOLISHED	26 m ²
SHED DEMOLISHED	16 m ²
EXISTING HOUSE (retained)	71.5 m ²
EXISTING VERANDAH (retained)	16.5 m ²
PROPOSED EXTENSION	80.5 m ²
PROPOSED STORAGE SHED	19.5 m ²
TOTAL SITE COVERAGE	188 m ²
PLOT RATIO	52 %

15. CONDENSATION MANAGEMENT:

ROOF AND WALL CONSTRUCTION TO COMPLY NCC PART 3.8.7
 3.8.7.2 PLIABLE BUILDING MEMBRANE
 a) where a pliable membrane is installed in an external must, it must
 i) comply with AS/NZ 4200.1
 ii) be installed in accordance with AS 4200.2
 iii) be a **vapour** permeable barrier
 iv) be located on the exterior side of the primary insulation layer of the wall assemblies that form the external building envelope of the building
 b) where a pliable membrane is not installed in an external wall, the primary water control layer must be separated from water sensitive materials by a drained cavity

3.8.7.3 FLOW RATE & DISCHARGE OF EXHAUST SYSTEMS
 a) an exhaust system installed in a kitchen, bathroom, toilet or laundry must have a minimum flow rate of-
 i) 25 litres per second for a bathroom or toilet
 ii) 40 litres per second for a kitchen or laundry
 b) exhaust from bathroom, laundry or toilet must be to outdoor air
HEALTH & AMENITY
 Refer to guidance in the 'Guide for Control of Condensation & Mould in Tasmanian Homes' that should be adhered to.

3.6.4 Human impact safety requirements

The thickness and type of glazing installed in areas of a building that have a high potential for human impact (an area of a building frequented by the occupants during everyday activities in which a person could fall into or against the glazed panel) must comply as follows:

- (a) Doors — in accordance with 3.6.4.1.
- (b) Door side panels — in accordance with 3.6.4.2.
- (c) Full height glass panels — in accordance with 3.6.4.3.
- (d) Glazed panels, other than doors or side panels, on the perimeter of rooms — in accordance with 3.6.4.4.
- (e) Bathrooms, ensuite and spa room glazing — in accordance with 3.6.4.5.
- (f) Visibility of glazing — in accordance with 3.6.4.6.

FINISHES SCHEDULE**ROOF SHEETING**

REFER TO SCHEDULES ON PLANS & ELEVATIONS.

WALL CLADDING:

REFER TO SCHEDULES ON PLANS & ELEVATIONS.

WINDOW FRAMES:

ALL WINDOWS U.N.O. TO BE FROM RICHARDS ALUMINIUM CAPRAL NARROWLINE 325. ALL POWDER COATED BLACK.
 SET ALL SLIDING DOOR SEALS & TRACKS TO FLOOR LEVEL TO BE FLUSH.

FLOORING:

POWDER/ LAUNDRY/ TILES OVER WATERPROOFING
 BATHROOM/ ENSUITE: MEMBRANE OVER CEMENT SHEET

KITCHEN: TILES OVER CEMENT SHEET

DINING / LIVING: TONGUE AND GROOVE TIMBER

BEDROOMS : CARPET OVER TIMBER FLOOR

DOWNPIPES

PVC 100 DIA. - PAINTED COLORBOND MONUMENT

PLASTERBOARD FINISH:

ALL WINDOWS TO BE FLUSH MOUNTED. NO ARCHITRAVES. SHADOWLINE FINISH TO WALL JOINS WITH CEILING AND FLOOR. NO CORNICES , 100 SKIRTING BOARDS FLUSH WITH SHADOWLINE OVER. PAINTED DULUX WHISPER WHITE U.N.O.

ALL DOORS 2400 HIGH UNLESS NOTED/ SHOWN OTHERWISE.

SCHEDULE

☛ SMOKE DETECTOR HARD WIRED
 dw DISHWASHER
 st STOVE
 s SINK
 b BASIN
 sh SHOWER
 tr TROUGH
 wm WASHING MACHINE
 dp DOWNPIPE
 ef EXHAUST FAN
 CL CEILING LEVEL
 FFL FINISHED FLOOR LEVEL
 RL REDUCED LEVEL
 U.N.O. UNLESS NOTED OTHERWISE

designer : gregory eade
 client : l.utting & h.lehos
 land title ref no: CT- 15561/ 1
 climate zone: 7
 humidity zone: 3
 corrosion environ: not closer than 1km to coast.

e a d e s i o n
 gregory eade building design
 26 browne street west hobart 7000
 accreditation no. C1133F t: 0427 871723
 e: geade@bigpond.net.au

Project
1 POWELL STREET, SANDY BAY
HOUSE EXTENSION

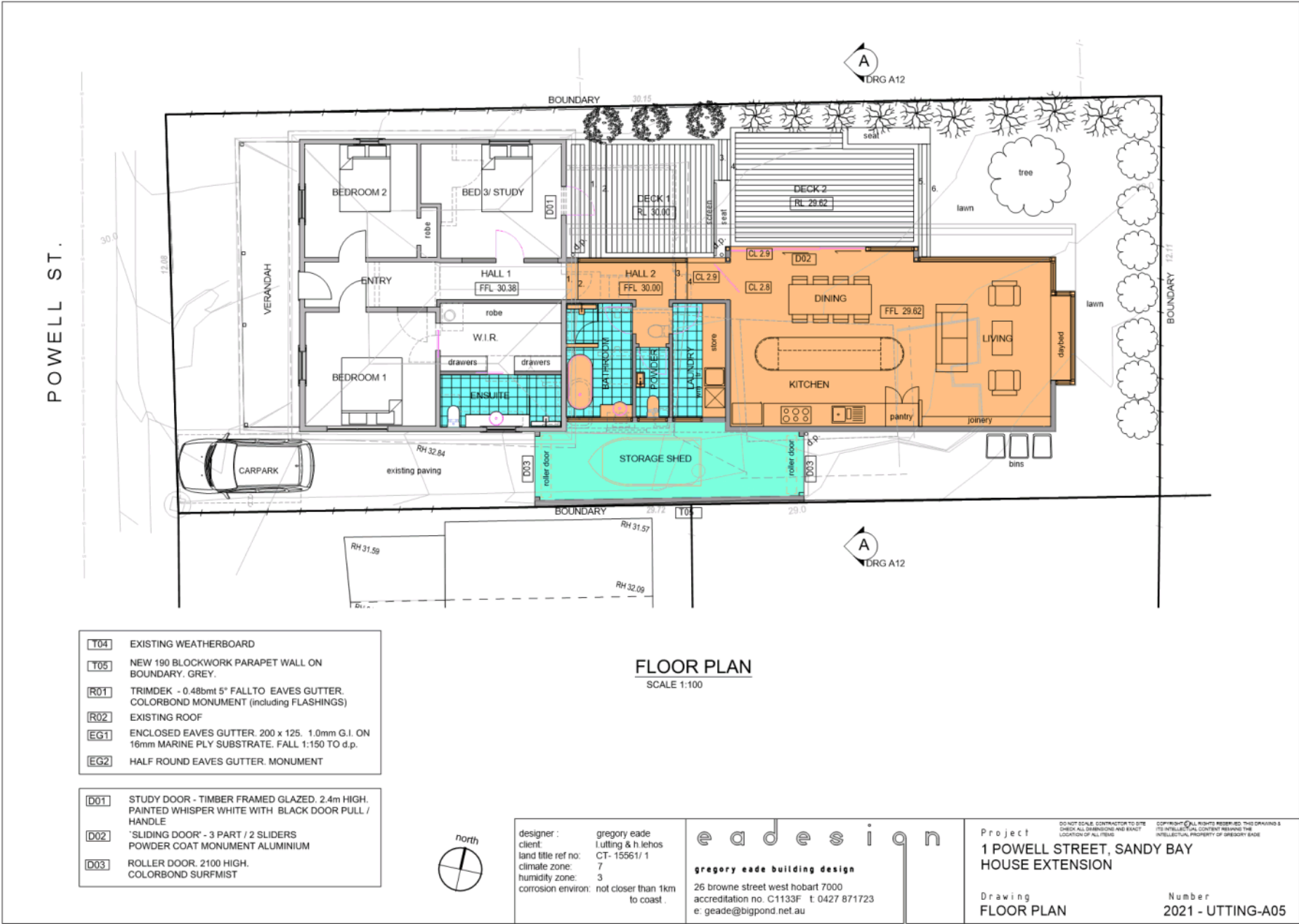
Drawing
NOTE SHEET

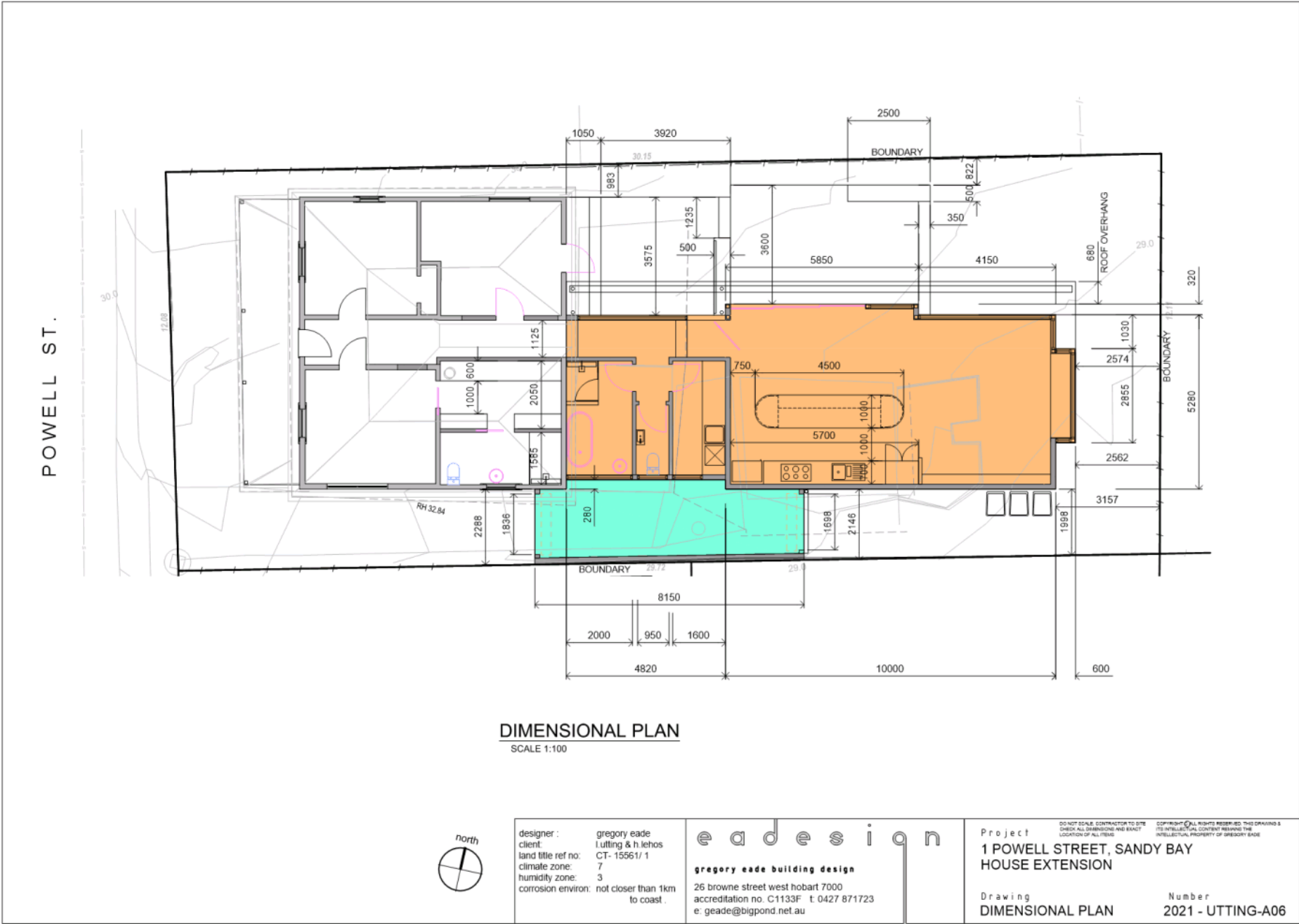
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2021 - UTTING-A02

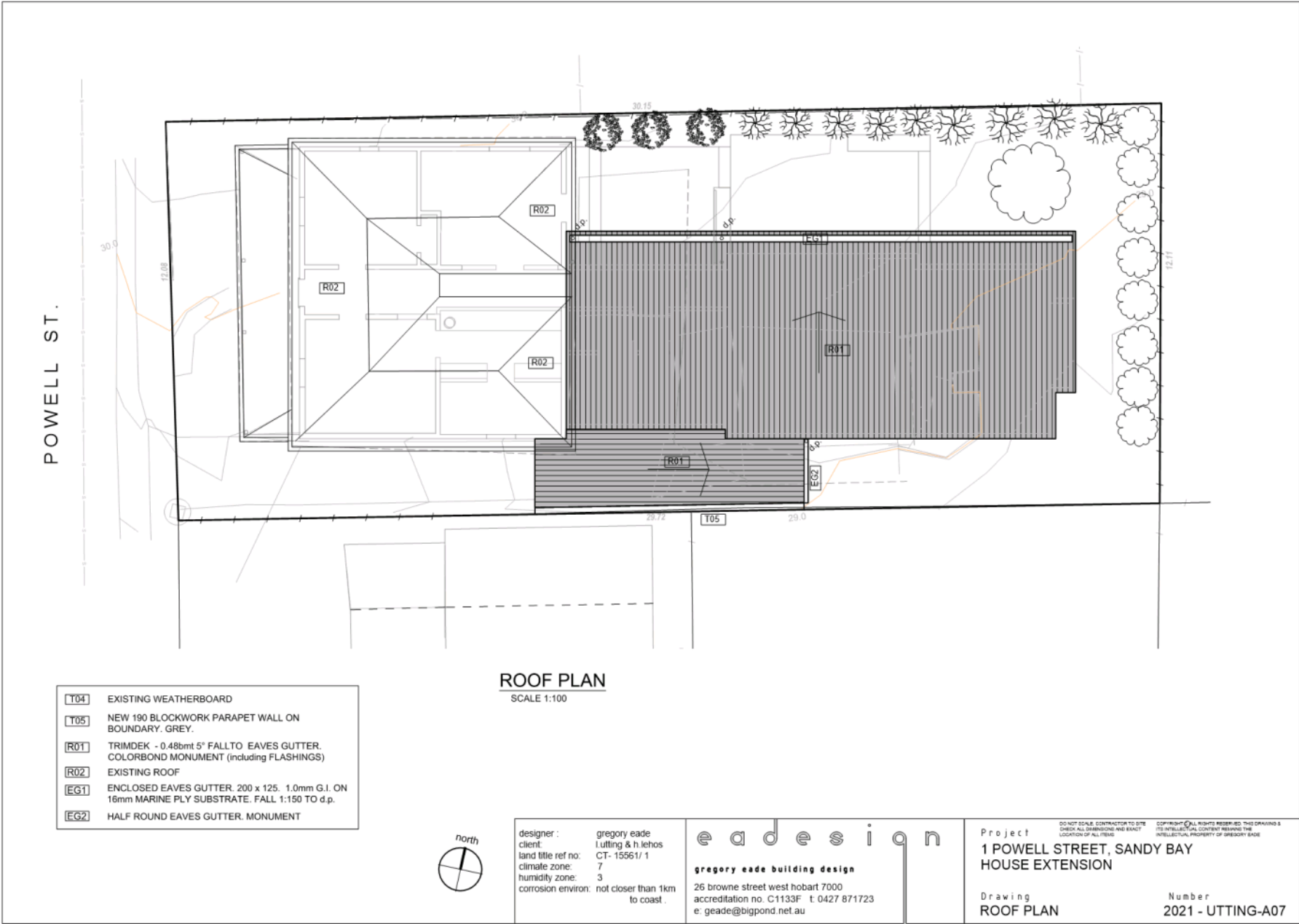
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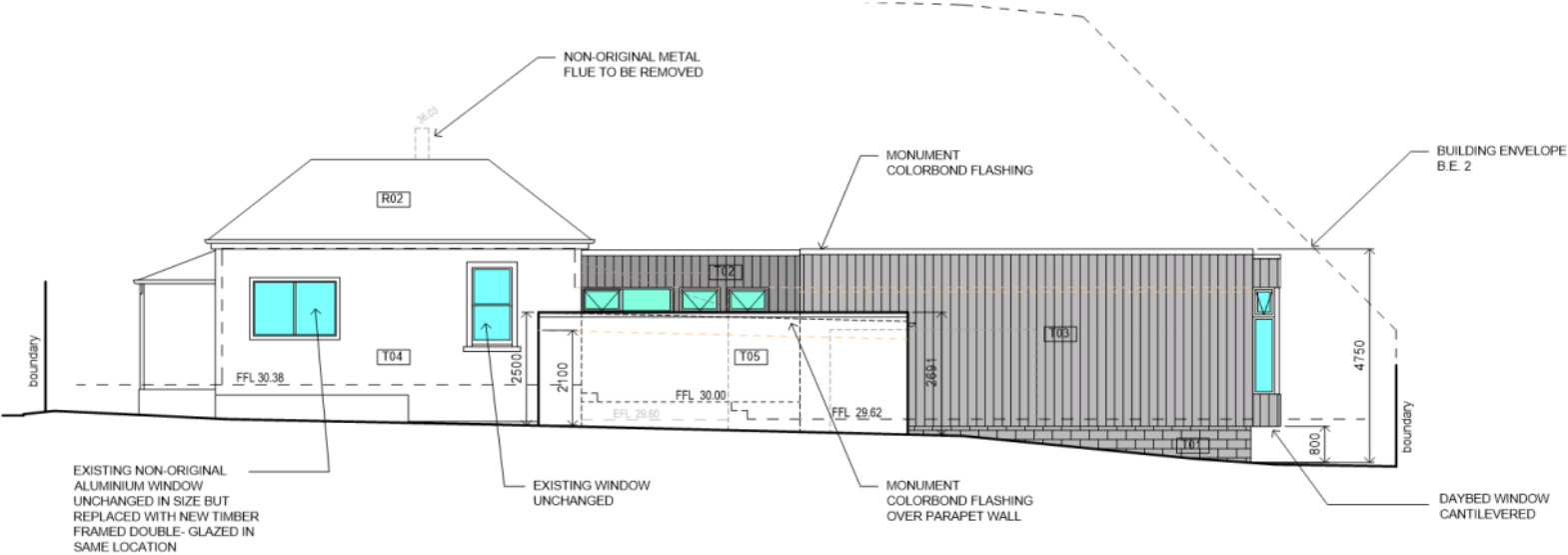












SOUTH ELEVATION
SCALE 1:100

T01		EBONY (CHARCOAL) BLOCKWORK
T02		RADIAL TIMBER VERTICAL - silver-top ash. shiplap, 90 x 19 + 2/ 38 x 19. charcoal stain to owner's specification
T03		RADIAL TIMBER VERTICAL - silver-top ash. shiplap, 90 x 19 + 2/ 38 x 19. clear finish to owner's specification

T04	EXISTING WEATHERBOARD
T05	NEW 190 BLOCKWORK PARAPET WALL ON BOUNDARY. GREY.
R01	TRIMDEK - 0.48bmt 5" FALL TO EAVES GUTTER. COLORBOND MONUMENT (including FLASHINGS)
R02	EXISTING ROOF
EG1	ENCLOSED EAVES GUTTER. 200 x 125. 1.0mm G.I. ON 16mm MARINE PLY SUBSTRATE. FALL 1:150 TO d.p.
EG2	HALF ROUND EAVES GUTTER. MONUMENT

D01	STUDY DOOR - TIMBER FRAMED GLAZED. 2.4m HIGH. PAINTED WHISPER WHITE WITH BLACK DOOR PULL / HANDLE
D02	'SLIDING DOOR' - 3 PART / 2 SLIDERS POWDER COAT MONUMENT ALUMINIUM
D03	ROLLER DOOR. 2100 HIGH. COLORBOND SURFMIST

designer : gregory eade
client : l. utting & h. lehoh
land title ref no: CT- 15561/ 1
climate zone: 7
humidity zone: 3
corrosion environ: not closer than 1km to coast.

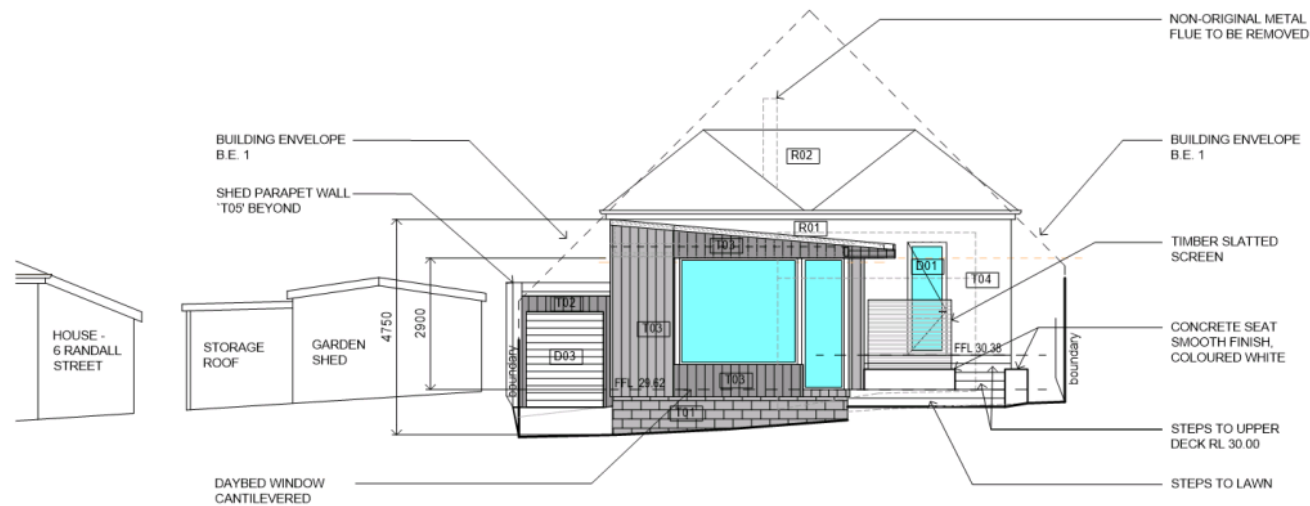
eades i o n
gregory eade building design
26 browne street west hobart 7000
accreditation no. C1133F t: 0427 871723
e: geade@bigpond.net.au

Project
1 POWELL STREET, SANDY BAY
HOUSE EXTENSION

Drawing
SOUTH ELEVATION

Number
2021 - UTTING-A08

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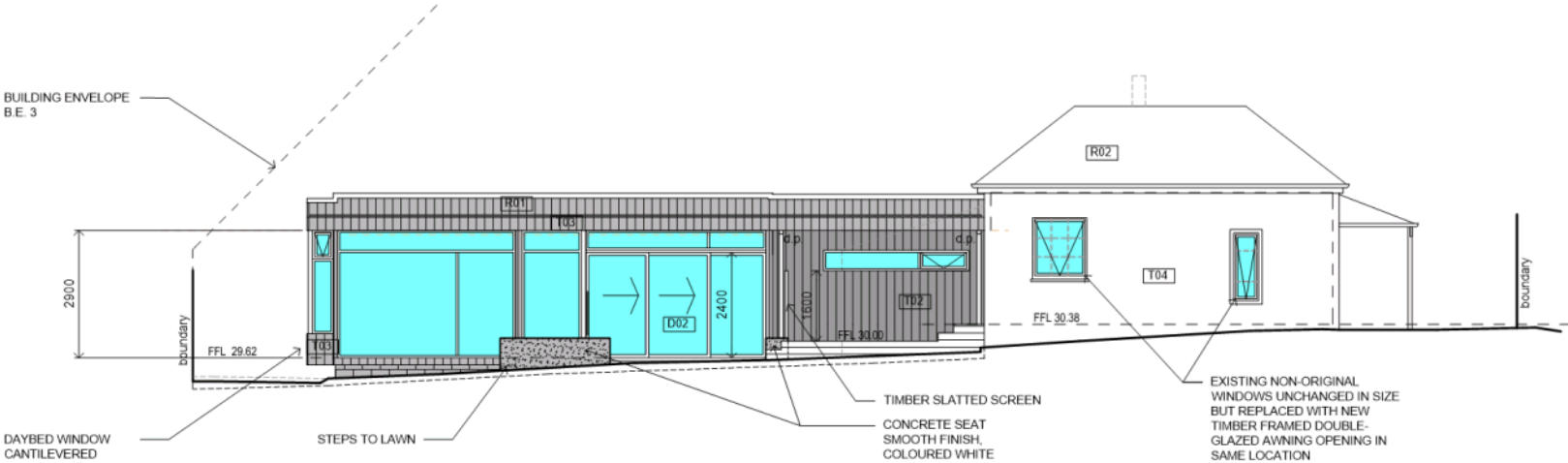
EAST ELEVATION
SCALE 1:100

T01	EBONY (CHARCOAL) BLOCKWORK
T02	RADIAL TIMBER VERTICAL - silver-top ash. shiplap, 90 x 19 + 2/ 38 x 19. charcoal stain to owner's specification
T03	RADIAL TIMBER VERTICAL - silver-top ash. shiplap, 90 x 19 + 2/ 38 x 19. clear finish to owner's specification

T04	EXISTING WEATHERBOARD
T05	NEW 190 BLOCKWORK PARAPET WALL ON BOUNDARY. GREY.
R01	TRIMDEK - 0.48bmt 5" FALL TO EAVES GUTTER. COLORBOND MONUMENT (including FLASHINGS)
R02	EXISTING ROOF
EG1	ENCLOSED EAVES GUTTER. 200 x 125. 1.0mm G.I. ON 16mm MARINE PLY SUBSTRATE. FALL 1:150 TO d.p.
EG2	HALF ROUND EAVES GUTTER. MONUMENT

D01	STUDY DOOR - TIMBER FRAMED GLAZED. 2.4m HIGH. PAINTED WHISPER WHITE WITH BLACK DOOR PULL / HANDLE
D02	'SLIDING DOOR' - 3 PART / 2 SLIDERS POWDER COAT MONUMENT ALUMINIUM
D03	ROLLER DOOR. 2100 HIGH. COLORBOND SURFMIST

<p>designer : gregory eade client : l. utting & h. lehohs land title ref no: CT- 15561/ 1 climate zone: 7 humidity zone: 3 corrosion environ: not closer than 1km to coast.</p>	<p>e a d e s i o n gregory eade building design 26 browne street west hobart 7000 accreditation no. C1133F t: 0427 871723 e: geade@bigpond.net.au</p>	<p>Project 1 POWELL STREET, SANDY BAY HOUSE EXTENSION</p> <p>Drawing EAST ELEVATION</p>	<p>Number 2021 - UTTING-A09</p>
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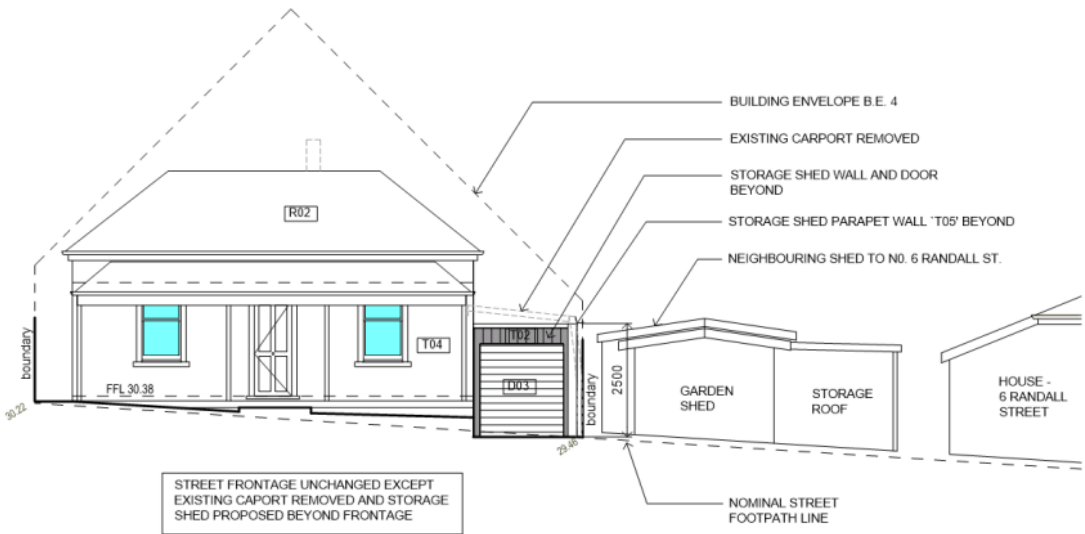
NORTH ELEVATION
SCALE 1:100

- T01** EBONY (CHARCOAL) BLOCKWORK
- T02** RADIAL TIMBER VERTICAL - silver-top ash, shiplap, 90 x 19 + 2/ 38 x 19, charcoal stain to owner's specification
- T03** RADIAL TIMBER VERTICAL - silver-top ash, shiplap, 90 x 19 + 2/ 38 x 19, clear finish to owner's specification

- T04** EXISTING WEATHERBOARD
- T05** NEW 190 BLOCKWORK PARAPET WALL ON BOUNDARY, GREY.
- R01** TRIMDEK - 0.48bmt 5" FALL TO EAVES GUTTER, COLORBOND MONUMENT (including FLASHINGS)
- R02** EXISTING ROOF
- EG1** ENCLOSED EAVES GUTTER, 200 x 125, 1.0mm G.I. ON 16mm MARINE PLY SUBSTRATE, FALL 1:150 TO d.p.
- EG2** HALF ROUND EAVES GUTTER, MONUMENT

- D01** STUDY DOOR - TIMBER FRAMED GLAZED, 2.4m HIGH, PAINTED WHISPER WHITE WITH BLACK DOOR PULL / HANDLE
- D02** 'SLIDING DOOR' - 3 PART / 2 SLIDERS POWDER COAT MONUMENT ALUMINIUM
- D03** ROLLER DOOR, 2100 HIGH, COLORBOND SURFMIST

