



CITY OF HOBART

AGENDA

City Planning Committee Meeting

Open Portion

Monday, 2 November 2020

at 5:00 pm

SUPPLEMENTARY ITEMS

ORDER OF BUSINESS

COMMITTEE ACTING AS PLANNING AUTHORITY 4

APPLICATIONS UNDER THE HOBART INTERIM PLANNING SCHEME 2015

- 11 45 Elizabeth Street, Hobart and Adjacent Road Reserve - Partial
Demolition, Alterations, Extension and Change of Use to General
Retail Hire and Five Multiple Dwellings 5
- 12 63 Davey Street, Hobart - Demolition, New Building for 30 Multiple
Dwellings and 21 Student Accommodation Units including
Carparking, and Associated Infrastructure and Access Works - PLN-
19-319..... 174
- 13 19 Allison Street, West Hobart - Partial Demolition and Alterations 518

14	42-44 Burnett Street, North Hobart - PLN-20-633	553
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The Acting General Manager reports:

“That in accordance with the provisions of Part 2 Regulation 8(6) of the *Local Government (Meeting Procedures) Regulations 2015*, these supplementary matters are submitted for the consideration of the Committee.

Pursuant to Regulation 8(6), I report that:

- (a) information in relation to the matter was provided subsequent to the distribution of the agenda;
- (b) the matter is regarded as urgent; and
- (c) advice is provided pursuant to Section 65 of the Act.”

COMMITTEE ACTING AS PLANNING AUTHORITY

In accordance with the provisions of Part 2 Regulation 25 of the *Local Government (Meeting Procedures) Regulations 2015*, the intention of the Committee to act as a planning authority pursuant to the *Land Use Planning and Approvals Act 1993* is to be noted.

In accordance with Regulation 25, the Committee will act as a planning authority in respect to those matters appearing under this heading on the agenda, inclusive of any supplementary items.

The Committee is reminded that in order to comply with Regulation 25(2), the General Manager is to ensure that the reasons for a decision by a Council or Council Committee acting as a planning authority are recorded in the minutes.




**11 45 ELIZABETH STREET, HOBART AND ADJACENT ROAD RESERVE -
PARTIAL DEMOLITION, ALTERATIONS, EXTENSION AND CHANGE OF
USE TO GENERAL RETAIL HIRE AND FIVE MULTIPLE DWELLINGS
PLN-20-524 - FILE REF: F20/116130**


Address:	45 Elizabeth Street, Hobart and Adjacent Road Reserve
Proposal:	Partial Demolition, Alterations, Extension and Change of Use to General Retail Hire and Five Multiple Dwellings
Expiry Date:	11 November 2020
Extension of Time:	Not applicable
Author:	Victoria Maxwell

RECOMMENDATION

That pursuant to the *Hobart Interim Planning Scheme 2015*, the Council refuse the application for partial demolition, alterations, extension and change of use to general retail and hire and five multiple dwellings at 45 ELIZABETH STREET HOBART TAS 7000 for the following reasons:

- 1 The proposal does not meet the acceptable solution or the performance criterion with respect to clause E13.7.1 P1 (a) and (b) of the Historic Heritage Code of the Hobart Interim Planning Scheme 2015 because the proposed demolition will result in the loss of early significant fabric and items (signage and architectural details) that contribute to the historic heritage significance of a heritage listed place.
- 2 The proposal does not meet the acceptable solution or the performance criterion with respect to clause E17.7.2 P1 (a) to (i) of Signs Code of the Hobart Interim Planning Scheme 2015 because the proposed demolition will result in the loss of early significant fabric and items (signage) that contribute to the historic heritage significance of a heritage listed place.

Attachment A:	PLN-20-524 - 45 ELIZABETH STREET HOBART TAS 7000 - Planning Committee or Delegated Report ↓ 
Attachment B:	PLN-20-524 45 ELIZABETH STREET HOBART TAS 7000 - CPC Agenda Documents ↓ 
Attachment C:	PLN-20-524 45 ELIZABETH STREET HOBART TAS 7000_ Consultant Heritage Officer Report - Final 20201029 ↓ 

Attachment D: PLN-20-524 45 ELIZABETH STREET HOBART TAS 7000 -
CPC Supporting Documents ↓ 

**APPLICATION UNDER HOBART INTERIM PLANNING SCHEME 2015**

Type of Report:	Committee
Council:	9 November 2020
Expiry Date:	11 November 2020
Application No:	PLN-20-524
Address:	45 ELIZABETH STREET , HOBART ADJACENT ROAD RESERVE
Applicant:	(Giameos Constructions & Developments Pty Ltd) C/- All Urban Planning Pty Ltd 19 Mawhera Avenue
Proposal:	Partial Demolition, Alterations, Extension and Change of Use to General Retail and Hire and Five Multiple Dwellings
Representations:	2 Representations
Performance criteria:	Historic Heritage Code - Heritage Place - demolition and works other than demolition Central Business zone - Building Height Signs Code - Sign on heritage property

1. Executive Summary

- 1.1 Planning approval is sought for Partial Demolition, Alterations, Extension and Change of Use to General Retail and Hire and Five Multiple Dwellings at 45 ELIZABETH STREET HOBART TAS 7000.

1.2 More specifically the proposal includes:

- partial change of use from General Retail and Hire (previous photographic shop and laboratories) to five (5) apartments on the existing first to fifth floors with an extension to create a sixth floor,
- the basement fronting Kempt Street will be converted to storage compartments for the five apartments, bike and rubbish storage, as well as the main lobby and entrance to lift and stair well,
- the ground floor (Elizabeth Street side) retains a commercial tenancy,
- the apartments on levels 1 to 3 will be three (3) bedroom and two (2) bathroom, with one (1) apartment per floor, extending the length of the building and wrapping around the new lift shaft and stair well,
- on level 4, through the demolition of a portion of the concrete floor and roof, the two storey apartment 4 will be created with two (2) bedrooms and two (2) bathrooms within the height of that floor (creating levels 4 and 5),
- apartment 5 spans the rear of Levels 4, 5 and 6 with the construction of the additional sixth floor,
- with Level 4 access stairs to apartment 5, the two storey living space will be canter-levered out over the Kemp Street road reserve,
- the existing lift and internal stairs will be removed and relocated to comply with contemporary building standards,
- installation of new windows on Kemp Street facade for each floor,
- new side windows on levels 1 to 4 north west elevation,
- demolition and replacement of the rear steel external fire stairs located in the Kemp Street road reserve,
- decorative steel cladding attached to the new external fire stair,
- demolition of internal walls,
- two storey void to create a deck behind the Kodak sign on the Elizabeth Street frontage,
- new side openings on levels 4 and 5 North East and North West elevations, requiring removal of previous painted wall signs and illuminated Kodak signs,
- demolition of the *Kodak House* masonry sign on level 4,
- installation of a steel replica *Kodak House* sign,
- removal of lift roof plant structure and replacement with mechanical plant above sixth floor extension and excavation in the basement for lift infrastructure,
- removal and relocation of stormwater pit in Kemp Street road reserve.

1.3 The proposal relies on performance criteria to satisfy the following standards and codes:

- 1.3.1 Zone Development Standards - Height
- 1.3.2 Historic Heritage Code - Heritage Place
- 1.3.3 Signs code

- 1.4 Two (2) representations objecting to the proposal were received within the statutory advertising period between 22nd September and 6th October 2020.
- 1.5 The proposal is recommended for refusal.
- 1.6 The final decision is delegated to the Council, because the application is recommended for refusal.

2. Site Detail

- 2.1 The subject site is located on the north eastern side of the Elizabeth Street Mall, between the same and Kemp Street in the central city. Directly adjacent development are predominantly two to three storey commercial buildings, with taller buildings of similar or greater number of floors located within the surrounding city block. The building was constructed for Kodak as an unusual tall and narrow building in the 1920s and is notable for its bright yellow paint finish and unusual proportions.

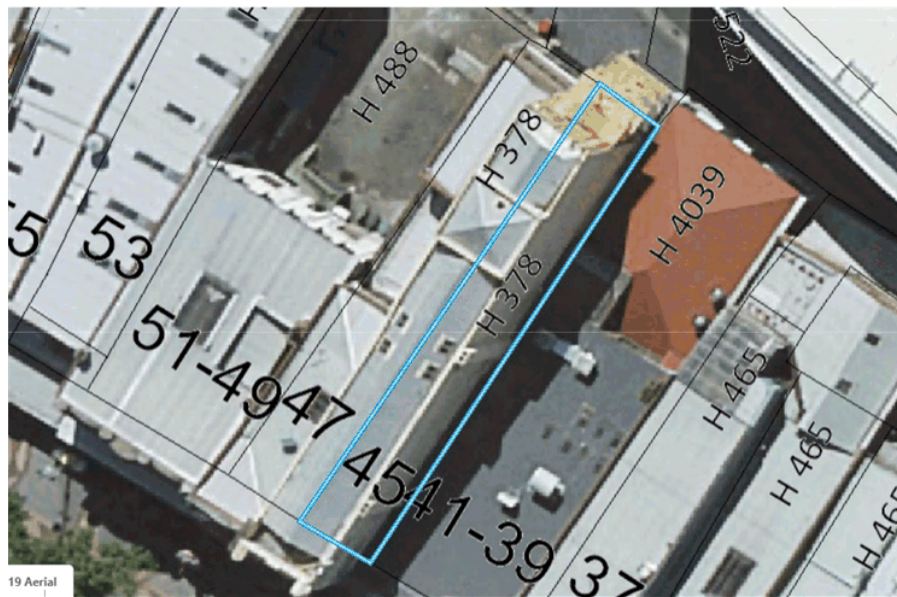


Figure 1: Site Plan (Geo Cortex, 2020)

- 2.2 The primary facade in Elizabeth Street contains what appear to be original windows and associated hardware. The facade itself is a traditional and ornamented design. In 1929 the Kodak company extended the building adding the current 4th floor. The construction methodology and details of the upper level are thus slightly different from the lower floors. This 1929 extension has a substantial masonry Kodak House identifying sign. In the latter part of the 20th century the Kodak company added illuminated neon signs and also painted signs to the exterior of the building.

- 2.3 The unusual proportions, the yellow tone and the various signs of Kodak house make it distinctive in Central Hobart and identifiable with other historic Kodak buildings around Australia.

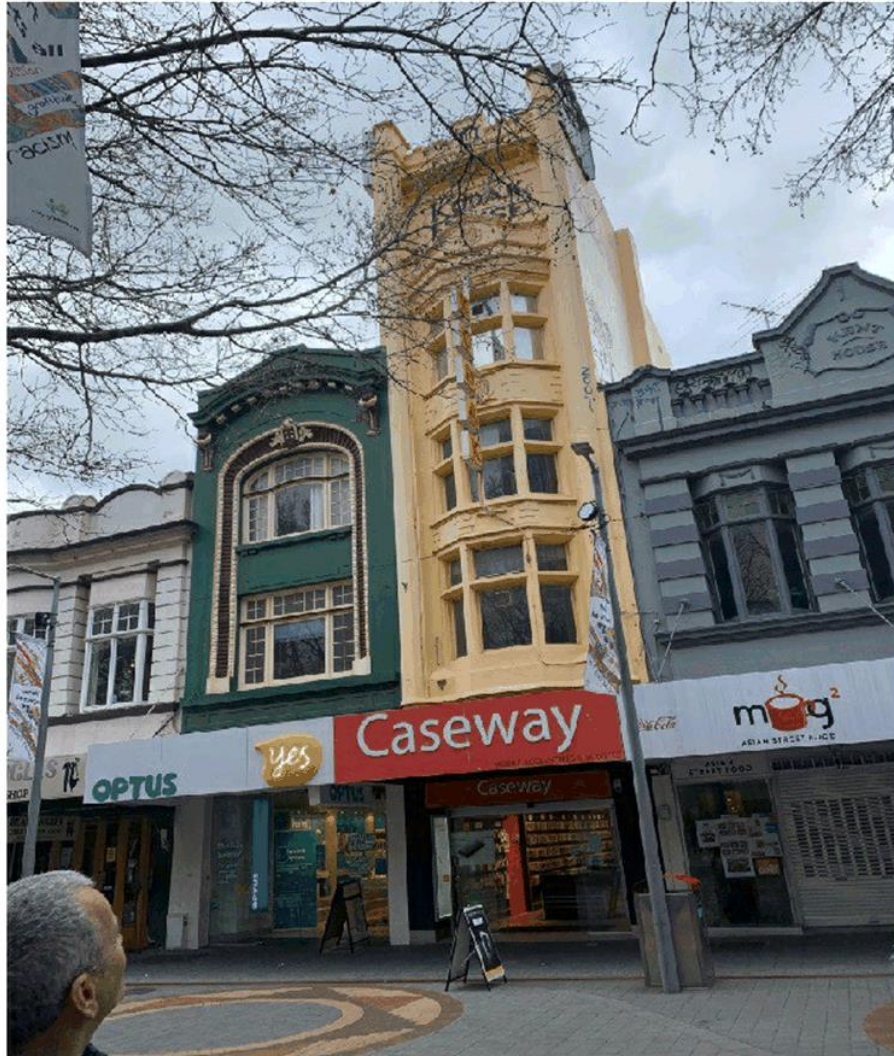


Figure 2: Elizabeth Street (mall) elevation (Officer photo, 2020_

- 2.4 The rear elevation contains an external fire stair which extends into the Kemp Street road reserve. The difference in height between Elizabeth Street and Kemp Streets is such that street level at Elizabeth Street is actually a floor above ground at the Kemp Street frontage, making the basement for the building street level for that rear street. Reference throughout this report will be ground level at Elizabeth Street. Part of the reason for the significant difference in floor levels is the depression in which the property sits in Kemp Street. The site is subject to flooding from the nearby subterranean Hobart Rivulet, which flows under Wellington Court to the north. In previous flood events, the basement area has been known to be subject to inundation and Council responsible for replacement cost for damaged stock. Because the basement level is only to be used for storage and lobby, this inundation concern has not proved fatal to future development.



Figure 3: Kemp Street elevation (officer photo, 2020)

- 2.5 The interior layout reflect previous office uses in the first and second floors, with photographic laboratories located on the upper floors. A disused elevator is located towards the rear of the building and the internal staircase wraps around the lift shaft. North facing windows sit centrally in the middle floors, providing light to the the building, whilst large bay windows at the front of the building provide virtually floor to ceiling western light. Internal dividing walls and lowered false ceiling divide the main area on each floor between the lift and Elizabeth Street. There is an east facing room with external access on the fire stairs with a window fronting Kemp Street on each floor.

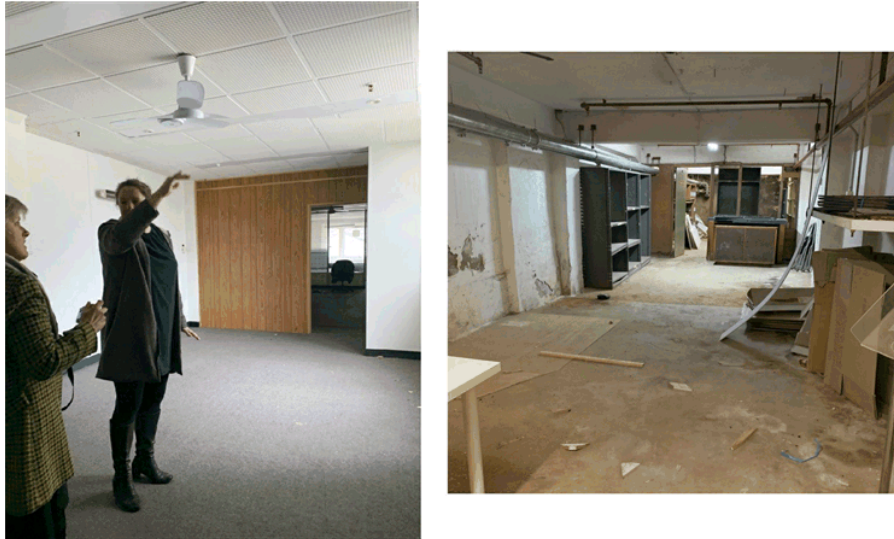


Figure 4: Internal views of level 1 and basement (officer photo, 2020)

- 2.6 Level 4, which is located behind the Kodak house masonry sign, does not have west facing windows and relies on skylights for light. The front portion still retains remnants of the dark rooms and processing laboratory equipment.

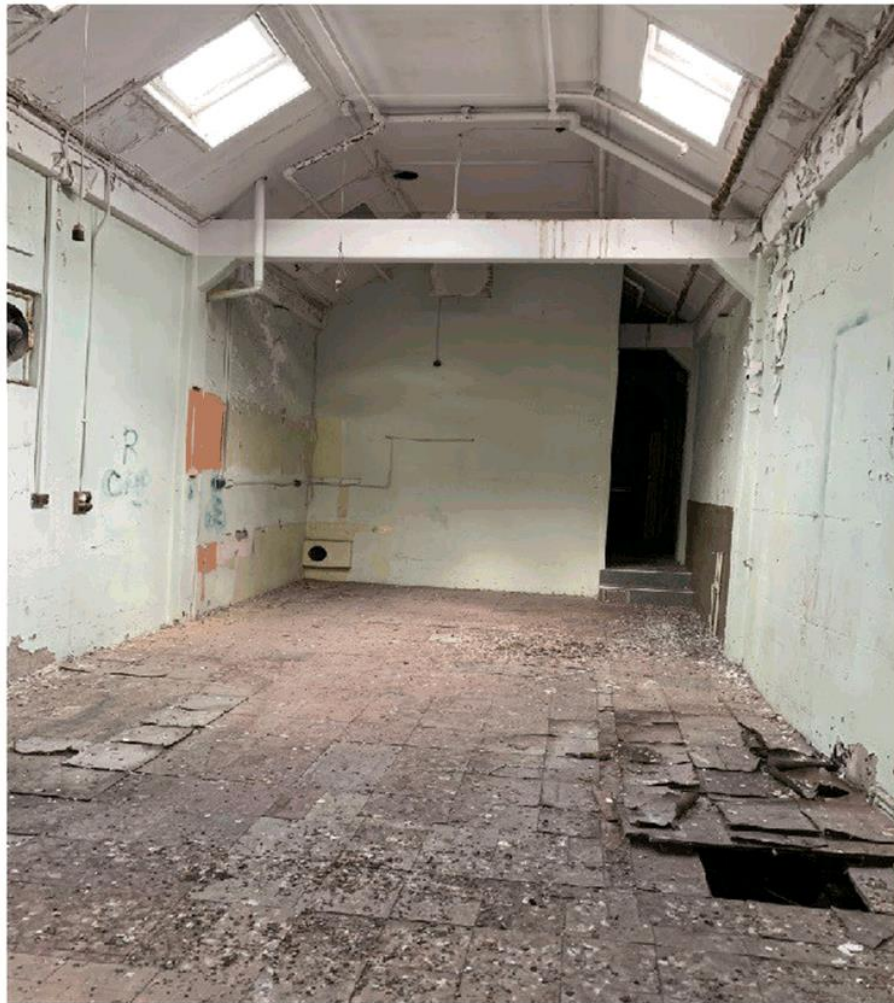


Figure 5: Level 4 (floor to be lowered and roof to be removed (Officer photo, 2020)

3. Proposal

- 3.1 Planning approval is sought for Partial Demolition, Alterations, Extension and Change of Use to General Retail and Hire and Five Multiple Dwellings at 45 ELIZABETH STREET HOBART TAS 7000.

3.2 More specifically the proposal includes:

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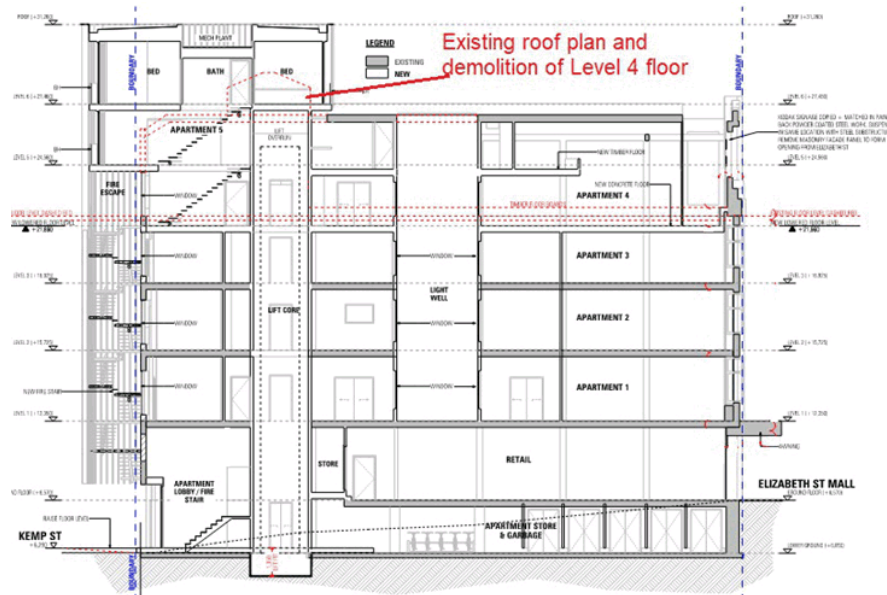


Figure 6: Section plan (annotated) showing proposed uses and demolition (Jaws, 2020)

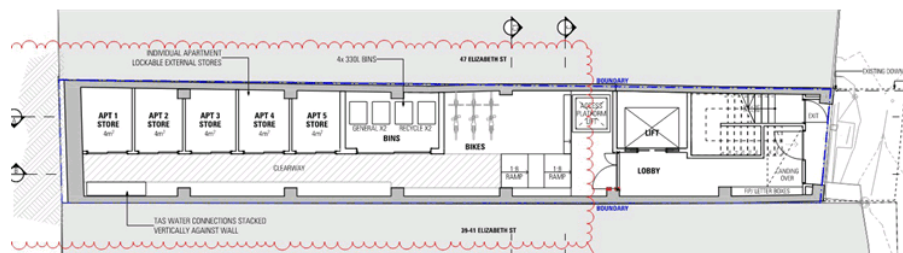


Figure 7: Basement floor plan (note this is ground level on Kemp St) (JAWS 2020)

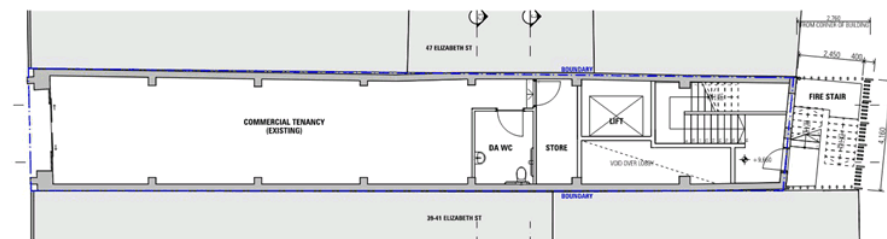


Figure 8: Ground Floor plan (Elizabeth St level) (JAWS, 2020)

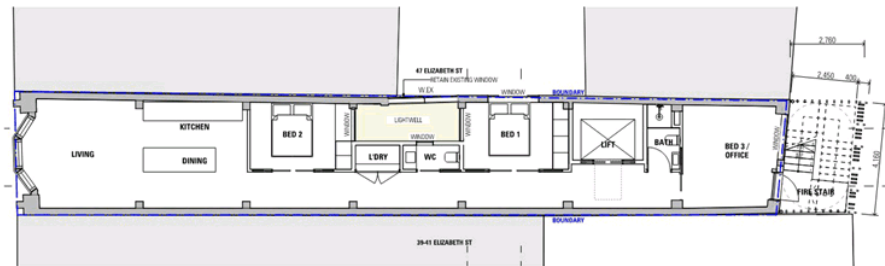


Figure 9: Levels 1 to 3 Floor plan (JAWS, 2020)

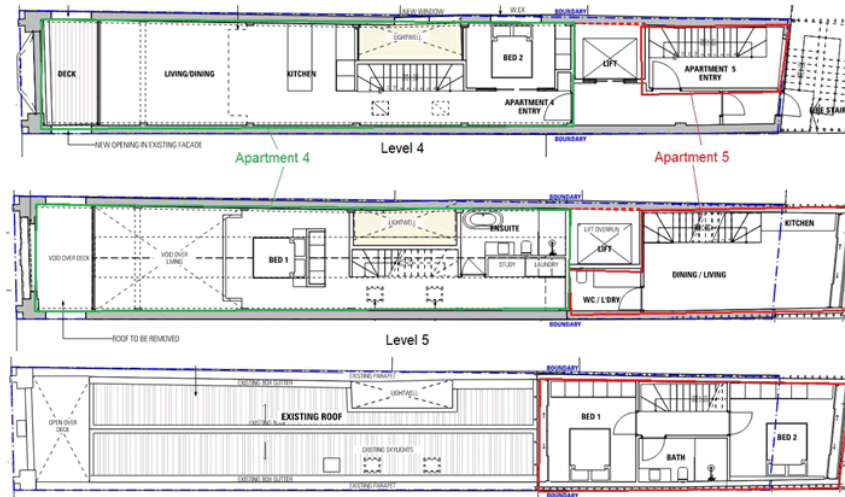


Figure 10: Annotated plan of Level 4 alterations to create 2 floors within and addition of level 6 for Apartments 4 and 6 (JAWS, 2020)

4. Background

- 4.1 The building was initially constructed in the early 1920 as a photographic laboratory and shop. The Kodak sign has been prominent in the facades since construction. The Elizabeth St facade was raised in the following decade to create the turret fenestrations and Kodak masonry sign visible today. Later in the 20th century illuminated signs were installed on the side elevations, which have since fallen into disrepair.

- 4.2 PLN-901024 - Shop alterations
 PLN-971144 - Illuminated vertically projecting wall signs
 PLN-991055 - Shop Front
 PLN-03-1726-01 - Offices and Demolition of Stair on Kemp St - Expired
 PLN-05-00149 - 01 internally illuminated signs and painting of Vodafone logo - exempt but required Kodak plaster lettering and crest to be retained - Expired
 PLN-10-00711-01 - Minor internal works and upgrade of existing shop signage
 PAE-20-140 - pre-lodgment advice for the current proposal.

The site has been subject to flood inundation from the Hobart Rivulet in the past. Correspondence from 1996 was found where Council reimbursed the land owner for damaged stock stored in the basement.

5. Concerns raised by representors

- 5.1 Two (2) representations objecting to the proposal were received within the statutory advertising period between 22nd September and 6th October 2020.
- 5.2 The following table outlines the concerns raised in the representations received. Those concerns which relate to a discretion invoked by the proposal are addressed in Section 6 of this report.

The application should have been accompanied by a Conservation Plan, Heritage Impact Statement or at least a Statement of Significance as required by E 13.5 of the planning scheme, to assess the cultural significance of the existing place and outline measures for the retention and protection. This would have been more useful than the Archaeological report, avoiding the fatal flaw in the current design.
Supports the adaptation of an existing building to residential units, but only where minimal impact on the cultural significance occurs as dictated by the Burra Charter Article 21.1.
The current design introduces unacceptable loss of heritage fabric.
The planning report articulates the iconic Kodak signage, representing the cultural heritage and story of the building. In any assessment of heritage significance, the importance of the "Kodak House" building name would be paramount. The lettering, arrangement, materials and placement all relate to the period of its construction and provide a "voice" in the lettering.
The current proposal underestimates the integral importance of the building signage. It is not acceptable to cut a new opening into the building facade.

The Desired Future Character Statement 22.1.3.1 (d) of the Central Business zone requires historic cultural heritage values of places to be protected. Building height is addressed in Clause 22.4.1 P1.2(f), which requires assessment against the degree of consistency with the desired future character statements. The degree of consistency is inadequate, given the significance, configuration and heritage value of the building name. The currently proposed increase in height will have a direct negative impact on the protection of heritage values - failing to comply with clause 22.4.1 P1.2 (f)
The proposal fails to comply with E 13.7.1 P1 and E 13.7.2 P1, P2, P3 and P4 in that the demolition must not result in the loss of historic cultural heritage values unless there are exceptional circumstance. There are no exceptional circumstances to warrant the destruction of the facade panel with the Kodak House lettering.
In response to E 13.7.1 P1 the proposal will result in the loss of the building sign and masonry substrate, failing E 13.7.1 P1.
The proposal is not undertaken in sympathetic manner, causing a loss of historic cultural heritage significance, simply to enhance the convenience and amenity of an additional upper level unit, which is contrary to E 13.7.2 P1, due to the loss of historic cultural heritage significance - namely the upper level façade panel with its integral building name signage.
The heritage values need to take priority, and the proposed development modified to accommodate the limitations of the existing fabric, rather than the other way around.
The proposal fails to comply with E13.7.2 P2 as the development has not been designed to be subservient and complementary to the place, especially in relation to materials and fenestration.
The proposed cladding will be an unattractive addition when viewed from areas such as Collins Street and Elizabeth Street Mall.
The proposal fails to comply with E13.7.2 P3 as the materials and fenestration do not respond in a positive manner to the dominant heritage characteristics of the place.
The proposal fails to comply with E13.7.2 P4, as the proposed extension has resulted in an architectural solution that has a directly and irreversibly negative impact upon the historic cultural heritage significance of the place, through loss of building fabric and elements which contribute to that significance.
The proposed new replacement sign, to supposedly perpetuate the 'memory' of Kodak is located in a manner that has a severe negative impact upon the significance of the building, and does not minimise that impact as required by E17.7.2 (a).

The proposed replacement sign will involve the creation of a new void in the upper level façade and the destruction of the existing building name – the new sign will not be placed so as to allow the architectural details of the building to remain prominent, as required by E17.7.2 (b).

The existing historic building name signage is to be removed. This lettering is an integral part of the building's architectural detailing and cultural heritage value. The proposal involves the complete loss of this element and associated building fabric. E17.7.2 (e) requires that new signs not dominate or obscure historic signs. This proposal goes one step further – it demolishes it completely and replaces it with a so-called 'memory'.

Any approval of the proposal should be subject to the retention of the existing Kodak House building name, or the proposal be amended. If this is not possible, the application must then be refused.

Comments in the planning report regarding a previous approval 15 years ago, prior to the building's heritage listing are irrelevant.

6. Assessment

- 6.1 The *Hobart Interim Planning Scheme 2015* is a performance based planning scheme. To meet an applicable standard, a proposal must demonstrate compliance with either an acceptable solution or a performance criterion. Where a proposal complies with a standard by relying on one or more performance criteria, the Council may approve or refuse the proposal on that basis. The ability to approve or refuse the proposal relates only to the performance criteria relied on.
- 6.2 The site is located within the Central Business zone of the *Hobart Interim Planning Scheme 2015*.
- 6.3 The existing use is General Retail and Hire. The proposed use is General Retail and Hire and Five Multiple Dwellings. The existing use is a Permitted use in the zone. The proposed uses are both Permitted uses in the zone.
- 6.4 The proposal has been assessed against:
 - 6.4.1 Part D - 22.0 Central Business Zone
 - 6.4.2 E6.0 Parking and Access Code
 - 6.4.3 E7.0 Stormwater Management Code

- 6.4.4 E13.0 Historic Heritage Code
- 6.4.5 E 17.0 Signs Code
- 6.5 The proposal relies on the following performance criteria to comply with the applicable standards:
- 6.5.1 Central Business Zone Development Standards:-
Height - clauses 22.4.1 P1.2 and P5
- 6.5.2 Historic Heritage Code:
Heritage Place - Demolition E 13.7.1 P1.
Heritage Place - Works other than demolition E 13.7.2 P1, P2, P3 and P4.
- 6.5.3 Signage Code:-
Standards for signs on Heritage places - E 17.7.2 P1
- 6.6 Each performance criterion is assessed below.
- 6.7 Central Business zone - Height D 22.4.1 P1
- 6.7.1 The acceptable solution at clause 22.4.1 requires building height in the Central Business Core Area to be no more than 15m if on or within 15m of a south west or south east facing frontage or 20m if on or within 15m of a north west or north east frontage.
- 6.7.2 The proposal includes a zero setback on both frontages, with no change to the Elizabeth Street frontage (south west) existing height 18.9m and the rear Kemp Street frontage (north east) proposed height of 25m. The existing Kemp Street frontage height already exceeds the height limit, being 22.9m.
- 6.7.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied upon.
- 6.7.4 The performance criteria at clause 22.4.1 P1 provide as follows:
P1.1 - Development contained within the Amenity Building Envelope in

Figure 22.3 must make a positive contribution to the streetscape and townscape, having regard to:

- (a) the height, bulk and design of existing and proposed buildings;*
- (b) the need to minimise unreasonable impacts on the view lines and view cones in Figure 22.6 and on the landform horizons to kunanyi/ Mt Wellington and the Wellington Range from public spaces within the Central Business Zone and the Cove Floor;*
- (c) the need to minimise unreasonable impacts on pedestrian amenity from overshadowing of the public footpath for city blocks with frontage to a Solar Penetration Priority Street in Figure 22.2; and*
- (d) the need to minimise unreasonable impacts on the amenity of public open space from overshadowing.*

P1.2 - Development outside the Amenity Building Envelope in Figure 22.3 must provide significant benefits for civic amenities such as public space, pedestrian links, public art or public toilets, unless a minor extension to an existing building that already exceeds the Amenity Building Envelope, and must make a positive contribution to the streetscape and townscape, having regard to:

- (a) the height, bulk and design of existing and proposed buildings;*
- (b) the need to minimise unreasonable impacts on the view lines and view cones in Figure 22.6 and on the landform horizons to kunanyi/Mt Wellington and the Wellington Range from public spaces within the Central Business Zone and the Cove Floor;*
- (c) the need to minimise unreasonable impacts on pedestrian amenity from overshadowing of the public footpath for city blocks with frontage to a Solar Penetration Priority Street see Figure 22.2;*
- (d) the need to minimise unreasonable impacts on the amenity of public open space from overshadowing;*
- (e) the need to minimise unreasonable impacts on pedestrian amenity from adverse wind conditions; and*
- (f) the degree of consistency with the Desired Future Character Statements in clause 22.1.3.*

Desired Future Character Statements - Townscape and Streetscape Character -

22.1.3.1 Objectives:

(a) That the Central Business Zone provides a compact built focus to the region, reflecting an appropriate intensity in its role as the heart of settlement.

(b) That the Central Business Zone develops in a way that reinforces the layered landform rise back from the waterfront, having regard to the distinct layers of the landform, respecting the urban amphitheatre, including the amphitheatre to the Cove, while providing a reduction in scale to the Queens Domain, the Domain and Battery Point headlands and the natural rise to Barracks Hill (see Figures 22.7 and 22.8).

(c) That the Central Business Zone consolidates within, and provides a transition in scale from, its intense focus in the basin, acknowledging also the change in contour along the Macquarie Ridge, including both its rising and diminishing grades, including to the low point of the amphitheatre to the Cove (see Figures 22.7, 22.8 and 22.9).

(d) That the historic cultural heritage values of places and precincts in the Central Business Zone be protected and enhanced in recognition of the significant benefits they bring to the economic, social and cultural value of the City as a whole.

22.1.3.2 Building Siting, Bulk and Design

The siting, bulk and design of a building above the street wall and beyond the Amenity Building Envelope (see Figure 22.3) must be consistent with the objectives in clause 22.1.3.1, having regard to:

(a) the consolidation of the Central Business Zone in a manner which provides separate building forms and a layered visual effect rather than the appearance of a contiguous wall of towers;

(b) maintaining a level of permeability through city blocks by reductions in bulk as height increases allowing for sunlight into streets and public spaces;

(c) the building proportion and detail reflecting and reinforcing the

streetscape pattern;

(d) the building not being an individually prominent building by virtue of its height or bulk, thus reinforcing a cohesive built form and the containment provided by the urban amphitheatre;

(e) reinforcing consistent building edges and height at the street wall allowing for solar penetration where possible;

(f) the provision of weather protection for footpaths to enhance pedestrian amenity and encourage, where appropriate, interior activity beyond the building entrance; and

(g) the provision of permeability in support of the open space network.

- 6.7.5 There is no height increase proposed to the frontage onto Elizabeth Street (south west facade). However, the north east facade fronting Kemp Street is increasing from 22.9m to 25m and as it extends into Kemp Street road reserve, it does not fit within the Amenity Building Envelope. The height of the existing building is 22.9m at the highest point of the lift machinery.

Because the proposal extends outside the Amenity Building Envelope, 22.4.1 P1.1 does not apply. The second Performance Criteria P1.2 relates to development outside the Amenity Building Envelope and requires such development to be considered for the provision of significant civic amenity benefits, in lieu of such extension. Because the height increase is a minor extension to an existing building that already exceeds the Amenity Building Envelope the need for civic amenities benefit assessment is not applicable in this instance. The question arises as to the degree of "minor extension".

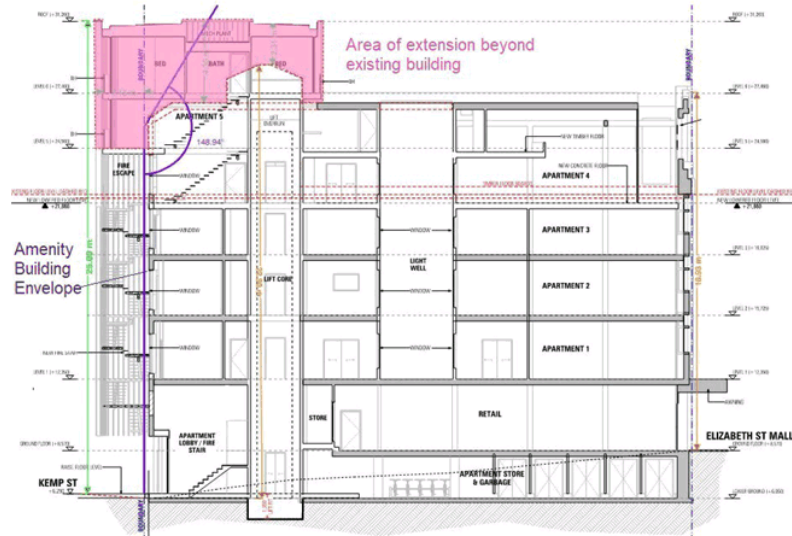


Figure 15: Annotated plan showing the extension in relation to the Amenity Building Envelope and existing building mass (Jaws, 2020)

The applicant indicates that the extension is less than 2m (less than 10%) above the existing maximum height, however, this figure relates to the top of the lift mechanism. The actual extension above the roof line is 4.3m. This is 16.8% of the existing height of the building. The floor area of the extension outside the existing building mass is approximately 45m², which is also less than 10% of the 748m² total floor area. Interpretation of this performance criteria was sought and advised that there is a difference between minor extension and minor protrusion under the scheme. The latter relates to arials, eaves, vents, water tanks, heating/cooling equipment, etc and these can extend no more than 0.6m from the Amenity Building Envelope. Advice is that the meaning of "minor extension" is specifically targeted to circumstances such as the proposal, where the extension is minor in relation to the maximum height or floor area. Being less than 10%, the extension is therefore considered acceptable, even though the building extension actually involves the addition of a another storey on the bulk of the building. Therefore the contribution of civic amenities is not required in this instance.

Notwithstanding the civic amenities provision, the proposal must still be assessed under (a) to (f) of this performance criteria.

The height, bulk and design of the proposed building in relation to existing, is designed to protect the main facade, retaining all extensions outside view lines from the mall and main public visual corridors. The extension will be highly visible from Wellington Court, however it is

considered to be fit within the skyline, being lower than the NAB building, Drysdale house and the Movenpick hotel in the Elizabeth Street bus mall. Given the tall narrow dimensions of the overall Kodak House building, the extension is considered to fit into the streetscape acceptably when considered against the surrounding tall buildings.

There are no view lines either from Macquarie Street or the Cenotaph that incorporate the subject site. It is significantly screened by the Royal Hobart Hospital, University of Tasmania and NAB building.

Shadow diagrams were provided, indicating that the main public areas of Wellington Court will not be significantly overshadowed or shaded by the proposed building extension beyond that of the existing building.

Given the existing design of the building, the increase of 4m from the roofline will not significantly alter wind conditions in public spaces. The canter levered portion of the building is not considered likely to cause any significant change to wind patterns, given the difference in height from the Wellington Court plaza area to any significant degree greater than currently exists.

The relevant Desired Future Character Statements are 22.1.3.1 facilitating high density residential development within the activity centre above ground floor are met through this proposal.

As previously mentioned, the tall narrow design of Kodak House is emblematic of that brand and company's architecture in other capital cities around Australia. Maintenance of this unique character building is an important feature in the capital's cultural and heritage significance. Retaining the architectural proportions and characteristic signage is essential in projecting the unique associate with the capital city status. The loss of the Kodak signage is not considered to pursue 22.1.3.1 (d) of this objective.

The proposal is considered to meet all of the Building, siting, bulk and design Desired Future Character Statements by retaining the characteristic architectural style of the building and its presence in the streetscape, whilst enabling the layered visual effect. The shadow diagrams have shown how the design still permits a level of permeability of sunlight into public spaces. The building has always been designed to be an individually prominent building in the streetscape, but is effectively contained within the urban amphitheatre through other larger building in the vicinity.

It is highlighted that the proposal generally meets the Desired Future Character Statements, other than the Heritage concerns relating to the loss of the historic cultural value of the building through the proposed removal of the emblematic signage. This is significant in relation to 22.1.3.1 (d), however Tribunal decision 67/19P (*Hexa Pacific Pty Ltd vs Hobart City Council and Ors*) indicated that in assessing compliance with this clause, weight should be given to the "degree of consistency" with the Desired Future Character Statements as a whole, not individually. Clearly the majority of Desired Future Character Statement Objectives are acceptably met.

A representation ground raised non compliance with the Heritage Desired Future Character Statement consideration. However, given the above-mentioned Tribunal decision, the fact that the proposal meets the majority of the Desired Future Character Statements this representation ground is not supported.

6.7.6 The proposal complies with the performance criteria.

6.8 Central Business zone - Building height adjacent to a Heritage Place - Part D
22.4.1 A5

6.8.1 The acceptable solution at clause 22.4.1 A5 requires building height of development not separated from a Heritage place by another building to not exceed 1 storey or 4 metres higher than the facade building height of a heritage building on the same street frontage, or if between two heritage buildings, not exceed the facade building height of the Heritage Places, or comply with the building height clauses 22.4.1 A1 and A2.

6.8.2 The proposal includes an extension beyond the building height and increases the maximum height on a lot next to heritage places.

6.8.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.

6.8.4 The performance criterion at clause 22.4.1 P5 provides as follows:

Building height within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

(a) not unreasonably dominate existing buildings of cultural heritage significance; and
(b) not have a materially adverse impact on the historic cultural heritage significance of the heritage place;
(c)
for city blocks with frontage to a Solar Penetration Priority Street in Figure 22.2, not exceed the Amenity Building Envelope illustrated in Figure 22.3, unless it can be demonstrated that the overshadowing of the public footpath on the opposite side of the Solar Penetration Priority Street does not unreasonably impact on pedestrian amenity.

- 6.8.5 The application was referred to the Consultant Heritage Officer who provided the following advice;

The proposal does not comply with the acceptable solution. The new extension to Kemp Street adjoins 47 Elizabeth Street, which is listed on Table E13.1 as a heritage place (Ref No. 992). The subject place exceeds the height of 47 Elizabeth Street by more than 1 storey.

The proposal should therefore be assessed against 22.4.1 P5.

The existing form and proportions of 45 and 47 Elizabeth Street are complementary to one another. The proposed upper level extension does not alter this relationship when viewed from Kemp Street and will not be visible from Elizabeth Street. The increase in building height will not dominate 47 Elizabeth Street and as such the proposal will not adversely impact on the historic cultural heritage significance of the place at 47 Elizabeth Street.

- 6.8.6 The proposal complies with the performance criterion.

6.9 Historic Heritage code - E 13.7.1 - Heritage Place - Demolition

- 6.9.1 There is no acceptable solution for 13.7.1.
- 6.9.2 The proposal includes internal demolition, removal of the external fire stairs and demolition of the Kodak House signage.
- 6.9.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.

- 6.9.4 The performance criterion at clause 13.7.1 P1 provides as follows:

Demolition must not result in the loss of significant fabric, form, items, outbuildings or landscape elements that contribute to the historic cultural heritage significance of the place unless all of the following are satisfied;

- (a) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;*
- (b) there are no prudent and feasible alternatives;*
- (c) important structural or façade elements that can feasibly be retained and reused in a new structure, are to be retained;*
- (d) significant fabric is documented before demolition.*

- 6.9.5 The application was referred to the Consultant Heritage Officer, who provided the following advice;

The following historical overview is taken from the Central Area Heritage Review Reference C9:

The building stands on land that was originally granted to John McDougall. There were buildings occupying this allotment by the 1840s. By the early 1900s, there was a co-joined shop occupying the site. This shop was one of two conjoined shops owned by Mr A Spencer Brownell, who was presumably connected with Brownell Brothers Ltd, a large department store located in Liverpool Street (now Myers). Brownell owned the property until the early 1920s, when the shop was purchased by the Kodak Company, and replaced by Kodak House in c1924. The new building was connected to the City's sewerage system in 1924. Two years later, plans were submitted to the HCC for the addition of a fifth floor to the building. The architect for the project was G Stanley Crisp of 137 Macquarie Street. The new fifth floor was planned to be used as a processing room. The facade was transformed with the words 'Kodak House' being added in large lettering to the new top section with a definitive castle-like form. Kodak (Australasia) Pty Ltd continued to own and occupy the property up until 2002, when it was sold.

There were several other Kodak House buildings constructed in Australian Capitals :

- Kodak House, George Street Sydney c.1930, demolished and replaced with a c.1960s building
- Kodak House, 250-252 Queen Street Brisbane, c.1914, now demolished

- Kodak House, 252 Collins Street Melbourne, 1935, extant.

Archival images and site photographs



Figure 16: Elizabeth Street Hobart 1937 (Source: Libraries Tasmania NS3304-1-1_96-104)



Figure 17: Hobart skyline, undated (Source: Libraries Tasmania PH30-1-6130)

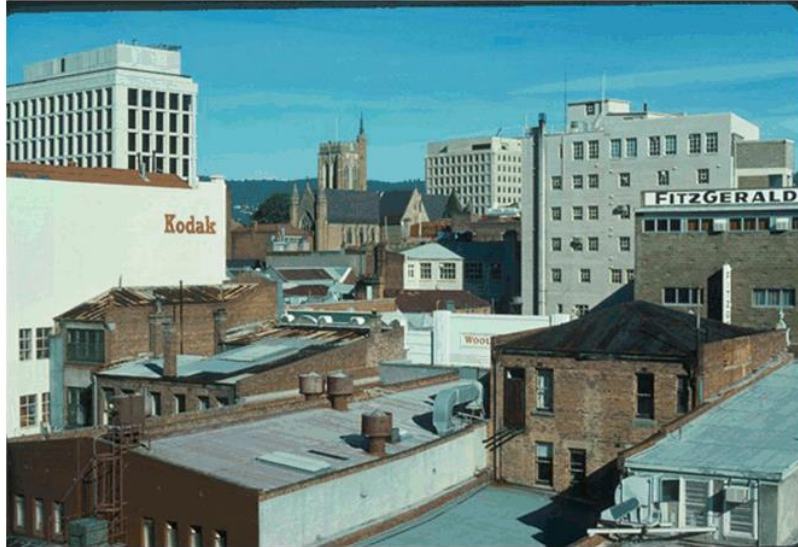


Figure 18: Hobart skyline, c.1976 (Source: Libraries Tasmania NS3373-1-262)



Figure 19: Kodak House (Source: Purcell October 2020)

This assessment has been drafted with reference to the two (2) representations made during the statutory period between 22nd September and 6th October 2020, as summarised within Section 5 of the Draft Council Planning report.

The proposal seeks approval for the demolition of the following features to

the building interiors:

- non-original lightweight partition walls across all levels
- original masonry walls
- toilets to levels 2 and 3
- the central stair and lift shaft
- concrete floor to level 4 and partial floors for the creation of a lightwell

Externally the proposal seeks approval for the demolition of:

- The rear fire exit stair widows and walls to the rear elevation for a new fire stair
- Partial demolition of the roof and roof members
- Isolated demolition of south east and north west elevations for the creation of openings
- Rendered 'Kodak House' signage to the south west elevation

The rendered 'Kodak House' signage is significant fabric which contributes to the cultural heritage significance of the place. Its demolition would diminish its cultural heritage values.

The proposed demolition of the signage and partial demolition of the boundary walls does not substantiate exceptional circumstances as outlined in E13.7.1 (a) and (b), which might warrant the proposed demolition.

The remaining scope of proposed demolition has been carefully considered in design development and could be otherwise supported. It does not result in a loss of historic cultural heritage values of the place.

6.9.6 The proposal does not comply with the performance criterion.

6.10 Historic Heritage code - E 13.7.2 - Heritage Place - Works other than Demolition

6.10.1 There is no acceptable solution for E 13.7.2.

6.10.2 The proposal includes internal alterations to create the five apartments, installation of new lift and internal stairs, external fire stairs and screening, new north and south facing windows.

6.10.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.

6.10.4 The performance criterion at clause E 13.7.2 P1, P2, P3 and P4 provides

as follows:

P1 Development must not result in any of the following:

- (a) loss of historic cultural heritage significance to the place through incompatible design, including in height, scale, bulk, form, fenestration, siting, materials, colours and finishes;*
- (b) substantial diminution of the historic cultural heritage significance of the place through loss of significant streetscape elements including plants, trees, fences, walls, paths, outbuildings and other items that contribute to the significance of the place.*

P2 Development must be designed to be subservient and complementary to the place through characteristics including:

- (a) scale and bulk, materials, built form and fenestration;*
- (b) setback from frontage;*
- (c) siting with respect to buildings, structures and listed elements;*
- (d) using less dominant materials and colours.*

P3 Materials, built form and fenestration must respond to the dominant heritage characteristics of the place, but any new fabric should be readily identifiable as such.

P4 Extensions to existing buildings must not detract from the historic cultural heritage significance of the place.

- 6.10.5 The application was referred to the Consultant Heritage Officer, who advised the following;

The proposed additions are limited to a minor rooftop extension and upgrade to the rear fire exit. The design of these elements is sympathetic to the place and does not detract from its cultural heritage significance. It enhances the presentation of Kemp Street through the removal of the existing unsympathetic exit stair and replacement with an element sympathetic in its form, material, colour and finish. The proposed fenestration will break perceptions of bulk through visual relief. This element integrates with the rooftop extension in proportions which do not detract from the characteristic form of the existing Kemp Street elevation.

The proposed additions are sufficiently setback from the Elizabeth Street frontage such that the dominant characteristics of this elevation will be retained.

The proposal does not impact on significant streetscape elements and will not result in a loss of historic cultural heritage significance.

The proposal complies with the performance criterion E13.7.2 P1.

The proposal is sufficiently set back from the Elizabeth Street façade and will not impact the dominant characteristics of this elevation. The addition to Kemp Street is of a scale and bulk which does not dominate or detract from the existing. As per E13.7.2 materials, colour and fenestration are complementary to the place.

The proposal complies with the performance criterion E13.7.2 P2.

The proposed materials, built form and fenestration are complementary to the place while being readily identifiable as new additions.

The proposal complies with the performance criterion E13.7.2 P3.

The proposed replacement exit stair is of a form and scale consistent with that of the existing. The roof top addition is considered a minor extension and one which does not detract from the historic cultural heritage significance of the place.

The proposal complies with the performance criterion E13.7.2 P4.

6.10.6 The proposal complies with the performance criteria.

6.11 Signs Code - E 17.7.2 - Sign on Heritage Place

6.11.1 There is no acceptable solution for E 17.7.2.

6.11.2 The proposal includes the replacement of the masonry Kodak house sign with a steel plate sign.

6.11.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.

6.11.4 The performance criterion at clause E 17.7.2 P1 provides as follows:

A sign on a Heritage Place listed in the Historic Heritage Code or within a Heritage Precinct or Cultural Landscape Precinct must satisfy all of the following:

(a) be located in a manner that minimises impact on cultural heritage significance of the place or precinct;

(b) be placed so as to allow the architectural details of the building to remain prominent;

(c) be of a size and design that will not substantially diminish the cultural heritage significance of the place or precinct;
(d) be placed in a location on the building that would traditionally have been used as an advertising area if possible;
(e) not dominate or obscure any historic signs forming an integral part of a building's architectural detailing or cultural heritage values;
(f) have fixtures that do not damage historic building fabric, including but not restricted to attachments to masonry and wood, such as to using non-corrosive fixings inserted in mortar joints;
(g) not project above an historic parapet or roof line if such a projection impacts on the cultural heritage significance of the building;
(h) be of a graphic design that minimises modern trademark or proprietary logos not sympathetic to heritage character;
(i) not use internal illumination in a sign on a Heritage Place unless it is demonstrated that such illumination will not detract from the character and cultural heritage values of the building.

- 6.11.5 The application was referred to the Consultant Heritage Officer, who advised the following;

The interpretative signage proposed to replace that of the significant rendered 'Kodak House' will diminish the cultural heritage significance of the place by virtue of the demolition of the existing signage. Further it does not allow for the retention of significant architectural details.

- 6.11.6 The proposal does not comply with the performance criterion.

7. Discussion

- 7.1 Planning approval is sought for Partial Demolition, Alterations, Extension and Change of Use to General Retail and Hire and Five Multiple Dwellings at 45 ELIZABETH STREET HOBART TAS 7000.
- 7.2 The application was advertised and received two (2) representations. The representations raised concerns including inappropriate loss of heritage fabric and cultural heritage for the site, concerns over increased height, design and inappropriate sign proposed and non compliance with scheme standards. Pertinent comments from the representations are as follows;

"The planning report articulates the iconic Kodak signage, representing the cultural heritage and story of the building. In any assessment of heritage significance, the importance of the "Kodak House" building name would be paramount. The

lettering, arrangement, materials and placement all relate to the period of its construction and provide a "voice" in the lettering".

"The current proposal underestimates the integral importance of the building signage. It is not acceptable to cut a new opening into the building facade".

"There are no exceptional circumstances to warrant the destruction of the facade panel with the Kodak House lettering ... simply to enhance the convenience and amenity of an additional upper level unit".

"The proposed new replacement sign, to supposedly perpetuate the 'memory' of Kodak, is located in a manner that has a severe negative impact upon the significance of the building".

"The proposed replacement sign will involve the creation of a new void in the upper level façade and the destruction of the existing building name – the new sign will not be placed so as to allow the architectural details of the building to remain prominent".

"The existing historic building name signage is to be removed. This lettering is an integral part of the building's architectural detailing and cultural heritage value. ... and replaces it with a so-called 'memory'".

"Any approval of the proposal should be subject to the retention of the existing Kodak House building name..".

The above comments made in the representations are reflected and generally supported in the assessment by the Consultant Heritage Officer and the recommendation.

The Consultant Heritage Officer provided the following concluding comments;

The streetscape contribution of the place is recognised within Datasheet C9 of the Central Heritage Review. The painted signage to the party walls has a long history and association with the Hobart skyline (see archival images) and contributes to the historic character of the Elizabeth Street Mall and to the landmark qualities of the building.

The proposal will directly impact architectural features which contribute to the principal characteristics of this Inter-War commercial building (Criterion d).

The proposal to remove the painted and rendered signage from Kodak House will diminish its historic cultural heritage values, and exceptional circumstances as

defined by E13.7.1 P1 have not been sufficiently demonstrated against P1a and b. It is considered that a prudent and feasible alternative would be for the retention of the rendered and painted signage with a reconfiguration of the internal room functions and configuration for Apartment 4 with private outdoor space provided through an enlarged light well over levels 4 and 5. It is recognised that this would impact on the existing roof form, however this is considered an acceptable and a preferable alternative to the current proposal.

- 7.3 The proposal has been assessed against the relevant provisions of the planning scheme and is considered to not perform well.
- 7.4 The proposal has been assessed by other Council officers, including the Council's Development Engineer, Cultural Heritage Officer, Stormwater Technical Officer, Cadastral Surveyor and Consultant Heritage Officer. The officers have raised objection to the proposal.
- 7.5 The proposal is recommended for refusal.

8. Conclusion

- 8.1 The proposed Partial Demolition, Alterations, Extension and Change of Use to General Retail and Hire and Five Multiple Dwellings at 45 ELIZABETH STREET HOBART TAS 7000 does not satisfy the relevant provisions of the *Hobart Interim Planning Scheme 2015*, and as such is recommended for refusal.

9. Recommendations

That: Pursuant to the *Hobart Interim Planning Scheme 2015*, the Council refuse the application for Partial Demolition, Alterations, Extension and Change of Use to General Retail and Hire and Five Multiple Dwellings at 45 ELIZABETH STREET HOBART TAS 7000 for the following reasons:

- 1 The proposal does not meet the acceptable solution or the performance criterion with respect to clause E13.7.1 P1 (a) and (b) of the Historic Heritage Code of the Hobart Interim Planning Scheme 2015 because the proposed demolition will result in the loss of early significant fabric and items (signage and architectural details) that contribute to the historic heritage significance of a heritage listed place.
- 2 The proposal does not meet the acceptable solution or the performance criterion with respect to clause E17.7.2 P1 (a) to (i) of Signs Code of the Hobart Interim Planning Scheme 2015 because the proposed demolition will result in the loss of early significant fabric and items (signage) that contribute to the historic heritage significance of a heritage listed place.



(Victoria Maxwell)

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.



(Karen Abey)

Manager Development Appraisal

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

Date of Report: 29 October 2020

Attachment(s):

Attachment A - CPC Agenda Documents

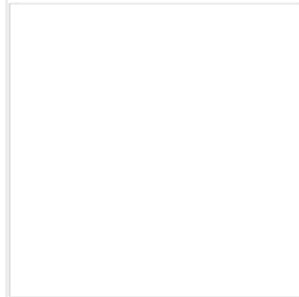
Attachment B - Referral Officer Reports

Attachment C - CPC Supporting Documents

Planning: #211278

Property

45 ELIZABETH STREET HOBART TAS 7000

**People**

Applicant

*

Giameos Constructions & Developments Pty Ltd

C/- All Urban Planning Pty Ltd

19 Mawhera Avenue

SANDY BAY TAS 7005

0400109582

frazer@allurbanplanning.com.au

Owner

*

Giameos Holdings Pty Ltd

Level 2, 100 Melville Street

HOBART TAS 7000

62343366

george.giameos@gcdconstructions.com.au

Entered By

FRAZER ERIC READ

0400 109 582

frazer@allurbanplanning.com.au

Use

Multiple dwellings

Details

Have you obtained pre application advice?

☒ Yes

If YES please provide the pre application advice number eg PAE-17-xx

Are you applying for permitted visitor accommodation as defined by the State Government Visitor Accommodation Standards? Click on help information button for definition. If you are not the owner of the property you MUST include signed confirmation from the owner that they are aware of this application.

*

☐ No

Is the application for SIGNAGE ONLY? If yes, please enter \$0 in the cost of development, and you must enter the number of signs under Other Details below.

*

☐ No

If this application is related to an enforcement action please enter Enforcement Number

Details

What is the current approved use of the land / building(s)?

commercial

Please provide a full description of the proposed use or development (i.e. demolition and new dwelling, swimming pool and garage)

alterations, extensions and change of use of upper levels to residential

Estimated cost of development

1000000.00

Existing floor area (m2)	Proposed floor area (m2)	Site area (m2)

Carparking on Site

N/A

Total parking spaces	Existing parking spaces	
		<input type="checkbox"/> Other (no selection chosen)

Other Details

Does the application include signage?

☐ Yes

** Please be advised that you are required to lodge plans of the sign. The plans should show: dimensions, location, colours, wording, method of illumination, does it flash, method of fixing to wall, etc.*

How many signs, please enter 0 if there are none involved in this application?

1

Tasmania Heritage Register

Is this property on the Tasmanian Heritage Register?

☐ No

Documents

Required Documents

Title (Folio text and Plan and Schedule of Easements)

Certificates of Title 45 Elizabeth St and Purdys Mart.pdf

Plans (proposed, existing)

20035_45 Elizabeth St DEVELOPMENT APPLICATION REV02.pdf

Supporting Documents

Archaeological Report

Advice - re archaeology 45 Elizabeth Street.pdf

Planning Report

Planning Report - 45 Elizabeth Street 15082020.pdf



Submission to Planning Authority Notice

Council Planning Permit No.	PLN-20-524	Council notice date	20/08/2020
TasWater details			
TasWater Reference No.	TWDA 2020/01266-HCC	Date of response	18/09/2020
TasWater Contact	Anthony Cengia	Phone No.	0474 933 293
Response issued to			
Council name	HOBART CITY COUNCIL		
Contact details	coh@hobartcity.com.au		
Development details			
Address	45 ELIZABETH ST, HOBART	Property ID (PID)	5661393
Description of development	partial demolition, alterations, extension and change of use to general retail and hire and five multiple dwellings		
Schedule of drawings/documents			
Prepared by	Drawing/document No.	Revision No.	Date of Issue
Jaws Architects	20035 Sheet DA02, DA03, DA07, DA08, DA09	02	12/08/2020
Jaws Architects	20035 Sheets DA05 & DA06	03	28/08/2020
Jaws Architects	20035 Sheet DA04	03	14/09/2020
Gandy & Roberts	20.0371 Sheet H010	2	14/09/2020
Conditions			
SUBMISSION TO PLANNING AUTHORITY NOTICE OF PLANNING APPLICATION REFERRAL			
Pursuant to the <i>Water and Sewerage Industry Act 2008</i> (TAS) Section 56P(1) TasWater imposes the following conditions on the permit for this application:			
CONNECTIONS, METERING & BACKFLOW			
<p>1. A suitably sized water supply with metered connections and sewerage system and connection to the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit.</p> <p>Advice: TasWater will not accept direct fire boosting from the network unless it can be demonstrated that the periodic testing of the system will not have a significant negative effect on our network and the minimum service requirements of other customers serviced by the network. To this end break tanks may be required with the rate of flow into the break tank controlled so that peak flows to fill the tank do not also cause negative effect on the network.</p>			
<p>2. Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost.</p>			
<p>3. Prior to commencing construction/use of the development, any water connection utilised for construction/the development must have a backflow prevention device and water meter installed, to the satisfaction of TasWater.</p>			
DEVELOPMENT ASSESSMENT FEES			
<p>4. The applicant or landowner as the case may be, must pay a development assessment fee of \$351.28 to TasWater, as approved by the Economic Regulator and the fee will be indexed, until the date paid</p>			



to TasWater.

The payment is required within 30 days of the issue of an invoice by TasWater.

Advice

General

For information on TasWater development standards, please visit

<http://www.taswater.com.au/Development/Development-Standards>

For application forms please visit <http://www.taswater.com.au/Development/Forms>

Boundary Trap Area

The proposed development is within a boundary trap area and the developer will need to provide a boundary trap that prevents noxious gases or persistent odours back venting into the property's sanitary drain. The boundary trap is to be contained within the property boundaries and the property owner remains responsible for the ownership, operation and maintenance of the boundary trap.

Declaration

The drawings/documents and conditions stated above constitute TasWater's Submission to Planning Authority Notice.

Authorised by

Jason Taylor

Development Assessment Manager

TasWater Contact Details

Phone	13 6992	Email	development@taswater.com.au
Mail	GPO Box 1393 Hobart TAS 7001	Web	www.taswater.com.au

AllUrbanPlanning PTY LTD

Planning Report
Kodak Building
45 Elizabeth Street

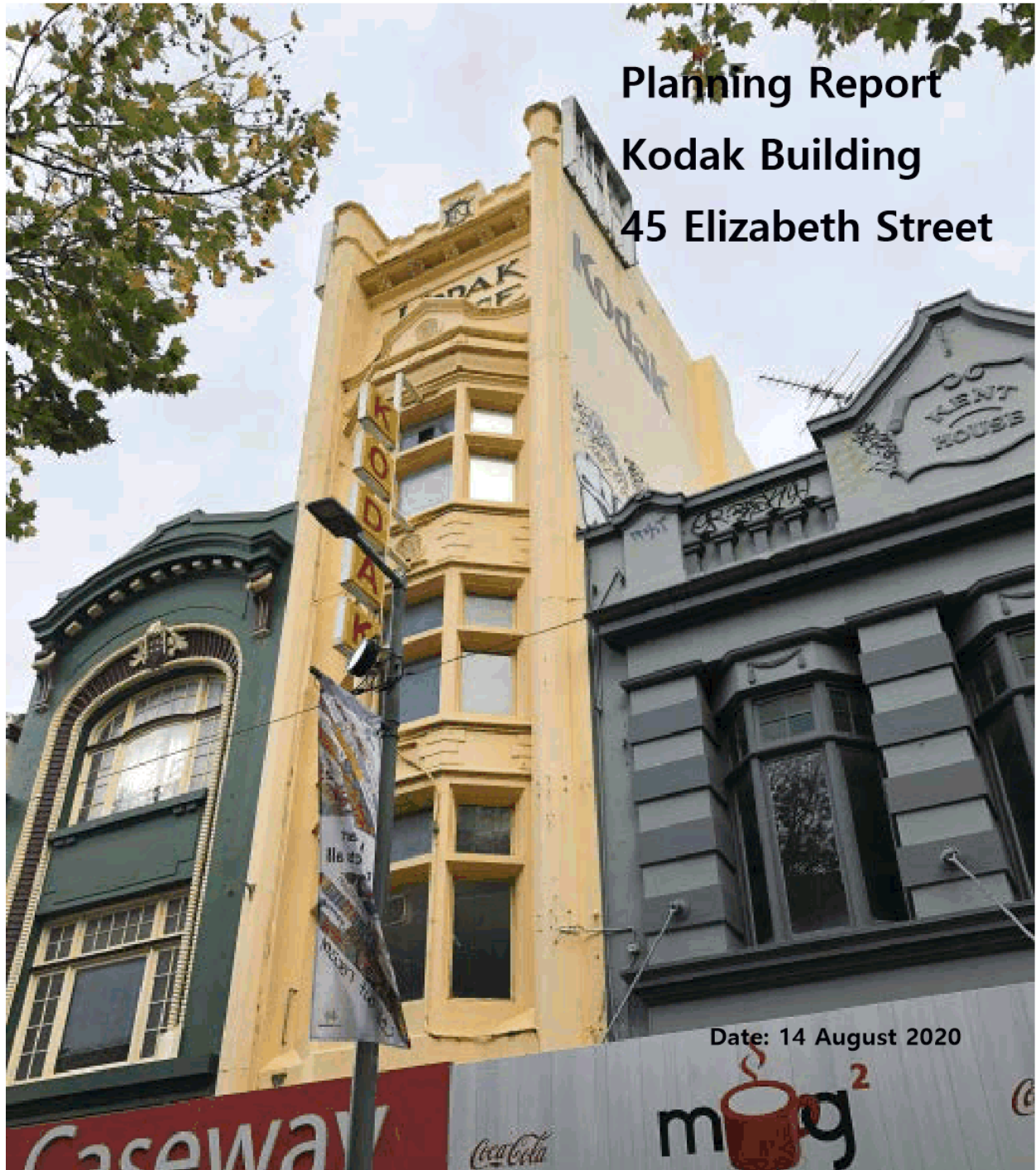


Table of contents

1.	Introduction	2
1.1	Site & Surrounds	2
2.	Proposal	3
3.	The Planning Scheme	5
3.1	Central Business Zone	5
3.2	Desired Future Character Statements	7
3.3	Use Table	9
3.4	Use Standards	9
3.5	Development Standards for Buildings and Works	11
4.	Planning Scheme Codes	19
4.1	Potentially Contaminated land Code	19
4.2	Road and Railway Assets Code	19
4.3	Parking and Access Code	19
4.4	Stormwater Management Code	19
4.5	Historic Heritage Code	20
4.6	Inundation Prone Areas Code	26
4.7	Signage	26
4.8	Amenity Standards in the Central Business Zone	27
5.	Conclusion	38

1. Introduction

All Urban Planning Pty Ltd has been engaged by Giameos Constructions & Developments Pty Ltd to prepare the following planning assessment for adaptive reuse and extension of the Kodak building at 45 Elizabeth Street under the provisions of the *Hobart Interim Planning Scheme 2015* (planning scheme).

The proposal, retaining ground floor commercial use, is to upgrade the building and convert the upper levels from vacant commercial tenancies to residential including new internal and external access stairs, light well and internal lift. A top floor apartment extension is also proposed at the north east, Purdy's Mart end of the building.

Architect's Project Description

Kodak House was constructed in 1920 to capitalise on the growing recreational photography market, with the top floor added in 1929. Overlooking the Elizabeth Street Mall, the building is memorable for its slender appearance, yellow colouring and its iconic KODAK signage.

Framed by two existing party walls, five new apartments are proposed to fill the space previously occupied by the printing and developing facilities on the upper levels of the store.

The site falls one level for the length of the building, allowing access to the apartments from Purdy's Mart at the rear, whilst retaining a retail tenancy with a frontage onto the Mall.

Every effort has been made to maintain the existing form and presence of the building, with an additional part-floor set to the rear without compromising the memorable streetscape image.

A new lift and light well are inserted into the existing structure, allowing equitable access and a high level of amenity to be provided for the apartments. An escape stair and upper level apartment sit within the footprint of the addition, but outside the original shell, encompassed by a lattice of steel members to screen its presence. Mechanical plant is also incorporated into the addition, hidden within the new roofscape to maintain a refined presence from the rear.

A new opening is proposed to be carefully cut into the masonry façade of the street frontage, the existing embossed building signage replaced with painted steel replica lettering to maintain the memory of the original tenant.

1.1 Site & Surrounds

The proposal relates to the Kodak building at 45 Elizabeth Street including building overhangs on Kemp Street, owned by Hobart City Council adjacent to the north east (rear) of the site. The proposal includes the following titles:

Address	CT	Owner
45 Elizabeth Street	231481/1	Giameos Holdings Pty Ltd
Kemp Street	No title	Hobart City Council
Purdy' Mart	113200/2	Hobart City Council

A request for General Manager's consent to lodge the application pursuant to Section 52 of the Act has been made and will be necessary to accompany the application.