<p>designer : gregory eade client: i. utting & h. lehoss land title ref no: CT- 15561/ 1 climate zone: 7 humidity zone: 3 corrosion environ: not closer than 1km to coast.</p>	<p>eadesign gregory eade building design 26 browne street west hobart 7000 accreditation no. C1133F t: 0427 871723 e: geade@bigpond.net.au</p>	<p>Project 1 POWELL STREET, SANDY BAY HOUSE EXTENSION</p> <p>Drawing NORTH ELEVATION</p>	<p>Number 2021 - UTTING-A10</p>
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WEST ELEVATION
SCALE 1:100

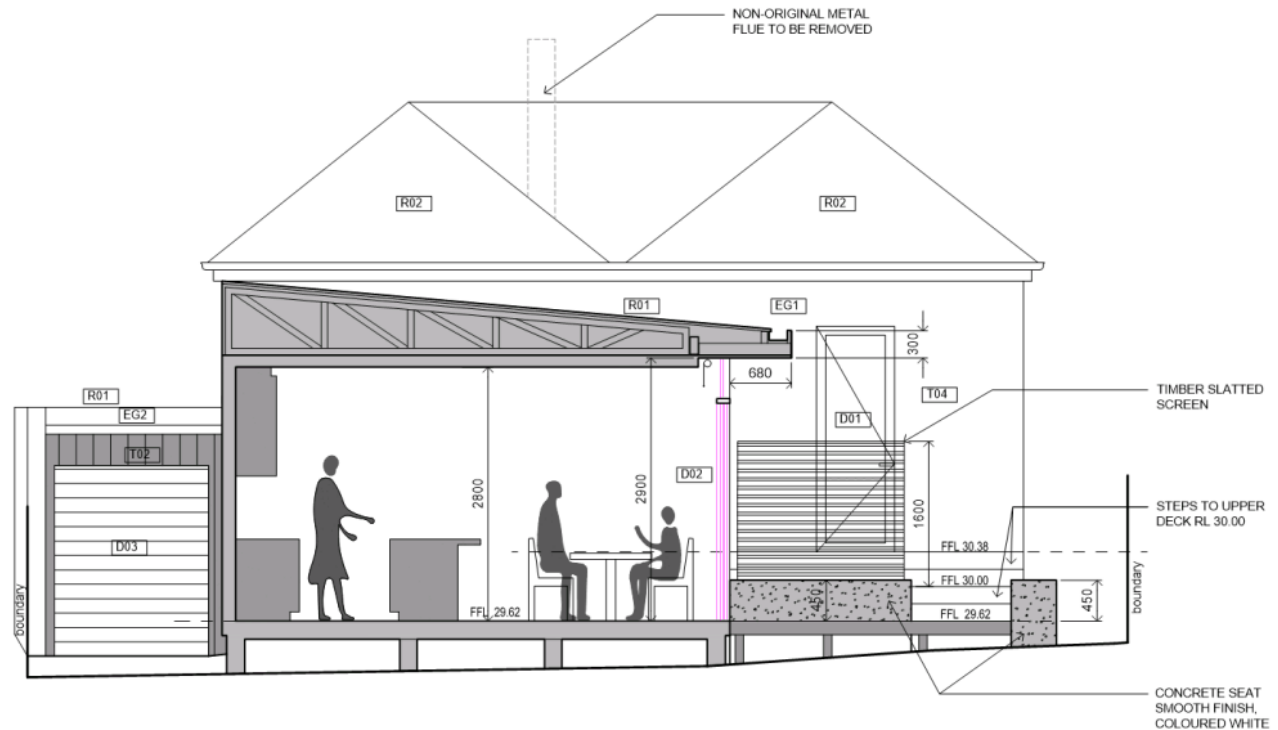
T01		EBONY (CHARCOAL) BLOCKWORK
T02		RADIAL TIMBER VERTICAL - silver-top ash, shiplap, 90 x 19 + 2/ 38 x 19, charcoal stain to owner's specification
T03		RADIAL TIMBER VERTICAL - silver-top ash, shiplap, 90 x 19 + 2/ 38 x 19, clear finish to owner's specification

T04	EXISTING WEATHERBOARD
T05	NEW 190 BLOCKWORK PARAPET WALL ON BOUNDARY, GREY.
R01	TRIMDEK - 0.48bmt 5" FALL TO EAVES GUTTER, COLORBOND MONUMENT (including FLASHINGS)
R02	EXISTING ROOF
EG1	ENCLOSED EAVES GUTTER, 200 x 125, 1.0mm G.I. ON 16mm MARINE PLY SUBSTRATE, FALL 1:150 TO d.p.
EG2	HALF ROUND EAVES GUTTER, MONUMENT

D01	STUDY DOOR - TIMBER FRAMED GLAZED, 2.4m HIGH, PAINTED WHISPER WHITE WITH BLACK DOOR PULL / HANDLE
D02	'SLIDING DOOR' - 3 PART / 2 SLIDERS POWDER COAT MONUMENT ALUMINIUM
D03	ROLLER DOOR, 2100 HIGH, COLORBOND SURFMIST

designer : client: land title ref no: climate zone: humidity zone: corrosion environ:	gregory eade l. utting & h. lehoss CT- 15561/ 1 7 3 not closer than 1km to coast.	 gregory eade building design 26 browne street west hobart 7000 accreditation no. C1133F t: 0427 871723 e: geade@bigpond.net.au	<small>DO NOT SCALE. CONTRACTOR TO SITE CHECK ALL DIMENSIONS AND EXACT LOCATION OF ALL ITEMS.</small> Project 1 POWELL STREET, SANDY BAY HOUSE EXTENSION Drawing WEST ELEVATION <small>COPYRIGHT © ALL RIGHTS RESERVED. THIS DRAWING & ITS INTELLECTUAL CONTENT REMAINS THE INTELLECTUAL PROPERTY OF GREGORY EADE</small> Number 2021 - UTTING-A11
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T01	EBONY (CHARCOAL) BLOCKWORK
T02	RADIAL TIMBER VERTICAL - silver-top ash, shiplap, 90 x 19 + 2/38 x 19, charcoal stain to owner's specification
T03	RADIAL TIMBER VERTICAL - silver-top ash, shiplap, 90 x 19 + 2/38 x 19, clear finish to owner's specification
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R01	TRIMDEK - 0.48bmt 5" FALL TO EAVES GUTTER, COLORBOND MONUMENT (including FLASHINGS)
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D02	'SLIDING DOOR' - 3 PART / 2 SLIDERS POWDER COAT MONUMENT ALUMINIUM
D03	ROLLER DOOR, 2100 HIGH, COLORBOND SURFMIST



SECTION A
SCALE 1:50

designer : gregory eade
client : l. utting & h. lehoh
land title ref no: CT- 15561/ 1
climate zone: 7
humidity zone: 3
corrosion environ: not closer than 1km to coast.

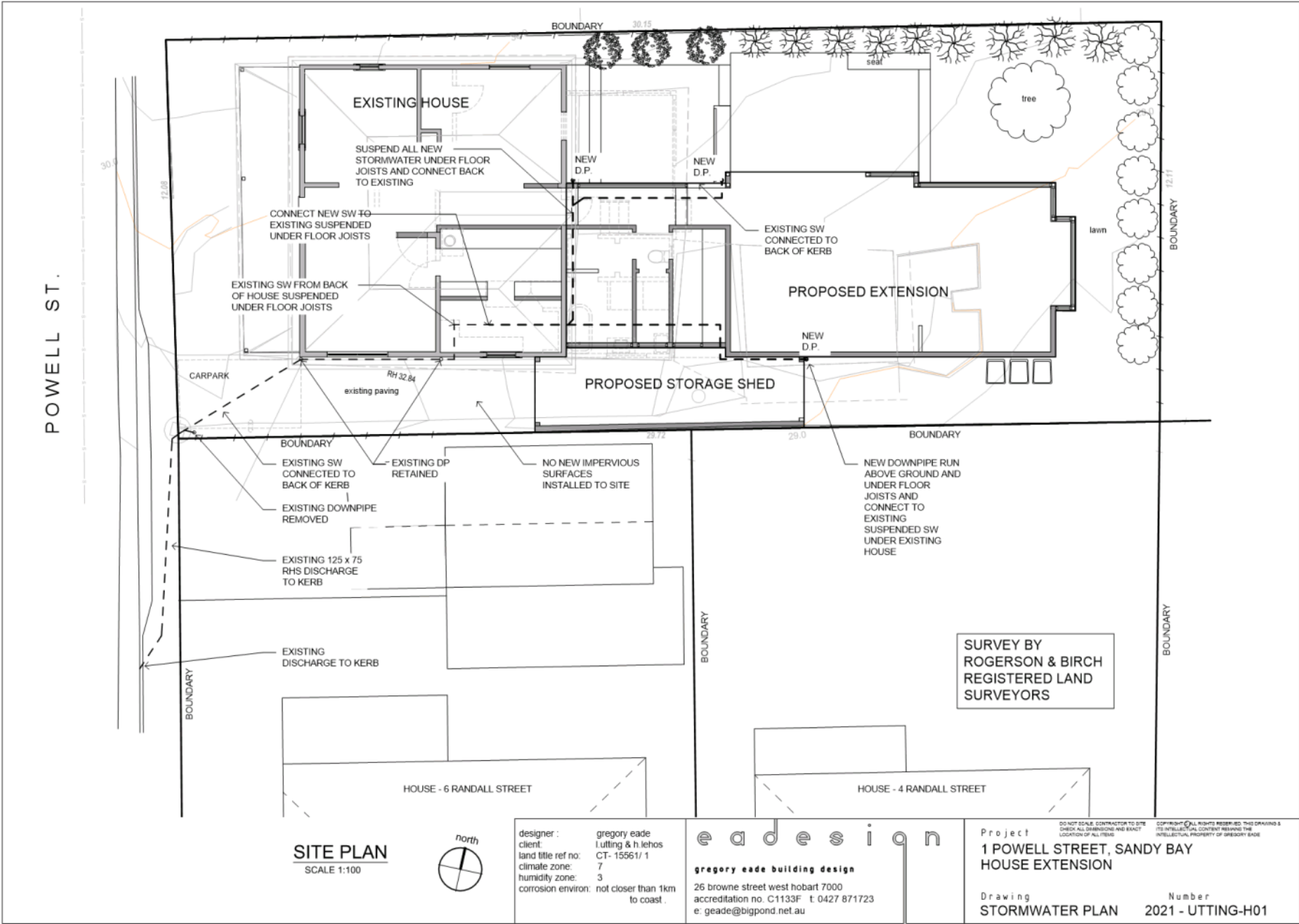
e a d e s i o n
gregory eade building design
26 browne street west hobart 7000
accreditation no. C1133F t: 0427 871723
e: geade@bigpond.net.au

Project
1 POWELL STREET, SANDY BAY
HOUSE EXTENSION

Drawing
SECTION A

Number
2021 - UTTING-A12

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Application Referral Cultural Heritage - Response

From:	Allie Costin
Recommendation:	Proposal is acceptable without conditions.
Date Completed:	
Address:	1 POWELL STREET, SANDY BAY
Proposal:	Partial Demolition, Alterations, and Extension
Application No:	PLN-22-134
Assessment Officer:	Lucy Ockenden,

Referral Officer comments:

The proposal related to a single storey, modest mid Victorian workers cottage with hipped roof, open front verandah and single storey mono-pitched rear addition, front car port and rear shed. Constructed of weatherboard, the property stands at the termination of Powell Street and is almost entirely surrounded by the rear and side gardens of properties on Princess, Parliament and Randell Streets and thus its return and rear elevations are almost entirely hidden from the public realm, whilst being prominent from private views.

The building is one of a large group of largely mostly similar scaled weatherboard cottages that sit close to each other on small plots, making up a relatively high density townscape, although individual designs of cottages do vary to a degree.

The building forms part of the Upper Sandy Bay Road Heritage Precinct (SB2) as set out in the Hobart Interim Planning Scheme 2015.

This precinct is significant for reasons including:

1. The early subdivision pattern of the main streets enhanced by the later street additions to form a coherent precinct of high overall heritage integrity.
2. The very fine examples of housing seen throughout the precinct that represent all of the major architectural styles.
3. The consistency of housing forms and the relatively low level of intrusive elements.
4. The high visual integrity of the streetscapes and the mix of development that allows the historical layers and development of the precinct to be seen and understood.
5. The extensive group of early buildings that represent the first phase of development of the Sandy Bay Precinct.

Given the modest nature of the site, it is considered that the property can clearly be read as part of the early sub-division and development of the Sandy Bay Precinct and as such is a contributory element to the character of the Heritage Precinct.

The proposal seeks the demolition of the rear extensions back to the core parts of the original cottage, to allow for the construction of an elongated rear extension that would occupy just over half the width of the original rear elevation and extend out almost to the rear boundary. Constructed with a low mono pitched roof hidden behind a low parapet, the rear addition would be clad in vertical shiplap timber in a mixture of charcoal stain and clear finish. The front car port and rear shed would also be demolished and a new storage shed constructed on the

return boundary with No.6 Randell Street.

With regard to the proposed demolition, E.13.8.1 states that demolition must not result in the loss of buildings or works that contribute to the historic cultural heritage significance of the precinct. The rear additions and garden structures highlighted for demolition could be described as being entirely in keeping with the character of the precinct, given that their form, scale, construction materials all speak of an adhoc approach to the extension of the cottage as befitting its status as a workers cottage. However, the extensions are themselves of little merit and almost entirely hidden from view and therefore make no discernable visual contribution to the character of the Precinct. As such, it is therefore considered that the extensions do not in themselves make a positive contribution to the Heritage Precinct.

With regards to the proposed rear and side extension, as with all applications relating to Heritage Precincts, the principal starting point is the degree to which development would be visible from the public realm. In this instance, due to the arrangement of sub-divisions, the rear of the site is not significantly enclosed by the built form of neighbouring properties and thus development within the rear garden would be partially visible.

The scale of the development is considered to be out of proportion with the original scale of the cottage to a degree. However, it is noted that it would sit entirely behind the main bulk of the cottage and its roof form would spring from below the original eave level, creating a pleasing and respectful transition between the old and new parts of the site. Whilst the side storage shed addition is considered unfortunate, it would stand some distance back from the original front building line and when coupled with the removal of the previous car port that stood onto the front boundary, it is considered that the overall streetscape appearance of the cottage would be improved. It is also considered that the intended use of timber as a cladding material would act as a suitable link between the old and new parts of the site and the intended colouration is considered to be relatively restrained in this instance.

As such, it is therefore considered that the impact of the proposal upon the characteristics of the wider Heritage Precinct would not result in detriment to the above as stated under Clauses E.13.8.1 P1 and E.13.8.2 P1 and P3 of the Hobart Interim Planning Scheme 2015.



Nick Booth
Heritage Officer
8 April 2022

**7.2.3 3/180-184 BATHURST STREET, HOBART - CHANGE OF USE TO
VISITOR ACCOMMODATION
PLN-22-130 - FILE REF: F22/40692**

Address: 3/180-184 Bathurst Street, Hobart
Proposal: Change of Use to Visitor Accommodation
Expiry Date: 17 May 2022
Extension of Time: Not applicable
Author: Michael McClenahan

RECOMMENDATION

That pursuant to the *Hobart Interim Planning Scheme 2015*, the Council approve the application for a change of use to visitor accommodation, at 3/ 180-184 Bathurst Street, Hobart 7000 for the reasons outlined in the officer's report and a permit containing the following conditions be issued:

GEN

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-22-130 - 3 180-184 BATHURST STREET HOBART TAS 7000 - Final Planning Documents except where modified below.

Reason for condition

To clarify the scope of the permit.

PLN 18

Prior to the commencement of the approved use, a management plan for the operation of the visitor accommodation must be submitted and approved as a Condition Endorsement, to the satisfaction of the Council's Director City Life. The management plan must include measures to limit, manage and mitigate unreasonable impacts upon the amenity of long term residents. These measures must include, but are not limited to, the following requirements:

1. To limit, manage, and mitigate noise generated as a result of the visitor accommodation.
2. To limit, manage, and mitigate behaviour issues caused as a result of the visitor accommodation.
3. To maintain the security of the building where the visitor accommodation would be located, including managing and/or limiting access to shared areas and facilities.
4. To specify the maximum permitted occupancy of the visitor accommodation.
5. To provide a name and contact phone number of a person who will respond to any complaints regarding behaviour of guests. If the property is sold the Visitor Accommodation Management Plan (VAMP) must be updated with new contact details.

Once approved, the management plan must be implemented prior to the commencement of the approved use and must be maintained for as long as the visitor accommodation is in operation. The VAMP must be provided to adjacent property owners and occupiers within 14 days of being approved. If the property is sold, the updated VAMP (in accordance with 5. above) must be provided to adjacent property owners and occupiers within 10 business days of settlement.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To ensure that visitor accommodation does not cause an unreasonable loss of residential amenity.

ADVICE

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's [website](#) for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

NOISE REGULATIONS

Click [here](#) for information with respect to noise nuisances in residential areas.

FEES AND CHARGES

Click [here](#) for information on the Council's fees and charges.


VISITOR ACCOMMODATION


More information on visitor accommodation, including when building approval is required, can be found [here](#).

In all cases, check with your insurance company that you have adequate cover.

If you are providing food for consumption on the property, you may require a food business registration in accordance with the *Food Act 2003*. Click [here](#) for more information, or call our Environmental Health team on 6238 2711.

Visitor accommodation is also considered to be a commercial use and also not eligible to residential parking permits. Under the current policy for the issuing of residential parking permits, the proposed change of use to visitor accommodation would not entitle the property to a residential parking permit, or a transferable “bed and breakfast” parking permit.

Attachment A: PLN-22-130 - 3/180-184 BATHURST STREET
HOBART TAS 7000 - Planning Committee or
Delegated Report 

Attachment B: PLN-22-130 - 3/180-184 BATHURST STREET
HOBART TAS 7000 - CPC Agenda Documents 



**APPLICATION UNDER HOBART INTERIM PLANNING SCHEME 2015**

Type of Report: Committee
Council: 17 May 2022
Expiry Date: 17 May 2022
Application No: PLN-22-130
Address: 3 / 180 - 184 BATHURST STREET , HOBART
Applicant: Anna Plunkett
52 Lights Street
Garth Plunkett
52 Lights Street
Proposal: Change of Use to Visitor Accommodation
Representations: Two
Performance criteria: Interim Planning Directive No. 6
Parking and Access Code

1. Executive Summary

- 1.1 Planning approval is sought for a change of use to Visitor Accommodation, at 3/ 180-184 Bathurst Street, Hobart.
- 1.2 More specifically the proposal includes:
 - Complete change of use of an existing two bedroom multiple dwelling to Visitor Accommodation use
- 1.3 The proposal relies on performance criteria to satisfy the following standards and codes:
 - 1.3.1 Interim Planning Directive No. 6 - Visitor Accommodation
 - 1.3.2 Parking and Access Code - Number of Parking Spaces
- 1.4 Two (2) representations objecting to the proposal were received within the statutory advertising period between 08/04/2022 - 26/04/2022.
- 1.5 The proposal is recommended for approval subject to conditions.

- 1.6 The final decision is delegated to the Council, because the application has been called-in by an Elected Member.

2. Site Detail

- 2.1 The subject site is located at 3/ 180-184 Bathurst Street, Hobart and comprises a detached two bedroom multiple dwelling on a strata lot, shared with two additional strata dwellings. The site is located internally on the site and is accessible via a small pathway. The site is located on the fringe of the Hobart CBD and the surrounding area is characterised by a combination of residential, visitor accommodation, and business and professional services uses.



Figure 1: Aerial image of the subject site (highlighted in yellow) with parent title bordered in blue, and surrounding area.

3. Proposal

- 3.1 Planning approval is sought for a change of use to Visitor Accommodation, at 3/ 180-184 Bathurst Street, Hobart.
- 3.2 More specifically the proposal is for:
- Complete change of use of an existing two bedroom multiple dwelling to Visitor Accommodation use



Figure 2: Floor plan of existing dwelling.

4. Background

4.1 There is no relevant background for this application.

5. Concerns raised by representors

5.1 Two (2) representations objecting to the proposal were received within the statutory advertising period between 08/04/2022 - 26/04/2022.

- 5.2 The following table outlines the concerns raised in the representations received. Those concerns which relate to a discretion invoked by the proposal are addressed in Section 6 of this report.

Local homeless people should have access to this accommodation on public housing rental basis rather than making them available to tourists.
Greater Hobart is Australia's least affordable capital city relative to income according to the <i>Rental Affordability Index</i> (RAI). The RAI found that tenants in Greater Hobart spend around 34 per cent of their income on rent - placing the median tenant in rental stress.
There are currently 481 investment properties being used as visitor accommodation within the Hobart City Council municipality including a nine per cent increase between 1 July 2021 - 31 December 2021.
Changes to the availability of housing stock can have significant impact on rents. Short stay accommodation has had a negative impact on the Hobart rental market by reducing supply and increasing prices.
We strongly recommend that the application for the proposed 'change of use to visitor accommodation' of premises at Unit 3, 180 - 184 Bathurst Street, Hobart is refused. In our opinion, the application does not comply with clause 3.1 (e) of <i>Planning Directive No.6</i> ... because the proposal is not compatible with the character and use of the area and it would cause an unreasonable loss of residential amenity.
Hobart should remain a suburb which prioritises long-term residents. The proliferation of short-term accommodation diminishes the "retaining of the primary residential function of the area" as set out in Performance Criteria (d) of clause 3.1 (e) of the <i>Planning Directive No.6</i> .

6. Assessment

- 6.1 The *Hobart Interim Planning Scheme 2015* is a performance based planning scheme. To meet an applicable standard, a proposal must demonstrate compliance with either an acceptable solution or a performance criterion. Where a proposal complies with a standard by relying on one or more performance criteria, the Council may approve or refuse the proposal on that basis. The ability to approve or refuse the proposal relates only to the performance criteria relied on.
- 6.2 The site is located within the Inner Residential Zone of the *Hobart Interim Planning Scheme 2015*.

- 6.3 The existing use is Residential (multiple dwelling). The proposed use is Visitor Accommodation. The existing use is a permitted use in the zone. The proposed use is a permitted use in the zone.
- 6.4 The proposal has been assessed against:
- 6.4.1 Interim Planning Directive No.6
- 6.4.2 E6.0 Parking and Access Code
- 6.5 The proposal relies on the following performance criteria to comply with the applicable standards:
- 6.5.1 Interim Planning Directive No.6
- Visitor Accommodation - 3.1 (e) P2*
- 6.5.2 Parking and Access Code:
- Number of Parking Spaces - E6.6.1 P1*
- 6.6 Each performance criterion is assessed below.
- 6.7 Visitor Accommodation - 3.1 (e) P2
- 6.7.1 The acceptable solution at clause 3.1 (e) A2 requires that Visitor Accommodation is not for a lot, as defined in the *Strata Titles Act 1998*, that is part of a strata scheme where another lot within that strata scheme is used for a residential use.
- 6.7.2 The proposal includes a change of use to Visitor Accommodation on a lot that is part of a strata scheme where another lot within that strata scheme is used for a residential use.
- 6.7.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.
- 6.7.4 The performance criterion at clause 3.1 (e) P2 provides as follows:
- Visitor Accommodation within a strata scheme must not cause an unreasonable loss of residential amenity to long term residents*

occupying other lots within the strata scheme, having regard to:

- (a) the privacy of residents;*
- (b) any likely increase in noise;*
- (c) the residential function of the strata scheme;*
- (d) the location and layout of the lots;*
- (e) the extent and nature of any other non-residential uses; and*
- (f) any impact on shared access and common property.*

- 6.7.5 The proposed application would seek to change the approved use of an existing two bedroom multiple dwelling from Residential to Visitor Accommodation. The current dwelling is part of a three lot strata scheme, shared with two conjoined multiple dwellings directly on the Bathurst Street frontage, with the dwelling in question detached from these and located internally on the site. Two representations were received during the statutory advertising period, whilst both raised concerns not related to the above performance criterion, only one directly raised objection to compliance with Planning Directive No. 6 citing incompatibility with the objectives of the clause and that such a change of use would cause an unreasonable loss of residential amenity.

The matter of housing availability is understood to remain a pressing concern in the Hobart Municipality, as well as the Greater Hobart Region. As addressed in Section 7 of this report, these concerns cannot be considered under planning assessment as *Planning Directive No.6* does not include the matter as a relevant assessment criteria.

In response to the concern that the change of use would not be consistent with the objective of the clause, specifically the compatibility with the character and use of the area, an insight of the immediate neighbourhood has been provided. It is worth noting that the "character and use of the area" is referred to in the objectives to this standard; given that this analysis is not required pursuant to the performance criteria in clause 3.1 (e) P2, this does not provide a separate basis to refuse the application.

There are approximately 372 properties in a 100m radius from the subject site, the majority of these are residential buildings along Barrack, Bathurst, Goulburn, and Melville Streets. It must be noted that the Best

Western Hotel at 156 Bathurst Street is included in this area, the majority of which is Visitor Accommodation use. Of the remaining residential buildings, a cursory review indicated that at least 6 approved Visitor Accommodation uses had been granted to addresses in this area. Compatible is understood to mean "consistent with/similar to/in harmony with/in broad correspondence with" the surrounding area. The proposed change of use, taking into account the context of the surrounding area is assessed as remaining consistent with the character and use presently existing in the surrounding area.

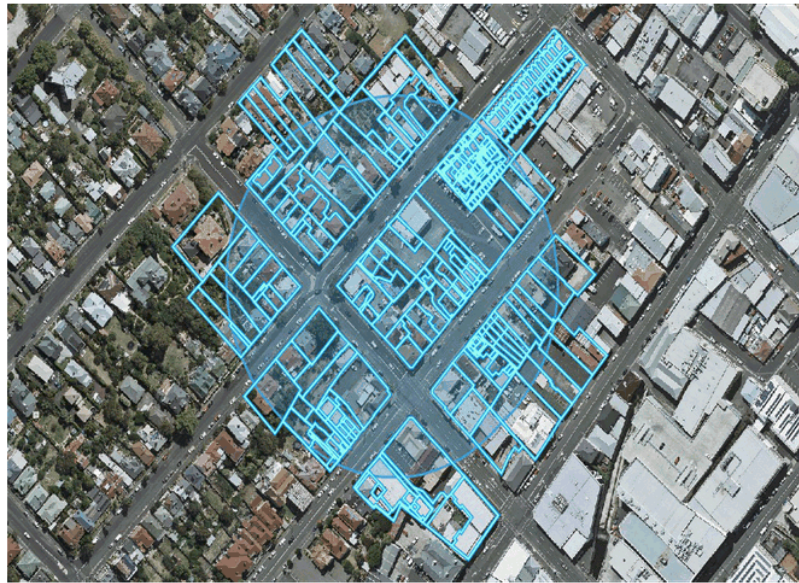


Figure 3: Land titles in the surrounding area.

With respect to the sub-clauses regarding privacy, the proposed visitor accommodation use will be accessed via a dedicated pathway from the Bathurst Street frontage and will not offer an opportunity for visitors to cross common property or other strata lots that would cause an unreasonable loss of amenity. The existing dwelling currently has two bedrooms and will not have accommodation capacity for a large number of people, accounting for this size it is also likely that the proposed change of use would be operating at noise levels similar to that of private long-term residents. The proposal will not alter the location or layout of lots or impact upon shared access or common property. The other two residential lots will not be impacted upon by the use and a clause requiring a visitor accommodation management plan will be included on the permit to further ensure behaviour and amenity is appropriately managed.

6.7.6 The proposal complies with the performance criterion.

6.8 Number of Parking Spaces - E6.6.1 P1

6.8.1 The acceptable solution at clause 6.6.1 A1 requires that the number of on-site car parking spaces must be no less than and no greater than the number specified in Table E6.1, which is one on-site space.

6.8.2 The proposal includes no on-site car parking spaces.

6.8.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.

6.8.4 The performance criterion at clause 6.6.1 P1 provides as follows:

The number of on-site car parking spaces must be sufficient to meet the reasonable needs of users, having regard to all of the following:

(a) car parking demand;

(b) the availability of on-street and public car parking in the locality;

(c) the availability and frequency of public transport within a 400m walking distance of the site;

(d) the availability and likely use of other modes of transport;

(e) the availability and suitability of alternative arrangements for car parking provision;

(f) any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces;

(g) any car parking deficiency or surplus associated with the existing use of the land;

(h) any credit which should be allowed for a car parking demand deemed to have been provided in association with a use which existed before the change of parking requirement, except in the case of substantial redevelopment of a site;

(i) the appropriateness of a financial contribution in lieu of parking towards the cost of parking facilities or other transport facilities, where such facilities exist or are planned in the vicinity;

(j) any verified prior payment of a financial contribution in lieu of parking for the land;

(k) any relevant parking plan for the area adopted by Council;

(l) the impact on the historic cultural heritage significance of the site if subject to the Local Heritage Code;

(m) whether the provision of the parking would result in the loss, directly or indirectly, of one or more significant trees listed in the Significant Trees Code.

6.8.5 Referral was made to Council's Development Engineer who has provided the following assessment:

The number of on-site car parking spaces must be sufficient to meet the reasonable needs of users, having regard to all of the following:

(a) car parking demand;

- As advised by successive CoH traffic engineers the empirical parking assessment indicates that the provision of 1 off-site car parking space will operate at an adequate level of service to meet likely demands associated with the development. It is acknowledged on-street car parking capacity may become significantly reduced during peak periods however, should still offer an adequate level of service and would not be considered unusual when compared to other areas in close proximity to the CBD. The customers utilising the visitor accommodation and any visitors will need to compete for available car parking spaces on the public road.

There is limited survey data to indicate the percentage of persons utilising visitor accommodation dwellings that utilise vehicles as a mode of transport.

(b) the availability of on-street and public car parking in the locality;

- There is a relatively large supply of on-street parking in the

surrounding road network however, it is in very high demand. Much of the available parking is in the form of time-restricted parking, with authorised residents excepted. Observations indicate that there is a pool of parking that would be available to meet the potential demands including visitors and overflow parking, particularly after normal working hours.

(c) the availability and frequency of public transport within a 400m walking distance of the site;

- Metro Tasmania operate regular bus services along Liverpool Street which is within 400 metres of the subject site. Skybus currently operate a stop from 157 Collins Street approx. 500m away.