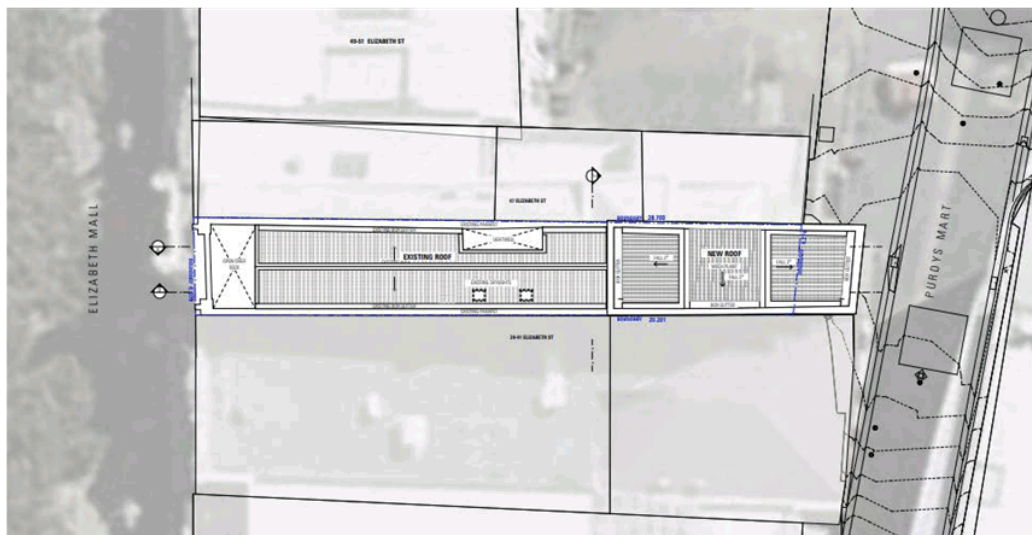


Figure 1 – the site (Source: JAWS)

2. Proposal

The proposal is for redevelopment of the site to convert the upper levels for 5 multiple dwellings including a replacement new rear fire escape and penthouse extension overhanging Kemp Street.

The proposal includes:

Demolition

The proposed extent of demolition is shown on the accompanying demolition plans and includes:

- Concrete Floors on Level 5.
- Partial floors to form light well
- existing non-compliant lift and access stair
- service bathrooms originally designed to support commercial use
- existing non-compliant steel Fire Stair
- Partial roof to add new residential levels
- Kemp Street windows and walls to allow new fire stair access and windows to apartment bedrooms
- openings for natural ventilation and light

Use & Development

Basement (Ground floor Kemp Street)



- Garbage, store and bike parking
- New entry lift and internal stair access from Kemp Street

Level 1 – (Ground floor Elizabeth Street Mall)

- Commercial tenancy fronting Elizabeth Street Mall (including access WC)
- Lift, stair and void over entry lobby

Level 2/3/4

- Apartments 1, 2 and 3 (each 3 bedroom)

Level 5

- Apartment 4 (3 bedroom)
- Entry Apartment 5

Level 6

- Upper floor Apartment 4
- Living Apartment 5 (2 bedroom)

Level 7

- Top floor bedrooms Apartment 5

Waste Storage and Collection (22.4.10)

Residential Waste:

Calculations below are based on a weekly residential waste generation rate for each 3 bedroom apartment of 120L of General Waste & 120L of Recycling per apartment per week.

It is proposed that residential waste will be managed as follows:

- 2 x 660 L Bins will be located in the Lower Ground Basement.
- These will be accessed by residents off the Lift Lobby.
- 1 x 660 L Bin for General Waste & 1 x 660 L Bin for General Recycling
- The Body Corporate nominated Caretaker will be responsible for moving the bins using the low level platform lift and manually pushing to the kerbside, Kemp Street Location nominated on the Lower Ground Basement Plan. The private collection service is proposed to come weekly for servicing in early morning periods, with bins being moved back to the Lower Ground Level following collection via prior arrangement. The bin collection location on Kemp Street is constrained due to the width of existing building and site.

Commercial Waste:

Calculations below are based on a weekly commercial waste generation rate for Retail (non-Food) of 50L / 100m² per day.

It is proposed that commercial waste will be managed as follows:

- 2 x 240 L bins to be located in the storeroom at the rear of the tenancy.

- The Tenant will move bins to Elizabeth Street Mall for weekly Council collection service.

3. The Planning Scheme

Under Clause 8.10.1 of the planning scheme the planning authority must, in addition to the matters required by ss51(2) of the Act, take into consideration:

- (a) *all applicable standards and requirements in this planning scheme; and*
- (b) *any representations received pursuant to and in conformity with ss57(5) of the Act,*
but in the case of the exercise of discretion, only insofar as each such matter is relevant to the particular discretion being exercised.

Relevantly, a standard is applicable if the site is within the relevant zone and the standard deals with a matter that could affect or be affected by the proposed development; cl.7.5.2.

A standard is defined to mean the objective for a particular planning issue and the means for satisfying that objective through either an acceptable solution or corresponding performance criterion.

Compliance with a standard is achieved by complying with either the acceptable solution or corresponding performance criterion; cl.7.5.3.

The objective of the standard may be considered to help determine whether the proposed use or development complies with the performance criterion of that standard; cl.7.5.4. The acceptable solution is not relevant to the assessment of the corresponding performance criteria.

3.1 Central Business Zone

The site is zoned Central Business.



Figure 2 - Zoning plan (Source: iplan)



The Zone Purpose Statements under Clause 22.1.1 are as follows:

- 22.1.1.1 *To provide for business, civic and cultural, community, food, hotel, professional, retail and tourist functions within a major centre serving the region or sub-region.*
- 22.1.1.2 *To maintain and strengthen Hobart's Central Business District and immediate surrounds including, the waterfront, as the primary activity centre for Tasmania, the Southern Region and the Greater Hobart metropolitan area with a comprehensive range of and highest order of retail, commercial, administrative, community, cultural, employment areas and nodes, and entertainment activities provided.*
- 22.1.1.3 *To provide a safe, comfortable and pleasant environment for workers, residents and visitors through the provision of high-quality urban spaces and urban design.*
- 22.1.1.4 *To facilitate high density residential development and visitor accommodation within the activity centre above ground floor level and surrounding the core commercial activity centre.*
- 22.1.1.5 *To ensure development is accessible by public transport, walking and cycling.*
- 22.1.1.6 *To encourage intense activity at pedestrian levels with shop windows offering interest and activity to pedestrians.*
- 22.1.1.7 *To encourage a network of arcades and through-site links characterised by bright shop windows, displays and activities and maintain and enhance Elizabeth Street Mall and links to it as the major pedestrian hub of the CBD.*
- 22.1.1.8 *To respect the unique character of the Hobart CBD and maintain the streetscape and townscape contribution of places of historic cultural heritage significance.*
- 22.1.1.9 *To provide a safe, comfortable and enjoyable environment for workers, residents and visitors through the provision of high-quality spaces and urban design¹.*

The proposal is considered consistent with these Purpose Statements in that:

- The proposal for conversion of vacant upper levels of the building for residential is consistent with Clause 22.1.1.4.
- The existing building with its exposed steel fire escape stair turns its back to Kemp Street. The proposed building upgrades will provide a new building face to that frontage with a new double height lobby entry and screened escape stair. It will provide activity, interest and an improved public face to this aspect of the site. The proposal will complement and contribute to planned upgrades to Kemp Street associated with the new Vibe hotel. In this respect it is considered to further Clauses 22.1.1.3 and 22.1.1.9 with high quality public spaces and urban design.
- The proposal is located adjacent to the bus mall and includes provision for secure bike parking in the existing basement of the building. The proposal will support Clause 22.1.1.5 encouraging development accessible by public transport, walking and cycling.
- The proposed upgrades will maintain an active frontage to Elizabeth Street consistent with Clause 22.1.1.6.
- The constraints of the narrow site do not allow a retail frontage to Kemp Street also however the proposed new lobby and architectural upgrades on this frontage will enhance activation of the site to

¹ Note: this statement is a repeat of 22.1.1.3 other than the word "pleasant" is replaced by "enjoyable".

this aspect. In this respect the proposal furthers Clause 22.1.1.7 and will support Kemp Street and Purdy's Mart as part of the network of pedestrian arcades, links that support the Elizabeth Street Mall and pedestrianisation of the CBD.

- The proposed upgrades and adaptive reuse of the iconic Kodak building support Clause 22.1.1.8 in that they will respect and enhance the unique character of Hobart and the historic cultural heritage significance of the place.

3.2 Desired Future Character Statements

Townscape and Streetscape Character

22.1.3.1 Objectives:

- (a) *That the Central Business Zone provides a compact built focus to the region, reflecting an appropriate intensity in its role as the heart of settlement.*

Comment:

The proposal will support a compact built focus for the region and is consistent with this Statement.

- (b) *That the Central Business Zone develops in a way that reinforces the layered landform rise back from the waterfront, having regard to the distinct layers of the landform, respecting the urban amphitheatre, including the amphitheatre to the Cove, while providing a reduction in scale to the Queens Domain, the Domain and Battery Point headlands and the natural rise to Barracks Hill (see Figures 22.7 and 22.8).*

Comment:

The proposed extension will remain well below the height of buildings in the vicinity that dominate the townscape including the NAB building, RHH, Vibe hotel, 22 Elizabeth Street, the Trafalgar building and the Hyatt Hotel. It will not conflict with the objective to reinforce the layered land form rising from the waterfront to the enclosing ridges of the city. The proposal supports this desired future character.

- (c) *That the Central Business Zone consolidates within, and provides a transition in scale from, its intense focus in the basin, acknowledging also the change in contour along the Macquarie Ridge, including both its rising and diminishing grades, including to the low point of the amphitheatre to the Cove (see Figures 22.7, 22.8 and 22.9).*

Comment:

The proposal for intensification of built form on this site within the city basin is consistent with this statement.

- (d) *That the historic cultural heritage values of places and precincts in the Central Business Zone be protected and enhanced in recognition of the significant benefits they bring to the economic, social and cultural value of the City as a whole.*

Comment:

As discussed further below the proposed alterations, additions and conversion have been designed to respect the unique qualities of the site and the inner areas of Hobart.

**Building Siting, Bulk and Design****22.1.3.2 Building Siting, Bulk and Design**

The siting, bulk and design of a building above the street wall and beyond the Amenity Building Envelope (see Figure 22.3) must be consistent with the objectives in clause 22.1.3.1, having regard to:

- (a) the consolidation of the Central Business Zone in a manner which provides separate building forms and a layered visual effect rather than the appearance of a contiguous wall of towers;*

Comment

The proposal will accentuate the tall slender form of the Kodak building and will support the desired layered visual effect of the central areas of the city.

- (b) maintaining a level of permeability through city blocks by reductions in bulk as height increases allowing for sunlight into streets and public spaces;*

Comment

The proposed modest extension to the slender building will not have a tangible shadowing impact on the publicly accessible areas of the city and is therefore consistent with this objective.

- (c) the building proportion and detail reflecting and reinforcing the streetscape pattern;*

Comment:

The proposal will support the pattern of terraced shops aligned to the Elizabeth Street spine and furthers this objective.

- (d) the building not being an individually prominent building by virtue of its height or bulk, thus reinforcing a cohesive built form and the containment provided by the urban amphitheatre;*

Comment:

The proposed extension is sited to the rear of the building and will not be viewed in the context of the primary Elizabeth Street Mall frontage. On Kemp Street/Purdy's Mart the building will remain subservient to the dominant scale of the Vibe Hotel and NAB towers and the Argyle Street carpark. It will not be individually prominent by virtue of its height or bulk. The treatment of the replacement stair and top floor extension will present as a contemporary and subservient addition.

- (e) reinforcing consistent building edges and height at the street wall allowing for solar penetration where possible;*

Comment

The proposal is well setback from Elizabeth Street and will maintain solar access to the street. The extension to the rear will be sited at the southern end of Kemp Street and hence will not overshadow that space.

- (f) the provision of weather protection for footpaths to enhance pedestrian amenity and encourage, where appropriate, interior activity beyond the building entrance; and*

Comment:

Kemp Street is not characterised by pedestrian awnings due to its narrow dimensions.

- (g) the provision of permeability in support of the open space network.*



Comment:

The proposal will not conflict with this objective.

These Statements are relevant to the assessment of the Development Standards as discussed below.

3.3 Use Table

The proposed apartments fall with the Residential Use Class. The apartments above ground level are Permitted under the Use Table 22.2. The proposal does not involve any residential use at ground floor level to either Kemp Street or Elizabeth Street other than for access to the upper level dwellings.

The proposed ground level commercial tenancy is also permitted under Table 22.2.

The proposed uses are therefore Permitted.

3.4 Use Standards

Hours of Operation (22.3.1)

Use Standard	Assessment
<p>A1</p> <p>Hours of operation of a use within 50 m of a residential zone must be within:</p> <p>(a) 6.00 am to 10.00 pm Mondays to Saturdays inclusive;</p> <p>(b) 7.00 am to 9.00 pm Sundays and Public Holidays. except for office and administrative tasks.</p>	<p>The site is not within 50m of a Residential Zone. This Standard therefore does not apply.</p>

Noise (22.3.2)

Use Standard	Assessment
<p>A1</p> <p>Noise emissions measured at the boundary of a residential zone must not exceed the following:</p> <p>(a) 55dB(A) (LAeq) between the hours of 7.00 am to 7.00 pm;</p> <p>(b) 5dB(A) above the background (LA90) level or 40dB(A) (LAeq), whichever is the lower, between the hours of 7.00 pm to 7.00 am;</p> <p>(c) 65dB(A) (LMax) at any time.</p> <p>Measurement of noise levels must be in accordance with the methods in the Tasmanian Noise Measurement Procedures Manual, issued by the Director of Environmental Management, including</p>	<p>Complies.</p> <p>The proposal will not involve noise emissions that would exceed these limits at the boundary of a residential zone.</p> <p>The closest boundary with a residential zone is over 500m away on Goulburn Street.</p> <p>The proposal therefore will comfortably comply with 22.3.2A1.</p>

<p><i>adjustment of noise levels for tonality and impulsiveness.</i></p> <p><i>Noise levels are to be averaged over a 15 minute time interval.</i></p>	
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External Lighting (22.3.3)

Use Standard	Assessment
<p>A1</p> <p><i>External lighting within 50 m of a residential zone must comply with all of the following:</i></p> <p><i>(a) be turned off between 11:00 pm and 6:00 am, except for security lighting;</i></p> <p><i>(b) security lighting must be baffled to ensure they do not cause emission of light outside the zone.</i></p>	<p>The site is not within 50m of a Residential Zone. This Standard therefore does not apply.</p>

Commercial Vehicle Movements (22.3.4)

Use Standard	Assessment
<p>A1</p> <p><i>Commercial vehicle movements, (including loading and unloading and garbage removal) to or from a site within 50 m of a residential zone must be within the hours of:</i></p> <p><i>(a) 6.00 am to 10.00 pm Mondays to Saturdays inclusive;</i></p> <p><i>(b) 7.00 am to 9.00 pm Sundays and Public Holidays.</i></p>	<p>The site is not within 50m of a Residential Zone. This Standard therefore does not apply.</p>

Take Away Food Premises (22.3.6)

Use Standard	Assessment
<p>A1</p> <p><i>Hours of operation must be within 7.00am to 12.00am.</i></p>	<p>The specific use of the ground floor tenancy is unknown at this stage. However, if it were to include a takeaway food premises it would operate within the permitted hours of 7am to 12am and would therefore comply with A1.</p>

Hotel Industries (22.3.7)

Use Standard	Assessment
A1 <i>Hours of operation must be within 7.00am to 12.00am.</i>	Not applicable. The proposal does not include a Hotel Industry.

The proposal does not involve a Manufacturing or Processing Use or an Adult Entertainment Venue. The Use Standards under 22.3.5 and 22.3.8 therefore do not apply.

3.5 Development Standards for Buildings and Works

The Development Standards for the Central Business Zone apply differently depending on whether a site is within the Core or Fringe Area, on a Solar Penetration Priority Street or within the Active Frontage Overlay.

In this case the site is:

- located within the Central Business Core Area
- located on a Solar Penetration Priority Street
- located within the Active Frontage Overlay
- located adjacent to a heritage place



Having regard to these overlays the following Development Standards apply to height and setback on the land.

22.4.1 Building Height*Objective*

To ensure that building height contributes positively to the streetscape and does not result in unreasonable impact on residential amenity of land in a residential zone.

Acceptable Solutions	Performance Criteria
<p>A1</p> <p><i>Building height within the Central Business Core Area in Figure 22.2 must be no more than:</i></p> <p>(a) 15m if on, or within 15m of, a south-west or south-east facing frontage;</p> <p>(b) 20m if on, or within 15m of, a north-west or north-east facing frontage;</p> <p>(c) 30m if set back more than 15m from a frontage;</p> <p><i>unless an extension to an existing building that:</i></p> <p>(i) <i>is necessary solely to provide access, toilets, or other facilities for people with disabilities;</i></p>	<p>This standard applies a 15m height within 15m of Elizabeth Street (a SW facing frontage) and 20m within 15m of Kemp Street (a NE frontage). Given the site is approximately 28m deep the 20m from Kemp Street applies for approximately 13m from that frontage and 15m for the remaining part of the site to Elizabeth Street.</p> <p>The proposed upper floor extension to Kemp Street exceeds the 20m permitted standard under A1b) with a height of approximately 25.2m above NGL and is therefore to be assessed under P1.</p> <p>The upper level extension is setback approximately 20m from the Elizabeth Street frontage and complies in that the proposal does</p>

<p>(ii) <i>is necessary to provide facilities required by other legislation or regulation.</i></p>	<p>not increase the height of the building within this envelope.</p>
<p>P1.2</p> <p><i>Development outside the Amenity Building Envelope in Figure 22.3 must provide significant benefits for civic amenities such as public space, pedestrian links, public art or public toilets, unless a minor extension to an existing building that already exceeds the Amenity Building Envelope, and must make a positive contribution to the streetscape and townscape, having regard to:</i></p> <p>(a) <i>the height, bulk and design of existing and proposed buildings;</i></p> <p>(b) <i>the need to minimise unreasonable impacts on the view lines and view cones in Figure 22.6 and on the landform horizons to kunanyi/Mt Wellington and the Wellington Range from public spaces within the Central Business Zone and the Cove Floor;</i></p> <p>(c) <i>the need to minimise unreasonable impacts on pedestrian amenity from overshadowing of the public footpath for city blocks with frontage to a Solar Penetration Priority Street see Figure 22.2;</i></p> <p>(d) <i>the need to minimise unreasonable impacts on the amenity of public open space from overshadowing;</i></p> <p>(e) <i>the need to minimise unreasonable impacts on pedestrian amenity from adverse wind conditions; and</i></p> <p>(f) <i>the degree of consistency with the Desired Future Character Statements in clause 22.1.3.</i></p>	<p>The existing building with fire stairs overhanging the Kemp Street frontage is 22.7m high (measured to the top of the lift overrun) and 20.2m to the gutter line. It already exceeds the Amenity Building envelope under Figure 22.3. The proposal with a height of 25.5m to Kemp Street is also not contained within the Amenity Building Envelope and is therefore to be assessed under P1.2.</p> <p>The existing and proposed elevations accompany the application.</p> <p>The proposal is considered a minor extension to the existing building in that:</p> <ul style="list-style-type: none"> it involves a modest increase in height of the building of 1.8m from 22.7m to 25.5m. The Level 7 floor area increase of approximately 45m² to the existing 748m² building. This represents only a 6% increase to the exiting building. <p>As a minor extension the proposal is not required to meet the significant civic amenity benefit test of P1.2.</p> <p>The proposal is considered to make a positive contribution to the streetscape and townscape and therefore satisfy P1.2 in that:</p> <ul style="list-style-type: none"> The height and bulk of the proposed minor extension is modest in comparison to the nearby Vibe Hotel, NAB building, RHH, Wellington Centre, Argyle Street Carpark and Hyatt hotel buildings in particular. The design is considered a sensitive and well resolved contemporary adaptation of the heritage building to support the reuse of underutilised upper floor commercial areas. The proposed modest extension will not impact on view lines and view cones on Figure 22.6 and views from public spaces

	<p>within the CBD to kunanyi/Mt Wellington to the enclosing ranges of the city. The building sits behind the NAB tower in this only relevant view shown as View B1 on Figure 22.6</p>  <ul style="list-style-type: none">• The accompanying shadow diagrams show that the proposal will have no impact on Elizabeth Street (a pedestrian priority street as shown below by the dotted line).  <ul style="list-style-type: none">• The shadow diagrams also show no overshadowing impacts to nearby public open space including Kemp Street, Purdy's Mart and Collins Street.• The proposed modest extension will not have tangible impacts upon the wind environment at pedestrian level.• the proposed extension to the existing building is considered consistent with the DFCs as discussed above.
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<p>A4</p> <p><i>Building height of development on the same title as a place listed in the Historic Heritage Code, where the specific extent of the heritage place is specified in Table E13.1, and directly behind that place must:</i></p> <p><i>(a) not exceed 2 storeys or 7.5m higher (whichever is the lesser) than the building height of any heritage building within the place, and be set back between 5m and 10m from the place (refer figures 22.4 i and 22.4 ii); and</i></p> <p><i>(b) not exceed 4 storeys or 15m higher (whichever is the lesser) than the building height of any heritage building within the place, and be set back more than 10m from the place (refer figures 22.4 i and 22.4 ii);</i></p> <p><i>or</i></p> <p><i>(c) comply with the building height in clauses 22.4.1 A1 and A2;</i></p> <p><i>whichever is the lesser.</i></p>	<p>The whole of the title forms the listed place and A4/P4 do not apply.</p>
<p>P4</p> <p><i>Development on the same site as a place listed in the Historic Heritage Code and directly behind that place must:</i></p> <p><i>(a) be designed, sited, arranged, finished, constructed or carried out so as to not unreasonably detract from those characteristics of the place which contribute to its historic cultural heritage significance; and</i></p> <p><i>(b) for city blocks with frontage to a Solar Penetration Priority Street in Figure 22.2, not exceed the Amenity Building Envelope illustrated in Figure 22.3, unless it can be demonstrated that the overshadowing of the public footpath on the opposite side of the Solar Penetration Priority Street does not unreasonably impact on pedestrian amenity.</i></p>	<p>The whole of the title forms the listed place and A4/P4 do not apply.</p>
<p>A5</p> <p><i>Building height of development within 15m of a frontage and not separated from a place listed in</i></p>	<p>The proposal does not comply with A5 a) in that the new extension on the Kemp Street frontage adjoins another listed place at 47 Elizabeth Street</p>

<p><i>the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:</i></p> <p><i>(a) not exceed 1 storey or 4m (whichever is the lesser) higher than the facade building height of a heritage building on the same street frontage (refer figure 22.5 ii); and</i></p> <p><i>(b) not exceed the facade building height of the higher heritage building on the same street frontage if the development is between two heritage places (refer figure 22.5 ii);</i></p> <p><i>or</i></p> <p><i>(c) comply with the building height in Clauses 22.4.1 A1 and A2;</i></p> <p><i>whichever is the lesser.</i></p>	<p>and will exceed the height of that building by more than 1 storey.</p> <p>The proposal is therefore to be assessed under P5.</p>
<p>P5</p> <p><i>Building height within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:</i></p> <p><i>(a) not unreasonably dominate existing buildings of cultural heritage significance; and</i></p> <p><i>(b) not have a materially adverse impact on the historic cultural heritage significance of the heritage place;</i></p> <p><i>(c) for city blocks with frontage to a Solar Penetration Priority Street in Figure 22.2, not exceed the Amenity Building Envelope illustrated in Figure 22.3, unless it can be demonstrated that the overshadowing of the public footpath on the opposite side of the Solar Penetration Priority Street does not unreasonably impact on pedestrian amenity.</i></p>	<p>The proposal is considered to satisfy P5 in that:</p> <p>It will not unreasonably dominate particularly from the primary Elizabeth Street aspect and will therefore not have a material adverse impact on the adjacent heritage place at 47 Elizabeth Street.</p> <p>The proposal does not exceed the amenity building envelope to the solar priority Elizabeth Street and will therefore not unreasonably impact on pedestrian amenity. There will be no impact on solar access as demonstrated through the shadow diagrams.</p>

22.4.2 Setback*Objective:*

To ensure that building setback contributes positively to the streetscape and does not result in unreasonable impact on residential amenity of land in a residential zone.

Acceptable Solutions	Performance Criteria
<p>A1</p> <p><i>Building setback from frontage must be parallel to the frontage and must be no more than:</i></p> <p>0 m</p>	Complies.

Design (22.4.3)*Objective*

To ensure that building design contributes positively to the streetscape, the amenity and safety of the public and adjoining land in a residential zone.

Development Standard	Assessment
<p>A1</p> <p><i>Building design must comply with all of the following:</i></p> <p>(a) <i>provide the main pedestrian entrance to the building so that it is clearly visible from the road or publicly accessible areas on the site;</i></p> <p>(b) <i>for new building or alterations to an existing façade provide windows and door openings at ground floor level in the front façade no less than 40% of the surface area of the ground floor level façade;</i></p> <p>(c) <i>for new building or alterations to an existing facade ensure any single expanse of blank wall in the ground level front façade and facades facing other public spaces is not greater than 30% of the length of the facade;</i></p> <p>(d) <i>screen mechanical plant and miscellaneous equipment such as heat pumps, air conditioning units, switchboards, hot water units or similar from view from the street and other public spaces;</i></p> <p>(e) <i>incorporate roof-top service infrastructure, including service plants and lift structures, within the design of the roof;</i></p>	<p>The proposal complies with A1 as follows:</p> <p>a) the main pedestrian entrances will be provided to Elizabeth and Kemp Streets;</p> <p>b) and c) windows and door openings will be provided to both frontages in excess of 40%;</p> <p>d) mechanical plant will be screened from public view; and</p> <p>e) no significant rooftop infrastructure is proposed;</p> <p>f) does not include security shutters.</p>

<i>(f) not include security shutters over windows or doors with a frontage to a street or public place;</i>	
<p>A2</p> <p><i>Walls of a building facing a residential zone must be coloured using colours with a light reflectance value not greater than 40 percent.</i></p>	Not applicable. The proposal does not face a residential zone.
<p>A3</p> <p><i>The facade of buildings constructed within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:</i></p> <p><i>(a) include building articulation to avoid a flat facade appearance through evident horizontal and vertical lines achieved by setbacks, fenestration alignment, design elements, or the outward expression of floor levels; and</i></p> <p><i>(b) have any proposed awnings the same height from street level as any awnings of the adjacent heritage building.</i></p>	<p>Complies.</p> <p>The proposed treatment of the narrow NE façade adjoining another listed place at 47 Elizabeth Street will be articulated with a simple ribbed vertical screen in front of the zig zag of the fire stair and upper level apartments.</p> <p>There are no awnings on the Kemp Street frontage of properties.</p>
<p>A4</p> <p><i>For new buildings or alterations to existing façades within the Active Frontage Overlay (Figure 22.1) provide windows with clear glazing and door openings at ground floor level in the front façade and façades facing other public space boundaries no less than 80% of the surface area;</i></p>	The proposal maintains the ground floor shop front on Elizabeth Street and will provide new clear glazing and doors to Kemp Street and complies with A4.
<p>A5</p> <p><i>For new buildings or alterations to existing façades within the Active Frontage Overlay (Figure 22.1) awnings must be provided over public footpaths.</i></p>	The existing pedestrian awning to the Mall will be maintained. Kemp Street however is also within the Active Frontage Overlay under Figure 22.1 and will not include a pedestrian awning as such but will achieve an overhang of similar utility. The proposal arguably satisfies A5.
<p>P5</p> <p><i>Awnings may not be provided over the public footpath only if there is no benefit to the streetscape or pedestrian amenity.</i></p>	In the event that Council considers that A5 is not met, the proposal is considered to satisfy P5 in that there are no existing awnings in this part of Kemp Street and no benefit to the streetscape or pedestrian amenity is considered likely to be achieved.

Passive Surveillance (22.4.4)*Objective:**To ensure that building design provides for the safety of the public.*

Development Standard	Assessment
<p>A1</p> <p><i>Building design must comply with all of the following:</i></p> <p>(a) <i>provide the main pedestrian entrance to the building so that it is clearly visible from the road or publicly accessible areas on the site;</i></p> <p>(b) <i>for new buildings or alterations to an existing facade provide windows and door openings at ground floor level in the front facade which amount to no less than 40 % of the surface area of the ground floor level facade;</i></p> <p>(c) <i>for new buildings or alterations to an existing facade provide windows and door openings at ground floor level in the facade of any wall which faces a public space or a car park which amount to no less than 30 % of the surface area of the ground floor level facade;</i></p> <p>(d) <i>avoid creating entrapment spaces around the building site, such as concealed alcoves near public spaces;</i></p> <p>(e) <i>provide external lighting to illuminate car parking areas and pathways;</i></p> <p>(f) <i>provide well-lit public access at the ground floor level from any external car park.</i></p>	<p>The proposal satisfies A1 in that:</p> <p>a) the main pedestrian entrances to the site will be clearly visible from the street;</p> <p>b) complies</p> <p>c) complies</p> <p>d) the proposed design avoids the creation of concealed spaces</p> <p>e) Not applicable</p> <p>f) the proposal does not include an external carpark. Not applicable.</p>

Landscaping (22.4.5)

Clause 22.4.5 confirms that landscaping is not regulated in this zone in this planning scheme. It is not considered necessary in the Hobart context.

Outdoor Storage Areas (22.4.6)*Objective:**To ensure that building design provides for the safety of the public.*

Development Standard	Assessment
A1	Not applicable. The proposal does not include any outdoor storage areas.



<p><i>Outdoor storage areas for non-residential uses must comply with all of the following:</i></p> <p>(a) <i>be located behind the building line;</i></p> <p>(b) <i>all goods and materials stored must be screened from public view;</i></p> <p>(c) <i>not encroach upon car parking areas, driveways or landscaped areas.</i></p>	
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Fencing (22.4.7)

No fences are proposed and this Standard therefore does not apply.

Pedestrian Links (22.4.8)

This standard relates to the preservation of existing through site links. It does not apply to this site that does not have existing through site links.

Residential Amenity (Draft 22.4.9)

The proposal is considered against these draft provisions below.

4. Planning Scheme Codes

The site is not within any specific mapped planning scheme overlays. The proposal is considered in relation to the relevant codes below.

4.1 Potentially Contaminated land Code

Preliminary discussions with Council have confirmed that the site is not identified as likely to be contaminated or adjacent to a site that is likely to be contaminated. This Code therefore does not apply.

4.2 Road and Railway Assets Code

On the basis that the proposal does not involve a new access or onsite parking it will not result in increased traffic movements to and from the site. The proposal therefore does not trigger the application of this Code (Clause E5.2).

4.3 Parking and Access Code

This code applies to all use and development.

Use Standards

Number of Car Parking Spaces - Central Business Zone (E6.6.5)

The proposal complies with A1a) in that no on-site parking is provided as part of the proposal.

4.4 Stormwater Management Code

The existing site has 100% site coverage. The proposal therefore does not increase impervious surfaces and will connect to existing stormwater.

4.5 Historic Heritage Code

Heritage Places

The site is listed as Heritage Places under Table E13.1 of the Code.

The land is not within a Heritage Precinct.

The entirety of the CBD area including the subject land is within an Area of Archaeological Potential.

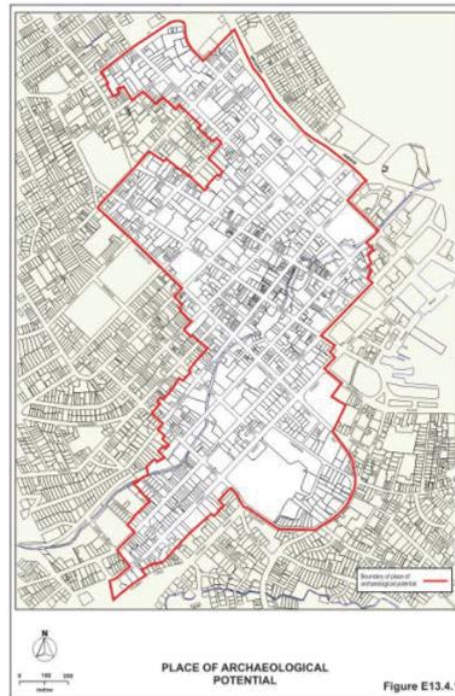


Figure 3 – Place of Archaeological Potential (Figure E13.4.1 of the Historic Heritage Code)

**E13.7 Development Standards for Heritage Places****E13.7.1 Demolition***Objective*

To ensure that demolition in whole or part of a heritage place does not result in the loss of historic cultural heritage values unless there are exceptional circumstances.

Development Standard	Assessment
<p>P1</p> <p><i>Demolition must not result in the loss of significant fabric, form, items, outbuildings or landscape elements that contribute to the historic cultural heritage significance of the place unless all of the following are satisfied;</i></p> <p><i>(a) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;</i></p> <p><i>(b) there are no prudent and feasible alternatives;</i></p> <p><i>(c) important structural or façade elements that can feasibly be retained and reused in a new structure, are to be retained;</i></p> <p><i>(d) significant fabric is documented before demolition.</i></p>	<p>The proposed demolition of existing non compliant or deteriorated structural elements is considered to satisfy P1 as follows:</p> <p>a) There are a number of environmental , social, economic and safety reasons of greater value to the community including;</p> <ul style="list-style-type: none"> • environmental benefits of ‘adaptive re-use’ of older building fabric. Bringing to life existing building fabric with minor demolition. • that the alterations are necessary to facilitate environmental benefits attributed to living in the CBD with less reliance on public transport. • environmental benefits of cutting Light and additional openings into the boundary walls – allowing cross ventilation and light to apartment rooms per floor. • environmental benefits of removing a portion of the roof structure on Elizabeth Street Mall end of building by providing outdoor living amenity for the Level 4 Apartment. • social benefits of bringing apartments into the centre of the CBD. Mixed use, passive surveillance, supporting 24 hour use of facilities, reduces congestion and travel times, establishment of inner urban communities to support local infrastructure. • economic benefits of turning the unused floors into apartments. The unusual proportion of the tenancies per plate have meant the floors have not been leased since Kodak folded.

	<ul style="list-style-type: none"> • Much of the demolition is for safety reasons and to bring services up to compliance levels currently expected in the NCC. In this case the lift, stair, bathrooms, ventilation, room sizes, escape stair compliance, fire rating and sprinkler system require upgrade as do disability access requirements. • There are safety benefits for Kemp Street with improved passive surveillance and lighting associated with the change to residential. <p>b) There are no prudent and feasible alternatives;</p> <ul style="list-style-type: none"> • The building has not been successfully leased or tenanted for many years. Existing Fabric is falling into disrepair and being damaged due to non-use. This points to no feasible alternative than to change use and upgrade accordingly. The consequential minor demolition cannot be avoided. <p>c) Important Structural and façade elements are being retained;</p> <ul style="list-style-type: none"> • The main and important Elizabeth Street Mall façade will be retained with only a small portion of demolition affecting this façade proposed. There is a nominally 3 x 3 metre rendered and painted masonry panel holding a Kodak sign. This sign is culturally significant in the history of the building. It is in slight relief, painted on a raised masonry base. The removal of the wall panel currently supporting the sign is proposed for the increased amenity of the residence behind. The additional light and deck and views from the apartment looking over this wall will add significantly to the future residence. The proposal looks to reinterpret and reinstate the sign over a clear opening. The Sign would be traced and made in equal font and size in a Powdercoated material in the same colour. The proposal is to rehang the new sign in the same location over the opening. The new sign is to be supported by a discrete steel frame in the opening. Of
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	<p>note a similar approach was approved by a previous DA in 2001 for the sign to be placed over a new glass window.</p> <ul style="list-style-type: none"> The existing concrete frame portal structure through the building will be retained and celebrated in the new apartment layouts. The steel escape stair to the Kemp Street Façade needs to be removed and upgraded to meet NCC requirements for safe movement and access. The proposed design interprets this design feature in a contemporary and safe manner using similar materials etc. <p>d) Significant fabric is documented before demolition;</p> <ul style="list-style-type: none"> Existing Drawings are provided as part of the application. It is intended that a photographic record of the building including all fabric to be demolished will be provided to Council as a condition on the permit prior to commencement of works.
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E13.7.2 Buildings and Works other than Demolition*Objective*

To ensure that development at a heritage place is:

(a) undertaken in a sympathetic manner which does not cause loss of historic cultural heritage significance; and

(b) designed to be subservient to the historic cultural heritage values of the place and responsive to its dominant characteristics.

Development Standard	Assessment
<p>P1</p> <p>Development must not result in any of the following:</p> <p>(a) loss of historic cultural heritage significance to the place through incompatible design, including in height, scale, bulk, form, fenestration, siting, materials, colours and finishes;</p>	<p>The proposal is considered to satisfy P1 in that:</p> <p>a) The building works are designed to assist in revitalising the existing building by bringing life and constructional compliance upgrades to the unused and degraded building fabric.</p> <p>The main works are transforming the existing floor plates from un-tenanted commercial plates to residential apartments. These works have been</p>

<p><i>(b) substantial diminution of the historic cultural heritage significance of the place through loss of significant streetscape elements including plants, trees, fences, walls, paths, outbuildings and other items that contribute to the significance of the place.</i></p>	<p>designed with minimal change to the external building fabric and the new apartments are located, in the most part, within the external walls of the building.</p> <p>The proposed new Apartment 5 and Fire New Escape Stair, are the new building elements visible from the public domain.</p> <p>These have been intentionally located at the Kemp Street end of this extremely long and narrow site to minimise visibility from the principal viewpoint in Elizabeth Street. The dominant Heritage characteristics of the Elizabeth Street frontage will remain unchanged as the new bulk of the apartment is located approximately 20m back from the Mall. The location of the new apartment at the Kemp Street end is also driven by a construction overlay, simplifying the construction methodology and logistic of an onsite build with regards to crane lift and access over neighbouring properties.</p> <p>The new material and colour palette of the fire stair and the apartment have been designed in a contemporary manner using steel and aluminum screening and glazing elements in a dark and recessive colour. This element has been designed to achieve a clear contrast between the lighter painted masonry form of the older concrete rendered original building.</p> <p>The form of the new apartment at the upper level has been combined with the design of the fire stair to read as one single new addition, with a simple strong shape complimenting the long slender proportion of the existing building. The vertical expression of the cladding and screening, using steel and aluminum in this façade, has taken cues from the light steel lines of the existing fire stair. The careful screening of the mechanical plant zone and lift overrun within this simple form, and the parapet detailing of the roof form has intentionally been kept minimal and simple. The strong</p>
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	<p>clear parapet lines of the existing construction have been retained and provide a cue for the new forms.</p> <p>The cultural heritage significance of this memorable building will be rejuvenated and complimented by its new use and the new building elements being added.</p> <p>b) The proposal will not involve the loss of streetscape elements.</p>
<p>P2</p> <p><i>Development must be designed to be subservient and complementary to the place through characteristics including:</i></p> <p>(a) <i>scale and bulk, materials, built form and fenestration;</i></p> <p>(b) <i>setback from frontage;</i></p> <p>(c) <i>siting with respect to buildings, structures and listed elements;</i></p> <p>(d) <i>using less dominant materials and colours.</i></p>	<p>The proposal is considered to satisfy P2 in that:</p> <p>a) As noted above in P1.</p> <p>b) The location of significant new works has been pushed some 20m back from the Elizabeth Street Mall Frontage.</p> <p>c) The building siting is not affected</p> <p>d) Use of less dominant materials as noted above in P1.</p>
<p>P3</p> <p><i>Materials, built form and fenestration must respond to the dominant heritage characteristics of the place, but any new fabric should be readily identifiable as such.</i></p>	<p>As discussed in relation to P1 above, the new fabric, material, form, texture and colour clearly identify the new works and satisfy this criteria.</p>
<p>P4</p> <p><i>Extensions to existing buildings must not detract from the historic cultural heritage significance of the place.</i></p>	<p>As discussed in relation to P1 above, the cultural heritage significance of this memorable building will be rejuvenated and complimented by the new building elements and P4 is considered satisfied.</p>



E13.10 Development Standards for Places of Archaeological Potential

Objective

To ensure that building, works and demolition at a place of archaeological potential is planned and implemented in a manner that seeks to understand, retain, protect, preserve and otherwise appropriately manage significant archaeological evidence.

Development Standard	Assessment
<p>P1</p> <p><i>Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:</i></p> <p>(a) <i>the nature of the archaeological evidence, either known or predicted;</i></p> <p>(b) <i>measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;</i></p> <p>(c) <i>strategies to avoid, minimise and/or control impacts arising from building, works and demolition;</i></p> <p>(d) <i>where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;</i></p> <p>(e) <i>measures proposed to preserve significant archaeological evidence 'in situ'.</i></p>	<p>An archaeological assessment prepared by Praxis accompanies the application and demonstrates that the site has been substantially disturbed and is not considered to have a high archaeological potential.</p>

4.6 Inundation Prone Areas Code

Preliminary advice from Council has confirmed that there is no need to undertake a full inundation report.

4.7 Signage

The proposed alterations to the existing Kodak sign on the Elizabeth Street façade have been designed to minimise impacts on the significance of the place and are therefore considered to satisfy the Signs Code to the extent that it complies including E17.7.2.

4.8 Amenity Standards in the Central Business Zone

Draft Amendment PSA-19-1 is before the Tasmanian Planning Commission and may come into effect before this application is determined. The following considers the proposal against the draft modified amendment in Council's submission to the TPC 19 May 2020.

Residential and Visitor Accommodation Amenity (22.4.9)*Objective:*

To ensure that buildings for residential or visitor accommodation uses provide reasonable levels of amenity and safety in terms of noise, access to daylight and natural ventilation, open space and storage

Development Standard	Assessment
<p>A1</p> <p><i>Residential or visitor accommodation development must demonstrate that design elements are able to achieve internal noise levels in accordance with relevant Australian Standards for acoustics control (AS3671:1989 – Road Traffic Noise Intrusion (Building Siting and Construction) and AS2107: 2016 – Acoustics (Recommended Design Sound Levels and Reverberation Times for Building Interiors)).</i></p>	<p>In the case of this proposal there are many design elements used to ameliorate Traffic Noise Intrusion and will ensure that internal noise levels under AS2107: 2016 and A1 can be met including:</p> <p>General Location of the site:</p> <p>The apartments straddle between Elizabeth Street Pedestrian Mall and Kemp Street. Both are very low level traffic streets, The most significant low level traffic noise issues would be from infrequent vehicles for servicing. The closest busy traffic street is Collins, located approximately 40m away. Traffic here is moving under 50Km hour with frequent stopping for intersection traffic lights. The distance is a great advantage in reducing the noise source feature.</p> <p>Location of Apartments:</p> <p>The apartments are on Levels 1 – 6. The elevated apartment Levels, above the commercial tenancy and lobby at ground level, allow noise protection from any passing pedestrian or vehicular traffic.</p> <p>Location of Bedrooms:</p> <p>Main Bedrooms are located off the light well . The bedrooms are protected by two layers of windows, the bedroom light-well window and a further window in the external boundary. This will significantly reduce low level frequency (traffic Noise) when sleeping.</p> <p>Location of Living Rooms:</p> <p>The heritage listed façade onto Elizabeth Street Mall has a single glazed, timber framed bay window for</p>

	<p>apartments 1-3. These glazed sections are not proposed to be reworked due to their heritage significance. The proportion of glazing in the wall opening of the room is relatively small. The wall elements will be internally insulated and lined. This will increase the acoustic isolation of these rooms to some degree.</p> <p>New External Windows and Doors:</p> <p>All new windows and door suites will be aluminium framed, laminated double glazed units. This type of glazing will assist in reducing the noise transmission from any external factors. In addition, the new external windows within the elevation of Kemp street are required to be Fire-rated so as not to act as a fire source feature to the escape stair. There can be no openings in this window suite. These factors will significantly reduce noise transmission from external sources.</p>
<p>A2</p> <p><i>Residential or serviced apartment components of a new building (including external elements such as a balcony, roof garden, terrace or deck) must:</i></p> <p><i>(a) if the building includes any single aspect dwellings or single aspect serviced apartments, be set back at least 5m from all side or rear boundaries and other buildings on the same site (refer Diagram 22.4.9A); or</i></p> <p><i>(b) if the building includes no single aspect dwellings and no single aspect serviced apartments, have at least two elevations of the building, and all habitable room windows, that are either:</i></p> <p><i>(i) set back at least 5m from a side or rear boundary or other building on the same site; or</i></p> <p><i>(ii) facing a frontage (refer Diagram 22.4.9B); or</i></p> <p><i>(c) be designed around an open courtyard or void with a minimum horizontal dimension of 5m in all directions, where:</i></p> <p><i>(i) the height is no greater than 4 12m; and</i></p> <p><i>(ii) at least two elevations of the building face a frontage.</i></p>	<p>Not applicable – the proposal is for adaptive reuse of an existing building rather than a new building.</p>

<p>P2</p> <p><i>Residential or serviced apartment components of a new building must be designed to allow for reasonable access to daylight into habitable rooms and private open space, and reasonable opportunity for air circulation and natural ventilation, having regard to:</i></p> <p><i>(a) proximity to side and rear boundaries;</i></p> <p><i>(b) proximity to other buildings on the same site;</i></p> <p><i>(c) the height and bulk of other buildings on the same site;</i></p> <p><i>(d) the size of any internal courtyard or void;</i></p> <p><i>(e) the use of light wells or air shafts;</i></p> <p><i>(f) development potential on adjacent sites, considering the zones and codes that apply to those sites; and</i></p> <p><i>(g) any assessment by a suitably qualified person.</i></p>	<p>Not applicable see A2 above.</p>
<p>A3</p> <p><i>Every habitable room in a dwelling:</i></p> <p><i>(a) must have at least one external window;</i></p> <p><i>(b) must have at least one external window visible from all points of the room if a living room;</i></p> <p><i>(c) where the only external window in the room is located within a recess, that recess must be:</i></p> <p><i>(i) a minimum width of 1.2m; and</i></p> <p><i>(ii) a maximum depth of 1.5 times the width, measured from the external surface of the external window; and</i></p> <p><i>(d) must have a room depth from an external window of:</i></p> <p><i>(i) not more than 2.5 times the ceiling height; or</i></p> <p><i>(ii) if an open plan layouts (where the living, dining and kitchen are combined), not more than 8m.</i></p>	<p>Complies.</p> <p>a) All habitable rooms will have a window or light well</p> <p>b) All living rooms will have at least one external window visible from all points of the room</p> <p>c) Not applicable – external windows are not located in a recess</p> <p>d) The proposal complies in that the depth of open plan living/dining rooms are less than 8m including apartment 4 that has a window to both Elizabeth Street and the light well</p>
<p>P3</p>	<p>A3 is satisfied.</p>

<p><i>Every habitable room in a dwelling must have reasonable access to natural daylight and ventilation from an external window, having regard to:</i></p> <ul style="list-style-type: none"> <i>(a) the orientation of the room;</i> <i>(b) the size and location of windows;</i> <i>(c) the size of the room;</i> <i>(d) the ceiling height;</i> <i>(e) the opportunity for cross-ventilation;</i> <i>(f) the proposed use of the room;</i> <i>(g) overshadowing of the site from existing development;</i> <i>(h) existing site constraints; and</i> <i>(i) any assessment by a suitably qualified person.</i> 	
<p>A4</p> <p><i>Private open space must be provided for at least 100% of dwellings or serviced apartments on a site.</i></p>	<p>The characteristics of the existing building and heritage site are such that private open space cannot be provided for all dwellings. Apartment 4 has a small deck on the Elizabeth Street frontage. The proposal is therefore assessed under P4.</p>
<p>P4</p> <p><i>Private open space may be provided to less than 100% of dwellings or serviced apartments on a site if:</i></p> <ul style="list-style-type: none"> <i>(a) communal open space is provided on site that exceeds size requirements under 22.4.9 A6 by 10m² for each dwelling unit or serviced apartment without private open space and is of high quality in terms of location, access to sunlight, outlook, facilities, landscaping and accessibility;</i> <i>(b) environmental conditions such as high winds or high levels of noise would significantly diminish the amenity of the private open space and this is unable to be mitigated by screening that does not unreasonably reduce access to daylight, as demonstrated by a suitably qualified person; or</i> <i>(c) the dwelling or serviced apartment is in an existing building that cannot reasonably accommodate private open space due to site constraints, or impacts on historic cultural heritage</i> 	<p>P4c) applies and is satisfied given the existing characteristics of the heritage place cannot reasonably accommodate private open space for all dwellings.</p> <p>Apartment 5 includes juliet balconies on Levels 6 and 7 that will allow the whole living area to convert to an outdoor space.</p>



<p><i>values of a place or precinct listed in the Historic Heritage Code.</i></p>	
<p>A5</p> <p><i>Private open space for dwellings or serviced apartments must:</i></p> <p><i>(a) have an area not less than:</i></p> <p><i>(i) 8m2 for 1 bedroom dwellings or serviced apartments;</i></p> <p><i>(ii) 10m2 for 2 bedroom dwellings or serviced apartments;</i></p> <p><i>(iii) 12m2 for 3 or more bedroom dwellings or serviced apartments;</i></p> <p><i>(b) not include plant and equipment such as outdoor components of an air conditioning unit.</i></p> <p><i>(c) unless drying facilities are provided elsewhere on the site, include a clothes drying area of at least 2m2 in addition to the minimum area in (a) above, that may be in a separate location, and is screened from public view;</i></p> <p><i>(d) have a minimum horizontal dimension of 2m, or 1.5m for a 1 bedroom dwelling or serviced apartment;</i></p> <p><i>(e) where above ground floor level, not be located within 5m of private open space of any other dwelling or serviced apartment in another building (excluding between conjoined terrace-style dwellings or serviced apartments); and</i></p> <p><i>(f) be screened visually and acoustically from mechanical plant and equipment, service structures and lift motor rooms</i></p>	<p>The proposal is assessed under P5.</p>
<p>P5</p> <p><i>Private open space for dwellings or serviced apartments must provide reasonable amenity and be capable of meeting the projected outdoor recreation requirements of occupants, having regard to:</i></p> <p><i>(a) the size and minimum dimensions of the space, excluding space occupied by plant and equipment such as outdoor components of an air conditioning unit;</i></p>	<p>The POS for apartment 4 overlooking Elizabeth Street Mall and directly accessible from the living area will provide interest and reasonable amenity having regard to these considerations.</p>

<p><i>(b) the amount of space available for furniture or plantings;</i></p> <p><i>(c) the potential for significant noise intrusion;</i></p> <p><i>(d) proximity and overlooking to the private open space of existing adjacent residential and serviced apartment developments;</i></p> <p><i>(e) screening where necessary for privacy that does not unreasonably restrict access to daylight;</i></p> <p><i>(f) screening where necessary for noise and wind protection that does not unreasonably restrict access to daylight;</i></p> <p><i>(g) screening from public view for clothes drying areas; and</i></p> <p><i>(h) any advice from a suitably qualified person.</i></p>	
<p>A6</p> <p><i>Sites with 10 or more dwellings or serviced apartments must provide communal open space on the site that:</i></p> <p><i>(a) is at least 70m² , with an additional 2m² for every dwelling or serviced apartment over 10;</i></p> <p><i>(b) if provided in multiple locations, at least one single area must be a minimum of 40m² ;</i></p> <p><i>(c) has a minimum horizontal dimension of 3m;</i></p> <p><i>(d) includes at least 20% of the total area for plantings (including food growing), being deep soil planting if at ground level;</i></p> <p><i>(e) is directly accessible from common entries and pathways;</i></p> <p><i>(f) screens any communal clothes drying facilities from public view;</i></p> <p><i>(g) may be above ground floor level, including rooftops;</i></p> <p><i>(h) is screened visually and acoustically from mechanical plant and equipment, service structures and lift motor rooms;</i></p> <p><i>(i) does not include vehicle driveways, manoeuvring or hardstand areas; and</i></p>	<p>This standard does not apply to the proposal for 5 dwellings.</p>

<p><i>(j) includes no more than 20% of the total area located between 30 degrees East of South and 30 degrees West of South of:</i></p> <p><i>(i) a building on the site with a height more than 3m; or</i></p> <p><i>(ii) a side or rear boundary within 5m</i></p>	
<p><i>P6</i></p> <p><i>Sites with 10 or more dwellings or serviced apartments must provide communal open space on the site that provides reasonable amenity and outdoor recreation opportunities for occupants, having regard to:</i></p> <p><i>(a) the area and dimensions of the space;</i></p> <p><i>(b) the total number of dwellings or serviced apartments on the site;</i></p> <p><i>(c) the accessibility of the space;</i></p> <p><i>(d) the flexibility of the space and opportunities for various forms of recreation;</i></p> <p><i>(e) the availability and location of common facilities within the space;</i></p> <p><i>(f) landscaping;</i></p> <p><i>(g) the provision of gardens, trees and plantings (including food gardens) appropriate in area to the size of the communal open space;</i></p> <p><i>(h) accessibility to daylight, taking into account the development potential of adjacent sites;</i></p> <p><i>(i) the outlook from the space;</i></p> <p><i>(j) the level of noise intrusion from external noise sources; and</i></p> <p><i>(k) any advice from a suitably qualified person; unless:</i></p> <p><i>(i) the dwellings or serviced apartments are located in an existing building where communal open space cannot be reasonably achieved due to site constraints, or impacts on historic cultural heritage values of a place or precinct listed in the Historic Heritage Code;</i></p>	<p>See A6 above.</p>

<i>(ii) public open space that is of high quality in terms of location, access to sunlight, outlook, facilities, landscaping and accessibility and that can adequately accommodate the</i>	
A7 <i>Each multiple dwelling must be provided with a dedicated and secure storage space of no less than 6m³, located externally to the dwelling.</i>	The proposal includes secure storage in the basement for each dwelling that satisfies this requirement.
P7 <i>Each multiple dwelling must be provided with adequate storage space.</i>	See A7 above.

Waste Storage and Collection (22.4.10)*Objective:*

To ensure the storage and collection of waste provides for a reasonable level of amenity and safety for surrounding occupants and for traffic, cyclists, pedestrians and other road and footpath users.

Development Standard	Assessment
A1 <i>Bulk waste bins that are commercially serviced must be provided for sites:</i> <i>(a) with more than one commercial tenancy;</i> <i>(b) with one commercial tenancy that is greater than 100m²;</i> <i>(c) with more than 4 dwellings or visitor accommodation units (or 3 if a mixed use site); and</i> <i>(d) with more than 2 dwellings or visitor accommodation units (or 1 if a mixed use site) if fronting a pedestrian priority street (Figure E6.7.12);</i> <i>unless:</i> <i>(i) there are no more than 4 individual bins for kerbside collection at any one time per commercial site or any site fronting a pedestrian priority street (Figure E6.7.12);</i> <i>(ii) There are no more than 8 individual bins for kerbside collection at any one time per residential or mixed use site not fronting a pedestrian priority street (Figure E6.7.12); or</i>	The waste management approach is detailed in Section 2 above and relies on commercial service from the Kemp Street kerbside. The proposal is therefore assessed under P1.

<p>(iii) <i>Individual bins are commercially serviced without being placed on the kerbside for collection.</i></p>	
<p>P1</p> <p><i>Bulk waste bins that are commercially serviced must be provided unless kerbside collection would not unreasonably compromise the amenity of the surrounding area or the flow and safety of vehicles, cyclists and pedestrians, and:</i></p> <p>(a) <i>the frontage of the site has a width equivalent to 5m for each dwelling, accommodation unit or tenancy with individual bins; or</i></p> <p>(b) <i>bulk waste bin storage and collection cannot reasonably be provided on site due to:</i></p> <p>(i) <i>impacts on historic cultural heritage values of a place or precinct listed in the Historic Heritage Code; or</i></p> <p>(ii) <i>site constraints, if for an existing building.</i></p>	<p>The waste management approach is outlined in Section 2 above and satisfies the requirements of P1.</p>
<p>A2</p> <p><i>An on-site storage area, with an impervious surface (unless for compostables), must be provided for bins that:</i></p> <p>(a) <i>if for separate bins per dwelling, visitor accommodation or commercial tenancy:</i></p> <p>(i) <i>provides an area for the exclusive use of each dwelling, accommodation unit or tenancy, and is not located between the building and a frontage;</i></p> <p>(ii) <i>is set back not less than 4.5m from a frontage unless within a fully enclosed building;</i></p> <p>(iii) <i>is not less than 5.5m horizontally from any dwelling or accommodation unit unless for bins associated with that dwelling, or within a fully enclosed building; and</i></p> <p>(iv) <i>is screened from the frontage and any dwelling or accommodation unit by a wall to a</i></p>	<p>The basement level plan and waste management plan outlined in Section 2 above demonstrate that A2 is satisfied.</p>

<p><i>height not less than 1.2m above the finished surface level of the storage area.</i></p> <p><i>(b) If for bulk waste bins:</i></p> <p><i>(i) is located on common property;</i></p> <p><i>(ii) Includes dedicated areas for storage and management of recycling and compostables;</i></p> <p><i>(iii) is not less than 5.5m from any dwelling or accommodation unit unless within a fully enclosed building;</i></p> <p><i>(iv) is set back not less than 4.5m from a frontage if fronting a pedestrian priority street (Figure E6.7.12);</i></p> <p><i>(v) is screened from any public road, dwelling or accommodation unit by a wall to a height not less than 1.8m above the finished surface level of the storage area;</i></p> <p><i>(vi) is accessible to each dwelling, accommodation unit or tenancy without the requirement to travel off-site; and</i></p> <p><i>(vii) where the development is mixed use, have separate storage spaces for commercial and residential bins with separate access to each.</i></p>	
<p><i>P2</i></p> <p><i>A storage area for waste and recycling bins must be provided that is:</i></p> <p><i>(a) capable of storing the number of bins required for the site;</i></p> <p><i>(b) of sufficient size to enable convenient and safe access and manoeuvrability for occupants, and waste collection vehicles where relevant;</i></p> <p><i>(c) in a location on-site that is conveniently and safely accessible to occupants, without compromising the amenity and flow of public spaces;</i></p> <p><i>(d) screened from view from public spaces and dwellings or accommodation units; and</i></p> <p><i>(e) if the storage area is for common use, separated from dwellings or units on the site to minimise impacts caused by odours and noise.</i></p>	<p>See A2 above.</p>

<p>A3</p> <p><i>Bulk waste bins must be collected on site by private commercial vehicles, and access to storage areas must:</i></p> <p><i>(a) in terms of the location, sight distance, geometry and gradient of an access, as well as off-street parking, manoeuvring and service area, be designed and constructed to comply with AS2890.2:2018: Parking Facilities: - Off-Street Commercial Vehicle Facilities;</i></p> <p><i>(b) ensure the vehicle is located entirely within the site when collecting bins; and</i></p> <p><i>(c) include a dedicated pedestrian walkway, alongside or independent of vehicle access ways.</i></p>	<p>The proposed kerbside pick up is to be assessed under P3.</p>
<p>P3</p> <p><i>A waste collection plan demonstrates the arrangements for collecting waste do not compromise the safety, amenity and convenience of surrounding occupants, vehicular traffic, cyclists, pedestrians and other road and footpath users, having regard to:</i></p> <p><i>(a) the number of bins;</i></p> <p><i>(b) the method of collection;</i></p> <p><i>(c) the time of day of collection;</i></p> <p><i>(d) the frequency of collection;</i></p> <p><i>(e) access for vehicles to bin storage areas, including consideration of gradient, site lines, manoeuvring, direction of vehicle movement and pedestrian access;</i></p> <p><i>(f) distance from vehicle stopping point to bins if not collected on site;</i></p> <p><i>(g) the traffic volume, geometry and gradient of the street; and</i></p> <p><i>(h) the volume of pedestrians using the street and whether it is a pedestrian priority street (Figure E6.7.12).</i></p>	<p>The proposed waste management plan shown on the basement plan and described in Section 2 above are considered to satisfy P3.</p>

5. Conclusion

Kodak House was constructed in 1920 to capitalise on the growing recreational photography market, with the top floor added in 1929. Overlooking the Elizabeth Street Mall, the building is memorable for its slender appearance, yellow colouring and its iconic KODAK signage.

The proposal, retaining ground floor commercial use, is to upgrade the building and convert the upper levels from vacant commercial tenancies to residential including new internal and external access stairs, light well and internal lift. A top floor apartment extension is also proposed at the north east, Purdy's Mart end of the building.

The design is considered a sensitive and well resolved contemporary adaptation of the heritage building to support the reuse of underutilised upper floors. It is considered to demonstrate a high degree of compliance with the planning scheme including the height and siting standards for the Central Business Zone and the Historic Heritage Code. The cultural heritage significance of this memorable building will be rejuvenated and complimented by its new use and additions.

The proposal has also been assessed to satisfy Council's proposed residential amenity standards for the Central Business Zone that are currently before the Tasmanian Planning Commission.

The proposal is assessed to comply with the planning scheme and a permit should be issued following public advertisement.

Frazer Read

Principal

DRAWING LEGEND

PROJECT DETAILS	
DESIGNING NAME	Suez Canal
ADVERTISED NUMBER	CE 3471
OWNER / CLIENT	Shawson Contractors in Development
PROJECT ADDRESS	Al-Hakim, Tia 1980
LAND LOT NO.	23181/1
CONTAINING (SQUARE AND VOLUME)	
KITAL, LUGER AREA	1 ha 12m ²
	Covering 78m ²
	Planned 100m ²
	Blocks 1x4
DESIGN NAME TITLE	TRC
SOIL CLASSIFICATION	TRC
EXISTING FREE ZONE AREA (SQUARE METERS)	5/4
APPROX. AREA	
CORRECTION TO INFLUENT	TRC
STREET NUMBER SITE NUMBER (STREET, LANDSCAPE, EXISTING SOIL, NAME, SITE NO., SITES, NAME, NUMBER, LANDSCAPE)	TRC
CLIMATE ZONE	TRC
PLANNING ZONE	TRC

DRAWING No.	DESCRIPTION
D401	COVER PAGE & DRAWINGS SCHEDULE
D402	GENERAL NOTES
D403	PROPOSED SITE PLAN / ROOF PLAN
D404	PROPOSED FLOOR PLANS
D405	SECTIONS
D406	ELEVATIONS 01
D407	ELEVATIONS 02
D408	ELEVATIONS 03
D409	ELEVATIONS 04
D410	SHADOW DIAGRAMS 01
D411	SHADOW DIAGRAMS 02

PROJECT SUMMARY				
LEVEL	EXISTING FLOOR AREA, m^2	PROPOSED FLOOR AREA, m^2	DECKS, m^2	USE
BASEMENT	125	123	-	-
LEVEL 1 / GROUND	123	115	-	1 x COMMERCIAL
LEVEL 2	125	119	-	1 x APARTMENT
LEVEL 3	125	119	-	1 x APARTMENT
LEVEL 4	125	119	-	1 x APARTMENT
LEVEL 5	125	112	8	1 x APARTMENT
LEVEL 6	-	111	2	1 x APARTMENT
LEVEL 7	-	46	4	1 x APARTMENT
TOTAL	706m²	803m²	16m²	4 x APARTMENTS + 1 COMMERCIAL EXISTING

JACOB ALLIEN WADE PTY LTD
ARL 82 088 250 470
THE ORDINANCE OFFICE
21 CAULTRAY SQUARE
BATTERY POINT TASMANIA,
6017 MALM 7904

TELEPHONE: 05 8325 4000
FAX: 05 8322 5728
jawa@jacoballenwade.com
www.jacoballenwade.com

45 ELIZABETH ST

45 ELIZABETH ST

Glennco Construction & Development

DRAWING

DRAWING NAME

DRAWING NO. **20035_DA01**

SCALE	60-80
DATE	August 2000
DRAWN	TJ
CHECKED	CW

ACCEPTED DESIGN
ACCEPTED FRAMES
PLOT DATE
CNO NO

S/S
CC MTH
E/NO/2020

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STATUS

DEVELOPMENT APPLICATION

VERSION	DESCRIPTION	DATE
00	LOWEN: CARGO AND FLIGHT DETAIL	12/06/2020



DO NOT SCALE DRAWINGS. WHENEVER DIMENSIONS ARE GIVEN, THEY SHALL BE THE DIMENSIONS SHOWN ON THE DRAWING. ALL DIMENSIONS SHALL BE GIVEN IN FEET AND INCHES. ALL DIMENSIONS SHALL BE GIVEN TO THE CENTER OF THE DIMENSION LINE. ALL DIMENSIONS SHALL BE GIVEN TO THE CENTER OF THE DIMENSION LINE. ALL DIMENSIONS SHALL BE GIVEN TO THE CENTER OF THE DIMENSION LINE. ALL DIMENSIONS SHALL BE GIVEN TO THE CENTER OF THE DIMENSION LINE.

NOTE: INFORMATION OF OTHER SERVICES INDICATED ON PLANS ARE INDICATIVE ONLY FOR GENERAL CONSIDERATION. FINAL LOCATION OF ALL UTILITIES SHALL BE DETERMINED AND VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION.

THESE PLANS AND SPECIFICATIONS ARE TO BE USED TO IDENTIFY THE EXISTING CONDITIONS OF THE PROPERTY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXISTING CONDITIONS OF THE PROPERTY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXISTING CONDITIONS OF THE PROPERTY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXISTING CONDITIONS OF THE PROPERTY.

DRAWING LEGEND



JASON ALLEN ARCHITECTS LTD
4000 100th Street
Edmonton, Alberta T6A 1K6
Canada
Phone: 780.441.1111
Fax: 780.441.1112
www.jasonallenarchitects.com

45 ELIZABETH ST
45 ELIZABETH ST
Edmonton, T6A 1K6
For
Glenn Construction & Development

DRAWING
DRAWING NAME: DEMOLITION PLANS

DRAWING NO: 20035_DA02
SCALE: 1/8" = 1'-0"
DATE: August 2020
DRAWN BY: JAL
CHECKED BY: JAL
APPROVED BY: JAL
DATE: 08/10/2020
PROJECT: 45 ELIZABETH ST
SHEET: 001 OF 001

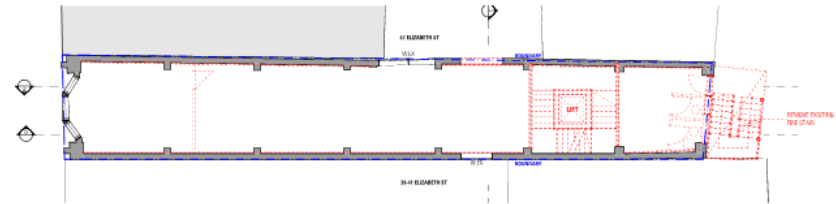
STATUS

DEVELOPMENT APPLICATION

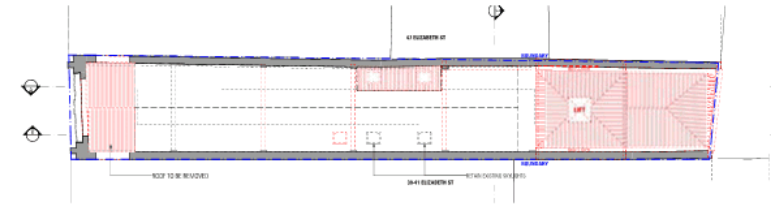
REVISION	DESCRIPTION	DATE
1	ISSUED FOR PERMIT	08/10/2020



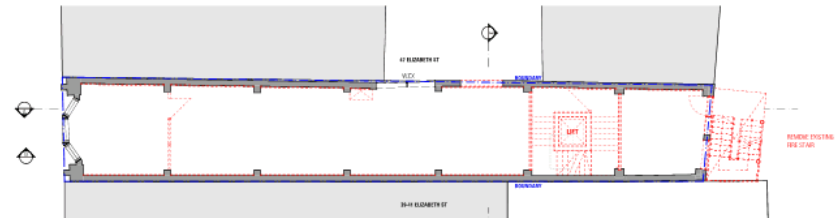
Copyright © 2020 Jason Allen Architects Ltd.