(d) the availability and likely use of other modes of transport;

- The site is located a convenient walking/cycle distance from shops, services and the CBD.

(e) the availability and suitability of alternative arrangements for car parking provision;

- Council and private car parks available within 550m during business hours.

(f) any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces;

- Not applicable.

(g) any car parking deficiency or surplus associated with the existing use of the land;

- Not applicable.

(h) any credit which should be allowed for a car parking demand deemed to have been provided in association with a use which existed before the change of parking requirement, except in the case of substantial redevelopment of a site;

- Not applicable.

(i) the appropriateness of a financial contribution in lieu of parking towards the cost of parking facilities or other transport facilities, where such facilities exist or are planned in the vicinity;

- Not applicable.

(j) any verified prior payment of a financial contribution in lieu of parking for the land;

- Not applicable.

(k) any relevant parking plan for the area adopted by Council;

- Not applicable.

(l) the impact on the historic cultural heritage significance of the site if subject to the Local Heritage Code; and

- Not applicable.

(m) whether the provision of the parking would result in the loss, directly or indirectly, of one or more significant trees listed in the Significant Trees Code.

- No impact.

The following advice has been included on the planning permit;

Visitor accommodation is also considered to be a commercial use and also not eligible to residential parking permits.

Under the current policy for the issuing of residential parking permits, the proposed change of use to visitor accommodation would not entitle the property to a residential parking permit, or a transferrable "bed and breakfast" parking permit.

Based on the above assessment and given the submitted documentation, the parking provision may be accepted under Performance Criteria P1:E6.6.1 of the Planning Scheme. This is particularly due to the actual parking demands that will be generated by the development.

6.8.6 The proposal complies with the performance criterion.

7. Discussion

7.1 Planning approval is sought for a change of use to Visitor Accommodation, at 3/ 180-184 Bathurst Street, Hobart.

7.2 The application was advertised and received two (2) representations. The representations raised concerns including that the change of use would not be compliant with the performance criterion under the *Planning Directive No.6* with respect to impacts to privacy, impacts to residential amenity, and impacts to the primary residential function of the area

All representations also raised concern about the impacts of the increasing loss of dwellings from Residential to Visitor Accommodation on housing availability to the wider community, contributing to housing stress and the loss of long term residential accommodation. Whilst such concerns have considerable social weight and highlight a growing issue in the Hobart Municipality, and Greater Hobart Region, they are not considered relevant for planning assessment, under the current State Government issued *Planning Directive 6* and therefore cannot be considered under the discretionary assessment.

7.3 The proposal has been assessed against the relevant provisions of the planning scheme and is considered to perform well.

7.4 The proposal has been assessed by other Council officers, including the Council's Development Engineer. The officers have raised no objection to the proposal, subject to conditions.

7.5 The proposal is recommended for approval.

8. Conclusion

8.1 The proposed change of use to Visitor Accommodation, at 3/ 180-184 Bathurst Street, Hobart satisfies the relevant provisions of the *Hobart Interim Planning Scheme 2015*, and as such is recommended for approval.

9. Recommendations

That: Pursuant to the *Hobart Interim Planning Scheme 2015*, the Council approve the application for a change of use to Visitor Accommodation, at 3/ 180-184 Bathurst Street, Hobart for the reasons outlined in the officer's report and a permit containing the following conditions be issued:

GEN

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-22-130 - 3 180-184 BATHURST STREET HOBART TAS 7000 - Final Planning Documents except where modified below.

Reason for condition

To clarify the scope of the permit.

PLN 18

Prior to the commencement of the approved use, a management plan for the operation of the visitor accommodation must be submitted and approved as a Condition Endorsement, to the satisfaction of the Council's Director City Life. The management plan must include measures to limit, manage and mitigate unreasonable impacts upon the amenity of long term residents. These measures must include, but are not limited to, the following requirements:

1. **To limit, manage, and mitigate noise generated as a result of the visitor accommodation.**
2. **To limit, manage, and mitigate behaviour issues caused as a result of the visitor accommodation.**
3. **To maintain the security of the building where the visitor accommodation would be located, including managing and/or limiting access to shared areas and facilities.**
4. **To specify the maximum permitted occupancy of the visitor accommodation.**
5. **To provide a name and contact phone number of a person who will respond to any complaints regarding behaviour of guests. If the property is sold the Visitor Accommodation Management Plan (VAMP) must be updated with new contact details.**

Once approved, the management plan must be implemented prior to the

commencement of the approved use and must be maintained for as long as the visitor accommodation is in operation. The VAMP must be provided to adjacent property owners and occupiers within 14 days of being approved. If the property is sold, the updated VAMP (in accordance with 5. above) must be provided to adjacent property owners and occupiers within 10 business days of settlement.

Advice:

This condition requires further information to be submitted as a Condition Endorsement. Refer to the Condition Endorsement advice at the end of this permit.

Reason for condition

To ensure that visitor accommodation does not cause an unreasonable loss of residential amenity.

ADVICE

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's [website](#) for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

NOISE REGULATIONS

Click [here](#) for information with respect to noise nuisances in residential areas.

FEES AND CHARGES

Click [here](#) for information on the Council's fees and charges.

VISITOR ACCOMMODATION

More information on visitor accommodation, including when building approval is required, can be found [here](#).

In all cases, check with your insurance company that you have adequate cover.

If you are providing food for consumption on the property, you may require a food business registration in accordance with the *Food Act 2003*. Click [here](#) for more information, or call our Environmental Health team on 6238 2711.

Visitor accommodation is also considered to be a commercial use and also not eligible to residential parking permits. Under the current policy for the issuing of residential parking permits, the proposed change of use to visitor accommodation would not entitle the property to a residential parking permit, or a transferable "bed and breakfast" parking permit.



(Michael McClenahan)

Development Appraisal Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.



(Ben Ikin)

Senior Statutory Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

Date of Report: 2 May 2022

Attachment(s):

Attachment B - CPC Agenda Documents

Planning: #251710

Property

3/180-184 BATHURST STREET HOBART TAS 7000

People**Applicant ***

Anna Plunkett
52 Lights Street
Emerald beach NSW 2456
0423 345 172
anna.plunkett@yahoo.com.au

Applicant *

Garth Plunkett
52 Lights Street
Emerald beach NSW 2456
0400 400 086
garthplunkett@yahoo.com

Owner *

Garth Plunkett
52 Lights Street
Emerald beach NSW 2456
0400 400 086
garthplunkett@yahoo.com

Owner *

Anna Plunkett
52 Lights Street
Emerald beach Nsw 245)
0423 345 171
anna.plunkett@yahoo.com.au

Entered By

ANNA PLUNKETT
0423 345 171
anna.plunkett@yahoo.com.au

Use

Other

Details

Have you obtained pre application advice?

☐ No

If YES please provide the pre application advice number eg PAE-17-xx

Are you applying for permitted visitor accommodation as defined by the State Government Visitor Accommodation Standards? Click on help information button for definition. If you are not the owner of the property you MUST include signed confirmation from the owner that they are aware of this application. *

☒ Yes

Is the application for SIGNAGE ONLY? If yes, please enter \$0 in the cost of development, and you must enter the number of signs under Other Details below. *

☒ No

If this application is related to an enforcement action please enter Enforcement Number

Details

What is the current approved use of the land / building(s)? *

Residential

Please provide a full description of the proposed use or development (i.e. demolition and new dwelling, swimming pool and garage) *

Short term accommodation facility (air bnb)

Estimated cost of development *

0.00

Existing floor area (m2)

Proposed floor area (m2)

Site area (m2)

Carparking on Site

Total parking spaces

1

Existing parking spaces

1

N/A

☒ Other (no selection chosen)

Other Details

Does the application include signage? *

☒ No

How many signs, please enter 0 if there are none involved in this application? *

0

Tasmania Heritage Register

Is this property on the Tasmanian Heritage Register?

☒ No

Documents

Required Documents

Title (Folio text and Plan and Copy signed Transfer.pdf
Schedule of Easements) *

Plans (proposed, existing) * 182 bathurst.jpg

Covering Letter Cover Letter - Change of Use Application.pdf

Building self assessment Office Use Only (4).pdf
Form permitted visitor

accommodation

COVER LETTER – APPLICATION FOR CHANGE OF USE

3/182 BATHURST ST, HOBART 7000

Garth and Anna Plunkett

28th February, 2022

0400 400 086

We have recently bought the residential property of 3/182 Bathurst St, Hobart which is a two bedroom property. We are intending to use the home for our holidays at this stage, and when we are not there, we are intending to AirBnb the property for short term rentals for a maximum total of 4 occupants.

Regards,

Garth and Anna Plunkett

Office Use Only
[Insert council branding and contact details]
Application no.
Date received:
Fee:
Paid:

Guidance Information

Visitor Accommodation Use in Existing Habitable Buildings Standard Application Package

The Standard Application Package has been approved by the Minister for Planning to provide a simple pathway for seeking approval for the use of existing homes or habitable buildings for Visitor Accommodation as prescribed below. It comprises an Application for Planning Permit and a building self-assessment Form.

Completed forms must be lodged with the relevant planning/permit authority.

Application for Planning Permit

The Application for Planning Permit form relates to *Planning Directive No. 6 – Exemption and Standards for Visitor Accommodation in Planning Schemes*, issued by the Minister for Planning under former sections 13(1)(a) and (4) of the *Land Use Planning and Approvals Act 1993*, and effective from 1 July 2018 and as modified on 1 August 2018.

The Application for Planning Permit form applies to the change of use of an existing habitable building where it is 'Permitted' under Planning Directive No. 6, as set out below:

Planning Scheme	Requirements
Interim planning schemes	<p>Change of use to Visitor Accommodation if:</p> <ul style="list-style-type: none"> located within the General Residential Zone; Inner Residential Zone, Low Density Residential Zone, Rural Living Zone, Environmental Living Zone, or Village Zone; not located within the Battery Point Heritage Precinct (BP1); guests are accommodated within existing habitable buildings; the use occupies not more than 200m² gross floor area per lot; the use is not within a strata scheme¹ that includes another lot, as defined in section 3 of the <i>Strata Titles Act 1998</i>, that is used for Residential use; and all other requirements in the planning scheme are met that are necessary for a 'Permitted' use.
Sullivans Cove Planning Scheme 1997	<p>Change of use to Bed and Breakfast Establishment or Visitor Accommodation if:</p> <ul style="list-style-type: none"> located within Activity Area 1.0 Inner City Residential (Wapping); guests are accommodated within existing habitable buildings; the use occupies not more than 200m² floor area per lot; and all other requirements in the planning scheme are met that are necessary for a 'Permitted' use.

¹ Strata scheme is defined in section 3 of the *Strata Titles Act 1998*.

The Application for Planning Permit form does not apply if:

- the use is exempt from requiring a planning permit under Planning Directive No.6, as set out below:

Planning Scheme	Exempt Qualification
Interim planning schemes	Visitor Accommodation use in a dwelling (including an ancillary dwelling) if...
<i>Flinders Planning Scheme 2000</i>	Visitor Accommodation use in a House, House and Ancillary Apartment or Grouped House if...
<i>Sullivans Cove Planning Scheme 1996</i>	Bed and Breakfast Establishment or Visitor Accommodation uses in a dwelling if...

- (i) it is used by the owner or occupier as their main place of residence, and only let while the owner or occupier is on vacation or temporarily absent; or
- (ii) it is used by the owner or occupier as their main place of residence, and visitors are accommodated in not more than 4 bedrooms.

- the use requires a 'Discretionary' planning permit under the planning scheme. For example, a change of use to Visitor Accommodation that does meet the requirements for a 'Permitted' use under Planning Directive No. 6, or other provisions in the planning scheme apply requiring discretionary assessment, such as off-street parking, bushfire planning, heritage, or non-residential use standards in zones (e.g. external lighting requirements).

Applicants should use the standard Council planning application form.

- other uses or if any development (not otherwise exempt) is proposed, in addition to the change of use to Visitor Accommodation.

Applicants should use the standard Council planning application form.

Building self-assessment form

The building self-assessment form is mandated under the *Director's Determination – Short or Medium Term Visitor Accommodation*, issued by the Director of Building Control under section 20(1)(e) of the *Building Act 2016*, and effective from 1 July 2018.

This Determination applies only to existing dwellings or residential premises where a fee is being charged for the use of short or medium term visitor accommodation.

The building self-assessment form must be completed in the following situations where the property is used or intended to be used as visitor accommodation:

- owner occupiers of residential premises of more than four bookable rooms, or
- investment properties or shacks (not occupied by the owner) that have a gross floor area of not more than 200m² used for visitor accommodation.

The building self-assessment form requires the owner or occupier to declare that the property meets the minimum building standards with respect to an occupancy permit, plumbing, and essential building services.

The Determination and the building self-assessment form apply, irrespective of the planning requirements. The planning and building requirements are mutually exclusive. If any premises intended to be let for short-term visitor accommodation is a lot in a strata title scheme, and any other premises in that scheme are occupied as a residence by long term residents, the proponent is not permitted to use the building self-assessment process, unless the premises is located within Activity Area 1.0 Inner City Residential (Wapping) under the Sullivans Cove Planning Scheme 1997.

APPLICATION FOR PLANNING PERMIT
CHANGE OF USE TO VISITOR ACCOMMODATION

Section 58 of Land Use Planning and Approvals Act 1993

To: HOBART CITY COUNCIL Planning Authority

The Proposal:

(Must tick one)



Interim Planning Schemes:

Change of use to Visitor Accommodation if:

- guests are accommodated in existing habitable buildings;
- the use has a gross floor area of not more than 200m² per lot;
- the use is not within a strata scheme² that includes another lot, as defined in section 3 of the *Strata Titles Act 1998*, that is used for Residential use; and
- the land is within one of the following zones:
 - General Residential;
 - Inner Residential, excluding land within the Battery Point Heritage Precinct 1 (BP1);
 - Low Density Residential;
 - Rural Living;
 - Environmental Living;
 - Village.



Sullivans Cove Planning Scheme 1997:

Change of use to Bed and Breakfast Establishment or Visitor Accommodation, where guests are accommodated in existing habitable buildings and the use has a floor area of not more than 200m² per lot, and the land is within the Activity Area 1.0 Inner City Residential (Wapping).

Description:

Brief description of the proposed change of use, including whether the whole or part of the building(s) are to be used:

The 2 bedroom residence will be used as a short-term accommodation facility (Air BnB)

Applicant: Who is making the application?

Applicant Name: GARTH AND ANNA PLUNKETTBusiness /
Company Name: Postal Address: 52 LIGHTS ST,
EMERALD BEACHNEW SOUTH WALES 2456Phone
No: 0400 400086Email address: anna.plunkett@yahoo.com.au

² Strata scheme means the complex of lots and common property (together with the system of administration and management) created on the registered strata plan.

The Land: Detail address and title particulars of the land for the proposed change of use

Street Address: 3/182 BATHURST ST

HOBART 7000

Certificate of Title Reference No.

Describe the way the land is used now:

Currently used as residential

The Owner: Owner's name and address, if land is not in applicant's ownership

(If more than one owner, all names and addresses must be provided)

Owner Name:

Business / Company Name:

Postal Address:

Phone No:

Email address:

The Applicant: Is the applicant the owner of the land?

(Must tick one)

☒ Yes - please complete Section A below.☐ No - please complete Section B below, and if relevant Sections C and D.**Section A: Owner's Verification**

I/we am/are the owner(s) of the land.

Owner(s): ANNA PLUNKETT / GARETA PLUNKETT
Name: [print] Signed [Signature] Date 28/2/22**Section B: Applicant's Verification**

I/we, the applicant declare that the owner /each of the owners of the land have been notified of the intention to make this application.

Applicant: Name: [print] Signed Date

Section C: If the application involves land owned or administered by a council

The [] consents to the making of this permit application.

General Manager: Name: [print] Signed Date

Section D: If the application involves land owned or administered by the Crown

The application must be signed by the Minister or relevant delegate responsible for the land and accompanied with written permission.

Declaration (to be completed for all applications)

I declare that the information I have given in this permit application to be true and correct to the best of my knowledge.

Applicant: Name: [print] Signed Date

ANNA PLUNKETT  28/2/22

GARA PLUNKETT  28/2/22

Personal Information Protection Statement

As required under the *Personal Information Protection Act 2004*

1. Personal information is managed in accordance with the *Personal Information Protection Act 2004* and may be accessed by the individual to whom it relates, on request to the relevant planning authority.
2. Information can be used for other purposes permitted by the *Local Government Act 1993* and regulations made by or under that Act, and, if necessary, may be disclosed to other public sector bodies, agents or contractors of the relevant planning authority.

Planning Application Checklist

The Planning Authority requires the following to assess this Planning Application, with all documentation provided as required by the planning authority:

- (a) Completed Planning Application Form - all relevant sections filled in and signed by land owner (if required) and applicant.
- (b) A copy of the current certificate of title for all land to which the permit sought is to relate (available from Service Tasmania or from www.thelist.tas.gov.au).
- (c) Either:
 - (i) a basic floor plan of the existing habitable building(s) to scale, including identification of the gross floor area for the proposed change of use to visitor accommodation, or
 - (ii) a signed declaration by the applicant confirming the area of the existing habitable building(s) for the proposed change of use to visitor accommodation has a gross floor area³ of not more than 200m²
- (d) Payment of the prescribed fee (up to \$250.00).

Failure to provide the required information may result in your application not being able to be accepted or processed.

³ Or floor area in the case of the Sullivans Cove Planning Scheme 1997.

BUILDING SELF-ASSESSMENT FORM**Director's Determination – Short or Medium Term Visitor Accommodation****Section 20(1)(e) of Building Act 2016**

This building self-assessment form must be completed in the following situations where the property is used or intended to be used for visitor accommodation, and a fee is being charged for such use:

- > owner occupiers of residential premises of more than four bookable rooms, or
- > investment properties or shacks (not occupied by the owner) that have a gross floor area of not more than 200m² per lot used for visitor accommodation.

The completed form must be lodged with the relevant Permit Authority.

If any premises intended to be let for short-term visitor accommodation is a lot in a strata title scheme, and any other premises in that scheme are occupied as a residence by long term residents, the proponent is not permitted to use the building self-assessment process, unless the premises is located within Activity Area 1.0 Inner City Residential (Wapping) under the Sullivans Cove Planning Scheme 1997.

To: HOBART CITY COUNCIL Permit Authority
13 ELIZABETH ST Address
HOBART 7001 Suburb/postcode

Owner / Occupier details:

(Only an owner or occupier may complete this form)

Owner/ ☒ Occupier: ANNA AND GARTH PLUNKETT
 (Delete one not applicable)
 Postal Address: 52 LEANES ST Phone No: 0400400086
EMERALD BEACH NSW 2458
 Email address: anna.plunkett@yahoo.com.au

Address of Property used or intended to be used for Visitor Accommodation:

Street Address: 3/182 BATHURST ST
HOBART TAS 7000
 Certificate of Title Reference No.

Owner / Occupier Declaration:

I/we, as the owner / occupier of the property, declare that the property meets the following minimum building requirements, as set out below:

Owner/ ☒ Occupier: ANNA PLUNKETT Signed [Signature] Date 28/2/22
 (Delete one not applicable)
[Signature] GARTH PLUNKETT [Signature] 28/2/22

Occupancy Permit:

(Must tick one)

The owner or occupier is to declare that –

- ☒ (a) if an occupancy permit has been issued, the premises is fit for occupation consistent with that permit, and the maximum number of occupants stated on the permit will not be exceeded;

OR

- ☐ (b) an occupancy permit or occupancy certificate was not required (as the premises was constructed / altered before 1994).

Plumbing:

(Must tick (a) or (b) and (c) or (d))

The owner or occupier is to declare that –

- ☒ (a) the premises is connected to a reticulated sewerage system;

OR

- ☐ (b) the premises is connected to an on-site wastewater management system that:
- is in good working order and will be maintained to perform to the same standard as it was designed; and
 - has a land application distribution area designed, installed and in good serviceable condition; and
 - the maximum number of occupants of the premises the system is designed for is not exceeded; and
 - there is a maintenance contract in place for the servicing of the system.

- ☒ (c) the premises is connected to a reticulated drinking water supply system;

OR

- ☐ (d) a private drinking water supply (including from a tank, well, dam, etc.) is provided for the premises that meets the requirements of the *Public Health Act 1997*.

Essential Building Services:

(Must tick one)

The owner or occupier is to declare that –

- ☒ (a) regarding Essential Building Services, the premises has an approved schedule of maintenance, and fire safety features are maintained in accordance with Part 7 (regulations 72 to 78) of the *Building Regulations 2016* and the Director's Maintenance of Prescribed Essential Building Services Determination;

OR

- ☐ (b) the premises is not required to have an approved essential maintenance schedule, but the following fire safety features are installed and maintained in accordance with manufacturer's instructions:
- a smoke alarm with a 10-year non-removable lithium battery, or
 - a hard wired smoke alarm (and are interconnected where there is more than one alarm fitted);
- (a) if any storey of the premises contains a bedroom –
- (i) installed in every corridor, or hallway, situated in the storey, that is associated with a bedroom; and

- (ii) if there is no corridor, or hallway, situated in the storey, that is associated with a bedroom, between that part of the premises containing the bedroom and the remainder of the premises; and
- (b) in any other storey of the premises that does not contain a bedroom.
- If multistorey premises are let for visitor accommodation:
 - i. emergency evacuation lighting is provided; and
 - ii. exits are provided that are clearly marked and mapped for the visitor.

Garth and Anna Plunkett

17th March, 2022

RE: APPLICATION FOR CHANGE OF USE

To whom it may concern,

We, Garth and Anna Plunkett, purchasers of 3/182 Bathurst St Hobart have notified by a signed document the individuals currently on the title, Laurene Gaye Mandelson and Angela Jayne Davies, of our intention to apply for a change of use permit through the council.

Regards,

Garth and Anna Plunkett

Signed.....



**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

SEARCH OF TORRENS TITLE

VOLUME	FOLIO
58199	3
EDITION	DATE OF ISSUE
5	14-Aug-2019

SEARCH DATE : 23-Dec-2021

SEARCH TIME : 02.43 PM

DESCRIPTION OF LAND

City of HOBART

Lot 3 on Strata Plan 58199 (formerly being STR1862) and a
general unit entitlement operating for all purposes of the
Strata Scheme being a 1 undivided 1/3 interest

Derived from Strata Plan 58199

Derivation : Whole of 0A-0R-26Ps (Section C.C) Gtd to G

Flexmore

Prior CT 4206/51

SCHEDULE 1M674172 TRANSFER to LAURENE GAYE MANDELSON and ANGELA JAYNE
DAVIES Registered 23-Jan-2018 at 12.01 PMSCHEDULE 2Reservations and conditions in the Crown Grant if any
The registered proprietor holds the lot and unit entitlement
subject to any interest noted on common property
Folio of the Register volume 58199 folio 0UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



FOLIO PLAN
RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



Conveyancing and Law of Property Act 1884

STRATUM PLAN

No. 1862

Sheet 1 of 4 Sheets

City or Town HOBAIR

Locality 180 + 184 BATHURST STREET

Reference to Title C.F. 4200 - 17

Site comprises the whole of Lot 1 on Plan No. 100/100 in the

Lands Titles Office

The name of the building is 190 - 194 BATHURST STREET, HOBART

N SITE PLAN

SCALE 1:400

MEASUREMENTS IN METRES

REGISTERED NUMBER

58199

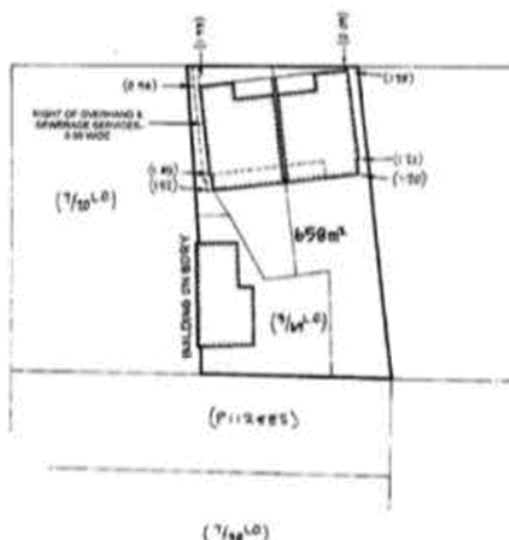
External
surface
boundaries of
the site and
the location of
the building
in relation
thereto to
be delineated
in this space

STREET

BARRACK

BATHURST

STREET



REGISTERED this 7th day of October 1935, No. 1862

This plan is lodged for registration by

OGILVIE, M'KENNA.

E. R. Thayer
Recorder of Titles

2008.08.19(木)



FOLIO PLAN

RECORDED OF TITLES

Issued Pursuant to the Land Titles Act 1980



Sheet 2 of 4 Sheets

No. 1862

A **Town Clerk/Assault Clerk**

GROUND FLOOR

SCALE 1:100

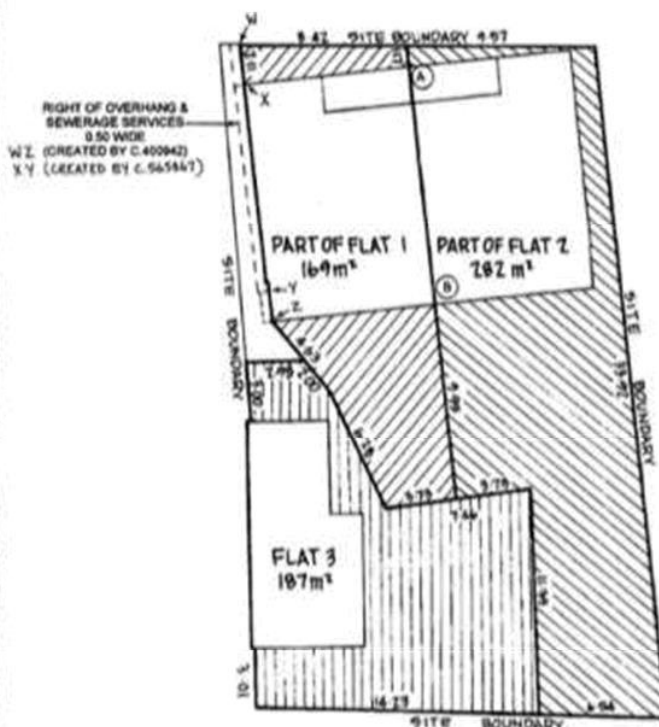
ALL HORIZONTAL FLAT BOUNDARIES ARE SHOWN BY HEAVY UNBROKEN LINES.

ALL BOUNDARIES ARE THE INTERFACES OF BRICK WALLS UNLESS OTHERWISE STATED.

BOUNDARY A - B ALONG CENTRE LINE OF BRICK WALL.

THE HATCHED PORTIONS OF THE FLATS ARE OPEN AND EXTEND VERTICALLY FROM GROUND LEVEL FOR A HEIGHT OF 2.50M AND HORIZONTALLY AS DEFINED BY MEASUREMENT.

THE BUILDING ON PLAYS 1 & 2 EXTENDS HORIZONTALLY TO THE OUTER FACE OF WALLS AND VERTICALLY FROM CENTRE LINE OF FLOOR TO THE UPPER FACE OF THE ROOF ABOVE.



BUILDING

THE ~~WALL~~ ON FLAT 3 EXTENDS VERTICALLY FROM GROUND LEVEL TO THE UPPER FACE OF THE ROOF ABOVE, AND HORIZONTALLY AS DEFINED BY MEASUREMENT, TO THE OUTER FACE OF WALLS.

the List

FOLIO PLAN
RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



If further sheets are required to illustrate the flats, the sheets should be pinned here. Further sheets must be of paper supplied for the purpose by the Recorder of Titles and bearing his seal, and be numbered consecutively, commencing from sheet 4.

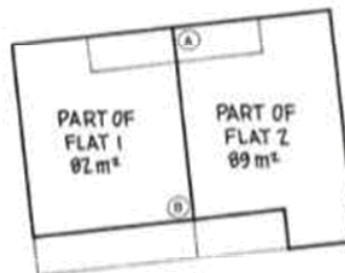
Sheet 3 of 4 Sheets

No. 1862

[Signature]
Town Clerk / Council Clerk

BASEMENT
SCALE 1:100

ALL HORIZONTAL BOUNDARIES ARE SHOWN BY HEAVY UNBROKEN LINES.
ALL BOUNDARIES ARE THE OUTER FACES OF BRICK WALLS UNLESS OTHERWISE STATED.
BOUNDARY A - B ALONG CENTRE LINE OF BRICK WALL.



the List

FOLIO PLAN
RECORDER OF TITLES
Issued Pursuant to the Land Titles Act 1980

Tasmanian Government

Sheet A of A Sheets

No. 1862

[Signature]
Town Clerk/Municipal Clerk

The address for service of notices on the company is:—

NO. 182 & 184 BATHURST ST - HOBART
7000

SURVEYOR'S CERTIFICATE

I, JOHN L. CERUTTY
of ROSEY PARK,
a surveyor registered under the Land Surveyors
Act 1909, hereby certify that the building
erected on the site described and delineated on
sheet 1 of this plan is within the external bound-
aries of the title stated on sheet 1.