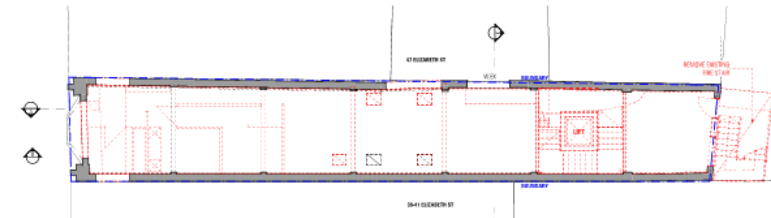
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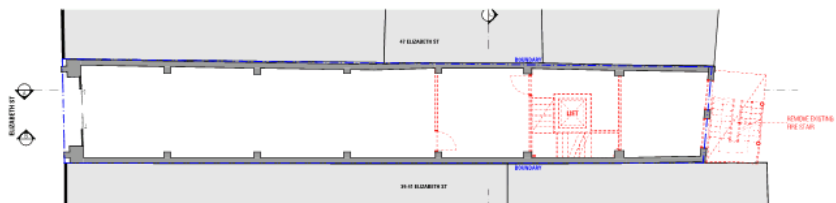
LEVEL 5/ EXISTING ROOF PLAN
Scale 1/8" = 1'-0"



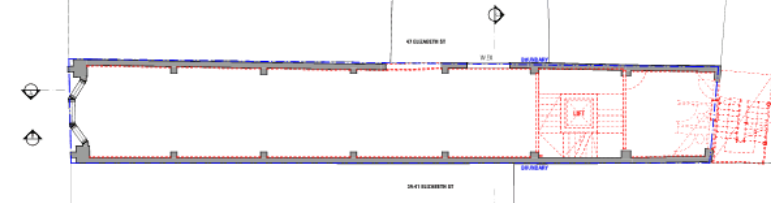
LEVEL 1
Scale 1/8" = 1'-0"



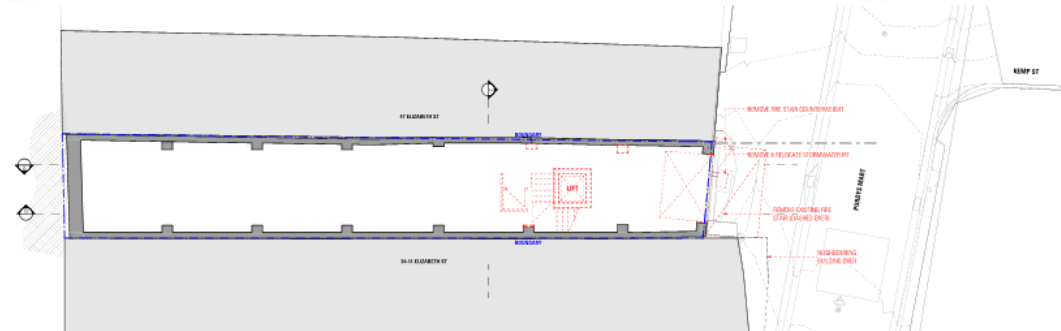
LEVEL 4
Scale 1/8" = 1'-0"



GROUND FLOOR
Scale 1/8" = 1'-0"

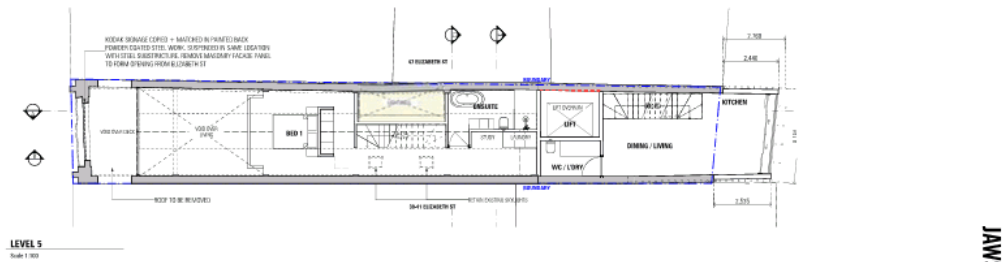
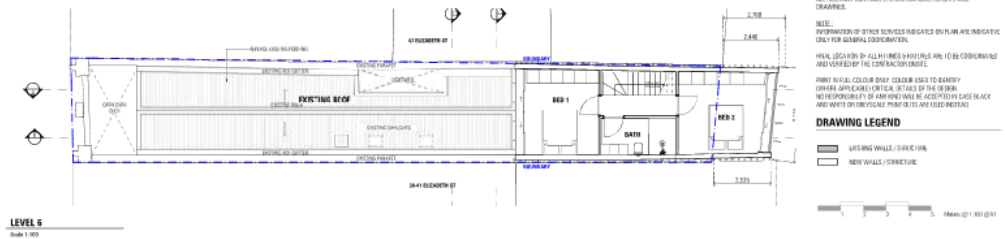


LEVEL 3
Scale 1/8" = 1'-0"



LOWER GROUND
Scale 1/8" = 1'-0"





WM

JACOB SALERNO HAS BE PTY LTD

21 CASTRAY'S PLUNGE
BATTERY POINT TASMANIA.

TELEPHONE: 03 8323 4300
E-MAIL: info@ccr.com.au

www.palmer.co.uk

45 ELIZABETH ST

Heart, The 2008

Glennco Construction & Development

DRAWING

PROPOSED FLOOR PLANS

20035 DA04

DE(13) CW

FLOP DATE: 1408072230
CAO NO: F10000-99-BLVDG-DURHAM-FLEETMANBURNHAM-FLYDASH
10000-99-BLVDG-DURHAM-FLEETMANBURNHAM-FLYDASH

STATUS

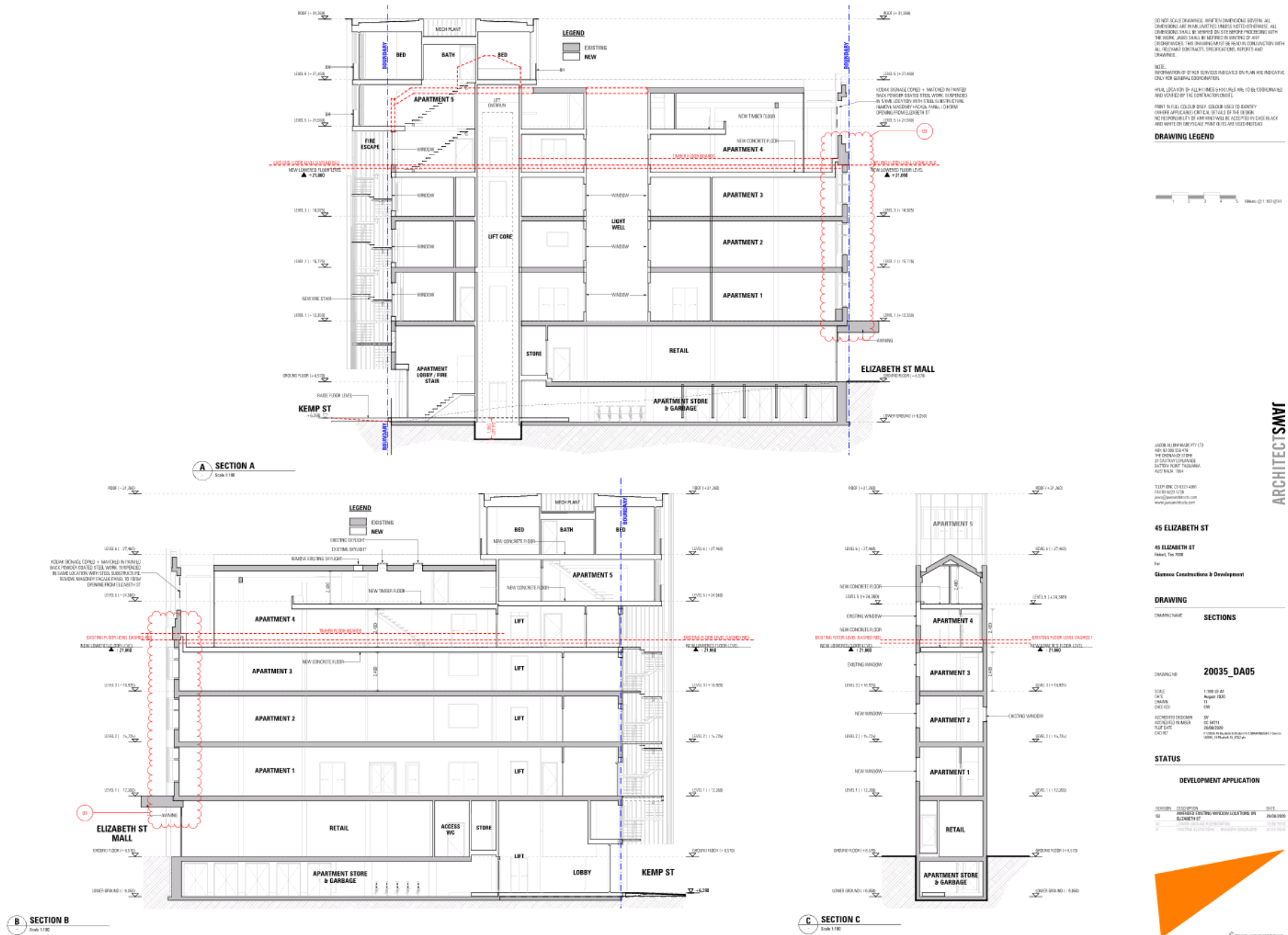
DEVELOPMENT APPLICATION

Q2 - 1997	Q3 - 1997	Progress
20	20	100%

100

100

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From: [Frazer Read](#)
To: [Victoria Maxwell](#)
Cc: [Frazer Read](#)
Subject: FW: 45 Elizabeth Street | Kodak | PLN-20-524
Date: Tuesday, 15 September 2020 6:20:50 AM
Attachments: [image002.png](#)
[image003.png](#)
[20_0371_20200914 H.pdf](#)
[20035_45 Elizabeth St_DA04 REV02.pdf](#)

Dear Victoria,

Please see email below confirming new garbage layout OK from Cam Cecil.

Please find attached the updated DA04_ Rev 02 showing the basement changes for garbage and access to meters - and updated Concept Services plan showing the response to the TAS WATER RFI portion, including better access and clearway to the stacked meters.

I will lodge the information through the Portal.

Regards

Frazer Read
Principal

Call 0400 109 582 Email frazer@allurbanplanning.com.au
19 Mawhera Ave, Sandy Bay Tasmania 7005
allurbanplanning.com.au

AllUrbanPlanning



From: Cameron Cecil [<mailto:cecilc@hobartcity.com.au>]
Sent: Monday, 14 September 2020 4:37 PM
To: Catherine Williams <Catherine.Williams@jawsarchitects.com>; Victoria Maxwell <maxwellv@hobartcity.com.au>
Cc: Karen Abey <abeyk@hobartcity.com.au>; Ben Ikin <ikinb@hobartcity.com.au>
Subject: RE: 45 Elizabeth Street | Kodak | PLN-20-524

Hi Catherine,

Per discussion 4x330L bins is acceptable, and it would be better if the ramp was done in two 150mm lifts. For future projects I would encourage you to make use of the sustainability Victoria guide to waste management for multi-unit developments as the Council have no problem supporting waste

plans not completed by a specialist providing they meet the recommendations in this guide. Similarly, I note that the online calculator has a print function, and inclusion of the output from the calculator would be recognised and supported (and would save me from having to do it when completing my assessment!).

Regards,

Cameron Cecil B.Eng (civil)
Development Engineer | City Planning



City of **HOBART**

16 Elizabeth Street, Hobart, Tasmania, Australia, 7000 | hobartcity.com.au
Telephone (03) 6238 2188

From: Catherine Williams [<mailto:Catherine.Williams@jawsarchitects.com>]

Sent: Friday, 11 September 2020 12:44 PM

To: Victoria Maxwell <maxwellv@hobartcity.com.au>; Cameron Cecil <cecilc@hobartcity.com.au>

Subject: FW: 45 Elizabeth Street | Kodak | PLN-20-524

Importance: High

CAUTION: This email originated from outside of the organisation. Do not click links or open attachments unless you recognise the sender and know the content is safe.

Hi Cameron

We just received an RFI about Garbage. Victoria gave me your name.

I have attached a slightly altered version . This looks to using 4 x 330L bins and pushing up a short 1:8 ramp directly accessing the door.

It's a tricky site and unfortunately we don't have too many options down here.

Hoping this reconfiguration may address your concerns about manoeuvring the larger 660 bins in such a contorted manner.

I am hoping to speak with you before we resubmit formally to try and short cut the process.

Kind regards

Cath

CATHERINE WILLIAMS ASSOCIATE

DIRECT LINE: 03 6218 2105 | www.jawsarchitects.com



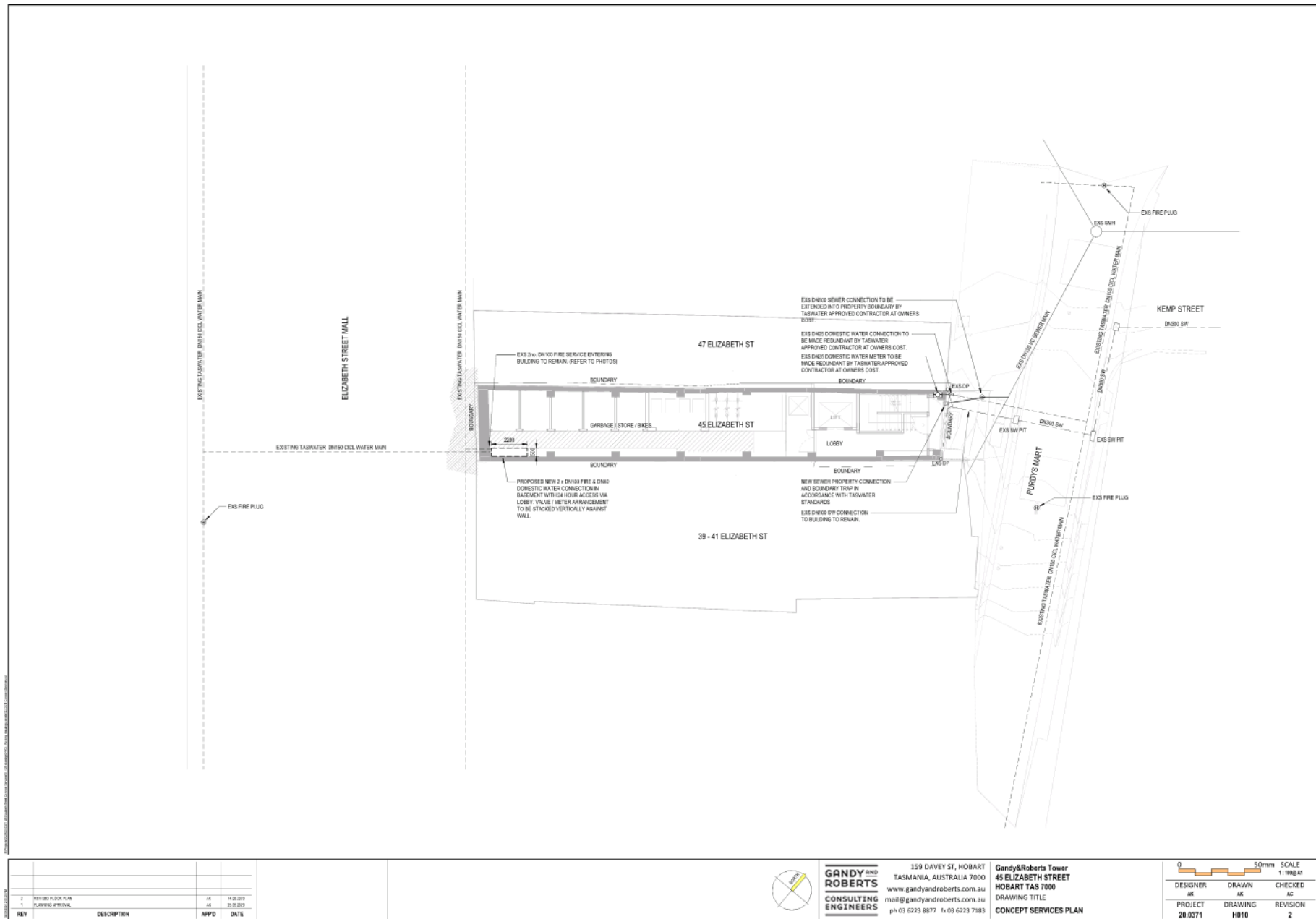
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NOTE: INFORMATION OF OTHER SERVICES INDICATED ON PLANS ARE INDICATIVE ONLY FOR GENERAL CONSTRUCTION.

REAR LOCATION OF ALL HALLWAYS SHALL BE TO BE CONSIDERED AND VERIFIED BY THE CONSTRUCTION TEAM.

REAR WALL COLOR MAY BE USED TO IDENTIFY (UNDER APPLICABLE CRITERIA) DETAILS OF THE DESIGN. NO DIMENSIONS OF ANY PART SHALL BE ACCEPTED FOR CONSTRUCTION AND SHALL BE VERIFIED BY THE CONSTRUCTION TEAM.

DRAWING LEGEND

EXISTING WALLS / STRUCTURE

NEW WALLS / STRUCTURE

Scale: 1/8" = 1'-0" (1/8" = 1'-0")

ARCHITECT'S MARK

JASON ALLEN ARCHITECTS LTD
400-400-0000
400-400-0000
400-400-0000
400-400-0000

TELEPHONE: 400-400-0000
FAX: 400-400-0000
WWW.JASONALLENARCHITECTS.COM

45 ELIZABETH ST

45 ELIZABETH ST

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45 ELIZABETH ST



31 August 2020

Victoria Maxwell
Development Appraisal Planner
Hobart City Council

Dear Victoria

Further Information – PLN – 20-524

45 Elizabeth Street – Partial Demolition, Alterations, Extension and change of use General Retail and Hire and Five Multiple Dwellings.

I refer to your request for information 27 August 2020 regarding the above and respond to each of the matters as follows:

Tas Water

Please see attached Concept Services Report addressing TasWater's request.

Heritage Code

HER Fi1 – It is intended that the exterior of the building will be repainted in a similar 'Kodak' yellow.

HER Fi2 – it is intended that the existing window frames and hardware on the upper levels on the Elizabeth Street façade will be retained. Glazing will be replaced within these frames.

HER Fi3 – Please see attached photos of the existing internal arrangements of the building. As explained on site, the existing lift is not operational or accessible at present and additional photographs are not possible at this stage. It is intended that a photographic record of all parts of the building to be demolished will be provided to Council as a condition of approval prior to commencement of work on site.

Waste Management

PA13 – As discussed in Section 2 of the Planning Report the ground floor basement area will include provision for domestic waste and recycling in 2 x 660l bins. The Body Corporate nominated Caretaker will be responsible for moving the bins using the low level platform lift and manually pushing to the kerbside, Kemp Street Location nominated on the Lower Ground Basement Plan. The private collection service is proposed to come weekly for servicing in early morning periods, with bins being moved back to the Lower Ground Level following collection via prior arrangement. The bin collection location on Kemp Street is constrained due to the width of existing building and site.

Waste for the commercial tenancy will be managed by 2x 240L bins located in the rear storeroom of that tenancy. The tenant will move bins to Elizabeth Street Mall for weekly Council collection.



The above information as already provided with the application is considered sufficient for Council to determine the application under the planning scheme. In this regard it is noted that the existing planning scheme provisions (as opposed to the draft residential amenity provisions) do not include specific provisions that deal with waste management and disposal.

Stormwater Code

Please see attached Concept Services Report.

Inundation Prone Areas Code

As discussed with Mohsen, the Inundation Prone Areas Code does not apply to this proposal that does not involve:

- Change of use to new use for a habitable room at the Lower Ground Level on Kemp Street Purdy's Mart (E15.2.1(b)) or at Ground Level on Elizabeth Street in that the Elizabeth Street level is already used as a shop and that the proposed Lower Level alterations do not create a habitable room. I note that the definition of a habitable room under the planning scheme excludes a corridor, stair lobby etc that is not occupied frequently or for extended periods.

any room of a dwelling other than a bathroom, laundry, toilet, pantry, walk-in wardrobe, corridor, stair, hallway, lobby, clothes drying room and other space of a specialised nature occupied neither frequently nor for extended periods.
- development at ground level (E15.2.1(c))

75CA Agreement – Overhand of Kemp Street/Purdy's Mart

This matter has now been resolved. Please see attached advice from HCC Manager Surveying Services.

Response to Other Queries

The following additional responses are provided by JAWS architects to clarify design matters discussed on site:

Bay Window | Floor Details

All existing steel framed bay window details, hardware and surrounds will be retained. The existing junctions of concrete floor slabs to masonry bay window framing will be maintained for levels 1, 2 and 3. Where new ceilings are added below the existing concrete floors, to satisfy National Building Code requirements for Fire Separation and Sound isolation between the individual apartments, this will be done in a sensitive manner that will not visually effect the frame or surround details.

The removal of the existing concrete floor at Level 4 and the new lower concrete floor will be sensitively detailed to step behind the existing feature Bay window, allowing the external fabric to visually remain unchanged.



If required, further construction details can be conditioned for endorsement prior to Building Permit, when sufficient building fabric knowledge and construction consultant team has worked through all the related issues that will contribute to this detail.

Elizabeth Street Mall - New Opening

Creating the opening in the Elizabeth Street façade at the end of Apartment 4 is integral to the amenity of that apartment. It provides the only outlook and views for the apartment. Due to facade detailing the signage panel of masonry is cleanly framed. This is the portion of the elevation proposed to be opened up.

We understand the Kodak sign represents the cultural heritage and story of the building and we are hopeful that the new life of the building and its amenity requirements can add an interesting layer to the story, particularly to this element of the facade.

The existing sign is thought to be rendered masonry of approximately 15mm relief integral with the rendered finish of the existing blockwork wall. It has been highlighted by careful paint-work over the relief.

We are seeking permission to replicate the signage element and sensitively suspend the sign back in its original location. Directly behind the opening is the proposed external deck to Apartment 4. The view through the suspended signage element to the sky from the mall will be dynamic. Conversely the view through the sign from the Apartment will also give a unique quality and story to the apartment.

There are a number of ways this may be done. We initially propose taking a Lidar Survey of the signage element on the façade. This would give electronic information at a highly detailed level of 0.015/mm accuracy. With this accuracy and detailed level of information of location, font, thickness and size, 3D cad/cam modelling can be used to form synthetic polymer cast of the individual letters. These letters can in turn can be rendered and painted. Another option is to use the survey information to produce a powder coated steel plate version of the letters, finished in a matt Black paint for durability. These could be given the same thickness as the relief. If it were to be a metal signage replica this would refer to the elegant steel framing of the existing bay windows below.

We propose 3 very slender steel flat supports spanning horizontally with each letter accurately located and mechanically fixed to the rear of the letters. The mid span of the horizontal flats would require a slender single vertical rod to allow all the members to be fine, elegant and simple. The framing would be in light recessive metal finish

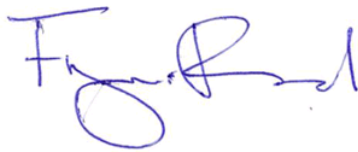
We believe this interpretation of the sign will not detract from, or detrimentally affect the Cultural Heritage of the whole façade, but instead allow the new life of the building to become a visible layer, contributing to an ongoing story of the building.

AllUrbanPlanning PTY LTD

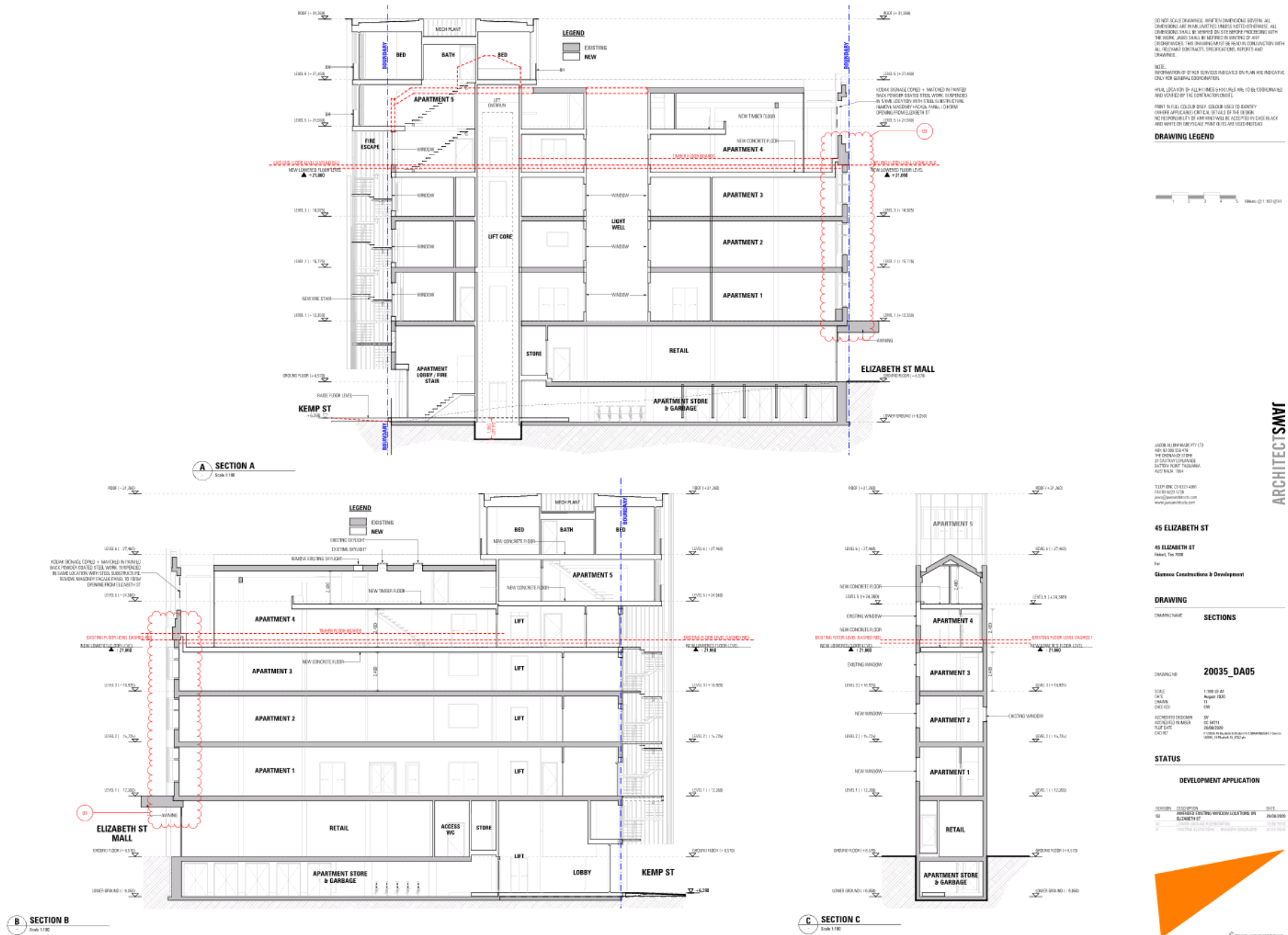
I trust the above and attached information stratifies Council and TasWater's queries.

I would be pleased to discuss as necessary.

Yours sincerely

A handwritten signature in blue ink, appearing to read 'Frazer Read', with a stylized, cursive script.

Frazer Read
Principal
All Urban Planning Pty Ltd





SOUTH-WEST ELEVATION - ELIZABETH ST (PROPOSED)

DEVELOPMENT APPLICATION

PERIOD	DESCRIPTION	DATE
00	AMATEUR THEATRE; WINDOW LOCATIONS ON BLINDETH ST	26/06/1980
01	LEADS COULAGE FURNITURE CO.	17/06/1980
02	OUTLINE SLACK TOWN - BROWNSIDE COURT/ST	01/07/1980



**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

SEARCH OF TORRENS TITLE

VOLUME 231481	FOLIO 1
EDITION 6	DATE OF ISSUE 03-Apr-2008

SEARCH DATE : 15-Aug-2020

SEARCH TIME : 08.38 AM

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Plan 231481

Derivation : Part of 21.1/2 Perches Sec. K. Gtd. to W. Bunster

Prior CT 3170/77

SCHEDULE 1C558944 TRANSFER to GIAMEOS HOLDINGS PTY LTD Registered
24-Feb-2005 at 12.01 PMSCHEDULE 2

Reservations and conditions in the Crown Grant if any

C430786 MORTGAGE to Commonwealth Bank of Australia

Registered 03-Apr-2008 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



ORIGINAL - NOT TO BE REMOVED FROM TITLES OFFICE

R.P. 1469
TASMANIA
REAL PROPERTY ACT, 1862, as amended
NOTE - REGISTERED FOR OFFICE
CONVENIENCE TO REPLACE



CERTIFICATE OF TITLE

Register Book
Vol. Fol.

3170 77

Cert. of Title Vol. 251. Fol. 102.

I certify that the person described in the First Schedule is the registered proprietor of an estate in fee simple in the land within described together with such interests and subject to such encumbrances and interests as are shown in the Second Schedule. In witness whereof I have hereunto signed my name and affixed my seal.

Recorder of Titles.



DESCRIPTION OF LAND

CITY OF HOBART
FIVE PERCHES on the Plan hereon

FIRST SCHEDULE (Continued overleaf)

KODAK (AUSTRALASIA) LIMITED

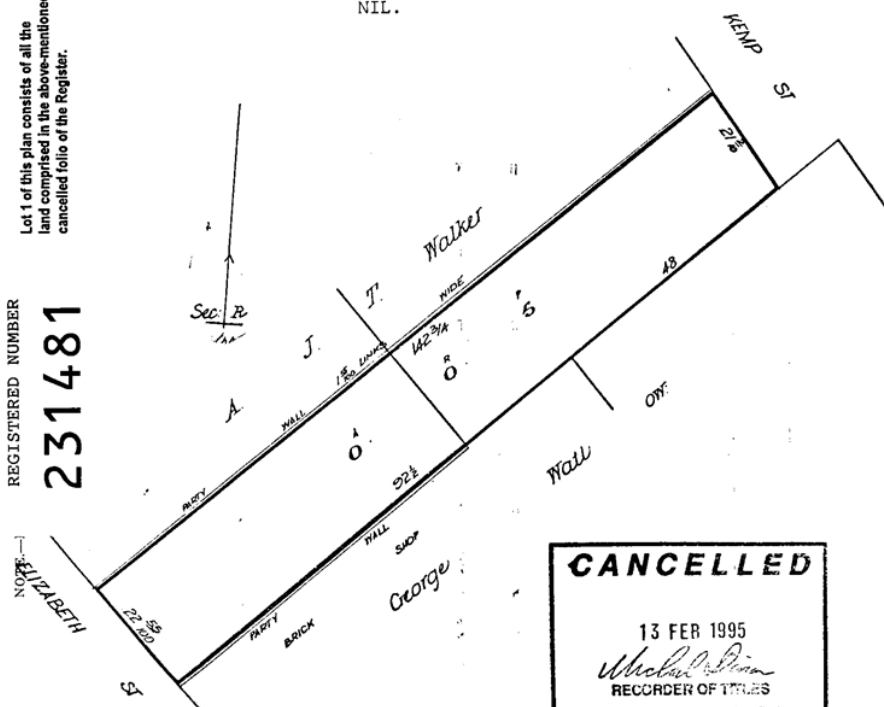
SECOND SCHEDULE (Continued overleaf)
NIL.

S ARE NO LONGER SUBSISTING.

Lot 1 of this plan consists of all the
land comprised in the above-mentioned
cancelled folio of the Register.

REGISTERED NUMBER

231481



CANCELLED

13 FEB 1995

RECORDER OF TITLES

NEW TITLE ISSUED

Part of 21.1/2 Perches/Gtd. to W. Bunster Meas. in Links 18/21 Hob.
Edition. Registered
Derived from C.T. Vol. 251. Fol. 102. Transfer 44816 A. S. Brownell

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

SEARCH OF TORRENS TITLE

VOLUME 113200	FOLIO 2
EDITION 1	DATE OF ISSUE 28-Nov-1996

SEARCH DATE : 15-Aug-2020

SEARCH TIME : 08.38 AM

DESCRIPTION OF LAND

City of HOBART

Lot 2 on Sealed Plan 113200

Derivation : For grantees see plan

Prior CTs 40361/1, 40361/2, 40365/1, 210533/1 and 207810/1

SCHEDULE 1B989230 TRANSFER to HOBART CITY COUNCIL Registered
28-Nov-1996 at noonSCHEDULE 2

Reservations and conditions in the Crown Grant if any

SP 113200 EASEMENTS in Schedule of Easements

B989230 FENCING PROVISION in Transfer

UNREGISTERED DEALINGS AND NOTATIONS

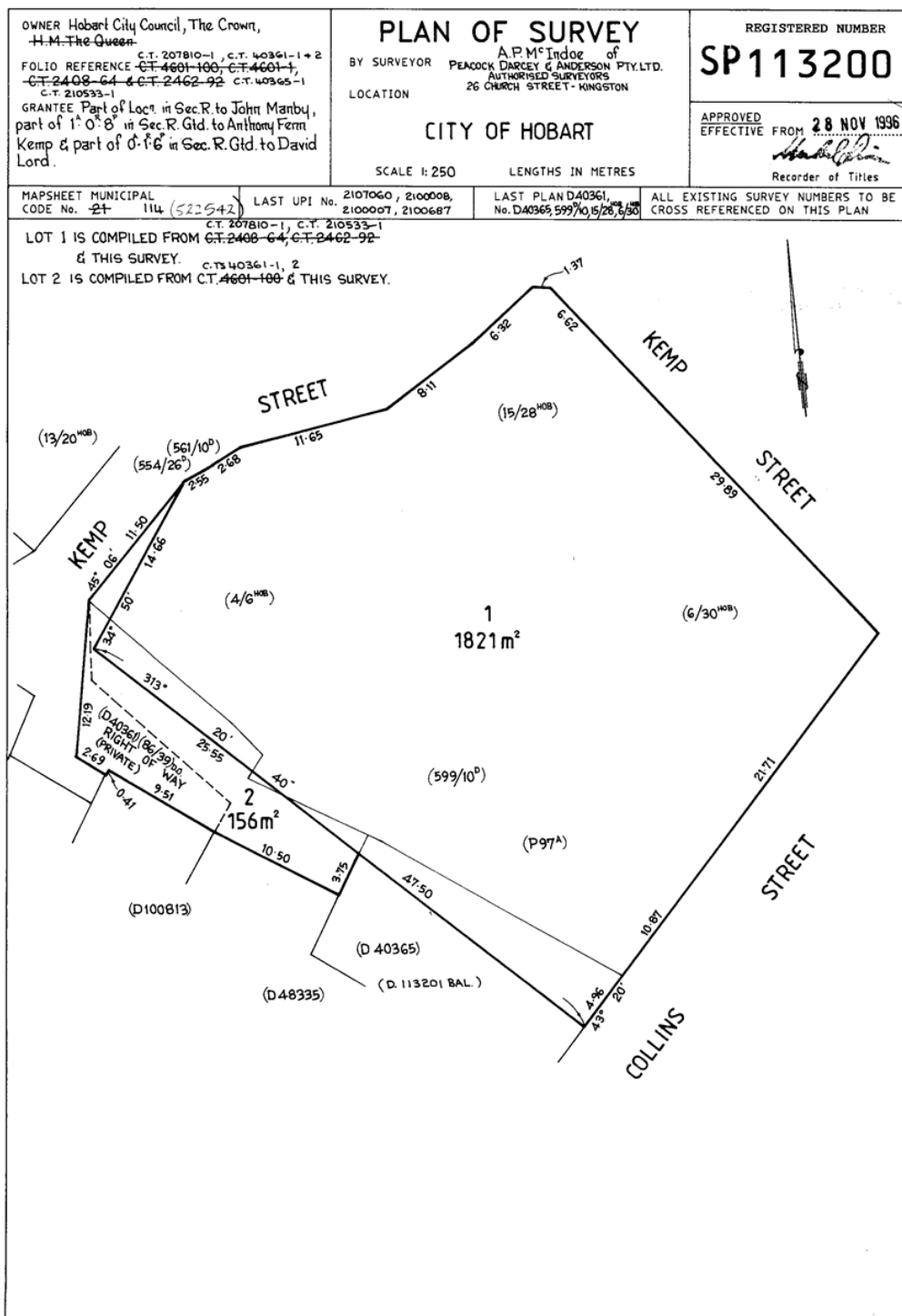
No unregistered dealings or other notations



FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



**SCHEDULE OF EASEMENTS**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



<p align="center">SCHEDULE OF EASEMENTS</p> <p>NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.</p>	<p align="center">Registered Number</p> <p align="center">SP113200</p>
<p align="center">EASEMENTS AND PROFITS PAGE 1 OF 1 PAGES</p> <p>Each lot on the plan is together with:- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and (2) any easements or profits a prendre described hereunder. Each lot on the plan is subject to:- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and (2) any easements or profits a prendre described hereunder. The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.</p> <p>No covenants or profits a prendre are intended to be created.</p> <p>That portion of lots 1 and 2 which formerly comprised the land in Certificate of Title Volume 4601 Folio 100 is subject to a right of way created by Conveyance No. 38/2762 (appurtenant to adjoining land) so far as the same is valid subsisting and capable of taking effect.</p> <p>Signed by the Honourable Thomas John Cleary) the Minister for the time being administer-) ing the Crown Lands Act 1976 for and on) behalf of the Crown in the presence of:-)</p> <p align="center"><i>[Signature]</i></p>	
<p>SUBDIVIDER :</p> <p>FOLIO REF :</p> <p>SOLICITOR & REFERENCE :</p>	<p>PLAN SEALED BY :</p> <p>DATE :</p> <p>.....</p> <p>REF No. General Manager</p>
<p>NOTE: THE COUNCIL GENERAL MANAGER MUST SIGN THE CERTIFICATE FOR THE PURPOSE OF IDENTIFICATION.</p>	

HERITAGE REFERRAL ASSESSMENT

Application Number:	PLN-20-524
Address:	45 ELIZABETH STREET HOBART TAS 7000
Description:	Partial Demolition, Alterations, Extension and Change of Use to General Retail and Hire and Five Multiple Dwellings
Planner:	Victoria Maxwell
Assessment Officer:	Lucy Burke-Smith, Associate, Purcell Asia Pacific Limited
Recommendation:	not acceptable

Historical Overview

The following historical overview is taken from the Central Area Heritage Review Reference C9:

The building stands on land that was originally granted to John McDougall. There were buildings occupying this allotment by the 1840s. By the early 1900s, there was a cojoined shop occupying the site. This shop was one of two conjoined shops owned by Mr A Spencer Brownell, who was presumably connected with Brownell Brothers Ltd, a large department store located in Liverpool Street (now Myers). Brownell owned the property until the early 1920s, when the shop was purchased by the Kodak Company, and replaced by Kodak House in c1924. The new building was connected to the City's sewerage system in 1924. Two years later, plans were submitted to the HCC for the addition of a fifth floor to the building. The architect for the project was G Stanley Crisp of 137 Macquarie Street. The new fifth floor was planned to be used as a processing room. The facade was transformed with the words 'Kodak House' being added in large lettering to the new top section with a definitive castle-like form. Kodak (Australasia) Pty Ltd continued to own and occupy the property up until 2002, when it was sold.

There were several other Kodak House buildings constructed in Australian Capitals¹:

- Kodak House, George Street Sydney c.1930, demolished and replaced with a c.1960s building, now too demoished
- Kodak House, 250-252 Queen Street Brisbane, c.1914, now demolished
- Kodak House, 252 Collins Street Melbourne, 1935, extant.

¹ Hobart City Council Central Area Heritage Review Reference C9
[https://purcellukcom-my.sharepoint.com/personal/lucyburkesmith_purcelluk_com/documents/tas/kodak house/heritage referral - final 20201029.docx](https://purcellukcom-my.sharepoint.com/personal/lucyburkesmith_purcelluk_com/documents/tas/kodak%20house/heritage%20referral%20-%20final%2020201029.docx)

Archival images and site photographs



Elizabeth Street Hobart 1937 (Source: Libraries Tasmania NS3304-1-1_96-104)



Hobart skyline, undated (Source: Libraries Tasmania PH30-1-6130)



Hobart skyline, c.1976 (Source: Libraries Tasmania NS3373-1-262)



Kodak House (Source: Purcell October 2020)

This assessment has been drafted with reference to the two (2) representations made during the statutory period between 22nd September and 6th October 2020, as summarised within Section 5 of the Draft Council Planning report.

HIPS 2015 Discretions	Applicable yes/no	Specific clauses
E13.0 Heritage Place	yes	E13.7.1 P1 (a), (b), (c) and (d) E13.7.2 P1 E13.7.2 P2 E13.7.2 P3 E13.7.2 P4
E17.0 Signs Code	yes	E17.7.2 P1 (a) to (i)
22.4.1	yes	A5/P5
22.4.3	yes	A3/P3

Historic Heritage Code

E13.7.1 Demolition

Objective:

To ensure that demolition in whole or part of a heritage place does not result in the loss of historic cultural heritage values unless there are exceptional circumstances.

E13.7.1 P1

Demolition must not result in the loss of significant fabric, form, items, outbuildings or landscape elements that contribute to the historic cultural heritage significance of the place unless all of the following are satisfied;

- (a) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;*
- (b) there are no prudent and feasible alternatives;*
- (c) important structural or façade elements that can feasibly be retained and reused in a new structure, are to be retained;*
- (d) significant fabric is documented before demolition.*

The proposal seeks approval for the demolition of the following features to the building interiors:

- non-original lightweight partition walls across all levels
- original masonry walls
- toilets to levels 2 and 3
- the central stair and lift shaft
- concrete floor to level 4 and partial floors for the creation of a lightwell

Externally the proposal seeks approval for the demolition of:

- The rear fire exit stair widows and walls to the rear elevation for a new fire stair
- Partial demolition of the roof and roof members
- Isolated demolition of south east and north west elevations for the creation of openings
- Rendered 'Kodak House' signage to the south west elevation

The rendered 'Kodak House' signage is significant fabric which contributes to the cultural heritage significance of the place. Its demolition would diminish its cultural heritage values.

The proposed demolition of the signage and partial demolition of the boundary walls does not substantiate exceptional circumstances as outlined in E13.7.1 (a) and (b), which might warrant the proposed demolition.

The remaining scope of proposed demolition has been carefully considered in design development and could be otherwise supported. It is not result in a loss of historic cultural heritage values of the place.

The proposal does not comply with the performance criterion.

E13.7.2 Buildings and Works other than Demolition

Objective:

To ensure that development at a heritage place is:

- (a) undertaken in a sympathetic manner which does not cause loss of historic cultural heritage significance; and*
- (b) designed to be subservient to the historic cultural heritage values of the place and responsive to its dominant characteristics.*

E13.7.2 P1

Development must not result in any of the following:

- (a) loss of historic cultural heritage significance to the place through incompatible design, including in height, scale, bulk, form, fenestration, siting, materials, colours and finishes;*
- (b) substantial diminution of the historic cultural heritage significance of the place through loss of significant streetscape elements including plants, trees, fences, walls, paths, outbuildings and other items that contribute to the significance of the place.*

The proposed additions are limited to a minor rooftop extension and upgrade to the rear fire exit. The design of these elements is sympathetic to the place and does not detract from its cultural heritage significance. It enhances the presentation of Kemp Street through the removal of the existing unsympathetic exit stair and replacement with an element sympathetic in its form, material, colour and finish. The proposed fenestration will break perceptions of bulk through visual relief. This element integrates with the rooftop extension in proportions which do not detract from the characteristic form of the existing Kemp Street elevation.

The proposed additions are sufficiently setback from the Elizabeth Street frontage such that the dominant characteristics of this elevation will be retained.

The proposal does not impact on significant streetscape elements and will not result in a loss of historic cultural heritage significance.

The proposal complies with the performance criterion.

E13.7.2 P2

Development must be designed to be subservient and complementary to the place through characteristics including:

- (a) scale and bulk, materials, built form and fenestration;*
- (b) setback from frontage;*
- (c) siting with respect to buildings, structures and listed elements;*
- (d) using less dominant materials and colours.*

The proposal is sufficiently set back from the Elizabeth Street façade and will not impact the dominant characteristics of this elevation. The addition to Kemp Street is of a scale and bulk which does not dominate or detract from the existing. As per E13.7.2 materials, colour and fenestration are complementary to the place.

The proposal complies with the performance criterion.

E13.7.2 P3

Materials, built form and fenestration must respond to the dominant heritage characteristics of the place, but any new fabric should be readily identifiable as such.

The proposed materials, built form and fenestration are complementary to the place while being readily identifiable as new additions.

The proposal complies with the performance criterion.

E13.7.2 P4

Extensions to existing buildings must not detract from the historic cultural heritage significance of the place.

The proposed replacement exit stair is of a form and scale consistent with that of the existing. The roof top addition is considered a minor extension and one which does not detract from the historic cultural heritage significance of the place.

The proposal complies with the performance criterion.

Signs Code**Objective:**

To ensure the size, design and siting of signs complements and does not impact on the cultural heritage significance of places or precincts listed in the Historic Heritage Code. R1

E17.7.2 P1

A sign on a Heritage Place listed in the Historic Heritage Code or within a Heritage Precinct or Cultural Landscape Precinct must satisfy all of the following:

- (a) be located in a manner that minimises impact on cultural heritage significance of the place or precinct;*
- (b) be placed so as to allow the architectural details of the building to remain prominent;*
- (c) be of a size and design that will not substantially diminish the cultural heritage significance of the place or precinct;*
- (d) be placed in a location on the building that would traditionally have been used as an advertising area if possible;*
- (e) not dominate or obscure any historic signs forming an integral part of a building's architectural detailing or cultural heritage values;*
- (f) have fixtures that do not damage historic building fabric, including but not restricted to attachments to masonry and wood, such as to using non-corrosive fixings inserted in mortar joints;*
- (g) not project above an historic parapet or roof line if such a projection impacts on the cultural heritage significance of the building;*
- (h) be of a graphic design that minimises modern trademark or proprietary logos not sympathetic to heritage character;*
- (i) not use internal illumination in a sign on a Heritage Place unless it is demonstrated that such illumination will not detract from the character and cultural heritage values of the building.*

The interpretative signage proposed to replace that of the significant rendered 'Kodak House' will diminish the cultural heritage significance of the place by virtue of the demolition of the existing signage. Further it does not allow for the retention of significant architectural details.

The proposal does not comply with the performance criterion.

Central Business Zone**22.4.1 Building Height****Objective***That building height:*

- (a) *contributes positively to the streetscape and townscape;*
- (b) *does not unreasonably impact on historic heritage character;*
- (c) *does not unreasonably impact on important views within the urban amphitheatre;*
- (d) *does not unreasonably impact on residential amenity of land in a residential zone; and*
- (e) *provides significant community benefits if outside the Amenity Building Envelope.*

22.4.1 A5

Building height of development within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

- (a) *not exceed 1 storey or 4m (whichever is the lesser) higher than the facade building height of a heritage building on the same street frontage (refer figure 22.5 ii); and*
 - (b) *not exceed the facade building height of the higher heritage building on the same street frontage if the development is between two heritage places (refer figure 22.5 ii);*
- or*
- (c) *comply with the building height in Clauses 22.4.1 A1 and A2; whichever is the lesser.*

The proposal does not comply with the acceptable solution. The new extension to Kemp Street adjoins 47 Elizabeth Street, which is listed on Table E13.1 as a heritage place (Ref No. 992). The subject place exceeds the height of 47 Elizabeth Street by more than 1 storey.

The proposal should therefore be assessed against 22.4.1 P5.

22.4.1 P5

Building height within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

- (a) *not unreasonably dominate existing buildings of cultural heritage significance; and*
- (b) *not have a materially adverse impact on the historic cultural heritage significance of the heritage place;*
- (c) *for city blocks with frontage to a Solar Penetration Priority Street in Figure 22.2, not exceed the Amenity Building Envelope illustrated in Figure 22.3, unless it can be demonstrated that the overshadowing of the public footpath on the opposite side of the Solar Penetration Priority Street does not unreasonably impact on pedestrian amenity.*

The existing form and proportions of 45 and 47 Elizabeth Street are complementary to one another. The proposed upper level extension does not alter this relationship when viewed from Kemp Street and will not be visible from Elizabeth Street. The increase in building height will not dominate 47 Elizabeth Street and as such the proposal will not adversely impact on the historic cultural heritage significance of the place at 47 Elizabeth Street.

The proposal complies with the performance criterion.

22.4.3 Design

Objective

That building height:

- (a) *contributes positively to the streetscape and townscape;*
- (b) *does not unreasonably impact on historic heritage character;*
- (c) *does not unreasonably impact on important views within the urban amphitheatre;*
- (d) *does not unreasonably impact on residential amenity of land in a residential zone; and*
- (e) *provides significant community benefits if outside the Amenity Building Envelope.*

22.4.3 A3

The facade of buildings constructed within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

- (a) *include building articulation to avoid a flat facade appearance through evident horizontal and vertical lines achieved by setbacks, fenestration alignment, design elements, or the outward expression of floor levels; and*
- (b) *have any proposed awnings the same height from street level as any awnings of the adjacent heritage building.*

Perceptions of flat facades will be mitigated by the cladding material nominated for the Kemp Street exit stair. The material selection will break perceptions of bulk through visual relief.

It is considered that the proposed façade will make a greater contribution to the streetscape than that of the existing. It is not considered to present impact to the historic heritage character of Kemp Street, nor will it impact on important views or residential amenity.

There is no change proposed to the awning to Elizabeth Street and no awnings to Kemp Street.

The proposal complies with this acceptable solution.

22.4.3 P3

The facade of buildings constructed within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

- (a) *be of a design sympathetic to the elevational treatment and materials of the existing heritage building; and*
- (b) *not unreasonably detract from the historic cultural heritage significance of the existing heritage place.*

Not applicable see 22.4.3 A3

Summary

The streetscape contribution of the place is recognised within Datasheet C9 of the Central Heritage Review. The painted signage to the party walls has a long history and association with the Hobart skyline (see archival images) and contributes to the historic character of the Elizabeth Street Mall and to the landmark qualities of the building.

The proposal will directly impact architectural features which contribute to the principal characteristics of this Inter-War commercial building (Criterion d).

The proposal to remove the painted and rendered signage from Kodak House will diminish its historic cultural heritage values, and exceptional circumstances as defined by E13.7.1 P1 have not been sufficiently demonstrated against P1a and b. It is considered that a prudent and feasible alternative would be for the retention of the rendered and painted signage with a reconfiguration of the internal room functions and configuration for Apartment 4 with private outdoor space provided through an enlarged light well over levels 4 and 5. It is recognised that this would impact on the existing roof form, however this is considered an acceptable and preferable alternative to the current proposal.

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Ms. Cath Williams

JAWS Architects

21 Castray Esplanade

HOBART TAS 7000

24th July 2020

Re – Archaeological Assessment, 45 Elizabeth Street, Hobart.

Dear Cath

Thank you for your instruction to provide a commentary around the possible archaeological potential of 45 Elizabeth Street, Hobart (the study area, as defined by Figure 1 below).

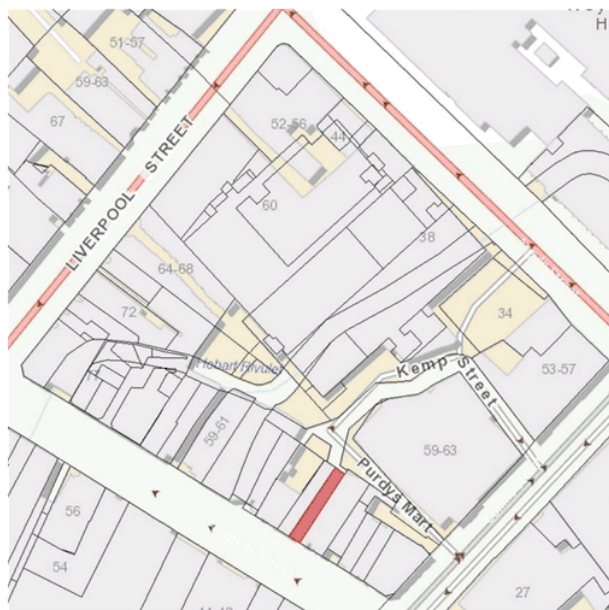


Figure 1 – The subject site (adapted from www.thelist.tas.gov.au)

The subject site is included in Table E.13.4 of the Hobart Interim Planning Scheme 2015 (Places of Archaeological Potential), as defined by Figure E.13.4.1 of the scheme, therefore Clause E.13.10.1 of the scheme applies.

	Acceptable Solution	Performance Criteria
<i>E.13.10.1 – Building and Works other than Demolition</i>	<i>A1. Building and works do not involve excavation or ground disturbance.</i>	<p><i>P1. Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:</i></p> <ul style="list-style-type: none"> <i>a) the nature of the archaeological evidence, either known or predicted;</i> <i>b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;</i> <i>c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition;</i> <i>d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;</i> <i>(a) measures proposed to preserve significant archaeological evidence 'in situ'.</i>

Further to Clause E13.5.1 of the Scheme, the Planning Authority **may** require the following to accompany any application for use or development of a place which is affected by the provisions of the Historic Heritage Code:

- (f) a statement of archaeological potential;*
- (g) an archaeological impact assessment;*
- (h) an archaeological method statement;*

Under the definitions of the scheme:

(f) means:

a report prepared by a suitably qualified person that includes all of the following:

- a. a written and illustrated site history;
- b. overlay plans depicting the main historical phases of site development and land use on a modern base layer;
- c. a disturbance history.
- d. a written statement of archaeological significance and potential accompanied by an archaeological sensitivity overlay plan depicting the likely surviving extent of important archaeological evidence (taking into consideration key significant phases of site development and land use, and the impacts of disturbance).

(g) means:

a report prepared by a suitably qualified person that includes a design review and describes the impact of proposed works upon archaeological sensitivity (as defined in a statement of archaeological potential).

(h) means:

a report prepared by a suitably qualified person that includes the following where relevant to the matter under consideration:

- a. strategies to identify, protect and/or mitigate impacts to known and/or potential archaeological values (typically as described in a Statement of Archaeological Potential);*
- b. collections management specifications including proposed storage and curatorial arrangements;*
- c. identification of measures aimed at achieving a public benefit;*
- d. details of methods and procedures to be followed in implementing and achieving (a), (b) and (c) above*
- e. expertise to be employed in achieving (d) above;*
- f. reporting standards including format/s and content, instructions for dissemination and archiving protocols.*

Brief background history of the site and building

In order to gain a cursory understanding of the possibility of archaeological remains in the study area, it is necessary to understand the development history. I present the following figures to provide that brief understanding. Whilst this does not necessarily negate the need for a detailed statement of archaeological potential for the site/wider environs, given the limited excavation involved in the current proposal, I propose that this suffices for the current purposes.

The subject site appears to have been developed prior to 1827 with a large building in the centre of the site – the site itself was then part of a much larger allotment with that building straddling the centre portion of the current site.

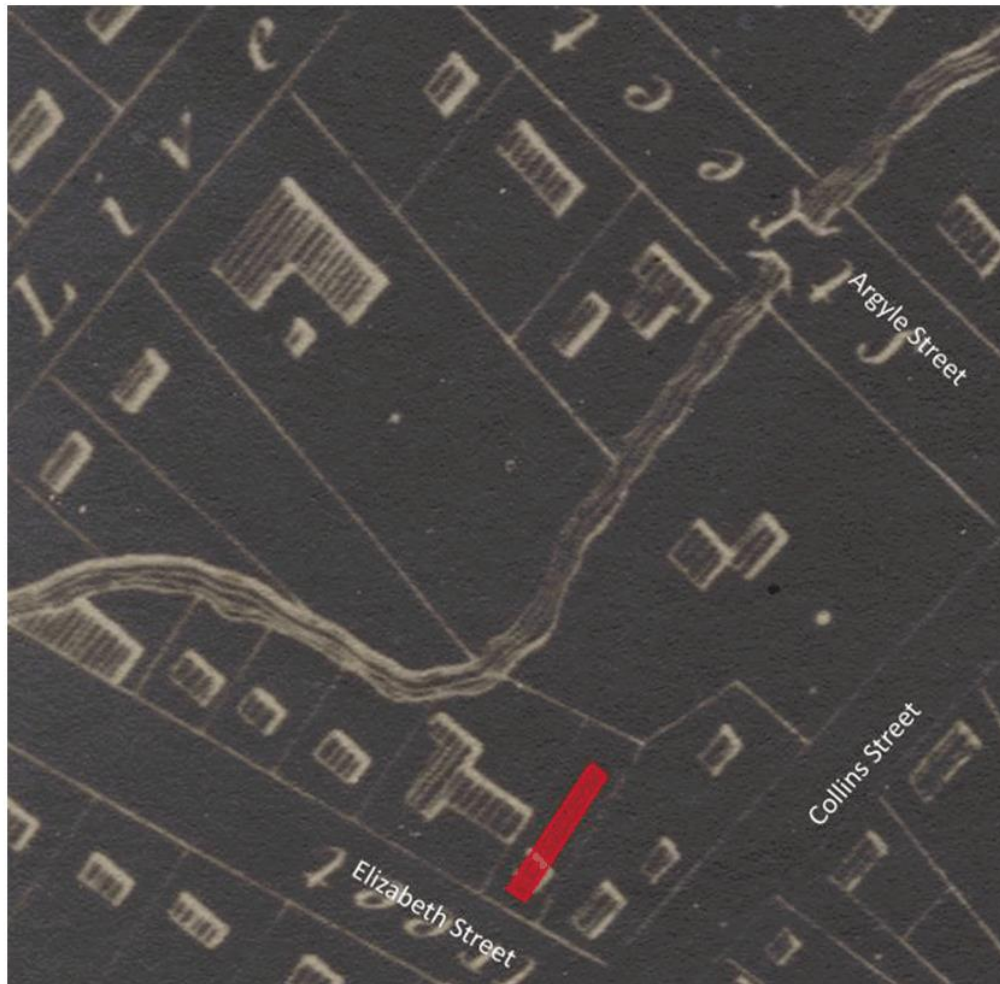


Figure 2— Excerpt from an undated (c1820s) survey plan of Hobart (Tasmanian Archive and Heritage Office, PH-30-1-693-1).



Figure 3 – Excerpt from Lee-Archer's 1828 plan of the Hobart waterfront and central area (Department of Primary Industry, Water and Environment 90411).

A similar arrangement of main building is shown on the c1832 survey of Hobart, with additional timber buildings in the backyard, one of which is depicted partially on the subject site, however the accuracy of this survey is dubious – it is known to show the *general* presence and location of buildings, but with little precision.



Figure 4 – A c1832 depiction of the study area. Lands Tasmania Hobart H5.

Sprent's c1845 survey (Figure 5) of Hobart shows either an extension or replacement of that earlier building, with a large L-shaped masonry building on the Elizabeth Street frontage. The purpose of that building is unknown however it was possibly of an industrial nature given the laneway running from the rear to the rivulet.



Figure 5 – a c1845 survey showing the study area with a larger masonry building present. Tasmanian Archive and Heritage Office AF393 series.

A c1880 panorama of Hobart includes a distant view of the backyard area of the building, showing it as open space with the buildings fronting Elizabeth Street and a notable drop in topography rearward on the site (Figure 6).

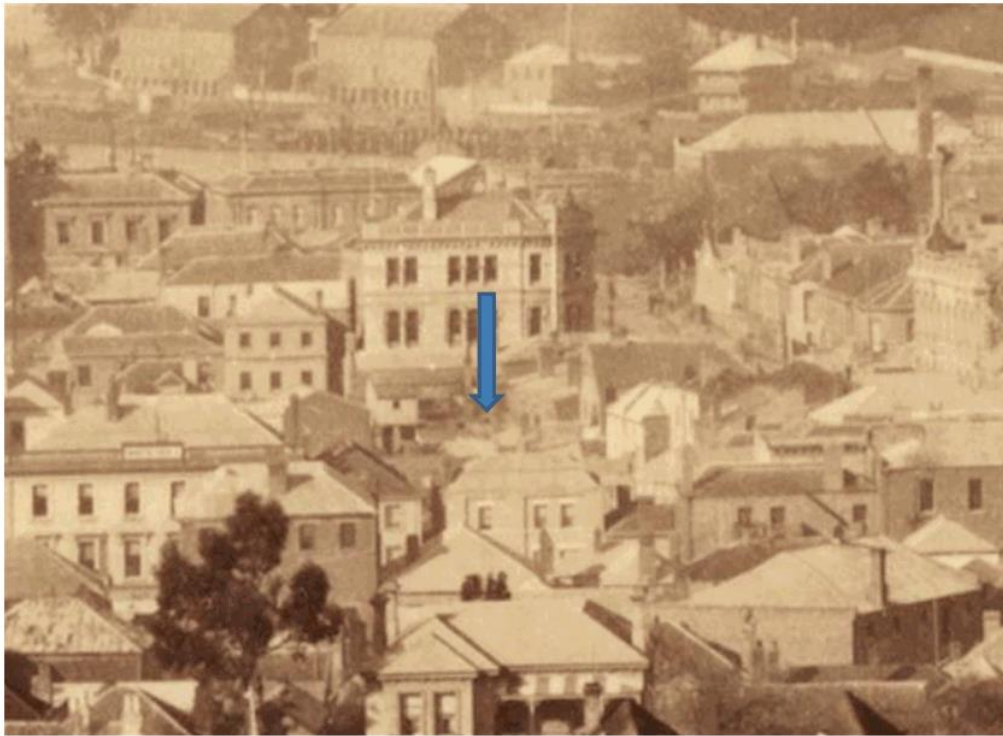


Figure 6 – Excerpt from a c1880 panorama of Hobart, the arrow denoting the backyard area of the subject site. Tasmanian Archive and Heritage Office NS2960.2.3.

The 1908 Metropolitan Drainage Board survey of Hobart shows that the front portion of the 'Kodak' building had been constructed, the rear yard as open space with a toilet at the far rear (Figure 7).

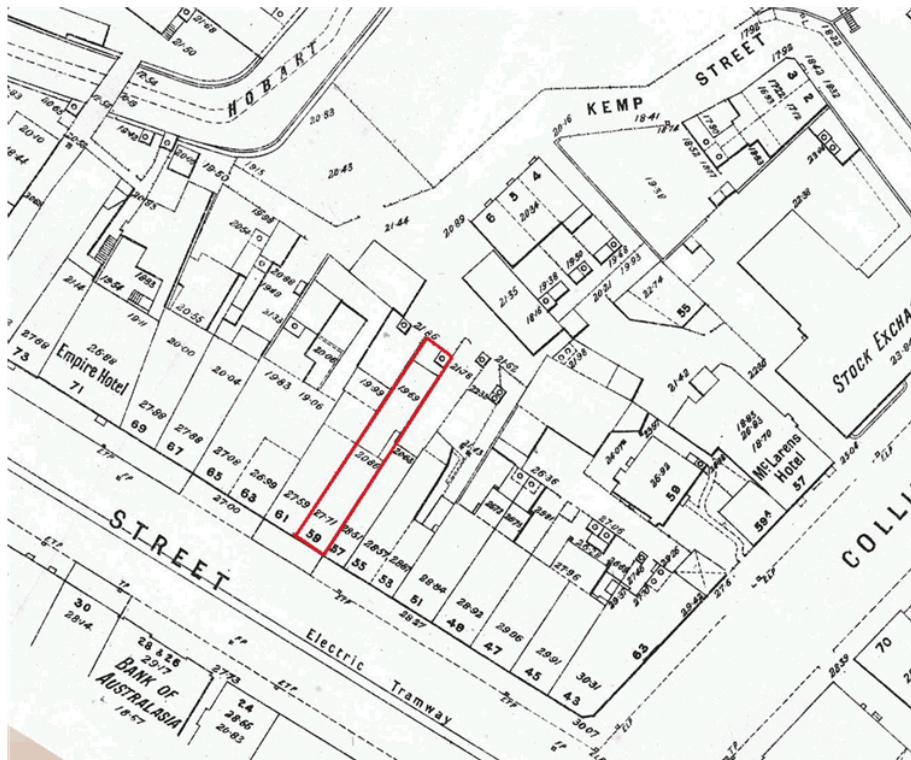


Figure 7 – a 1908 survey of the study area shows the development of the front portion of the 'Kodak' building. Metropolitan Drainage Board Hobart, 1908. Libraries Tasmania SD_ILS:553788.



Figure 8 – A c1925 photograph of the Kodak building (from the south). Author's collection.

The 1946 aerial photograph of Hobart shows the building in its current form with the rear extension completed prior to the 1920s). AN additional storey was added around 1926.

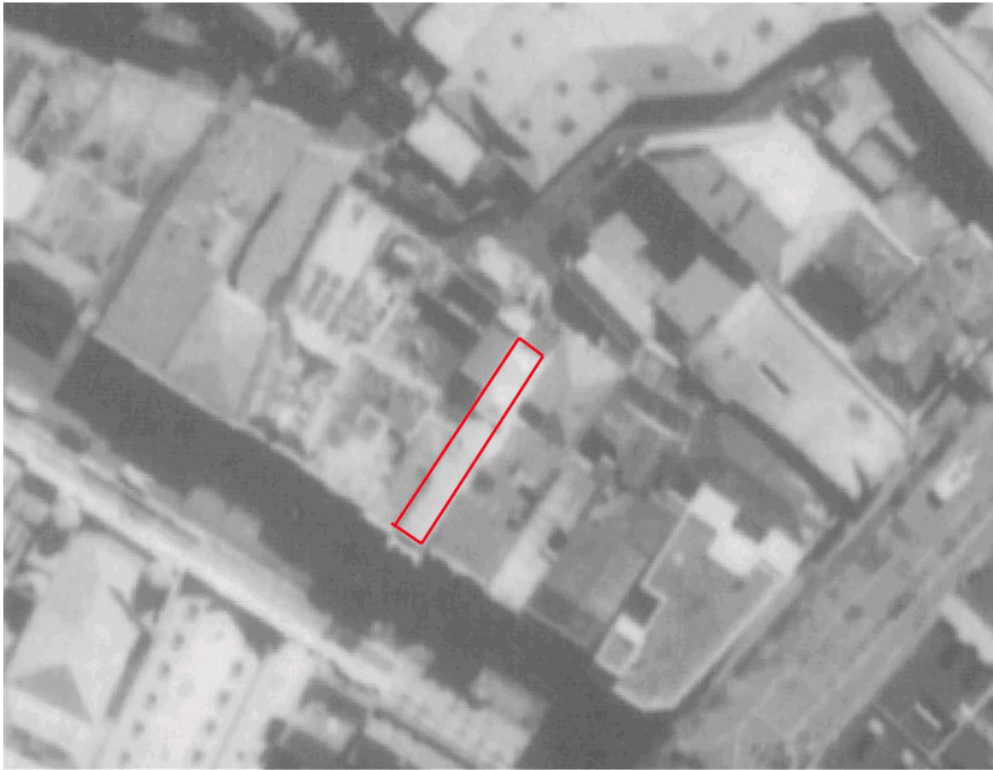


Figure 9 – the 1946 aerial run of Hobart, showing an adapted rear wing. Lands Tasmania Hobart 1946 Run 5, 10893.

The subject site therefore has a simple developmental history which can be summarised as:

- The central portion of the site was developed as early as the 1820s with a larger building straddling that and adjacent sites.
- That earlier building was either replaced or extended with a larger building towards Elizabeth Street by c1843.
- The front portion of the current building was constructed around 1900 and extended rearward prior to the 1920s.

Current conditions that may influence archaeological potential

The development of the site in the first quarter of the twentieth century are likely to have resulted in substantial excavation across the site. Figure 10 depicts a section through the building, which show the basement space of the existing building. Noting the presumed natural topography of the land (as depicted by the hatched line through the basement space, it appears that the Elizabeth Street frontage of the site has been excavated a full storey, with that diminishing to minimal excavation at the rear of the site.

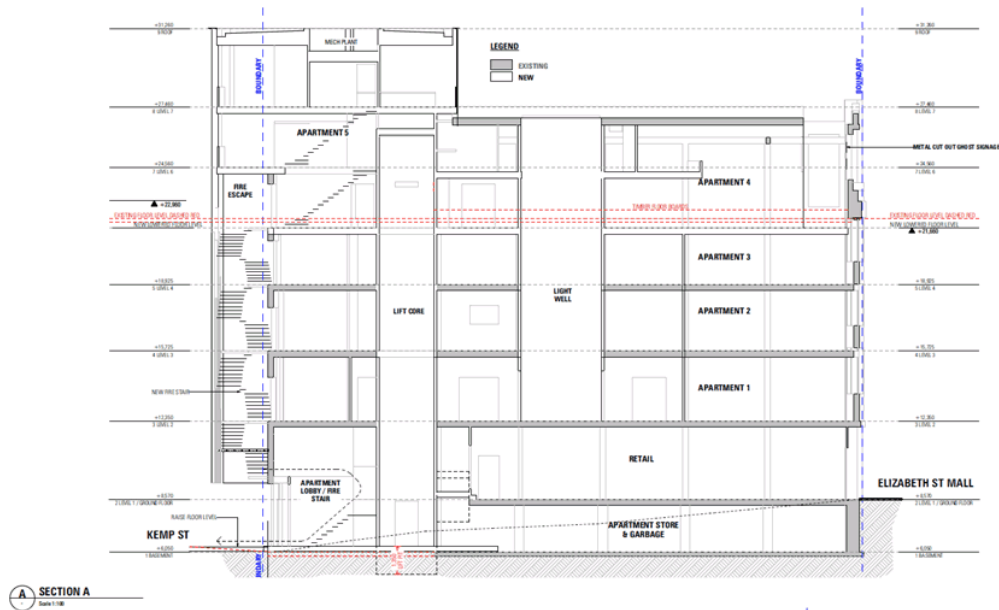


Figure 10 – Section through the building (with proposal marked-up) – showing the existing form of the basement. From JAWS Architects Drawing 20035_DD05.

Commentary on archaeological potential

Nineteenth century development was apparently limited to the central and Elizabeth Street frontage of the site. No early depictions indicate any major development on the rear (i.e. Kemp Street) portion of the site. Given the apparent excavations undertaken for the basement of the current building, this has largely impacted the central and Elizabeth Street portions of the site. This means a low likelihood of survival of any substantial remains of the c19th development – the area of that development being the area most impacted by those excavations. Given that disturbance history, overall, it is not considered that the site has high archaeological potential.

The proposed development

The proposed development does not require substantial excavation. That excavation is generally limited to the base of a new lift shaft in the central-rear portion of the site. Whilst this location is near to where c19th development was located, it is also in an area likely to have been subject to (at least) moderate prior disturbance. The model above predicts a low likelihood of archaeological potential in that area. Whilst the current document does not constitute a full and detailed statement of archaeological potential, I conclude that it is unlikely that the proposed works will have any substantial impact given the low likelihood of archaeological potential resulting from the likelihood of prior disturbance.

Accordingly, as a precautionary approach I recommend:

- That once the basement slab has been lifted, that an archaeologist monitor the excavations for the base of the lift shaft.
- That if any archaeological remains are encountered (e.g. early foundations, artifacts etc.) that these are managed in accordance with the Tasmanian Heritage Council's Practice Note 2 (*Managing Historical Archaeological Significance in the Works Application Process*). At most, this would involve the archaeological investigation of any structural remains and their photographic documentation prior to removal (if necessary).
- Any significant artifacts, or items of historical interest are to be retrieved and catalogued as part of any archaeological report and vested with the site owner.
- A report on these works, even in the event of negative results, be lodged with Hobart City Council to demonstrate compliance with these undertakings.

Please contact me if you have any further queries or require any clarification.

Regards



Brad Williams BA. (Hons.) Archaeology, MA Cultural Heritage Management, G.Dip. Environmental Planning.

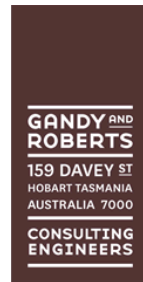
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Concept Services Report

Existing Infrastructure Assessment

45 Elizabeth St Hobart

for Giameos Constructions & Developments

31/08/2020

Version control

Revision	Description	Issue date	Issued by
1	Planning Approval	31/08/2020	Adam Kohl

PROJECT NUMBER **20.0371**
REPORT AUTHOR **Adam Kohl**
CHECKED BY **Andrew Cupit**

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20.0371 – Concept Services Report — 31/08/2020

Contents

1	Background.....	4
2	Existing Site Services	4
2.1	Existing Infrastructure	4
2.1.1	Sewer	4
2.1.2	Stormwater.....	4
2.1.3	Water services	4
2.2	Existing Building Services	4
2.2.1	Sewer	4
2.2.2	Stormwater.....	5
2.2.3	Water	5
2.3	Conclusion.....	5
2.3.1	Sewer	5
2.3.2	Stormwater.....	5
2.3.3	Water Services.....	5
3	Drawings.....	6
3.1	Drawing 20.0371 Rev 1- H010 CONCEPT SERVICES PLAN.....	6
4	Calculations.....	6
5	Appendix.....	7
5.1	Site Photos	7
	7

1 Background

45 Elizabeth St Hobart is currently proposed to be redeveloped. Gandy and Roberts have been engaged to provide a concept services report in support of the development application.

2 Existing Site Services

2.1 Existing Infrastructure

2.1.1 Sewer

A Taswater asset search has verified a DN150 sewer gravity main runs along the rear of the building in Purdys Mart, with an existing connection (assumed DN100) to the site. Refer **Drawing 20.0371 H010**.

2.1.2 Stormwater

A Hobart City Council asset search has verified a DN300 stormwater main is located at the rear of the building, in Purdys Mart, with an existing connection (assumed DN100) to the site. A DN300 stormwater main also runs past the building in Elizabeth street. Additional investigation would be required to confirm if the building currently has a connection to the stormwater main in Elizabeth St. Refer **Drawing 20.0371 H010**

2.1.3 Water services

A Taswater asset search has verified an existing DN100 water main runs along the rear of the building in Purdys Mart, with an existing DN25 domestic connection and water meter located in the basement garage. An existing fire plug is located in Purdys Mart close to the rear of the building.

Elizabeth Street has 2no. DN150 water mains, one on the near side and one on the far side of the street, with a DN100 fire connection from each main. It appears that the current fire services are connected directly to the property without boundary backflow and low flow bypass metering. An existing fire plug is located in front of the building on the far side of Elizabeth Street. Refer **Drawing 20.0371 H010**.

2.2 Existing Building Services

2.2.1 Sewer

The amenities at the rear of the building are serviced by sanitary stack located externally on the rear wall of the building. Additional sanitary pipework is located inside the building with long pipe runs to isolated fixtures in the middle and front portions of the building.

2.2.2 Stormwater

The roof at the rear of the building is drained via downpipes on the external rear wall. The western side of the main roof is drained via a shared downpipe with building #47. This downpipe runs from the front of the building externally on the side wall and picks up some of the roof drainage on #47 before entering a downpipe located on the rear of #47.

Additional investigation would be required to determine how the eastern side of the main roof is serviced.

2.2.3 Water

The domestic water supply enters the rear of the building via a DN25 connection and manifolds into 2no. DN32 copper pipes that reticulate around the building. Domestic water also reticulates to fixtures at the rear of the building via the external wall.

The sprinkler control valve is located within the basement, with the sprinkler riser located in the central stairwell with branches on each level servicing the sprinklers. A branch from the fire sprinkler service located in the basement, reticulates throughout the building servicing the fire hose reels.

A fire hydrant system does not exist in the building.

2.3 Conclusion**2.3.1 Sewer**

The proposed development will have a sewer design flow of approx. 140 fixture units in accordance with AS3500.2 and can be adequately serviced by the existing DN100 sewer connection.

It would be recommended to install new DN100 uPVC pipework to the property connection with a new boundary trap and low-level vent.

2.3.2 Stormwater

The stormwater design flow for the development effectively remains unchanged at 5.6 L/s and the site can be adequately serviced by the existing DN100 connection.

It would be recommended to install new DN100 uPVC pipework to the property connection and separating from the shared downpipe with building #47.

2.3.3 Water Services

The proposed development will have a domestic water demand including fire hose reels of approx. 1.4 L/s and will require an upgraded DN40 connection in accordance with Taswater current standards.

The proposed development will have a fire service demand of 10 L/s for the fire hydrant system and 12 L/s for the sprinkler system. Connections are of adequate size, however the connections will require upgrading to current Taswater standards. It is proposed to install a new fire and domestic water connection in the basement with 24 hour unrestricted access via the lobby. The existing DN25

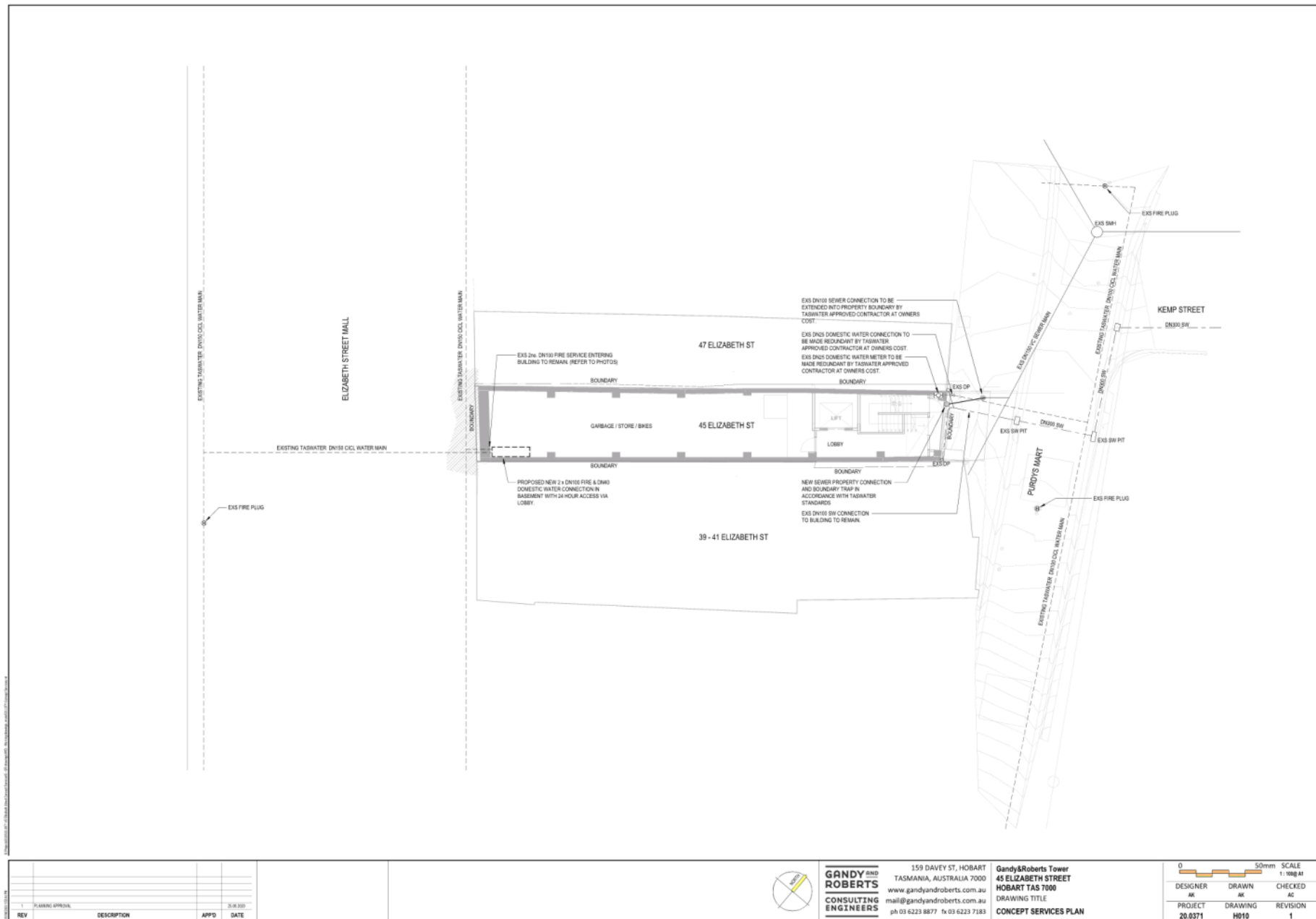
20.0371 – Concept Services Report — 31/08/2020

domestic water connection in Purdys Mart is proposed to be capped and sealed to Taswater approval.

3 Drawings

3.1 Drawing 20.0371 Rev 1- H010 CONCEPT SERVICES PLAN

4 Calculations



Level	Basins	Bath	DWM	Water Closet	Sink	CWM	FHR	TRO	Shower	ET's	Area Type
Basement	0	0	0	0	0	0	0	0	0	0	Lobby / Storage
Ground / Level1	1	0	0	1	0	0	0	0	0	0.24	Commercial Tenancy BE01
Level 2	2	0	1	2	1	1	0	1	1	1	Apartment RA03
Level 3	2	0	1	2	1	1	0	1	1	1	Apartments RA03
Level 4	2	0	1	2	1	1	0	1	1	1	Apartments RA03
Level 5 - 7	2	0	2	3	2	2	0	2	2	1.5	Apartments RA02

Totals	9	0	5	10	5	5	0	5	5	4.74	
Fixture Units	9	0	15	40	15	25		25	10		
Loading Units	9	0	15	20	15	15	0	15	10		
Total Fixture Units	139										
Total Loading Units	99										
Fixture Unit Flow (Sewer)	1.03	L/s								Extrapolated from AS3500.3	
Loading Unit Flow (Water)	4	L/s									
Average Dry Weather Flow	0.025596	L/s									
d' From WSA02 Figure C1	21.61551										
Peak Dry Weather Flow	0.553271	L/s									

Water Demands

Domestic Flow	1.4 L/s	750kPa
Fire Hydrant Flow	10 L/s	750kPa
Fire Sprinkler Flow	12 L/s	750kPa

Total Site Area 0.012 HA

5 Appendix.

5.1 Site Photos



Shared downpipe on external side wall.

20.0371 – Concept Services Report – 31/08/2020



DN25 domestic water connection at rear of building.

20.0371 – Concept Services Report – 31/08/2020



2no. DN100 fire service connections from Elizabeth St.



DN25 water meter located in basement.

20.0371 – Concept Services Report – 31/08/2020



Services running on external wall.





Enquiries to: City Planning
Phone: (03) 6238 2715
Email: coh@hobartcity.com.au

19 August 2020

Frazer Read (All Urban Planning)
19 Mawhera Avenue
SANDY BAY TAS 7005

<mailto:frazer@allurbanplanning.com.au>

Dear Sir/Madam

**45 ELIZABETH STREET, HOBART - WORKS OVERHANGING COUNCIL LAND NOTICE
OF LAND OWNER CONSENT TO LODGE A PLANNING APPLICATION - GMC-20-55**

Site Address:

46 Elizabeth Street, Hobart

Description of Proposal:

Replacement of Fire Stair and Alterations and Extensions overhanging Kemp Street and
Purdy's Mart

Applicant Name:

Frazer Read
All Urban Planning

PLN (if applicable):

n/a

I write to advise that pursuant to Section 52 of the *Land Use Planning and Approvals Act 1993*, I grant my consent on behalf of the Hobart City Council as the owner/administrator of the above land for you to make application to the City for a planning permit for the development described above and as per the attached documents.


Please note that the granting of the consent is only for the making of the application and in no way should such consent be seen as prejudicing any decision the Council is required to make as the statutory planning authority.

Hobart Town Hall
50 Macquarie Street
Hobart TAS 7000

Hobart Council Centre
16 Elizabeth Street
Hobart TAS 7000

City of Hobart
GPO Box 503
Hobart TAS 7001

T 03 6238 2711
F 03 6234 7109
E coh@hobartcity.com.au
W hobartcity.com.au

 CityofHobartOfficial
ABN 39 055 343 428
Hobart City Council

This consent does not constitute an approval to undertake any works and does not authorise the owner, developer or their agents any right to enter or conduct works on any Council managed land whether subject to this consent or not.

If planning approval is granted by the planning authority, you will be required to seek approvals and permits from the City as both landlord, land manager, or under other statutory powers (such as other legislation or City By-Laws) that are not granted with the issue of a planning permit under a planning scheme. This includes the requirement for you to reapply for a permit to occupy a public space under the City's Public Spaces By-law if the proposal relates to such an area.

Accordingly, I encourage you to continue to engage with the City about these potential requirements.

Yours faithfully



(N D Heath)

GENERAL MANAGER

Relevant documents/plans:

Plans by Jaws Architects
20035_DA01 - 20035_DA-11

Approved - General
Manager Consent Only
(GMC 20-56)
19/04/2020

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NOTE:
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DRAWING LEGEND

45 ELIZABETH STREET
DEVELOPMENT APPLICATION - 3/08/2020



PROJECT DETAILS	
DESIGNER NAME	Scott Venable
ACCREDITATION NUMBER	CC 5476
COUNTRY / CLIENT	Exterior Construction & Development
PROJECT ADDRESS	Hobart, Tas 7000
LAND TITLE REF. NO. (OFFICIALS FORD AND VOLUME)	201461/1
TOTAL FLOOR AREA	Site: 120m ² Existing: 100m ² Proposed: 120m ² Total: 240m ²
DESIGN WIND SPEED	TBC
SEA CLASSIFICATION	TBC
SEVERE FIRE PROTECT AREA (SALVING BURNING ATTACH LEVEL)	BA1
ALPHA AREA	TBC
COMPRESSION OVERSTRESS	TBC
OTHER KNOWN SITE HAZARDS (FLOODING, LANDSLIP, COASTAL EROSION, DRAINAGE, SOILS, SAND/SILTS, WIND, SUBSIDENCE, LANDFILL, ETC.)	TBC
CLIMATE ZONE	TBC
FLAMING ZONE	TBC

DRAWING No.	DESCRIPTION
DA01	COVER PAGE & DRAWING SCHEDULE
DA02	DEMOLITION PLANS
DA03	PROPOSED SITE PLAN / ROOF PLAN
DA04	PROPOSED FLOOR PLANS
DA05	SECTIONS
DA06	ELEVATIONS 01
DA07	ELEVATIONS 02
DA08	ELEVATIONS 03
DA09	ELEVATIONS 04
DA10	SHADOW DIAGRAMS 01
DA11	SHADOW DIAGRAMS 02

PROJECT SUMMARY			
LEVEL	EXISTING FLOOR AREA (m ²)	PROPOSED FLOOR AREA (m ²)	DECKS (m ²) / USE
BASEMENT	125	125	-
LEVEL 1 / GROUND	125	115	-
LEVEL 2	125	118	1 x COMMERCIAL
LEVEL 3	125	118	1 x APARTMENT
LEVEL 4	125	118	1 x APARTMENT
LEVEL 5	125	112	1 x APARTMENT
LEVEL 6	111	111	1 x APARTMENT
LEVEL 7	-	8	1 x APARTMENT
TOTAL	760m ²	693m ²	14m ² 6 APARTMENTS + 1 COMMERCIAL (EXISTING)

ARCHITECT'S MARK

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ABN 12 101101010
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ST LOUIS ST LOUIS
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AUSTRALIA 7000

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45 ELIZABETH ST

45 ELIZABETH ST

Hobart, Tas 7000

For

Glasson Construction & Development

DRAWING

DRAWING NAME

COVER PAGE & DRAWING SCHEDULE

DRAWING NO.

20035_DA01

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AS SHOWN

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19/04/2020

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DA01

CHECKED

DATE

19/04/2020

PROJECT NO.

20035

STATUS

DEVELOPMENT APPLICATION

REVISION

DESCRIPTION

EXISTING ELEVATIONS - SHADOW DIAGRAMS

DATE

19/04/2020



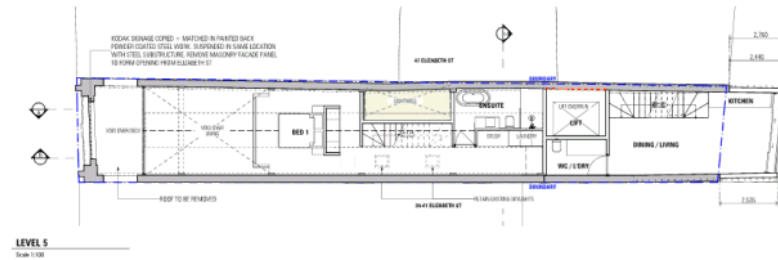
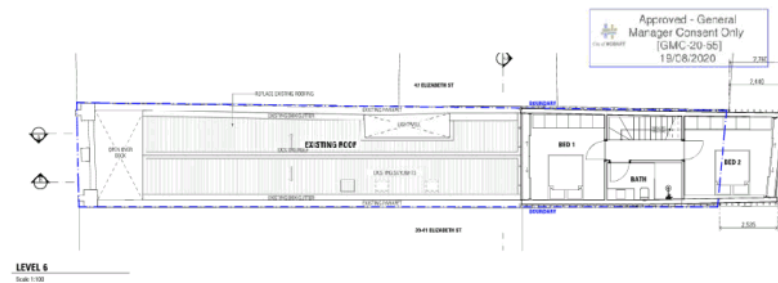




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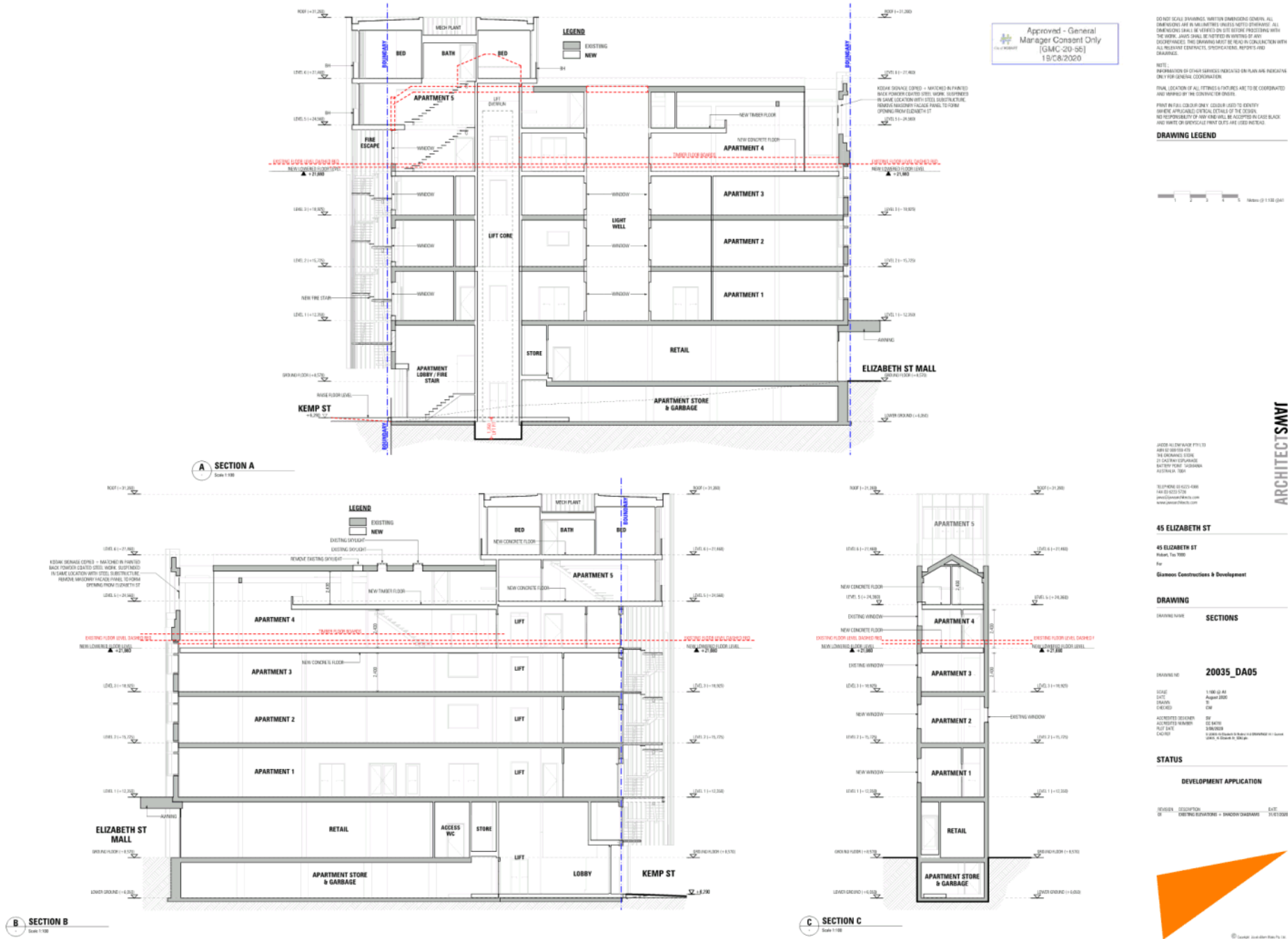
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ARCHITECTSMAW





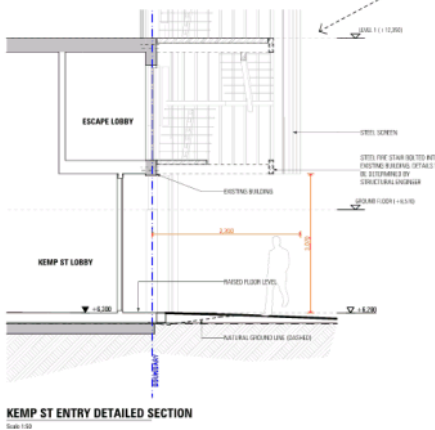
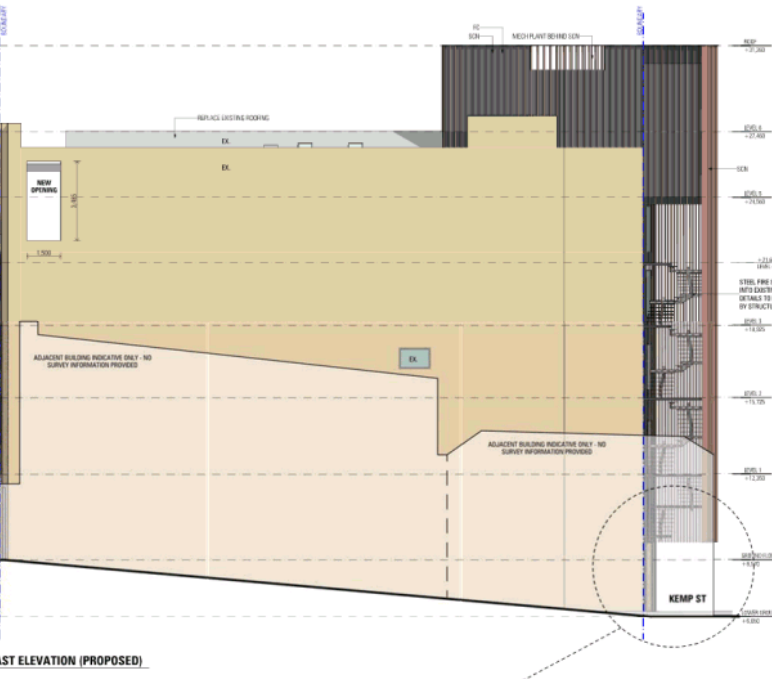
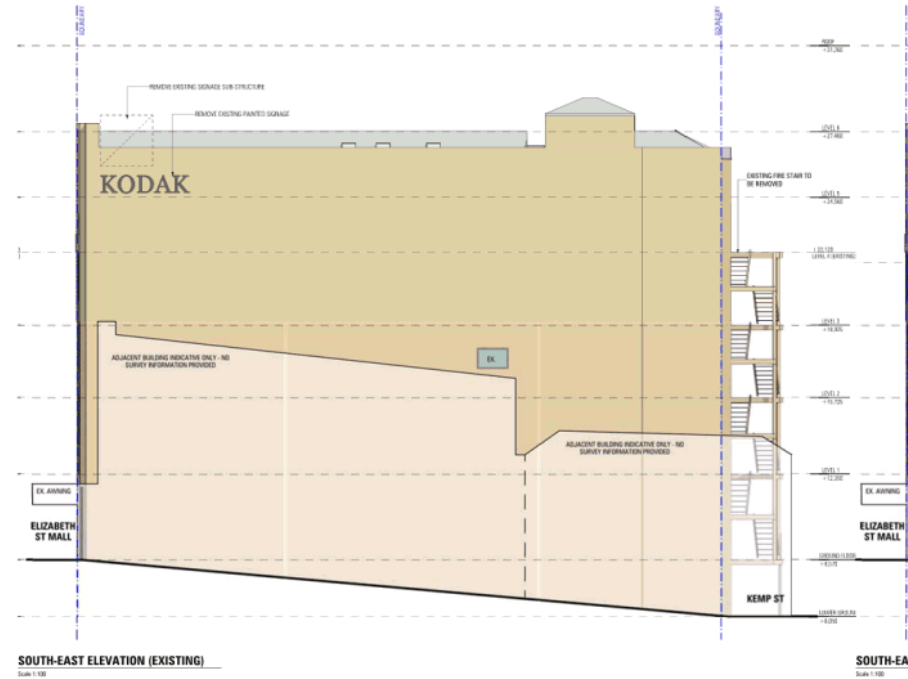
Approved - General
Manager Consent Only
(GMC-20-56)
18/08/2020

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 - SCN STEEL OR ALUMINUM SCREEN
 - FC FIBRE CEMENT CLADDING - CHARCOAL
 - EX EXISTING
 - BH BALUSTRADE / HANDRAIL - METAL OR GLASS
- Scale: 1:100 (2:1:100 (2:1:100))



ARCHITECT'S MARK

45 ELIZABETH ST
Rohrer, Tim 7000
Rohrer
Glasson Construction & Development

DRAWING
DRAWING NAME: ELEVATIONS 02

20035_DA07
DRAWING NO:
SCALE: 1:100 (2:1:100 (2:1:100))
DATE: 18/08/2020
DRAWN BY: JAMES
CHECKED BY: JAMES
APPROVED BY: JAMES
DATE: 18/08/2020

STATUS

DEVELOPMENT APPLICATION
REVISION: 01
DESCRIPTION: EXISTING ELEVATIONS + SHADOW CHARACTER
DATE: 18/08/2020



Approved - General
Manager Consent Only
[GMC-20-56]
19/08/2020

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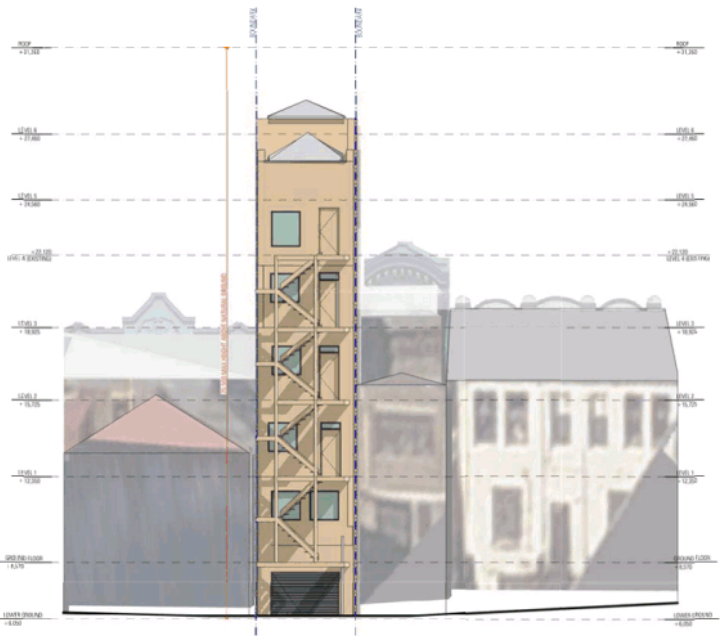
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EX EXISTING
BH BALUSTRADE / HANDRAIL - METAL OR GLASS

1 2 3 4 5 Metres (1:1,000 Scale)



NORTH-EAST ELEVATION - KEMP ST (EXISTING)
Scale 1:100



NORTH-EAST ELEVATION - KEMP ST (PROPOSED)
Scale 1:100

ARCHITECT'S MARK
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45 ELIZABETH ST

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James Architects & Development

DRAWING

DRAWING NAME: ELEVATIONS 03

DRAWING NO: 20035_DA08

SCALE: 1:100 (1:100)
DATE: August 2020
DRAWN: JMW
CHECKED: JMW
APPROVED: JMW
DATE: 19/08/2020
DRAWN: JMW
DATE: 19/08/2020

STATUS

DEVELOPMENT APPLICATION

REVISION DESCRIPTION DATE

01 EXISTING ELEVATIONS - SHADOW CHAIRMAN 19/08/2020



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Manager Consent Only
[GMC-20-56]
18/08/2020

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- FC FIBRE CEMENT CLADDING - CHARCOAL
- EX EXISTING
- BH BALUSTRADE / HANDRAIL - METAL OR GLASS

Scale: 1:100 (A4)

JAMES ALLAN ARCHITECTS
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FAX: 03 9247 4700
www.jamesarchitects.com

45 ELIZABETH ST

45 ELIZABETH ST

Robert, Tim 7000

James Constructions & Development

DRAWING

DRAWING NAME

ELEVATIONS 04

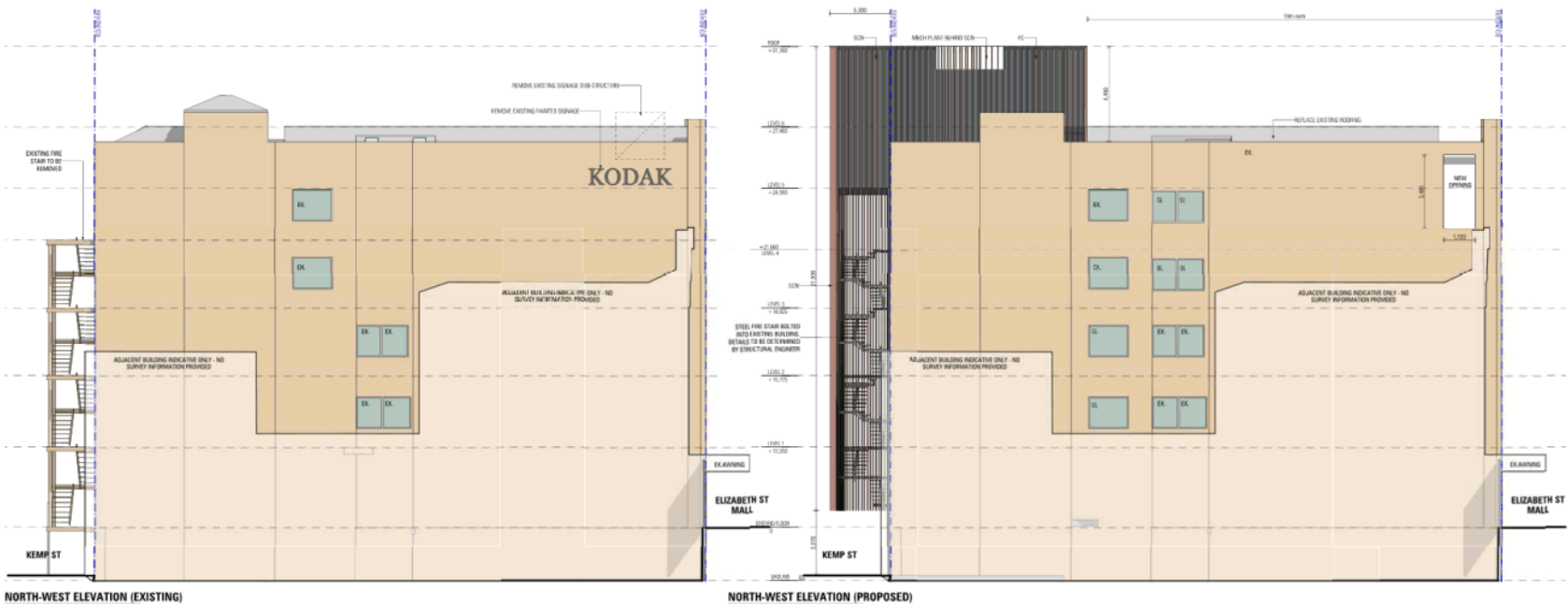
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CHECKED BY: JAW
APPROVED BY: JAW
DATE: 18/08/2020
PROJECT: 20035 - 45 ELIZABETH ST

STATUS

DEVELOPMENT APPLICATION

REVISION DESCRIPTION DATE
01 EXISTING ELEVATIONS + SHADOW CHAIRS 18/08/2020

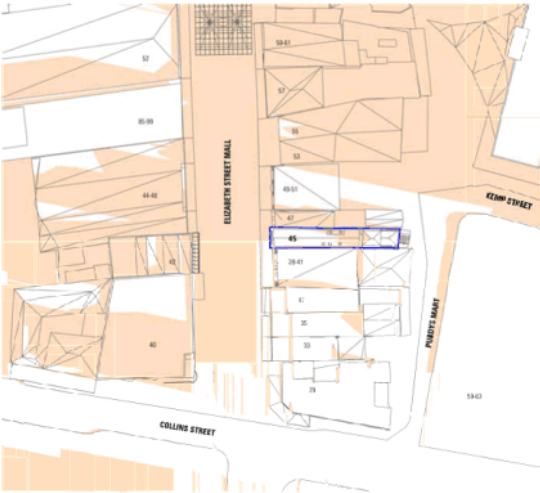


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NORTH-WEST ELEVATION (PROPOSED)

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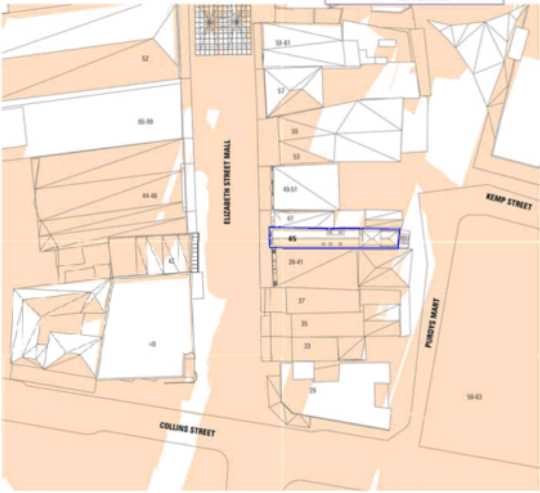
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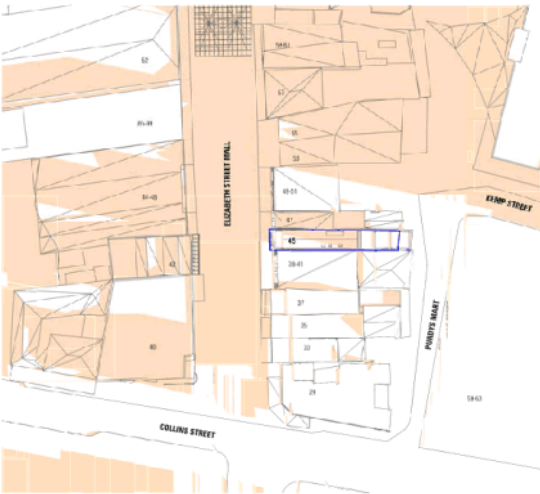


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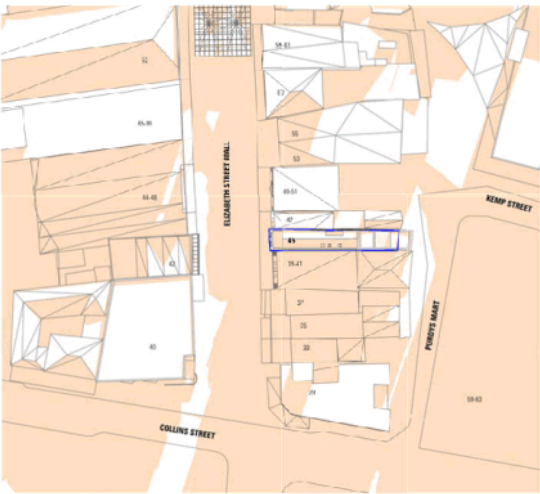
WINTER SOLSTICE - PROPOSED



21 JUNE - 9AM (PROPOSED)
Scale 1:500



21 JUNE - 12PM (PROPOSED)
Scale 1:500



21 JUNE - 3PM (PROPOSED)
Scale 1:500

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19/08/2020

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JACOBS ALLIANCE PTY LTD
ARCHITECTS
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TEL: (02) 9238 1000
FAX: (02) 9238 1001
WWW.JACOBS-PTD.COM

45 ELIZABETH ST

45 ELIZABETH ST
Sydney, NSW 2000

For
Glenwood Construction & Development

DRAWING

DRAWING NAME

SHADOW DIAGRAMS 01

DRAWING NO

20035_DA10

SCALE

1:500 (A4)

DATE

19/08/2020

DESIGNED

DM

APPROVED

DM

DATE

19/08/2020

CHECKED

DM

DATE

19/08/2020

STATUS

DEVELOPMENT APPLICATION

REVISION

DESCRIPTION

EXISTING SHADOWS - SHADOW DIAGRAMS

DATE

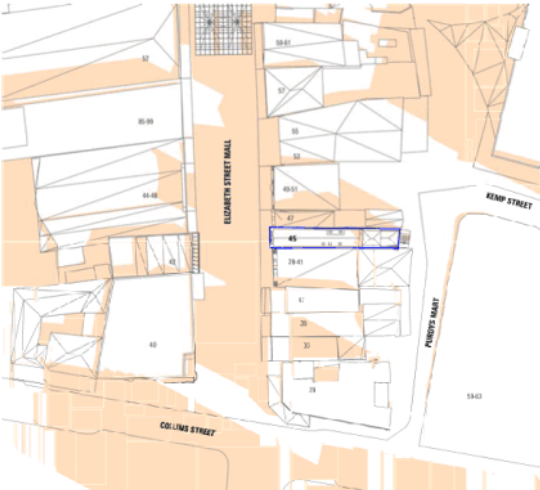
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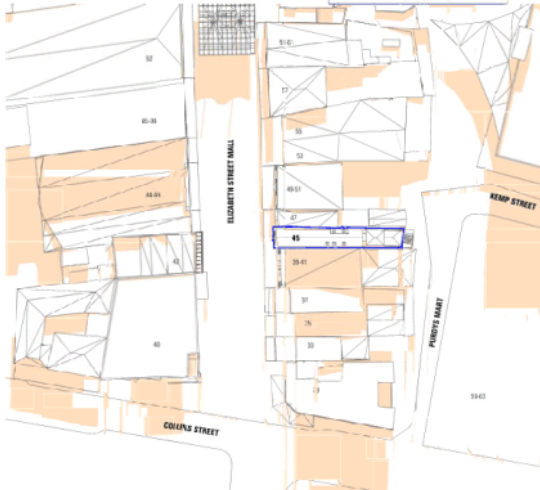
EQUINOX - EXISTING



20 MARCH - 9AM (EXISTING)
Scale 1:500



20 MARCH - 12PM (EXISTING)
Scale 1:500



20 MARCH - 3PM (EXISTING)
Scale 1:500

Approved - General
Manager Consent Only
G/MC-20-95
19/08/2020

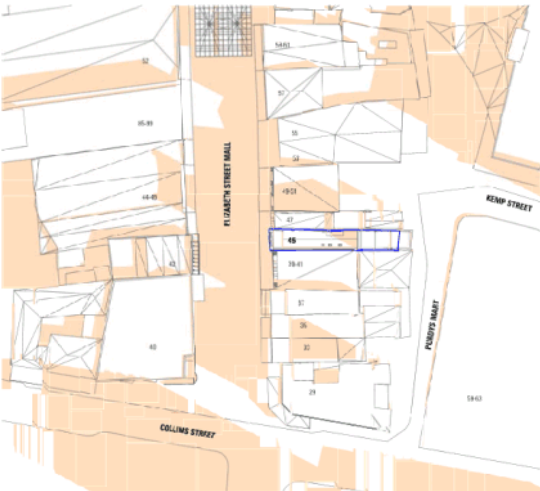
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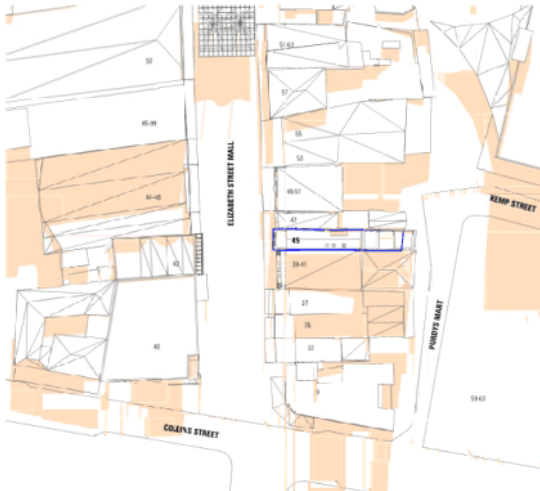
EQUINOX - PROPOSED



20 MARCH - 9AM (PROPOSED)
Scale 1:500



20 MARCH - 12PM (PROPOSED)
Scale 1:500



20 MARCH - 3PM (PROPOSED)
Scale 1:500

45 ELIZABETH ST

45 ELIZABETH ST
Melbourne, VIC 3000

For
Glasson Construction & Development

DRAWING

DRAWING NAME: SHADOW DIAGRAMS 02

DRAWING NO: 20035_DA11

SCALE: 1:500 (A4)
DATE: 19/08/2020
DRAWN BY: JMW

APPROVED BY: JMW
DATE: 19/08/2020
FLAT DATE: 19/08/2020

STATUS

DEVELOPMENT APPLICATION

REVISION DESCRIPTION DATE
01 EXISTING ELEVATIONS - SHADOW DIAGRAMS 19/08/2020







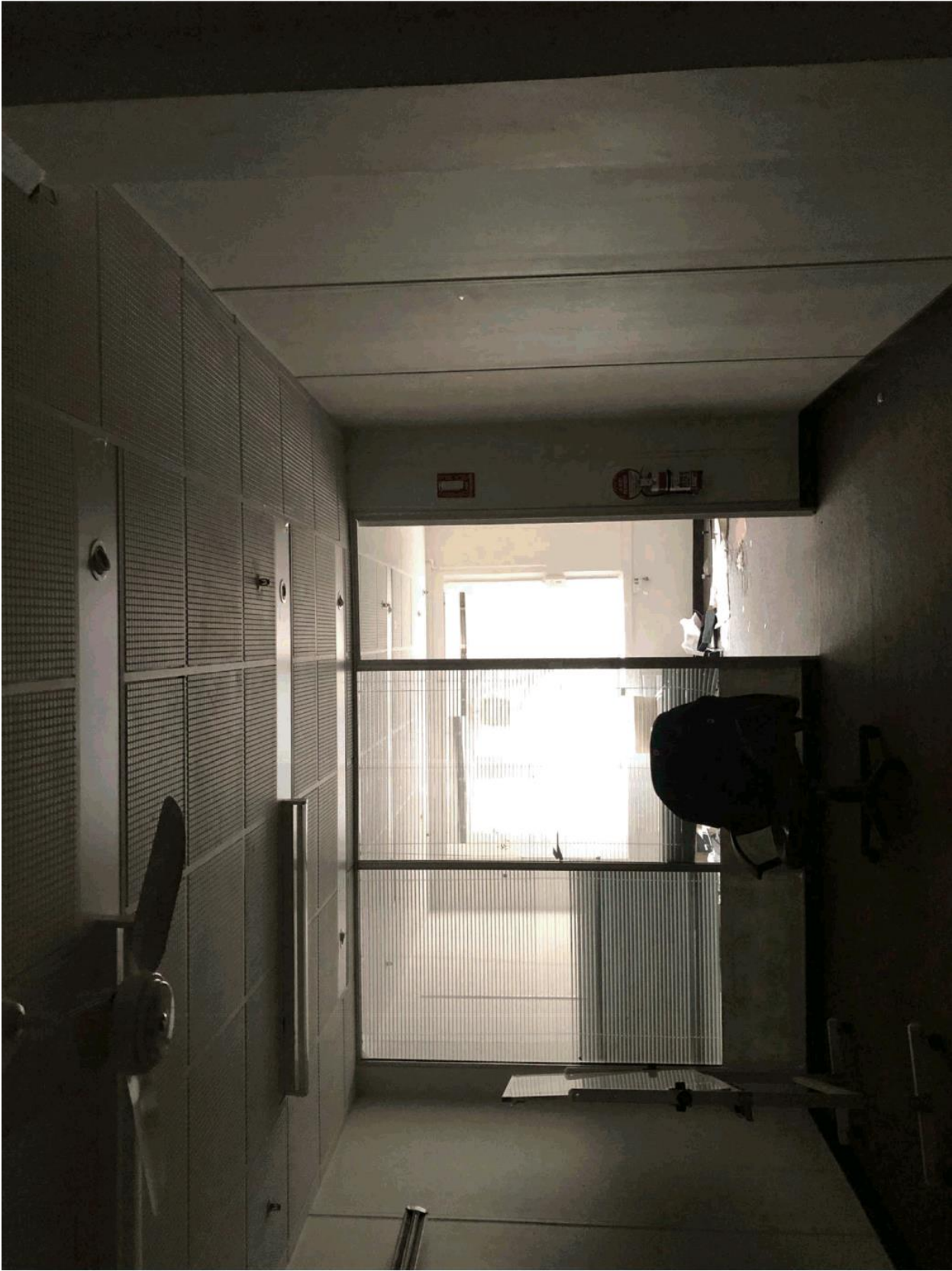






























- 12 63 Davey Street, Hobart - Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Infrastructure and Access Works - PLN-19-319**
File Ref: F20/116068
-

Memorandum of the Manager Development Appraisal of 30 October 2020 and attachments.

Delegation: Council



City of **HOBART**

MEMORANDUM: CITY PLANNING COMMITTEE

63 Davey Street, Hobart - Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Infrastructure and Access Works - PLN-19-319

At the Council meeting on 26 October 2020, the applicant requested that this item be deferred to allow the applicant to provide revised plans for consideration. The Council supported the deferral motion, with the item due to return to the Council on 9 November 2020.

A copy of the revised plans are at **Attachment A**. A copy of the planning report (along with the original plans) is at **Attachment B**.

It is important to note that the revised plans cannot be accepted as an amendment to the application, to replace the original plans. The Supreme Court has made it clear that it is not possible to amend an application after it has been advertised.

However, the Council can effectively amend an application by imposing conditions, as long as the changes do not make the proposal “substantially different”. The Council may decide that the proposal would satisfy the Scheme if it was altered by imposing certain conditions, which would effectively incorporate the revised plans.

This is a subtle but significant difference.

Through providing the revised plans, the applicant has addressed the first ground of refusal which is based on an analysis of streetscape and townscape. It is agreed by the Council’s Development Appraisal Planner that, if the revised plans are incorporated into a permit granted by the Council, then the acceptable solution would be met for clause 22.4.1 A1 of the Scheme.

Conclusion

Despite the proposed changes, there is no change to the recommendations to the Council; it is still recommended for refusal based on the grounds as stated in the planning report.

The Senior Cultural Heritage Officer has indicated that the revised plans do not sufficiently address the reasons for refusal which are provided in the planning report, and on that basis, the recommendation for refusal remains.

Given that the proposal continues to be recommended for refusal on heritage grounds, the assessment by the Development Appraisal Planner for clause 22.4.1 will continue on the original plans (not the revised plans) and also continues to be recommended for refusal on that basis.

Conditions

Legal advice has been provided that the changes as proposed by the revised plans would not be substantially different from the original proposal, and it is possible for the Council to approve the proposal with appropriate conditions which clarify the specific changes.

A set of conditions, including conditions which incorporate the changes in the revised plans, will be provided to Elected Members prior to the meeting on Monday.

RECOMMENDATION

Pursuant to the Hobart Interim Planning Scheme 2015, the Council refuse the application for Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Infrastructure and Access Works at 63 Davey Street and 186 Macquarie Street, and Adjacent Road Reserve, HOBART for the following reasons:


- 1 The proposal does not meet the acceptable solution or the performance criterion with respect to clause Part D 22.4.1 A1 and P1.1(a) of the Hobart Interim Planning Scheme 2015 because the development does not make a positive contribution to the streetscape and townscape, having regard to the height, bulk and design of existing and proposed buildings.
- 2 The proposal does not meet the acceptable solution or the performance criterion with respect to clause E13.8.2 P1 of the Historic Heritage Code of the Hobart Interim Planning Scheme 2015 because the proposal results in detriment to the historic cultural heritage significance of the precinct through its design and siting.
- 3 The proposal does not meet the acceptable solution or the performance criterion with respect to clause 22.4.1 P5 of the Historic Heritage Code of the Hobart Interim Planning Scheme 2015 because the proposed building unreasonably dominates and has a materially adverse impact on adjacent existing buildings of cultural heritage significance through its height.


As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

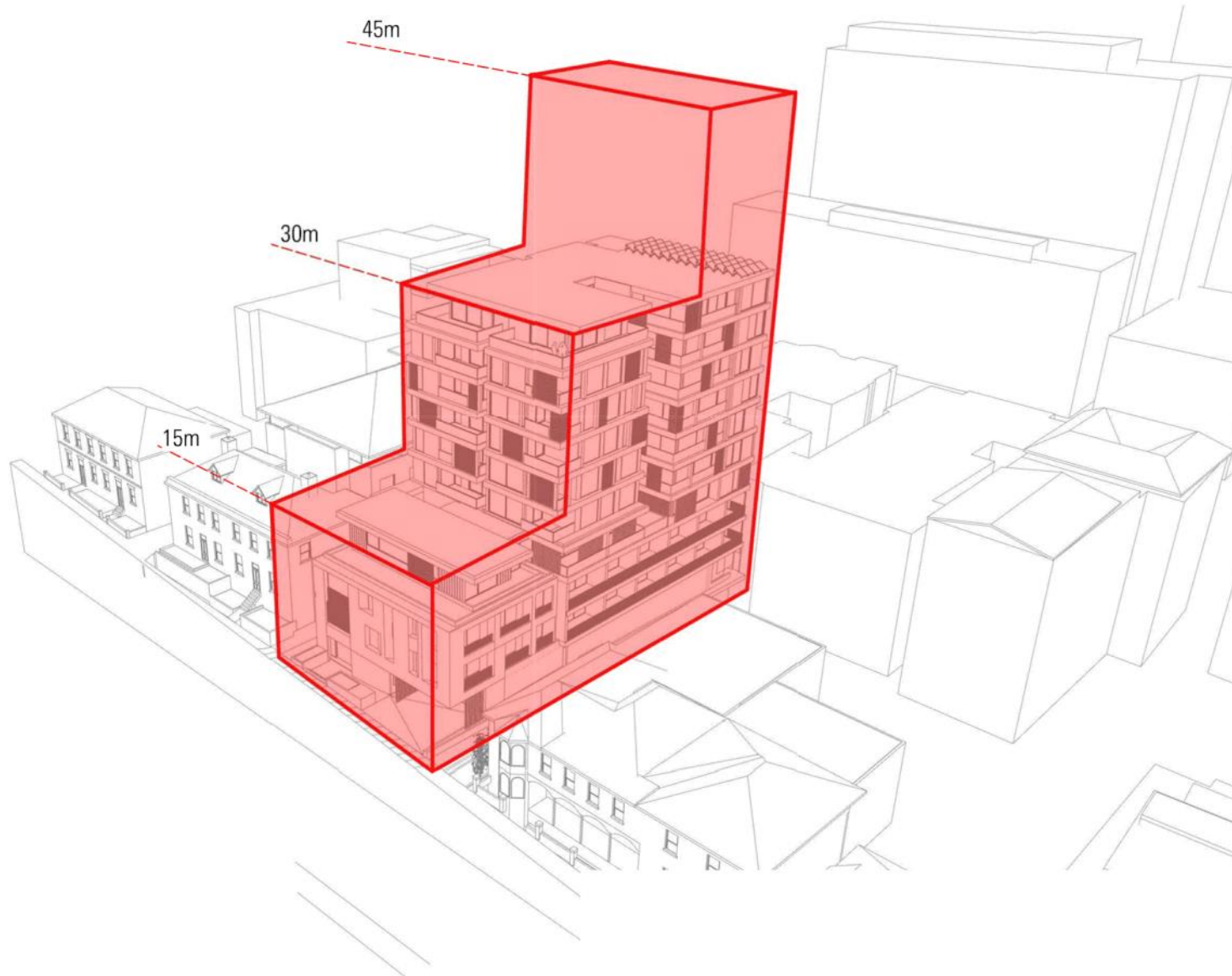


Karen Abey
**MANAGER DEVELOPMENT
APPRAISAL**

Date: 30 October 2020
File Reference: F20/116068

Attachment A: PLN-19-319 - 63 DAVEY STREET HOBART TAS 7000 -
Amended Plan - additional information ↓ 

Attachment B: PLN-19-319 - 63 DAVEY STREET HOBART TAS 7000 -
Original report and drawings ↓ 





DEVELOPMENT APPLICATION 30/04/2019



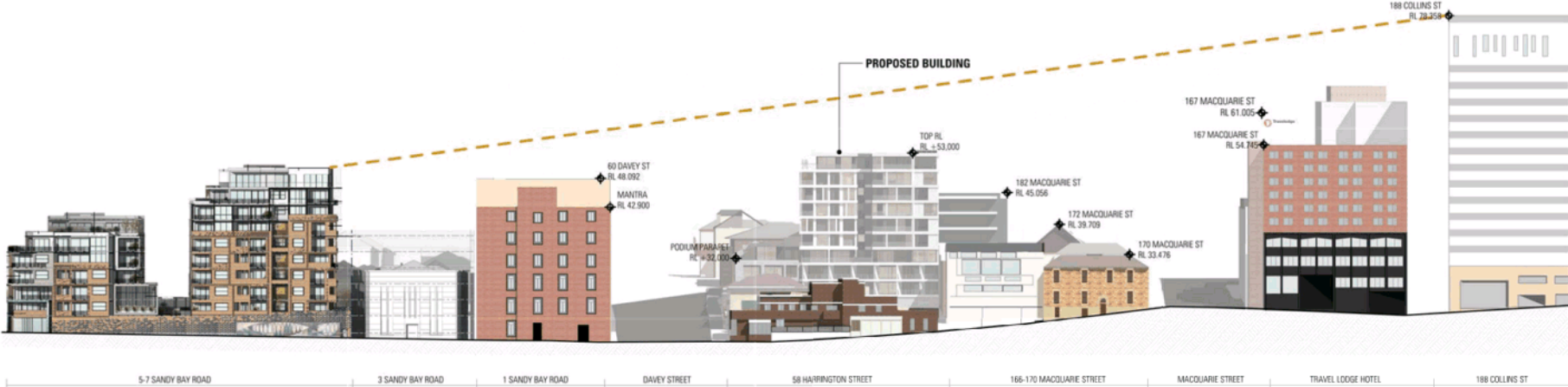
REVISED DESIGN 14/05/2020



CURRENT DESIGN 20/10/2020



SOUTH - EAST ELEVATION
1:200



HARRINGTON STREET ELEVATION



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REV.	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	05/04/2019
01	REVISED HEIGHT	26/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
03	TASWATER REVISION TO FRONT PLANTER	12/06/2020
04	EXTENDED DAVEY STREET ELEVATION	16/06/2020
05	REDUCED HEIGHT (30m)	20/10/2020

ARCHITECTS JAWA

DEVELOPMENT APPLICATION



SITE PLAN/ ROOF PLAN 1:200
1:200

DEVELOPMENT APPLICATION

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NOTE:
INFORMATION OF OTHER SERVICES INDICATED ON PLAN ARE INDICATIVE ONLY FOR GENERAL COORDINATION.
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PROJECT

DAVEY ST APARTMENTS
63 Davey Street Melbourne VIC 3000
For
Telford's Kinetic Unit Trust

DRAWING

SITE PLAN/ ROOF PLAN

1509_DAB3 |

SCALE: AS SHOWN @ A1
DATE: MARCH 2019
DRAWN: JAMES
CHECKED: JAMES
APPROVED: JAMES
PROJECT NUMBER: 2019/000000
CADD: JAMES
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STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2018
02	REVISED HEIGHT	28/03/2020
03	REVISED HEIGHT FOR DA	14/05/2020
04	TAGWATER REVISION TO FRONT PLANTER	10/06/2020
05	TAGWATER REVISION TO FRONT PLANTER	10/06/2020
06	REDUCED HEIGHT (30m)	25/10/2020



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DEVELOPMENT APPLICATION

REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	09/04/2019
01	REVISED HEIGHT	26/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
03	TASWATER REVISION TO FRONT PLANTER	12/06/2020
04	EXTENDED DAVEY STREET ELEVATION	16/06/2020
05	REDUCED HEIGHT (20m)	20/10/2020

ARCHITECTS JAWSTICH



REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	09/04/2019
01	REVISED HEIGHT	26/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
03	TASWATER REVISION TO FRONT PLANTER	12/06/2020
04	EXTENDED GAVEY STREET ELEVATION	16/06/2020
05	REDUCED HEIGHT (30m)	20/10/2020

DEVELOPMENT APPLICATION





REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	09/04/2019
01	REVISED HEIGHT	26/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
03	TASWATER REVISION TO FRONT PLANTER	12/06/2020
04	EXTENDED DAVEY STREET ELEVATION	16/06/2020
05	REDUCED HEIGHT (20m)	20/10/2020

ARCHITECTS JAWST

DEVELOPMENT APPLICATION





REV.	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	09/04/2019
01	REVISED HEIGHT	26/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
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04	EXTENDED DAVEY STREET ELEVATION	16/06/2020
05	REDUCED HEIGHT (30m)	20/10/2020

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REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	09/04/2019
01	REVISED HEIGHT	28/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
03	TASWATER REVISION TO FRONT PLANTER	12/06/2020
04	EXTENDED DAVEY STREET ELEVATION	16/06/2020
05	REDUCED HEIGHT (DOME)	20/10/2020

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- DRAWING LEGEND**
- BAL BALUSTRADE- GLASS
 - GLZ 1 GLAZED PANELS (SPRINKLERLESS)
 - GLZ 2 GLAZED PANELS (NONVACUUM ALUMINUM FRAME PWA)
 - ENGRH
 - MWS 1 METAL WALL SHEETING
 - MWS 2 METAL WALL SHEETING (VERTICAL)
 - PC 1 PRECAST CONCRETE PANELS (SHADE)
 - PC 2 PRECAST CONCRETE PANELS (SHADE LIGHT GREY)
 - PC 3 PRECAST CONCRETE PANELS (SHADE GREY)
 - SC STONE/TILE CLADDING- LARGE FORMAT
 - TM TIMBER CLADDING
 - SCRN SCREEN (POWDERCOATED ALUMINUM)

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PROJECT

DAVEY ST APARTMENTS
11 Davey Street Hobart TAS 7000
For
Tollpops Kiosk Unit Trust

DRAWING

ELEVATION - 4

1509_DA11 |

DATE	AS DRAWN 01/01/2019
DATE	REVISION 01/01/2019
DATE	01/01/2019
DATE	01/01/2019

ARCHITECTED BY DESIGNER
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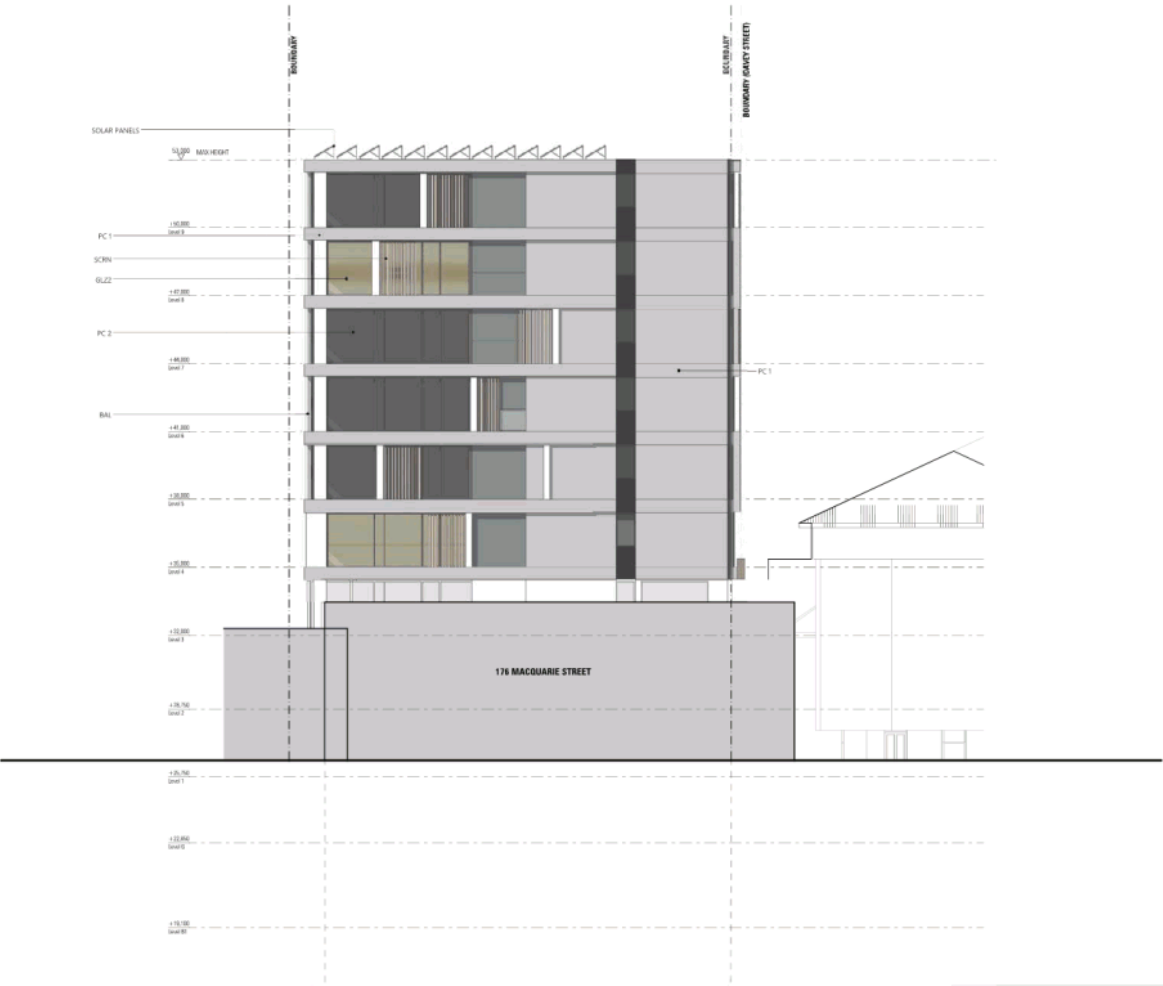
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11 Davey Street Hobart TAS 7000
11 Davey Street Hobart TAS 7000

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2019
02	REVISED HEIGHT	24/05/2019
03	REVISED HEIGHT FOR DA	14/05/2019
04	TAKEN INTO REVISION TO FRONT PLANTER	12/06/2019
05	EXTENDED DAVEY STREET ELEVATION	16/06/2019
06	REDUCED HEIGHT (DAH)	20/10/2019

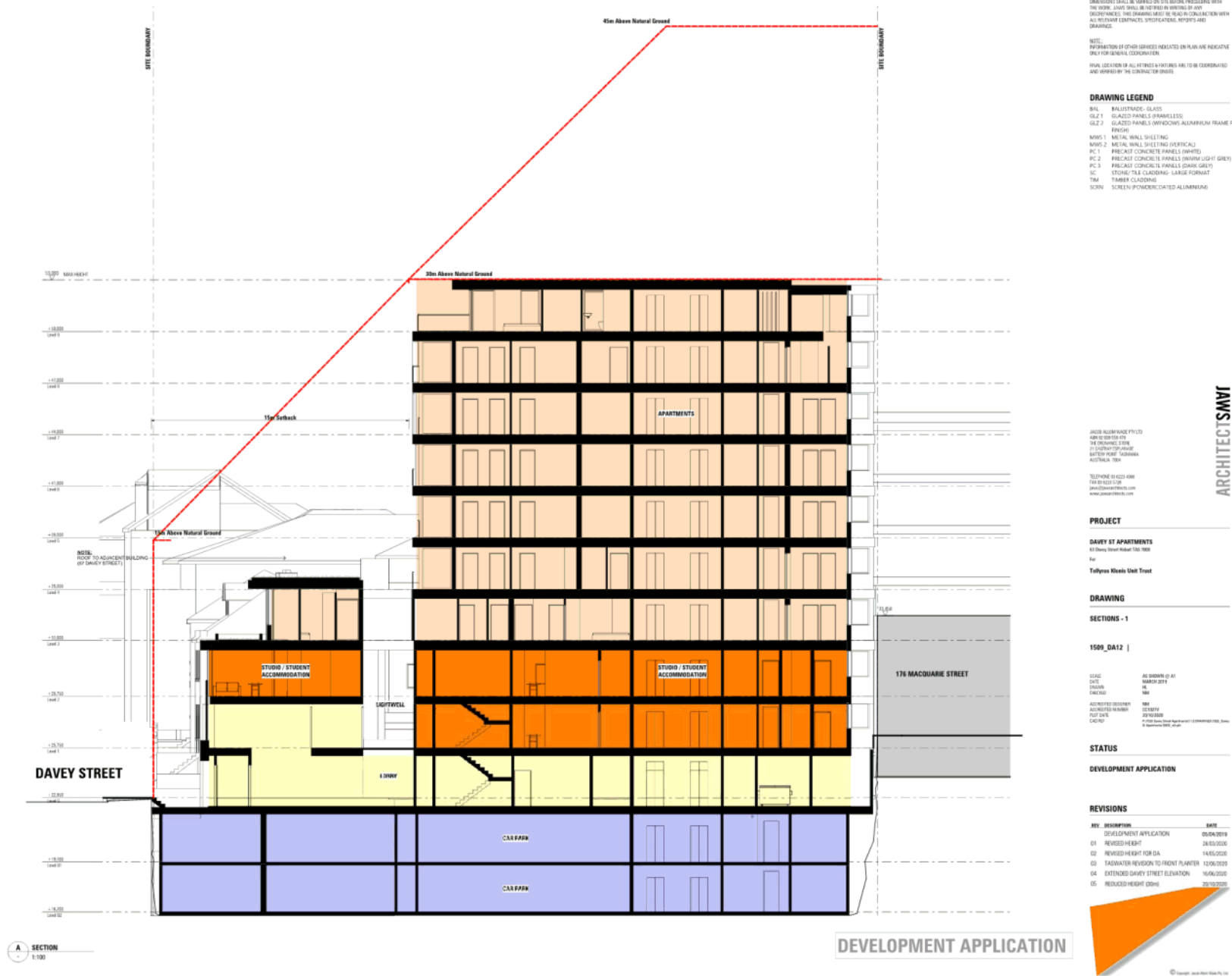


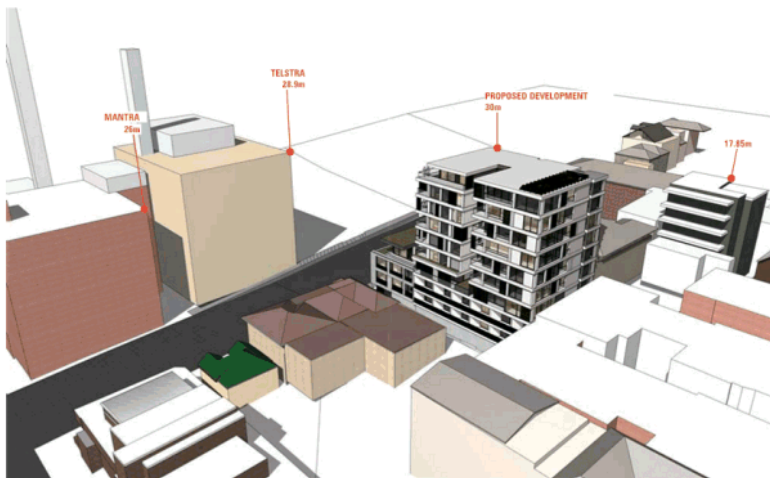
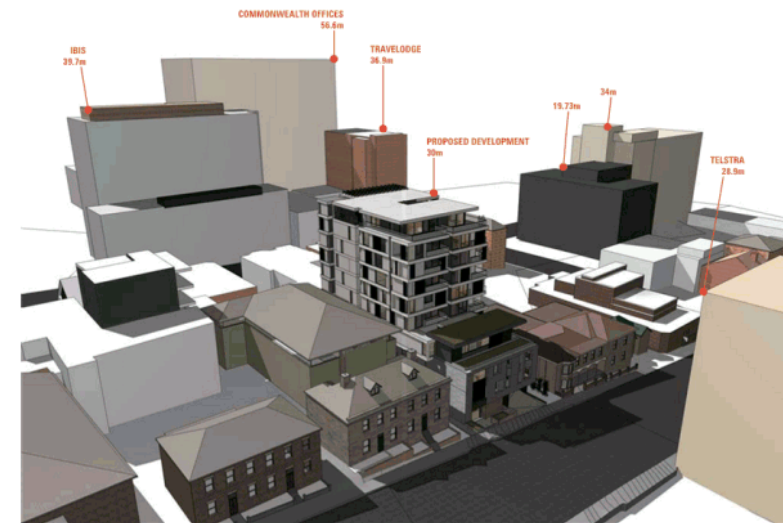
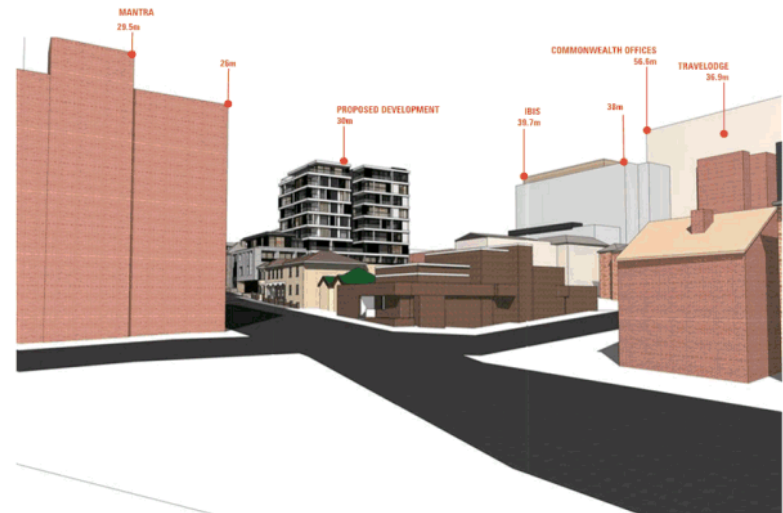
NORTH - WEST ELEVATION
1:100

DEVELOPMENT APPLICATION

ARCHITECTS







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DRAWING LEGEND

- BAL BALUSTRADE- GLASS
- GLZ.1 GLAZED PANELS (SPRINKLERLESS)
- GLZ.2 GLAZED PANELS (NONSPRINKLER ALUMINUM FRAME POOL ENCLOSURE)
- MWS.1 METAL WALL SHEETING
- MWS.2 METAL WALL SHEETING (VERTICAL)
- PC.1 PRECAST CONCRETE PANELS (WHITE)
- PC.2 PRECAST CONCRETE PANELS (DARK GRAY)
- PC.3 PRECAST CONCRETE PANELS (DARK GRAY)
- SC STONE/TILE CLADDING- LARGE FORMAT
- TM TIMBER CLADDING
- SCRN SCREEN (POWDER COATED ALUMINUM)

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PROJECT

DAVEY ST APARTMENTS
100/100 STATION ST
For
Tollways VicRoads Unit Trust

DRAWING

3D CONTEXT HEIGHT STUDY

1509_DAZ1 |

DATE: 05/04/2019
SCALE: 1:100
DRAWN: JAC
CHECKED: JAC
PROJECT: 1509
ADDRESS: 100/100 STATION ST
CLIENT: TOLLWAYS VICROADS UNIT TRUST
CADD: JAC

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2019
02	REVISED HEIGHT	26/05/2020
03	REVISED HEIGHT FOR DA	14/05/2020
04	TAKEN INTO REVISION TO FRONT PLANTER	12/06/2020
05	EXTENDED DAVEY STREET ELEVATION	16/06/2020
06	REDUCED HEIGHT (DAH)	20/10/2020

NOTE: SURVEY POINTS PROVIDED BY LEARY & COX

DEVELOPMENT APPLICATION



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**APPLICATION UNDER HOBART INTERIM PLANNING SCHEME 2015**

Type of Report:	Committee
Council:	26 October 2020
Expiry Date:	27 October 2020
Application No:	PLN-19-319
Address:	63 DAVEY STREET , HOBART 186 MACQUARIE STREET , HOBART ADJACENT ROAD RESERVE
Applicant:	(Tellyros Klonis Unit Trust, by their Agent, Ireneinc Planning and Urban Design) 49 Tasma Street 49 Tasma Street
Proposal:	Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Infrastructure and Access Works
Representations:	Three Hundred and Ninety Three (393)
Performance criteria:	Central Business Zone Development Standards; Parking and Access Code; Attenuation Code; Historic Heritage Code

1. Executive Summary

- 1.1 Planning approval is sought for Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Works, at 63 Davey Street, 186 Macquarie Street, and the adjacent Davey Street road reserve.