Dated this 27th day of JULY 1985

[Signature]
Registered Surveyor

COUNCIL CLERK'S CERTIFICATE

I certify that the subdivision shown in this plan
has been approved by the

HOBART CITY Council

Dated this 27th day of August 1985

[Signature]
Town Clerk/Municipal Clerk

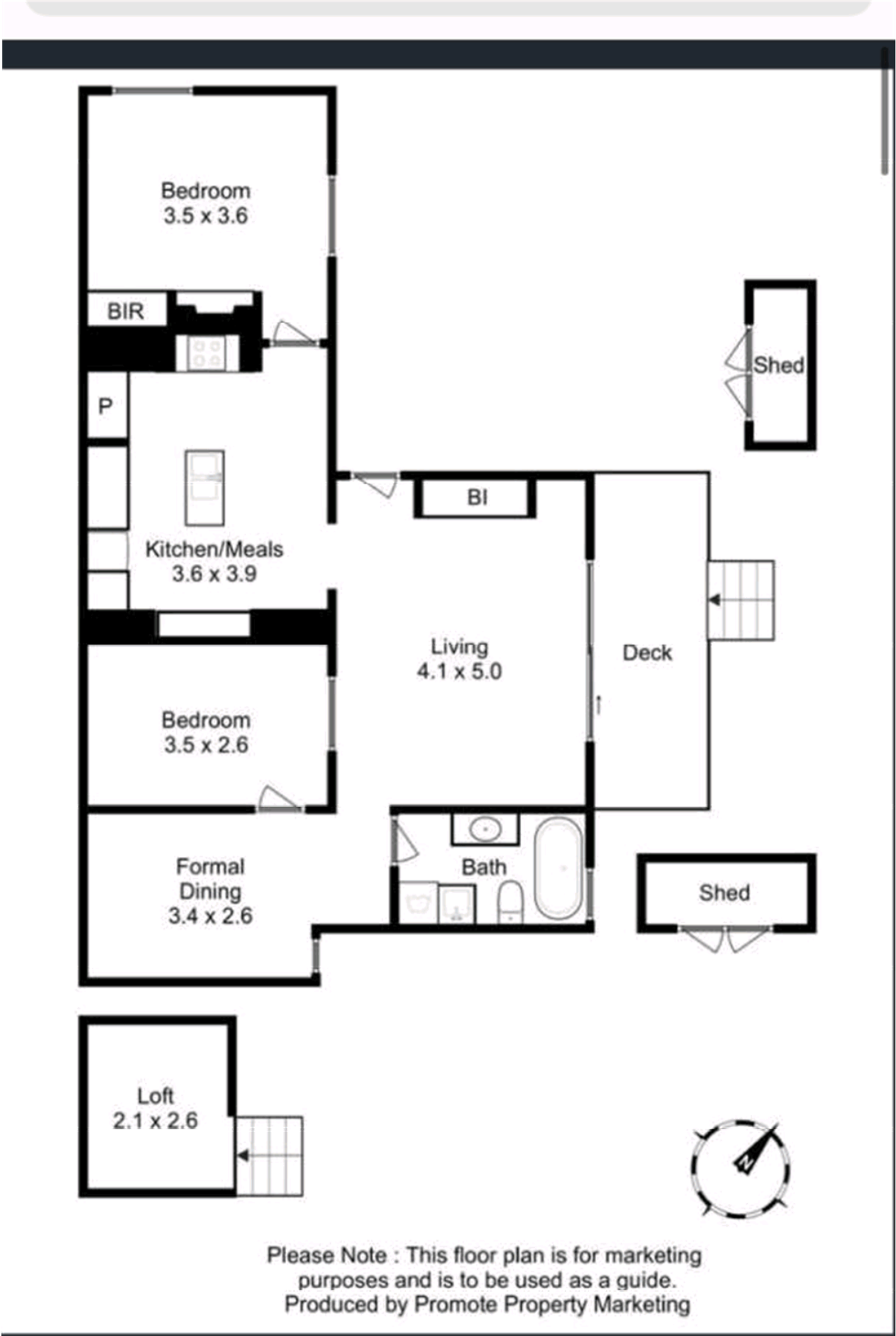
FOR OFFICE USE ONLY

UNIT ENTITLEMENTS

Plot	Unit Entitlement	For Office Use Only
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1	1	4206 49
2	1	50
3	1	51

Total 3



8. REPORTS

8.1 Regional Strategy - Adapting to a Changing Coastline in Tasmania File Ref: F22/36341

Report of the Senior Climate Change Officer and the Director City Life of 3 May 2022 and attachments.

Delegation: Council

REPORT TITLE: REGIONAL STRATEGY - ADAPTING TO A CHANGING COASTLINE IN TASMANIA**REPORT PROVIDED BY:** Senior Climate Change Officer
Director City Life**1. Report Purpose and Community Benefit**

1.1. The purpose of this report is:

1.1.1. To advise of the recent release by the Southern Tasmanian Councils Authority (STCA) of the draft '*Regional Strategy – Adapting to a changing coastline in Tasmania*' (the Strategy) (Attachment 1 and Attachment 2) and to seek Council's endorsement of the Strategy.

1.1.2. For the City to apply the Strategy's principles to the development of a series of integrated hazard management plans for Hobart coastal and riverine catchments, subject to its endorsement.

1.2. The Strategy's focus is on supporting local government in responding to and managing increased coastal hazards across southern Tasmania, due to climate change impacts and sea level rise.

1.3. The City of Hobart is developing an integrated response to increased and coincidental coastal hazards which will align to, and implement the Strategy and its principles.

2. Report Summary

2.1. The '*Regional Strategy – Adapting to a changing coastline in Tasmania*' (the Strategy) has been prepared by the STCA in collaboration with the Greater Hobart councils: Clarence City, Glenorchy City and Kingborough.

2.2. The Strategy is explicit to local government's coastal roles and responsibilities; their management of coastal assets, infrastructure, and areas under their jurisdiction; and development and strategic planning purposes. It uses a risk management approach informed by a suite of local government specific coastal hazard management principles.

2.3. The Strategy has been prepared through the STCA's climate program led by the City of Hobart. The STCA's climate program includes Clarence, Glenorchy and Kingborough, along with the 9 southern councils.

2.4. The STCA has released the Strategy for endorsement by southern councils and for information to north and north eastern councils and government and non-government stakeholders. To date it has been favourably received with a number of councils endorsing, requests being

received for 'final' copies and the Cradle Coastal Authority requested to use it to inform their coastal response.

- 2.5. Aligned to the Strategy and its principles the City of Hobart is preparing integrated hazard management plans for its coastal and riverine catchments. The Strategy's principles provide a cohesive framework for informing the City's response and enable transparency around key stakeholder roles and responsibilities.

3. Recommendation

That:

1. ***The Council endorse the Southern Tasmanian Councils Authority's 'Regional Strategy – Adapting to a changing coastline in Tasmania' (Attachment A and Attachment B) and advise the STCA of its resolution; and***
2. ***The Council apply the Strategy's coastal management principles to the development of a series of integrated hazard management plans for Hobart's coastal and riverine catchments***

4. Background

- 4.1. The STCA recognises the challenges to local government in responding to climate change risks and the need for clear guidance and coordination on a consistent approach to support their management.
- 4.2. Increasingly communities expect local government to take a 'climate' lead in providing solutions and taking action. They are also seeking to transfer risk or share risk, associated with climate impacts/accelerated natural hazards as can be seen by recent bushfires, floods and coastal erosion events across Australia. In response the STCA's climate program has led in the development of harmonised climate responses to support councils and build their capacity to respond. This includes the development of the Strategy to manage coastal risk and support local government responses.
- 4.3. Southern Tasmania has 3263 km's, or more than 40%, of the States coastline. It covers 10 municipal areas from Glamorgan Spring Bay to Huon Valley councils, including the shoreline of the Derwent Estuary and the City's 17 km's of coastline. It is highly variable and includes numerous offshore islands, along with embayments, estuaries, open back shorelines, rocky coastlines and coastal cliffs that are exposed to climate risk and hazard.
- 4.4. The southern coastline land tenures range from private through to public with the majority being with the Crown. It has the highest concentration of settlement as well as large numbers of places of cultural heritage significance. It provides significant habitat for native

fauna and flora, including 4 RAMSAR listed wetlands, 27 wetlands listed on the Directory of Important Wetlands. There are over 120 community groups caring for its, coastal areas and cultural heritage.

- 4.5. Coastal hazards, including inundation and erosion, resulting from natural processes can cause considerable damage to communities, industries and infrastructure. These are being magnified by climate change and sea level rise, presenting significant risk to southern Tasmanian communities and economies, and have the potential increase local government exposure to litigation if they are not appropriately managed.
- 4.6. The STCA has long identified the need for the development of a coastal strategy to support councils to develop responses aligned to their corporate functions, service delivery and municipal boundaries to support thriving and resilient communities.
- 4.7. The Strategy is built around the principles of:
 - 4.7.1. Public safety
 - 4.7.2. Private property
 - 4.7.3. Local government
 - 4.7.4. Legal risk and adaptation
 - 4.7.5. Coastal management and planning and
 - 4.7.6. Coastal Values
- 4.8. The Strategy's core coastal principles facilitate transparency and consistency specific to the key roles and responsibilities of Tasmanian councils. However, importantly it is not prescriptive, enabling flexibility for councils to develop local responses particular to their circumstance and resources.
- 4.9. The City intends to apply the Strategy's principles to its coastal climate program to prepare integrated hazards management plans for its coastal and riverine catchments. The principles will inform the development of its responses to managing coastal impacts and engage with its local coastal communities to assist them in understanding local impacts and identify key responses.

5. Proposal and Implementation

- 5.1. It is proposed that:
 - 5.1.1. The draft Strategy is endorsed by the City of Hobart
 - 5.1.2. The City notify the STCA of its resolution

- 5.1.3. The City's development of a series of integrated hazard management plans for Hobart's coastal and riverine catchments is aligned to the Strategy's principles.
- 5.2. In terms of the City's coastal & riverine catchment hazard response it is intended that it undertake a series of engagements to inform and work with communities to understand local coastal and riverine values/hazards and build their resilience and preferred adaptation pathways to manage and mitigate risk.
- 5.3. This would include 10 coastal and riverine engagements: one for the overall municipality's coastline and a series of 9 for its coastal catchment compartments. Each engagement will consider local integrated, cascading, compounding and coincidental risks; and includes the development of infographics & digital stories, as well as final reports and implementation plans.
- 5.4. The coastal compartments are based on sections of coast that share contiguous characteristics: geomorphology, coastal development, processes and or other values; as follows:
 - 5.4.1. (1) New Town Bay: New Town Rivulet to Cornelian Bay Point
 - 5.4.2. (2) Cornelian Bay: Cornelian Bay Point to Pipe Clay Point
 - 5.4.3. (3) Queens Domain Coastline: Domain Shipyards to Battery Point
 - 5.4.4. (4) Macquarie Point and Sullivans Cove: Domain Shipyards to Battery Point
 - 5.4.5. (5) Battery Point Coast: Battery Point to Sand Bay Rivulet
 - 5.4.6. (6) Marievile Esplanade Sandy Bay Rivulet to Lords Beach
 - 5.4.7. (7) Nutgrove & Long Beach: Red Chapel Beach to Blinking Billy Point
 - 5.4.8. (8) Derwent Water: Lords Beach to Red Chapel Beach
 - 5.4.9. (9) Porter Hill Coastline: Blinking Billy Point to Cartwright Creek
 - 5.4.10. (10) Hobart's coastline: New Town Rivulet to Cartwright Creek.
- 5.5. To support this work the City has already undertaken a series of 'coastal' studies. On coastal vulnerability and coastal values which will provide important background information to the Council's hazard response.

6. Strategic Planning and Policy Considerations

- 6.1. The Strategy and the City's development of integrated hazards management plans for its coastal and riverine catchments furthers key Strategic Planning and Policy considerations as follows:
 - 6.1.1. '2022 ELT Strategic Directions' which includes key actions to enhance the City's approach to resilience, climate change mitigation and adaptation:
 - 6.1.1.1. Review and update the Council's Hobart Coastal Hazard Study and finalise Action Plan
 - 6.1.1.2. Continue to support the STCA and its regional climate change initiatives
 - 6.1.2. 'City of Hobart 2021- 2022 Annual Plan':
 - 6.1.2.1. 6.4 Hobart is responsive and resilient to climate change and natural disasters.
 - 6.1.3. City of Hobart Strategic Plan 2019 – 2029' that includes over 20 climate actions that focus on increasing the City's understanding of its climate exposure and vulnerability and developing resilience and adaptation responses. These action includes:
 - 6.1.4. Pillar 6 Outcomes: 6.4 Hobart is responsive and resilient to climate change and natural disasters:
 - 6.1.4.1. # 6.4.1 Adopt a holistic approach to climate change mitigation and adaptation across all pillars of the strategic plan.
 - 6.1.4.2. # 6.4.2 Lead the development and implementation of a regional response to the global climate and biodiversity emergency.
 - 6.1.4.3. # 6.4.3 Provide flexible and timely responses to climate change impacts, risks and hazards.
 - 6.1.4.4. # 6.4.6 Anticipate, map, monitor and manage coastal and land stability hazards.
 - 6.1.4.5. # 6.4.7 Map, monitor and manage flood risks and impacts.
 - 6.1.4.6. # 6.4.8 Develop and implement resilient infrastructure to deal with extreme weather events.

- 6.1.4.7. # 6.4.9 Incorporate disclosure of climate change risk into the City's planning, operations, finances and risk management.
- 6.1.4.8. # 6.4.10 Increase community resilience to the impacts of climate change through information, programs and other activities.

7. Financial Implications

7.1. Funding Source and Impact on Current Year Operating Result

- 7.1.1. There is no funding impacts on current or futures year's budget through the endorsement of the Strategy.

7.2. Impact on Future Years' Financial Result

- 7.2.1. A modest allocation is sought through the 2022 – 2023 budget for the City's coastal & riverine catchment hazard community engagement. The engagement will inform, and work with, communities to understand local coastal values, hazards and build their resilience and preferred adaptation pathways to manage and mitigate risk.
- 7.2.2. The funding would cover 10 engagements: one for the overall coastline and the 9 coastal catchment compartments. Each will consider local integrated, cascading, compounding and coincidental risk; and includes the development of infographics & digital materials.

7.3. Asset Related Implications

- 7.3.1. The City's coastal & riverine catchment hazard project would enable the identification of assets and their exposure and vulnerability to integrated coastal and riverine hazards. This would be the identified through final reporting and inform asset management planning processes.

8. Legal, Risk and Legislative Considerations

- 8.1. The Strategy includes a principle on Legal Risk and Adaptation. It identifies that *'as coastal decision-makers, councils need to identify functions and decisions that may give rise to legal risk around climate change adaptation and identify how to manage these legal risks.'* *'They can be managed by early decision-making and action, based on the best science and consideration of all issues, including both legal and factual/scientific uncertainty.'*

8.2. It considers that Legal risk can arise from:

- 8.2.1. *Strategic planning*

- 8.2.2. *Public release of hazard risk information*
- 8.2.3. *Failure to release hazard risk information*
- 8.2.4. *Approval of new development*
- 8.2.5. *Construction and maintenance of protective infrastructure*
- 8.2.6. *Approval of private coastal protection works.*
- 8.3. It further identifies that *there is also a risk of public-law litigation brought by citizens challenging the decisions of government officials. These may involve appeals to planning tribunals that consider the merits of the original decision and either uphold, vary or replace the decision.*
- 8.4. *Where merit appeals are not available, limited rights of judicial review may enable citizens to challenge the decision-making process or the criteria applied. If successful, these challenges result in the decision being sent back to the original decision-maker for re-determination; they do not directly result in a decision being overturned or changed.*
- 8.5. The Strategy is designed, if applied, to mitigate the exposure of local government to potential liability for its 'coastal' decision making. It clearly articulates role and responsibilities for local government and suggests a transparent and risk based approach.
- 8.6. The Strategy also aligns to the Tasmanian Governments approach which has adopted the following principles to:
 - 8.6.1. communicate its approach and responsibilities to the management of coastal land; and
 - 8.6.2. underpin its land-use planning and decision-making in coastal risk areas:

9. Environmental Considerations

- 9.1. This report explicitly addresses climate impacts and hazards associated within the City's coastal and riverine compartments.

10. Marketing and Media

- 10.1. Marketing and media considerations associated with the City's development of an integrated responses to climate impacts associated within the City's coastal and riverine compartments, will be developed in line with the program.

11. Community and Stakeholder Engagement

11.1. The Strategy has been prepared with input from key stakeholders including:

11.1.1. Tasmanian government:

11.1.1.1. Renewables Climate Future Industries Tasmanian
(former Tasmanian Climate Change Office)

11.1.1.2. Natural Resource and Environment Tasmania,
Crown Land Services

11.1.1.3. Office of Security and Emergency Management

11.1.1.4. Tasmanian Planning Unit

11.1.2. Port Arthur Heritage Site Management Authority

11.1.3. Southern Tasmanian councils including the Cities of Hobart,
Glenorchy and Clarence and the Kingborough Councils

11.1.4. Local Government Association of Tasmania

11.2. The STCA has released it for consultation and information as follows:

11.2.1. Southern Tasmanian councils for (i) comment and (ii)
endorsement

11.2.2. North-west and north-east councils for information

11.2.3. Stakeholders, private, non-Government organisations for
information

11.3. It has been favourably received and, to date, been endorsed by a number of southern councils. Most recently the Cradle Coast Authority, has written to the STCA applauding it for the development of the Strategy and seeking to use it for their engagement with key stakeholders on the issue.

12. Delegation

12.1. This matter is delegated to the Council.

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.




Katrina Graham
SENIOR CLIMATE CHANGE OFFICER



Neil Noye
DIRECTOR CITY LIFE

Date: 3 May 2022
File Reference: F22/36341

Attachment A: Regional Strategy - Adapting to a Changing Coastline in Tasmania 2022 ↓ 

Attachment B: Regional Strategy - Adapting to a Changing Coastline in Tasmania 2022 Snapshot ↓ 



REGIONAL STRATEGY

ADAPTING TO A CHANGING
COASTLINE IN TASMANIA



DRAFT

STCA CHAIR FOREWORD

De esto magni omnibus. Gataspi caborum
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utem vent as eritatur?

Em as asit lat qui commoles nate andamet
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officimus dolorum vent proribus aut quat
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nihilles minciuntem. Ant et mosse verit
eumquat ad quide voloritaturque laut aspis

ACKNOWLEDGMENT

In recognition of the deep history and culture of the southern Tasmanian councils, we acknowledge the Tasmanian Aboriginal people as the Traditional Custodians of this land. We acknowledge the determination and resilience of the Palawa people of Tasmania who have survived invasion and dispossession and continue to maintain their identity, culture and rights.

We recognise that we have much to learn from Aboriginal people today, who represent the world's oldest continuing culture. We pay our sincere respects to Elders past and present and to all Aboriginal people living in and around southern Tasmania.

CONTENTS

STCA CHAIR FOREWORD	2
ACKNOWLEDGMENT OF COUNTRY	2
INTRODUCTION	5
1.0 INTRODUCTION	5
1.1 COASTAL PROCESSES	7
1.2 RISK MANAGEMENT	8
TASMANIAN CASE STUDY	8
<i>Example of assessing risk</i>	
2.0 PRINCIPLES	11
2.1 PUBLIC SAFETY	13
2.2 PRIVATE PROPERTY	14
2.3 LOCAL GOVERNMENT	15
2.4 LEGAL RISK AND ADAPTATION	16
PRACTICAL SCENARIO	18
<i>Provision of protective seawall</i>	
COASTAL HAZARDS MANAGEMENT 1960'S STYLE	19
2.5 COASTAL MANAGEMENT AND PLANNING	20
CASE STUDY	21
<i>Supporting council climate cooperation and collaboration</i>	
2.6 COASTAL VALUES	23
3.0 RISK MANAGEMENT PROCESS	25
3.1 IDENTIFY CHALLENGES	27
3.2 DETERMINE VULNERABILITY AND RISK	29
3.3 IDENTIFY OPTIONS	30
3.4 EVALUATE OPTIONS AND MAKE A PLAN	30
3.5 TAKE ACTION	31
3.6 MONITOR AND EVALUATE	31
4.0 ENABLERS AND DECISION MAKING	33
4.1 ENABLERS	33
4.2 DECISION MAKING PATHWAYS	33
5.0 SUPPORTING RESOURCES	35
6.0 ACKNOWLEDGEMENTS	36
7.0 REFERENCES USED BY BMT AND ISI	37



1.0 INTRODUCTION

This 'Regional Strategy for Adapting to a Changing Coastline in Tasmania' will help councils to employ a strategic approach to existing or potential hazards on the coastline that threaten harm to public and natural assets, infrastructure, people or property.

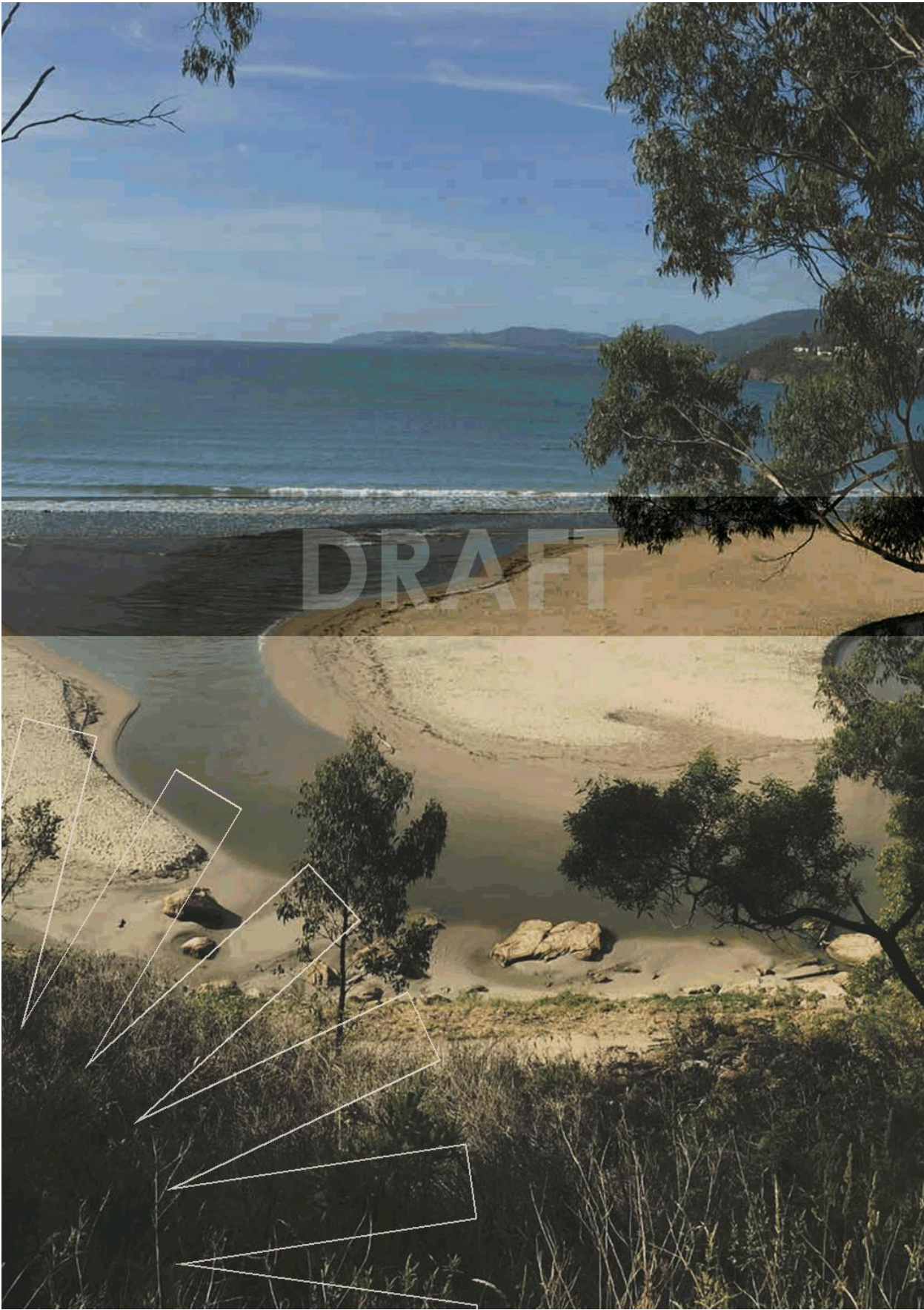


Coastal hazards such as inundation and erosion occur as the result of natural coastal processes. These hazards are magnified by a warming climate and rising sea levels.

This Strategy was developed by a collaboration between specialist coastal consultants¹ and the Southern Tasmanian Councils Authority's (STCA) climate program, with members representing ten coastal councils in southern Tasmania. The Tasmanian Government (Renewables, Climate and Future Industries Tasmania and Natural Resources and Environment Tasmania), the Local Government Association of Tasmania and the Port Arthur Historic Site Management Authority were consulted in development and provided support and feedback input for consideration by the committee.

A central feature of the Strategy is that a risk management approach is recommended for helping communities adapt to a changing coastline, as this is best practice in coastal management. To apply the risk management framework to development and use in the coastal zone on public and private land and to guide the decisions that are made, coastal issues should be viewed through the lens of a suite of Principles that are outlined in this Strategy document. These Principles are also a central feature of the Strategy.

¹ Impact Solutions International
and BMT Commercial Australia P/L



1.1 COASTAL PROCESSES

The diagram below presents a hypothetical coastline indicating most common features subject to coastal hazards. More urbanised Councils share many of these coastal features which are numbered and described in the key below.

Figure 1. Represented here are some of the coastal issues that may arise.

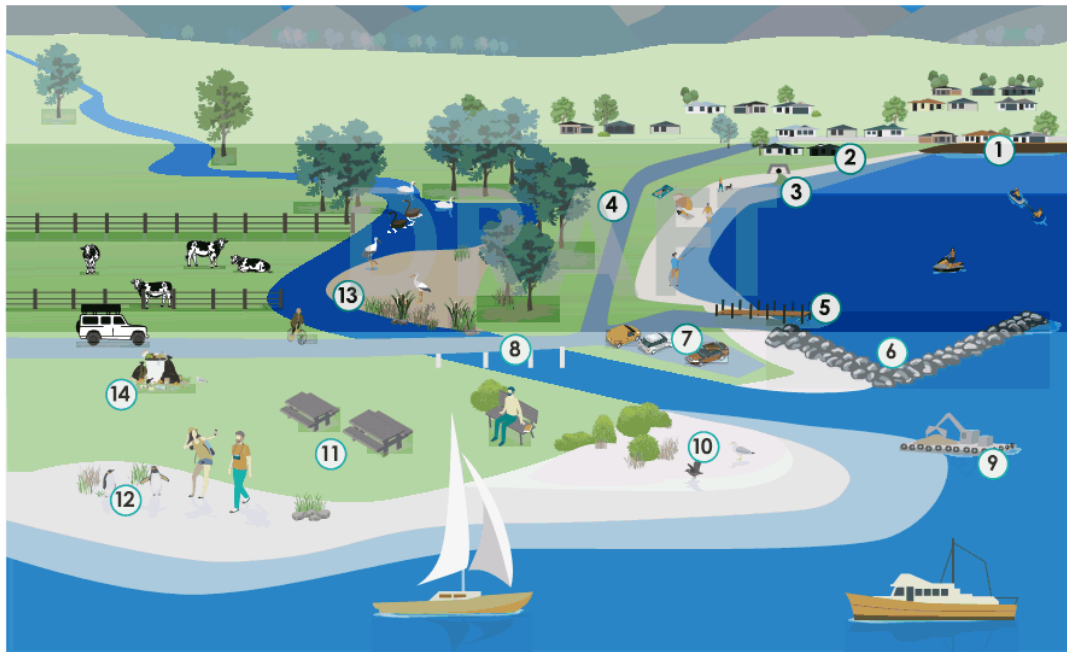


Table 1 Key to Figure 1

1. Dwellings and other structures on erodible cliffs	8. Bridges and river/estuary crossings
2. Dwellings and other structures vulnerable to beach erosion and inundation	9. Managing sand movement and accumulation with dredging
3. Stormwater outlets and other linear infrastructure	10. Erodible soft sediment spits and habitat
4. Roads vulnerable to inundation, erosion and saltwater intrusion	11. Public facilities such as picnic grounds and BBQs
5. Jetties and other public and private structures. Historical features and infrastructure	12. Foreshore and dune flora and fauna
6. Engineered hard structures including breakwaters and sea walls	13. River sediments and wetlands vulnerable to erosion
7. Public infrastructure including boat ramps, car parks	14. Litter and other illegal waste dumping

1.2 RISK MANAGEMENT

Risk management essentially involves a repeating cycle of identifying risks, hazards and vulnerabilities, identifying then evaluating solutions, preparing action plans, implementing actions and monitoring results. Outcomes from the risk management process described in this Strategy can be incorporated into existing council corporate risk registers.

Tasmanian Case Study

Example of assessing risk

Raspins Beach forms the western shore of Prosser Bay immediately north of Orford on Tasmania's east coast. The entire beach is a low-lying sandy shore. Due to erosion in the last 3 to 4 decades, the beach has receded over 100 metres, and at the northern end is now within 50m of the Tasman Highway. A camping ground and much open coastal forest has been lost.

The highway floods when king tides, low pressure systems and strong easterly swells, winds and rain combine. This essential transport corridor will continue to increase in vulnerability through 21st century and beyond. Agencies with an interest in this area include Department of State Growth, NRET, Parks and Wildlife Service and Glamorgan Spring Bay Council.

HIGHWAY ...



High band (Red)	area vulnerable to sea-level rise by 2050 from the mean high tide, rounded up to the nearest 100 mm.
Medium band (Orange)	area vulnerable to a 1% AEP storm event in 2050 rounded up to the nearest 100mm plus 300 mm added for freeboard.
Low band (Yellow)	area vulnerable to a 1% AEP storm event in 2100 rounded up to the nearest 100mm plus 300 mm added for freeboard.
AEP = Annual Exceedence Probability	

A risk management approach to helping communities adapt to changes such as a warming climate and sea level rise on the coastline and more generally, in the coastal zone, requires understanding the vulnerability of an area, asset or activity, and identifying where any actions should be focused. The assessment of risk can lead to identifying and implementing measures that help communities adapt to changes.

A risk assessment leads to many optional solutions for responding to risks. With stakeholder and community involvement, these options are assessed and can be used to develop local coastal hazard plans. These local coastal hazard plans outline what actions will be implemented – for example, re-vegetating dunes, replacing sand on beaches, or more cost intensive hard engineering solutions such as sea walls and groynes. Retreat and relocation are also options. Note that in some cases a deliberate action will be to take no action.

Various tools and approaches are used to help select options appropriate for the risk tolerance of Council, stakeholders and community. The 'CoastAdapt'² on line decision making tool is the recommended strategy to use at this point. This comprehensive tool was commissioned by the Australian government to support coastal managers to adapt to rising sea levels

and a warming climate. Cost benefit analysis and multi criteria analysis are another two examples of tools that can be used to help make necessary decisions. Other decision making tools have been identified and are described in more detail in CoastAdapt.

The output from these approaches is used to develop coastal hazard plans. The plans should identify clear objectives, actions, responsibilities and performance indicators.

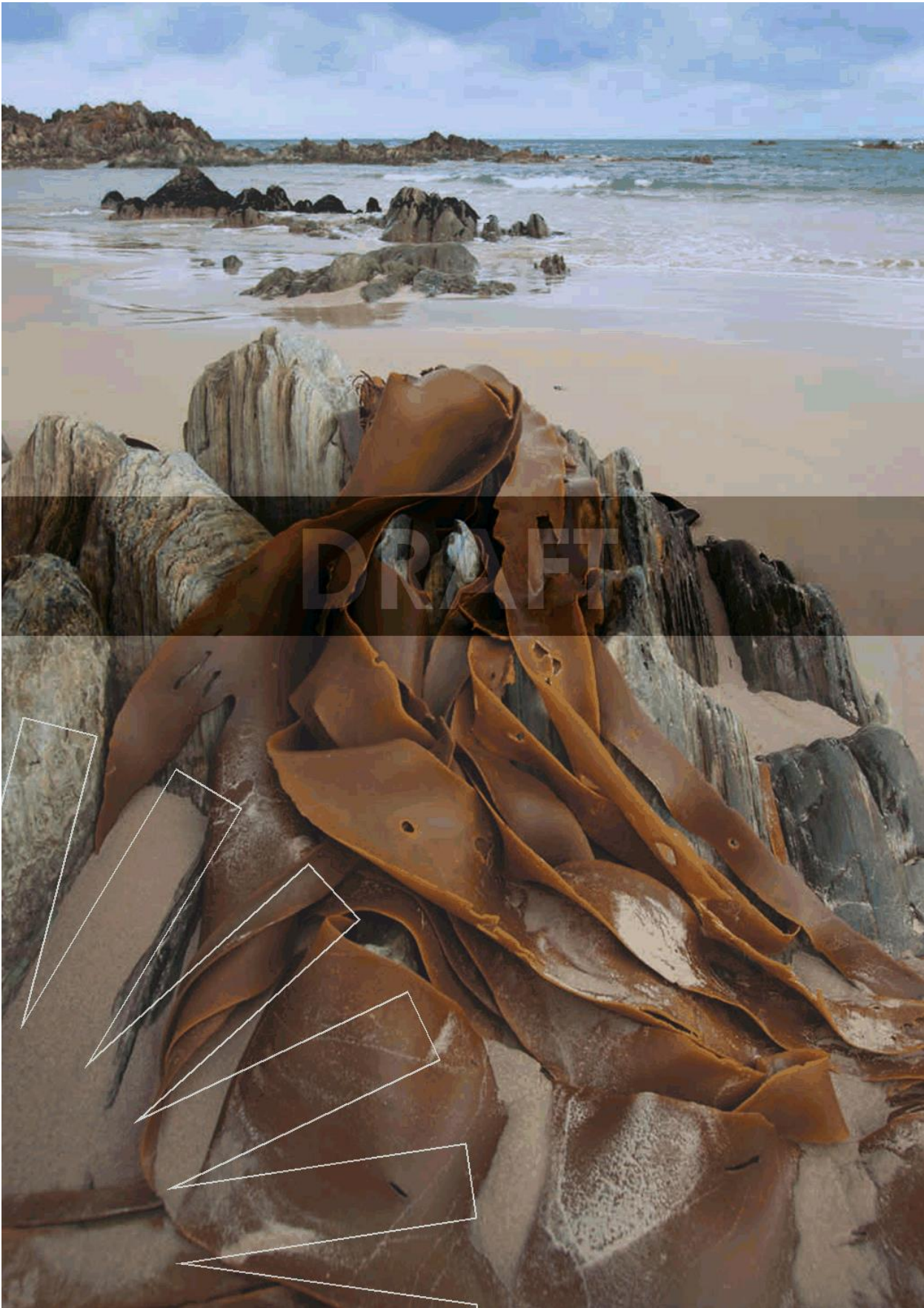
In the "Take Action" phase of the risk management cycle, a critical aspect is attracting funding or finance for actions.

The "Monitor and evaluate" phase helps identify actions that have achieved the desired objectives, and those that have not. This allows new information to be applied and planning processes to be revised if needed.

Each step of the process needs governance, resources, and engagement that supports the process within and outside the organisation. The risk management cycle can be followed again and again as many times as needed to continually improve plans if desired.

A detailed description of how to apply risk management to help communities adapt to changes on the coastline is provided in Section 3.

² National Climate Change Adaptation Research Facility (NCCARF) CoastAdapt online decision support framework www.coastadapt.com.au
There are 10 information manuals covering the scientific and technical information of the many disciplines involved in coastal adaptation.



2.0 PRINCIPLES

The ten coastal councils in southern Tasmania and specialist coastal consultants developed the Principles collaboratively.

The Principles reflect the knowledge and expertise of local government policy makers and resource managers with direct experience in developing or applying knowledge about adapting to climate change impacts on the coastline".

The committee and working group (the Regional Climate Change Initiative group of the Southern Tasmanian Councils Authority) was informed by the principles employed by the State Government which manages Crown land in the coastal zone.

Section 2 outlines each Principle, gives an example of how each Principle might be applied, and provides some context for why each is relevant to managing a changing coastline.

Tasmanian Government Principles

The Department of Natural Resources and Environment Tasmania Principles for Managing Coastal Hazards (<https://nre.tas.gov.au/about-the-department/managing-coastal-hazards>) which currently include the following principles, among others:

- The Crown does not have, nor does it accept, specific future obligations to repair or reduce the impacts of natural coastal hazards on private property; and
- The Crown does not accept obligations to repair or reduce the impacts of natural coastal hazards on any non-Government owned or managed assets sited on public land.





2.1 PUBLIC SAFETY

1 (a) Human safety is paramount. Areas of unacceptable risk should be identified and exposure to risk minimised or if risk is unavoidable, identify retreat pathways.

1 (b) The community will be provided with up-to-date climate change and coastal hazards information to inform decision making and to provide opportunities to participate in response planning.

To apply Principles 1(a) and 1(b) in making practical decisions, councils can ask and answer the following questions:

Is this area identified as medium to high hazard on the LISTmap, the Tasmanian Government's Land Information System Tasmania website?

Are there facilities or structures in this area which are used by the public? Are they safe and maintained?

What actions can be taken to alert the public to risk, reduce risk, and if necessary discourage, limit or prohibit access?

Are coastal risks and hazards across the municipality identified, quantified, mapped and monitored?

Have private property owners been informed of the hazards identified for their land and assets?

If the answers are yes, then the Principle has been applied. If the answer is No, then more work should be done in this area.



Context for this Principle

Ensuring the safety of people must be the primary concern in managing coastal hazards. Coastal land managers must warn and protect people from a range of hazards, including dangerous water conditions (large swells and rough seas, currents and undertows, pollution), unstable landforms (such as cliff edges), and inundation. Community education through signs, excluding access, and siting and design of access and public and private infrastructure must all be considered on a case-by-case basis.

2.2 PRIVATE PROPERTY



- 2.2 (a) Private property owners occupy coastal areas at their own risk.
- 2.2 (b) Property owners are responsible for managing risks to their property from coastal hazards in accordance with relevant policies and regulations and based on expert coastal advice.
- 2.2 (c) Where private coastal protection works are undertaken by property owners, beneficiaries should pay.

To apply Principles 2.2 (a), 2.2 (b) and 2.2 (c) as part of making practical decisions about managing coastal risks, councils can ask and answer the following questions:

Have property owners been informed of their responsibility for the risk they face?

Have property owners been informed that Council and the Crown accept no obligation to fund the repair of damage from erosion, inundation or extreme climate events?

Have private landowners been made aware of Council's policies applying to their property and any relevant knowledge supporting these policies?

If the answers are yes, then the Principle has been applied. If the answer is No, then more work should be done in this area.

Context for this Principle

Coastal hazards are a financial issue to manage. For example, insurance is an increasingly large budget item, and climate risks are increasing the costs of insurance. Addressing coastal hazards on public land adds a resourcing demand to the organisation.

Private sector financing will be required to address the challenges of climate change.

2.3 LOCAL GOVERNMENT

- 2.3 (a) Councils should actively monitor coastal risks and hazards within their municipal areas.
- 2.3 (b) Councils are responsible for the management and cost of coastal hazard impacts on their own assets and services.
- 2.3 (c) Councils are not responsible for the cost of coastal hazard impacts on private property, or on private assets located on public land.
- 2.3 (d) Access to public coastal land will not be available to private property owners for coastal protection works, except where significant public benefit is demonstrated.

To apply Principles 2.3 (a), 2.3 (b), 2.3 (c) and 2.3(d) when making practical decisions, councils can ask and answer the following questions:

Have property owners been informed that access to public coastal land will not be available to private property owners for coastal protection works, except where significant public benefit is demonstrated?

Does council have an inventory of their coastal hazards and risks?

Does council have an estimate of potential costs from the impact of hazards on council owned assets and services?

If the answers are yes, then the Principle has been applied. If the answer is No, then more work should be done in this area.

Context for this Principle

Where coastlines are already experiencing erosion, pressure from landowners and the community to create hard engineered defences can be intense. Experience from around the world suggests that the high costs of such action are rarely justified, and the defences are rarely successful in the long term. Also, these structures can result in the loss of beaches which impacts the values of the broader community. Protecting high value urban, city and commercially important foreshores can be an exception. Ecosystem based responses involve the management of ecosystems and their services to reduce the vulnerability of human communities to the impacts of climate change. These responses provide lower cost natural solutions by increasing coastal resilience to erosion, and they can buy time to look at the range of longer-term options including retreat. Soft solutions are those which do not cause any long-lasting effects and which can easily be removed or changed. Sand bagging provides a temporary solution and can be removed or shifted. These soft engineered solutions should be considered first.

The challenge of hard engineered solutions.

Stakeholders who live or work on the coastal fringes such as beachfronts or estuarine foreshores often put pressure on Councils or the Crown to take hard engineered structural approaches such as sea walls or levees to address erosion and inundation problems. Apart from these being costly to design and construct, many hard-engineered solutions result in consequences to other stakeholders. Examples include transferring the risk to other places or stakeholders which can occur if one home or a small area is protected, or the loss of beach which often follows construction of a sea wall. Importing sand by pumping or trucking, for example, creates very high ongoing maintenance costs. It is important to understand these unintended consequences before progressing to such options.

2.4 LEGAL RISK AND ADAPTATION



2.4 (a) Coastal legal risks can be identified, managed and reduced but can't be avoided.

2.4 (b) Well developed policy and action now will minimise the risk of legal challenges and liability in the future.

To apply Principles 2.4 (a) and 2.4 (b) as part of making practical decisions about managing coastal risks, councils can ask and answer the following questions:

Does council have a coastal asset register?

Does council understand the legal risks presented by sea level rise?

Does council have policies and plans in place that address coastal legal risks?

If the answers are yes, then the Principle has been applied. If the answer is No, then more work should be done in this area.

Context for this Principle

As coastal decision-makers, councils and other planning authorities need to identify functions and decisions that may give rise to legal risk around climate change adaptation and identify how to manage these legal risks. Information Manual 6 prepared by CoastAdapt specifically introduces the coastal legal risk issues and this is the recommended resource for assessing legal risk. This Section provides a very brief summary of the Manual.

Risk of legal challenge – decisions with climate change implications.

They can be managed by early decision-making and action, based on the best science and consideration of all issues, including both legal and factual/scientific uncertainty.

Legal risk can arise from:

- Strategic planning
- Public release of hazard risk information
- Failure to release hazard risk information
- Approval of new development
- Construction and maintenance of protective infrastructure
- Approval of private coastal protection works.

There is a risk of public-law litigation brought by citizens challenging the decisions of government officials. These may involve appeals to planning tribunals that consider the merits of the original decision and either uphold, vary or replace the decision. Where merit appeals are not available, limited rights of judicial review may enable

citizens to challenge the decision-making process or the criteria applied. If successful, these challenges result in the decision being sent back to the original decision-maker for re-determination; they do not directly result in a decision being overturned or changed.

The other risk of litigation is based on private law rights, under the law of negligence and nuisance. Such actions may be brought where a decision or careless action or inaction results in loss. These cases are brought by the individuals who have suffered loss such as property damage and/or reduced property values. This loss (and the right to sue) sometimes occurs many years – even decades – after the decision. If the council or other authority is found to owe them a duty of care, and to have breached that duty, the outcome of such actions is a liability to compensate for the loss caused.

To minimise climate legal risk, international and Australian experience suggests that early and proactive decision-making based on the best available science is important. The CoastAdapt Information Manual 6 'Legal Risk and Adaptation' is a very useful resource as it describes seven probable scenarios and the factors that a council must consider for managing risk in each situation.

The CoastAdapt coastal climate risk management tool is recommended to support coastal managers with making decisions about adapting to coastal hazards. An example from CoastAdapt is provided over page to show how the tool can be used to support practitioners to make decisions in this space.

Practical Scenario⁴

Provision of protective seawall

What is the action/decision to be made? A group of coastal landholders has asked Council to construct a seawall to protect their properties.

Background

The state government has released hazard mapping indicating areas likely to be inundated under sea-level rise. A group of concerned citizens has approached Council, as their properties are zoned as having a sea-level rise risk. They have requested that Council construct a seawall to protect their properties.

What is the decision-maker's power/authority?

The Fictional Government Act provides that the local government has the power to do anything that is necessary or convenient for the good rule and local government of its local government area.

Is there factual certainty?

Council seeks advice from its in-house engineer, who confirms that the properties are likely to be affected by sea-level rise. The engineer also advises Council that a seawall may protect those properties but may also exacerbate erosion for properties located further along the coast. It is also in an area of high wave energy, and the seawall will therefore need expensive ongoing maintenance.

Is there legal certainty?

Council seeks advice from its in-house lawyer, who advises that Council does not have an obligation to construct a seawall. However, it will have an ongoing obligation to maintain a seawall if one is constructed. The lawyer also advises that Council may be the subject of legal proceedings in negligence or nuisance from the neighbouring landholders if erosion is exacerbated and damages their properties.

Outcome

Council declines to construct a seawall. Council advises the property owners that they may apply for development approval to construct the seawall at their own cost, but that they will need to prove that the seawall will not impact on neighbouring landholders. The property owners will also be subject to ongoing management obligations. Council also decides to adopt a strategy for future seawall development and include it in its plan.

CoastAdapt Information Manual 6 'Legal Risk and Adaptation' describes 6 other probable scenarios:

Scenario 1: Should a council undertaking strategic planning review include newly released state hazard information?

Scenario 2: Assessing a development application for a large mixed-use coastal development

Scenario 3: Assessing a development application for 100 residential lots

Scenario 4: Council provision of infrastructure – upgrade of stormwater

Scenario 5: Provision of infrastructure – stormwater upgrade and community concerns

Scenario 7: Development approval for protective infrastructure (community-built seawall).

Councils can use these Scenarios and the Coast Adapt Information Manual 6 as an entry into the necessary process of understanding their coastal legal risk.

See: https://coastadapt.com.au/sites/default/files/information-manual/IM06_Legal_Risk.pdf

⁴ Bell-James, K., Baker-Jones, M., Barton E. 2017: Legal risk. A guide to legal decision making in the face of climate change for coastal decision makers. CoastAdapt Information Manual 6, 2nd edn, National Climate Change Adaptation Research Facility.



Graham Howard at Kingston Beach (2012)

Coastal Hazards Management 1960's Style

In November 1960 Graham Howard was only 24 years old and the recently appointed engineer for Kingborough Council.

A massive storm hit the area in November and Graham estimates that in excess of 30,000 cubic metres of sand was lost from Kingston Beach in one event. Huge waves had washed across the road and into the properties on the other side. When he stood on the shore afterwards there was a vertical drop of over 3 metres where the beach used to be. It was obvious that urgent action was required.

Graham knew nothing about seawalls so undertook research at the library and had multiple phone calls to contacts in Europe.

A loan of 16,000 pounds was negotiated with Treasury with the asset life of a seawall estimated to be 25 years.

- The wall is 800 metres long and runs the whole length of the beach;
- It is 2.8 metres high along its entire length with the top 1.8 metres being a curved construction;
- It is only 15 centimetres thick and made of high density concrete with steel reinforcing;
- It took 4 months to complete.

The wall continues to protect Kingston Beach and facilitate minimisation of localised erosion. It is an example of innovative coastal hazard management for its time.

2.5 COASTAL MANAGEMENT AND PLANNING

2.5 (a) Coastal hazard planning is enhanced where there is consultation between relevant levels of government, the private sector, community and other key stakeholders.

2.5 (b) Planning provisions (such as Local Provisions Schedule; Specific Area Plans) can be used so coastal hazard management plans are prepared in coastal areas identified as risky or hazardous.

2.5 (c) A pathway of adaptive responses to coastal hazards, informed by up-to date climate science should be adopted and intervention by humans in natural processes should be minimised.

2.5 (d) Public access and community services should be maintained wherever possible where consistent with the Principles of public safety, risk and financial considerations.

2.5 (e) When Councils make decisions based on the projected asset life of infrastructure, they must factor the risks of coastal sea level rise hazards in the calculation of asset life.

2.5 (f) Intensification of use or development should be avoided in public coastal hazard areas managed by Councils unless significant public benefit is demonstrated.

To apply Principles 2.5 (a) to 2.5 (f) as part of making practical decisions about managing coastal risks, councils can ask and answer the following questions:

Has Council identified coastal areas already subject to hazard and areas likely to become hazardous under climate change projections over this century?

Are provisions available so council planners can assess development applications that relate to intensification of use or development in coastal hazard areas in a manner consistent with the Principle?

Are Council planners and managers aware of these areas to include relevant issues in their decision making?

Is there a process in place to add to this knowledge as new data emerges?

Have vulnerable Council infrastructure assets been identified (present and future), and has a strategy to replace protect, or relocate them been developed based on the life of the asset, risk scenarios both present and anticipated, and identification of alternative locations?

Have local hazard management plans been developed for identified coastal areas?

Were relevant stakeholders consulted as part of developing these management plans?

Have owners of land in hazardous coastal areas been informed of Council policies towards rebuilding, extending, modernising, existing dwellings or building new dwellings?

Have real estate agents, property valuers, financial institutions and insurance companies been made aware of these policies, and is there a mechanism to inform prospective buyers?

If the answers are yes, then the Principle has been applied. If the answer is No, then more work should be done in this area.



Sandbags now protect 65 metres of coastline at Coningham Beach in Kingborough Council.

Context for this Principle

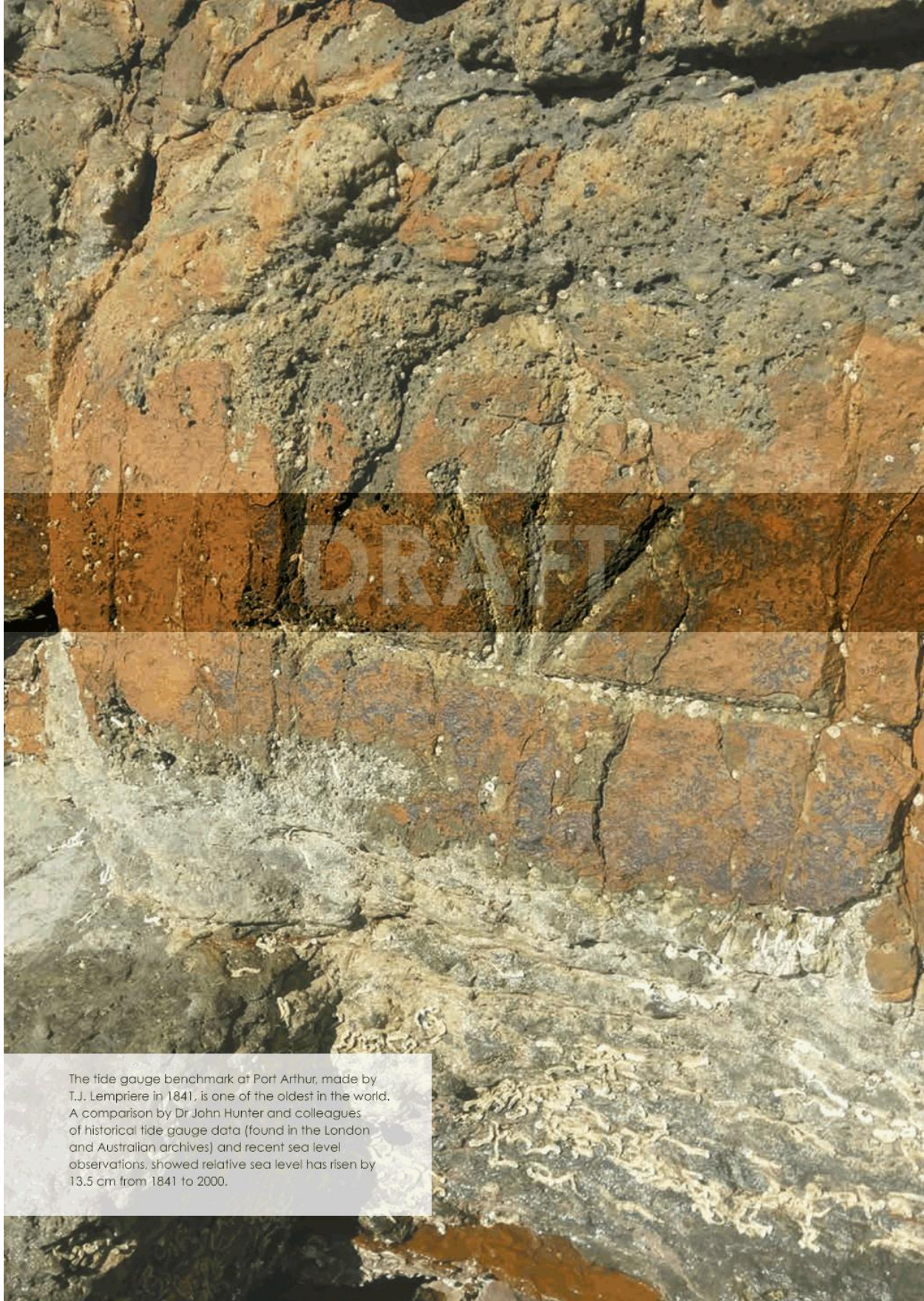
Good coastal zone planning can help avoid new development in hazardous areas and identify where development or infrastructure is appropriate. Innovative design and construction methods can be used to help accommodate changed conditions.

Case Study

Supporting council climate cooperation and collaboration

The Southern Tasmanian Councils Authority's climate program is delivered through its Regional Climate Change Initiative. Established in 2010, RCCI supports and coordinates climate change action across the following areas:

- **council leadership:** as the tier of government closest to communities, lead and share knowledge across Tasmanian councils and communities to: build capacity, avoid duplication and advocate to State and Federal Government, research sector and peak organisations
- **corporate (council) emissions reduction:** reducing energy use and emissions which councils are responsible across their buildings, fleet and services
- **community emissions reduction:** supporting programs to influence households, businesses and community groups to reduce emissions and energy use and realise cost savings
- **corporate (council) adaptation:** increase the capacity of councils to protect and future proof their assets and services against intensified natural hazards; reduce exposure to potential liability in decisions making; and minimise financial risks from the transition to a low carbon economy and increased natural disasters.
- **community (municipal / regional) adaptation:** assist and facilitate community building resilience and adaptive capacity by providing information on local climate change risks to enable informed decision making and risk assessment



The tide gauge benchmark at Port Arthur, made by T.J. Lempriere in 1841, is one of the oldest in the world. A comparison by Dr John Hunter and colleagues of historical tide gauge data (found in the London and Australian archives) and recent sea level observations, showed relative sea level has risen by 13.5 cm from 1841 to 2000.

2.6 COASTAL VALUES

- 2.6 (a) The importance of ecological, cultural, and Aboriginal heritage values will be recognised for their contribution to human well-being when responding to coastal hazards.
- 2.6 (b) Coastal hazards management will consider the impact of any action or inaction on known ecological and heritage values.
- 2.6 (c) Understanding climate change impacts to Aboriginal sites and landscapes will require input from Aboriginal Heritage Tasmania to gain access to data that may not be publicly available and to ensure a culturally appropriate response.

To apply Principles 2.6 (a), 2.6 (b), and 2.6 (c) as part of making practical decisions about managing coastal risks, councils can ask and answer the following questions:

Have significant values been identified using all state government, online, and local resources?

Has advice been sought from appropriate agencies and communities?

If the answers are yes, then the Principle has been applied. If the answer is No, then more work should be done in this area.

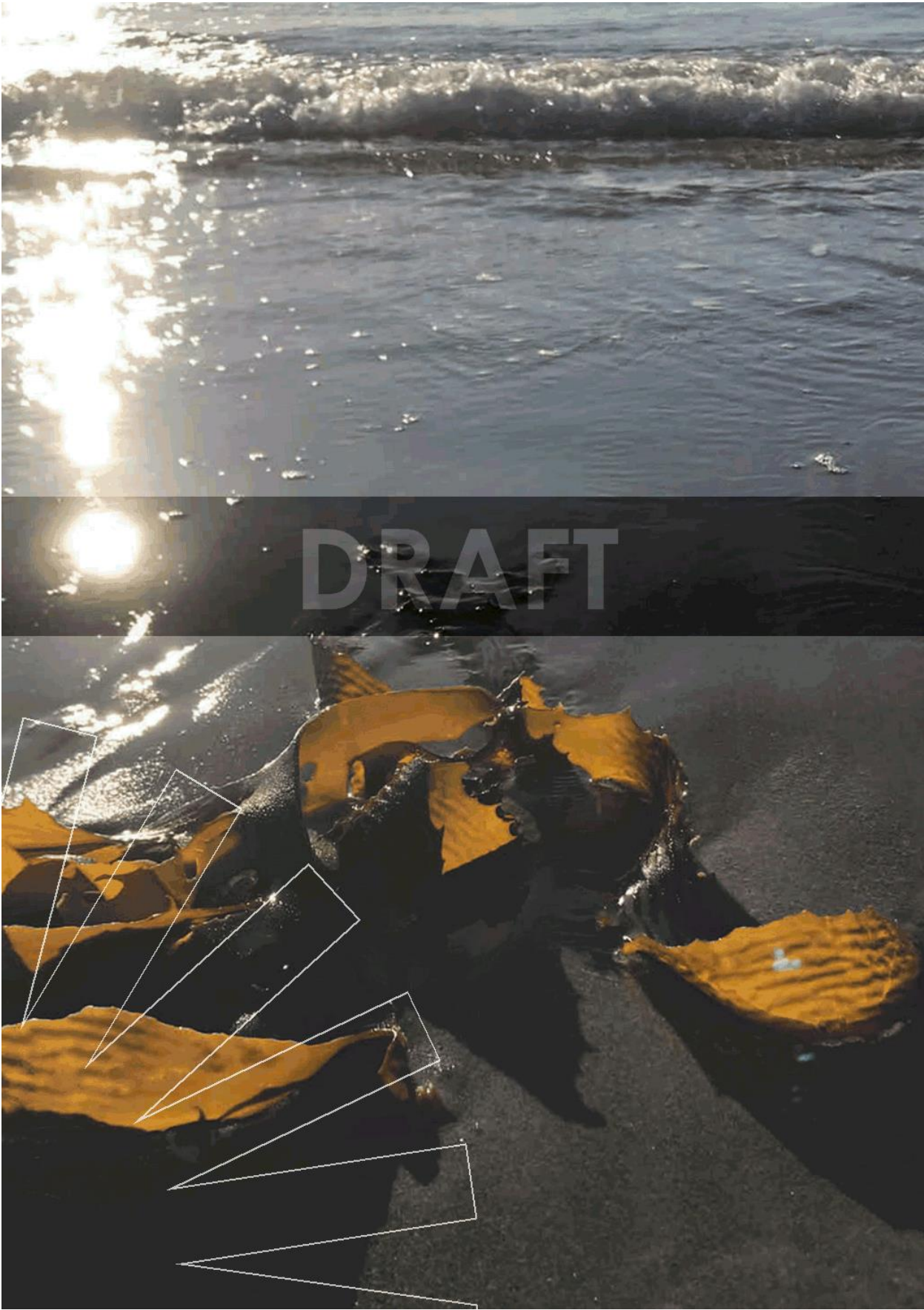
Context for this Principle

People of Tasmania cherish coastal environments for complex and dynamic values. They are home to rare, and threatened ecosystems; tangible and evocative connections to the indigenous use and custodianship of the land and sea; and the later layers of historic heritage places.

While some coastal ecosystems can naturally retreat if suitable land is available, the majority of these values are uniquely tied to coastal place and many climate change adaptation approaches cannot be applied to them: they cannot be replaced, they cannot be moved, and they cannot be elevated. This creates particular challenges for management, because the range of options is limited and likely to require location specific responses rather than generalised solutions.

Expertise in managing these values, and the places and landscapes to which they are attached, exists across Tasmania. Advice can be sought from a range of Government Agencies, including Department of Natural Resources and Environment (which includes Aboriginal Heritage Tasmania and Heritage Tasmania), and Government Business Enterprises such as the Port Arthur Historic Site Management Authority.

Information on the heritage and ecological values of an area is mapped on various overlays available on LISTmap.



3.0 RISK MANAGEMENT PROCESS

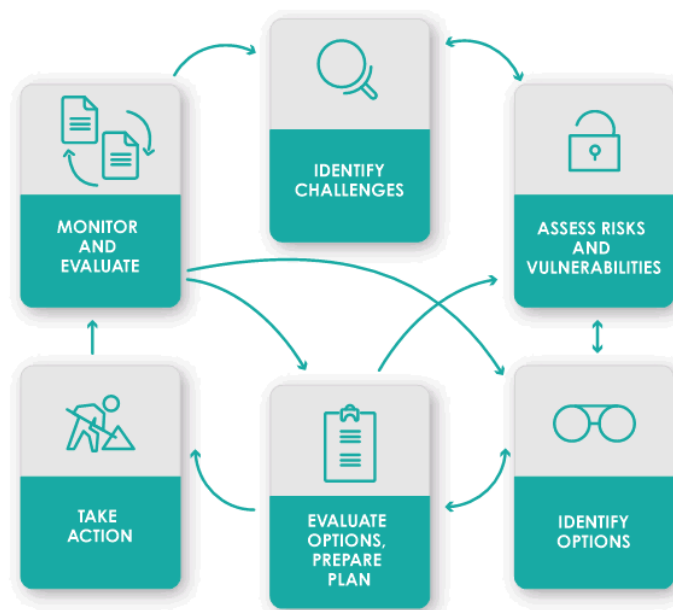
Current global leading practice in climate change adaptation recommends taking a risk-based approach. Understanding and managing risk are key elements incorporated in the principles for adaptation. Risk based approaches to adaptation include understanding the vulnerability of an area, asset or activity, and delves further into understanding why this vulnerability matters and where any actions should be focussed.