1.2 More specifically the proposal includes:

- The redevelopment of the site at 63 Davey Street, Hobart, involving the demolition of the existing single storey building at 63 Davey Street, including removal of the existing eight parking spaces in the forecourt immediately off Davey Street. The new works are to facilitate use and development for 51 apartments providing a mix of 30 residential and 21 student accommodation apartments.
- The proposed building is in the form of two interconnected building blocks, comprising a four storey building block that will front Davey Street. A second building component is set back 15m from the street frontage. This building component has 10 levels comprising a ground floor, two levels of student accommodation and a further seven levels of residential apartments. Single bedroom apartments are proposed for the student accommodation on levels 1 and 2, with two bedroom apartments across levels 3 to 9, and a three bedroom apartment on level 10 for residential occupation. The uppermost, rooftop level apartment includes a lift overrun and plant room above and is set back 30m from the street frontage. The proposal includes two levels of basement parking for 42 cars and five motorcycles accessed via car lifts. Bicycle parking, vehicle access and manoeuvring, a lobby, services and building access are provided at ground floor level.
- Infrastructure works are also proposed within the basement car parking level of the adjacent property at 186 Macquarie Street.
- Alterations to the access to the site, including altering the footpath levels, are proposed within the Davey Street road reservation.
- The building is proposed to have a maximum height of 35 metres above natural ground level measured to the top of its rooftop plant.
- The total gross floor area of the proposed building is 6,340m².
- External materials are listed as being contemporary, with a mix of precast concrete, stone/tile cladding, metal wall sheeting, timber, aluminium and glass shown. Green roof elements are proposed from level four upwards.

1.3 The proposal relies on performance criteria to satisfy the following standards and codes:

- 1.3.1 Central Business Zone Development Standards - Building Height; Setback
- 1.3.2 Parking and Access Code - Number of Car Parking Spaces (Central Business Zone); Design of Vehicular Accesses; and Facilities for Commercial Vehicles
- 1.3.3 Attenuation Code - Development for Sensitive Use in Proximity to Use with Potential to cause Environmental Harm

- 1.3.4 Historic Heritage Code - Heritage Precinct: Demolition and Buildings and Works; Places of Archaeological Potential: Building, Works and Demolition
- 1.4 Three Hundred and Ninety Three (393) representations (366 objecting to/ 27 supporting) the proposal were received within the statutory advertising period between 21/08 and 04/09/2020.
- 1.5 The proposal was considered by the Council's Urban Design Advisory Panel at its meeting on 27 August 2020. The minutes of this meeting are provided as an attachment to this report. The Panel found that the lower elements of the development were well-considered and made a positive contribution to the streetscape but felt more needed to be done in terms of landscaping, however ultimately the Panel concluded that the overall height of the proposal was not appropriate and that only a significant reduction in height could resolve their concerns in terms of the development's impact upon streetscape, townscape and heritage values.
- 1.6 The proposal is recommended for refusal.
- 1.7 The final decision is delegated to the Council, because the application is recommended for refusal, includes Council owned land, is for a major development, and more than five objections have been received.

2. Site Detail



Figure 1: Aerial view of the overall subject property and surrounds. 63 Davey Street is the smaller rectangular-shaped outlined property to the north-east (Source: HCC Geocortex).

- 2.1 63 Davey Street, Hobart (Figures 1 and 2) has an area of approximately 810m² and contains a single storey brick building that has been used by the Navy Club of Tasmania in the past and more recently as an antiques dealership (the Sullivans Cove Emporium), and currently as a dance studio. The site has a south-easterly facing frontage to Davey Street. The adjacent property at 186 Macquarie Street is included in the description of the site as infrastructure works within this site's basement car parking level are proposed in order to adequately cater for the servicing of the proposed development.



Figure 2: 63 Davey Street as it currently presents to the street. (Source: Google).

- 2.2 The property and the land to the north-east and south-west is within the Hobart 1 Heritage Precinct. The adjoining properties (61 Davey Street, and 174, 176, and 186 Macquarie Street) are also individually listed as heritage places within the Historic Heritage Code of the *Hobart Interim Planning Scheme 2015*. With the exception of the adjacent part of 186 Macquarie Street, the adjoining properties are also listed on the Tasmanian Heritage Register (Figure 3).



Figure 3: Showing heritage listings for the site and area. Purple denotes both Tasmanian Heritage Council and Hobart Interim Planning Scheme 2015 heritage listing. Red denotes Hobart Interim Planning Scheme 2015 heritage listing only. Light blue denotes a Heritage Precinct. The hatching indicates the area of archaeological potential (Source: HCC Geocortex).

- 2.3 The adjacent property to the north-east (at 61 Davey Street) contains a substantial two storey building that is used for consulting rooms, and also includes the Royal Australian Air Force Museum. The adjacent property to the south-west (part of 186 Macquarie Street) contains one of the several buildings that make up the St Helens Hospital complex. The properties to the rear of the site with frontage to Macquarie Street are used as consulting rooms and offices.
- 2.4 Under the *Hobart Interim Planning Scheme 2015*, the site is located within the Central Business Zone, the Central Business Core Area, and the area of archaeological sensitivity (Figures 4 and 5). The site is not within the Active Frontage Overlay and Davey Street is not a Solar Penetration Priority Street. The site's Davey Street frontage faces south east.



Figure 4: Showing the zoning of the site under the Hobart Interim Planning Scheme 2015 and surrounding area. The site is bordered in light blue. The blue denotes the Central Business Zone, the grey denotes the Urban Mixed Use Zone, and the green denotes the Open Space Zone. The uncoloured area around the Cove is covered by the Sullivans Cove Planning Scheme 1997. (Source: HCC Geocortex).



Figure 5: The site is bordered blue. The light blue highlighting is the Core Height Area, the yellow is the Fringe Height Area, the orange indicates a solar penetration priority street. (Source: HCC Geocortex).

3. Proposal

- 3.1 Planning approval is sought for Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Works, at 63 Davey Street, 186 Macquarie Street, and the adjacent Davey Street road reservation.

3.2 More specifically the proposal is for:

- The redevelopment of the site at 63 Davey Street, Hobart, involving the demolition of the existing single storey building at 63 Davey Street, including removal of the existing eight parking spaces in the forecourt immediately off Davey Street. The new works are to facilitate use and development for 51 apartments providing a mix of 30 residential and 21 student accommodation apartments.
- The proposed building is in the form of two interconnected building blocks, comprising a four storey building block that will front Davey Street. A second building component is set back 15m from the street frontage. This building component has 10 levels comprising a ground floor, two levels of student accommodation and a further seven levels of residential apartments. Single bedroom apartments are proposed for the student accommodation on levels 1 and 2, with two bedroom apartments across levels 3 to 9, and a three bedroom apartment on level 10 for residential occupation. The uppermost, rooftop level apartment includes a lift overrun and plant room above and is set back 30m from the street frontage. The proposal includes two levels of basement parking for 42 cars and five motorcycles accessed via car lifts. Bicycle parking, vehicle access and manoeuvring, a lobby, services and building access are provided at ground floor level.
- Infrastructure works are also proposed within the basement car parking level of the adjacent property at 186 Macquarie Street.
- Alterations to the access to the site, including altering the footpath levels, are proposed within the Davey Street road reservation.
- The building is proposed to have a maximum height of 35 metres above natural ground level measured to the top of its rooftop plant.
- The total gross floor area of the proposed building is 6,340m².
- External materials are listed as being contemporary, with a mix of precast concrete, stone/tile cladding, metal wall sheeting, timber, aluminium and glass shown. Green roof elements are proposed from level four upwards.

3.3 Images of the proposed development:



Figure 6: The Davey Street (south-eastern) elevation of the proposed building, in context with the existing buildings to either side. (Source: Irenelnc/JAWS)



Figure 7: North-eastern (side) elevation of the proposed building. (Source: Irenelnc/JAWS).

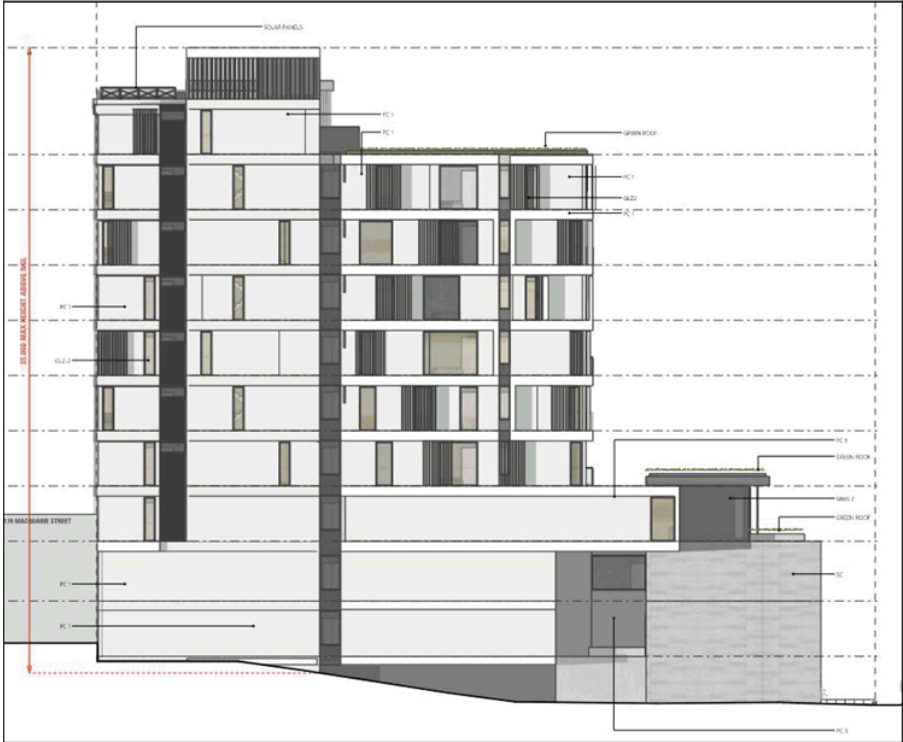


Figure 8: South-western (side) elevation of the proposed building (Source: Irenelnc/JAWS).

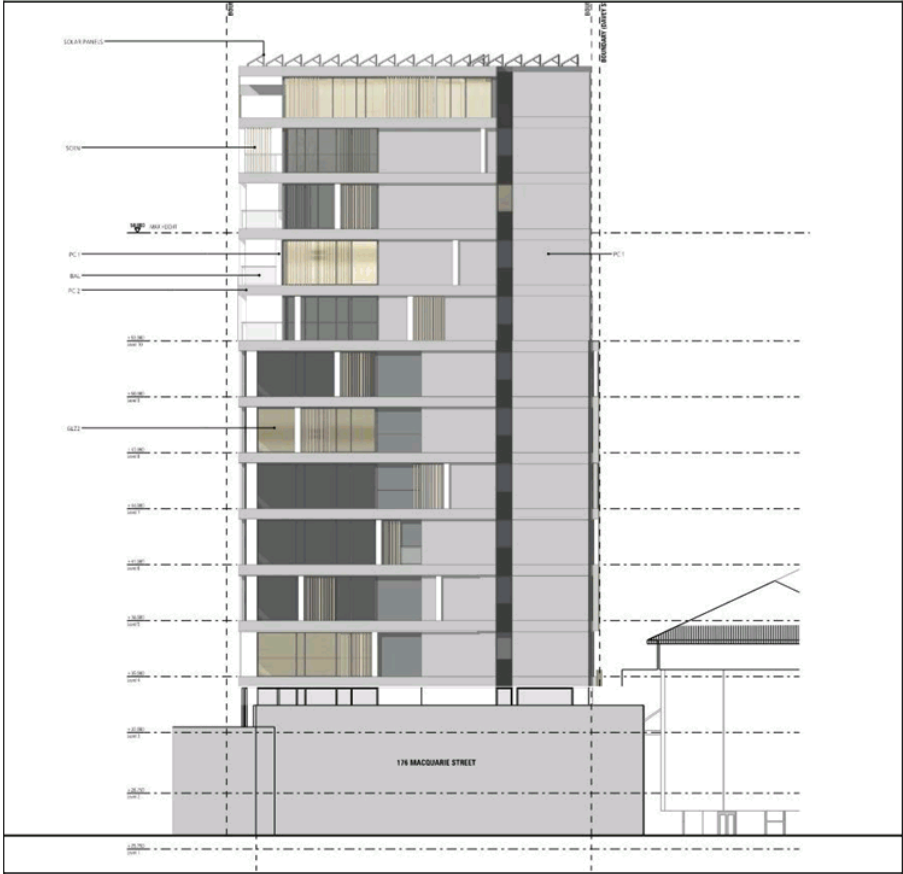


Figure 9: North-western (rear) elevation of the proposed building (Source: Irenelnc/JAWS).



Figure 10: Architect's render of the proposed building as viewed from further down Davey Street, adjacent St David's Park (Source: Irenelnc/JAWS).

4. Background

- 4.1 An early, pre-lodgement version of this proposal was considered by Council's Urban Design Advisory Panel in January 2019. The version considered by the Panel at the time consisted of 14 storeys and a total of 54 residential apartments, along with the two basement car parking levels and ground floor entry and services level. There was a greater number of three-bedroom apartments in this proposal and the maximum height of the building was 44.9m.

- 4.2 The current proposal was considered by the Council's Urban Design Advisory Panel at its meeting on 27 August 2020. The minutes of this meeting are provided as an attachment to this report. The Panel found that the lower elements of the development were well-considered and made a positive contribution to the streetscape but felt more needed to be done in terms of landscaping, however ultimately the Panel concluded that the overall height of the proposal was not appropriate and that only a significant reduction in height could resolve their concerns in terms of the development's impact upon streetscape, townscape and heritage values.
- 4.3 Alterations to the access to the site, including altering the footpath levels, are proposed within the Davey Street road reservation. Because of the dual ownership of Davey Street, both Crown and General Manager consent to lodge the application have been provided.

5. Concerns raised by representors

- 5.1 Three Hundred and Ninety Three (393) representations (366 objecting/ 27 supporting) to the proposal were received within the statutory advertising period between 21/08 and 04/09/2020.
- 5.2 The following table summarises the concerns raised in the representations received. Those concerns which relate to a discretion invoked by the proposal are addressed in Section 6 of this report.

<u>For</u>
The city needs more housing supply, helping to solve the current housing crisis.
The proposal provides more jobs and investment for Hobart and the construction sector. The development will help to stimulate the economy.
The design of the building is fantastic. The stepped design is appropriate, reduces bulk and perceived size. The proposal includes great streetfront activation and occupies only a small parcel of land.
The scale of the building is appropriate upon what is an under used site. The height of the development looks to be appropriate given nearby by tall buildings.
Hobart needs this kind of sensible infill housing. The proposal is a perfect example of what inner city living should be. Will bring greater energy and life to the area.
The development would be beneficial for local businesses.

The development will reduce traffic and parking issues – reduces the demand for vehicles to get people to Hobart.
The development would be a positive investment in these uncertain times.
The developers will be sympathetic to the heritage area and the development will look and feel outstanding for the current environment.
The city will be greatly enhanced by this type of development.
<u>Against</u>
Negative impact on streetscape, townscape and existing low-rise urban form.
Height too high. Bulk and scale inappropriate and not compatible. Proposal does not respect the human scale of the area or acknowledge its surroundings. The proposal is greedy, lacks foresight and is simply for profit and short-term gain. It is a poor planning outcome.
Negative impacts on the skyline.
Removal of existing building has merit, but the proposed replacement is not the right development for the site.
Would set an unwanted precedent.
Building out of character and of poor design. Low budget, an eye-sore, ugly and generic. Not at all complementary, unique or different. Development may suit some mainland cities but not Hobart.
Building out of context with surrounding area and does not allow for transition in height between the Macquarie Ridge and Sullivans Cove. The proposal is not sensitive to or appropriate for its surroundings.
Negative impact on and domination of Heritage Precinct, heritage streetscape and surrounding heritage buildings. Destroys the charm of the city. The desirable values that make Hobart so appealing and popular, particularly with tourists, are being degraded by such proposals.
Impact on views to Kunanyi / Mt Wellington and visual amenity, particularly from St Davids Park.
Where not compliant with scheme standards the proposal does not meet performance criteria. The proposal fails to meet key scheme requirements.
In comparing the height of the proposal with disconnected taller buildings nearby, the proposal fails to justify the position of the development within a heritage precinct with a distinct streetscape character.
Traffic impacts on Davey Street where traffic congestion is already an issue.

A low rise development would be more appropriate.
Density of apartments is too high. Will produce substandard living conditions.
No need for more student accommodation of which there is currently an excess. The proposed use of the building is questionable. Need more low-income housing not this type of accommodation.
Proposal completely disregards the recent decision on the Welcome Stranger proposal.
Negative impacts on adjacent public spaces. Shadowing and wind tunnel concerns.
Negative impacts on the amenity of rooms within the adjacent St Helens hospital.
Impacts on adjoining businesses some of which rely on quiet environments and are highly noise sensitive. Noise impacts during construction would be problematic. Concerns about construction impacts and inconveniences upon adjacent properties.
Privacy impacts caused by overlooking from apartment windows.
The proposal is another Empress Towers, which is a bad outcome.
Council must stand up to such developments and developers who show such disregard for the local area and be serious about protecting the values of Hobart.
The Leigh Woolley plan for height limits should have been adopted by Council to prevent proposal's such as this.
Poor provision for bicycles and their users in the design of the building.
Decisions on development must not be rushed given current economic uncertainty and recovery from the pandemic.
Proposals should be made to comply with planning standards.

6. Assessment

- 6.1 The *Hobart Interim Planning Scheme 2015* is a performance based planning scheme. To meet an applicable standard, a proposal must demonstrate compliance with either an acceptable solution or a performance criterion. Where a proposal complies with a standard by relying on one or more performance criteria, the Council may approve or refuse the proposal on that basis. The ability to approve or refuse the proposal relates only to the performance criteria relied on.
- 6.2 The site is located within the Central Business Zone of the *Hobart Interim Planning Scheme 2015*.
- 6.3 The previously approved use is General Retail and Hire. The proposed use is

Residential (Multiple Dwelling and Communal Residence). The existing use is a No Permit Required use in the zone. The proposed use is a Permitted use in the zone, provided it is above ground floor level which is the case in this proposal.

6.4 The proposal has been assessed against:

6.4.1 Part D - 22 Central Business Zone

6.4.2 E5.0 Road and Railway Assets Code

6.4.3 E6.0 Parking and Access Code

6.4.4 E7.0 Stormwater Management Code

6.4.5 E13.0 Historic Heritage Code

6.5 The proposal relies on the following performance criteria to comply with the applicable standards:

6.5.1 D 22.0 Central Business Zone

Building Height - Part D 22.4.1 P1.1; P5
Setback - Part D 22.4.2 P1

6.5.2 E6.0 Parking and Access Code

Number of Car Parking Spaces - Central Business Zone - Part E 6.6.5 P1
Design of Vehicular Accesses 6.7.2 P1
Facilities for Commercial Vehicles 6.7.13 P1

6.5.3 E9.0 Attenuation Code

Development for Sensitive Use in Proximity to Use with Potential to cause Environmental Harm - Part E 9.7.2 P1

6.5.4 E13.0 Historic Heritage Code

Heritage Precinct - Demolition - Part E 13.8.1 P1
Heritage Precinct - Buildings and Works other than Demolition - Part E 13.8.2 P1
Places of Archaeological Potential - Building, Works and Demolition - Part E 13.10.1 P1

6.6 Each performance criterion is assessed below.

6.7 Building Height - Part D 22.4.1 P1.1

6.7.1 The acceptable solution A1 at clause Part D 22.4.1 requires building height for a new building within the Central Business Core Area for a site with a south-east facing frontage to be no more than 15m if on or within 15m of the frontage and 30m if set back more than 15m from a frontage.

6.7.2 The proposed building includes an initial height of 14.6m, which is maintained for 15m into the site before it rises to 30m in height, which continues for a further 15m into the site before the building rises to its maximum height of 35m. The height of the building then reduces to approximately 32m at the rear of the building, on the rear boundary line of the property. The proposed development exceeds the applicable acceptable solution but does not exceed the limitations of the amenity building envelope, which sets a maximum height of 45m, 30m into a site.

6.7.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.

6.7.4 The performance criterion P1.1 at clause Part D 22.4.1 provides as follows:

Development contained within the Amenity Building Envelope in Figure 22.3 must make a positive contribution to the streetscape and townscape, having regard to:

(a) the height, bulk and design of existing and proposed buildings;

(b) the need to minimise unreasonable impacts on the view lines and view cones in Figure 22.6 and on the landform horizons to kunanyi/ Mt Wellington and the Wellington Range from public spaces within the Central Business Zone and the Cove Floor;

(c) the need to minimise unreasonable impacts on pedestrian amenity from overshadowing of the public footpath for city blocks with frontage to a Solar Penetration Priority Street in Figure 22.2; and

(d) the need to minimise unreasonable impacts on the amenity of public open space from overshadowing.

- 6.7.5 The subject site and proposed development are not implicated by the areas defined in Figure 22.6 as a view line or view cones.

The site does not have frontage to a solar penetration priority street.

The proximity and orientation of the subject site/proposed building to St David's Park downhill to the east/north-east are such that overshadowing of this nearest area of public open space will not be problematic. Shadow diagrams have not been supplied with the application, however analysis of shadow cast by the proposed development utilising Council's City Model demonstrates that shadow cast by the proposed building would not reach the park. Figures 11 to 13 below demonstrate the shadow cast on June 21. It should be noted that the local topography, the presence of taller buildings further to the west/north-west over Macquarie Street and other larger buildings such as 1 Sandy Bay Road and 2 Heathfield Avenue further to the east would prevent the proposed building contributing any significant degree of additional shadow. In any case it is concluded that public open space will not be affected by shadow.



Figure 11: Shadow cast by the proposed development at 9am on June 21. (Source: HCC K2Vi model).



Figure 12: Shadow cast by the proposed development at 12pm on June 21. (Source: HCC K2Vi model).



Figure 13: Shadow cast by the proposed development at 3pm on June 21. (Source: HCC K2Vi model).

- 6.7.6 Part (a) of the performance criteria P1.1 therefore has the most relevance to the assessment of the proposal's height. The development must make a positive contribution to streetscape and townscape, having regard to the height, bulk and design of existing and proposed buildings. The assessment of the proposal's performance against this clause can be broken into two parts - how it performs in terms of contribution to streetscape; and how it performs in terms of contribution to townscape.

Streetscape

The proposed development exhibits a distinct response to the immediate streetscape in that its initial podium section scales well with existing

buildings to either side and within the surrounding block. It is noted from the application that the front part of the building has been designed in direct response to the facade scale and setback of adjacent heritage buildings. The front part of the building fills in the gap that currently exists given the existing building on the site has an irregular, larger front setback than adjoining and nearby buildings, and in doing so generates consistency of character and in turn a more positive contribution to the streetscape. Streetscape character is however not confined to the building's immediate appearance when viewed from street level immediately adjacent. So, whilst it may be possible to view the proposed building in relative isolation in direct context with the street from a vantage point close by, it must be viewed from further afield to appreciate its overall bulk and height and contribution to the streetscape, and therefore from this, the overall building must be considered in terms of contribution to streetscape.

The application includes the following representations of the proposed development within the Davey and Harrington Street elevations:

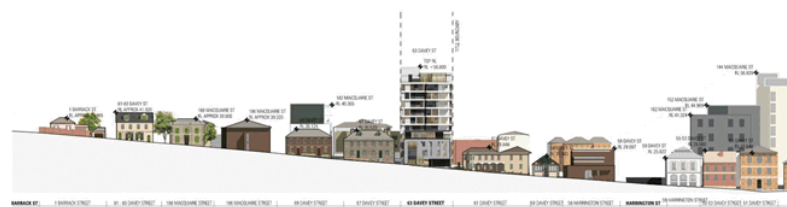


Figure 14: The proposed development in the context of Davey Street (Source: Irenelnc/JAWS).

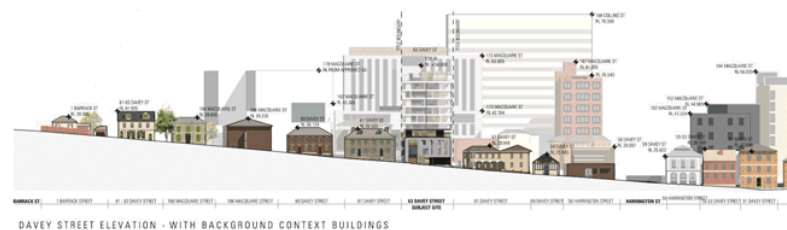


Figure 15: The proposed development in the context of Davey Street with taller buildings on the Macquarie ridge backdrop included (Source: Irenelnc/JAWS). Note that the backdrop building to the left at 179 Macquarie Street has been approved but does not currently exist.

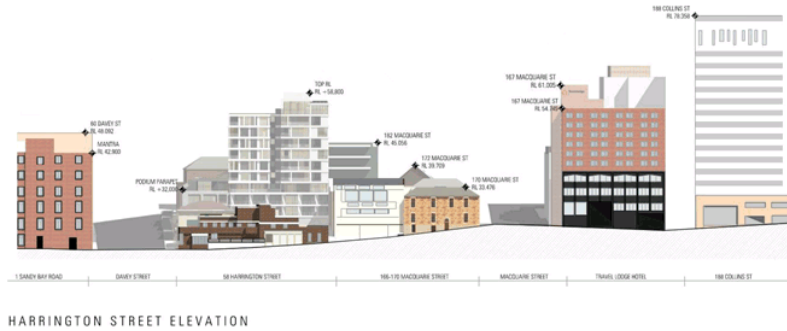


Figure 16: The proposed development in the context of Harrington Street (Source: IrenelInc/JAWS).

6.7.7 'Streetscape' is defined in the *Hobart Interim Planning Scheme 2015* as meaning:

'the visual quality of a street depicted by road width, street planting, characteristics and features, public utilities constructed within the road reserve, the setbacks of buildings and structures from the lot boundaries, the quality, scale, bulk and design of buildings and structures fronting the road reserve. For the purposes of determining streetscape with respect to a particular site, the above factors are relevant if within 100 m of the site.'

As pointed out in the submitted documentation, the area within 100m of the site takes in the immediate block between Harrington/Barrack Street and Macquarie/Davey Street, and also areas beyond this which include some taller buildings. Depictions of this taken from the application's analysis of surrounding buildings can be seen in Figures 17 and 18, below.

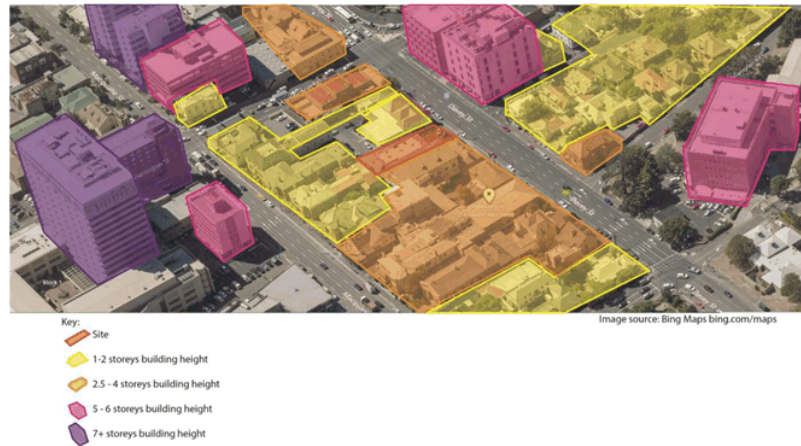


Figure 17: The application's depiction of building heights in urban context. The yellow covers buildings of 1-2 Storeys, the orange covers buildings of 2.5-4 Storeys. The subject site is highlighted red (Source: Irenelnc).

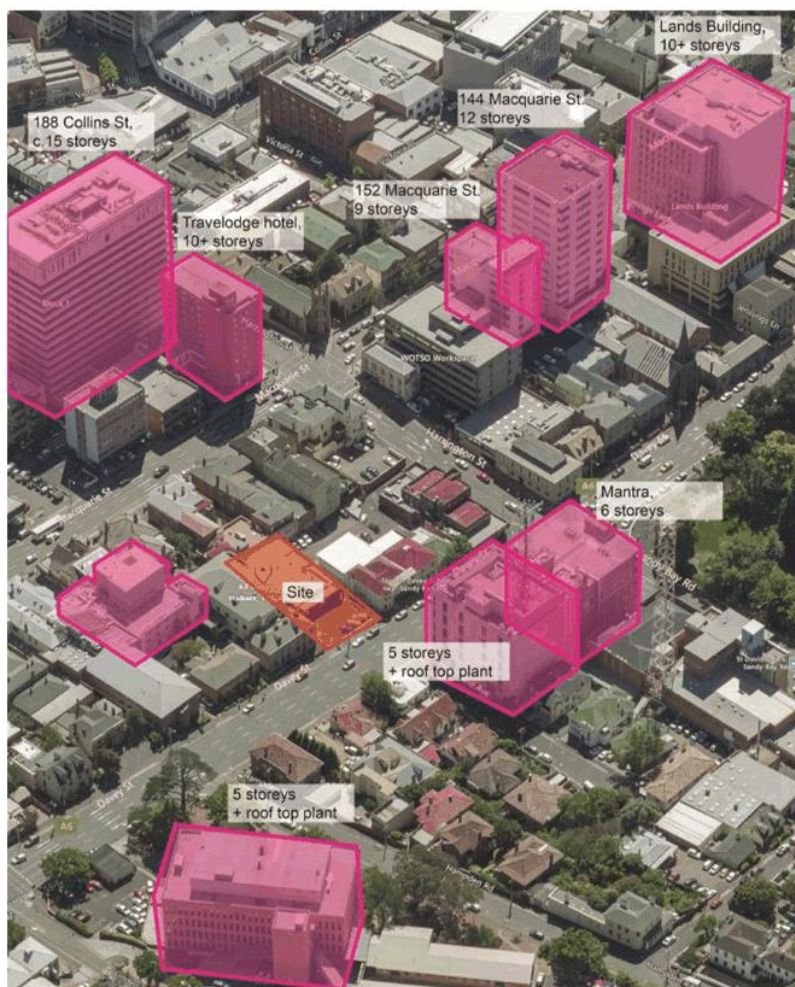


Figure 18: An additional depiction of surrounding building heights from the application (Source: Irenelnc).

- 6.7.8 The application identifies several taller buildings within 100m of the site including the Commonwealth Executive Building at 188 Collins Street over Macquarie Street, the Lands Building further down Macquarie Street, 1 Sandy Bay Road (Mantra) and the Telstra Exchange Building over and down Davey Street and well as the Repatriation Hospital further up and across Davey Street. However, it is arguable that the Executive Building, the Lands Building and the Repatriation Hospital are 'within' 100m of the site, as can be seen in Figure 16 below:



Figure 19: Analysis of properties within 100m radius (yellow outline) of the subject site (63 Davey Street title - highlighted red). The blue outlines represent the roofs of taller buildings not considered to be within the 100m radius. (Source: HCC Geocortex).

- 6.7.9 As can be seen in Figure 19 above, a number of the taller buildings identified in the application as contributing to the streetscape within 100m of the site aren't actually within 100m of the site. As such, using these buildings to consider streetscape, given its definition, is questionable. It is possible that they may contribute to wider townscape considerations however, which will be discussed further in due course.

For context, although a 100m radius extends beyond it, the local block within which the subject site is located, bounded by Davey and Macquarie Streets and Harrington and Barrack Streets has a distinct streetscape character that is worthy of detailed consideration. The block is occupied predominantly by older buildings with a typical scale of one to two storeys, with some taller examples up to four storeys.

The overall block has a low profile in the context of those closer to the centre of the city and this profile is generally consistent, following the slope of the block downwards from Barrack to Harrington Street and also from Macquarie to Davey Street. The single anomalous building within the block is a newer element of St Helen's Hospital which is more or less central within the block and rises above most other buildings at approximately 18m in height but with a relatively narrow cross-section. Most other buildings within the block exhibit a highly consistent, low rise form. Notably this block is covered entirely within a Heritage Precinct (H1) and contains numerous examples of heritage-listed properties, listed both with Hobart City Council and the Tasmanian Heritage Council.

The description of the prevailing Heritage Precinct, which extends across

additional city block further to the north-east, includes reference to it containing some of the most significant groups of early Colonial architecture in Australia with original external detailing, finishes and materials demonstrating a very high degree of integrity, distinctive and outstanding visual and streetscape qualities; as well the continuous two and three storey finely detailed buildings contribute to a uniformity of scale and quality of street space. Given other blocks within this precinct are not as uniform in terms of scale as the block in question, it can be considered that this particular block exhibits unique qualities given the uniformity of scale that remains and as such its streetscape is especially significant, readily identifiable and worthy of increased protection. In many respects therefore, because of the previous acknowledgement of these significant values and additional protections already applied to this block, streetscape as opposed to townscape is the more important consideration in terms of this proposal. Heritage impact considerations are discussed in greater detail elsewhere in this report.

The imagery above suggests that there is a clear disconnect between the subject site and taller buildings in the area. Within 100m of the subject site (63 Davey Street lot), there are a handful of taller buildings, including 1 Sandy Bay Road (Mantra), the Telstra Exchange Building at 2 Heathfield Avenue, the Travelodge and Ibis Hotels at 167-169 and 173 Macquarie Street respectively and the mixed commercial building further down at 156-162 Macquarie Street. These buildings are identified in the application as having between 5-6 storeys and 7+ storeys - The Travelodge and the Ibis are the two 7+ storey examples within 100m of the site. These buildings have heights of 36.9m and 39.7m respectively. The 5-6 storey buildings have heights of 26m, 28.9m and 19.73m.

Notably, none of these more prominent buildings are located within the street block surrounded by Harrington, Barrack, Macquarie and Davey Streets, which is the immediate block in which the subject site is located. As discussed above, it is possible to focus on this block as having the most relevance to streetscape in the context of this proposal, however simply put, in taking the character of buildings within 100m of the site into consideration, there are too few examples to confidently state that the streetscape character is overwhelmingly defined by taller buildings. Given none are immediately adjacent to the subject site, this highlights the individual prominence of the proposed building even further. The distinct prevailing lower scale character suggests that any new building should transition down to the lower buildings adjacent, not upwards to what are disconnected, taller ones further afield.

Townscape

The definition of townscape in the *Hobart Interim Planning Scheme 2015* is:

'the urban form of the city and the visual quality of its appearance, it includes the urban landscape and visual environment of the city. As a concept it strives to give order to the form of the city, the pattern of landscape and development of the urban landscape.'

Townscape is a broader concept to consider. This is essentially how a building fits when viewing an area at a more macro scale, wider than the 100m that is the focus for streetscape consideration. Consideration of townscape is to take into account the variety of heights and building forms that contribute to an area.

As previously mentioned there are more numerous examples of taller buildings if a wider view of the area is taken into account, including the Commonwealth Executive Building at 188 Collins Street (56.6m) and the Lands Building at 144 Macquarie Street (34m), however whilst this may be the case, there are still only a handful of taller buildings informing the local area around the subject site. It is not predominantly characterised by such development.

When viewed from certain vantage points, there is no doubt that the taller buildings in the nearby area, most notably on the Macquarie Ridge, provide a backdrop into which the proposed building would blend. The local topography may also assist when viewing from certain vantage points, however these vantage points are limited. When able to be read in context with the taller buildings nearby the subject site, taking into account local topography, on a broader, conceptual scale, the proposed building could be considered to be complementary. This might be achieved when viewing the building from elevated positions distant from the site, such as West Hobart or Sandy Bay, however this is considered to be neither an overriding factor, nor where the primary consideration of the impact of the building should be focused.

It is difficult to deny the fact that the proposed building would be the only example of such a building in the immediate block surrounding it, and the proposed building doesn't have the benefit of such scale context when viewed from all directions or from semi-close proximity. There are not always taller buildings to provide a backdrop, for instance on occasions when driving up Davey Street, or driving down Macquarie Street. Looking

up Davey Street for example, and as demonstrated in renders submitted with the application, the taller parts of the proposed building have nothing more than blue sky as a backdrop. There are not enough taller buildings immediately surrounding the site to confidently claim that the prevailing character of the local townscape can be defined by them. If some of the taller buildings were adjacent or even within the same block, it would be easier to conclude that is complementary in a practical, tangible sense. Before and after views of the proposed development taken from common public vantage points on Davey Street and Macquarie Street have been taken from Council's City Model and can be seen in Figures below. These views demonstrate some of the occasions when the proposed building makes a tangible change to the existing backdrop and highlight its individual prominence.

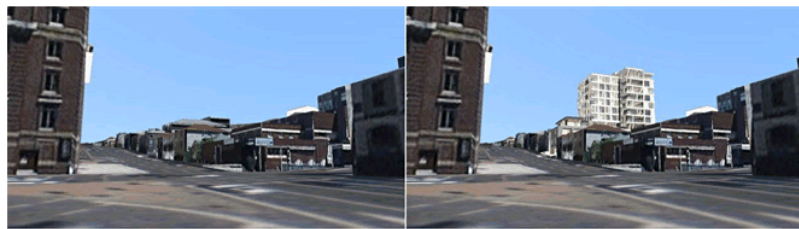


Figure 20: Before and after views towards the site/proposed development from the St David's Park corner on Davey Street and Sandy Bay Road. (Source: HCC K2Vi model).

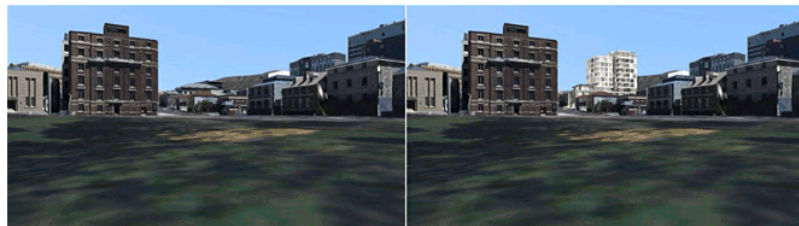


Figure 21: Noting the lack of trees in the model which would provide some relief, before and after views towards the site/development from within St David's Park close to Davey Street. (Source: HCC K2Vi model).



Figure 22: Noting the lack of trees in the model which would provide

some relief, before and after views towards the site/development from the corner of Salamanca Place and Davey Street, adjacent to the St David's Park corner. (Source: HCC K2Vi model).

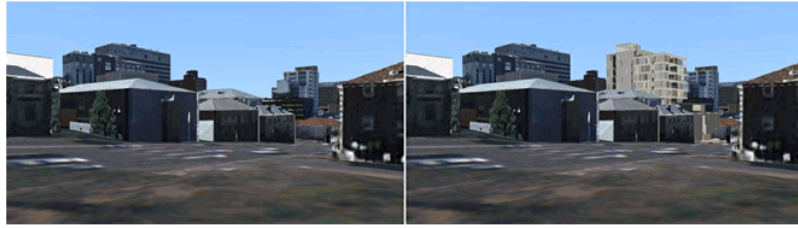


Figure 23: Before and after views towards the site/proposed development from the corner of Davey Street and Hampden Road. Note the presence of the taller buildings in the background allow for the proposed building to blend into the backdrop to some extent. (Source: HCC K2Vi model).

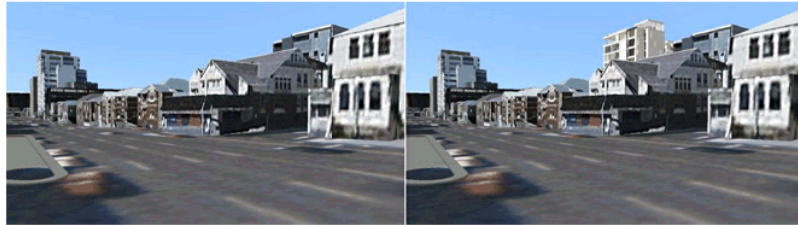


Figure 24: Before and after views towards the site/proposed development from Macquarie Street in front of the old Hutchins School building (181-183 Macquarie Street). Note the taller part of the St Helens Hospital appearing to provide some transition from this angle (Source: HCC K2Vi model).



Figure 25: Before and after views towards the site/proposed development from the northern side of the Macquarie Street/Harrington Street intersection. (Source: HCC K2Vi model).

- 6.7.10 The proposed building would change the form and character that the local block contributes to the wider townscape. This block represents a transition down and away from the centre of the city area, with the topography rising towards the south-east before levelling out. The block's

distinct character of lower buildings and consistent heights is representative of blocks continuing further to the south east up and along Davey Street. Taller, prominent buildings are not characteristic of the townscape in this direction. The proposed building would materially change this form, and in doing so would not positively contribute to the townscape. The lack of tall buildings in the local block is what makes it distinct in its form and character. Taller buildings need to be tucked closer to any that might currently exist in order to better provide for a non-prominent transition. To introduce such a variation where it cannot be anything but immediately prominent is hardly a positive outcome. Converse to the way that the proposed building can be considered to rectify the irregular front setback to Davey Street and therefore be seen as a positive outcome in terms of the immediate streetscape, the height of the proposed building does the opposite, creating an anomaly instead of rectifying one. The height, bulk and design of the building do not allow for consistency with the characteristics of the buildings immediately around it. If the proposed building was lower and not so individually prominent then there would be a greater argument for its consistency, appropriateness for the location and therefore the possibility of a positive contribution.

6.7.11 At its meeting to consider this application, the Council's Urban Design Advisory Panel commented on the various aspects of the proposal in terms of streetscape and townscape. There was broad support for the podium elevation on Davey Street and the Panel found that the massing, materials and height of this part of the proposed building to be well considered and made a positive contribution to the streetscape. However the principal concern of the Panel remained the overall height of the proposal, specifically with regard to impact on the values of the local Heritage Precinct, but categorically that the height of the tower elements would cause the proposal to be prominent in the townscape and streetscape, adversely impacting the qualities of the Heritage Precinct, especially the Davey Street streetscape and St Davids Park. The Panel concluded that objective (b) of Clause 22.4.1 (that a development does not unreasonably impact on historic heritage character) and P1.1(a) of the Scheme were not met.

6.7.12 The proposal does not comply with the performance criterion.

6.8 Building Height - Part D 22.4.1 P5

6.8.1 The acceptable solution A5 at clause Part D 22.4.1 requires building height of development within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot or

road, must (a) not exceed 1 storey or 4m (whichever is the lesser) higher than the facade building height of a heritage building on the same street frontage; and (b) not exceed the facade building height of the higher heritage building on the same street frontage if the development is between to heritage places; or (c) comply with the acceptable building height applicable to the site's frontage orientation, whichever is the lesser.

- 6.8.2 The proposed development within 15m of the frontage exceeds the two storey facade of the adjacent heritage building at 61 Davey Street by more than 4m or 1 storey.
- 6.8.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.
- 6.8.4 The performance criterion P5 at clause Part D 22.4.1 provides as follows:

Building height within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

(a) not unreasonably dominate existing buildings of cultural heritage significance; and
(b) not have a materially adverse impact on the historic cultural heritage significance of the heritage place;

(c) for city blocks with frontage to a Solar Penetration Priority Street in Figure 22.2, not exceed the Amenity Building Envelope illustrated in Figure 22.3, unless it can be demonstrated that the overshadowing of the public footpath on the opposite side of the Solar Penetration Priority Street does not unreasonably impact on pedestrian amenity.

- 6.8.5 The subject site does not have frontage to a Solar Penetration Priority Street.
- 6.8.6 The Council's Senior Cultural Heritage Officer has assessed this aspect of the proposal's impact upon the adjacent heritage buildings, and states that:

The heritage listed buildings in Davey Street are shown below. The discussion in relation to 22.4.1 P5 follows.



Adjacent heritage listed building at 61 Davey Street. Source:
Council image



Adjacent heritage listed building at 65 Davey Street. Source:
Council image

The adjacent heritage listed buildings have the following attributes:
simple uncomplicated, well mannered, restrained and modest
design, cohesive character and scale, symmetry or regular rhythm,

clear horizontal lines, and a fenestration pattern of traditional sash windows of similar proportions. In addition, they have narrow eaves and a simple roof form that has a practical purpose, but also offers an aesthetic function to delineate proportions and define the area between the walls and a pitched roof. Each heritage listed building is also solidly anchored to the ground.

65 Davey Street has two storeys with attic windows, 61 Davey Street has two storeys. This proposal is four storeys. There are no four storey buildings in this section of Davey Street. One of the characteristics in this block is that the buildings step down Davey Street in an orderly fashion and this can be seen in eaves line of each building and this is demonstrated in the applicant's documentation of the streetscape (see above). Even the recent infill to St Helen's Hospital, respects this pattern, and overall, the listed buildings exhibit a modulated height and rhythm that is rare in Hobart.

While the physical measurement of height difference might be considered minor, the new proposal has design features which contribute to the building having a taller perceived or apparent height, thus leading to it dominating and asserting itself within the existing streetscape.

In summary, the design features that give the proposal a greater perceived and less respectful height in this well mannered streetscape are as follows:

- The proposal is approximately 8.4 metres above the eaves line of the adjacent heritage listed property at 61 Davey Street.
- The proposal has three levels of square, sharp edged and contemporary lines which contrast with the subtle modulated elevations of the adjacent heritage listed buildings to create a more prominent and monolithic form.
- The projecting solid eaves of the darker 'mini penthouse' is a contemporary form that is heavier than any roof form of the heritage listed places adding to the height and heaviness of the four storey form.
- The proposal has a deep undercroft at ground level for vehicular and pedestrian access which results in the street façade being elevated above the ground and appearing higher than it actually is.
- The large vertical window configuration over two floors provide a

verticality to the three storey portion that gives the building even great height.



BEFORE: Davy Street



AFTER: Proposed

The above image demonstrates an obvious change in building height in the historic streetscape. Source: Applicant's supporting documentation

In addition, the four storey element including the dark 'mini penthouse' on top will obscure the roof scape including chimneys of adjacent buildings. A close look at the applicant's submitted

documentation demonstrates how much taller in the streetscape it will appear.



The above image demonstrates an obvious change in the historic streetscape and shows the real and perceived height of the front four storey element . Source: Applicant's supporting documentation

Not only is it higher than adjacent buildings, but the design of the proposal will result in it appearing even higher and more out of scale and proportion, 'stealing the thunder' of existing heritage listed buildings. It projects further into the streetscape and asserts itself, making its presence felt in all directions. It is sharper and of a form that is more prominent, flamboyant and 'monolithic' than the adjacent polite heritage listed buildings such that it will detract from and be more prominent than the heritage listed buildings. This has an unreasonable impact on the historic heritage character of heritage places such that they are obscured, appear dominated and lesser in scale.

A building that was two or two and half storeys high would be a more appropriate response where the heritage and streetscape values are the most significant in Hobart.

It is concluded that the proposal unreasonably dominates the adjacent buildings by virtue of the height of the lower element that is four storeys high and has a materially adverse impact on the restrained heritage qualities of the adjacent places through its height different design, form, fenestration pattern vertical facade treatment and alternative roof form by upstaging the adjacent buildings. The proposal does not satisfy 22.4.1 P5.

6.8.7 The Senior Cultural Heritage Officer's report is provided as an attachment

to this report.

6.8.8 The proposal does not comply with the performance criterion.

6.9 Setback - Part D 22.4.2 P1

6.9.1 The acceptable solution A1 at clause Part D 22.4.2 requires building setback to be parallel to a frontage and to be no more than 0m.

6.9.2 The proposal includes the front facade of proposed building's initial podium element above ground floor level having a front setback ranging between 2.4m at its north-eastern end to 2.9m at its south-western end. At ground level a 6m wide driveway access is located to the right hand side under the overhang of the building above, whilst to the left is the pedestrian access to the site, its lobby and reception, in front of which is an external entry forecourt accessed via a small number of stairs, with retained planters to either side.

6.9.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.

6.9.4 The performance criterion P1 at clause Part D 22.4.2 provides as follows:

Building setback from frontage must satisfy all of the following:

(a) be consistent with any Desired Future Character Statements provided for the area;

(b) be compatible with the setback of adjoining buildings, generally maintaining a continuous building line if evident in the streetscape;

(c) enhance the characteristics of the site, adjoining lots and the streetscape;

(d) provide for small variations in building alignment only where appropriate to break up long building facades, provided that no potential concealment or entrapment opportunity is created;

(e) provide for large variations in building alignment only where appropriate to provide for a forecourt for space for public use, such as outdoor dining or landscaping, provided that no potential concealment or entrapment opportunity is created and the forecourt is afforded very good passive surveillance.

- 6.9.5 The proposed development demonstrates a degree of consistency towards the frontage setbacks of existing buildings to either side on Davey Street, particularly those to the uphill side which have uniform frontage setbacks of a similar distance, whilst the adjacent building to the downhill side is set closer to its Davey street frontage. When considered in context with the existing building on the subject site, which maintains a highly atypical frontage setback of approximately 13m, the proposed development is much more consistent and therefore in keeping with the setback character in the immediate area on this side of Davey Street.

Desired Future Character Statements for the Central Business Zone address the siting, bulk and design of buildings, and where most relevant to setback refer to the need to reinforce streetscape pattern and consistency in building edges and height at the street wall.

In filling out and occupying the majority of the space left at the front of the site by the existing building, the proposal rectifies what is an irregular gap in the street edge and local streetscape character on this side of Davey Street. The proposed building replaces an unsightly carparking area with a more hardened street edge, promoting the street wall for its initial podium section and incorporating hard edges at ground level with planters on either side of the main access which is characteristic of properties to either side. Examples of small areas of landscaping within the immediate local streetscape allow for appropriate tree species and low lying plants to provide some softening of built form and promotion of greenery up and down both sides of Davey Street. The proposal suggests an intent to further promote this well established theme with the inclusion of planters and trees such as pines or conifers, examples of which can be seen in front of buildings to either side.

The alignment of the building to the frontage at ground level is not suggestive of any unreasonable inclusion of concealment spaces or or entrapment opportunities. All areas at the front of the site can be reasonably surveilled from the street or footpath.

Where the larger variations to the building alignment occur (vehicle access into building, entry forecourt) these variations are not considered to be significant, and in terms of the driveway access, this element of the development is encompassed by the overhang of the first floor of the building above.

- 6.9.6 At its meeting to consider the proposal the Urban Design Advisory Panel

considered that at ground level the extent of landscaping could be more substantial and that the use of quality materials (especially paving) must be extended to the full frontage of the site including the driveway and service areas. On the question of landscaping generally, the Panel felt that there remained a lack of detail and any approval should include appropriate conditions regarding the engagement of a landscape architect and the submission of detailed landscaping plans for approval.

- 6.9.7 The proposal complies with the performance criterion. The inclusion of conditions as recommended by the Urban Design Advisory Panel is considered appropriate.

6.10 Number of Car Parking Spaces - Central Business Zone - Part E 6.6.5 P1

- 6.10.1 The acceptable solution A1 at clause Part E 6.6.5 requires (a) no on-site parking to be provided; or (c) on-site parking to be provided at a maximum rate of 1 space per dwelling for residential uses.
- 6.10.2 The proposal includes 42 residential parking spaces catering for the 30 residential apartments, exceeding the acceptable requirement.
- 6.10.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.
- 6.10.4 The performance criterion P1 at clause Part E 6.6.5 provides as follows:

Car parking provision:

(a) is in the form of a public car parking station provided as part of a development which utilises a major existing access; or

(b) must not compromise any of the following:

- (i) pedestrian safety, amenity or convenience;*
- (ii) the enjoyment of 'al fresco' dining or other outdoor activity;*
- (iii) air quality and environmental health;*
- (iv) traffic safety.*

- 6.10.5 The Council's Senior Development Engineer provides the following assessment of the proposal against this clause:

(i) pedestrian safety, amenity or convenience

- The proposed singular access for 42 car-parking spaces causes a concentration of vehicle movements across the footpath.
- The sight distance to pedestrians does not comply with the Australian

Standard due to the height of the proposed boundary fence exceeding 1.2m.

- Pedestrian safety and convenient use of the footpath will therefore be compromised. A condition is recommended for maximum boundary fence height.

(ii) the enjoyment of 'al fresco' dining or other outdoor activity

- None near the proposed development.

(iii) air quality and environmental

- This is not compromised (beyond what is typically accepted for a multi-storey apartment complex).

(iv) traffic safety.

- There are kerb-side parking spaces adjacent to the access that inhibit the sight distance to vehicles on Davey Street.
- Davey Street is heavily trafficked, particularly during peak hours.
- The gradient of Davey Street is in a favourable direction and improves the sight distance.
- Traffic safety is not compromised beyond what is typical for an access servicing a multi-storey apartment complex.
- The design has been assessed by a consulting traffic engineer and has been found to be acceptable (refer TIA).

6.10.6 The officer's report is provided as an attachment to this report.

6.10.7 The proposal complies with the performance criterion.

6.11 Design of Vehicle Accesses 6.7.2 P1

6.11.1 The acceptable solution A1 at clause Part E 6.7.2 requires the design of vehicular accesses to meet the relevant Australian Standard.

6.11.2 The proposal includes new access arrangements that do not meet the relevant Australian Standard.

6.11.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.

6.11.4 The performance criterion P1 at clause Part E 6.7.2 provides as follows:

Design of vehicle access points must be safe, efficient and convenient, having regard to all of the following:

- (a) avoidance of conflicts between users including vehicles, cyclists and pedestrians;*
- (b) avoidance of unreasonable interference with the flow of traffic on adjoining roads;*
- (c) suitability for the type and volume of traffic likely to be generated by the use or development;*
- (d) ease of accessibility and recognition for users.*

6.11.5 The Council's Senior Development Engineer provides the following assessment of the proposal against this clause:

- (a) avoidance of conflicts between users including vehicles, cyclists and pedestrians;*

Vehicles and Cyclists:

- There are kerb-side parking spaces adjacent to the access that inhibit the sight distance to vehicles on Davey Street.
- Davey Street is heavily trafficked, particularly during peak hours.
- The gradient of Davey Street is in a favourable direction and improves the sight distance.
- Traffic safety is not compromised beyond what is typical for an access servicing a multi-storey apartment complex.
- The design has been assessed by a consulting traffic engineer and has been found to be acceptable (refer TIA).

Pedestrians:

- The proposed singular access for 42 car-parking spaces causes a concentration of vehicle movements across the footpath.
- The sight distance to pedestrians does not comply with the Australian Standard due to the height of the proposed boundary fence exceeding 1.2m.
- Pedestrian safety and convenient use of the footpath will therefore be compromised. A condition is recommended for maximum boundary fence height.

- (b) avoidance of unreasonable interference with the flow of traffic on adjoining roads;*

- Assessed by a consulting traffic engineer and found to be acceptable (refer TIA).

- (c) suitability for the type and volume of traffic likely to be generated by the use or development;*

- The width and gradient of the access is acceptable for servicing the 42 parking spaces proposed.

- Assessed by a consulting traffic engineer and found to be acceptable (refer TIA).

(d) *ease of accessibility and recognition for users.*

- The location of the access will permit easy use.
- The access is consistent with surrounding properties and as such ease of recognition is acceptable.

6.11.6 The officer's report is provided as an attachment to this report.

6.11.7 The proposal complies with the performance criterion.

6.12 Facilities for Commercial Vehicles 6.7.13 P1

6.12.1 The acceptable solution A1 at clause Part E 6.7.13 requires commercial vehicle facilities to be provided on site in accordance with the relevant Australian Standard.

6.12.2 The proposal does not provide commercial vehicle facilities on site in accordance with the relevant Australian Standard.

6.12.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.

6.12.4 The performance criterion P1 at clause Part E 6.7.13 provides as follows:

Commercial vehicle arrangements for loading, unloading or manoeuvring must not compromise the safety and convenience of vehicular traffic, cyclists, pedestrians and other road users.

6.12.5 The Council's Senior Development Engineer provides the following assessment of the proposal against this clause:

- The traffic engineering consultant has advised that private collection will be undertaken from the kerb-side, and that the associated risk and interruption to convenience is tolerable
- The Department of State Growth has endorsed the TIA with proposed private collection from the carriageway
- On this basis, the proposed commercial vehicle arrangements (i.e. waste collection) can be supported. A condition is recommended for a waste management plan.

6.12.6 The officer's report is provided as an attachment to this report.

6.12.7 The proposal complies with the performance criterion.

6.13 Development for Sensitive Use in Proximity to Use with Potential to cause Environmental Harm - Part E 9.7.2 P1

6.13.1 There is no acceptable solution for new sensitive (residential) uses located in close proximity (200m) of a use with the potential to cause environmental harm.

6.13.2 The proposal includes new residential use within 200m of a live music venue.

6.13.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.

6.13.4 The performance criterion P1 at clause Part E 9.7.2 provides as follows:

Development for sensitive use, including subdivision of lots within a sensitive zone, must not result in potential to be impacted by environmental harm from use with potential to cause environmental harm, having regard to all of the following:

(a) the nature of the use with potential to cause environmental harm; including:

(i) operational characteristics;

(ii) scale and intensity;

(iii) degree of hazard or pollution that may emitted from the activity;

(b) the degree of encroachment by the sensitive use into the Attenuation Area or the attenuation distance;

(c) measures in the design, layout and construction of the development for the sensitive use to eliminate, mitigate or manage effects of emissions.

6.13.5 The Council's Environmental Development Planner provides the following assessment of the proposal against this clause:

The Duke Hotel has live music, a function room and an outdoor area, operating most nights and sometimes after midnight. The main source from the venue would be patron noise from the outdoor area.

The attenuation distance for the music venue is 200m under the Code, and the proposed residential development would be a minimum of 112m from the music venue.

The proposed apartment building would be of solid construction, with most of the proposed apartments being on the far side of the building away from the music venue, and no decks are proposed facing the venue.

Given the separation distance, building design, topography, buildings between the two sites and the relatively high ambient noise levels in the area, noise nuisance to the residents of the proposed apartments from The Duke Hotel is not considered a credible risk. The exercise of discretion is recommended.

A construction environmental management plan condition is also recommended.

6.13.6 The proposal complies with the performance criterion.

6.14 Heritage Precinct - Demolition - Part E 13.8.1 P1

6.14.1 There is no acceptable solution for demolition within a Heritage Precinct.

6.14.2 The proposal includes demolition of the existing building and associated elements on the site to make way for the proposed development within Heritage Precinct H1.

6.14.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.

6.14.4 The performance criterion P1 at clause provides as follows:

Demolition must not result in the loss of any of the following:

- (a) buildings or works that contribute to the historic cultural heritage significance of the precinct;*
- (b) fabric or landscape elements, including plants, trees, fences, paths, outbuildings and other items, that contribute to the historic cultural heritage significance of the precinct;*
unless all of the following apply;
 - (i) there are, environmental, social, economic or safety reasons of*

greater value to the community than the historic cultural heritage values of the place;

(ii) there are no prudent or feasible alternatives;

(iii) opportunity is created for a replacement building that will be more complementary to the heritage values of the precinct.

- 6.14.5 The Council's Senior Cultural Heritage Officer provides the following assessment of the proposal against this clause:

The building of 63 Davey Street, is of a scale and siting that results in it being subservient to and sits recessively in this highly important streetscape. However, it dates to 1979 and has a carpark to the front and little architectural merit and it does not make a positive contribution to the stated historic cultural heritage significance of the precinct. In this instance (a) and (b) of E13.8.1 P1 must be satisfied prior to meeting sub-clauses (i), (ii) and (iii). For the reasons outlined above it is concluded that clause E13.8.1 P1 is satisfied.

- 6.14.6 The officer's report is provided as an attachment to this report.

- 6.14.7 The proposal complies with the performance criterion.

6.15 Heritage Precinct - Buildings and Works other than Demolition - Part E 13.8.2 P1

- 6.15.1 There is no acceptable solution for buildings and works within a Heritage Precinct.

- 6.15.2 The proposal includes a new building and associated works within Heritage Precinct H1.

- 6.15.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.

- 6.15.4 The performance criterion at clause Part E 13.10.1 P1 provides as follows:

Design and siting of buildings and works must not result in detriment to the historic cultural heritage significance of the precinct, as listed in Table E13.2.

- 6.15.5 The Council's Senior Cultural Heritage Officer provides the following

assessment of the proposal against this clause:

Assessment of this proposal must consider the building as a whole within the Heritage Precinct.

A Heritage Precinct is defined in E13.3.1 Definition of Terms in the Historic Heritage Code as:

"means an area shown on the planning scheme maps as a heritage precinct and described in Table E13.2 as having particular historic cultural heritage significance because of the collective heritage value of individual places as a group for their streetscape or townscape values."

Streetscape is defined in 4.1 of the Scheme as:

"means the visual quality of a street depicted by road width, street planting, characteristics and features, public utilities constructed within the road reserve, the setbacks of buildings and structures from the lot boundaries, the quality, scale, bulk and design of buildings and structures fronting the road reserve.

For the purposes of determining streetscape with respect to a particular site, the above factors are relevant if within 100 m of the site."



Part of the streetscape on Davey Street with the subject site in the

centre. Source: Council image

For the purposes of assessing this proposed building against E13.8.2 P1 it is: a tiled/stone square facade element fronting Davey Street, that has three floors, a separate apartment element with a darker horizontal overhanging roof form, set back and in, that forms a street front element of four stories, a higher tower 15 metres back from the street frontage with a 'penthouse' and service structure on top. Overall, the building has a height of approximately 36 metres above the ground level at the street frontage, and a RL of 58.8. The proposal is shown below. The taller 'greyed out' buildings behind (eg Commonwealth Centre and Ibis Hotel) are not relevant in the consideration of clause E13.8.2 P1 as they are outside the Heritage Precinct and not in the streetscape as defined. The tower component of the proposed building is also 'greyed out' but this must not be misconstrued as 'being in the background' or outside the Heritage Precinct and therefore not part of this proposal.

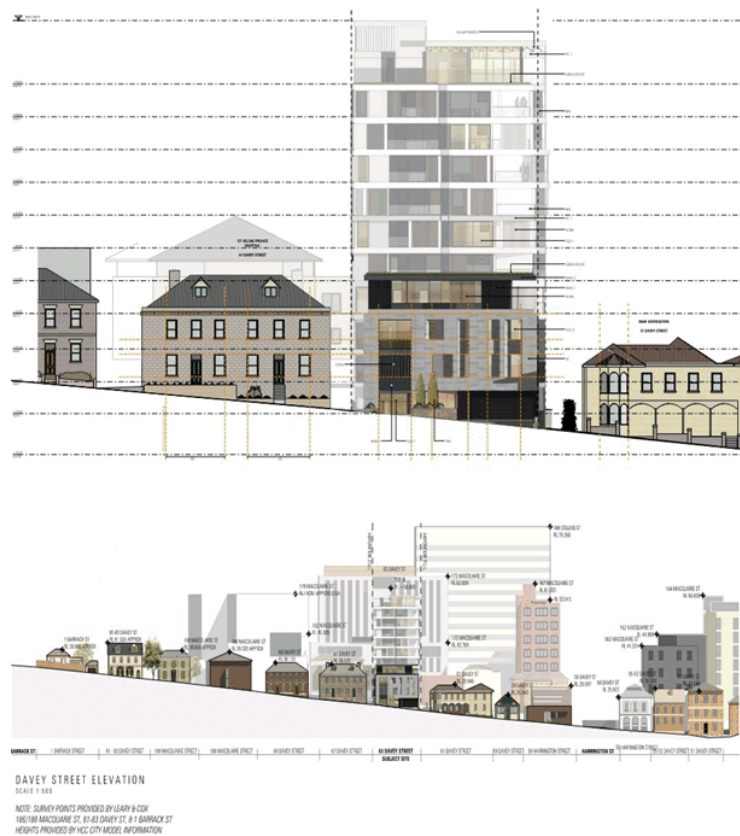


Image: The subject site is in the centre. The 11 storey building

behind is shown as 'greyed out' and the buildings outside the Heritage Precinct in Macquarie Street are also shown as 'grey out' which could be misconstrued as an existing building or not part of this proposal. Source: Screenshot from applicant's documentation.

In consideration of clause E13.8.2 P1, detriment means "damage or loss to such value or thing". This is stated in *Hexa Pacific Pty Ltd v Hobart City Council and Ors [2020] TASRMPAT 1* at [83].

Comparisons with the Welcome Stranger at 58 Harrington Street proposal and subsequent Tribunal decision must be drawn carefully. The sites are near (a heritage listed building separates them) and both are located in the same Heritage Precinct. The Welcome Stranger proposal differed by having two tower components of 10 floors and 13 floors and was located on a corner site. This proposal has a tower component of 11 floors.

However, in relation to that decision and clause E13.8.2 P1, the Tribunal stated "The Proposal is to be located within an area of the Precinct where the streetscape largely comprises buildings that fall within the description set out in Statement 3 for the Heritage Precinct. In the Tribunal's view, the Proposal whilst comprising elements of different heights and setbacks, includes two tower elements which introduce a development scale so at odds in the location with the identified statements of significance (and in particular Statement 3), and would result in the Heritage Precinct as a whole being detrimentally impacted." *Hexa Pacific Pty Ltd v Hobart City Council and Ors [2020] TASRMPAT 1* at [92].

The current proposal is, based on height measurement, approximately 4.3 metres lower than the Welcome Stranger proposal when the measurement is taken from the street ground level. The subject site is up the hill from the Welcome Stranger site with the subject site dropping down Davey Street approximately 1.6 metres across the street frontage. The RL of the top of the Welcome Stranger was 63.00, while in comparison the RL at the highest point for the current proposal is 58.80.

This proposal differs from the Welcome Stranger proposal in that it is 11 floors high (including the ground floor) and measures approximately 36 metres above the ground level at the street frontage. The following image shows the proposal inserted into the streetscape.



BEFORE: Davey Street



The above image demonstrates an obvious change in the historic streetscape. Source: Applicant's supporting documentation

In summary the proposal is of a scale that is at odds with the streetscape that is within an area of the precinct that largely comprises buildings that fall within the description within the statements of significance of '*continuous two to three storey finely detailed buildings*'. That is, within the block of Davey, Barrack, Macquarie and Harrington Street the buildings of the precinct are characterised by one, two and three storey buildings around the edge with the maximum height of any building to the rear is 5 storeys. Where the uniformity of streetscape and scale is so central

to the heritage values of this block, a building that is taller by the extent proposed cannot enhance the heritage values because it will be out of scale and context with its surroundings. In this respect, there is detriment to an element of a wider precinct and therefore there will be detriment in this case to the precinct values as a whole. The proposal does not satisfy E13.8.2 P1.

6.15.6 The officer's report is provided as an attachment to this report.

6.15.7 The proposal does not comply with the performance criterion.

6.16 Places of Archaeological Potential - Building, Works and Demolition - Part E
13.10.1 P1

6.16.1 The acceptable solution A1 at clause Part E 13.10.1 requires buildings and works to not involve excavation or ground disturbance.

6.16.2 The proposal includes demolition of the existing building and excavation for the proposed new development.

6.16.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.

6.16.4 The performance criterion P1 at clause Part E 13.10.1 provides as follows:

Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:

(a) the nature of the archaeological evidence, either known or predicted;

(b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;

(c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition;

(d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;

(e) measures proposed to preserve significant archaeological evidence 'in situ'.

- 6.16.5 The Council's Senior Cultural Heritage Officer provides the following assessment of the proposal against this clause:

The Praxis Environment report identifies an area of high archaeological potential and this is denoted in an area of red in figure 7.1 (p.48) of the Praxis report. It is acknowledged that the disturbance history may be greater than observations and historical records of the site, however, the report suggests taking a cautious approach and that structural remains associated with the c.1830 dwelling and outbuilding relating to the potential remains of the domestic occupation of the site. The report recommends that "Any excavation proposed in areas of high archaeological potential must be preceded by an archaeological impact assessment, and if necessary an archaeological method statement, which details measures to be taken to avoid or mitigate impact upon the archaeological resource. That method statement must be in accordance with industry standard (e.g. the Tasmanian Heritage Council's Practice Note 2 – Managing Historical Archaeological Significance in the Works Application Process) and implemented in the works process." This can be achieved by a condition of permit and as such the proposal can satisfy E13.10.1 P1.

- 6.16.6 The officer's report is provided as an attachment to this report.

- 6.16.7 The proposal complies with the performance criterion.

7. Discussion

- 7.1 Planning approval is sought for Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Infrastructure and Access Works, at 63 Davey Street, 186 Macquarie Street, and the adjacent Davey Street road reservation.

- 7.2 The application was advertised and received 393 representations. The representations raised concerns including the proposal's lack of compliance with scheme standards and lack of justification; its incompatibility with and impacts upon the surrounding area and heritage precinct; its impacts upon local amenity, traffic and adjacent properties and uses; the poor quality of the development; the lack of need for the accommodation being proposed; and the negative impacts the proposal would have on Hobart's character.

Twenty seven representations were in favour of the proposal, citing its provision of desperately needed housing; the provision of jobs and stimulation of the economy and construction sector; the good design and appropriate height and scale of the building; the proposal representing positive investment in uncertain times; and positive impacts on local businesses, city parking and Hobart in general.

- 7.3 The proposal has been assessed against the relevant provisions of the planning scheme and is considered to not perform well.
- 7.4 The proposal has been assessed by other Council officers, including the Council's Development Engineer, Roads, Traffic and Environmental Engineers, Environmental Development Planner and Senior Cultural Heritage Officer. The Senior Cultural Heritage Officer has recommended the proposal be refused on heritage grounds. The other officers are supportive of the proposal subject to conditions. The proposal was also referred to the Tasmanian Heritage Council due to the property at 186 Macquarie Street being included in the overall development site. The Tasmanian Heritage Council's Regional Heritage Advisor was satisfied that the minimal level of works on the listed site was such that should the application be approved advice should be included on the permit to ensure heritage approval is gained before any works, excavation, ground disturbance or other heritage works occurs on the listed site.
- 7.5 The proposal is recommended for refusal.