The following six-step iterative framework can be followed to help understand and manage coastal hazards. An important consideration of adaptation is that the framework can be completed in ways that meet the purpose of each Council. Those that are starting their adaptation journey are able to scan

through the process fairly rapidly, getting an idea of the scale of the challenge, and the level of detail and investment that is required to address the issue they face. Others may have a better overview of the issue in their area or have greater exposure and may wish to follow the process in more detail from the outset. Additional support for undertaking risk management can be found in the online tool 'CoastAdapt' – the section titled Risk Assessment.

The risk management framework recommended in this Strategy is one that is consistent with the international risk standard. It is presented in the figure below:

Figure 2. Risk management framework, consistent with ISO31000 (Palutikof, Rissik, Webb et al. 2019)



5 CoastAdapt National Climate Change Adaptation
Research Facility www.coastadapt.com.au



3.1 IDENTIFY CHALLENGES

This is a critical step in adaptation planning. By getting the framing right for the challenge, Councils will set themselves up for success in the short and medium term. The iterative nature of the risk framework enables this to be revisited regularly in response to new knowledge, as coastal risks change, or as the needs of stakeholders change.

This initial step is focused on developing a clear understanding of what Council is trying to achieve, and clearly defining the area and scale that will be the focus of adaptation.

Once defining the scale and extent of the plan, it is important to establish a vision and goals for the adaptation challenge. At this early stage this supports engagement with stakeholders and demonstrates leadership from Council.

A component of this first stage is to get appreciation of the magnitude of the challenge being faced. This can be done by undertaking a first pass risk screening. Risk screening can be done following a desk-top approach and does not require a detailed understanding of climate effects on the area. Use expert opinion, simple maps, figures and climate change projections to determine whether you have a climate challenge to address (a first-pass risk screening). Guidance can be obtained from the links to on-line resources provided in Section 5 of this Strategy.

By understanding where risks may be present and what may be affected, you are able to fine tune and prioritise the approach for the next steps in the risk assessment. This includes understanding what detailed information may be required and also which internal and external stakeholders should be involved.

Establishing governance and organisation structures that ensure Council is able to achieve its goals is also important at this early stage. Resourcing the process properly and ensuring that the process is supported throughout the organisation means that staff involved will be empowered to act appropriately and effectively across the organisation and that relevant sections are involved.

Undertake internal and external engagement to ensure that you have strong support within and outside of your organisations. It is important to set up processes that enable engagement to be continual throughout the process.

At this stage of the risk management process it may be necessary to establish an internal business case to ensure that resources are made available to support the process. The information gathered in this step is essential for supporting the business case.



3.2 DETERMINE VULNERABILITY AND RISK

This step of the process involves determining the vulnerability and eventually the risk that your council area and assets face from the effects of climate change. It is important that this is done using an approach for your risk assessment and reporting that is fit for the challenge faced and for the purpose of your organisation, and that will support adaptation planning and action.

The need to communicate with internal and external stakeholders remains essential. This includes communicating the results of the first-pass risk screening to your internal and external stakeholders and using the results and discussion to narrow down your focus to areas that are most at risk. At this stage you are able to undertake a second pass risk assessment. The second pass risk assessment includes understanding the vulnerability of your council area, organisation and community to coastal hazards.

At this stage it is necessary to gather existing data about coastal risks and in some cases to develop new information if none is available. There are several useful national and state data bases which may be enough and these should be accessed and reviewed before proceeding with new data collection. Council and other assets should be mapped as this will help with a risk assessment approach. Determining the vulnerability of your organisation, assets and community requires gaining an understanding of the adaptive capacity of each of these attributes.

The second pass risk assessment can be based on any new information that is gathered or developed such as updated sea-level rise mapping. Existing information, maps and stakeholder and expert knowledge can also be used to support the second-pass risk assessment.

Internal and external stakeholder engagement should be used to identify consequence scales for the risk assessment. It is also important to use existing consequence scales from Council's risk register or any disaster risk assessment frameworks which may be applied in Council. It is useful to consider risks in a number of categories such as economic risk, social risk and environmental risk.

Further information on risk assessment approaches and access to tools and supporting information can be obtained using the links provided in Section 5.

If any major risks are identified in the risk workshop, a deeper understanding may be required in key areas. This may require further investment to get refined data to support more detailed work and satisfy concerned stakeholders. It is important to communicate the results of the risk assessment with decision makers to discuss what can be done to address the issues that have been identified. At this stage it is useful to understand the legal risks of not addressing the identified risk.

Stakeholder engagement continues to be an important exercise at this stage.

3.3 IDENTIFY OPTIONS

Once the risk assessment has been undertaken it is possible to identify adaptation options that address climate change risks. In identifying options consideration should be given to options which address climate change, but which also address other existing pressures, although it is also necessary to identify options that may address the climate change risk alone. Option identification should include reassessment of options and strategies from existing strategies and plans within Council, helping to determine whether they are still relevant under a changing climate.

It is important to recognise that different options will be needed to address different climate related pressures, different assets and that these will vary between localities. Maps and spreadsheets can be useful in helping to link options selected to the specific risks they will address.

It is important to consider suites of different options which may be able to be undertaken together. These may include policy and planning options, community capacity building options, ecosystem-based adaptation options etc. It is also important to recognise the potential for having sequences of options, enabling you to take a pathways approach and avoid unnecessary expenditure and adverse side-effects from your actions. Such sequences may include initially using cheaper ecosystem-based options such as dune restoration and enhancement through replanting. Once these options stop working more engineering and cost intensive solutions may be necessary (e.g. beach nourishment), and ultimately perhaps the need for sea-walls to be constructed or options such as relocation to be considered.

The Principles presented in section 2 should be key considerations when selecting options.

3.4 EVALUATE OPTIONS AND MAKE A PLAN

In the previous stage Council, together with its stakeholders will have identified a series of possible options to address coastal risks. These options now form the basis of a strategy or plan. Options need to be evaluated and prioritised in conjunction with stakeholders using tools and approaches that help you to select options appropriate for the risk tolerance of your organisation and its stakeholders.

Approaches such as Cost Benefit Analysis (CBA) and Multi Criteria Analysis are useful tools that can help to prioritise options or suites of options. They can help to ensure that prioritised options make the most economic sense and deliver what is needed/wanted by stakeholders. Using the output from these

approaches a plan can be developed that list the options and identifies thresholds at which options will be implemented.

The plan should include clear objectives for each of the options and list performance indicators. It should also identify potential barriers to action and establish mechanisms to overcome them.

Always try to select options that will allow you to keep your options open (i.e. do not commit to something that cannot be changed or built on if required).

Identify suitable indicators to assess performance of options and determine how best, and most cost effectively these can be monitored. It is important to consider

the benefits or negative impacts that each option may have on community, environment etc. This will help to determine the level of engagement that may be required before an option can be implemented. The plan should:

- Identify thresholds and trigger levels when actions should be taken
- Identify people/groups responsible for actions
- Include review points when plan will be updated.

It is important to note that in some cases a deliberate action will be not to take any action. If this is the case, the process through which you arrived at this decision should be clearly documented.

Once the strategy or plan is developed, appropriate endorsement procedures should be followed to ensure it is supported at all levels of Council. This may require establishing processes for community consultation. This will support implementation of the plan.

3.5 TAKE ACTION

Once a plan or strategy is developed and signed off by Council and stakeholders, the plan or strategy can be implemented. Supporting activities may be developing specific business cases for actions or suites of actions, collecting additional data where required, influencing the implementation of other plans, and overcoming any barriers to action.

A critical aspect of implementation is attracting funding or finance for actions. Some adaptation options will be expensive

and approaches which help obtaining the necessary funds is critical. This may include identifying and approaching potential funders (e.g. State and Commonwealth Government) but may also involve working with the private sector to develop partnerships.

Collaborations and partnerships which support implementation of plans will be essential and can include partnerships with community groups, with other organisations such as NRM groups etc.

3.6 MONITOR AND EVALUATE

The last stage of the iterative cycle is to measure and evaluate adaptation plans. This helps to determine which adaptation actions are not achieving desired outcomes and need to be changed, but importantly also helps with accountability of projects.

Monitoring also helps to identify when an action is no longer effective for the degree of change that is happening, and a new action or suite of actions is required.

Over time it is important to reflect on what is being achieved, whether aspects of the planning process should be revisited, especially if new knowledge and information has become available.

Internal and external reporting of progress is essential. Consideration should be given to how best to do this and what level of detail is required. This may be an important element of community engagement and capacity building.

Following the monitoring and evaluation, the whole adaptation risk management cycle should be followed again in a way that fits the purpose of Council and which is aimed at continually improving the strategy or plan or adjusting it as new lessons are learned.



4.0 ENABLERS AND DECISION MAKING

4.1 ENABLERS

Identifying, planning for and managing risks requires more than simply following risk assessment guidance. It requires barriers to be removed and enablers to be installed, including:

- Leadership – Council and Executive leaders need to drive the process and inspire adaptation action by their organisations
- Governance – Effective governance structures to support adaptation planning and action
- Resources – Adaptation planning and implementation needs to be supported by adequate and targeted financial resources and experienced, equipped and committed personnel
- Learning and improvement – Council must commit to continual monitoring, evaluation, review and response, to ensure the long-term success of communities managing coastal hazards and adapting to a changing coastline.

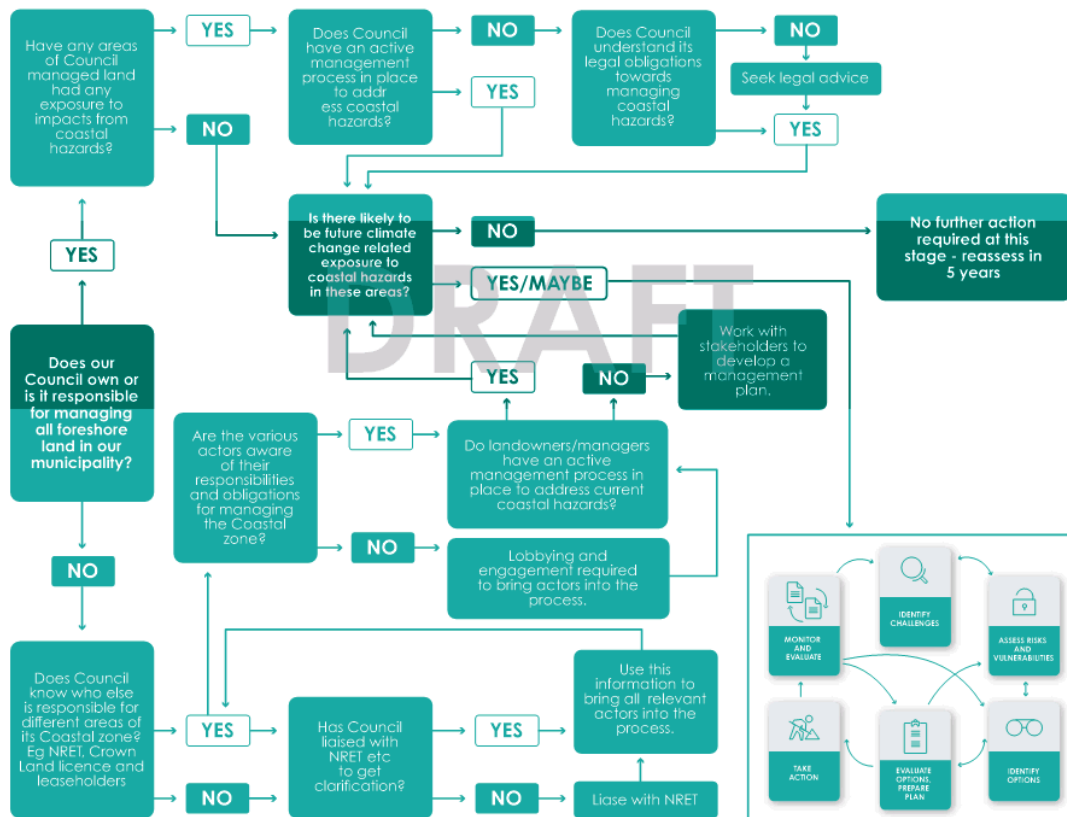
4.2 DECISION MAKING PATHWAYS

A simple process can be followed to determine Council's exposure and possible responses. This process is most appropriate for Councils beginning their adaptation journey. Those with more mature processes may find the flow diagram useful only to verify their system.

The process is summarised in the following flow diagram which is intended to support Councils when working through challenges which they face on the coast. The process provides guidance on actions Councils should take to get clarification on their ownership and responsibilities on the coast, and what they should do in response. Ultimately the process leads Councils to the need to undertake a climate risk management assessment and to develop an appropriate climate change adaptation plan.

It should be noted that this process will not be useful for councils which have mature, well developed adaptation management processes in place, and who have done the required assessments to drive them. It is more likely to be useful to those who are beginning to get to grips with the impact and management of climate change.

Decision Making Pathways diagram



5.0 SUPPORTING RESOURCES

The Tasmanian Government has undertaken a series of initiatives to support local government to better understand and manage the risks associated with coastal hazards, as follows:

- Tasmanian State Coastal Policy 1996.
- Department of Premier and Cabinet (DPAC) Mitigating Natural Hazards through Land Use Planning and Building Control Coastal Hazards Technical Report 2016.
- Department of Premier and Cabinet (DPAC) Tasmanian Coastal Adaptation Pathways Program.
- Department of Premier and Cabinet (DPAC) Coastal Hazards Management for Existing Settlements and Values project.
- NRET Managing Coastal Hazards nre.tas.gov.au/about-the-department/managing-coastal-hazards
- Tasmanian Coastal Works Manual; Page, L., Thorp, V. (2010) Tasmanian Coastal Works Manual: A best practice management guide for changing coastlines; Department of Primary Industries, Parks, Water and Environment.

There are a number of state and national resources which can be used to support climate adaptation planning in local government. Resources include:

- CoastAdapt is a resource that was developed by the National Climate Change Adaptation Research Facility at Griffith University. It was funded by the Commonwealth and has a focus on delivering a range of information to support coastal users to adapt to climate change. It also includes the C-CADS (Coastal Climate Change Decision Support) Framework which provides comprehensive guidance and resources to support risk-based adaptation planning. www.CoastAdapt.com.au
- Regional Climate Change Adaptation Project (RCCAP) http://www.dpac.tas.gov.au/divisions/climatechange/what_you_can_do/local_government/local_government_adaptation/local_government_adaptation_planning_resources/corporate_adaptation_planning
- The resources to the Tasmanian Coastal Adaptation Planning are also located here http://www.dpac.tas.gov.au/divisions/climatechange/what_you_can_do/local_government/local_government_adaptation/local_government_adaptation_planning_resources/community-based_coastal_adaptation_planning
- Climate Change in Australia is a resource developed by CSIRO and the Bureau of Meteorology. It provides climate change projection data, excellent reports and guidance that can be used to underpin climate change adaptation. www.Climatechangeinaustralia.gov.au
- For analysis of complex physical climate risk see XDI (<https://xdi.systems>).
- Coastal Risk Australia enables users to assess the extent of sea-level rise at their scale of interest. This is a user-friendly resource which can help assess risk. It is based on bathtub modelling and there is uncertainty associated with it which is explained on the site. www.Coastalriskaustralia.com.au
- Climate Compass was developed by CSIRO and various contributors in Australia and funded by the Commonwealth Government. It was designed to support Commonwealth Departments to address climate risk and has also been used by large infrastructure operators. www.Climatecompass.gov.au
- Adapt NRM was developed by CSIRO and NCCARF and includes guidance to support NRM groups to update their NRM plans to reflect a climate affected future. It was designed to help move NRM groups forward without changing their direction and momentum. www.AdaptNRM.csiro.au

6.0 ACKNOWLEDGEMENTS

This strategy document is a substantially revised and reduced version of the draft document. Impact Solutions International and BMT (Rees, C and Rissik, D); ***Regional Coastal Hazards Strategy: Addressing the Effects of Climate Change on Coastal Hazard in Tasmanian Southern Councils***; September 2020. The draft document was revised by the STCA's Regional Climate Change Initiative Working Group.



The contribution of all southern councils through briefings, workshops and discussions with the consultants is gratefully acknowledged. The Covid-19 pandemic disrupted plans to hold a summit with member Councils and elected Councillors to present the results of the study. Alternative methods such as review and input into drafting the strategy will be employed instead.

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DRAFT

REGIONAL CLIMATE CHANGE INITIATIVE

Regional Strategy - Adapting to a changing coastline in Tasmania has been prepared under the auspices of the Southern Tasmanian Councils Authority, Regional Climate Change Initiative by the 12 councils of southern Tasmania: Brighton, Clarence City, Central Highlands, Derwent Valley, Glamorgan Spring Bay, Glenorchy City, Hobart City, Huon Valley, Kingborough, Sorell, Southern Midlands and Tasman.

This version is released for consultation across key private and public stakeholders and will inform the final Regional Strategy Adapting to a changing coastline in Tasmania.

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Photography unless otherwise indicated: Katrina Graham, Senior Climate Change Officer, City of Hobart

February 2022



SOUTHERN TASMANIAN COUNCILS

REGIONAL STRATEGY FOR ADAPTING TO A CHANGING COASTLINE IN TASMANIA **SNAPSHOT**



Coastal hazards are the result of natural processes that can cause considerable damage to communities, industries and infrastructure. These hazards are magnified by sea level rise resulting from a warming climate and may present a significant risk to Tasmanian communities and local economies if they are not appropriately managed.

The Regional Coastal Hazards Strategy aims to increase the capability of Southern Tasmanian Councils to prepare for and respond to coastal hazards including erosion and flooding from the sea. The Strategy's objectives are to:

- apply a risk management approach that incorporates evidence-based, best practice management principles, systems and processes informed by up-to-date climate science and technology
- encourage consistency and co-ordination across southern Tasmanian councils through a regional approach
- The Principles are consistent with relevant Tasmanian Government policies and regulations and should be applied through the Strategy's risk management framework to coastal hazards on public and private land.

Public Safety

1. Human safety is paramount. Areas of unacceptable risk should be identified and exposure to risk minimised or if risk is unavoidable identify retreat pathways.
2. The community will be provided with up-to-date climate change and coastal hazards information to inform decision making and opportunities to participate in response planning.

Private property

3. Private property owners occupy coastal areas at their own risk.
4. Property owners are responsible for managing risks to their property from coastal hazards in accordance with relevant policies and regulations and based on expert coastal advice.
5. Where private coastal protection works are undertaken by property owners, beneficiaries should pay.

Local Government

6. Councils should actively monitor coastal hazard areas within their municipal areas.
7. Councils are responsible for the management of coastal hazard impacts on their assets and services.
8. Councils are not responsible for the cost of coastal hazard impacts on private property, or on private assets located on public land.
9. Access to public coastal land will not be available to private property owners for coastal protection works, except where significant public benefit is demonstrated.

Legal Risk and Adaptation

10. Coastal legal risks can be identified managed and reduced but can't be avoided

11. Well-developed policy and action now will minimise the risk of legal challenges and liability in the future

Coastal Management and Planning

12. Managing coastal hazard planning is enhanced where there is consultation between relevant levels of government, the private sector, community and other key stakeholders.
13. Planning provisions (such as Local Provisions Schedule; Specific Area Plans) can be used so coastal hazard management plans are prepared in areas identified as risky or hazardous.
14. A pathway of adaptive responses to coastal hazards, informed by up-to date climate science should be adopted and intervention by humans in natural processes should be minimised.
15. Public access and community services should be maintained wherever possible where consistent with the Principles of public safety, risk and financial considerations.
16. When Councils make decisions based on the projected asset life of infrastructure, they must factor the risks of coastal sea level rise hazards in the calculation of asset life.
17. Intensification of use or development should be avoided within coastal hazard areas on public land managed by Councils unless significant public benefit is demonstrated.

Coastal Values

18. Aboriginal and cultural heritage and ecological values will be preserved to the greatest extent practicable when responding to coastal hazards.

The Strategy is an initiative of the Southern Tasmanian Councils Authority's Regional Climate Change Initiative. It has been developed collaboratively between the ten coastal Councils in southern Tasmania and is consistent with the policy position of the Tasmanian Government's: DPAC Mitigating Natural Hazards through Land Use Planning and Building Control Coastal Hazards Technical Report 2016 and NRET Managing Coastal Hazards <https://nre.tas.gov.au/about-the-department/managing-coastal-hazards>

Contact:

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8.2 Significant Tree Nomination - Public Walkway between Beaumont Road and Ruth Drive, Lenah Valley
File Ref: F22/6290

Report of the Manager City Futures and the Director city Life of 28 April 2022 and attachments.

Delegation: Council

**REPORT TITLE: SIGNIFICANT TREE NOMINATION - PUBLIC
WALKWAY BETWEEN BEAUMONT ROAD AND RUTH
DRIVE, LENA VALLEY**

REPORT PROVIDED BY: Manager City Futures
Director City Life

1. Report Purpose and Community Benefit

- 1.1. The purpose of this report is to consider one of the nominations for the Significant Tree Register that was received from a member of the community between September and November 2021.
- 1.2. This report does not consider all nominated trees, only one located at a public walkway between Beaumont Road and Ruth Drive in Lenah Valley, as it is subject to another application.
 - 1.2.1. The significant tree nomination process benefits the community by ensuring that trees of value to residents and visitors are adequately protected.

2. Report Summary

- 2.1. The proposal is to consider whether the tree at the public walkway between Beaumont Road and Ruth Drive, Lenah Valley should be pursued for listing as a significant tree.
- 2.2. There were 127 valid significant tree nominations submitted during the public nomination period between 13 September and 8 November 2021.
- 2.3. Assessment of each of these trees is ongoing; however, the Beaumont Road public walkway is subject to an application to remove this tree.
- 2.4. Consideration of this tree at an early stage will provide certainty about whether or not the tree is intended to be listed in the Significant Tree Register, and allow Council to assess other matters concerning this tree independently of the nomination.
- 2.5. The owner of an adjacent lot (43 Beaumont Road) submitted feedback that they do not support the proposed nominations (**Attachment A**).
- 2.6. The City's Tree Assessment Panel assessed the tree against the Significant Tree Assessment Criteria (**Attachment B**); the panel recommendation is that the tree does not meet the criteria.
- 2.7. The nominated tree at the public walkway between Beaumont Road and Ruth Drive, Lenah Valley is not recommended for listing as a significant tree.

3. Recommendation

That the nominated tree at the public walkway between Beaumont Road and Ruth Drive, Lenah Valley not be listed as a significant tree in the Significant Tree Register, and the Significant Tree Code of the Hobart Interim Planning Scheme 2015.

4. Background

- 4.1. At its meeting on 18 December 2017, Council considered a report on 'Significant Trees – Vegetation Management'. One of Council's resolutions following this report was that:

'Nominations for additions to the Significant Tree List in the *Hobart Interim Planning Scheme 2015* be invited through a public process commencing in February 2018 and that in future nominations be sought from the public every 3 years.'

- 4.2. The previous round of nominations, which commenced in 2018, resulted in the inclusion of 77 additional listings for trees and hedges in the Significant Tree Register.
- 4.3. Trees that are listed in this register are also inserted into the Significant Tree Code list of the *Hobart Interim Planning Scheme 2015*. Trees on the list are subject to the controls of the Significant Tree Code, which requires that a permit be granted for lopping, pruning, removal or destruction of the tree unless:
- The tree has died of natural causes;
 - The works are necessary for emergency access;
 - The works are emergency works by a public authority;
 - The tree poses an immediate danger to persons or property;
 - The pruning of a tree is to improve its health or appearance, provided its normal growth habit is not retarded.
- 4.4. The current round of nominations was open between 13 September 2021 and 8 November 2021.
- 4.5. There were 127 valid nominations submitted for individual trees, groups of trees, or hedges. Some of these nominations were made by more than one person.

- 4.6. Assessment of the nominations is still ongoing. Given the number of nominations received and the detailed assessment process, the assessment is likely to take several months.
- 4.7. The nominated tree that is the subject of this report is a *Eucalyptus viminalis* (white gum) on a public walkway between Beaumont Road and Ruth Drive (refer to image and map below).



White gum at public walkway between Beaumont Road and Ruth Drive



Location of white gum (red circle) at public walkway between Beaumont Road (to the left) and Ruth Drive (to the right).

- 4.8. The following background information is relevant to this tree:
- 4.8.1. The adjacent landowner at 43 Beaumont Road applied to remove this tree as part of the development of a new house (PLN-20-638).
 - 4.8.2. General Manager Consent (GMC-20-68) was provided for the tree to be removed, subject to the applicant funding the removal of the tree, developing a landscaping plan (approved by the City of Hobart) to replace the tree, funding implementation of the landscape plan, and obtaining support from neighbouring properties.
 - 4.8.3. Removal of the tree is exempt from requiring a planning permit, but requires a Public Spaces By-Laws permit.
 - 4.8.4. Further consideration of the removal of this tree was paused pending a decision on whether or not the tree is to be listed as a significant tree.

5. Proposal and Implementation

- 5.1. The proposal is to determine whether the nominated tree at the public walkway between Beaumont Road and Ruth Drive, Lenah Valley should be listed as a significant tree.

Assessment of Nominated Tree

- 5.2. A Tree Assessment Panel (the Panel) consisting of a City of Hobart (City) Development Planner, City Program Leader Arboriculture and Nursery, City Cultural Heritage Officer and City Urban Design Officer has assessed the tree.
- 5.3. The Panel has inspected the tree and made an assessment, based on whether it is considered that the tree meets any of the ten criteria under the Significant Tree Assessment Criteria (**Attachment B**):
- 5.4. The nomination and assessment comments are detailed in Table 1 (below).

Table 1

Nominator's reasons for nomination	Significant Tree Assessment Panel comments
<p>I grew up in Ruth Drive and I have studied the wildlife that live in the local area and in that tree.</p> <p>Thanks to the subdivision most of the mature local gums have been removed meaning local birdlife and possums have lost important habitat.</p> <p>I don't understand why humans think they have the right to remove a beautiful tree just because it drops a few leaves and bits of bark.</p> <p>The tree is considered to meet category 9 of the Significant Tree Assessment Criteria.</p> <p>Further supporting documentation was provided by the nominator, provided as Attachment C.</p>	<p>This tree is an attractive feature of a public walkway, and is one of the few remnant trees in a recently subdivided area. However, the tree is not of sufficient age, size, prominence or individual ecological significance to be considered to meet the threshold for listing under the Significant Tree Assessment Criteria.</p> <p>Category 9 of the criteria states:</p> <p>Trees that have a significant contribution to the integrity of an ecological community: <i>These trees are remnant native species which provide significant habitat for rare, threatened or locally uncommon or common native species. For example trees that provide breeding and foraging habitat, resting sites and refuge.</i></p> <p>While all trees, particularly native trees, provide ecological benefits, the threshold to meet this criteria requires that the tree has an individually significant contribution. This might include trees that have established nesting hollows or are known nesting sites for threatened species. This tree is not considered to individually meet the threshold for listing under this category.</p>

- 5.5. This recommendation has been made objectively on whether the tree meets the Significant Tree Assessment criteria.
- 5.6. The panel applies a consistent approach to assessing trees, only recommending those that meet a high threshold of significance.

Consultation

- 5.7. Two adjacent landowners in Beaumont Road were notified of the nomination of the tree and invited to provide feedback.
- 5.8. A submission was received from one of the notified landowners (43 Beaumont Road).
- 5.9. The submission from the landowner of 43 Beaumont Road is provided in full in **Attachment A** and the comments included the following :
- 5.9.1. The tree is not a tree of significance;
- 5.9.2. Two arborists were engaged to provide advice about the tree and both said it is not a tree of significance and it is under stress and declining health.
- 5.9.3. Signs of distress include:
- the increasing amount of sap seeping out of the tree;
 - the bare branches in the canopy and limited amount of foliage;
 - the changes to the tree bark and cracks in the tree trunk.
- 5.9.4. The City's civil works to construct a footpath caused damage to the tree's root system and the decline in health is noticeable;
- 5.9.5. This tree is unsuitable for the built-up residential area with dwellings surrounding it.
- 5.9.6. The tree needs to be removed and the walkway planted with more suitable trees and shrubs for residential urban areas to make it safer for pedestrians and homes.
- 5.9.7. In September 2021, the City indicated the tree could be removed subject to a landscaping plan, open space permit and removal by a professional arborist, all at the landowner's cost. These requests were satisfied and additional information provided.
- 5.9.8. A planning permit and General Manager Consent were granted to remove the tree.

Panel Response

- 5.10. In response to this submission, it is noted that the Panel has assessed the tree and determined it does not meet the Significant Tree Assessment Criteria. It is therefore not recommended to be listed as a significant tree.

- 5.11. The suggestion from the submitter that the City remove the tree has been referred to the Bushland, Biodiversity and Waterways Unit and will be considered separately.

6. Strategic Planning and Policy Considerations

- 6.1. The proposal is consistent with the objectives of the Capital City Strategic Plan 2019-29, in particular the following outcomes:
- 6.1.1. Hobart keeps a strong sense of place and identity, even after the city changes;
 - 6.1.2. Hobart's cityscape reflects the heritage, culture and natural environment that make it special;
 - 6.1.3. In City decision-making, we consider how different aspects of Hobart life connect and contribute to sense of place;
 - 6.1.4. The natural environment is part of the city and biodiversity is preserved, secure and flourishing.

7. Financial Implications

- 7.1. Funding Source and Impact on Current Year Operating Result
- 7.1.1. None.
- 7.2. Impact on Future Years' Financial Result
- 7.2.1. None.
- 7.3. Asset Related Implications
- 7.3.1. None.

8. Legal, Risk and Legislative Considerations

- 8.1. The tree is not recommended for listing as a significant tree in the Significant Tree Register and the Significant Tree Code of the *Hobart Interim Planning Scheme 2015*. If the Council supports this, the City's Bushland, Biodiversity and Waterways Unit will consider the health of the tree and any considerations about removal of the tree.

9. Environmental Considerations

- 9.1. The proposal considers the contribution this tree makes to the environment.

10. Social and Customer Considerations

- 10.1. The proposal is not considered to have any negative impact on social inclusion.

11. Marketing and Media

- 11.1. The significant tree nomination process was promoted to the community through the City News newsletter, Facebook, the City's website and in an article in the Mercury newspaper.

12. Community and Stakeholder Engagement

- 12.1. No further community or stakeholder engagement is considered necessary for this proposal.


13. Delegation

- 13.1. This matter is delegated to the Council.

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.






Sandra Hogue
MANAGER CITY FUTURES



Neil Noye
DIRECTOR CITY LIFE

Date: 28 April 2022
File Reference: F22/6290

Attachment A: 43 Beaumont Road submission ↓ 
Attachment B: Significant Tree Assessment Criteria ↓ 
Attachment C: Additional Information from Nominator ↓ 

To Whom It May Concern

Significant Tree Nomination – Beaumont Road walkway (between 41 & 43 Beaumont Road)

We are building at 43 Beaumont Road, Lenah Valley and we believe this tree is not a tree of significance.

We engaged two arborists, one written and the other verbal, about this tree being a tree of significance. Both have said that it is not a tree of significance and that it is under stress and declining health. The signs of this distress include:

- The increasing amount of sap seeping out of the tree.
- The bare branches in the canopy and reduced foliage.
- The changes to the tree bark and cracks in the tree trunk

The soil surrounding the tree has also changed. The Council's original civil works to construct a footpath directly near the tree caused damage to the tree's root system through soil disturbance and excavation. The footpath has been in for a few years, and it often takes two to three years after this disturbance has occurred that a decline in the health of the tree becomes noticeable. It is very noticeable now.

We feel this tree is unsuitable for the residential, highly built-up area it has now become. Dwellings completely surround it. We believe the tree needs to be removed and the walkway planted out with more suitable trees and shrubs for residential urban areas to make it safer for pedestrians and homes.

Back on 9th September 2021, we were advised by Council that if we provided the below information, the tree could be removed (all at our cost):

1. A landscape plan and we fulfil that plan at our cost
2. Get an open space permit (as the tree is mainly on Council land)
3. At our cost organise to get the tree removed by a professional arborist

On 20th September I provided all the above plus more:

1. Landscape plan by a Landscape Architect firm that undertakes large Council design projects – note: we are landscaping, at our expense, the entire walkway, not just the area where the tree is to be removed as the Council stipulated.
2. Public Liability Insurance
3. Safe Work Method Statement
4. Traffic Management Plan
5. Arborist contact details and insurance
6. Date of tree removal – being 8th October 2021

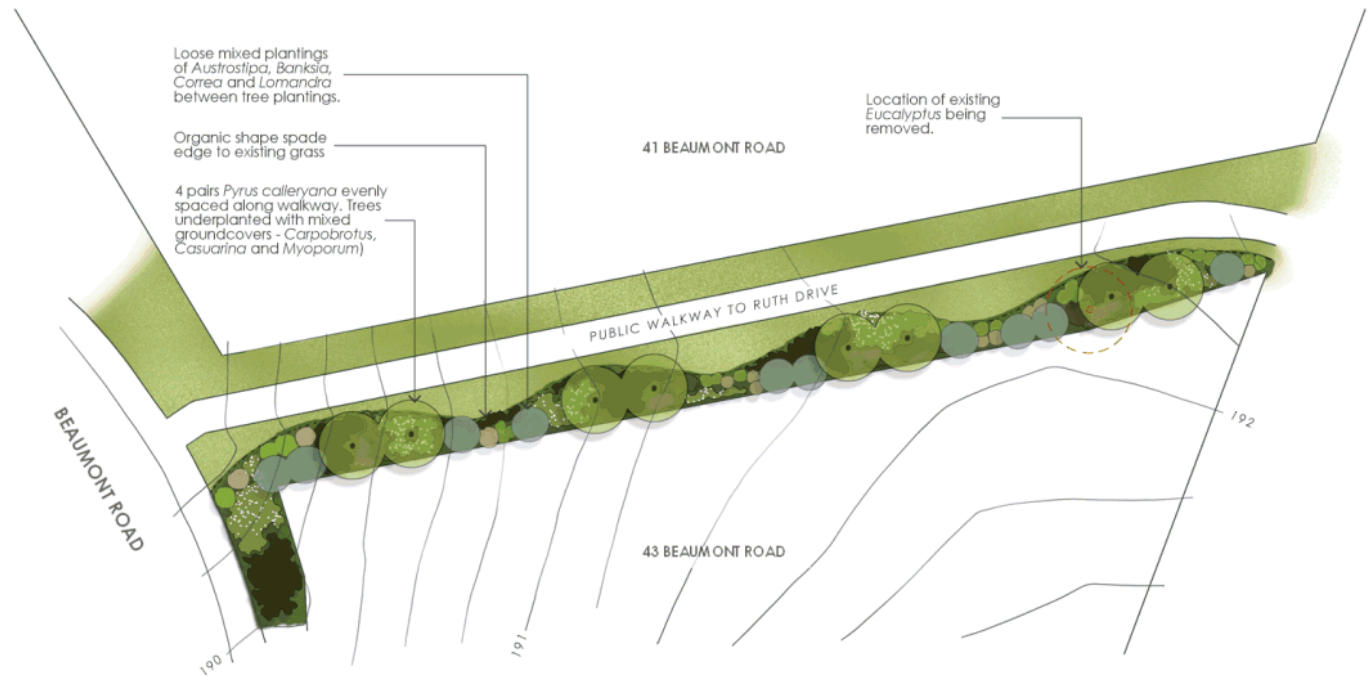
We also have GMC and a planning permit to remove the tree. From the very beginning of going through the planning permit process, the Council has always said the tree can be removed, except when we provide everything the Council asked for, the Council changes the goalposts and expects more. When we offer more, the goalposts change again.

This area and new subdivision will be our forever home, and the tree will eventually cause damage to our property and the property on the other side of the walkway at 41 Beaumont Road. Heaven forbid it injures one of our four children when a limb ends up falling.

We wanted this urban area to be a beautiful, attractive walkway for all people to enjoy, and that was why we were happy to work with the Council to revegetate the area with the distressed gum tree removed. We are still wanting to work with Council on this.

We are now asking that the Council remove this tree due to its declining health and not being suitable for the urban landscape it now occupies and is surrounded by. We would also like permission to plant out the walkway per the landscape plan we submitted which is attached. There are no Council services along the walkway, and we undertook a dial-before-you-dig, also attached.

We hope the Council will decide that this tree is not a tree of significance, arrange for it to be removed and work with us to make it a lush area for pedestrians to enjoy.



Grass / Strappy <i>Austrostipa stipioides</i> Coastal Spear Grass Mature size (HxW) .8m x .8m	Small Shrub <i>Banksia spinulosa</i> "Birthday Candles" Mature size (HxW) .3m x .6m	Groundcover <i>Carpobrotus rossii</i> "White Out" Mature size (HxW) .1m x 2m	Groundcover <i>Casuarina glauca</i> "Cousin It" Mature size (HxW) .1m x 1m	Medium Shrub <i>Correa alba</i> White Coastal Correa Mature size (HxW) 1.5m x 1.5m	Grass / Strappy <i>Lomandra longifolia</i> "Tanika" Mature size (HxW) .6m x .6m	Grass / Strappy <i>Lomandra longifolia</i> Matt Rush Mature size (HxW) .8m x .8m	Groundcover <i>Myoporum parvifolium</i> Purple Creeping Boobialla Mature size (HxW) .1m x 3m	Medium Tree <i>Pyrus calleryana</i> "Capital" Pear Mature size (HxW) 10m x 3m
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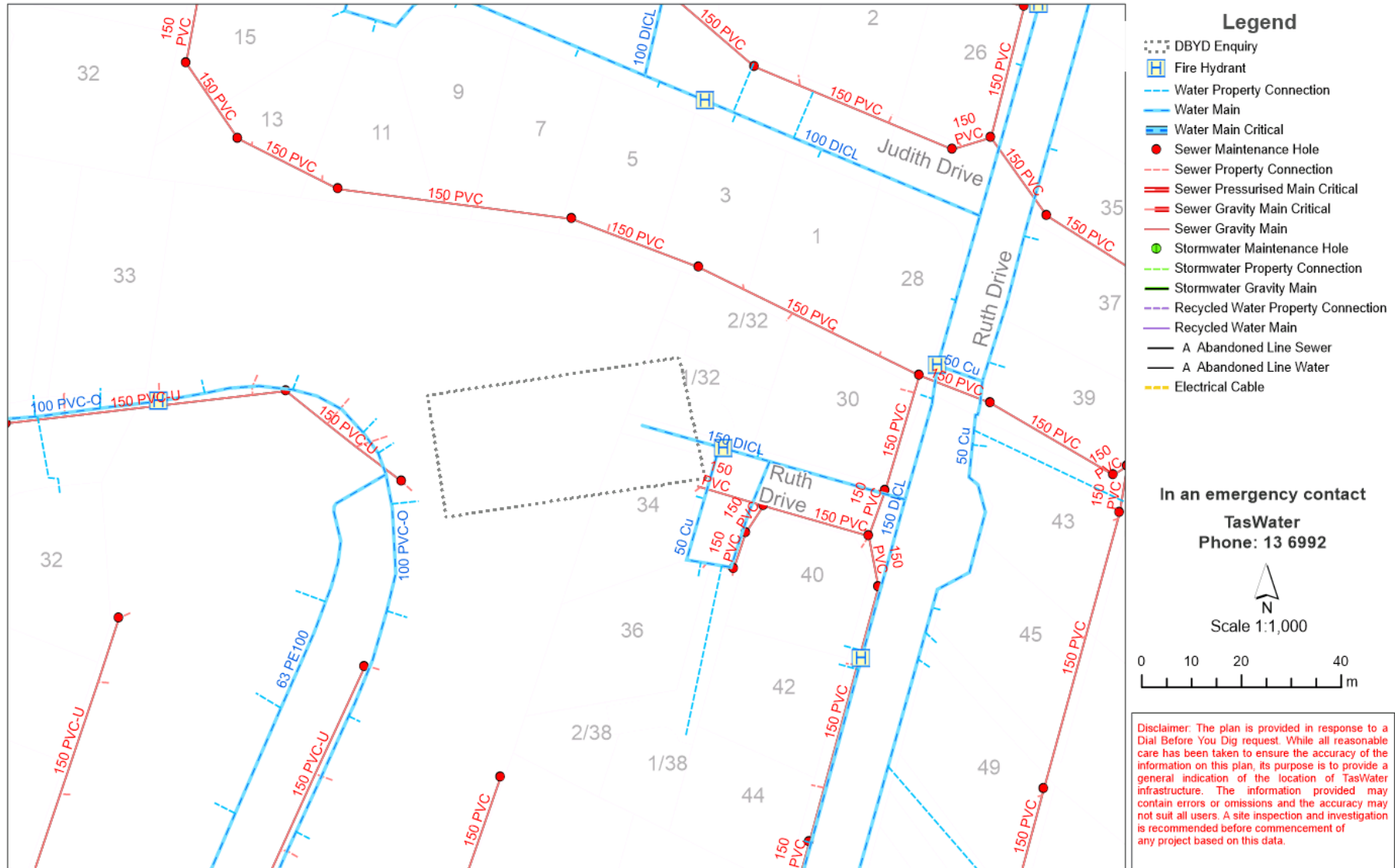




Job # 30655992 Seq # 203834020

Provided by: TasWater

Date Generated: 08/10/21 (valid for 30 days)

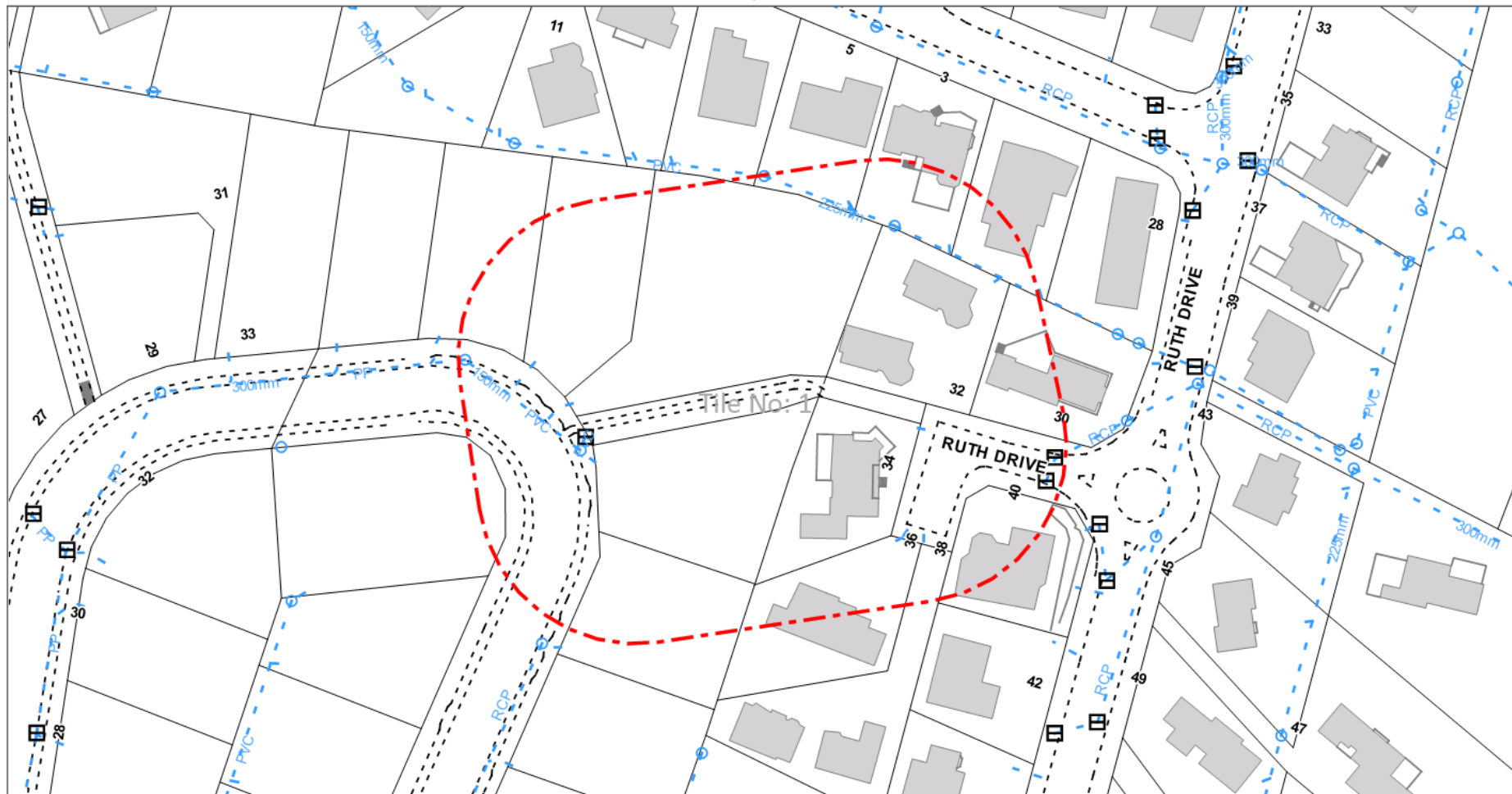


Please note most property connections are representative only. The actual location of the property connection may be significantly different to what is shown on this map.

TasWater Plan v4.1 (24/09/2019) Plans generated by
SmarterWX™ Automate



Sequence No: 203834023
Job No: 30655992
Location of Works: 34 Ruth Drive, Lenah Valley, TAS 7008



DISCLAIMER:

While reasonable measures have been taken to ensure the accuracy of the information contained in this plan response, neither City of Hobart or PelicanCorp shall have any liability whatsoever in relation to any loss, damage, cost or expense arising from the use of this plan response or the information contained in it or the completeness or accuracy of such information. Use of such information is subject to and constitutes acceptance of these terms.

A field survey is to be conducted before information contained in this plan is relied upon.

WARNINGS AND REQUIREMENTS:

Refer to the cover letter and information sheet for all warnings, requirements and contact details.

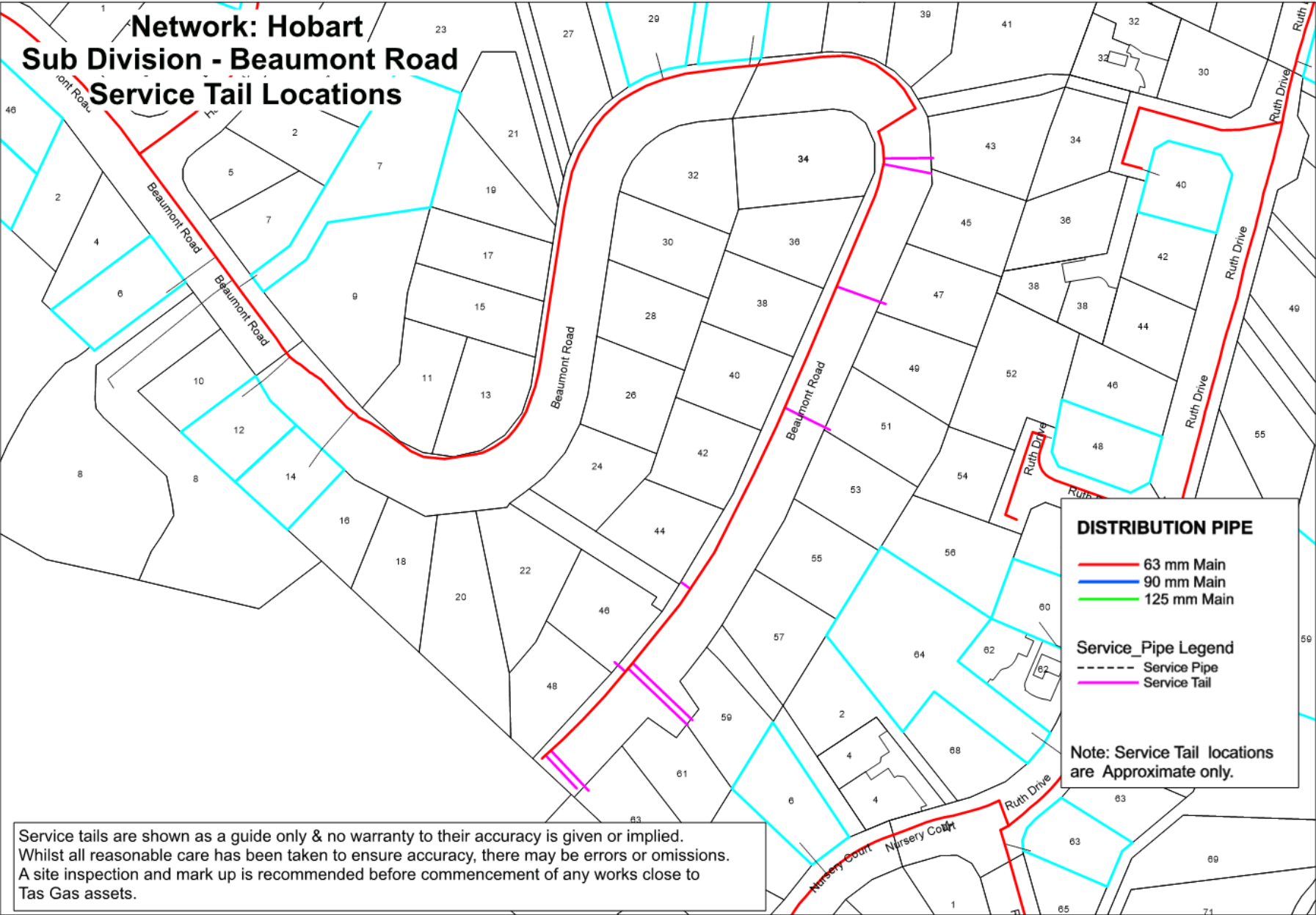
Abandoned old town gas (coal gas) pipes potentially emitting harmful gases and Volatile Organic Compounds (VOCs) may be found in many areas of the City. Refer to the cover letter and information sheet for more detail.

If in-ground parking sensors are located in the enquiry area please call City of Hobart Parking Operations at least 7 days prior to any works on telephone (03) 6238 2439.

- Stormwater Pit
- + Intake Point, Junction, Outlet Point
- Maintenance Shaft, Man Hole
- Pollution Control Unit
- Inspection Opening

- Stormwater Pipe
- Electrical and Other
- In-ground Parking Sensor
- ▨ Construction Risk Area
- ▨ Potential Electrical Underground Assets
- DBYD Enquiry Area





CATEGORIES OF SIGNIFICANCE

Significant Tree Assessment Criteria

Category	Description
1. Trees of outstanding aesthetic significance.	These are trees whose form, seasonal beauty and/or placement is such that they are not only magnificent examples of their species but are also a significant aesthetic component of the streetscape, park, garden or natural landscape in which they grow
2. Trees of outstanding dimensions in height, trunk circumference or canopy spread.	The biggest, tallest, widest examples of any species will always attract attention and admiration. For example an outstanding example of the species in form, structure and health.
3. Trees that are very old or venerable.	These trees may not be particularly beautiful but their age and associated links to the past, makes them significant to the community. For example trees which reveal previous landscapes or land use such as estate boundaries or orchards.
4. Trees that commemorate, or are reminders of, cultural practices, historic events or famous people.	These trees are not necessarily old but have cultural significance through association with a significant person or event. This can include: <ul style="list-style-type: none"> • Trees that have strong associations with specific community or cultural groups for spiritual, cultural, religious or other associated reasons. • Trees planted by royalty, political figures or community leaders or to commemorate significant international or local events. • Trees that have a recognised association with aboriginal culture or heritage or are valued for continuing and developing cultural traditions.
5. Trees that are recognised as a significant component of a natural landscape, historic site, town, park or garden.	These are trees that enhance the unique character of a natural landscape, streetscape, park or town and whose loss would fundamentally alter that character. These trees are also those identified as contributing to the state or local significance of a historic site, private garden or estate.



6. Trees that have local significance.	These trees are trees that, as a component of a streetscape, park or garden, are important to the local community and are recognised features of the immediate landscape or provide a sense of place. The loss of these trees may impact on the local community.
7. Trees of a species or variety that is rare or of very localised distribution.	These trees have important scientific ecological or horticultural significance. For example trees that are endangered as a species or unique in distribution or position in the natural landscape. This can include remnant native vegetation that indicates the former extent of the species, rare/threatened species or those at the edge of their natural range, or locally uncommon species.
8. Trees that are of horticultural or genetic value.	These trees could provide important and valuable propagating stock for scientific research, forestry or horticulture industries because their genetic characteristics differ from the type. For example trees that: <ul style="list-style-type: none"> • Are a source of seed from a local gene pool • Are particularly resistant to disease or climate extremes • Have a particular growth form • Have a tolerance to pests or disease
9. Trees that have a significant contribution to the integrity of an ecological community.	These trees are remnant native species which provide significant habitat for rare, threatened or locally uncommon or common native species. For example trees that provide breeding and foraging habitat, roosting sites and refuge.
10. Trees that are significant for reasons that are difficult to categorise.	Very simply, these trees are significant because they are interesting! Whether they are examples of the art of topiary or simply growing in extraordinary circumstances, they will immediately catch the attention of the passer by. For example interesting growth forms, outgrowths, branch fusion or pruning.

Urban Trees: Worth More Than They Cost

G M Moore

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Richmond Victoria 3021

ABSTRACT:

Trees are major urban infrastructure assets. While costs, and the damage and nuisance values attributed to trees are widely known, the benefits they provide are often subtle and under-appreciated. Cities are biodiversity hot spots due to the variety of habitats available in public and private open space. In the past decade tree populations in many Australian cities have declined, particularly with the loss of private open space.

At a time of climate change, it is worrying that both private and public open spaces are threatened by urban renewal and development that puts at risk long term sustainability. In many of these situations there is insufficient open space - public or private - for the planting of large trees and so the opportunities for the role of vegetation in ameliorating the heat island effect, reducing wind speed, providing shade and reducing energy use are reduced. This outcome raises questions about the economic viability of such developments, as well as their long term environmental sustainability.

Trees provide economic and ecological services benefits to society. They are assets which warrant the expenditure of resources such as labour, energy and water. Such expenditure is not wasted as trees and urban landscapes provide more economically and ecologically than they use. In any comprehensive and fair calculation urban trees and landscapes are worth more than they cost.

INTRODUCTION:

Mature trees are significant assets to our environment and our society regardless of where they occur or whether they are native or exotic. A great deal of effort has gone into managing, conserving and preserving these trees. Considerable human labour and time has been expended on the trees as well as real energy in the form of fossil fuel that has underpinned their maintenance. There has also been significant water allocated to their growth and development. They are community assets in every sense of the word – society has invested resources in their establishment and management, and they have matured as assets and are now returning great and diverse benefits (Moore 1997) to society in return.

For trees growing in parks and gardens there must be proper inventories that are computer-based, providing full and comprehensive information on the specimen, including its identity, location, age, condition and monetary value amongst other important details. A monetary value must be assigned to a tree using an acceptable amenity tree valuation program. This value raises the status of the tree to that of an asset, and allows for the proper recognition of trees in the decision making processes by those who may fail to recognize the inherent value of the tree.

In an analysis of Urban Tree Cover in Melbourne, Mullaly (2000) used aerial photographs to estimate changes in the cover of an inner suburb - a part of Richmond, now in the City of Yarra-and an eastern suburb – a part of Balwyn, now in the City of Boroondara. Aerial photographs from 1993 were compared with those from the year 2000 (Table 1). There was a reduction in overall canopy cover of 2% in Richmond and 7% in Balwyn. While the reduction in cover was anticipated it was not expected that the reduction would be greater in the outer compared with the inner suburb. These results suggest that whilst there is recognition of loss of cover in inner city urban renewal, changes in the vegetation cover of other suburbs should not be underestimated.

Table 1: Changes in tree cover for developed and undeveloped land in Richmond and Balwyn between 1993 and 2000 (Modified from Mullaly 2000)

LAND TYPE	OWNERSHIP OF LAND	BALWYN			RICHMOND		
		1993	2000	CHANGE	1993	2000	CHANGE
Developed Land	PRIVATE	19.23	10.49	-8.24	7.01	5.17	-1.84
	PUBLIC	3.45	4.65	1.20	2.65	2.12	-0.43
	TOTAL	22.68	15.64	-7.04	9.66	7.39	-2.27
Undeveloped Land	PRIVATE	20.00	17.47	-2.53	5.89	5.78	-0.11
	PUBLIC	6.25	7.81	1.56	2.84	5.45	2.61
	TOTAL	26.25	25.28	-0.97	8.73	11.23	2.50

Upon further analysis (Table 1) it was noted that Balwyn had approximately 2.5 times more foliage cover per unit area in developed open space than Richmond in 1993. This would suggest that there has been a significant loss of tree cover in Balwyn and that a 7% loss represents a substantial change in this part of Melbourne. This loss of trees, however, is not as noticeable as in many parts of cities as there are still many substantial trees remaining. A 2% loss in the City of Richmond may seem almost insignificant. However, given the relatively low levels of cover, even 2% can make a substantial difference.

The initial assumption that little had changed in Richmond was proved to be further unjustified when the percentage of cover was related to land ownership. The analysis showed that there had been a considerable loss of cover in Richmond on privately owned property, but that this had been compensated for by significant tree planting in the public open space (Mullaly 2000). Significant losses of trees on private property due to intense high-density housing development had been compensated for, to some degree, by the planting of trees in local streets and parks. However many of the spaces suitable for planting larger specimen trees on public land had already been utilized, and as further high density inner city development proceeds, the loss of trees on private open space is unlikely to be compensated for by public planting.

The significance of these changes in a mere seven years should not be underestimated. These trends will have a profound influence in inner and outer city suburbs, and similar trends are likely in other Australian cities. It is ironic that at a time when the environment and climate change are major matters of public concern, in cities public and private open spaces are reducing and vegetation cover is depleted.

CLIMATE CHANGE, TREES and LANDSCAPES

The current drought affecting south eastern Australia is into its thirteenth year, and there have been major storm events in most States in each of the past three years. In parts of southern Australia, there has not been a dry period like it in recorded history. These events may be a part of natural cycles of perhaps five hundred years or more but current meteorological data are too recent to reveal such patterns. However, the current dry period and recent storm events are likely to indicate the climate changes that are to come, and which will be a permanent part of our environmental conditions (Table 2).

Table 2: Current data trends on global warming and predictions of the likely outcomes for climate and sea level related changes (Moore 2009a).

FACTOR	HOW ARE WE TRACKING	PREDICTION
Global temperature	The last 30 years have been the warmest of the past 200 years	Suggests that temperature rises will be at or above the worst case scenario of 6-8°C
Australia terrestrial temperatures	Have increased by 1°C in the past 50 years	Is in line with higher rather than lower temperature predictions
Sea levels	Have risen by 3mm per annum for the past 15 years	Consistent with higher sea level predictions
Atmospheric CO ₂ levels	These are above the predicted worst case scenario and could exceed 1000ppm	This suggests atmospheric temperature rises of 6-8°C
Safe Atmospheric CO ₂ levels	The environmentally safe level seems to be about 350ppm, and for the past 200,000 years they have been at about 280ppm	Atmospheric CO ₂ levels are likely to rise to between about 500 and 1000ppm, which could cause a major extinction event
Arctic Ice Cap	Melting more rapidly than expected. It seems the northern hemisphere is warming more rapidly than the south	Could melt as early as 2013 rather than 2040-2050 as was originally predicted
Melting Polar Ice Caps	Melting more rapidly	Only 3% of the extra energy absorbed in global warming has gone into heating the atmosphere. Most has gone in melting the ice caps
Reflection of radiation by ice caps	As they diminish in size less radiation is reflected from earth	Heating of the planet will accelerate to or above the worst case scenario

Regardless of how things eventually pan out, chronic drought and the possibility of more permanent global climate change are changing the environments within which trees are growing. Such changes are also resulting in the rapid change of the political, economic and social environments within which tree managers operate, and the decision making processes that ensue (Moore 2006). There will be more severe weather events more often in south eastern Australia, which will be associated with stronger winds and more intense

rainfall (Table 3). Storm events that were once considered one in one hundred year or one in thirty year events are likely to occur perhaps every decade or even annually.