8. Conclusion

- 8.1 The proposed Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Infrastructure and Access Works at 63 Davey Street and 186 Macquarie Street, and Adjacent Road Reserve, HOBART does not satisfy the relevant provisions of the *Hobart Interim Planning Scheme 2015*, and as such is recommended for refusal.

9. Recommendations

That: Pursuant to the *Hobart Interim Planning Scheme 2015*, the Council refuse the application for Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Infrastructure and Access Works at 63 Davey Street and 186 Macquarie Street, and Adjacent Road Reserve, HOBART for the following reasons:

- 1 The proposal does not meet the acceptable solution or the performance criterion with respect to clause Part D 22.4.1 A1 and P1.1(a) of the Hobart Interim Planning Scheme 2015 because the development does not make a positive contribution to the streetscape and townscape, having regard to the height, bulk and design of existing and proposed buildings.
- 2 The proposal does not meet the acceptable solution or the performance criterion with respect to clause E13.8.2 P1 of the Historic Heritage Code of the Hobart Interim Planning Scheme 2015 because the proposal results in detriment to the historic cultural heritage significance of the precinct through its design and siting.
- 3 The proposal does not meet the acceptable solution or the performance criterion with respect to clause 22.4.1 P5 of the Historic Heritage Code of the Hobart Interim Planning Scheme 2015 because the proposed building unreasonably dominates and has a materially adverse impact on adjacent existing buildings of cultural heritage significance through its height.



(Cameron Sherriff)

Development Appraisal Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.



(Ben Ikin)

Senior Statutory Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

Date of Report: 6 October 2020

Attachment(s):

Attachment B - CPC Agenda Documents

Attachment C - Planning Referral Officer Cultural Heritage Report

Attachment D - Planning Referral Officer Senior Development Engineer

Attachment E - Urban Design Advisory Panel Minutes



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NOTE: INFORMATION OF OTHER SERVICES INDICATED ON PLAN ARE INDICATIVE ONLY FOR GENERAL CONSIDERATION. FINAL LOCATION OF ALL UTILITIES AND SERVICES ARE TO BE COORDINATED AND VERIFIED BY THE CONTRACTOR ON SITE.

DRAWING LEGEND

Drawing No:	Description
DA01	CONTEXT PLAN
DA02	CONTEXT SITE SECTION
DA03	SITE PLAN/ROOF PLAN
DA04	FLOOR PLANS - BASEMENT & GROUND FLOOR
DA05	FLOOR PLANS - LEVEL 1 & 2
DA06	FLOOR PLANS - LEVEL 3 & 4
DA07	FLOOR PLANS - LEVEL 10
DA08	ELEVATION - 1
DA09	ELEVATION - 2
DA10	ELEVATION - 3
DA11	ELEVATION - 4
DA12	SECTIONS - 1
DA13	30 SITE VIEWS
DA14	30 SITE VIEWS
DA15	30 SITE VIEWS
DA16	30 SITE VIEWS
DA17	30 SITE VIEWS
DA18	30 SITE VIEWS
DA19	30 SITE VIEWS
DA20	30 SITE VIEWS
DA21	30 CONTEXT HEIGHT STUDY

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PROJECT

DAVEY ST APARTMENTS
61 Davey Street, Melbourne VIC 3000
For
Telford-Knox Unit Trust

DRAWING

CONTEXT PLAN

1509_DAO1 |

SCALE
DA01
DA02
DA03
DA04
DA05
DA06
DA07
DA08
DA09
DA10
DA11
DA12
DA13
DA14
DA15
DA16
DA17
DA18
DA19
DA20
DA21

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2019
02	REVISED HEIGHT	26/03/2020
03	REVISED HEIGHT FOR DA	14/05/2020
04	TASKS/REVISION TO PREP PLANTER	12/06/2020
05	EXTENDED DAVEY STREET ELEVATION	14/06/2020

DEVELOPMENT APPLICATION

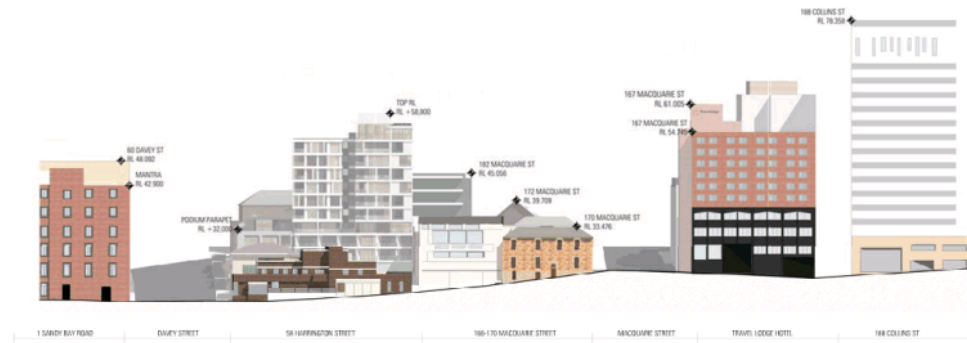


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NOTE:
INFORMATION OF OTHER SERVICES INDICATED ON PLANS ARE INDICATIVE ONLY FOR GENERAL COORDINATION.

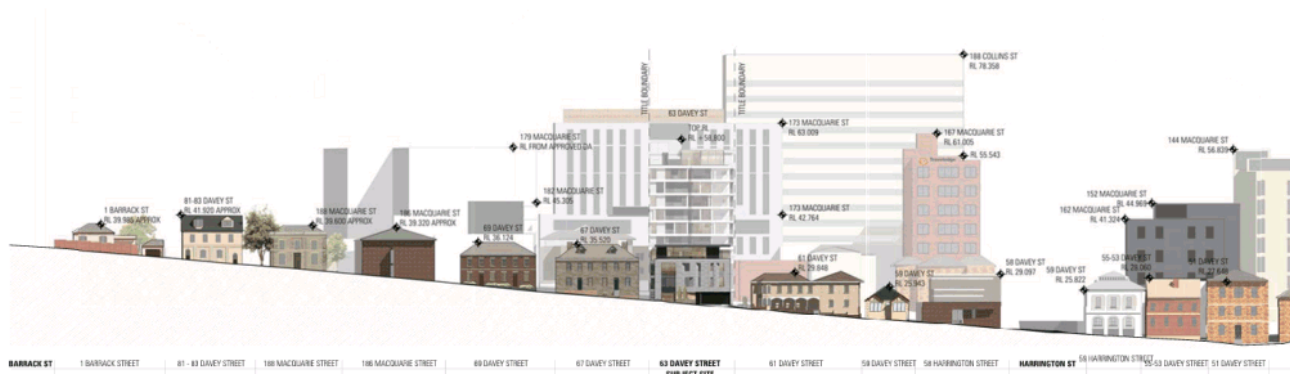
FINAL LOCATION OF ALL SERVICES INDICATED ARE TO BE COORDINATED AND VERIFIED BY THE CONTRACTOR (OWNER).

DRAWING LEGEND



HARRINGTON STREET ELEVATION
SCALE 1:500

NOTE: SURVEY POINTS PROVIDED BY LEARY & COX



DAVEY STREET ELEVATION
SCALE 1:500

NOTE: SURVEY POINTS PROVIDED BY LEARY & COX
186/188 MACQUARIE ST, 81-83 DAVEY ST, & 1 BARRACK ST
HEIGHTS PROVIDED BY HCC CITY MODEL INFORMATION

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PROJECT

DAVEY ST APARTMENTS
61 Davey Street Market 186-188
For
Tollways Kiosk Unit Trust

DRAWING

CONTEXT SITE SECTION

1509_DAB2 |

DATE: 05/04/2018
SCALE: 1:500
DRAWN: JACOB KLEIN
CHECKED: JACOB KLEIN
APPROVED: JACOB KLEIN
PROJECT: 1509_DAB2

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2018
02	REVISED HEIGHT	28/05/2018
03	REVISED HEIGHT FOR DA	14/05/2018
04	TAKEN INTO REVISION TO FRONT PLANTER	15/06/2018
05	EXTENDED DAVEY STREET ELEVATION	16/06/2018

DEVELOPMENT APPLICATION





SITE PLAN/ ROOF PLAN 1:200
1:200

DEVELOPMENT APPLICATION

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PROJECT

DAVEY ST APARTMENTS
61 Davey Street Sydney NSW 2000
For
Telford-Knox Unit Trust

DRAWING

SITE PLAN/ ROOF PLAN

1509_DAB3 |

SCALE
DATE
DRAWN
CHECKED

ACCREDITED DESIGNER
ACCREDITED NUMBER
PLAT DATE
CADD BY

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2019
01	REVISED HEIGHT	26/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
03	TAGWATER REVISION TO FRONT PLANTER	12/06/2020
03	TAGWATER REVISION TO FRONT PLANTER	10/06/2020





DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS GENERAL. ALL DIMENSIONS ARE IN METERS UNLESS NOTED OTHERWISE. ALL DIMENSIONS SHALL BE VERIFIED ON SITE BEFORE PROCEEDING WITH THE WORK. JAMES SHALL BE NOTIFIED BY WRITING OF ANY DISCREPANCIES. THIS DRAWING MUST BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACTS, SPECIFICATIONS, NOTICES AND DRAWINGS.

NOTE:
INFORMATION OF OTHER SERVICES INDICATED ON PLANS ARE INDICATIVE ONLY FOR GENERAL COORDINATION.

FINAL LOCATION OF ALL SERVICES INDICATED ARE TO BE COORDINATED AND VERIFIED BY THE CONTRACTOR DURING.

DRAWING LEGEND

DEVELOPMENT APPLICATION



REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	05/04/2019
01	REVISED HEIGHT	26/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
03	TASWATER REVISION TO FRONT PLANTER	12/06/2020
04	EXTENDED DAVEY STREET ELEVATION	16/06/2020

DEVELOPMENT APPLICATION



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NOTE: INFORMATION OF OTHER SERVICES INDICATED ON PLANS ARE INDICATIVE ONLY FOR GENERAL CONSIDERATION.

FINAL LOCATION OF ALL SERVICES AND UTILITIES ARE TO BE DETERMINED AND VERIFIED BY THE CONTRACTOR (S).

DRAWING LEGEND

APARTMENTS	No.
LEVEL 1	10
LEVEL 2	11
LEVEL 3	5
LEVEL 4	4
LEVEL 5	4
LEVEL 6	4
LEVEL 7	4
LEVEL 8	4
LEVEL 9	4
LEVEL 10	1

TOTAL 51 APARTMENTS

- KEY
- BATHROOM
 - KITCHEN
 - LAUNDRY
 - WARDROBE



JACOBY HOLDING PTY LTD
10/100 DAVEY STREET
MELBOURNE VIC 3000
21 CECILIA STREET
MELBOURNE VIC 3000
AUSTRALIA 1800
TELEPHONE 03 9222 4386
FAX 03 9222 1778
www.jacobys.com.au

PROJECT

DAVEY ST APARTMENTS
10 Davey Street, Melbourne VIC 3000
For
Tollfree Kiosk Unit Trust

DRAWING

FLOOR PLANS - LEVEL 3, 4-9

1509_DAVEY |

DATE: 05/04/2019
SCALE: 1:100
DRAWN: JAC
CHECKED: JAC
APPROVED: JAC
PROJECT: 1509_DAVEY

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2019
02	REVISED HEIGHT	28/03/2020
03	REVISED HEIGHT FOR DA	14/05/2020
04	TASKWATER REVISION TO FRONT PLANTER	15/06/2020
05	EXTENDED DAVEY STREET ELEVATION	16/06/2020



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NOTE:
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DRAWING LEGEND

APARTMENTS	No.
LEVEL 1	10
LEVEL 2	11
LEVEL 3	5
LEVEL 4	4
LEVEL 5	4
LEVEL 6	4
LEVEL 7	4
LEVEL 8	4
LEVEL 9	4
LEVEL 10	1

TOTAL: 51 APARTMENTS

KEY
BATHROOM
KITCHEN
LAUNDRY
WARDROBE



JAMES HOLDEN ARCHITECTS
100/100 STREET
THE BUILDING CENTRE
21 CECILIA STREET
MELBOURNE VIC 3000
AUSTRALIA 1000
TELEPHONE 03 9222 4000
FAX 03 9222 1178
JAMES@JAMESARCHITECTS.COM
WWW.JAMESARCHITECTS.COM

PROJECT

DAVEY ST APARTMENTS
10/100 STREET MELBOURNE VIC 3000
For
Tollfree Kiosk Unit Trust

DRAWING

FLOOR PLANS - LEVEL 10

1509_DAB7 |

DATE	AS DRAWN @ 10/04/2019
DRAWN	DAVEY
CHECKED	DAVEY
APPROVED	DAVEY
DATE	10/04/2019
DATE	10/04/2019
DATE	10/04/2019

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2019
02	REVISED HEIGHT	26/05/2019
03	REVISED HEIGHT FOR DA	14/05/2019
04	TASKWATER REVISION TO FRONT PLANTER	15/06/2019
05	EXTENDED DAVEY STREET ELEVATION	16/06/2019

DEVELOPMENT APPLICATION





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DRAWING LEGEND

- BAL BALCONY (GLZ)
- GLZ-1 GLAZED PANELS (FRAMESLESS)
- GLZ-2 GLAZED PANELS (FRAMES) ALUMINIUM FRAME (PORDERCAST)
- DAVS-1 METAL WALL SHEETING
- DAVS-2 METAL WALL SHEETING (VERTICAL)
- PC-1 PRECAST CONCRETE PANELS (HORIZONTAL)
- PC-2 PRECAST CONCRETE PANELS (HORIZONTAL GREEN)
- PC-3 PRECAST CONCRETE PANELS (HORIZONTAL GRAY)
- SC STONE TILE CLADDING (JUNO, CORNER)
- TM TRUSS CLADDING
- SCRN SCREEN (PORDERCAST ALUMINIUM)

JACOBLAND ARCHITECTS
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21 CECILIA STREET
MELBOURNE, VIC 3000
AUSTRALIA
1800 600 000
TEL: 03 9594 4000
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jacoblandarchitects.com.au
www.jacoblandarchitects.com.au

PROJECT

DAVEY ST APARTMENTS

61 Davey Street (Block 1) Unit 1000
For
Tollways Kiosk Unit Trust

DRAWING

ELEVATION - 1

1509_DAVEY

DATE: 05/04/2018
BY: JACOB LAND
CHECKED: JACOB LAND
APPROVED: JACOB LAND
PROJECT: 1509_DAVEY
PROJECT: 1509_DAVEY

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2018
02	REVISED HEIGHT	28/05/2018
03	REVISED HEIGHT FOR DA	14/05/2018
04	TAKEN INTO REVISION TO FRONT PLANTER	15/06/2018
05	EXTENDED DAVEY STREET ELEVATION	16/06/2018

DEVELOPMENT APPLICATION



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DRAWING LEGEND

BAL	BALCONY (GLZ-1)
GLZ-1	GLAZED PANELS (FRAMESLESS)
GLZ-2	GLAZED PANELS (FRAMES) ALUMINIUM FRAME (PIONEERCAST)
DAVEY	DAVEY
DAVEY 1	METAL WALL SHEETING
DAVEY 2	METAL WALL SHEETING (ARTISANAL)
PC-1	PRECAST CONCRETE PANELS (DAVEY)
PC-2	PRECAST CONCRETE PANELS (FRAMES) (DAVEY)
PC-3	PRECAST CONCRETE PANELS (FRAMES) (DAVEY)
SC	STONE TALL CLADDING (DAVEY) (FRAMES)
TM	TIMBER CLADDING
SCPA	SCREEN (PIONEERCAST) ALUMINIUM



ARTIST IMPRESSION

JAMES HILLER ARCHITECTS PTY LTD
100/100 DUNDAS STREET
THE HILLER GROUP
21 CECILIA STREET
MELBOURNE, VICTORIA
AUSTRALIA 3000

TELEPHONE 03 9222 4000
FAX 03 9222 1278
james.hiller@jameshiller.com.au
www.jameshiller.com.au

PROJECT

DAVEY ST APARTMENTS
170 Davey Street, Melbourne VIC 3000
For
Tollways Kinetics Unit Trust

DRAWING

ELEVATION - 2

1509_DAVEY |

DATE: 05/04/2019
SCALE: 1:100
DRAWN: JH
CHECKED: JH

APPROVED DESIGNER: JH
APPROVED REVIEWER: JH
FILED: JH
CADDIST: JH

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2019
02	REVISED HEIGHT	26/05/2020
03	REVISED HEIGHT FOR DA	14/05/2020
04	TAKEN INTO REVISION TO FRONT PLANTER	15/06/2020
05	EXTENDED DAVEY STREET ELEVATION	16/06/2020

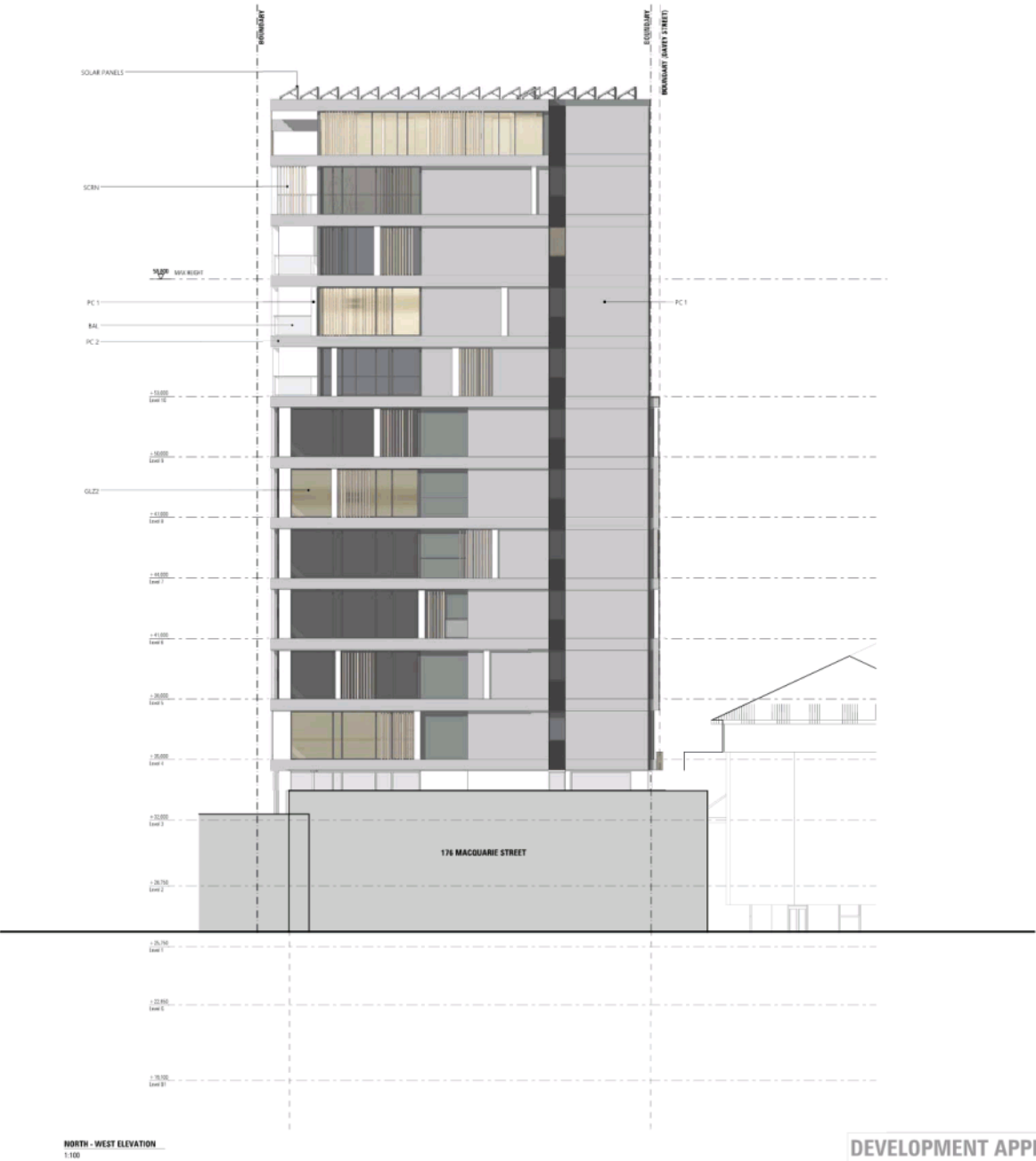
DEVELOPMENT APPLICATION





REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	05/04/2019
01	REVISED HEIGHT	26/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
03	TASWATER REVISION TO FRONT PLANTER	12/06/2020
04	EXTENDED DAVEY STREET ELEVATION	16/06/2020

ARCHITECT **JAWS**



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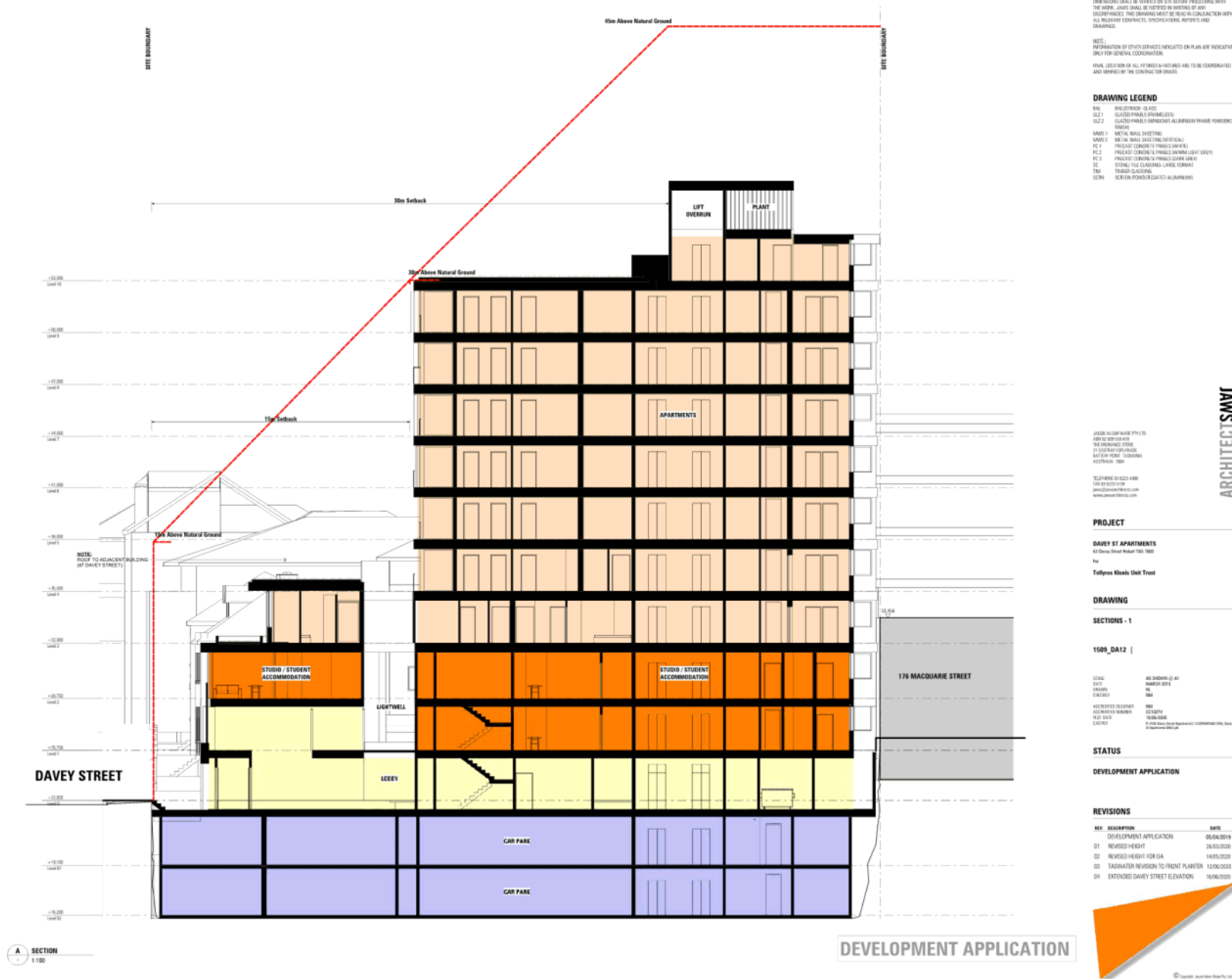
NOTE:
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DRAWING LEGEND	
RA1	RAILWAY - 0.400
GL2.1	GLAZED PANELS (FRAMINGLESS)
GL2.2	GLAZED PANELS (FRAMING ALUMINIUM FRAME) (PORDERCAST)
SCRN	SCREEN
DAVE1	METAL WALL SHEETING
DAVE2	METAL WALL SHEETING (VERTICAL)
PC1	PRECAST CONCRETE PANELS (HORIZONTAL)
PC2	PRECAST CONCRETE PANELS (HORIZONTAL) (GLAZED)
PC3	PRECAST CONCRETE PANELS (GLAZED)
SC	STONE TALL CLADDING (JANUS CORNER)
TM	TIMBER CLADDING
SCPA	SCREEN (PORDERCAST ALUMINIUM)

JAMES KILPATRICK PTY LTD
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AUSTRALIA - 1000
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FAX 03 9223 1178
james@jameskilpatrick.com.au
www.jameskilpatrick.com.au

PROJECT

DAVEY ST APARTMENTS
1509/1510/1511/1512/1513/1514/1515/1516/1517/1518/1519/1520/1521/1522/1523/1524/1525/1526/1527/1528/1529/1530/1531/1532/1533/1534/1535/1536/1537/1538/1539/1540/1541/1542/1543/1544/1545/1546/1547/1548/1549/1550/1551/1552/1553/1554/1555/1556/1557/1558/1559/1560/1561/1562/1563/1564/1565/1566/1567/1568/1569/1570/1571/1572/1573/1574/1575/1576/1577/1578/1579/1580/1581/1582/1583/1584/1585/1586/1587/1588/1589/1590/1591/1592/1593/1594/1595/1596/1597/1598/1599/1600/1601/1602/1603/1604/1605/1606/1607/1608/1609/1610/1611/1612/1613/1614/1615/1616/1617/1618/1619/1620/1621/1622/1623/1624/1625/1626/1627/1628/1629/1630/1631/1632/1633/1634/1635/1636/1637/1638/1639/1640/1641/1642/1643/1644/1645/1646/1647/1648/1649/1650/1651/1652/1653/1654/1655/1656/1657/1658/1659/1660/1661/1662/1663/1664/1665/1666/1667/1668/1669/1670/1671/1672/1673/1674/1675/1676/1677/1678/1679/1680/1681/1682/1683/1684/1685/1686/1687/1688/1689/1690/1691/1692/1693/1694/1695/1696/1697/1698/1699/1700/1701/1702/1703/1704/1705/1706/1707/1708/1709/1710/1711/1712/1713/1714/1715/1716/1717/1718/1719/1720/1721/1722/1723/1724/1725/1726/1727/1728/1729/1730/1731/1732/1733/1734/1735/1736/1737/1738/1739/1740/1741/1742/1743/1744/1745/1746/1747/1748/1749/1750/1751/1752/1753/1754/1755/1756/1757/1758/1759/1760/1761/1762/1763/1764/1765/1766/1767/1768/1769/1770/1771/1772/1773/1774/1775/1776/1777/1778/1779/1780/1781/1782/1783/1784/1785/1786/1787/1788/1789/1790/1791/1792/1793/1794/1795/1796/1797/1798/1799/1800/1801/1802/1803/1804/1805/1806/1807/1808/1809/1810/1811/1812/1813/1814/1815/1816/1817/1818/1819/1820/1821/1822/1823/1824/1825/1826/1827/1828/1829/1830/1831/1832/1833/1834/1835/1836/1837/1838/1839/1840/1841/1842/1843/1844/1845/1846/1847/1848/1849/1850/1851/1852/1853/1854/1855/1856/1857/1858/1859/1860/1861/1862/1863/1864/1865/1866/1867/1868/1869/1870/1871/1872/1873/1874/1875/1876/1877/1878/1879/1880/1881/1882/1883/1884/1885/1886/1887/1888/1889/1890/1891/1892/1893/1894/1895/1896/1897/1898/1899/1900/1901/1902/1903/1904/1905/1906/1907/1908/1909/1910/1911/1912/1913/1914/1915/1916/1917/1918/1919/1920/1921/1922/1923/1924/1925/1926/1927/1928/1929/1930/1931/1932/1933/1934/1935/1936/1937/1938/1939/1940/1941/1942/1943/1944/1945/1946/1947/1948/1949/1950/1951/1952/1953/1954/1955/1956/1957/1958/1959/1960/1961/1962/1963/1964/1965/1966/1967/1968/1969/1970/1971/1972/1973/1974/1975/1976/1977/1978/1979/1980/1981/1982/1983/1984/1985/1986/1987/1988/1989/1990/1991/1992/1993/1994/1995/1996/1997/1998/1999/2000/2001/2002/2003/2004/2005/2006/2007/2008/2009/2010/2011/2012/2013/2014/2015/2016/2017/2018/2019/2020/2021/2022/2023/2024/2025/2026/2027/2028/2029/2030/2031/2032/2033/2034/2035/2036/2037/2038/2039/2040/2041/2042/2043/2044/2045/2046/2047/2048/2049/2050/2051/2052/2053/2054/2055/2056/2057/2058/2059/2060/2061/2062/2063/2064/2065/2066/2067/2068/2069/2070/2071/2072/2073/2074/2075/2076/2077/2078/2079/2080/2081/2082/2083/2084/2085/2086/2087/2088/2089/2090/2091/2092/2093/2094/2095/2096/2097/2098/2099/2100/2101/2102/2103/2104/2105/2106/2107/2108/2109/2110/2111/2112/2113/2114/2115/2116/2117/2118/2119/2120/2121/2122/2123/2124/2125/2126/2127/2128/2129/2130/2131/2132/2133/2134/2135/2136/2137/2138/2139/2140/2141/2142/2143/2144/2145/2146/2147/2148/2149/2150/2151/2152/2153/2154/2155/2156/2157/2158/2159/2160/2161/2162/2163/2164/2165/2166/2167/2168/2169/2170/2171/2172/2173/2174/2175/2176/2177/2178/2179/2180/2181/2182/2183/2184/2185/2186/2187/2188/2189/2190/2191/2192/2193/2194/2195/2196/2197/2198/2199/2200/2201/2202/2203/2204/2205/2206/2207/2208/2209/2210/2211/2212/2213/2214/2215/2216/2217/2218/2219/2220/2221/2222/2223/2224/2225/2226/2227/2228/2229/2230/2231/2232/2233/2234/2235/2236/2237/2238/2239/2240/2241/2242/2243/2244/2245/2246/2247/2248/2249/2250/2251/2252/2253/2254/2255/2256/2257/2258/2259/2260/2261/2262/2263/2264/2265/2266/2267/2268/2269/2270/2271/2272/2273/2274/2275/2276/2277/2278/2279/2280/2281/2282/2283/2284/2285/2286/2287/2288/2289/2290/2291/2292/2293/2294/2295/2296/2297/2298/2299/2300/2301/2302/2303/2304/2305/2306/2307/2308/2309/2310/2311/2312/2313/2314/2315/2316/2317/2318/2319/2320/2321/2322/2323/2324/2325/2326/2327/2328/2329/2330/2331/2332/2333/2334/2335/2336/2337/2338/2339/2340/2341/2342/2343/2344/2345/2346/2347/2348/2349/2350/2351/2352/2353/2354/2355/2356/2357/2358/2359/2360/2361/2362/2363/2364/2365/2366/2367/2368/2369/2370/2371/2372/2373/2374/2375/2376/2377/2378/2379/2380/2381/2382/2383/2384/2385/2386/2387/2388/2389/2390/2391/2392/2393/2394/2395/2396/2397/2398/2399/2400/2401/2402/2403/2404/2405/2406/2407/2408/2409/2410/2411/2412/2413/2414/2415/2416/2417/2418/2419/2420/2421/2422/2423/2424/2425/2426/2427/2428/2429/2430/2431/2432/2433/2434/2435/2436/2437/2438/2439/2440/2441/2442/2443/2444/2445/2446/2447/2448/2449/2450/2451/2452/2453/2454/2455/2456/2457/2458/2459/2460/2461/2462/2463/2464/2465/2466/2467/2468/2469/2470/2471/2472/2473/2474/2475/2476/2477/2478/2479/2480/2481/2482/2483/2484/2485/2486/2487/2488/2489/2490/2491/2492/2493/2494/2495/2496/2497/2498/2499/2500/2501/2502/2503/2504/2505/2506/2507/2508/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BEFORE: Cleary's Gates



AFTER: Cleary's Gates



BEFORE: Murray Street



AFTER: Murray Street

DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS GENERAL. ALL DIMENSIONS ARE IN METERS UNLESS NOTED OTHERWISE. ALL DIMENSIONS SHALL BE VERIFIED ON SITE BEFORE PROCEEDING WITH THE WORK. JAMES SHALL BE NOTIFIED IN WRITING OF ANY DISCREPANCIES. THIS DRAWING MUST BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACTS, SPECIFICATIONS, NOTICES AND DRAWINGS.

NOTE: INFORMATION OF OTHER SERVICES INDICATED ON PLANS ARE INDICATIVE ONLY FOR GENERAL COORDINATION.

FINAL LOCATION OF ALL SERVICES IN FIELDS ARE TO BE COORDINATED AND VERIFIED BY THE CONTRACTOR DURING.

DRAWING LEGEND

DA1	RAILWAY - 0.000
DA2.1	GLAZED PANELS - FRAMING GLAZES
DA2.2	GLAZED PANELS - SUPPORTS ALUMINIUM FRAME - FIBREGLASS
DA3	DAVEY
DA4.1	METAL WALL SHEETING
DA4.2	METAL WALL SHEETING - METAL
PC1	PRECAST CONCRETE PANELS - 1000x1000
PC2	PRECAST CONCRETE PANELS - 1000x1000
PC3	PRECAST CONCRETE PANELS - 1000x1000
PC4	PRECAST CONCRETE PANELS - 1000x1000
PC5	PRECAST CONCRETE PANELS - 1000x1000
PC6	PRECAST CONCRETE PANELS - 1000x1000
PC7	PRECAST CONCRETE PANELS - 1000x1000
PC8	PRECAST CONCRETE PANELS - 1000x1000
PC9	PRECAST CONCRETE PANELS - 1000x1000
PC10	PRECAST CONCRETE PANELS - 1000x1000
PC11	PRECAST CONCRETE PANELS - 1000x1000
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PC63	PRECAST CONCRETE PANELS - 1000x1000
PC64	PRECAST CONCRETE PANELS - 1000x1000
PC65	PRECAST CONCRETE PANELS - 1000x1000
PC66	PRECAST CONCRETE PANELS - 1000x1000
PC67	PRECAST CONCRETE PANELS - 1000x1000
PC68	PRECAST CONCRETE PANELS - 1000x1000
PC69	PRECAST CONCRETE PANELS - 1000x1000
PC70	PRECAST CONCRETE PANELS - 1000x1000
PC71	PRECAST CONCRETE PANELS - 1000x1000
PC72	PRECAST CONCRETE PANELS - 1000x1000
PC73	PRECAST CONCRETE PANELS - 1000x1000
PC74	PRECAST CONCRETE PANELS - 1000x1000
PC75	PRECAST CONCRETE PANELS - 1000x1000
PC76	PRECAST CONCRETE PANELS - 1000x1000
PC77	PRECAST CONCRETE PANELS - 1000x1000
PC78	PRECAST CONCRETE PANELS - 1000x1000
PC79	PRECAST CONCRETE PANELS - 1000x1000
PC80	PRECAST CONCRETE PANELS - 1000x1000
PC81	PRECAST CONCRETE PANELS - 1000x1000
PC82	PRECAST CONCRETE PANELS - 1000x1000
PC83	PRECAST CONCRETE PANELS - 1000x1000
PC84	PRECAST CONCRETE PANELS - 1000x1000
PC85	PRECAST CONCRETE PANELS - 1000x1000
PC86	PRECAST CONCRETE PANELS - 1000x1000
PC87	PRECAST CONCRETE PANELS - 1000x1000
PC88	PRECAST CONCRETE PANELS - 1000x1000
PC89	PRECAST CONCRETE PANELS - 1000x1000
PC90	PRECAST CONCRETE PANELS - 1000x1000
PC91	PRECAST CONCRETE PANELS - 1000x1000
PC92	PRECAST CONCRETE PANELS - 1000x1000
PC93	PRECAST CONCRETE PANELS - 1000x1000
PC94	PRECAST CONCRETE PANELS - 1000x1000
PC95	PRECAST CONCRETE PANELS - 1000x1000
PC96	PRECAST CONCRETE PANELS - 1000x1000
PC97	PRECAST CONCRETE PANELS - 1000x1000
PC98	PRECAST CONCRETE PANELS - 1000x1000
PC99	PRECAST CONCRETE PANELS - 1000x1000
PC100	PRECAST CONCRETE PANELS - 1000x1000

JAMES KILMER ARCH PTY LTD
100/100 DUNDAS ST
THE SHEDDING STONE
21 CLOUTYR CLOSE
MELBOURNE VIC 3000
AUSTRALIA - 1000

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FAX 03 9222 4000
JAMES@JAMESARCH.COM
WWW.JAMESARCH.COM

PROJECT

DAVEY ST APARTMENTS

10/100 DUNDAS ST MELB 1000

For

Tollfree Kiosk Unit Trust

DRAWING

3D SITE VIEWS

1509_DA13 |

DATE:	AS SHOWN @ 10
SCALE:	1:1000
ORIGIN:	10
DATE:	10/10

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2018
02	REVISED HEIGHT	28/03/2018
03	REVISED HEIGHT FOR DA	14/05/2018
04	TAKEN INTO REVISION TO FRONT PLANTER	10/06/2018
05	EXTENDED DAVEY STREET ELEVATION	16/06/2018

DEVELOPMENT APPLICATION

ARCHITECTS
DAVEY





BEFORE: Hill Street



AFTER: Hill Street

DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS GENERAL. ALL DIMENSIONS ARE IN UNITS OF FEET AND INCHES. ALL DIMENSIONS SHALL BE VERIFIED ON SITE BEFORE PROCEEDING WITH THE WORK. SHOWN SHALL BE SUBJECT TO THE DISCREPANCIES. THIS DRAWING MUST BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACTS, SPECIFICATIONS, REPORTS AND DRAWINGS.

NOTE:
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FINAL LOCATION OF ALL UTILITIES AND SERVICES ARE TO BE COORDINATED AND VERIFIED BY THE CONTRACTOR PRIOR.

DRAWING LEGEND

DAV	DAVEY STREET
DAV 1	DAVEY STREET (10' WIDE)
DAV 2	DAVEY STREET (10' WIDE)
DAV 3	DAVEY STREET (10' WIDE)
DAV 4	DAVEY STREET (10' WIDE)
DAV 5	DAVEY STREET (10' WIDE)
DAV 6	DAVEY STREET (10' WIDE)
DAV 7	DAVEY STREET (10' WIDE)
DAV 8	DAVEY STREET (10' WIDE)
DAV 9	DAVEY STREET (10' WIDE)
DAV 10	DAVEY STREET (10' WIDE)
DAV 11	DAVEY STREET (10' WIDE)
DAV 12	DAVEY STREET (10' WIDE)
DAV 13	DAVEY STREET (10' WIDE)
DAV 14	DAVEY STREET (10' WIDE)
DAV 15	DAVEY STREET (10' WIDE)
DAV 16	DAVEY STREET (10' WIDE)
DAV 17	DAVEY STREET (10' WIDE)
DAV 18	DAVEY STREET (10' WIDE)
DAV 19	DAVEY STREET (10' WIDE)
DAV 20	DAVEY STREET (10' WIDE)

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FAX: 310.316.1112
WWW.JAMESHARRIS.COM

TELEPHONE: 310.316.1111
FAX: 310.316.1112
WWW.JAMESHARRIS.COM

PROJECT

DAVEY ST APARTMENTS

1000 Hill Street, Santa Monica, CA 90401

For
Toll-Free Home Unit Trust

DRAWING

3D SITE VIEWS

1509_0A14 |

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REVISIONS

REVISIONS

REVISIONS

REVISIONS

REVISIONS

REVISIONS

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REVISIONS

DEVELOPMENT APPLICATION



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REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	05/04/2011
01	REVISED HEIGHT	28/03/2012
02	REVISED HEIGHT FOR DA	14/05/2012
03	TADWATER REVISION TO FRONT PLANTER	12/06/2012
04	EXTENDED DRIVE STREET ELEVATION	06/09/2012

ARCHITECTS

BEFORE: Corner of Davey Street and Sandy Bay Road

AFTER: Corner of Davey Street and Sandy Bay Road

AFTER (WITH POTENTIAL DEVELOPMENT ON CORNER OF DAVEY/HARRINGTON ST): Corner of Davey Street and Sandy Bay Road

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DRAWING LEGEND

BA1	BAULSTRACK (GLASS)
GL1	GLAZED PANELS (FRAMINGLESS)
GL2	GLAZED PANELS (FAMCOLOUS ALUMINUM FRAME) POLYCARBONATE
	(RANDOM)
MWS1	METAL WALL SHEETING
MWS2	METAL WALL SHEETING (VERTICAL)
PC1	PRECAST CONCRETE PANELS (WHITE)
PC2	PRECAST CONCRETE PANELS (RANDOM LIGHT GREY)
PC3	PRECAST CONCRETE PANELS (DARK GREY)
SC	STONE/TILE CLADDING (LARGE FORMAT)
TRM	TRIMER CLADDING
SCRN	SCREEN (POWDER COATED ALUMINUM)

JACOB NLEDMAN WACE PTY LTD
ABN 52 008 100 479
THE ORCHARD STORE
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AUSTRALIA 7004

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FAX (313) 622-5106
JAV@JANSENPHARM.COM
WWW.JANSENPHARM.COM

PROJECT

DAVEY ST APARTMENTS
410 Davey Street, Portland, ME 04101

63 Dorsey Street, Woburn, MA 01801
For
Tollfree Klean's Unit Trust

DRAWING

3D SITE VIEWS

1509_DA16 |

SCAGL	AG SHENYI 安一
DATE	MARCH 2019
DRAWN	HL
CHECKED	NH
APPROVED DESIGNER	NH
APPROVED MANAGER	CC33779
PLD DATE	16/06/2020
CAD/REV	Puttini Design Street Agency P. Rappazzoni 090/06

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	05/04/2019
01	REVISED HEIGHT	24/03/2020
02	REVISED HEIGHT FOR DA	14/03/2020
03	TASWATER REVISION TO FRONT PLANTER	12/06/2020
04	EXTENDED DAVEY STREET ELEVATION	16/06/2020

DEVELOPMENT APPLICATION



BEFORE: Davy Street



AFTER: Davy Street



AFTER (WITH POTENTIAL DEVELOPMENT ON CORNER OF DAVEY/HARRINGTON ST): Davy Street

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FINAL LOCATION OF ALL RETAINED FEATURES ARE TO BE COORDINATED AND VERIFIED BY THE CONTRACTOR ON SITE.

DRAWING LEGEND

DAV	DAVEY STREET - 1/4" = 1'-0"
DAV-1	DAVEY STREET - 1/4" = 1'-0"
DAV-2	DAVEY STREET - 1/4" = 1'-0"
DAV-3	DAVEY STREET - 1/4" = 1'-0"
DAV-4	DAVEY STREET - 1/4" = 1'-0"
DAV-5	DAVEY STREET - 1/4" = 1'-0"
DAV-6	DAVEY STREET - 1/4" = 1'-0"
DAV-7	DAVEY STREET - 1/4" = 1'-0"
DAV-8	DAVEY STREET - 1/4" = 1'-0"
DAV-9	DAVEY STREET - 1/4" = 1'-0"
DAV-10	DAVEY STREET - 1/4" = 1'-0"
DAV-11	DAVEY STREET - 1/4" = 1'-0"
DAV-12	DAVEY STREET - 1/4" = 1'-0"
DAV-13	DAVEY STREET - 1/4" = 1'-0"
DAV-14	DAVEY STREET - 1/4" = 1'-0"
DAV-15	DAVEY STREET - 1/4" = 1'-0"
DAV-16	DAVEY STREET - 1/4" = 1'-0"
DAV-17	DAVEY STREET - 1/4" = 1'-0"
DAV-18	DAVEY STREET - 1/4" = 1'-0"
DAV-19	DAVEY STREET - 1/4" = 1'-0"
DAV-20	DAVEY STREET - 1/4" = 1'-0"
DAV-21	DAVEY STREET - 1/4" = 1'-0"
DAV-22	DAVEY STREET - 1/4" = 1'-0"
DAV-23	DAVEY STREET - 1/4" = 1'-0"
DAV-24	DAVEY STREET - 1/4" = 1'-0"
DAV-25	DAVEY STREET - 1/4" = 1'-0"
DAV-26	DAVEY STREET - 1/4" = 1'-0"
DAV-27	DAVEY STREET - 1/4" = 1'-0"
DAV-28	DAVEY STREET - 1/4" = 1'-0"
DAV-29	DAVEY STREET - 1/4" = 1'-0"
DAV-30	DAVEY STREET - 1/4" = 1'-0"
DAV-31	DAVEY STREET - 1/4" = 1'-0"
DAV-32	DAVEY STREET - 1/4" = 1'-0"
DAV-33	DAVEY STREET - 1/4" = 1'-0"
DAV-34	DAVEY STREET - 1/4" = 1'-0"
DAV-35	DAVEY STREET - 1/4" = 1'-0"
DAV-36	DAVEY STREET - 1/4" = 1'-0"
DAV-37	DAVEY STREET - 1/4" = 1'-0"
DAV-38	DAVEY STREET - 1/4" = 1'-0"
DAV-39	DAVEY STREET - 1/4" = 1'-0"
DAV-40	DAVEY STREET - 1/4" = 1'-0"
DAV-41	DAVEY STREET - 1/4" = 1'-0"
DAV-42	DAVEY STREET - 1/4" = 1'-0"
DAV-43	DAVEY STREET - 1/4" = 1'-0"
DAV-44	DAVEY STREET - 1/4" = 1'-0"
DAV-45	DAVEY STREET - 1/4" = 1'-0"
DAV-46	DAVEY STREET - 1/4" = 1'-0"
DAV-47	DAVEY STREET - 1/4" = 1'-0"
DAV-48	DAVEY STREET - 1/4" = 1'-0"
DAV-49	DAVEY STREET - 1/4" = 1'-0"
DAV-50	DAVEY STREET - 1/4" = 1'-0"
DAV-51	DAVEY STREET - 1/4" = 1'-0"
DAV-52	DAVEY STREET - 1/4" = 1'-0"
DAV-53	DAVEY STREET - 1/4" = 1'-0"
DAV-54	DAVEY STREET - 1/4" = 1'-0"
DAV-55	DAVEY STREET - 1/4" = 1'-0"
DAV-56	DAVEY STREET - 1/4" = 1'-0"
DAV-57	DAVEY STREET - 1/4" = 1'-0"
DAV-58	DAVEY STREET - 1/4" = 1'-0"
DAV-59	DAVEY STREET - 1/4" = 1'-0"
DAV-60	DAVEY STREET - 1/4" = 1'-0"
DAV-61	DAVEY STREET - 1/4" = 1'-0"
DAV-62	DAVEY STREET - 1/4" = 1'-0"
DAV-63	DAVEY STREET - 1/4" = 1'-0"
DAV-64	DAVEY STREET - 1/4" = 1'-0"
DAV-65	DAVEY STREET - 1/4" = 1'-0"
DAV-66	DAVEY STREET - 1/4" = 1'-0"
DAV-67	DAVEY STREET - 1/4" = 1'-0"
DAV-68	DAVEY STREET - 1/4" = 1'-0"
DAV-69	DAVEY STREET - 1/4" = 1'-0"
DAV-70	DAVEY STREET - 1/4" = 1'-0"
DAV-71	DAVEY STREET - 1/4" = 1'-0"
DAV-72	DAVEY STREET - 1/4" = 1'-0"
DAV-73	DAVEY STREET - 1/4" = 1'-0"
DAV-74	DAVEY STREET - 1/4" = 1'-0"
DAV-75	DAVEY STREET - 1/4" = 1'-0"
DAV-76	DAVEY STREET - 1/4" = 1'-0"
DAV-77	DAVEY STREET - 1/4" = 1'-0"
DAV-78	DAVEY STREET - 1/4" = 1'-0"
DAV-79	DAVEY STREET - 1/4" = 1'-0"
DAV-80	DAVEY STREET - 1/4" = 1'-0"
DAV-81	DAVEY STREET - 1/4" = 1'-0"
DAV-82	DAVEY STREET - 1/4" = 1'-0"
DAV-83	DAVEY STREET - 1/4" = 1'-0"
DAV-84	DAVEY STREET - 1/4" = 1'-0"
DAV-85	DAVEY STREET - 1/4" = 1'-0"
DAV-86	DAVEY STREET - 1/4" = 1'-0"
DAV-87	DAVEY STREET - 1/4" = 1'-0"
DAV-88	DAVEY STREET - 1/4" = 1'-0"
DAV-89	DAVEY STREET - 1/4" = 1'-0"
DAV-90	DAVEY STREET - 1/4" = 1'-0"
DAV-91	DAVEY STREET - 1/4" = 1'-0"
DAV-92	DAVEY STREET - 1/4" = 1'-0"
DAV-93	DAVEY STREET - 1/4" = 1'-0"
DAV-94	DAVEY STREET - 1/4" = 1'-0"
DAV-95	DAVEY STREET - 1/4" = 1'-0"
DAV-96	DAVEY STREET - 1/4" = 1'-0"
DAV-97	DAVEY STREET - 1/4" = 1'-0"
DAV-98	DAVEY STREET - 1/4" = 1'-0"
DAV-99	DAVEY STREET - 1/4" = 1'-0"
DAV-100	DAVEY STREET - 1/4" = 1'-0"

JACOB HILMAN ARCHITECTS
1000 10TH AVENUE
SUITE 1000
DENVER, CO 80202
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WWW.JACOBHILMANARCHITECTS.COM

TELEPHONE: 303.733.1111
FAX: 303.733.1111
WWW.JACOBHILMANARCHITECTS.COM

PROJECT

DAVEY ST APARTMENTS

1000 10TH AVENUE, SUITE 1000

Toll-free: 1-800-444-4444

DRAWING

3D SITE VIEWS

1509_0A17 |

DATE:	AS SHOWN @ 1/4" = 1'-0"
SCALE:	1/4" = 1'-0"
PROJECT:	1000 10TH AVENUE, SUITE 1000
CLIENT:	1000 10TH AVENUE, SUITE 1000
ARCHITECT:	1000 10TH AVENUE, SUITE 1000
DESIGNER:	1000 10TH AVENUE, SUITE 1000
PLANNER:	1000 10TH AVENUE, SUITE 1000
CONTRACTOR:	1000 10TH AVENUE, SUITE 1000

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV.	DESCRIPTION	DATE
01	DEVELOPMENT APPLICATION	05/04/2018
02	REVISED HEIGHT	26/05/2018
03	REVISED HEIGHT FOR DA	14/05/2018
04	TAKEN INTO REVISION TO FRONT PLANTER	15/06/2018
05	EXTENDED DAVEY STREET ELEVATION	16/06/2018

DEVELOPMENT APPLICATION

ARCHITECTS
JACOB HILMAN ARCHITECTS
1000 10TH AVENUE, SUITE 1000
DENVER, CO 80202
TEL: 303.733.1111
WWW.JACOBHILMANARCHITECTS.COM

BEFORE: St. David's Park

AFTER: St. David's Park.

AFTER (WITH POTENTIAL DEVELOPMENT ON CORNER OF DAVEY/HARRINGTON ST): St. David's Park

DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS CONTROL. ALL DIMENSIONS ARE IN MILLIMETERS UNLESS NOTED OTHERWISE. ALL DIMENSIONS SHALL BE VERIFIED ON SITE BEFORE PROCEEDING WITH THE WORK. JAWS SHALL BE FURRED IN WRITING OF ANY DISCREPANCIES. THIS DRAWING MUST BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACTS, SPECIFICATIONS, REPORTS AND DRAWINGS.

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AND VERIFIED BY THE CONTRACTOR-ON-SITE.

DRAWING LEGEND

BA1	BALUSTRADE - GLASS
GL2.1	GLAZED PANELS (FRAMESLESS)
GL2.2	GLAZED PANELS (W/FRAMES) ALUMINIUM FRAME (P/POSSIBLE) (R/NO)
MAWS.1	METAL WALL SHEETING
MAWS.2	METAL WALL SHEETING (VERTICAL)
PC.1	P/CAST CONCRETE PANELS (WHITE)
PC.2	P/CAST CONCRETE PANELS (M/NO LIGHT GREY)
PC.3	P/CAST CONCRETE PANELS (DARK GREY)
SC	S/IDEAL TILE CLADDING - LARGE FORMATS
TRM	TIMBER CLADDING
SCRN	SCREEN (POWDER COATED) ALUMINIUM

JACOB ALLEN WAGE PH/LT
ARM 52 009 008 479
THE CREWANCE STORE
21 CASTRAY ESPRANGUE
BATTERY FORT JACKMANA
AUSTRALIA 7004

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FAX (313) 622-5106
jans@jansarchitects.com
www.jansarchitects.com

PROJECT

DAVEY ST APARTMENTS

Tollyros Needs Unit Trust

DRAWING

3D SITE VIEWS

1509_DA18 |

SCALE	AS SHOWN @ AS
DATE	MARCH 2019
DRAWN	HL
CHECKED	NBO
APPROVED DESIGN	NBO
APPROVED NUMBER	DC19279
PLT DATE	14/04/2020
CADREF	P:\2019\Bany Street Area Dr.Rajasekaran\DWG\

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	05/04/2019
01	REVISED HEIGHT	26/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
03	TASWATER REVISION TO FRONT PLANTER	12/06/2020
04	EXTENDED DAVEY STREET ELEVATION	16/06/2020

DEVELOPMENT APPLICATION

ARCHITECT JAWS

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BEFORE: Davvy Street

AFTER: Davey Street.

AFTER (WITH POTENTIAL DEVELOPMENT ON CORNER OF DAVEY/HARRINGTON ST): Davey Street

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HYAL. LOCATION OF ALL FITTINGS & FIXTURES ARE TO BE COORDINATED AND VERIFIED BY THE CONTRACTOR ON-SITE.

DRAWING LEGEND

BN	BAULTRACK GLASS
GL1	GLAZED PANELS (FRAMERLESS)
GL2	GLAZED PANELS (WIDENED ALUMINUM FRAME) (MONOCOLOR)
	(RANDOM)
MWS1	METAL WALL SHEETING
MWS2	METAL WALL SHEETING (VERTICAL)
PC1	PRECAST CONCRETE PANELS (WHITE)
PC2	PRECAST CONCRETE PANELS (RANDOM LIGHT GRAY)
PC3	PRECAST CONCRETE PANELS (DARK GRAY)
SC	STONE / TILE CLADDING (LARGE FORMAT)
TRM	TRUSS CLADDING
SCRN	SCREEN (POWDER COATED ALUMINUM)

JACOB ALLEN WADE FTH/LTJ
484 52 000 000 470
746 OCEANOGRAPHY
21 SATURN DRIVE
BATHING POOL TASMANIA
AUSTRALIA 7250

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FAX 855 8225 5100
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WWW.220ANALYTICALS.COM

PROJECT

DAVEY ST APARTMENTS.

83 Cherry Street West, Suite 1000
 Toronto, Ontario M5H 2B4
 Canada
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 Fax: 416-593-9301
 Email: info@canadapost.ca
 Website: www.canadapost.ca

Tollymore Woods Unit Trust

DRAWING

3D SITE VIEWS

1509_DA19 |

SCAG	AG SHOWS @ AT
DATE	MARCH 2019
NAME	HE
CHEN	NO
ACCEPTED DESIGN	NO
ACCEPTED NAME	001007
FILE DATE	10/06/2019
END	

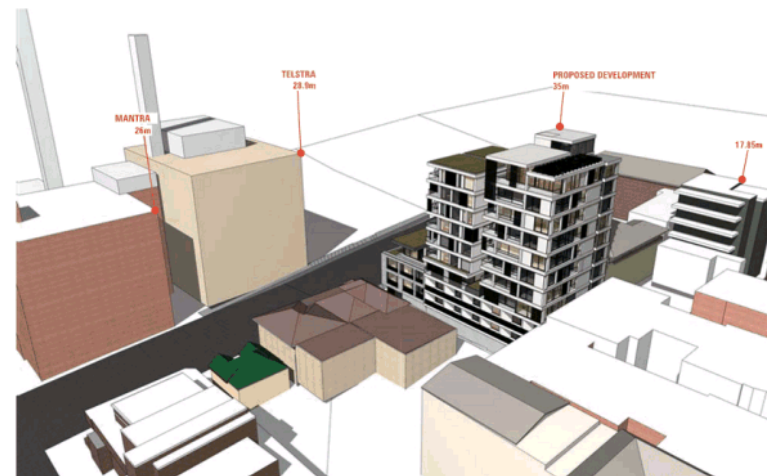
STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	05/04/2019
01	REVISED HEIGHT	24/03/2020
02	REVISED HEIGHT FOR DA	14/05/2020
03	TADWATER REVISION TO FRONT PLANTER	12/06/2020
04	EXTENDED DAVEY STREET ELEVATION	16/06/2020

DEVELOPMENT APPLICATION



NOTE:
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FINAL LOCATION OF ALL FITTINGS & FIXTURES ARE TO BE COORDINATED
AND VERIFIED BY THE CONTRACTOR ON SITE.

DRAWING LEGEND



PROJECT

DAVEY ST APARTMENTS
63 Davey Street, Robert TAC 7009
For
Tollfree Kline's Unit Trust

DRAWING

CONTEXT SITE SECTION

1509_DAO2

DATE	AS DRYW (p. A1)
DATE	MARCH 2018
DRAWN	HUNT
CHECKED	NW
ACCREDITED DESIGNER	NW
ACCREDITED NUMBER	CE1879
PLT DATE	15/06/2020
CAD BY	P:\340 Tracy Court Apartments\31-2\WARRINGHAM_340_

STATUS

DEVELOPMENT APPLICATION

REVISIONS

REV	DESCRIPTION	DATE
	DEVELOPMENT APPLICATION	05/04/2019
01	REVISED HEIGHT	16/02/2020
02	REVISED HEIGHT FOR DA	14/02/2020
03	TASWATER REVISION TO FRONT PLANTER	12/09/2020
04	EXTENDED GAWY STREET ELEVATION	16/09/2020

DEVELOPMENT APPLICATION

May 2020

JAWS
ARCHITECTS**DESIGN STATEMENT** – 63 Davey Street, Hobart

Ideally positioned for vibrant inner-city living, this apartment complex contains a mix of residential and studio / student accommodation. Situated between two Heritage-listed buildings in a Heritage Precinct, the site has previously had numerous residential uses which have been demolished over the years, leaving an asphalt carpark and brick store facility as a negative void in an otherwise elegant historic Hobart location.

Program Description and Form Concept

The Ground Floor of the complex provides entry to two levels of car parking and the 51 Apartments above. A wide range of apartment types is provided, consisting of 21 x Studio | Student apartments, 29 x 2-bedroom apartments, and 1 x 3-bedroom, penthouse apartment.

The first two levels above ground contain the studio apartments with private open space predominantly orientated to the north, creating a podium to reference the scale of surrounding buildings.

A service core on the south side of the building provides access to all levels, with a separate stair to the east serving the podium levels. The southern location of the core allows the apartments to enjoy northerly aspect and views over Salamanca and Battery Point. This also maintains privacy to adjacent hospital facilities. Level 3 contains the first level of standard two-bedroom apartments and is setback from the frontage to accommodate a green roof on top of the podium. Refer to Diagram 01.

The layout and massing of the building has been designed to maximise the site coverage whilst maintaining setbacks to provide enhanced amenity for residents.

The building is configured to read as separate, yet integrated components of the one entity. The form of the building is broken down to create a multi-layered response which addresses its location within the streetscape, the local heritage precinct and the broader townscape setting, achieving a synergy between heritage values and new forms. Refer to Diagrams 02 + 03.

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EXPERIENCE CREATIVE QUALITY



DIAGRAM 01

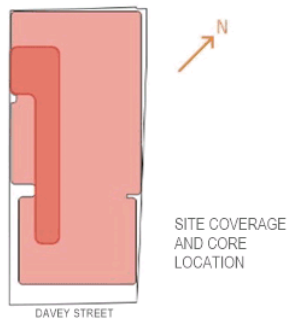


DIAGRAM 02

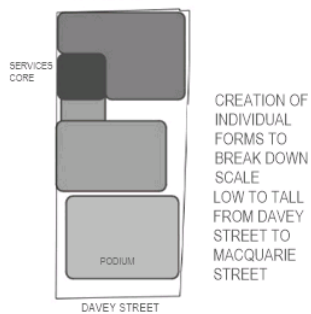
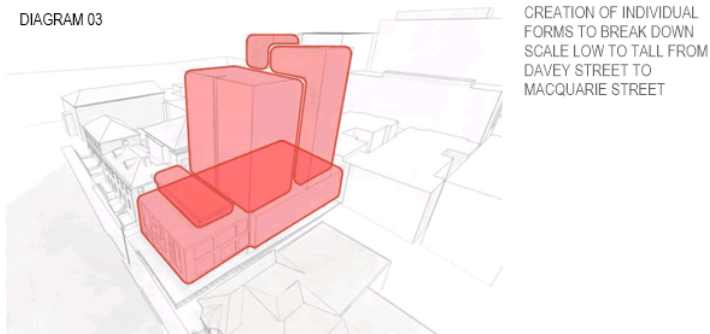


DIAGRAM 03



Streetscape and Heritage Precinct Response

The heritage response has been driven by the streetscape view i.e. 'view-field' from the Davey Street. The podium building reads as a sensitive addition in the street, thoughtfully responding to the adjacent Heritage buildings in setback, height, material & fenestration techniques. The apartments, Level 3 and above, are set back more than 15m behind the streetscape podium with enough space

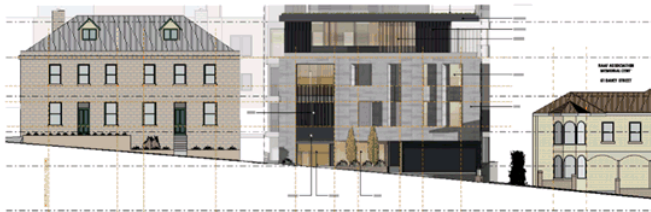
that the character and significance of the heritage streetscape is not compromised. Additionally, the higher level apartments are materially distinct from the streetscape, reducing prominence. From a pedestrian and vehicular perspective the podium element is the defining image of the development when experienced in its streetscape scale context. The new buildings engage in a compositional manner to the Heritage context and merge into the adjacent city blocks, contributing to the ongoing and evolving development of the city.

The podium fills the existing void within the streetscape, creating a complimentary street edge condition with high quality finishes and contemporary design. The removal of the open asphalt carpark and insertion of a new respectful active building element helps to repair the fabric of the streetscape, enhancing the street level experience.

The design of the front façade is articulated with window openings that reference the symmetry, proportions and rhythm of the adjoining heritage buildings, expressed in a contemporary manner.

Street trees at the front of the building further reference the adjacent buildings, with species of plantings adding another layer of connection along the streetscape.

The ground floor lobby presents a welcoming entrance with a generously proportioned double height volume which is animated by natural light from above. The vehicular access to the basement carpark is accessed from the opposite side of the building through a recessed alcove.



DAVEY STREET PART ELEVATION



DAVEY STREET _ ARTIST'S IMPRESSION

The building form steps up at the rear of the site with respect to the underlying topography, recognising the Macquarie Ridge to the north west of the site and the scale of development within the surrounding townscape in which it is situated.

The highest point of the development is located at the deepest setback of the site, with the service equipment and lift overruns at the rear.

Height Analysis

The development height has been a continuous process of analysis and contextual review. The design team has had extensive consultation with urban designers and heritage experts, Paul Davies, Ireneinc and Praxis, to establish a thoughtful response to the Heritage precinct and Streetscape principles applying to this particular site.

Council's K2Vi model was used as a tool to establish a contextual response at the outset. Preliminary advice from UDAP in 2019 was also taken into account to help establish urban design principles and guide the design.

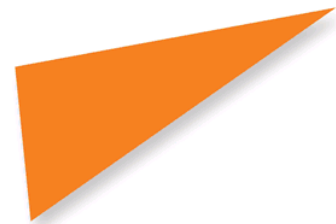
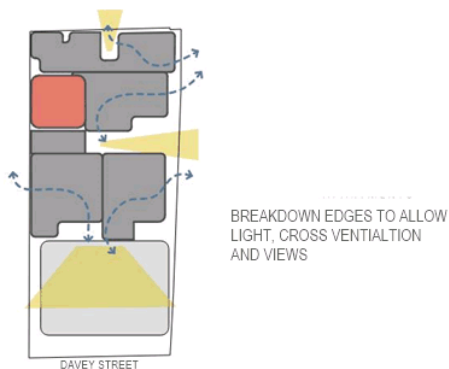
Originally submitted in early 2019, the proposal has been significantly modified in response the recent RMPAT decision made in regards to the nearby 'Welcome Stranger' project. The height has been re-assessed with the assistance of Ireneinc Planning and Praxis to reduce the maximum height of the scheme from 45m to 35m.

Residential Amenity

Above the podium a large central light well opens to the north, splitting the building into two components to break down the massing and allow light deep into the plan.

This breaking down of the edges of each of the building components allows the forms to read more as a family of buildings rather than a singular block. Refer Diagram 04.

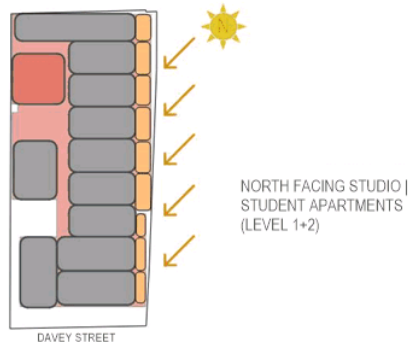
DIAGRAM 03



Balconies and terraces for the apartments are orientated to maximise northern aspect where possible and take advantage of the view corridor to the east view over St David's Park and south down the River Derwent. In conjunction with windows, these private open areas allow for maximised cross ventilation opportunities.

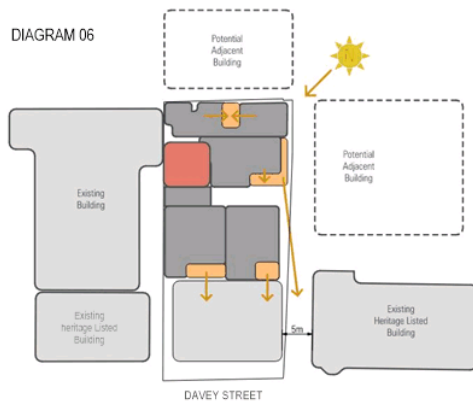
The podium levels accommodate 21 affordable Studio / Student style accommodation. 85% of these have been orientated with north aspect. Refer Diagram 05.

DIAGRAM 05



The development has thoughtfully located private outdoor spaces to work with existing conditions and also considered future potential development. The view and open aspect amenity of the each apartment has been maintained. Refer Diagram 06.

DIAGRAM 06



Strong horizontal banding defines each level, with an overlay of balcony screens, windows and planter boxes providing a finer level of detail. Each apartment is configured to maximise solar access and the stunning views towards the waterfront. The patterning and liveliness of the apartment facades creates a unique and individual sensibility for each apartment, while the planning economically repeats.

A penthouse level is set back further again, reinforcing the vertical split through the interplay of volumes.

The treatment of the building facades is distinguished from the lower podium, with interstices to provide separation between apartments and create greater distinction and articulation in the building form. The design, finish and materiality, while still integrated with the treatment of the lower level, have been articulated for greater visual interest and to avoid expansive blank facades.



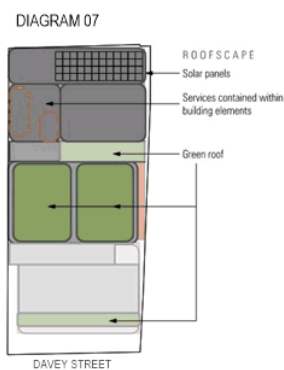
APARTMENT DETAIL _ ARTIST'S IMPRESSION

Sustainable Initiatives

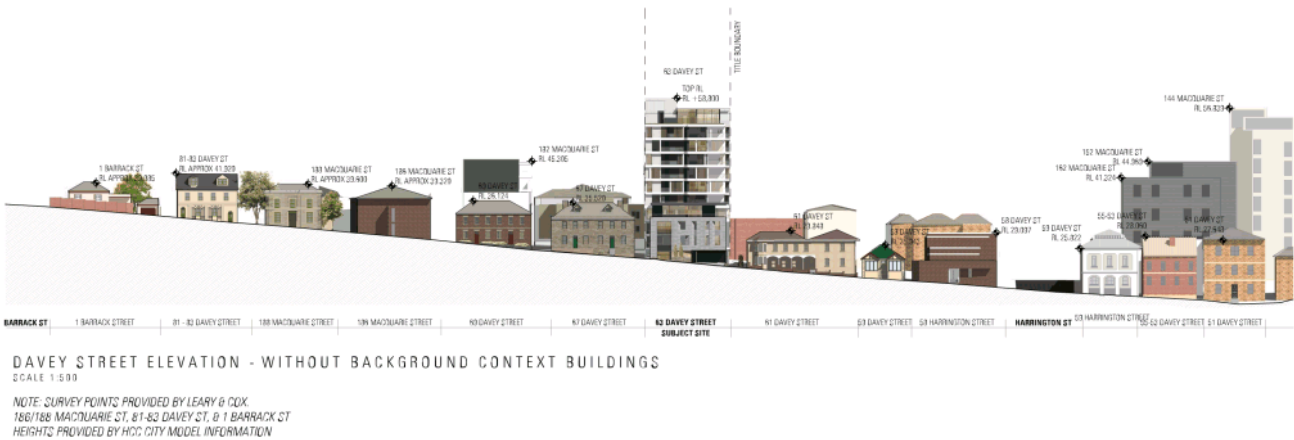
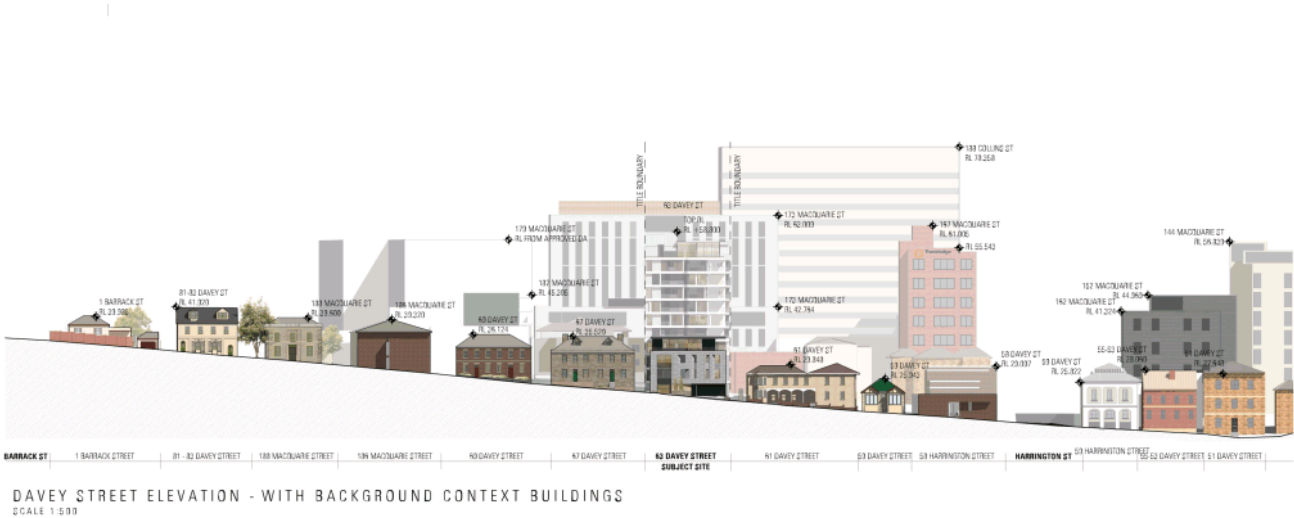
Green roof terraces have been provided at various levels to improve thermal insulation qualities, soften the façade edges and provide a positive outlook for residents. These areas will be planted with hardy vegetation that have minimal water requirements and are low maintenance. Refer to Diagram 07.

A solar array with a northwest aspect will off-set the building's reliance on purchased energy.

Other required service equipment zones have been carefully integrated within the building elements to eliminate an ad-hoc screened services enclosure at the highest levels. The efficient planning arrangement and servicing affords reduced material waste, while the site location allows reduced reliance on car travel.



The complex aims to reflect a contemporary ambition for sustainable inner city living, respectfully inserted into a historic Hobart precinct.



PROJECT	
DAVEY ST APARTMENTS	
83 Davey Street, Sydney NSW 1500	
For	
Tollymoo Kinola Unit Trust	
DRAWING	
DAVEY ST BLOCK ELEVATION	
1509_BFI 	
DATE	AS SHOWN up to 41
DRAWN	JANE TOLLYMOO
CHECKED	HELEN TOLLYMOO
APPROVED	MARK TOLLYMOO
APPROVED NUMBER	1509/2020
PROJECT DATE	15/01/2020
CAD FILE	1509 Davey Street Apartments.dwg (15/01/2020), Davey St Apartments.dwg (15/01/2020)

63 DAVEY STREET, HOBART



ireneinc & smithstreetstudio
PLANNING & URBAN DESIGN

PLANNING TAS PTY LTD TRADING AS IRENEINC PLANNING & SMITH STREET STUDIO PLANNING & URBAN DESIGN ABN 78 114 905 074

63 DAVEY STREET, HOBART

Planning Submission to Hobart City Council

Last Updated - July 2020

Author - Phil Gartrell

Reviewed: Irene Duckett

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ireneinc PLANNING & URBAN DESIGN

CONTENTS

CONTENTS	3
1. INTRODUCTION	4
1.1 SUBJECT SITE	5
1.2 SITE SURROUNDS	6
1.3 CONSULTATION	8
2. PROPOSED DEVELOPMENT	9
3.1 CENTRAL BUSINESS ZONE	10
3.1.1 Zone Purpose Statements	10
3.1.2 Desired Future Character Statements	12
3.1.3 Use Status	14
3.1.4 Use Standards	14
3.1.5 Development Standards	15
4. CODES	28
4.1 POTENTIALLY CONTAMINATED LAND CODE	28
4.2 ROAD AND RAILWAY ASSETS CODE	28
4.2.1 Development Standards	28
4.3 PARKING AND ACCESS CODE	30
4.3.1 Use Standards	30
4.3.2 Development Standards	33
4.4 STORMWATER MANAGEMENT CODE	40
4.4.1 Development standards	40
4.5 HISTORIC HERITAGE CODE	41
4.5.1 Development Standards for Heritage Places	43
4.5.2 Development Standards for Heritage Precincts	43
4.5.3 Development Standards for Places of Archaeological Potential	47
4.6 SIGNS CODE	48
5. CONCLUSION	49

1. INTRODUCTION

Planning Tas trading as Ireneinc Planning and Urban Design have prepared the following assessment on behalf of Tellyros Klonis Unit Trust to accompany an application for the use and development of land at 63 Davey Street, Hobart.

This report has been prepared in response to plans prepared by JAWS Architecture.

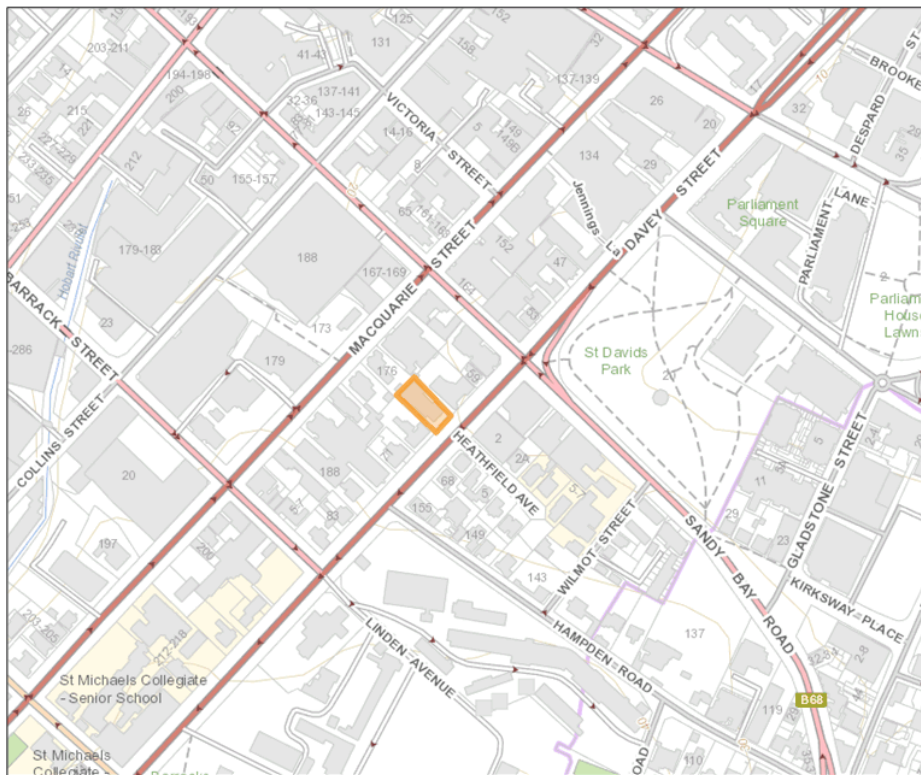


Figure 1: Site location (source: www.thelist.tas.gov.au © State of Tasmania)

1.1 SUBJECT SITE

The subject land is located at 63 Davey Street, Hobart 'Navy Club' (CT54396/1), with a site area of 809m². The site has a fall from north west to the street frontage on the south east boundary.

The application also includes the adjoining site, identified as 186 Macquarie Street St Helens Private Hospital (CT 110411/1), due to a requirement for access and potential works related to upgrades of existing sewer pipe and connection. Further detail is provided in the accompanying civil documentation.

The site has an existing single-storey building, formerly the Old Navy Club, built to the side and rear boundaries and setback approximately 13m from the frontage to Davey Street. The building is currently used as furniture storage with on-site carparking within the front setback.



Figure 2: Aerial image of the subject site (red) and adjoining site at 186 Macquarie Street (blue) (Source: www.thelist.tas.gov.au © State of Tasmania)

The site is currently accessed via a shared 4m (approx.) wide entrance from Davey Street, which also includes a right of way easement benefitting the adjoining to the land to the southwest. There is an existing sewer main that runs along the rear boundary of the property.



Figure 3: Frontage of existing building to Davey Street

1.2 SITE SURROUNDS

St Helens Private Hospital is located to the south west of the site at 186 Macquarie Street, whilst the main entry to the hospital is from Macquarie Street. The Davey Street frontage of the hospital is a three storey sandstone heritage building. The upper floor is formed by the dormer windows within the roof space. The building as with many of the other buildings along the frontage, is set back for a small landscaped area. To the rear of the heritage building is a contemporary hospital building with underground parking below. The remainder of the street through to Barrack Street is characterised by 2-3 storey buildings, many of heritage value.

To the north east, 61 Davey Street is a two storey heritage building (RAAF Association Memorial Centre). East of this is a single storey heritage cottage at 59 Davey Street and the art deco Welcome Stranger Hotel on the corner at 58 Harrington Street.

On the opposing side of the street the 6 storey red brick Mantra hotel on the corner of Sandy Bay Road and the 5-6 storey Telstra Building occupy the streetscape, with apartments and residential buildings west of Heathfield Avenue.



Figure 4: View down Davey Street



Figure 5: View looking up Davey Street



Figure 6: View from Street frontage of St David's park

A further analysis of the surrounding site context is provided in the accompanying Urban Design Statement.

1.3 CONSULTATION

The original proposal has been through a number of design iterations that have been discussed with Council's planning and heritage officers. The previous design was also presented to Council's Urban Design Advisory Panel (UDAP).

As a result of these discussions and the recent Tribunal ruling in regard to the proposal at 58 Harrington Street, the proposal has been revised.

2. PROPOSED DEVELOPMENT

The proposal involves the demolition of the existing single storey building at 63 Davey Street, including removal of the existing 8 parking spaces in the forecourt.

The new works are to facilitate use and development for 51 apartments providing a mix of residential apartments and student accommodation. The 21 apartments across Level 1 and Level 2 will be allocated for student accommodation, whilst the remaining 30 apartments will be residential.

The building form is set back at its closest point to the frontage of the site by 2.8m to allow for landscaping and setback consistent with the adjoining heritage buildings. The ground floor provides lobby, services, access and vehicle manoeuvring for the proposed building, and 42 vehicle parking spaces.

The proposal will require infrastructure works within the basement car parking level on the adjoining property at 186 Macquarie Street. The owner/operator of 186 Macquarie Street has been notified in accordance with Section 52 of LUPAA.

The vehicular access to the site is to be reconfigured to the north east side of the frontage allowing two way traffic flow. Council Landowner Consent was previously provided for these works, and the revised application does not seek to modify them.

The basement carparking is accessed via car lifts located internally within the building. Bicycle storage and motorbike parking has also been accommodated internally.

Investigations have been made with TasWater to accommodate an existing sewer main currently located along the rear boundary of the property, to within the proposed development.

From ground floor to Level 3 the proposed building is setback approximately 2.8m from the frontage, with respect to the neighbouring building located to the south. The design of the front façade is articulated with window openings reflecting the proportions and rhythm of the adjoining heritage buildings, expressed in a contemporary manner.

Level 3 is setback approximately 6.7m from the frontage to accommodate a green roof whilst also creating a podium to reference the scale of surrounding buildings. Apartments on the lower levels are generally single bedroom apartments with open space predominantly orientated to the north.

Levels 4-9 are setback 15m from the frontage and accommodate 2 bedrooms, generally with a larger footprint and a relatively larger open space allocation. The floor plate is divided with interstices to provide separation between apartments and to also create greater distinction and articulation in the building form, which is reflected in the upper floor plans. Levels 10 is setback 30m from the street frontage and provides a 3 bedroom apartment.

The treatment of the building facades above Level 4 is distinguished from the lower podium. The design, finish and materials are clearly contemporary, while also still integrated with the treatment of the lower level and have been articulated for greater visual interest and to minimise the expansive blank facades.

The facades are cognisant of their location within the broader city, and have used spaces and colour to layer the overall massing and form of the components of the building.

3. PLANNING SCHEME REQUIREMENTS

The following is an assessment of the proposal in response to the standards of the *Hobart Interim Planning Scheme 2015* (HIPS 2015).

The site is located within the Central Business Zone of the HIPS 2015, as shown in the figure below. The site is within the Central Business Core Area and is not located on a Solar Penetration Priority Street.

Figure 7 describes the subject site within the Central Business Zone (blue).

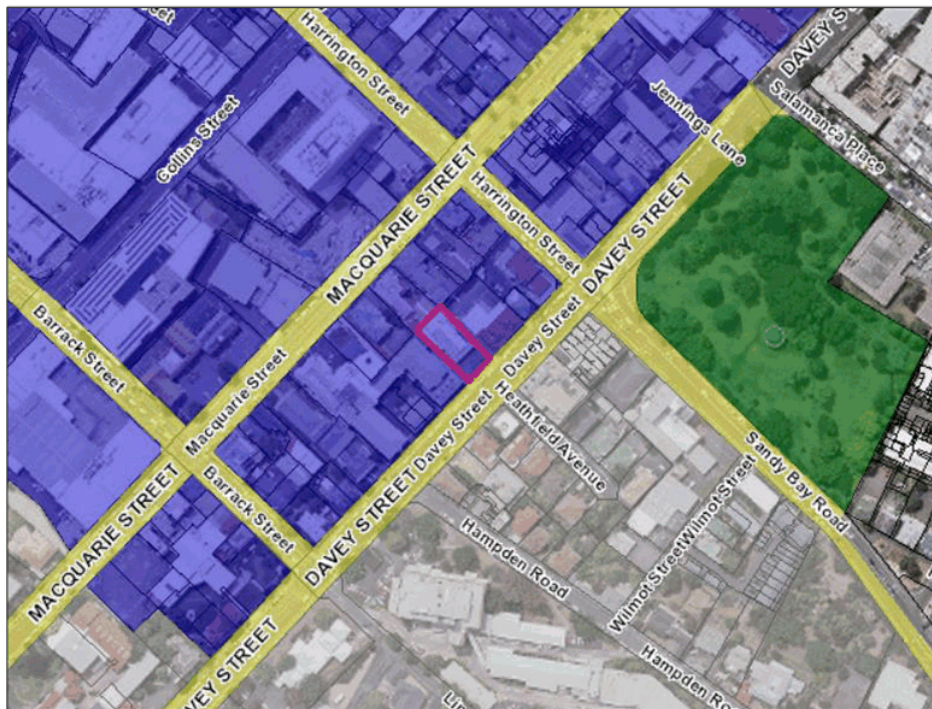


Figure 7: Site Zoning (source: www.thelist.tas.gov.au © State of Tasmania).

3.1 CENTRAL BUSINESS ZONE

3.1.1 ZONE PURPOSE STATEMENTS

The purpose statements of the Central Business Zone are as follows with respective responses to the proposed development.

22.1.1.1 - To provide for business, civic and cultural, community, food, hotel, professional, retail and tourist functions within a major centre serving the region or sub-region.

The proposal is for apartments that will be used for residential and student accommodation purposes. The proposal will contribute to the vibrancy of the city by providing for 24hr presence. Whilst the ground floor use providing building and car park access is discretionary, the lobby and services are provided on the first floor.

22.1.1.2 - To maintain and strengthen Hobart's Central Business District and immediate surrounds including, the waterfront, as the primary activity centre for Tasmania, the Southern Region and the Greater Hobart metropolitan area with a comprehensive range of and highest order of retail, commercial, administrative, community, cultural, employment areas and nodes, and entertainment activities provided.

The proposal will provide residential and student accommodation in a location that is well connected to the service functions of the CBD, with good connections to the amenities of Sullivan's Cove and St David's Park and on the edge of the Urban Mixed Use Zone. The design of apartments will provide a variety of accommodation with a level of amenity and quality that is responsive to the State's capital city.

22.1.1.3 - To provide a safe, comfortable and pleasant environment for workers, residents and visitors through the provision of high quality urban spaces and urban design.

The relationship that the building has to the street frontage has been given a considerable degree of attention to ensure that urban design cues are taken from the historic values of the neighbouring buildings. The façade will fill the existing void within the streetscape with high quality finishes and contemporary design that integrates with the existing form of the street.

22.1.1.4 - To facilitate high density residential development and visitor accommodation within the activity centre above ground floor level and surrounding the core commercial activity centre.

The proposal is highly consistent with this statement.

22.1.1.5 - To ensure development is accessible by public transport, walking and cycling.

The site is well connected to a variety of transport options. Parking within the building is limited in acknowledgement of the site's location in proximity to open space, services, entertainment, and employment.

22.1.1.6 - To encourage intense activity at pedestrian levels with shop windows offering interest and activity to pedestrians.

Davey Street is not traditionally an active retail area of the city, due to the existing form of historic masonry buildings with small openings and formal facades; and the high vehicle traffic of the main arterial way. Notwithstanding this, the proposal provides a frontage that is respectful of the existing qualities of the heritage precinct with detailing at a pedestrian scale. The street setback and landscaped area have been included within the design with respect to the heritage qualities and to enhance the street level experience.

22.1.1.7 - To encourage a network of arcades and through-site links characterised by bright shop windows, displays and activities and maintain and enhance Elizabeth Street Mall and links to it as the major pedestrian hub of the CBD.

The location of the site and the enclosed nature of existing development on adjoining sites precludes the ability to provide through site linkages.

22.1.1.8 - To respect the unique character of the Hobart CBD and maintain the streetscape and townscape contribution of places of historic cultural heritage significance.

The proposed development has been designed with regard to the adjoining heritage buildings and the qualities of the heritage precinct in which the site is located. The proposal responds by creating

a complimentary street edge condition to complete the streetscape form, the recessive use of materials and the siting of larger parts of the building well setback from the principal view lines.

A heritage report is currently being prepared to provide further analysis to outline how the proposed development maintains and contributes to the streetscape.

22.1.1.9 - To provide a safe, comfortable and enjoyable environment for workers, residents and visitors through the provision of high quality spaces and urban design.

The proposal has been designed within the limitations presented by the site. The building is in close proximity to St. David's Park and Salamanca Place which provide high quality urban spaces. Residents are provided with ample private open space and the proposal is in close proximity to key sites within the CBD and Sullivan's Cove and provides a safe, comfortable and enjoyable environment for residents.

3.1.2 DESIRED FUTURE CHARACTER STATEMENTS

Consideration of the Desired Future Character Statements (DFCS) is triggered in relation to standard 22.4.2 Setback P1 (a).

Townscape and Streetscape Character -

22.1.3.1 Objectives:

(a) That the Central Business Zone provides a compact built focus to the region, reflecting an appropriate intensity in its role as the heart of settlement.

The proposed development reinforces the role of the zone as a focus to the region with development that is at a scale consistent with existing development within the city centre.

(b) That the Central Business Zone develops in a way that reinforces the layered landform rise back from the waterfront, having regard to the distinct layers of the landform, respecting the urban amphitheatre, including the amphitheatre to the Cove, while providing a reduction in scale to the Queens Domain, the Domain and Battery Point headlands and the natural rise to Barracks Hill (see Figures 22.7 and 22.8).

(c) That the Central Business Zone consolidates within, and provides a transition in scale from, its intense focus in the basin, acknowledging also the change in contour along the Macquarie Ridge, including both its rising and diminishing grades, including to the low point of the amphitheatre to the Cove (see Figures 22.7, 22.8 and 22.9).

The proposed development is reflective of the underlying natural rise of the landform to Macquarie Street Ridge by stepping up along the cove slope to accentuate the Macquarie ridge, and the fall towards the amphitheatre of the cove. The positioning of the podium and tower form at the rear of the lot will consolidate the existing emphasis of development on Macquarie street on the surrounds of the basin, while the stepping down in a transition towards the Cove amphitheatre.

A further analysis of the urban form is contained within the Urban Form Supporting Statement.

(d) That the historic cultural heritage values of places and precincts in the Central Business Zone be protected and enhanced in recognition of the significant benefits they bring to the economic, social and cultural value of the City as a whole.

The setback of the proposed building reinforces the existing setback pattern of existing heritage buildings evident within the streetscape. The proposed building will bring substantial social and economic benefits to the CBD by providing much needed residential apartments and studio/serviced apartments.

The historic cultural heritage values of the precinct are articulated in the Statements of Significance for the City Centre H1 Heritage Precinct as follows:

H1 - City Centre

This precinct is significant for reasons including:

- 1. It contains some of the most significant groups of early Colonial architecture in Australia with original external detailing, finishes and materials demonstrating a very high degree of integrity, distinctive and outstanding visual and streetscape qualities.*
- 2. The collection of Colonial, and Victorian buildings exemplify the economic boom period of the early to mid nineteenth century.*
- 3. The continuous two and three storey finely detailed buildings contribute to a uniformity of scale and quality of street space.*
- 4. It contains a large number of landmark residential and institutional buildings that are of national importance.*
- 5. The original and/or significant external detailing, finishes and materials demonstrating a high degree of importance.*

The proposal complements and enhances the heritage characteristics of the precinct, by reintroducing a 3 storey facade with a hard edge to the street, demonstrating a level of consistency with that evident within the streetscape and wider precinct.



Figure 8: Existing streetscape illustrating the substantial void created by the current conditions on-site (source: JAWS Architects)



AFTER: Davey Street

Figure 9: Proposed building form and introduction of a contemporary façade to the streetscape to reinforce the contiguous 2-3 storey heritage properties along Davey Street (source: JAWS Architects)

A further analysis will be provided under the Heritage Code.

22.1.3.2 Building Siting, Bulk and Design

The siting, bulk and design of a building above the street wall and beyond the Amenity Building Envelope (see Figure 22.3) must be consistent with the objectives in clause 22.1.3.1, having regard to:...

These statements are not applicable as the proposed development is within the Amenity Building Envelope.

3.1.3 USE STATUS

The proposed development is for a mix of residential and student accommodation. Advice from Council indicates that student accommodation is also considered residential (communal residence). The residential use class is defined as follows:

Residential

use of land for self contained or shared living accommodation. Examples include an ancillary dwelling, boarding house, communal residence, home-based business, hostel, residential aged care home, residential college, respite centre, retirement village and single or multiple dwellings.

Residential use is a permitted use in the zone, provided it is above ground floor.

3.1.4 USE STANDARDS

The only applicable Use Standards is as follows:

Clause 22.3.2 - Noise

Objective: To ensure that noise emissions do not cause environmental harm and do not have unreasonable impact on residential amenity on land within a residential zone.

A1 - Noise emissions measured at the boundary of a residential zone must not exceed the following:

- (a) 55dB(A) (LAeq) between the hours of 7.00 am to 7.00 pm;
- (b) 5dB(A) above the background (LA90) level or 40dB(A) (LAeq), whichever is the lower, between the hours of 7.00 pm to 7.00 am;
- (c) 65dB(A) (LAm_{ax}) at any time.

Measurement of noise levels must be in accordance with the methods in the *Tasmanian Noise Measurement Procedures Manual*, issued by the Director of Environmental Management, including adjustment of noise levels for tonality and impulsiveness. Noise levels are to be averaged over a 15 minute time interval.

P1 - Noise emissions measured at the boundary of a residential zone must not cause environmental harm within the residential zone.

The nearest residential zone is approximately 350m to the northwest of the site and is therefore unlikely to be impacted by any noise generated by the development. It is unlikely that the residential nature of the use will generate excessive noise.

3.1.5 DEVELOPMENT STANDARDS

The figure and relevant development standards of the zone are discussed below.

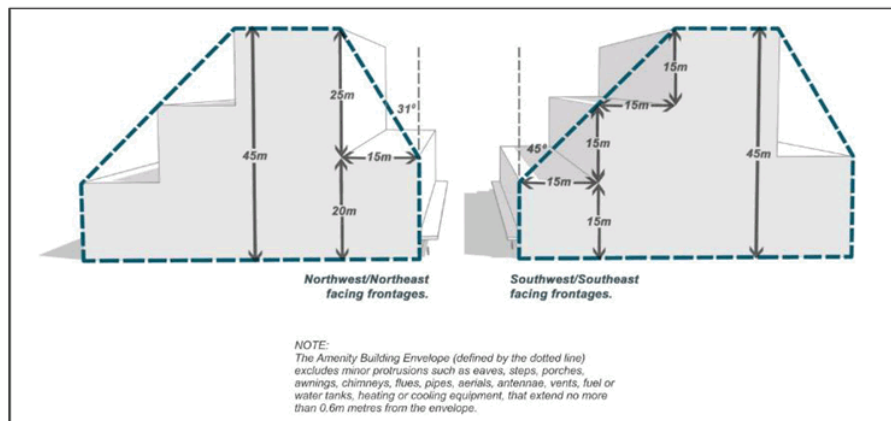


Figure 10: Amenity Building Envelope (Figure 22.3, HIPS 2015)

Clause 22.4.1 - Building Height

Objective: To ensure that building height contributes positively to the streetscape and does not result in unreasonable impact on residential amenity of land in a residential zone.

A1 - Building height within the Central Business Core Area in Figure 22.2 must be no more than:

- (a) 15m if on, or within 15m of, a south-west or south-east facing frontage;
 - (b) 20m if on, or within 15m of, a north-west or north-east facing frontage;
 - (c) 30m if set back more than 15m from a frontage;
- unless an extension to an existing building that:
- (i) is necessary solely to provide access, toilets, or other facilities for people with disabilities;
 - (ii) is necessary to provide facilities required by other legislation or regulation.

P1.1 - Development contained within the Amenity Building Envelope in Figure 22.3 must make a positive contribution to the streetscape and townscape, having regard to:

- (a) the height, bulk and design of existing and proposed buildings;
- (b) the need to minimise unreasonable impacts on the view lines and view cones in Figure 22.6 and on the landform horizons to kunanyi/ Mt Wellington and the Wellington Range from public spaces within the Central Business Zone and the Cove Floor;
- (c) the need to minimise unreasonable impacts on pedestrian amenity from overshadowing of the public footpath for city blocks with frontage to a Solar Penetration Priority Street in Figure 22.2; and
- (d) the need to minimise unreasonable impacts on the amenity of public open space from overshadowing.

Response

The proposed building has a south-east facing frontage and sits at a height of approximately 14.6m, then setback 15m and extends to a height of approximately 30m before being setback a further 15m to extend to a maximum height of approximately 35m from NGL. The proposal complies with A1(a) but does not comply with (c) and is required to be assessed against the performance criteria.

P1.1 Proposed development is contained within the Amenity Building Envelope, as shown in the following figure.

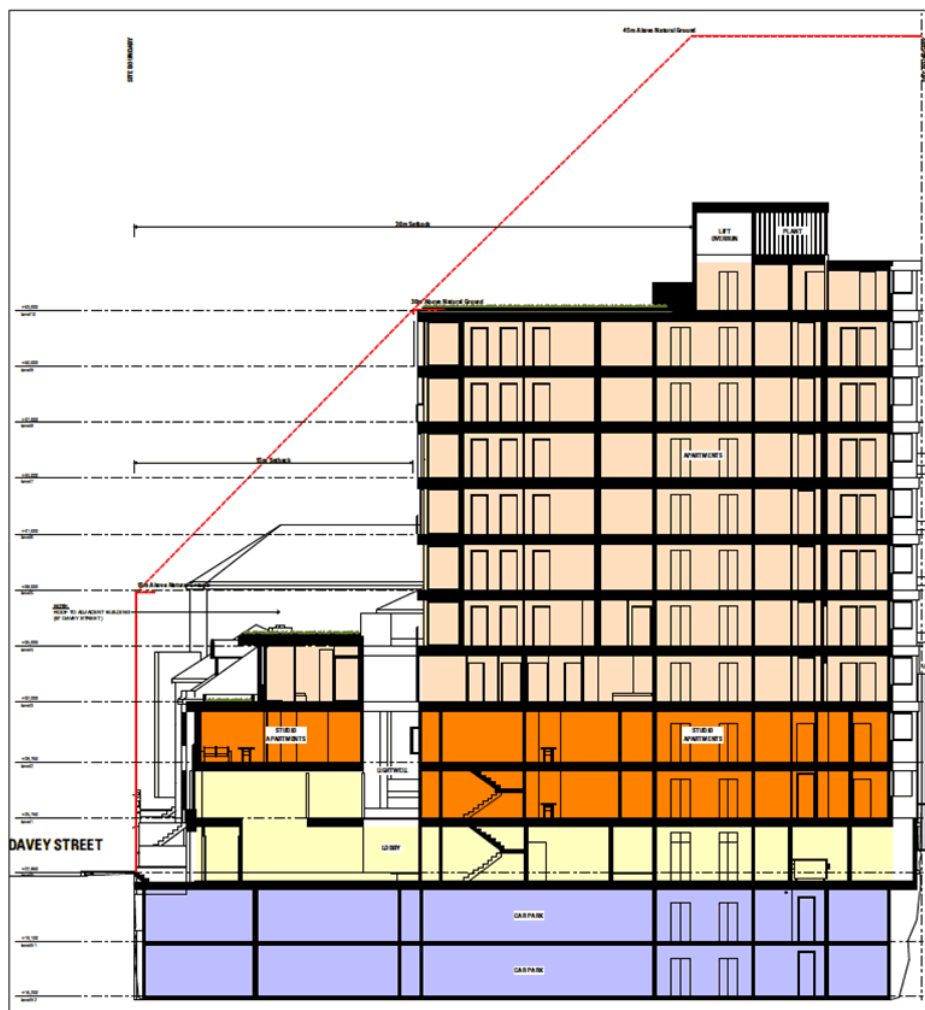


Figure 11: Eastern elevation illustrating the permitted building envelope (source: JAWS Architects)

The red line in the figure above illustrates the staggered offsets as specified by the amenity building envelope.

(a) The frontage of the building has been designed to respect the front façade scale and setback of adjacent heritage buildings. The resulting building will fill the existing void within the street, thereby improving the existing setback pattern and positively contributing the streetscape.

The building form steps up at the rear of the site with respect to the underlying topography to an overall building height of 34m with an RL of 56.6. The height and form recognise the Macquarie Ridge to the north west of the site and the scale of development within the surrounding townscape in which it is situated.

Buildings within proximity to the subject site that inform the overall townscape include the Lands Building, 144 Macquarie Street (BH=51.4m); Ibis Hotel, 173 Macquarie Street, Leisure Inn 167-169 Macquarie Street (11 storey); and Commonwealth Government Centre, 188 Collins Street (BH=58m).

With the underlying topography contributing to the final relative height of these buildings the surrounding building heights are significantly higher than that proposed.

These buildings are relevant in this analysis as the proposed building should not be considered in isolation. Both streetscape and townscape are defined terms in the scheme which, as per the definitions below, facilitate interpretation and analysis of surrounding built forms generally within 100m of the site.

Townscape

"The urban form of the city and the visual quality of its appearance, it includes the urban landscape and visual environment of the city. As a concept it strives to give order to the form of the city, the pattern of landscape and development of the urban landscape."

Townscape is a far broader term, which allows consideration of the characteristics of the city as a whole, rather than focusing on a singular space or precinct.

Streetscape

"the visual quality of a street depicted by road width, street planting, characteristics and features, public utilities constructed within the road reserve, the setbacks of buildings and structures from the lot boundaries, the quality, scale, bulk and design of buildings and structures fronting the road reserve."

Streetscape is a more refined term which is generally considered within a 100m radius of the street and surrounding buildings. The following diagram illustrates the extent of buildings within a 100m radii of the site.

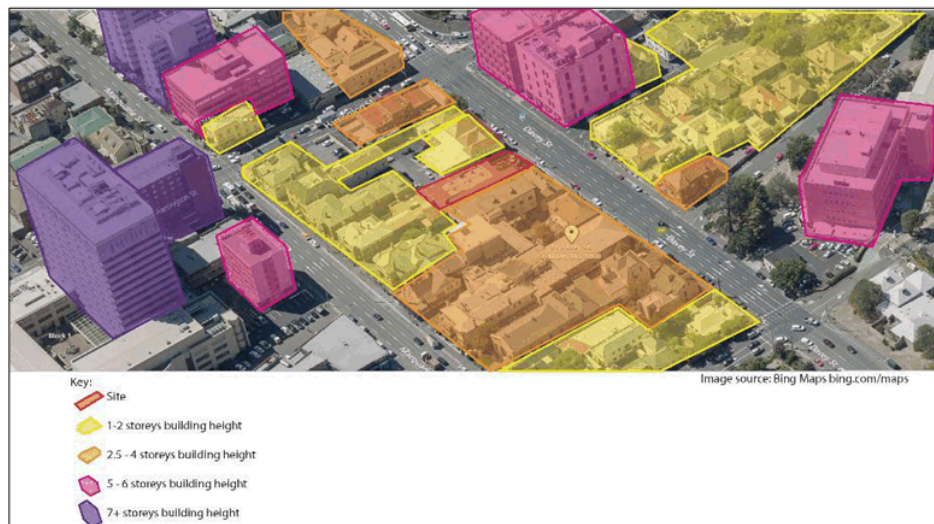


Figure 12: Indication of building heights in urban context (source: Bing Maps - adapted by Ireneinc)

The figure above illustrates the variable building heights within the block bounded by Davey, Macquarie, Harrington and Barrack Streets. The building forms outside of this block are also relevant given the way in which the townscape can be interpreted from various points around the CBD. The primary consideration however is how the overall form, bulk and scale of the building fits within the immediate surrounds and the streetscape.

The following figure details how the proposal sits within the streetscape along Davey Street and when viewed from the corner of Sandy Bay Road and Davey Street.

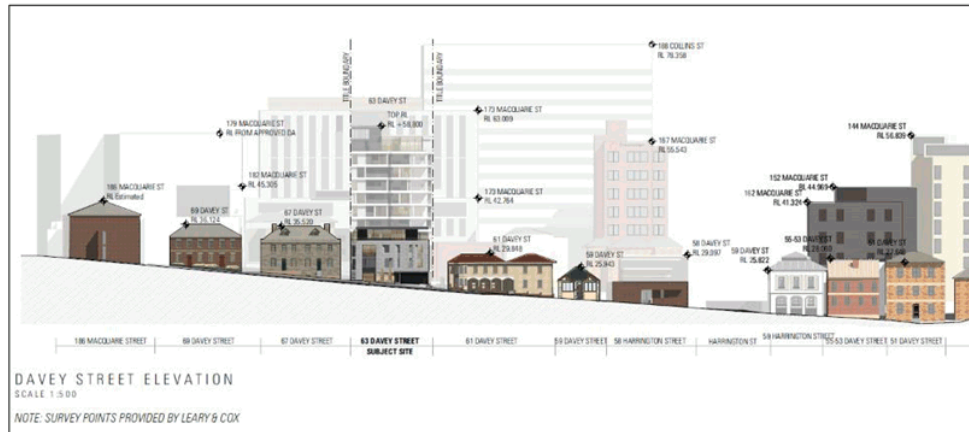


Figure 13: Davey Street streetscape, illustrating surrounding buildings and variable heights (source: JAWS Architects)

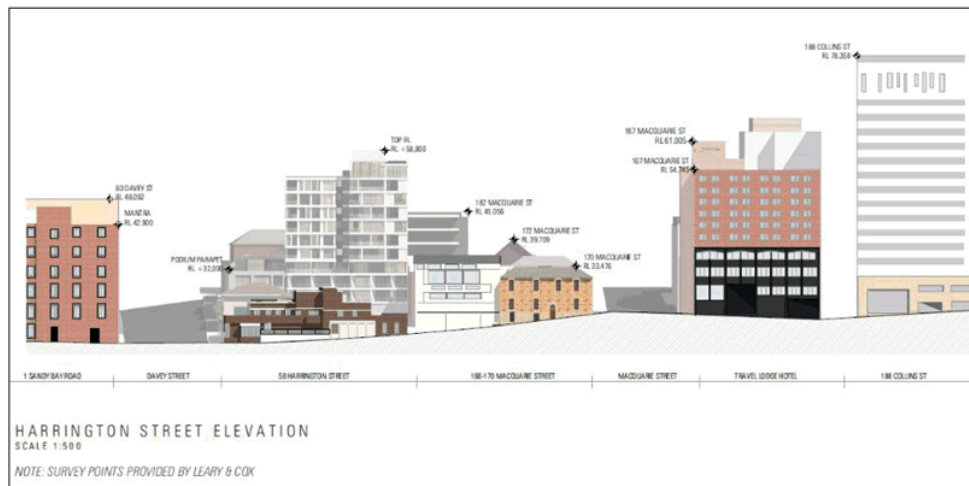


Figure 14: Harrington Street streetscape (source: JAWS Architects)

When viewed in context with surrounding development, it is clear that the consideration of height, bulk and scale of the building requires a wider context to understand the variations in building form within the CBD.

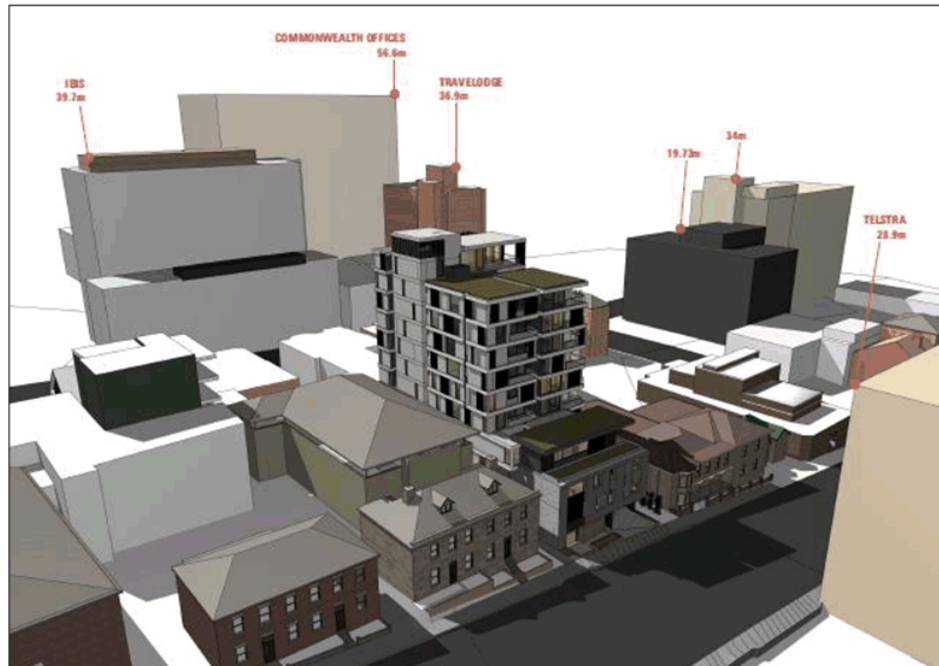


Figure 15: Northern aerial view of proposed development (source: JAWS Architects)

It is acknowledged that during the appeal proceedings for the proposal at 58 Harrington Street (Hexa Pacific Pty Ltd v Hobart City Council & Ors (2019) TASRMPAT) , the overall height, bulk and scale of the building was considered appropriate by Council Planners, despite being substantially higher at approximately 45m.

(b) The subject site is not located within the view cones mapped in Figure 22.6, as identified below:

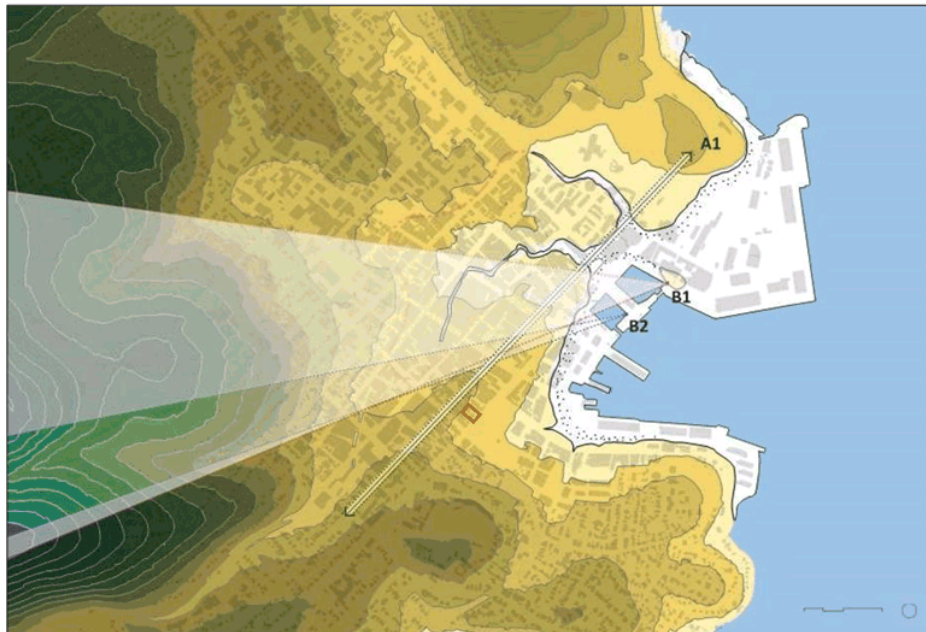


Figure 16: view lines and view cones as specified under Figure 22.6 HIPS, 2015 and site location (red) (source: HIPS, 2015)

(c) the proposal does not front a solar penetration priority street.

(d) The nearest public open space to the site is St David's Park, which is located to the north east of the subject site and will not be unreasonably impacted by overshadowing.

The question of height, scale and overall bulk has been addressed further under Acceptable Solution A5 and within the responses to the Heritage Code. A more detailed assessment of the surrounding urban form and analysis of the block bounded by Davey and Macquarie Streets is provided in the accompanying Urban Design Report.

A5 - Building height of development within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

(a) not exceed 1 storey or 4m (whichever is the lesser) higher than the facade building height of a heritage building on the same street frontage (refer figure 22.5 ii); and

(b) not exceed the facade building height of the higher heritage building on the same street frontage if the development is between two heritage places (refer figure 22.5 ii);

or

(c) comply with the building height in Clauses 22.4.1 A1 and A2; whichever is the lesser.

P5 - Building height within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

(a) not unreasonably dominate existing buildings of cultural heritage significance; and

(b) not have a materially adverse impact on the historic cultural heritage significance of the heritage place;

(c) for city blocks with frontage to a Solar Penetration Priority Street in Figure 22.2, not exceed the Amenity Building Envelope illustrated in Figure 22.3, unless it can be demonstrated that the overshadowing of the public footpath on the opposite side of the Solar Penetration Priority Street does not unreasonably impact on pedestrian amenity.

Response

A5 - The building is located between two lots which are listed in the Historic Heritage Code; 'St Helens Private Hospital' at 186 Macquarie Street to the south west, and 'RAAF Association Memorial Centre' at 61 Davey Street.

The proposed building, within 15m of the frontage, does not exceed the façade height of the higher heritage building at 186 Macquarie Street, however the proposed 4 storey façade within 15m of the frontage exceeds the two storey façade of 61 Davey Street by more than 4m or 1 storey.

The proposal is assessed in response to the Performance Criteria.

P5 -

(a) As mentioned above, the façade of the building has been designed specifically to respond to the dominant characteristics of the streetscape, being 3-4 storey and built within 2-3m of the frontage.

The larger building form is setback 30m from the frontage, ensuring that the streetscape along Davey Street is maintained and the adjoining heritage buildings to the west and east along Davey Street are not dominated by the larger building form behind. This is clearly illustrated in the accompanying photo montages and 3D renders. Of specific relevance are the following renders:



Figure 17: Existing view from junction of Davey Street and Sandy Bay Road.



Figure 18: Proposed building when viewed from the junction between Davey Street and Sandy Bay Road (source: JAWS Architects)

The setback of the larger form from the street provides a substantial distinction from the streetscape, reducing the overall prominence of the building when experienced from street level, where the primary façade becomes the defining image of the development.

The proposed building does not unreasonably dominate the heritage precinct as the larger form is respectfully setback from the streetscape, allowing the dominant 2-3 storey heritage facades to remain as the defining feature of the precinct. The façade of the proposed development contributes to this feature by replacing the existing void with a sympathetic but wholly contemporary façade which respects the characteristics of the adjoining buildings through materials, fenestration and colour.

The overall design and fenestration of the façade is identifiable as new a building form and rather than replicate the historical traits of the adjoining buildings, the proposal reintroduces a building façade to the streetscape with the design elements, colours and materials ensuring that the building fits appropriately within the heritage context whilst presenting as contemporary form.

(b) The primary façade facing the street responds appropriately to the adjoining heritage buildings by presenting at a similar height, whilst also responding to the topography of Davey Street. As outlined in the accompanying architectural statement:

The podium fills the existing void within the streetscape, creating a complimentary street edge condition with high quality finishes and contemporary design. The removal of the open asphalt carpark and insertion of a new respectful active building element helps to repair the fabric of the streetscape, enhancing the street level experience.

The articulation of the front façade with windows openings references the symmetry, proportions and rhythm of the adjoining heritage buildings, expressed in a contemporary manner.

The overall form, materials, colours ensure that the building sits appropriately within the streetscape and allows the primary historic buildings to retain their prominence within the streetscape.



Figure 19: Elevation view montage along Davey Street (source: JAWS Architects)

(c) The site does not front any solar penetrations priority streets and does not exceed the amenity building envelope illustrated in figure 22.3 of the scheme and in figure 7 of this report.

Clause 22.4.2 - Setback

Objective: To ensure that building setback contributes positively to the streetscape and does not result in unreasonable impact on residential amenity of land in a residential zone.

A1 - Building setback from frontage must be parallel to the frontage and must be no more than:

0m

P1 - Building setback from frontage must satisfy all of the following:

- (a) be consistent with any Desired Future Character Statements provided for the area;
- (b) be compatible with the setback of adjoining buildings, generally maintaining a continuous building line if evident in the streetscape;
- (c) enhance the characteristics of the site, adjoining lots and the streetscape;
- (d) provide for small variations in building alignment only where appropriate to break up long building facades, provided that no potential concealment or entrapment opportunity is created;
- (e) provide for large variations in building alignment only where appropriate to provide for a forecourt for space for public use, such as outdoor dining or landscaping, provided that no potential concealment or entrapment opportunity is created and the forecourt is afforded very good passive surveillance.

Response

The landscaping and associated walls and access are setback 0m from the frontage, however the façade of the building is setback approximately 2.7m from the frontage.

Therefore, the performance criteria must be addressed.

P1

(a) The Desired Future Character Statements have been responded to in section 3.1.1 of this report and the development is found to be consistent with them.

(b) The proposal is setback approximately 2.7m from the frontage to allow for access to the site and provide landscaping. This setback and associated landscaping is consistent with adjoining buildings along Davey Street and is to ensure a continuous building line with respect to the requirements of the Heritage Precinct in which the site is located.

(c) The current building on the site is setback approximately 12.7m from the frontage, with the area in front of the building used for vehicle parking. The proposal will improve the existing characteristics of the streetscape by filling the void created by the existing setback. Landscaping and treatment of the front building will be more consistent with adjoining lots and will greatly improve the appearance of the site from the existing condition.

(d) The building alignment is parallel with the existing frontage with little opportunity for entrapment spaces.

(e) No large variations to the building alignment is proposed. The building will serve to reinforce the existing streetscape pattern of 1-2 storey building facades to the frontage which is a substantial improvement over existing.

Clause 22.4.3 - Design

Objective: To ensure that building design contributes positively to the streetscape, the amenity and safety of the public and adjoining land in a residential zone.

A1 - Building design must comply with all of the following:

- (a) provide the main pedestrian entrance to the building so that it is clearly visible from the road or publicly accessible areas on the site;*
- (b) for new building or alterations to an existing facade provide windows and door openings at ground floor level in the front façade no less than 40% of the surface area of the ground floor level facade ;*
- (c) for new building or alterations to an existing facade ensure any single expanse of blank wall in the ground level front façade and facades facing other public spaces is not greater than 30% of the length of the facade;*
- (d) screen mechanical plant and miscellaneous equipment such as heat pumps, air conditioning units, switchboards, hot water units or similar from view from the street and other public spaces;*
- (e) incorporate roof-top service infrastructure, including service plants and lift structures, within the design of the roof;*
- (f) not include security shutters over windows or doors with a frontage to a street or public place;*

Response

The proposal responds to the acceptable solution as follows:

- (a) The main entrance to the building is clearly visible from Davey Street.
- (b) The ground floor façade is comprised of openings with a surface area in excess of 40%.
- (c) there is no single expanse of a blank wall greater than 30% on the front façade
- (d) Detailed location of plant is to be finalised but is capable of being accommodated where it is screened from the street and public places.
- (e) Lift over run is incorporated into the building through the design of the roof. Further services are located in the basement levels.
- (f) no security shutters over windows or doors with a frontage to a street are included in the proposal.

A2 - Walls of a building facing a residential zone must be coloured using colours with a light reflectance value not greater than 40 percent.

Response

The nearest residential zone is approximately 349m to the north-west of the development site, as such the provision does not apply.

A3 - The facade of buildings constructed within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

- (a) include building articulation to avoid a flat facade appearance through evident horizontal and vertical lines achieved by setbacks, fenestration alignment, design elements, or the outward expression of floor levels; and*
- (b) have any proposed awnings the same height from street level as any awnings of the adjacent heritage building.*

Response

A3 - The proposal adjoins heritage places to the east and west along Davey Street.

- (a) The building façade within 15m of the frontage has been articulated with horizontal and vertical lines as distinguished in the design of building elements, including finishes, windows, and openings as illustrated in the street front elevation.

As outlined in the architectural statement:

The podium fills the existing void within the streetscape, creating a complimentary street edge condition with high quality finishes and contemporary design. The removal of the open asphalt carpark and insertion of a new respectful active building element helps to repair the fabric of the streetscape, enhancing the street level experience.

The design of the front façade is articulated with window openings that reference the symmetry, proportions and rhythm of the adjoining heritage buildings, expressed in a contemporary manner.

Street trees at the front of the building further reference the adjacent buildings, with species of plantings adding another layer of connection along the streetscape.

- (b) no awnings are proposed.

Clause 22.4.4 - Passive Surveillance

Objective: To ensure that building design provides for the safety of the public

A1 - Building design must comply with all of the following:

- (a) provide the main pedestrian entrance to the building so that it is clearly visible from the road or publicly accessible areas on the site;*
- (b) for new buildings or alterations to an existing facade provide windows and door openings at ground floor level in the front facade which amount to no less than 40 % of the surface area of the ground floor level facade;*
- (c) for new buildings or alterations to an existing facade provide windows and door openings at ground floor level in the facade of any wall which faces a public space or a car park which amount to no less than 30% of the surface area of the ground floor level facade;*
- (d) avoid creating entrapment spaces around the building site, such as concealed alcoves near public spaces;*
- (e) provide external lighting to illuminate car parking areas and pathways;*
- (f) provide well-lit public access at the ground floor level from any external car park.*

Response

The proposal addresses the Acceptable Solution as follows:

A1 (a) the main pedestrian entrance to the buildings is clearly visible from Davey Street.

(b) the ground level facade of the building exceeds 40% windows and openings.

(c) Openings on the ground floor facade exceed 30% of the frontage.

(d) The design of the building does not create any entrapment spaces.

(e) & (f) no external car parking areas or pathways are proposed. Car parking will be located within the basement levels and will be accessed via internal lifts and stair wells. The internal car park will be provided with lighting in accordance with relevant Australian Standard.

As such the proposal is capable of complying with the acceptable solution.

Clause 22.4.5 - Landscaping does not apply.

Clause 22.4.6 - Outdoor Storage Areas; all storage is located internally, this does not apply.

Clause 22.4.7 - Fencing and Clause - 22.4.8 - Pedestrian Links do not apply.

4. CODES

4.1 POTENTIALLY CONTAMINATED LAND CODE

To our knowledge former activities of the subject land have not been used for potentially contaminating land uses. Engineers have advised that the sewer main is downhill of a hospital that may have potentially contaminating activities use, which may have used the infrastructure.

Current development on the land precludes the ability for further investigation of the subject infrastructure.

4.2 ROAD AND RAILWAY ASSETS CODE

The following Use and Development Standards are relevant.

4.2.1 DEVELOPMENT STANDARDS

Clause E5.5.1 - Existing road accesses and junctions

Objective: To ensure that the safety and efficiency of roads is not reduced by increased use of existing accesses and junctions.

A3 - The annual average daily traffic (AADT) of vehicle movements, to and from a site, using an existing access or junction, in an area subject to a speed limit of 60km/h or less, must not increase by more than 20% or 40 vehicle movements per day, whichever is the greater.

P3 - Any increase in vehicle traffic at an existing access or junction in an area subject to a speed limit of 60km/h or less, must be safe and not unreasonably impact on the efficiency of the road, having regard to:

- (a) the increase in traffic caused by the use;*
- (b) the nature of the traffic generated by the use;*
- (c) the nature and efficiency of the access or the junction;*
- (d) the nature and category of the road;*
- (e) the speed limit and traffic flow of the road;*
- (f) any alternative access to a road;*
- (g) the need for the use;*
- (h) any traffic impact assessment; and*
- (i) any written advice received from the road authority.*

The proposal will require the relocation of the existing access to the site and will result in intensification of the use. The proposal is required to respond to the Performance Criteria.

P3

(a) The accompanying TIA identifies that the proposal will provide 42 on-site car parking spaces for residents only, resulting in approximately 4.5 vehicles trips per apartment per day. This number is slightly higher than what is likely to be expected (3.75 trips per apartment per day).

This will result in a total of around 150 vehicle movements per day and approximately 15 movements per hour during peak traffic periods.

(b) The traffic generated by the use will be primarily private residential traffic, which will be turning left out of the site onto the right-hand lane of Davey Street.

(c) & (d) As per the accompanying TIA, the two-way traffic activity generated by the proposal is not considered to result in any impacts on the efficiency of the access or the road. The current access to the site provides access to the rear of St. Helens Private Hospital and the peak hour traffic flows to and from the site are not expected to increase over existing. Davey Street is considered a Category 1 road, and supports a high number of vehicle movements per day. It is not anticipated that the proposal will result in any impacts to the efficiency of the road.

(e) The speed limit along Davey Street is signposted at 50km/hr. The speed of traffic flow along the portion of Davey Street between Harrington Street and Barack Street is governed by the traffic lights at the intersection between Davey and Harrington Street and Sandy Bay Road. As per the TIA, intersections and junctions are considered to reach capacity when the total conflicting approach traffic volumes are around 1,500 vehicles/hour. The conflicting traffic volume at the new driveway will be around half this volume, therefore there will not be an operational issue.

(f) n/a

(g) the proposal will continue to provide access to the rear of St. Helens Private Hospital and is required to provide much needed additional residential and accommodation options within close proximity to the CBD and Salamanca Place.

(h) Please refer to the attached TIA for details.

(i) Due to recent legislative changes, the Davey Street road reserve is now managed by the Department of State Growth.

Clause E5.6.2 - Road accesses and junctions

Objective: To ensure that the safety and efficiency of roads is not reduced by the creation of new accesses and junctions.

A2 - No more than one access providing both entry and exit, or two accesses providing separate entry and exit, to roads in an area subject to a speed limit of 60km/h or less.

P2 - For roads in an area subject to a speed limit of 60km/h or less, accesses and junctions must be safe and not unreasonably impact on the efficiency of the road, having regard to:

- (a) the nature and frequency of the traffic generated by the use;
- (b) the nature of the road;
- (c) the speed limit and traffic flow of the road;
- (d) any alternative access to a road;
- (e) the need for the access or junction;
- (f) any traffic impact assessment; and
- (g) any written advice received from the road authority.

On completion, the proposal will have one new access providing both entry and exit from the site. The existing access to the site falls across two titles, with a right of way located partially within the site at 63 Davey Street. This Right of Way is provided to allow sufficient room for the one-way access to the rear of the St. Helens Private Hospital, which will be retained. The only modifications proposed are to the crossover within the road reserve, as per the accompanying documentation.

As the right of way is for access to the adjoining property (St Helens Private Hospital) the new development will incorporate one new access that will provide entry and exit from the development, thereby complying with A2.

Clause E5.6.4 - Sight distance at accesses, junctions and level crossings

Objective: To ensure that the safety and efficiency of roads is not reduced by the creation of new accesses and junctions.

A1 - Sight distances at:

- (a) an access or junction must comply with the Safe Intersection Sight Distance shown in Table E5.1; and
- (b) rail level crossings ...

The applicable sight distance for streets with a speed limit of 60km/hr or less is 80m in either direction.

As per the accompanying TIA, the rise in topography along Davey Street toward the intersection with Barrack Street provides additional sight distance to the east when exiting the site. Given that movements to and from will be from a one-way street, the only applicable sight distance is that to the east toward the intersection with Harrington Street and Sandy Bay Road. The available sight distance in this direction is over 80m.

A single on-street parking space will be retained between the new access point to the west and the existing access to the west (for St Helens Private Hospital). Given the topographical changes detailed above, the retention of this space will not impact on sight distances for the existing access.

Therefore, the proposal complies with A1(a)

4.3 PARKING AND ACCESS CODE**4.3.1 USE STANDARDS****Clause E6.6.1 - Number of Car Parking Spaces**

Objective: To ensure that:

- (a) there is enough car parking to meet the reasonable needs of all users of a use or development, taking into account the level of parking available on or outside of the land and the access afforded by other modes of transport.
- (b) a use or development does not detract from the amenity of users or the locality by:
 - (i) preventing regular parking overspill;
 - (ii) minimising the impact of car parking on heritage and local character.

A1 - The number of on-site car parking spaces must be:

- (a) no less than and no greater than the number specified in Table E6.1; except if:
 - (i) the site is subject to a parking plan for the area adopted by Council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;
 - (ii) the site is subject to clauses E6.6.5, E6.6.6, E6.6.7, E6.6.8, E6.6.9 or E6.6.10 of this planning scheme.

As the site is subject to clause E6.6.5 in accordance with (a)(ii), A1 does not apply.

Clause E6.6.2 - Number of Accessible Car Parking Spaces for People with a Disability

Objective: To ensure that a use or development provides sufficient accessible car parking for people with a disability.

A1 - Car parking spaces provided for people with a disability must:

- (a) satisfy the relevant provisions of the Building Code of Australia;*
- (b) be incorporated into the overall car park design;*
- (c) be located as close as practicable to the building entrance.*

P1 - No Performance Criteria.

Residential use does not generate a requirement for accessible spaces, however two accessible parking spaces have been provided in accordance with the Australian Standard. The parking spaces have been incorporated into the overall design and located in proximity to lifts to ensure the most practical entrance to the building.

Therefore, complying with A1.

Clause E6.6.3 - Number of Motorcycle Parking Spaces

Objective: To ensure enough motorcycle parking is provided to meet the needs of likely users of a use or development.

A1 - The number of on-site motorcycle parking spaces provided must be at a rate of 1 space to each 20 car parking spaces after the first 19 car parking spaces except if bulky goods sales, (rounded to the nearest whole number). Where an existing use or development is extended or intensified, the additional number of motorcycle parking spaces provided must be calculated on the amount of extension or intensification, provided the existing number of motorcycle parking spaces is not reduced.

Car parking within the CBZ is not required, however the proposal provides 49 car parking spaces, therefore only 1 motorcycle space is required.

2 motorcycle spaces have been provided on Basement levels 1 and 2.

These spaces have been designed in accordance with the Australian Standard and the proposal complies with A1.

Clause E6.6.4 - Number of Bicycle Parking Spaces

Objective: To ensure enough bicycle parking is provided to meet the needs of likely users and by so doing to encourage cycling as a healthy and environmentally friendly mode of transport for commuter, shopping and recreational trips.

A1 - The number of on-site bicycle parking spaces provided must be no less than the number specified in Table E6.2.

As per Table E6.2, residential use does not generate a requirement for bicycle parking.

However, a secure bicycle parking area has been provided within Level 1 to encourage alternate forms of transportation, given the proximity of the site to the CBD.

It is considered that the proposal complies with A1.

Clause E6.6.5 - Number of Car Parking Spaces - Central Business Zone

Objective: To ensure that pedestrian activity generated by retailing, entertainment and multi-storey office uses in the central business district is not compromised through the provision of on-site car parking.

- A1** - (a) No on-site parking is provided; or
 (b) on-site parking is provided at a maximum rate of 1 space per 200m² of gross floor area for commercial uses; or
 (c) on-site parking is provided at a maximum rate of 1 space per dwelling for residential uses; or
 (d) on-site parking is required operationally for an essential public service, including, hospital, police or other emergency service.

P1 - Car parking provision:

- (a) is in the form of a public car parking station provided as part of a development which utilises a major existing access; or
 (b) must not compromise any of the following:
 (i) pedestrian safety, amenity or convenience;
 (ii) the enjoyment of 'al fresco' dining or other outdoor activity;
 (iii) air quality and environmental health;
 (iv) traffic safety.

The proposed development includes 42 residential parking spaces. A1(a) (b) and (d) do not apply, therefore A1(c) applies. As the proposal is providing more than 1 space per residential unit, the performance criteria has been addressed:

P1

(a) n/a

(b) (i) As detailed in the TIA, the proposal and car parking provision is not anticipated to result in any impacts to pedestrian safety or amenity.

(ii) & (iii) As per the accompanying TIA, the proposal will not result in any impacts on any 'al fresco' dining or outdoor activities. The proposed development and car parking will not result in any impacts on air quality or environmental health.

(iv) As detailed in the TIA, the car parking areas and access have been designed in accordance with Australian Standards and are not considered to result in any undue impacts on traffic safety both within the site and on Davey Street.

4.3.2 DEVELOPMENT STANDARDS

Clause E6.7.1 - Number of Vehicular Accesses**Objective:** To ensure that:

- (a) safe and efficient access is provided to all road network users, including, but not limited to: drivers, passengers, pedestrians, and cyclists, by minimising:
 - (i) the number of vehicle access points; and
 - (ii) loss of on-street car parking spaces;
- (b) vehicle access points do not unreasonably detract from the amenity of adjoining land uses;
- (c) vehicle access points do not have a dominating impact on local streetscape and character.

A1 - The number of vehicle access points provided for each road frontage must be no more than 1 or the existing number of vehicle access points, whichever is the greater.

P1 - The number of vehicle access points for each road frontage must be minimised, having regard to all of the following:

- (a) access points must be positioned to minimise the loss of on-street parking and provide, where possible, whole car parking spaces between access points;
- (b) whether the additional access points can be provided without compromising any of the following:
 - (i) pedestrian safety, amenity and convenience;
 - (ii) traffic safety;
 - (iii) residential amenity on adjoining land;
 - (iv) streetscape;
 - (v) cultural heritage values if the site is subject to the Local Historic Heritage Code;
 - (vi) the enjoyment of any 'al fresco' dining or other outdoor activity in the vicinity.

The subject land includes a partial right of way on its frontage for the access for the neighbouring land to the south west. This access point is to be retained and reduced in size and a new access created on the north east side to meet with the recommendations of the Traffic Engineer. The application is required to be assessed in response to the Performance Criteria.

P1

- (a) The proposal will result in the removal of one metered on-street parking space, as detailed in the TIA and accompanying civil documentation. Existing whole parking spaces will remain in place and the removal of one space is only the extent required to ensure safe and efficient access to the site and access point to St. Helens Private Hospital.
- (b) (i) the additional access point is not considered to impact on pedestrian safety, amenity or convenience. As detailed in the accompanying TIA, pedestrians will be able to access the site directly from Davey Street away from the driveway through the entry foyer or passageway adjacent to the driveway.
- (ii) Given that Davey Street is a one-way street, the only consideration to traffic safety is vehicles exiting the site into the right-hand lane. It has been established in the TIA that vehicles will be able to exit the site in a forward direction and the access is considered to be safe and efficient.
- (iii) As adjoining land is not residential, this does not apply.

- (iv) The volume of the proposed vehicle access has been adopted into the form of the building with the entrance setback from the frontage to minimise the impact on the visual qualities of the street.
- (v) The site is located in a heritage precinct but is not a heritage place. The access point will not impact on the values of the precinct as discussed further in response to the standards of the Heritage Code. A detailed assessment of the building with regard to streetscape and cultural heritage has been provided under section 4.5 of this report and the accompanying Heritage Report.
- (vi) not applicable.

A2 - In the Central Business Zone and Particular Purpose Zone 10 (Royal Hobart Hospital) no new vehicular access is provided unless an existing access point is removed.

P2 - In the areas covered by the Active Frontage Overlay (Figure 22.1) and Pedestrian Priority Street Overlay (Figure E6.7.12) and in Particular Purpose Zone 10 any new vehicular access point must not compromise any of the following:

- (a) pedestrian safety, amenity and convenience;*
- (b) traffic safety;*
- (c) streetscape;*
- (d) cultural heritage values if the site is subject to the Historic Heritage Code;*
- (e) the enjoyment of any 'al fresco' dining or other outdoor activity in the vicinity.*

The existing crossover and access to the site services both 63 Davey Street and 186 Macquarie Street, for which there is a benefiting right of way (appurtenant to 186 Macquarie Street).

The siting of the proposed building will preclude the continued use of the access to 63 Davey Street.

Under the current conditions, vehicles entering and exiting 63 Davey Street are required to cross over the burdening right of way and potentially across the adjoining title, as the width of the access to 63 Davey Street is only 3m between the title boundary and the existing brick/concrete wall which frames the driveway and a portion of this 3m is covered by the burdening right of way.

Given the substantial intensification of traffic as a result of the proposed use/development, the existing access to Davey Street would not provide the required width for two-way entry and exit and would not be safe or efficient.

Therefore, whilst the existing crossover and burdening right of way will be retained to ensure continued legal access for 186 Macquarie Street, use of the portion of the access to 63 Davey Street will no longer be feasible from this location, and the existing crossover will be reduced from 6m to 4m. Therefore the existing access point for 63 Davey will be removed, in compliance with the Acceptable Solution A2.

Notwithstanding this, it is our view that P2 provides additional constraints which apply to the Pedestrian Priority Street Overlay, or the Particular Purpose Zone 10, but this does not form a prohibition against new crossovers for all remaining areas within the zone.

Clause E6.7.2 - Design of Vehicular Accesses

Objective: To ensure safe and efficient access for all users, including drivers, passengers, pedestrians and cyclists by locating, designing and constructing vehicle access points safely relative to the road network.

A1 - Design of vehicle access points must comply with all of the following:

- (a) in the case of non-commercial vehicle access; the location, sight distance, width and gradient of an access must be designed and constructed to comply with section 3 - "Access Facilities to Off-street Parking Areas and Queuing Areas" of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking;*
- (b) in the case of commercial vehicle access; the location, sight distance, geometry and gradient of an access must be designed and constructed to comply with all access driveway provisions in section 3 "Access Driveways and Circulation Roadways" of AS2890.2 - 2002 Parking facilities Part 2: Off-street commercial vehicle facilities.*

P1 - Design of vehicle access points must be safe, efficient and convenient, having regard to all of the following:

- (a) avoidance of conflicts between users including vehicles, cyclists and pedestrians;*
- (b) avoidance of unreasonable interference with the flow of traffic on adjoining roads;*
- (c) suitability for the type and volume of traffic likely to be generated by the use or development;*
- (d) ease of accessibility and recognition for users.*

A1 (a) The TIA indicates that based on AS2890.1, the desirable driveway sight distance for the site is 69m for approach vehicle speeds of 50km/hr from a point 2.5m back from the edge of the road and 76m for approach speeds of 55km/hr.

The available sight distances for vehicles exiting the site is over 100m, which complies with the requirements outlined in the Australian Standard. The design of the access is considered compliant with the relevant Australian Standards.

Therefore, the proposal complies with A1(a).

Clause E6.7.3 - Vehicular Passing Areas Along an Access

Objective: *To ensure that:*

- (a) the design and location of access and parking areas creates a safe environment for users by minimising the potential for conflicts involving vehicles, pedestrians and cyclists;*
- (b) use or development does not adversely impact on the safety or efficiency of the road network as a result of delayed turning movements into a site.*

A1 - Vehicular passing areas must:

- (a) be provided if any of the following applies to an access:*
 - (i) it serves more than 5 car parking spaces;*
 - (ii) is more than 30 m long;*
 - (iii) it meets a road serving more than 6000 vehicles per day;*
- (b) be 6 m long, 5.5 m wide, and taper to the width of the driveway;*
- (c) have the first passing area constructed at the kerb;*
- (d) be at intervals of no more than 30 m along the access.*

P1 - Vehicular passing areas must be provided in sufficient number, dimension and siting so that the access is safe, efficient and convenient, having regard to all of the following:

- (a) avoidance of conflicts between users including vehicles, cyclists and pedestrians;*
- (b) avoidance of unreasonable interference with the flow of traffic on adjoining roads;*

- (c) suitability for the type and volume of traffic likely to be generated by the use or development;
- (d) ease of accessibility and recognition for users.

The access to the site is for both entry and exit and allows passing of vehicles within the site, therefore no passing area is required.

Clause E6.7.4 - On-Site Turning

Objective: To ensure safe, efficient and convenient access for all users, including drivers, passengers, pedestrians and cyclists, by generally requiring vehicles to enter and exit in a forward direction.

A1 - On-site turning must be provided to enable vehicles to exit a site in a forward direction, except where the access complies with any of the following:

- (a) it serves no more than two dwelling units;
- (b) it meets a road carrying less than 6000 vehicles per day.

Each parking space and access aisle has been designed in accordance with AS 2890.1 to ensure vehicles can manoeuvre on-site and exit the site in a forward direction. With regard to waste disposal vehicles, the TIA specifies the following:

The collection service could occur from the parking lane with the bins wheeled from the temporary external bin storage area to the back of the garbage truck in the parking lane.

Given that this will occur outside of peak traffic periods, this is deemed to be acceptable.

Therefore, it has been determined that this can be achieved in compliance with AS 2890.2.

Clause E6.7.5 - Layout of Parking Areas

Objective: To ensure that parking areas for cars (including accessible parking spaces), motorcycles and bicycles are located, designed and constructed to enable safe, easy and efficient use.

A1 - The layout of car parking spaces, access aisles, circulation roadways and ramps must be designed and constructed to comply with section 2 "Design of Parking Modules, Circulation Roadways and Ramps" of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking and must have sufficient headroom to comply with clause 5.3 "Headroom" of the same Standard.

The TIA finds that the car parking spaces, access and circulation areas comply with dimension requirements associated with AS2890.1:2004.

Therefore, the proposal complies with A1.