Table 3: Likely outcomes from climate related changes in south eastern Australia.

Generally warmer winters and hotter summers
A more tropical climate extending southward
More easterly winds leading to summer storms
More frequent major storm events
More days of extreme fire risk weather
More bushfire prone regions, extending to peri-urban parts of major cities
Changed weather and fire patterns
Fewer frosts, and in some places elimination of frosts completely
Many more days above 30C and double the number of days above 35c
Higher summer rainfall with more intense rainfall events
Flooding of lowland coastal areas – probably minor
For every one degree temperatures rise, the snowline rises 100m
Agricultural productivity will change, in some cases improving
Some crops will not be grown but others become viable
Housing and building construction processes will change
Energy demands and patterns of use will alter

Such changes will have profound impacts on urban tree managers. Increased storm events could see higher rates of windthrow and major branch failure. In recent storm events there have been lengthy and widespread power outages, often due to falling trees and branches. Such incidents have attracted major media coverage, and the events are often described as an *Act of God* or perhaps an example of the *Fury of Mother Nature*. Such descriptions allow authorities to dodge the responsibility of managing the consequences of such events, and minimize the opportunities for learning from extreme weather events. There is also the common and predictable public demand for urban tree removal.

However these events should have been used to inform management practices that might be appropriate under a changed climate scenario, where the undergrounding of services, particularly in areas of high population density should be adopted immediately. It is curious that undergrounding of services is often opposed on the grounds of its high installation cost. However, not undergrounding is simply too costly to society to be maintained for much longer into the future under a changed climate.

It has long been argued that if installation and long term maintenance costs are considered then, undergrounding is cost effective. However installation and maintenance are often done by different sectors, in some States installation is by private energy providers and tree maintenance by private land owners and local government, in others States installation is by State Governments and maintenance by local governments and in yet other States there are even greater numbers of entities involved. Such an arrangement is simply untenable, because Australian society cannot afford such a regime which is economically and environmentally unsustainable. Perhaps it is also time to note that costs to government and costs to society are not necessarily the same thing.

Recent and tragic bushfires in New South Wales, Canberra and Victoria have raised many concerns about tree management and infrastructure. While the findings of the Victorian Royal Commission into the 2009 bushfires are yet to be finalized or released, there would be few who could argue that undergrounding of electricity services would not have been an advantage during these terrible fires. Sadly it would seem that yet another opportunity to manage the vegetation/infrastructure interface in a way that is appropriate to a future and changed climate is to be lost. The above ground cabling has been replaced, just as it was, and the chance for a modern, safer, underground system appears to have been lost.

CITIES AND TREE VALUE:

Urban trees and landscapes are assets that require the expenditure of resources – labour, energy, and even water - on their proper management. The question that might be asked: “What is the value of the benefits that are provided by trees? Or perhaps what does society get in return?” (Table 4). What is the value of shade provided by trees that drop temperatures by up to 8C, reduce air conditioner use and reduce carbon emissions? Estimates put the savings at between 12-15% per annum. Manchester University’s Adaptation Strategies for Climate Change in the Urban Environment Project has found increasing green space in cities by 10% reduces surface temperatures by 4°C due to water evaporating into the air from trees and other vegetation (Fisher 2007).

What is the value of reduced wind speeds of up to 10% due to the presence of trees under a climate change scenario when winds will be stronger? What role might this play in bushfire management, especially at a time when so few are considering the positive role that vegetation can have in managing fire behaviour? The presence of shady trees can increase the useful life of asphalt pavement by at least 30%, which can be of considerable value in the hot climate of Australia, where asphalt degrades quite rapidly. Little scientific research work has been done in Australia on these benefits from vegetation and there is even less economic data to inform decisions.

What is the value of the pollutants removed from the air of Australian cities? In New York in 1994 the value of the city’s trees in removing pollutants was estimated at US\$10 million per annum. Planting 11 million trees in the Los Angeles basin saves US\$50 million per annum on air conditioning bills. Still the only Australian study of its kind by economists notes that an Adelaide street tree provides a minimum annual benefit of about \$200 per year, noting that it is an under-estimate of real value (Killicoat, Puzio and Stringer, 2002). The value returned to the City of Melbourne by its approximately 70 thousand public trees alone it would be more than \$14 million per annum. Other studies show a cost/benefit ratio of 1 to 6 in favour of urban trees and landscapes.

There is also the role of trees and public open space under a changed climate in holding and absorbing water during intense rainfall events. Such a role has profound implications for the behaviour of storm water systems in cities. What is their value in reducing localized flooding? What will happen in suburbs, which occur in all major cities, where housing development has been so intense that there is no capacity to plant trees on house blocks, and where streets are so narrow that street trees that have been planted will not be able to mature as they will inevitably restrict emergency vehicle access.

Figure 4. Estimates of various environmental economic values for 100,000 large mature urban trees growing in an Australian city (modified from Moore 2009b)

Parameter	Value per tree	Quantity	Unit Price AUD\$	Value AUD\$	Reference
Carbon sequestered in trees	12.5 tonne	1.25million tonne	\$20 per t	\$25 million	Moore 2009
Street Tree value	\$ AUD 200per annum			\$20million per annum	Killicoat et al 2002
Electricity saving	30KWh	3 million kWh	\$0.17 per kWh	\$510,000 per annum	Fisher 2007
Carbon emissions saved	1.2Kg for each kWh	3,600 tonne	\$20 per t	\$72,000 per annum	Moore
Water saving from electricity generation	30 kWh per tree at 100L per kWh	300 mill L	\$1.50 per kilolitre	\$45000	Moore
Prolonged life of bitumen footpaths	\$450 per m ² for life of 20 years		\$225 per m ² for an extended life of 50% (10 years)	\$47,250,000	Moore

Notes on estimations and calculations:

- the estimate of 12.5 tonne is for a large mature urban tree
- the price of AUD\$20 per tonne is based on the Australian carbon market price
- the electricity saving is based on reduced energy use due to shade from trees
- the price used for electricity is based on a rounded Victorian rate per kWh
- value of prolonged bitumen is based on an extended life from 20 to 30 years
- 100L of water is used to generate each kWh by brown coal powered generators
- water valued at \$1.50 per kilolitre
- assumes tree canopy of 75m² shading bitumen covering 30% of its canopy area

A recent Australian National University study found that suburban street trees were more effective than native forests at capturing carbon because of their relative youth. The study was commissioned by the Australian Capital Territory Government as part of refining its climate change strategy and was the first time carbon stocks and carbon storage rates have been measured for an entire state or territory (ABC News 2009).

The benefits of urban trees and landscapes already mentioned have not included how gardens improve human health, extend life spans, reduce violence and vandalism, lower blood pressure and save our society a fortune on medical and social infrastructure costs. So if urban trees and landscapes are lost because politicians don't think they are worthy of some of our resources society could pay a very high price indeed. It is lucky that as we

let the turf in our parks and ovals die that we don't have a problem with children lacking exercise and becoming obese. If we did, we might be paying a far higher price than was ever dreamed possible – society won't know what it's got till it's gone!

As the populations of Australia and its major cities continue to grow, by the year 2050, the pressure on public open space will be enormous. There will be a tendency for politicians and bureaucrats to see any open space whether public or private as ornamental and therefore ripe for development. However, these cities will only be sustainable if the open space is sufficient to balance the resource demands of a modern society.

It is often forgotten that the major cities of Australia are biodiversity hot spots (Roetman and Daniels 2008). The parks, gardens, streets and front and backyards provide a very diverse range of plant species that generate a myriad of habitats and niches for wildlife such as birds and mammals, reptiles, spiders and insects. There is also a diverse range of soil types that contribute to massive soil microflora and fauna. High density urban developments and inner city renewal make it virtually impossible to grow trees in places that were once green and leafy. The real and full costs of such developments are rarely ever calculated.

ARBORICULTURE AND URBAN FORESTRY: A MATTER OF SEMANTICS?

It is interesting that at present the phrase *urban forestry* is often used as a synonym for *arboriculture*. However, the terms do have different meanings and while the semantics may not be of interest to urban tree managers, the consequences for tree management and urban tree populations might be. It should be remembered that in Australia arboriculture and urban forestry come from different traditions that are underpinned by different, and sometimes conflicting, philosophies. Urban forestry comes from a forestry tradition of managing groups of trees for their production values, while arboriculture comes from a horticultural tradition that focuses on tree as a specimen.

Both approaches have value and application in the management of urban trees, as the discussion of the loss of urban tree cover in Balwyn and Richmond illustrates. This study used an urban forestry paradigm as well as a classic aerial forestry analytical technique. However, there is a need for a word of caution about the use of the term “urban forestry” in relation to urban trees. In focusing on the urban forest it is easy for the importance of the individual specimen to be minimized and undervalued, which could see the removal of individual trees as long as the forest is maintained. Clearly neglecting the removal of single trees could see the forest as a whole reduced as a consequence, but the arboricultural focus on the specimen ensures that the forest is undiminished.

While this paper is not the place for a lengthy discussion of the differences in the philosophies supporting *arboriculture* and *urban forestry*, it is worth remembering that they can lead to quite different outcomes in urban tree management. Both have their place and application, and at present they often aspire to the same goals in the face of climate change and urban development. However, the terms should be applied knowledgeably and in the appropriate environmental context.

WATER, DROUGHT AND CHANGED WEATHER PATTERNS

There has been huge public interest in efficient and effective water use and conservation. In many parts of south eastern Australia, restrictions to water use have been applied to urban gardens, parks and streetscapes and these have placed the vegetation under considerable stress. There have been debates about whether trees –native or exotic- should be irrigated over the summer, and suggestions that perhaps the drought should take its course and consequently trees could be left to die. This is neither asset nor environmental management! Our knowledge of trees and particularly their root biology can be applied to effective and efficient management practices.

Effective and efficient use of water is both wise and sustainable. Subsurface irrigation under mulch early in the morning provides water at a time when it is most needed by trees. They photosynthesise most in the morning and in many species stomata are often closed by about 2.00 pm especially if soil water is limiting. Furthermore for many species evapotranspiration cools them reducing the risks of heat damage especially on hot windy days, the frequency of which is likely to increase under climate change.

In most States, however, water restrictions seem to assign a low or zero value to potable water released to the environment (Fisher 2007). This ignores the economic value of the ecological services that urban vegetation provides and which can lessen the carbon footprint of cities (Fisher 2007). The water used to maintain trees and urban landscapes during drought and summer is neither wasted nor lost. It returns real economic and sustainable value in the years ahead.

Despite the current, popular view that turf and lawns are profligate water users and are unsustainable in the Australian environment, natural turf is usually a more sustainable option than sealed surfaces or artificial turf if you consider the latter's fossil fuel chemical base and imbedded energy. Turf is quite a complex ecosystem that has a significant effect on temperature and the heat island effect, and if properly managed also sequesters a considerable amount of carbon. Perhaps it is not the villain that many think it is when they consider only the water component of a more complex equation.

Consider the following scenario. In a small backyard the lawn (8 x 4m) has been replaced with artificial turf at a cost of \$6000. The owner has done so because they have heard that lawn is not good for water use or the environment. The artificial turf is made from fossil fuel, imported from overseas and has high embedded energy. The purchase and installation of a locally made 5000L tank would cost \$1200 and provide enough water for such a small lawn year round. Already the owner misses the birds that used to come fossicking in the lawn. Her local council is also replacing a turf oval, which they cannot irrigate due to local water authority restrictions, with artificial turf. They are doing so as part of their water policy. However, the product is imported with high embedded energy and carbon, and the council is not harvesting the water that runs off or passes through the new artificial turf surface. Efficient irrigation and water recycling and a water efficient native grass would be a far more sustainable option for a low use oval. The council has also used couch grass on many of its other sporting ovals, unaware that its high binding strength could cause serious knee injuries to teenage football, hockey or cricket players.

Trees and urban landscapes are assets in every sense of the word and resources for allocated for their proper and sustained management. Amongst these resources may be the need for an allocation of water, used wisely and sustainably. If the focus is solely on water such that trees and other vegetation are left to die, then consequently the carbon that they sequester would be released into the atmosphere. It has been estimated that some 10% of the inner city of Melbourne trees are drought stressed and at risk of death, and that for the city more broadly 15% of trees are at risk. Should these trees die it would represent a massive loss of sequestered carbon. Such an outcome would be environmentally irresponsible, and highlights the need for those managing urban vegetation to appreciate the larger environmental picture.

CONCLUSION

Mature trees will continue to have a significant place in urban landscapes and they must be managed to ensure that they remain healthy and fulfill the full potential of their lifespans. As climate changes, the impact of vegetation on stormwater runoff could save billions of dollars in infrastructure costs to Australia's cities. It is not economically possible to retrofit larger stormwater drains and alter the levels at which they enter waterways. However, trees hold rainwater on their canopies, and through transpiration significantly reduce the amount of water entering drains. Estimates suggest that trees may hold up to 40% of the rain water that impacts on them and that as little as 40% of water striking trees may enter drains. Furthermore, tree root systems may act as effective biofilters of the storm water before it enters watertables or river systems (Denman 2006).

Given that carbon dioxide is the most significant of the greenhouse gases, especially for the states of South Eastern Australia, and considerable electricity is derived from coal powered generators. The public is becoming increasingly aware that power generation is producing large volumes of greenhouse emissions and that the clearing of trees for powerlines and general tree pruning is reducing the level of carbon sequestered in the canopy structures of urban trees. Thus the power generating and distribution companies and authorities are compounding their contributions to the greenhouse effect and global warming. On the one hand they are major greenhouse gas emitters, and on the other they are causing significant carbon losses by their line clearing activities. Line clearing compounds the negative effects of power generation on greenhouse gas production.

Governments through their agencies are still major clearers of trees, forests and ecosystems. In most States approaches to road side vegetation at a time of climate change are inappropriate. Trees and roadside ecosystems are assets that fix carbon, provide shade, filter air and protect from wind, and provide wildlife corridors and habitat just to mention a few of the obvious benefits. Are these benefits properly costed for road related projects where a balance of safety, cost and the environment has to be achieved? It is to be hoped that an old-fashioned engineering philosophy to trees and the environment that is as inflexible as concrete is no longer the reigning paradigm at a time of climate change. However, roadside vegetation is still being cleared right across the country, despite the fact that it sequesters massive amounts of carbon that could be used to partially offset the carbon produced by the vehicles that use the roads. Once again it is clear that the real and full economics of the situation have not been properly considered.

It is highly likely that the Australian Government will become a signatory to the post-Kyoto successor. Consequently, it would seem that the present situation, which often substantially undervalues trees and urban vegetation, will change once the impact of the protocols on greenhouse gas emissions are recognized. The economic algorithms and paradigms that have applied to the management of trees and public open space in urban environments are changing rapidly. As a consequence the economic imperatives that apply to managing trees will change under a thorough cost/benefit analysis.

The future role of trees in the urban landscape, and indeed of public and private open space are being redefined by those who have little interest or expertise in urban vegetation management and are driven by other imperatives. It is time to address some of these issues before changes are made that degrade the landscape, and which could take decades to remedy. This is the century of the environment and the value of urban trees and vegetation will rise, simply because they provide more than they cost. As a truly Australian urban landscape, which values trees and recognizes aridity and changed climate, emerges, it will be understood that urban trees and landscapes are worth much more than they cost and that they are the keys to urban sustainability.

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8.3 City Planning - Advertising Report
File Ref: F22/37428

Memorandum of the Director City Life of 27 April 2022 and attachment.

Delegation: Committee



City of **HOBART**

MEMORANDUM: CITY PLANNING COMMITTEE

City Planning - Advertising Report

Attached is the advertising list for the period 5 April 2022 to 26 April 2022.

RECOMMENDATION

That:

- 1. The information be received and noted.***

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

Neil Noye
DIRECTOR CITY LIFE

Date: 27 April 2022
File Reference: F22/37428

Attachment A: City Planning - Advertising Report ↓

Application	Street	Suburb	Development	Works Value	Expiry Date	Referral	Proposed Delegation	Advertising Period Start	Advertising Period End
PLN-19-701	121 GOULBURN STREET	WEST HOBART	Partial Demolition, Alterations, Driveway, Front Fencing and Change of Use to Visitor Accommodation	\$0	09/05/2022	ayersh	Director	08/04/2022	26/04/2022
PLN-21-508	6 AGENA RETREAT	LENAH VALLEY	Dwelling	\$235,000	04/05/2022	ayersh	Director	20/04/2022	05/05/2022
PLN-21-650	1 BUTTERWORTH STREET	WEST HOBART	Partial Demolition, Alterations, Extension, and Front Fencing	\$120,000	08/05/2022	ayersh	Director	20/04/2022	05/05/2022
PLN-21-739	26 RATHO STREET	LENAH VALLEY	Partial Demolition, Alterations and Extension	\$250,000	08/05/2022	ayersh	Director	20/04/2022	05/05/2022
PLN-21-416	44 SUMMERHILL ROAD	WEST HOBART	Subdivision (One Additional Lot)	\$20,000	08/05/2022	baconr	Director	07/04/2022	24/04/2022
PLN-22-68	6 CATO AVENUE	WEST HOBART	Partial Demolition, Alterations and Extension	\$25,000	16/05/2022	baconr	Director	14/04/2022	02/05/2022
PLN-22-129	155 - 165 ARGYLE STREET	HOBART	Partial Change of Use to Storage and Front Fencing	\$4,000	28/04/2022	ikinb	Director	05/04/2022	22/04/2022

Application	Street	Suburb	Development	Works Value	Expiry Date	Referral	Proposed Delegation	Advertising Period Start	Advertising Period End
PLN-21-763	78 ALEXANDER STREET AND 12 FRENCH STREET	SANDY BAY	Partial Demolition, Alterations, Extension, Two Multiple Dwellings (One Existing, One New), and Associated Hydraulic Infrastructure	\$750,000	30/04/2022	langd	Council	11/04/2022	29/04/2022
PLN-22-112	60 LIVERPOOL STREET	HOBART	Alterations and Signage	\$0	11/05/2022	langd	Director	12/04/2022	30/04/2022
PLN-22-65	424 STRICKLAND AVENUE	SOUTH HOBART	Dwelling	\$980,000	05/05/2022	langd	Director	14/04/2022	02/05/2022
PLN-22-163	16 OAKLEY STREET	NEW TOWN	Alterations	\$14,844	08/06/2022	langd	Director	14/04/2022	02/05/2022
PLN-21-578	12 WARWICK STREET	HOBART	Partial Demolition, Alterations, Extension, and Change of Use to Dwelling and New Dwelling	\$500,000	14/05/2022	langd	Director	20/04/2022	05/05/2022
PLN-22-133	106 CASCADE ROAD	SOUTH HOBART	Temporary Housing	\$0	20/05/2022	langd	Council	21/04/2022	06/05/2022
PLN-22-142	12 ANDREW STREET	NORTH HOBART	Partial Demolition, Alterations, and Extension	\$400,000	29/04/2022	maxwellv	Director	05/04/2022	22/04/2022

Application	Street	Suburb	Development	Works Value	Expiry Date	Referral	Proposed Delegation	Advertising Period Start	Advertising Period End
PLN-22-51	95 HAMPDEN ROAD	BATTERY POINT	Signage and Alteration to Operating Hours	\$2,300	27/04/2022	maxwellv	Council	07/04/2022	24/04/2022
PLN-22-102	650 SANDY BAY ROAD	SANDY BAY	Alterations	\$1,500	06/05/2022	maxwellv	Director	07/04/2022	24/04/2022
PLN-21-691	6 RUSSELL CRESCENT	SANDY BAY	Partial Demolition, Alterations, and Partial Change of Use to Food Services	\$950,000	24/05/2022	maxwellv	Director	21/04/2022	06/05/2022
PLN-22-107	18 - 20 ASHFIELD STREET	SANDY BAY	Partial Demolition, Alterations, Front Fencing, and Signage	\$65,000	18/05/2022	maxwellv	Director	22/04/2022	07/05/2022
PLN-22-202	5 RANDALL STREET	SANDY BAY	Change of Use to Visitor Accommodation	\$0	27/06/2022	maxwellv	Director	22/04/2022	07/05/2022
PLN-22-130	3 / 180 - 184 BATHURST STREET	HOBART	Change of Use to Visitor Accommodation	\$0	17/05/2022	mcclenahanm	Director	08/04/2022	26/04/2022
PLN-21-801	30 HAMILTON STREET	WEST HOBART	Swimming Pool	\$95,000	06/05/2022	mcclenahanm	Director	12/04/2022	30/04/2022
PLN-21-793	5 CLARENDON STREET	NEW TOWN	Dwelling and Front Fencing	\$345,000	10/05/2022	mcclenahanm	Director	20/04/2022	05/05/2022

Application	Street	Suburb	Development	Works Value	Expiry Date	Referral	Proposed Delegation	Advertising Period Start	Advertising Period End
PLN-21-771	26 ROSEHILL CRESCENT	LENAH VALLEY	Partial Demolition, Alterations, Extension, Swimming Pool, Garage and Alterations to Driveway	\$500,000	07/05/2022	mcclenahanm	Director	20/04/2022	05/05/2022
PLN-22-196	20 MCVILLY DRIVE	HOBART	Signage	\$0	17/05/2022	mcclenahanm	Director	20/04/2022	05/05/2022
PLN-22-119	69 GOULBURN STREET	HOBART	Change of Use to Visitor Accommodation	\$0	10/05/2022	ockendenl	Director	06/04/2022	23/04/2022
PLN-22-200	4 MITAH CRESCENT	SANDY BAY	Change of Use to Visitor Accommodation	\$1,000	17/05/2022	ockendenl	Director	22/04/2022	26/04/2022
PLN-22-220	63 DAVEY STREET	HOBART	Partial Demolition and Alterations	\$50,000	23/05/2022	ockendenl	Director	22/04/2022	07/05/2022
PLN-21-857	133 NELSON ROAD	MOUNT NELSON	Partial Demolition, Alterations and Extension	\$790,000	09/05/2022	sherriffc	Director	05/04/2022	22/04/2022
PLN-21-704	19 BEACH ROAD	SANDY BAY	Alterations	\$35,523	16/05/2022	sherriffc	Council	07/04/2022	24/04/2022
PLN-22-64	520 SANDY BAY ROAD	SANDY BAY	Partial Demolition, Alterations and Partial Change of Use to Dwelling	\$900,000	13/05/2022	sherriffc	Director	08/04/2022	26/04/2022

Application	Street	Suburb	Development	Works Value	Expiry Date	Referral	Proposed Delegation	Advertising Period Start	Advertising Period End
PLN-22-176	9 BOA VISTA ROAD	NEW TOWN	Carport	\$15,000	12/05/2022	sherriffc	Director	11/04/2022	29/04/2022
PLN-21-479	19 ABERDEEN STREET	GLEBE	Partial Demolition, Alterations and Extension	\$100,000	20/05/2022	sherriffc	Director	12/04/2022	30/04/2022
PLN-22-192	1-3 O'CONOR COURT SANDY BAY TAS 7005 (CT 174882/1)	SANDY BAY	Partial Demolition and Outbuilding (Garage and Workshop)	\$19,000	16/05/2022	sherriffc	Director	12/04/2022	30/04/2022
PLN-22-184	57 ELIZABETH STREET	HOBART	Signage	\$2,000	18/05/2022	sherriffc	Director	14/04/2022	02/05/2022
PLN-21-883	44 QUAYLE STREET	SANDY BAY	Partial Demolition and Two Multiple Dwellings (One Existing, One New)	\$500,000	30/05/2022	smeea	Director	08/04/2022	26/04/2022
PLN-22-83	2 / 678 HUON ROAD	FERN TREE	Alterations to Previously Approved Multiple Dwelling	\$100,000	18/06/2022	smeea	Director	22/04/2022	26/04/2022
PLN-22-131	23 SALVATOR ROAD	WEST HOBART	Change of Use to Visitor Accommodation	\$0	12/05/2022	widdowsont	Director	06/04/2022	23/04/2022
PLN-22-28	121 HARRINGTON STREET	HOBART	Partial Demolition, Extension, Alterations & Decks	\$100,000	27/04/2022	widdowsont	Director	11/04/2022	29/04/2022

Application	Street	Suburb	Development	Works Value	Expiry Date	Referral	Proposed Delegation	Advertising Period Start	Advertising Period End
PLN-22-188	34 MONTAGU STREET	LENAH VALLEY	Partial Demolition and Alterations	\$8,000	13/05/2022	widdowsont	Director	14/04/2022	02/05/2022
PLN-22-168	183 - 185 SANDY BAY ROAD	SANDY BAY	Signage	\$0	15/05/2022	widdowsont	Director	22/04/2022	07/05/2022
PLN-22-186	2 BROMBY STREET	NEW TOWN	Partial Demolition and Alterations	\$9,500	19/05/2022	widdowsont	Director	22/04/2022	07/05/2022

8.4 Delegated Decision Report (Planning)
File Ref: F22/39518

Memorandum of the Director City Life of 2 May 2022 and attachment.

Delegation: Committee



City of **HOBART**

MEMORANDUM: CITY PLANNING COMMITTEE

Delegated Decision Report (Planning)

Attached is the delegated planning decisions report for the period 20 April 2022 to 29 April 2022.

RECOMMENDATION

That:

- 1. The information be received and noted.***

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

Neil Noye
DIRECTOR CITY LIFE

Date: 2 May 2022
File Reference: F22/39518

Attachment A: Delegated Decision Report (Planning) ↓

2 May 2022

Delegated Decisions Report (Planning)

14 applications found.

				Approved	All
Planning Description	Address	Works Value	Decision	Authority	
PLN-21-17 Dwelling	24 TABART STREET NEW TOWN TAS 7008	\$ 500,000	Approved	Delegated	
PLN-21-628 Subdivision (One Additional Lot)	32 GRAYLING AVENUE SOUTH HOBART TAS 7004	\$ 0	Approved	Delegated	
PLN-21-795 Dwelling	13 RUSHWOOD COURT LENA VALLEY TAS 7008	\$ 600,000	Approved	Delegated	
PLN-21-858 Partial Demolition, Alterations, Three Multiple Dwellings (Two Existing, One New), Alterations to Car Parking, and Fencing	4 MIDWOOD STREET NEW TOWN TAS 7008	\$ 200,000	Approved	Delegated	
PLN-21-873 Partial Demolition, Alterations, Extension, Carport and Workshop	7 NORFOLK CRESCENT SANDY BAY TAS 7005	\$ 500,000	Approved	Delegated	
PLN-22-105 Dwelling and Associated Works	649B NELSON ROAD MOUNT NELSON TAS 7007	\$ 120,000	Approved	Delegated	
PLN-22-129 Partial Change of Use to Storage and Front Fencing	155-165 ARGYLE STREET HOBART TAS 7000	\$ 4,000	Approved	Delegated	
PLN-22-142 Partial Demolition, Alterations, and Extension	12 ANDREW STREET NORTH HOBART TAS 7000	\$ 400,000	Approved	Delegated	
PLN-22-152 Change of Use to Visitor Accommodation	41 LOCHNER STREET WEST HOBART TAS 7000	\$ 0	Approved	Delegated	
PLN-22-156 Partial Demolition, Alterations, and Extension	72 KING STREET SANDY BAY TAS 7005	\$ 35,000	Approved	Delegated	
PLN-22-159 Change of Use to Visitor Accommodation	3/37 REGENT STREET SANDY BAY TAS 7005	\$ 0	Approved	Delegated	
PLN-22-160 Change of Use to Visitor Accommodation	4 WIGNALL STREET NORTH HOBART TAS 7000	\$ 0	Approved	Delegated	
PLN-22-202 Change of Use to Visitor Accommodation	5 RANDALL STREET SANDY BAY TAS 7005	\$ 0	Approved	Delegated	
PLN-22-37 Carport	220 CAMPBELL STREET NORTH HOBART TAS 7000	\$ 10,000	Approved	Delegated	

9. QUESTIONS WITHOUT NOTICE

Section 29 of the *Local Government (Meeting Procedures) Regulations 2015*.
File Ref: 13-1-10

An Elected Member may ask a question without notice of the Chairman, another Elected Member, the Chief Executive Officer or the Chief Executive Officer's representative, in line with the following procedures:

1. The Chairman will refuse to accept a question without notice if it does not relate to the Terms of Reference of the Council committee at which it is asked.
2. In putting a question without notice, an Elected Member must not:
 - (i) offer an argument or opinion; or
 - (ii) draw any inferences or make any imputations – except so far as may be necessary to explain the question.
3. The Chairman must not permit any debate of a question without notice or its answer.
4. The Chairman, Elected Members, Chief Executive Officer or Chief Executive Officer's representative who is asked a question may decline to answer the question, if in the opinion of the respondent it is considered inappropriate due to its being unclear, insulting or improper.
5. The Chairman may require a question to be put in writing.
6. Where a question without notice is asked and answered at a meeting, both the question and the response will be recorded in the minutes of that meeting.
7. Where a response is not able to be provided at the meeting, the question will be taken on notice and
 - (i) the minutes of the meeting at which the question is asked will record the question and the fact that it has been taken on notice.
 - (ii) a written response will be provided to all Elected Members, at the appropriate time.
 - (iii) upon the answer to the question being circulated to Elected Members, both the question and the answer will be listed on the agenda for the next available ordinary meeting of the committee at which it was asked, where it will be listed for noting purposes only.

10. CLOSED PORTION OF THE MEETING

That the Committee resolve by majority that the meeting be closed to the public pursuant to regulation 15(1) of the *Local Government (Meeting Procedures) Regulations 2015* because the items included on the closed agenda contain the following matters:

- Closed portion of the meeting
- Questions without notice in the Closed portion
- Planning Appeal

The following items were discussed: -

- | | |
|----------------|---|
| Item No. 1 | Minutes of the last meeting of the Closed Portion of the Committee Meeting |
| Item No. 2 | Consideration of supplementary items to the agenda |
| Item No. 3 | Indications of pecuniary and conflicts of interest |
| Item No. 4 | Planning Authority Items – Consideration of Items with Deputations |
| Item No. 5 | City Acting as Planning Authority |
| Item No. 5.1 | Applications under the Hobart Interim Planning Scheme 2015 |
| Item No. 5.1.1 | PLN-21-782 1/4 Sunvale Avenue, Sandy Bay - Partial Change of Use to Visitor Accommodation
LG(MP)R 15(4)(a) |
| Item No. 6 | Questions Without Notice |