Clause E6.7.9 - Surface Treatment of Parking Areas

Objective: To ensure that parking spaces and vehicle circulation roadways do not detract from the amenity of users, adjoining occupiers or the environment by preventing dust, mud and sediment transport.

A1 - Parking spaces and vehicle circulation roadways must be in accordance with all of the following;

- (a) paved or treated with a durable all-weather pavement where within 75m of a property boundary or a sealed roadway;

(b) drained to an approved stormwater system, unless the road from which access is provided to the property is unsealed.

All parking spaces and vehicle circulation roadways will be treated with appropriate pavements and drain to approved stormwater system as shown on the concept stormwater plan.

The proposal complies with the acceptable solution.

Clause E6.7.7 - Lighting of Parking Areas

Objective: To ensure parking and vehicle circulation roadways and pedestrian paths used outside daylight hours are provided with lighting to a standard which:

- (a) enables easy and efficient use;*
- (b) promotes the safety of users;*
- (c) minimises opportunities for crime or anti-social behaviour; and*
- (d) prevents unreasonable light overspill impacts.*

A1 - Parking and vehicle circulation roadways and pedestrian paths serving 5 or more car parking spaces, used outside daylight hours, must be provided with lighting in accordance with clause 3.1 "Basis of Design" and clause 3.6 "Car Parks" in AS/NZS 1158.3.1:2005 Lighting for roads and public spaces Part 3.1: Pedestrian area (Category P) lighting.

P1 - Parking and vehicle circulation roadways and pedestrian paths used outside daylight hours must be provided with lighting to a standard which satisfies all of the following:

- (a) enables easy and efficient use of the area;*
- (b) minimises potential for conflicts involving pedestrians, cyclists and vehicles;*
- (c) reduces opportunities for crime or anti-social behaviour by supporting passive surveillance and clear sight lines and treating the risk from concealment or entrapment points;*
- (d) prevents unreasonable impact on the amenity of adjoining users through light overspill;*
- (e) is appropriate to the hours of operation of the use.*

Parking and vehicle circulation, roadways and pedestrian paths are provided with lighting that will satisfy the Performance Criteria, if it is not in accordance with the Acceptable Solution.

E6.7.8 - Landscaping of Parking Areas

The site is in the Central Business Zone, no landscaping is required.

Clause E6.7.9 - Design of Motorcycle Parking Areas

Objective: To ensure that motorcycle parking areas are located, designed and constructed to enable safe, easy and efficient use.

A1 - The design of motorcycle parking areas must comply with all of the following:

- (a) be located, designed and constructed to comply with section 2.4.7 "Provision for Motorcycles" of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking;*
- (b) be located within 30 m of the main entrance to the building.*

The proposal includes 2 motorcycle spaces. These parking spaces are capable of being designed and constructed in accordance with AS/NZS 2890.1:2004.

Clause E6.7.10 - Design of Bicycle Parking Facilities

Objective: To encourage cycling as a healthy and environmentally friendly mode of transport for commuter, shopping and recreational trips by providing secure, accessible and convenient bicycle parking spaces.

A1 - The design of bicycle parking facilities must comply with all the following;

- (a) be provided in accordance with the requirements of Table E6.2;
- (b) be located within 30 m of the main entrance to the building.

P1 - The design of bicycle parking facilities must provide safe, obvious and easy access for cyclists, having regard to all of the following:

- (a) minimising the distance from the street to the bicycle parking area;
- (c) providing clear sightlines from the building or the public road to provide adequate passive surveillance of the parking facility and the route from the parking facility to the building;
- (d) avoiding creation of concealment points to minimise the risk.

(a) Although the proposal does not generate a requirement for bicycle parking a lockable bicycle parking area has been provided on the ground floor to encourage alternate transport options.

(b) Bicycle parking is located at the rear of the ground floor and is within 10m of the entrance to the lobby of the building.

The proposal complies with A1.

A2 - The design of bicycle parking spaces must be to the class specified in table 1.1 of AS2890.3-1993 Parking facilities Part 3: Bicycle parking facilities in compliance with section 2 "Design of Parking Facilities" and clauses 3.1 "Security" and 3.3 "Ease of Use" of the same Standard.

P2 - The design of bicycle parking spaces must be sufficient to conveniently, efficiently and safely serve users without conflicting with vehicular or pedestrian movements or the safety of building occupants.

The bicycle parking area is secure and is compliant with A2.

Clause E6.7.11 - Bicycle End Trip Facilities**Objective:**

To ensure that cyclists are provided with adequate end of trip facilities.

A1 - For all new buildings where the use requires the provision of more than 5 bicycle parking spaces for employees under Table E6.2, 1 shower and change room facility must be provided, plus 1 additional shower for each 10 additional employee bicycle spaces thereafter.

The proposal does not generate a requirement for bicycle spaces or end of trip facilities. The office space and reception area are ancillary to the residential and visitor accommodation components. The bicycle parking area is provided for residents and no change facilities are required. The proposal complies with the Acceptable Solution.

Clause E6.7.12 - Siting of Car Parking

Objective: *To ensure that the streetscape, amenity and character of urban areas is not adversely affected by siting of vehicle parking and access facilities.*

A1 - Parking spaces and vehicle turning areas, including garages or covered parking areas in the Inner Residential Zone, Urban Mixed Use Zone, Village Zone, Local Business Zone and General Business Zone must....

This provision does not apply to developments in the Central Business Zone.

A2 - In the Central Business Zone on-site parking at ground level adjacent to a street block frontage must comply with all of the following:

- (a) new vehicular access points are not provided;*
- (b) an active street frontage is retained;*
- (c) parked cars are not visible from the street.*

No parking for the development is proposed on the frontage of the street in accordance with the Acceptable Solution. The existing condition of the site currently has parking on the frontage; this will be removed as part of the proposed development improving the streetscape and ensuring parking will not be visible from the frontage.

Clause E6.7.13 - Facilities for Commercial Vehicles

Objective: *To ensure that facilities for commercial vehicles are provided on site, as appropriate.*

A1 - Commercial vehicle facilities for loading, unloading or maneuvering must be provided on-site in accordance with Australian Standard for Off-street Parking, Part 2: Commercial Vehicle Facilities AS 2890.2:2002, unless:

- (a) the delivery of all inward bound goods is by a single person from a vehicle parked in a dedicated loading zone within 50 m of the site;*
- (b) the use is not primarily dependent on outward delivery of goods from the site.*

As per A1(b), the development is for residential and student accommodation and is therefore not primarily dependent on the outward delivery of goods from the site. Therefore, no onsite commercial vehicle facilities are required.

Waste vehicles will be able to stop within the parking lane outside the site where waste bins can then be wheeled out. The traffic consultant has advised that private waste removal contractors generally undertake waste removal outside of peak traffic periods, between 12am and 7am.

Waste vehicles utilising the on-street parking spaces during these periods is not anticipated to result in any impacts on parking or vehicle movements along Davey Street or from the site.

Please refer to the statement provided by Milan Prodanovic, dated 10th June 2020.

Clause E6.7.14 - Access to a Road

Objective: To ensure that access to the road network is provided appropriately.

A1 - Access to a road must be in accordance with the requirements of the road authority.

P1 - No performance criteria

The road authority for Davey Street is the Department of State Growth. As the application requires changes to the access point within the road reserve and changes to on-street parking consent has been requested as part of this application.

4.4 STORMWATER MANAGEMENT CODE

4.4.1 DEVELOPMENT STANDARDS

Clause E7.7.1 - Stormwater Drainage and Disposal

Objective: To ensure that stormwater quality and quantity is managed appropriately.

A1 - Stormwater from new impervious surfaces must be disposed of by gravity to public stormwater infrastructure.

P1 - Stormwater from new impervious surfaces must be managed by any of the following:

- (a) disposed of on-site with soakage devices having regard to the suitability of the site, the system design and water sensitive urban design principles*
- (b) collected for re-use on the site;*
- (c) disposed of to public stormwater infrastructure via a pump system which is designed, maintained and managed to minimise the risk of failure to the satisfaction of the Council.*

As shown in the concept servicing plans and accompanying Stormwater Report, basement level stormwater is required to be pumped to the existing public stormwater infrastructure. All other impervious areas will be disposed via gravity to public infrastructure. However, due to the pump system for the basement level, the proposal is required to be assessed in relation to the Performance Criteria.

The basement stormwater system proposed is considered capable of meeting P1(c) through a pump system that is capable of being designed, maintained and managed to the satisfaction of Council.

Further detail is provided in the accompanying stormwater report and civil plans.

A2 - A stormwater system for a new development must incorporate water sensitive urban design principles R1 for the treatment and disposal of stormwater if any of the following apply:

- (a) the size of new impervious area is more than 600 m²;*
- (b) new car parking is provided for more than 6 cars;*

(c) a subdivision is for more than 5 lots.

A2 (a) The area of new impervious surfaces is less than 600m².

(b) As discussed in Section 3 of the accompanying Stormwater Report new car parking is underground and does not contribute to the stormwater system on the site due to the parking areas being below ground and not subject to rainwater runoff which is the primary method of hydrocarbons entering the stormwater system.

Refer to attached stormwater report for further detail.

(c) No subdivision is proposed.

Therefore, although the proposal triggers a requirement for WSUD principals, it is not considered that these measures are necessary.

A3 - A minor stormwater drainage system must be designed to comply with all of the following:

(a) be able to accommodate a storm with an ARI of 20 years in the case of non-industrial zoned land and an ARI of 50 years in the case of industrial zoned land, when the land serviced by the system is fully developed;

(b) stormwater runoff will be no greater than pre-existing runoff or any increase can be accommodated within existing or upgraded public stormwater infrastructure.

A3 (a) As per standard stormwater system design the proposal has been designed in accordance with ARI 20.

(b) as discussed in section 2 of the accompanying stormwater report, the proposal will not result in an increase beyond what is considered to be a permissible site discharge for the existing stormwater infrastructure.

A4 - A major stormwater drainage system must be designed to accommodate a storm with an ARI of 100 years.

Not Applicable.

4.5 HISTORIC HERITAGE CODE

The site and existing building are not identified as a heritage place on the HIPS or the Tasmanian Heritage Register. However, the site is located within the H1 Heritage Precinct and is mapped as a Place of Archaeological Potential.

Buildings on either side are registered as a heritage place. The listings are included in the Scheme as follows:

Ref. No.	Name	Street No.	Street/Location	C.T.	General Description
808	'RAAF ASSOCIATION MEMORIAL CENT'	59-61	Davey Street	208274/1	
1890	St Helens Hospital	186	Macquarie Street	110411/1	Original portion (previously known as 184-186 Macquarie Street, also includes that part of the address

					previously known as 65-67 Davey Street)
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The adjacent heritage place at 186 Macquarie Street is also listed on the Tasmanian Heritage Register, as per the following:

Ref. No.	Name	Street No.	Street/Location	C.T.	General Description
2,263	Johnsons Edghill Terrace	186	Macquarie Street	110411/1	



Figure 20: HIPS 2015 & Tasmanian Heritage Register heritage listing (Source: adapted from The LIST)

As the site is within the H1 Heritage Precinct, the following Statement of Historic Cultural Heritage Significance applies:

H1 - City Centre

This precinct is significant for reasons including:

1. It contains some of the most significant groups of early Colonial architecture in Australia with original external detailing, finishes and materials demonstrating a very high degree of integrity, distinctive and outstanding visual and streetscape qualities.
2. The collection of Colonial, and Victorian buildings exemplify the economic boom period of the early to mid nineteenth century.

3. The continuous two and three storey finely detailed buildings contribute to a uniformity of scale and quality of street space.
4. It contains a large number of landmark residential and institutional buildings that are of national importance.
5. The original and/or significant external detailing, finishes and materials demonstrating a high degree of importance.

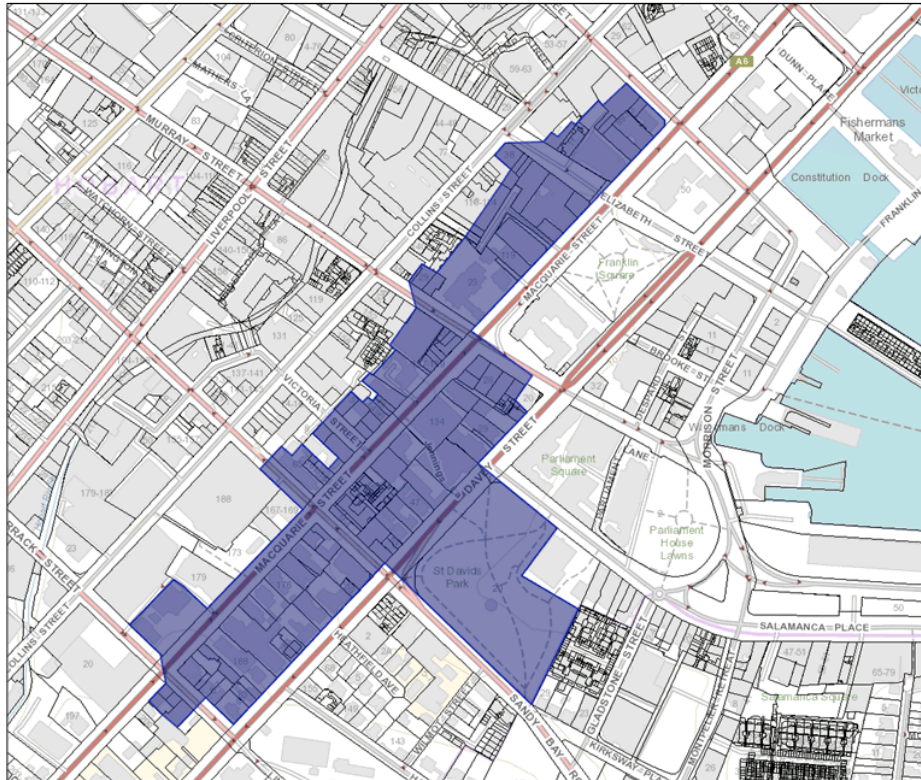


Figure 21: Extents of Heritage Precinct H1 - City Centre (Source: The LIST)

4.5.1 DEVELOPMENT STANDARDS FOR HERITAGE PLACES

The provisions do not apply as the site at 63 Davey Street is not identified on the HIPS 2015 or on the Tasmanian Heritage Register as a heritage place.

4.5.2 DEVELOPMENT STANDARDS FOR HERITAGE PRECINCTS

Clause E13.8.1 - Demolition

Objective: To ensure that demolition in whole or in part of buildings or works within a heritage precinct does not result in the loss of historic cultural heritage values unless there are exceptional circumstances.

A1 - No acceptable solution.

P1 - Demolition must not result in the loss of any of the following:

- (a) buildings or works that contribute to the historic cultural heritage significance of the precinct;*
- (b) fabric or landscape elements, including plants, trees, fences, paths, outbuildings and other items, that contribute to the historic cultural heritage significance of the precinct; unless all of the following apply;*
 - (i) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;*
 - (ii) there are no prudent or feasible alternatives;*
 - (iii) opportunity is created for a replacement building that will be more complementary to the heritage values of the precinct.*

Response

P1

(a) & (b) The existing building on the site is not identified as a heritage place under the HIPS 2015 or under the Tasmanian Heritage Register, and its position in the streetscape with a significant setback, parking area and screened entrance does not contribute to the historic cultural values of the precinct which are identified as:

This precinct is significant for reasons including:

- 1. It contains some of the most significant groups of early Colonial architecture in Australia with original external detailing, finishes and materials demonstrating a very high degree of integrity, distinctive and outstanding visual and streetscape qualities.*
- 2. The collection of Colonial, and Victorian buildings exemplify the economic boom period of the early to mid nineteenth century.*
- 3. The continuous two and three storey finely detailed buildings contribute to a uniformity of scale and quality of street space.*
- 4. It contains a large number of landmark residential and institutional buildings that are of national importance.*
- 5. The original and/or significant external detailing, finishes and materials demonstrating a high degree of importance.*

The removal of the existing building will not result in the removal of any buildings or works that contribute to the historic cultural heritage significance of the site and is not considered to result in any detriment to the character of the precinct. These statements are addressed in detail in the accompanying Heritage Report.

Clause E13.8.2 - Buildings and Works other than Demolition

Objective: *To ensure that development undertaken within a heritage precinct is sympathetic to the character of the precinct.*

A1 - No acceptable solution

P1 - Design of buildings and works must not result in detriment to the historic cultural heritage significance of the precinct, as listed in Table E13.2

As per the accompanying Heritage report, the design of the building has been developed in relation to the precinct values.

The report provides a response to the statements of heritage significance relevant to the precinct. It is considered that the statements do not provide any specific design requirements and that the height, setback and design standards under the Zone are more suitable to determine the suitability of the proposed development.

Notwithstanding the above, the building has been designed to reflect the built form along Davey Street with the podium design, which ensures that the higher portion of the building is set back from the streetscape and forms a recessive element to the two to four storey street edge. The rhythm and scale of penetrations in the building façade, together with the use of recessive materials and the siting of the podium to reinforce the streetscape ensures minimal impacts on the heritage significance of the precinct.

Although the proposed development inherently forms part of the precinct, it will be clearly distinguishable as a new contemporary element, allowing the dominant heritage characteristics to remain prominent at street level. This is illustrated in the figures below.

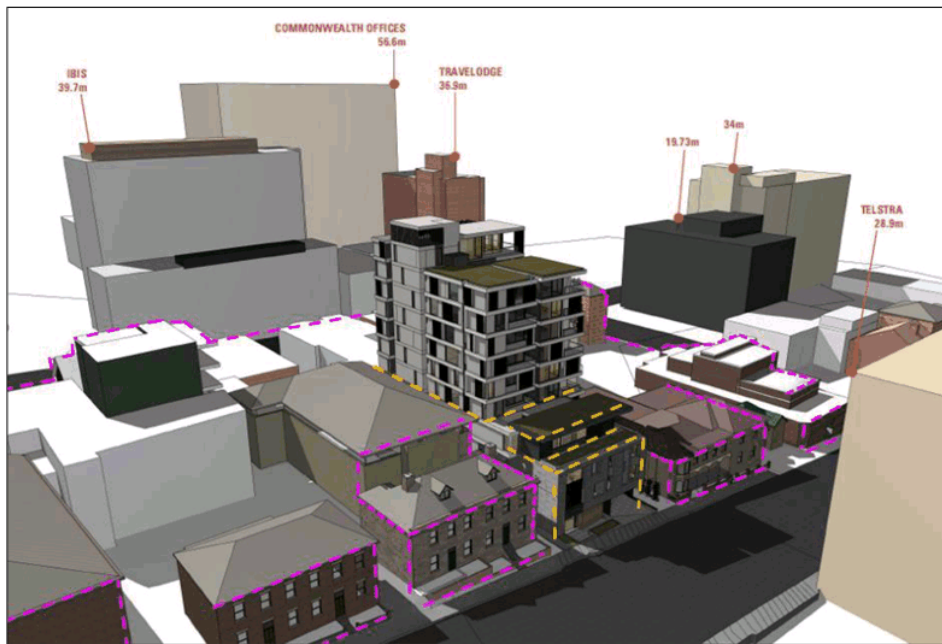


Figure 22: The introduction of the new façade element reinforces the heritage characteristics by reintroducing a key element to the streetscape. The larger form set behind appears distinctly separate to the heritage forms without impacting the way in which the precinct is interpreted from street-level (source: JAWS Architects - adapted by Ireneinc)

The following before and after figures overleaf illustrate how the proposal will be interpreted from street level.



Figure 23: Before and after render of the proposed development - reintroducing a key element to the streetscape (source: JAWS Architects)

It is reasonable to state that the heritage characteristics of the precinct are best experienced at street level. This proposal reintroduces a key element to the streetscape, that being a 3-4 storey façade, replacing an existing void which currently undermines the key elements of the streetscape and significance of the wider precinct.

The larger form is sufficiently setback to ensure no detriment to the experience of the precinct.

A further discussion is provided in the accompanying Heritage Report.

A2 - No acceptable solution

P2 - Design and siting of buildings and works must comply with any relevant design criteria / conservation policy listed in Table E13.2, except if a heritage place of an architectural style different from that characterising the precinct.

Clause E13.2 provides a description of the existing heritage precinct, and building qualities of value, but does not identify any relevant design criteria for new buildings, or conservation policy.

A further discussion is provided in the accompanying Heritage report.

A3 - No acceptable solution

P3 - Extensions to existing buildings must not detract from the historic cultural heritage significance of the precinct.

No extensions are proposed, as the existing building will be removed.

A4 - New front fences and gates must accord with original design, based on photographic, archaeological or other historical evidence.

P4 - New front fences and gates must be sympathetic in design, (including height, form, scale and materials), and setback to the style, period and characteristics of the precinct.

No new front fences or gates are proposed, therefore A3 does not apply.

A5 - Areas of landscaping between a dwelling and the street must be retained

No landscaping is currently provided on the site. The proposal includes a small area of landscaping between the building and the street, which provides a level of consistency with adjoining buildings in the street.

Therefore, complying with A5.

4.5.3 DEVELOPMENT STANDARDS FOR PLACES OF ARCHAEOLOGICAL POTENTIAL

The site is located in an area identified as having potential to contain archaeological remains and the application is required to address the provisions for Places of Archaeological Potential.

Clause E13.10.1 - Building, Works and Demolition

Objective: To ensure that building, works and demolition at a place of archaeological potential is planned and implemented in a manner that seeks to understand, retain, protect, preserve and otherwise appropriately manage significant archaeological evidence.

A1 - Building and works do not involve excavation or ground disturbance.

P1 - Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:

- (a) the nature of the archaeological evidence, either known or predicted;*
- (b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;*
- (c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition;*
- (d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;*
- (e) measures proposed to preserve significant archaeological evidence 'in situ'.*

As the proposed development incorporates two basement levels, there is a requirement to undertake excavation on the site. Therefore, the performance criteria have been assessed.

P1

(a) The accompanying SoHAP provides a detailed analysis of the history of the site and specifies a number of likely historical archaeological remains on the site from previous development. As per Figure 7.1 of the report, there are areas on the site of high archaeological potential.

(b) The SoHAP has recommended that an Archaeological Impact Assessment be undertaken prior to works on site, and if necessary, an archaeological method statement. Council are capable of requesting these documents as part of any subsequent RFI.

However, given the location of the high sensitivity area in the middle of the site, it is unlikely that the remains could be reasonably preserved.

(c) As areas of high archaeological potential cover a large area of the site there is limited opportunity to redevelop the site without impacts on potential archaeology.

(d) As per recommendations of the SoHAP, further archaeological investigations will be undertaken that will include recommendations for appropriate management of evidence for public benefit.

(e) As areas of high archaeological potential cover a large area of the site there is limited opportunity to redevelop the site while retaining archaeological evidence 'in situ'.

4.6 SIGNS CODE

No signage is proposed as part of this application.

5. CONCLUSION

The proposed residential and student accommodation development has been designed in accordance with the relevant scheme provisions of the Hobart Interim Planning Scheme.

There is a demonstrated need for additional residential and student accommodation options within Hobart and the proposal will aid in supplying a mix of apartments within close proximity to the CBD and a number of key social and cultural sites.

The design of the building has been undertaken in respect to the cultural heritage values of the adjoining properties and ensures that the general streetscape character is retained as far as practicable. This is achieved by reducing the building height at the frontage to be in line with the progressive building height of the adjoining buildings to the west and east and with reference to the changing topography of Davey Street.

The proposal has also been carefully designed to ensure that it sits within the current amenity building envelope, thereby reducing any undue impacts from height or bulk that would likely be apparent if the building extended beyond the envelope. The materials and finishes applied to the front façade have been chosen to maintain the general characteristics of the streetscape.

Although car parking is not required within the Central Business Zone, a total of 42 spaces have been provided for residents along with motorcycle and bicycle parking facilities. These amenities have been provided to provide flexibility for residents and guests and to enable alternate transport options.

The proposal will require modifications to the current crossover to the site, to reduce the width so that it is suitable for continued use for 186 Macquarie Street. The access will no longer be used for access to 63 Davey Street. Therefore, a new crossover will replace the access previously used for access to 63 Davey Street, improving access arrangements for both the development and the St. Helen's Private Hospital which current utilises a right of way over the existing crossover. These changes are considered to improve access to the rear of the Hospital whilst ensuring the proposed development can be accessed appropriately.

Council and State Growth Consent was previously provided for these works, and this revised application does not seek any changes to the existing arrangements. Therefore, the existing consent documents are considered to be sufficient. The changes proposed require Council Consent and State Growth Consent, which has been provided.

URBAN FORM SUPPORTING STATEMENT

63 Davey Street, Hobart

Last Updated - May 2020

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CONTENTS

URBAN FORM SUPPORTING STATEMENT	1
CONTENTS	2
1. INTRODUCTION	3
1.1 INTRODUCTION	3
1.2 DEVELOPMENT PROPOSAL	3
1.3 URBAN FORM ASSESSMENT	3
2. EXISTING URBAN FORM	4
2.1 LOCATION	4
2.2 SUBJECT SITE	5
2.3 TOPOGRAPHY	6
2.4 PATTERN OF BUILT FORM IN WIDER LOCAL CONTEXT	7
2.5 URBAN FORM FEATURES DEMONSTRATING VARIETY	9
2.6 EXISTING BUILDING HEIGHT, IN LOCAL URBAN CONTEXT	11
2.7 EXISTING URBAN FORM IN HERITAGE CONTEXT: HERITAGE PRECINCT	12
2.8 PATTERN OF BUILT FORM IN IMMEDIATE SURROUNDING URBAN CONTEXT	13
2.9 SITE SURROUNDS	14
2.10 EXISTING STREETScape	15
2.11 EXISTING BUILT FORM: VARIATION AND RHYTHM OF SCALE, MASSING AND HEIGHT	16
3. PROPOSAL CONSIDERED IN LOCAL URBAN CONTEXT	18
3.1 DEVELOPMENT PROPOSAL	18
3.2 PLANNING SCHEME CONTEXT	23
3.3 BUILT FORM CONSIDERATIONS	24
3.4 PROPOSED BUILDING SCALE & MASS RELATIVE TO LOCAL URBAN CONTEXT	24
3.5 PROPOSAL WITHIN DAVEY STREET FRONTAGE	24
3.6 PROPOSAL VIEWED FROM NEARBY HARRINGTON STREET	27
3.7 PROPOSAL SCALE & MASSING RELATIVE TO ADJACENT PUBLIC REALM	28
3.8 PROPOSAL RELATIVE TO HERITAGE CONTEXT	33
4. SUMMARY	35

1. INTRODUCTION

1.1 INTRODUCTION

- 1.1.1 Planning Tas trading as Ireneinc Planning and Urban Design have prepared the following urban form supporting statement assessment on behalf of Tellyros Klonis Unit Trust to accompany an application for the use and development of land at 63 Davey Street, Hobart.
- 1.1.2 This report has been prepared in response to architectural drawings prepared by JAWS Architecture.

1.2 DEVELOPMENT PROPOSAL

- 1.2.1 The development proposed is for the redevelopment of the site at 63 Davey Street, Hobart.
- 1.2.2 The proposal involves the demolition of the existing single storey building at 63 Davey Street, including removal of the existing 8 parking spaces in the forecourt immediately off Davey Street.
- 1.2.3 The new works are to facilitate use and development for 51 apartments providing a mix of residential and serviced apartments (visitor/ student accommodation).
- 1.2.4 The proposed building is in the form of two interconnected building blocks, comprising a four storey building block that will provide an active frontage to the Davey Street (currently the site frontage is occupied by an area of open car parking).
- 1.2.5 A second building component is set back 15m from the street frontage, this building component has 10 levels above ground levels (comprising a ground floor, two levels of serviced apartments and a further seven levels of apartments). An additional rooftop level (11 storeys above ground) is set back a further 30m from street frontage.
- 1.2.6 The proposal includes two levels of basement car parking.

1.3 URBAN FORM ASSESSMENT

- 1.3.1 The design analysis in this urban form supporting statement provides a concise study of built form considerations, to be assessed under the planning scheme including:
 - Existing built form considerations including building form (height, scale, massing) and pattern of built form (including for example, building setbacks) within the immediate and local context of the site.
 - The compatibility of the proposed development within the context of the above listed existing conditions.

These factors will be considered in detail in the below sections.

2. EXISTING URBAN FORM

2.1 LOCATION

- 2.1.1 The location of the site is on the periphery of the Hobart city central business district (CBD), with frontage to Davey Street, one of the main city streets in the state's capital city. The site is close to many shops, services and amenities associated with a central city location. It is well served by transport options including for walking and public transport as well as prominent city park, St David's Park.

The following figure describes the location of the site.

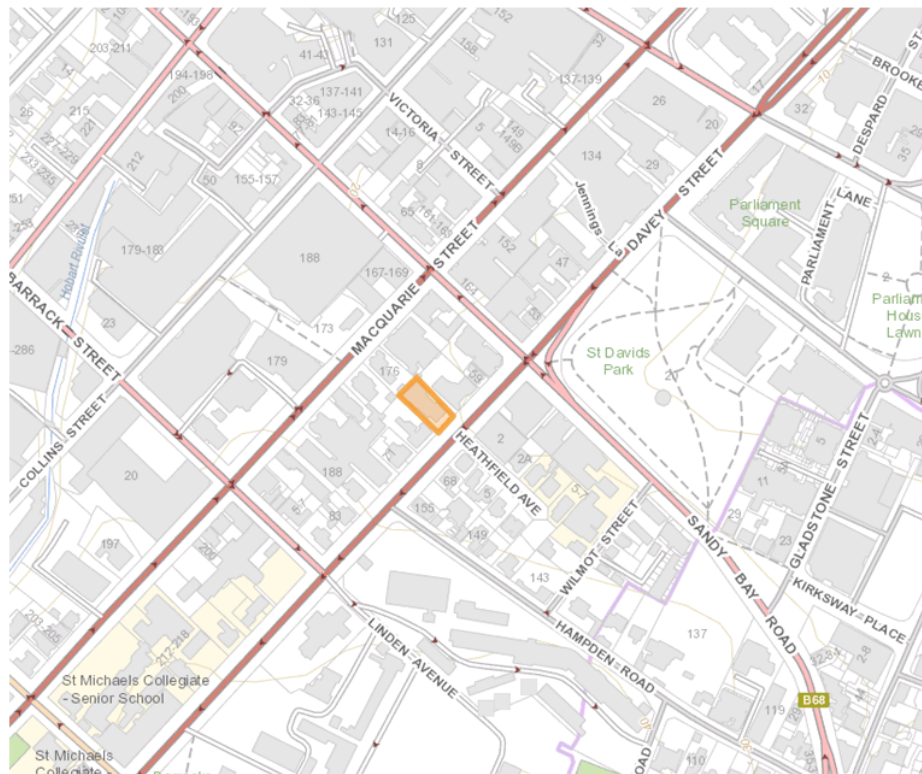


Figure 1: Site location (source: The LIST, www.theLIST.tas.gov.au © the State of Tasmania).

2.2 SUBJECT SITE

- 2.2.1 The subject land is located at 63 Davey Street, Hobart 'Navy Club' (CT54396/1), with a site area of 809m². The frontage of the site is on the south east boundary and the fall of the land is towards the frontage.
- 2.2.2 The application also includes the adjoining site, identified as 186 Macquarie Street 'St Helens Private Hospital (CT 110411/1), due to a requirement for access and works related to upgrades of existing sewer infrastructure.
- 2.2.3 The site has an existing single-storey building, formerly the Old Navy Club, built to the side and rear boundaries and setback approximately 13m from the frontage to Davey Street. The building is currently used as a furniture store with on-site carparking within the front setback.



Figure 2: Aerial image of the subject site (red) and adjoining site at 186 Macquarie Street (blue) (Source: The LIST, www.theLIST.tas.gov.au © the State of Tasmania).

The site is accessed via a shared 4m (approx.) wide entrance from Davey Street. This access also includes a right of way easement benefitting the adjoining to the land to the southwest. There is an existing sewer main that runs along the rear boundary of the property.

2.3 TOPOGRAPHY

- 2.3.1 The topography surrounding the area presents an amphitheatre with buildings on the Macquarie Street ridge and upper Davey Street forming part of the visual context of the site, as illustrated in the figure below.

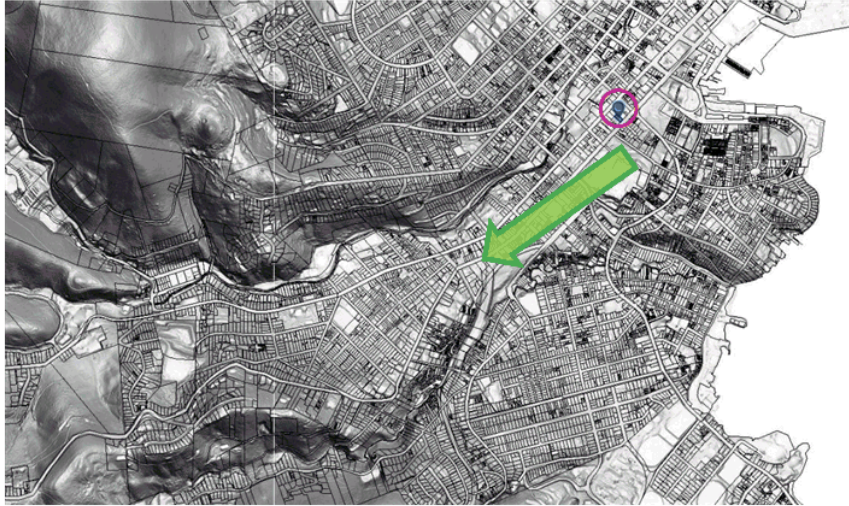


Figure 3: Topographical image Hobart city centre and surrounding context, with the subject site marked by blue pin (circled in red), topography rising in south west direction up Davey St (green arrow). (Source: www.theLIST.tas.gov.au © the State of Tasmania).

- 2.3.2 Davey Street rises in the direction of the street traffic, running to the south west, creating a strong change in the levels in the immediate streetscape as illustrated in the photography below.



Figure 4: View looking up Davey Street

2.4 PATTERN OF BUILT FORM IN WIDER LOCAL CONTEXT

- 2.4.1 The application site is centrally located in the south eastern periphery of the CBD, as illustrated in the Figure below.



Figure 5: Aerial photograph, site marked in blue. (Source: www.theLIST.tas.gov.au © the State of Tasmania).

- 2.4.2 Notable built form considerations within the existing local, urban context include:
- **Block structure:** strong pattern of urban blocks, in traditional 'perimeter block' form, with buildings filling blocks of broadly rectilinear form, building frontages addressing the streets.
 - **Urban grain:** the urban grain is notable for the relatively dense pattern of urban blocks set in a connected network of streets that are broadly arranged in grid pattern in the Hobart city centre / CBD, with some arterial routes such as Sandy Bay Road tapering off to link to areas beyond the city centre. Within this, a finer lot pattern is evident in the built form, even where lots have been amalgamated.
 - **Building form:** a wide variety of building forms are present in the local urban context in and around the application site. Variety of built form is related to the great mixture of land uses, and the evolution of buildings within the city structure (of blocks, streets and spaces) over a long period of time, with a great variety of building ages and architectural styles. This pattern of variety is a common feature of cities of Hobart's age and creates the diversity of individual building forms and appearance cumulatively contribute to the richness in character of the city.
- 2.4.3 The site is located on Davey Street, a primary arterial route that carries vehicular traffic on a south west course out from Hobart CBD heading south east with onward connections to Sandy Bay and Mount Wellington within Hobart and beyond to Kingston and the Channel in Kingborough.
- 2.4.4 Davey Street is one half of inner city 'couplet' transport loop, with Macquarie Street (running parallel, one block to the west to Davey Street) carrying vehicular traffic in the opposite direction, north east into the CBD, with onward links to the Brooker and Tasman Highways.
- 2.4.5 Davey Street and Macquarie Street are two major urban routes, with the character of buildings fronting these streets characterised by larger building forms, including historic buildings with

heritage value and a mix of a more commercial scale and use, whilst the radiating side streets and parallel streets demonstrate a domestic scale of dwellings either retained for visitor accommodation or adapted to other uses.

- 2.4.6 It is notable that a great variety of building scale and massing is evident within a relatively small area around the application site, as illustrated in the oblique aerial photograph presented in the figure below.

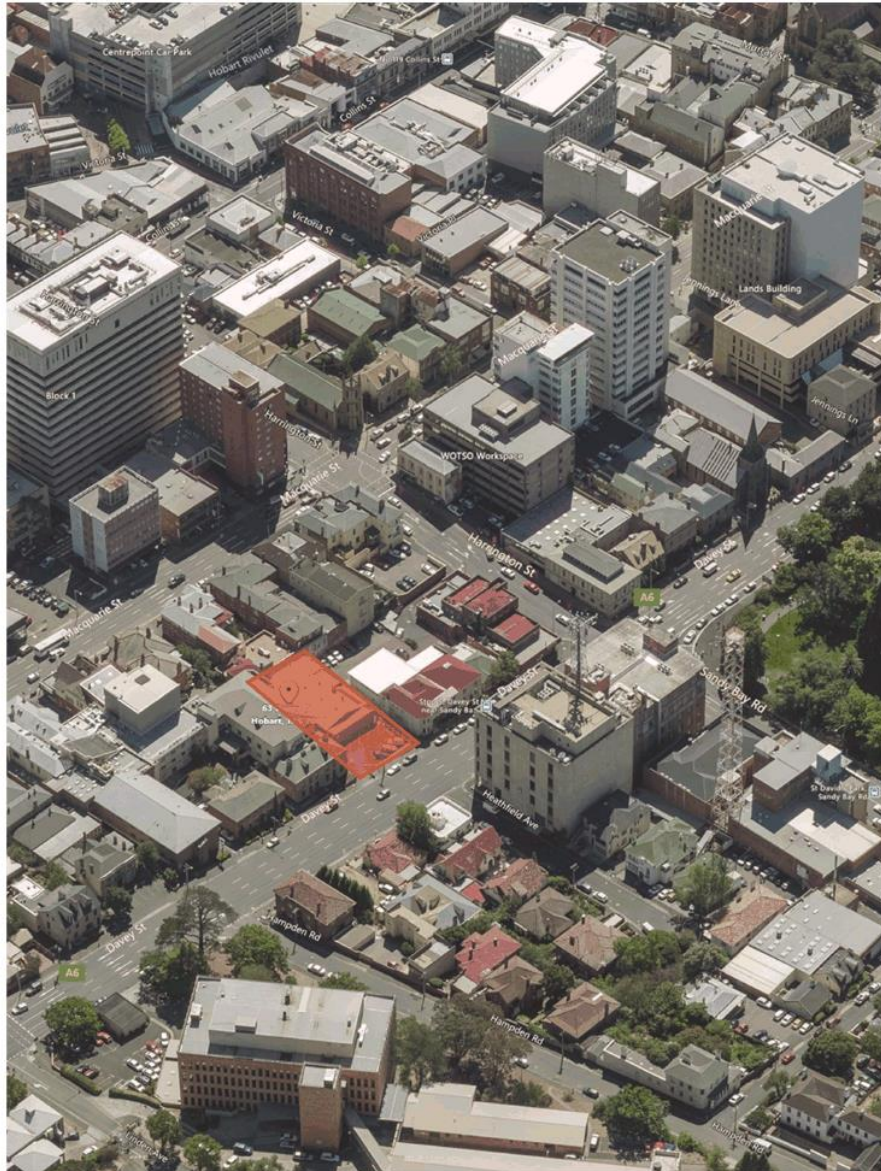


Figure 6: Oblique aerial photographic view of local area and urban form, site highlighted in red. (source: Bing Maps, www.bing.com/maps).

2.5 URBAN FORM FEATURES DEMONSTRATING VARIETY

2.5.1 The most notable features of the existing urban form in the local context around the subject site is the divergence of building form within the urban blocks in this part of the city. Taking the urban blocks either side of the parallel city streets of Davey Street and Macquarie Street (cumulatively forming the traffic 'couplet' circulatory) as examples of direct relevance, it is clear that each urban block, with buildings that front onto streets, contains a great variety of building form within relatively short sections of street and block.

2.5.2 Variety of urban building form relates to:

- **Age:** a great variety of building ages, varying from historic buildings with heritage value, through to contemporary buildings and infill development.
- **Scale, mass and height:** significant variety of building heights, scale and mass, from two storey properties to tall towers of 10 storeys and over in height.
- **Building use:** strong range of building uses, as appropriate for an area of urban mixed use, in the heart of the city. Uses include residential, hotels, commercial offices, shops, services, places of worship. Many buildings have adapted in use over time, including buildings that were originally residential properties, many now converted for alternative uses.
- **Architectural character:** notable range of architectural characters, reflective of the wide range of ages, forms and function / use of buildings.



Figure 7: Diagrammatic illustration of variety in urban form, mass & height in local area. Site highlighted in red. (Source of oblique aerial photographic: Bing Maps, www.bing.com/maps).

Urban form features demonstrating consistency

2.5.3 Some elements in the urban form in the local area demonstrate greater degrees of consistency, these include:

- **Building setbacks:** buildings predominantly have either zero setback (building built to back edge of the footpath) or a shallow setback of a few meters; generally this setback is for a small threshold space, sometimes including landscape features, as a transition between the public realm of the street and the private realm of the building.
- **Plot coverage:** built form has a high plot coverage, that is the proportion of the site parcel that is covered by building. This is a common pattern in more urban city centre locations, where land is of a premium and less space is afforded to gardens or open space.
- **Street frontage:** buildings present a generally consistent frontage to the street, with 'active frontages' comprising front elevations of buildings that have façades broken up with fenestration. Windows and doors to the street provide 'eyes on the street' and generate activity with comings and goings, all contribution to 'natural surveillance' and enhanced sense of safety and security for citizens.

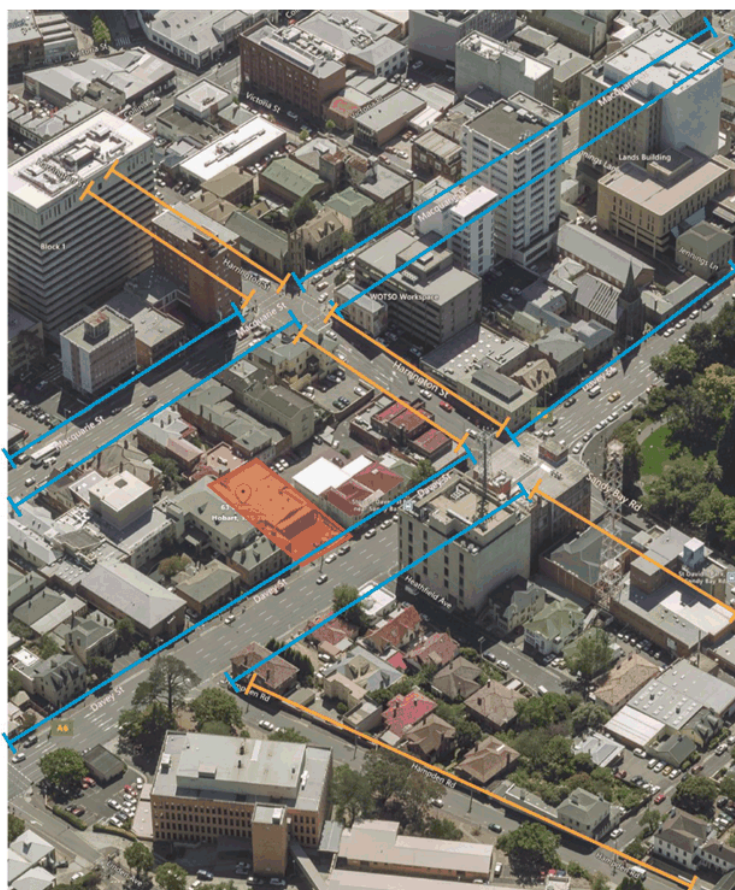


Figure 8: Diagrammatic illustration of consistency in urban form including street frontages and setbacks. Site highlighted in red. (Source of oblique aerial photographic: Bing Maps, www.bing.com/maps).

2.6 EXISTING BUILDING HEIGHT, IN LOCAL URBAN CONTEXT

2.6.1 A key consideration of the character of the area is the number of taller buildings within a short distance of the application site.

2.6.2 The figure below illustrates a range of building heights in close proximity to the application site, within a distance of circa 400m from the site.

2.6.3 The variety of building heights is considerable, ranging from:

- Single storey buildings (for instance residential buildings within heritage zones immediately south and east of the application site).
- Some of Hobart's tallest buildings, including offices from 9 to 15 storeys in height on Macquarie Street and Collins Street, and the Executive Building (c. 12 storeys) on Davey Street.
- In close proximity to the application site are a number of buildings around 5-6 storeys in height.

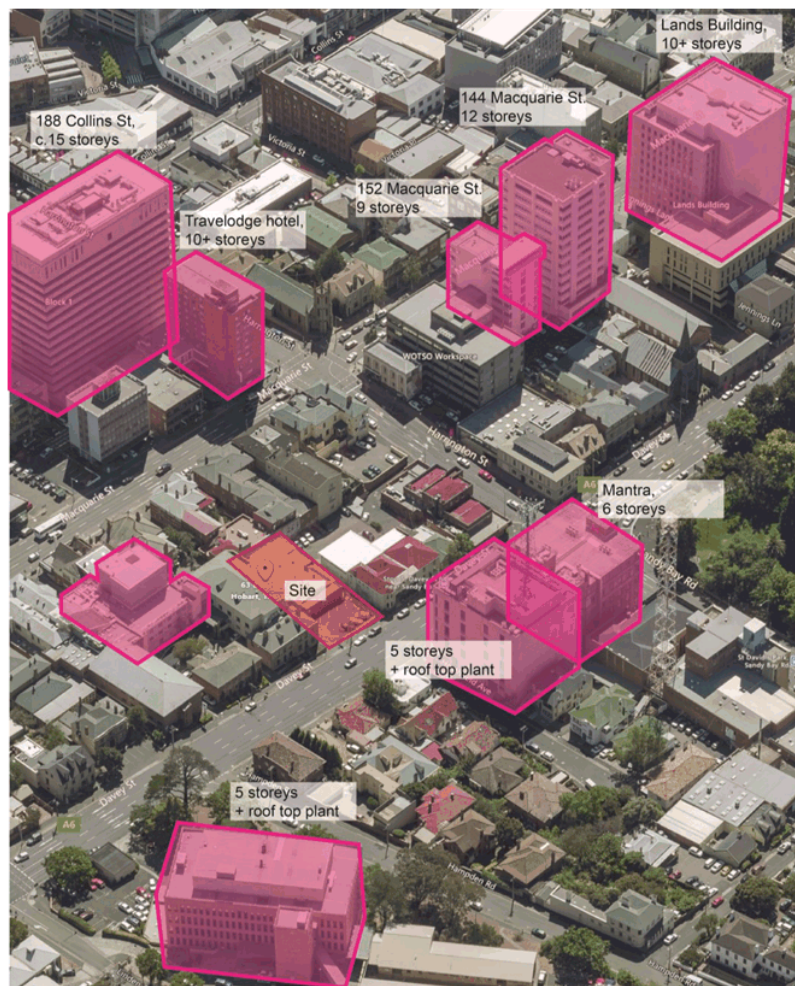


Figure 9: Taller buildings within circa 400m vicinity of application site (Source of oblique aerial photograph: Bing Maps, www.bing.com/maps).

2.7 EXISTING URBAN FORM IN HERITAGE CONTEXT: HERITAGE PRECINCT

- 2.7.1 The site and existing building are not identified as a heritage place on the HIPS or the Tasmanian Heritage Register. However, the site is located within the H1 Heritage Precinct and is mapped as a Place of Archaeological Potential. The site, as situated within the extent of the H1 Heritage Precinct is illustrated in the figure below.

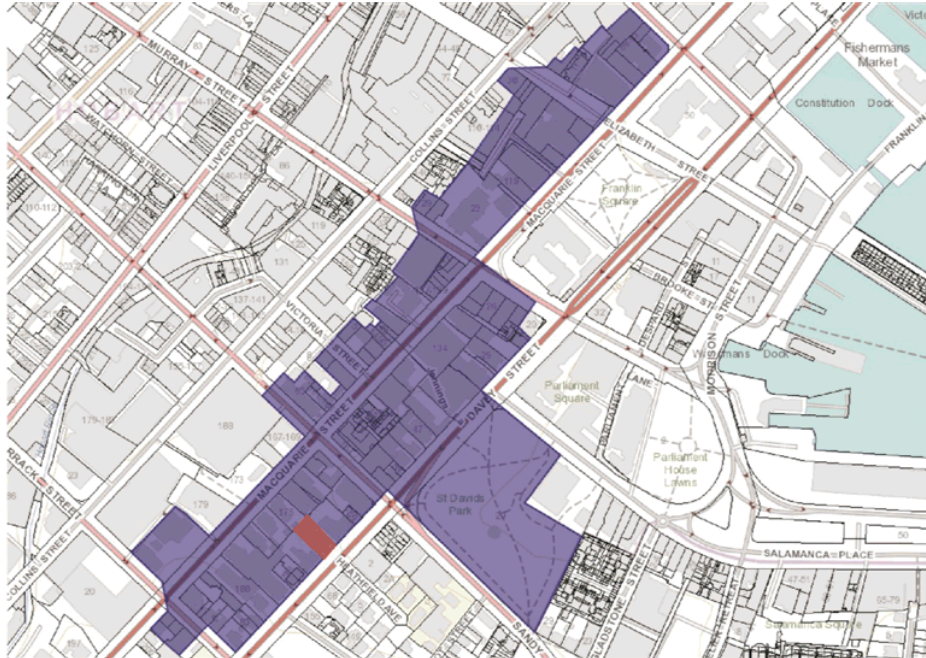


Figure 10: Extents of Heritage Precinct H1 - City Centre (in blue), site in red. (Source: www.theLIST.tas.gov.au © the State of Tasmania).

- 2.7.2 A heritage place is located on either side of the site as listed in Table E13.1 of the Heritage Code. The following figure identifies the site (blue) and the adjoining heritage listed buildings (in orange), comprising the RAAF Memorial Centre (61 Davey Street) and the St Helen's Hospital that has frontage to both Davey Street and Macquarie Street.



Figure 11: HIPS 2015 & Tasmanian Heritage Register heritage listing (Source: adapted from The LIST)

2.8 PATTERN OF BUILT FORM IN IMMEDIATE SURROUNDING URBAN CONTEXT

- 2.8.1 Assessment of existing urban form can also be considered at the closer scale of the immediate surrounding area and urban block within which the subject site is located.

Defining the Area

- 2.8.2 The term “surrounding area” is defined by the City of Hobart Interim Planning Scheme as being a distance of 100m from the site, for the purpose of streetscape analysis. An area of 100m radius from the site is illustrated in the figure below.

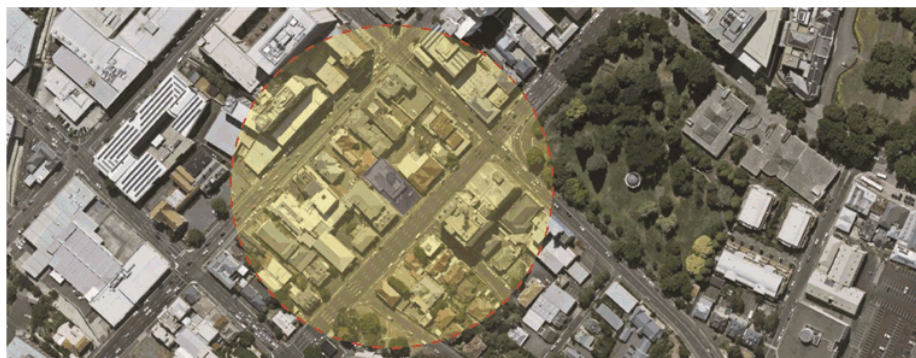


Figure 12: Aerial photograph, site marked in blue, 100m radii around site in yellow / red boundary (Source: www.theLIST.tas.gov.au © the State of Tasmania).

- 2.8.3 However, for purposes of urban form assessment consideration is also given to the surrounding neighbourhood, location and locality, that is townscape rather than streetscape. In this context the locality can also be defined by the area bounded by Davey Street, Macquarie Street, Harrington Street and Barrack Street, these four streets, below.

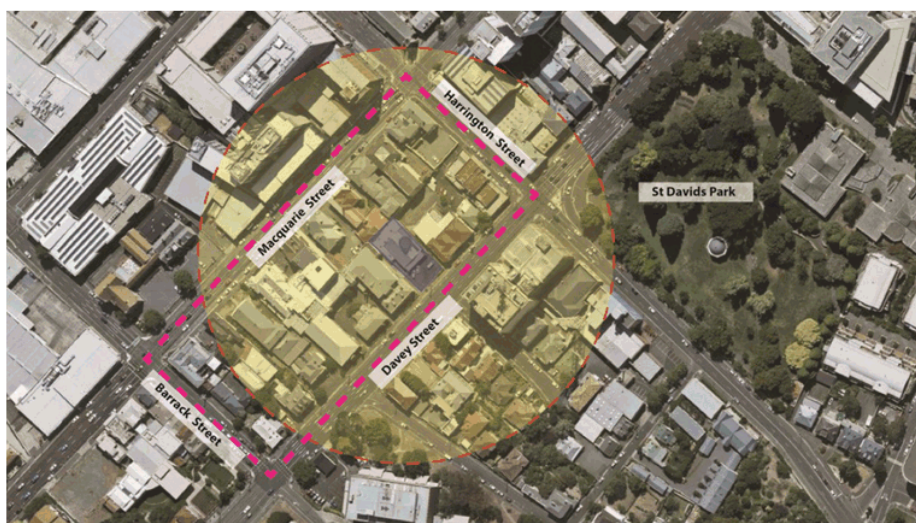
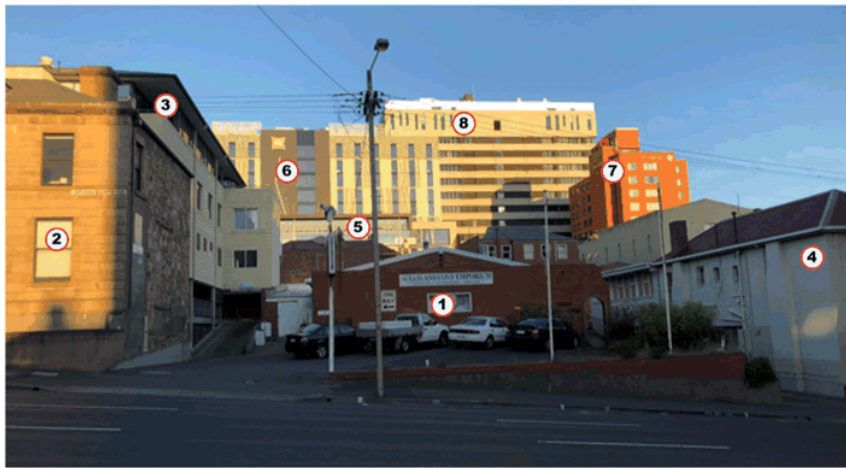


Figure 13: Aerial photograph, site in blue, 100m radii in yellow, perimeter block of locality bounded by magenta dashed area (Source: www.theLIST.tas.gov.au © the State of Tasmania).

2.9 SITE SURROUNDS

- 2.9.1 'St Helens Private Hospital is located to the south west of the site at 186 Macquarie Street, whilst the main entry to the hospital is from Macquarie Street. The Davey Street frontage of the hospital is a three-storey sandstone heritage building. The upper floor is formed by the dormer windows within the roof space. The building as with many of the other buildings along the frontage, is setback for a small landscaped area. To the rear of the heritage building is a contemporary hospital building with underground parking below. The remainder of the street through to Barrack Street is characterised by 2-3 storey buildings, many of heritage value.
- 2.9.2 To the north east, 61 Davey Street is a two-storey heritage building (RAAF Association Memorial Centre). East of this is a single storey heritage cottage at 59 Davey Street and the art deco Welcome Stranger Hotel on the corner at 58 Harrington Street (subject to a planning application currently before council).
- 2.9.3 On the opposing side of the street the 6-storey red brick Mantra hotel on the corner of Sandy Bay Road and the 5-6 storey Telstra Building occupy the streetscape, with apartments and residential buildings west of Heathfield Avenue.



KEY:

- | | |
|---|--|
| ① Subject site
• 63, Davey Street | ⑤ Ibis Styles Hotel, contemporary form
• 4 storeys building height to street frontage |
| ② St Helens Private Hospital
• Davey Street frontage, heritage sandstone | ⑥ Ibis Styles Hotel, contemporary form
• c. 10 storeys set back from street frontage |
| ③ St Helens Private Hospital
• Contemporary building extension | ⑦ Travelodge hotel
• 10+ storeys height |
| ④ RAAF Association Memorial Centre
• Two storey heritage building | ⑧ 188 Collins Street
• c. 15 storeys height |

Figure 14: Frontage of subject site to Davey Street with built form context

- 2.9.4 The Ibis Styles hotel on Macquarie Street, visible from the site on Davey Street (see figure above), is a local precedent of a building form that has multiple building components within the same development, notably with a smaller component (c. 4 storeys high) to the immediate street edge and a taller building component (c. 10 storeys high) set back from the street edge within the plot.

2.10 EXISTING STREETScape

- 2.10.1 The existing streetscape to Davey Street is illustrated in the following figures showing views of the street and buildings that address the street and are visible in urban blocks in the local area.
- 2.10.2 The subject site currently presents a 'gap' in the north western street frontage to Davey Street with the large area of open surface carparking between the street and the existing low-rise building on the site. Either side of the subject site the building line is relatively continuous with buildings providing a positive frontage, with shallow setbacks from the back of footpath. Buildings with direct frontage to Davey Street on the northwest side of the street, close to the site are predominantly two to three storeys in height with occasional single storey buildings.
- 2.10.3 Beyond the immediate street frontage to Davey Street the pattern of building height, scale and mass changes, with larger building forms visible from Davey Street. Many of these taller buildings are located within the local context of urban blocks, including the four storey contemporary building element of the St Helens private hospital and several taller hotels and office buildings a couple of urban streets away including on Macquarie Street and Collins Street, as illustrated in the figure below.



Figure 15: Davey Street frontage, facing north west from Heathfield Avenue, opposite the subject site on Davey Street (photo source: Google Street View)

- 2.10.4 A view of the opposite side of Davey Street is presented in the following figure. A similar pattern of built form is present with several two to three storey buildings directly fronting Davey Street.
- 2.10.5 Directly opposite the site are two mid-rise buildings of the Mantra Hotel (5 storeys) and the Telstra Building (5-6 storeys). Further beyond a pattern of taller buildings and buildings of greater mass are visible in the surrounding urban context.



Figure 16: Davey Street frontage, facing north (photo source: Google Street View)

2.11 EXISTING BUILT FORM:

VARIATION AND RHYTHM OF SCALE, MASSING AND HEIGHT

- 2.11.1 Existing urban form in the local context around the subject site are notable for a divergence of building form within the urban blocks in this part of the city.
- 2.11.2 Previous figures presented in this section illustrate the variation of scale, massing and height for built form on frontages that address streets around the site in the context of the urban blocks immediately in and around Davey Street, Macquarie Street, Harrington Street and Barrack Street. The figure below shows a variety of building scales and heights, ranging from lower rise buildings immediately neighbouring the site through to taller buildings opposite and one block back.

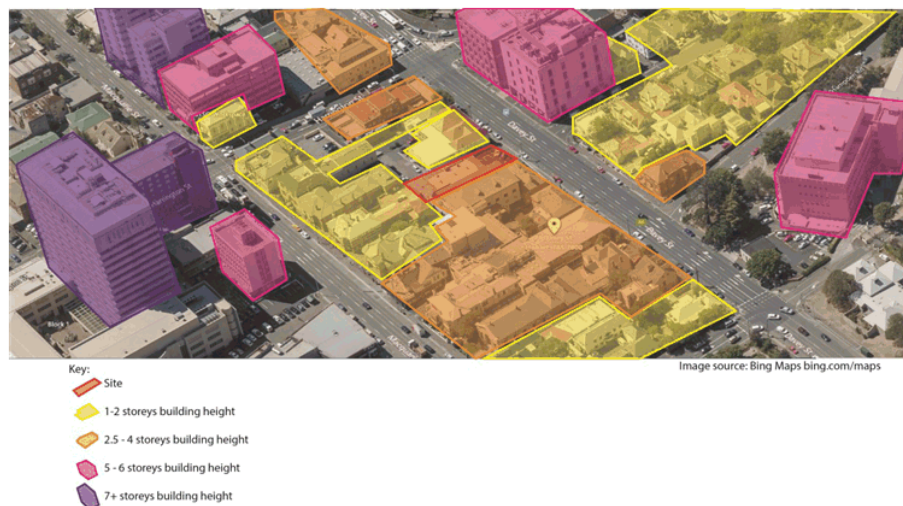


Figure 17: Indication of building heights in urban context (photo source: Bing Maps bing.com/maps)

2.11.3 It is evident that the urban blocks around the subject site accommodate a variety of urban form including with regards to:

- **Building height:** heights range from single - two storey (commonly residential or former residential properties, some now converted to office and other uses), several 5-6 storey buildings including Mantra and Telstra buildings opposite the site on Davey Street, through to taller buildings including 188 Collins Street (c. 15 storeys), 152 Macquarie Street (9 storeys), 144 Macquarie Street (12 storeys), Travelodge Hotel on corner of Macquarie and Harrington Streets (10+ storeys).
- **Building form and massing** is also varied. The majority of buildings within the urban block bounded by Davey/Macquarie/Harrington/Barrack streets range are lower-medium height of 2-3 storeys, many of historic character and heritage value. Neighbouring urban blocks including taller buildings of greater scale and massing, including the mid-rise building form of the Mantra and Telestra buildings on the corner of Sandy Bay Road and Davey Street. The tallest buildings on Macquarie and Collins Street are located a block away from the site but show precedent of buildings of greater form and mass within the city core.
- **Building heritage:** the age of buildings varies greatly, including some of the oldest heritage buildings in Hobart (and Tasmania), through to some of the most recent additions to the city (including Travelodge and Ibis Styles hotels on Macquarie Street).
- **Building design:** in common with the variation in building, function and use there is great range in architecture styles and appearance, from more traditional forms of building using local materials including stone and timber, through to more contemporary constructions in glass and steel, with many other variants and a great diversity of building materials.

3. PROPOSAL CONSIDERED IN LOCAL URBAN CONTEXT

3.1 DEVELOPMENT PROPOSAL

- 3.1.1 The proposal involves the demolition of the existing single storey building at 63 Davey Street, including removal of the existing 8 parking spaces in the forecourt.
- 3.1.2 The new works are to facilitate use and development for 51 apartments providing a mix of residential apartments and visitor accommodation. The building form is setback at its closest point to the frontage of the site by 2.8m to allow for landscaping and setback consistent with the adjoining heritage buildings. The ground floor provides lobby, services, access and vehicle manoeuvring for the proposed building whilst the basement levels provide a total of 42 vehicle parking spaces.
- 3.1.3 Illustration of the proposal in terms of form and massing is provided in the following figures that show computer model visualisation of the development within the context of surrounding, existing built form.



Figure 18: Illustrative view facing north across Davey Street, showing a model of the proposal set within the surrounding urban form shown for context, including heritage buildings (source: JAWS Architects)

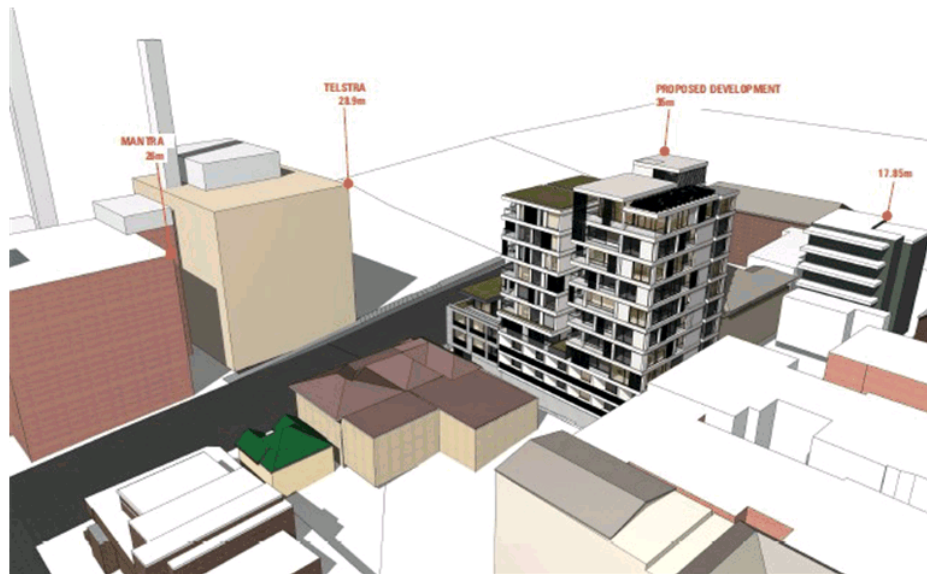


Figure 19: Illustrative views facing south from Macquarie Street showing a model of the proposal set within the surrounding urban form shown for context, including heritage buildings (source: JAWS Architects)

- 3.1.4 The vehicular access to the site is to be reconfigured to the north east side of the frontage allowing two-way traffic flow. Basement carparking is accessed from car lifts located internally within the building.
- 3.1.5 Bicycle storage and motorbike parking has also been accommodated internally within the building.



Figure 20: Ground level plan (left) illustrating two-way access and exit to the development from Davey Street. This vehicular access provides routes to an internal car lift, with two levels of basement parking as illustrated in basement plan (right). Source: JAWS Architects.

- 3.1.6 Investigations have been made with TasWater to accommodate an existing sewer main currently located along the rear boundary of the property, to within the proposed development.
- 3.1.7 From ground floor to Level 3 the proposed building is setback approximately 2.8m from the frontage, with respect to the neighbouring building located to the south. The design of the front façade is articulated with window openings reflecting the proportions and rhythm of the adjoining heritage buildings, expressed in a contemporary manner. Level 3 is setback approximately 6.7m from the frontage to accommodate a green roof whilst also creating a podium to reference the scale of surrounding buildings. Apartments on the lower levels are generally single bedroom apartments with open space predominantly orientated to the north.
- 3.1.8 Levels 4-9 are setback 15m from the frontage and accommodate 2 bedrooms, generally with a larger footprint and a relatively larger open space allocation. The floor plate is divided with interstices to provide separation between apartments and to also create greater distinction and articulation in the building form, which is reflected in the upper floor plans. Levels 10 is setback 30m from the street frontage and provides a 3-bedroom apartment.
- 3.1.9 The figure below illustrates the proposed building setbacks from Davey Street, with the context of the building envelope (as permitted in the planning scheme).

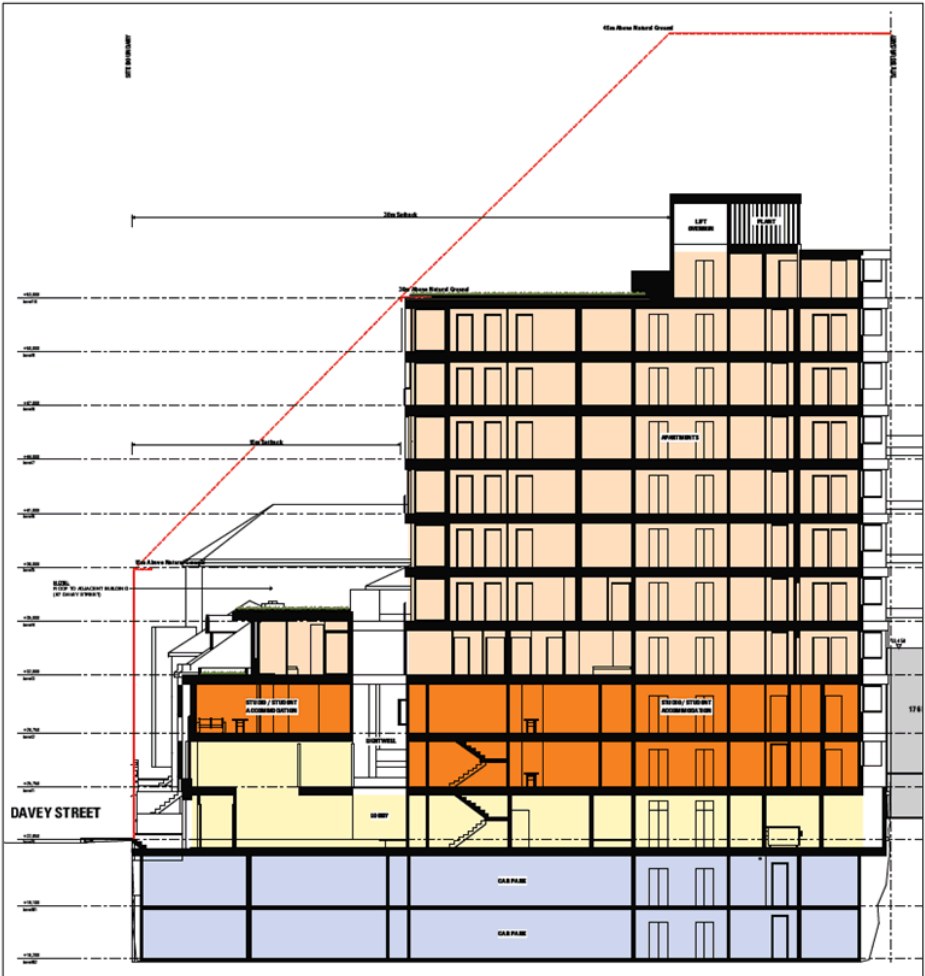


Figure 21: Section drawing illustrating the proposal set within the parameters of the building envelope (red line) shown for context (source: JAWS Architects)

- 3.1.10 As illustrated in the figure below the treatment of the building facades above Level 4 is distinguished from the lower podium.
- 3.1.11 The design, finish and materials are clearly contemporary, while also still integrated with the treatment of the lower level and have been articulated for greater visual interest and to minimise the expansive blank facades. The facades are cognisant of their location within the broader city and have used spaces and colour to layer the overall massing and form of the components of the building.



Figure 22: Illustrative north - east elevation drawing of the proposal (source: JAWS Architects)

3.2 PLANNING SCHEME CONTEXT

- 3.2.1 The Planning Report document submitted with the Development Application provides assessment of the proposal against the standards of the Hobart Interim Planning Scheme 2015 (HIPS 2015).
- 3.2.2 The site is located within the Central Business Zone of the HIPS 2015, as shown in the figure below. The site is within the Central Business Core Area and is not located on a Solar Penetration Priority Street.
- 3.2.3 The figure below describes the subject site within the Central Business Zone (blue).

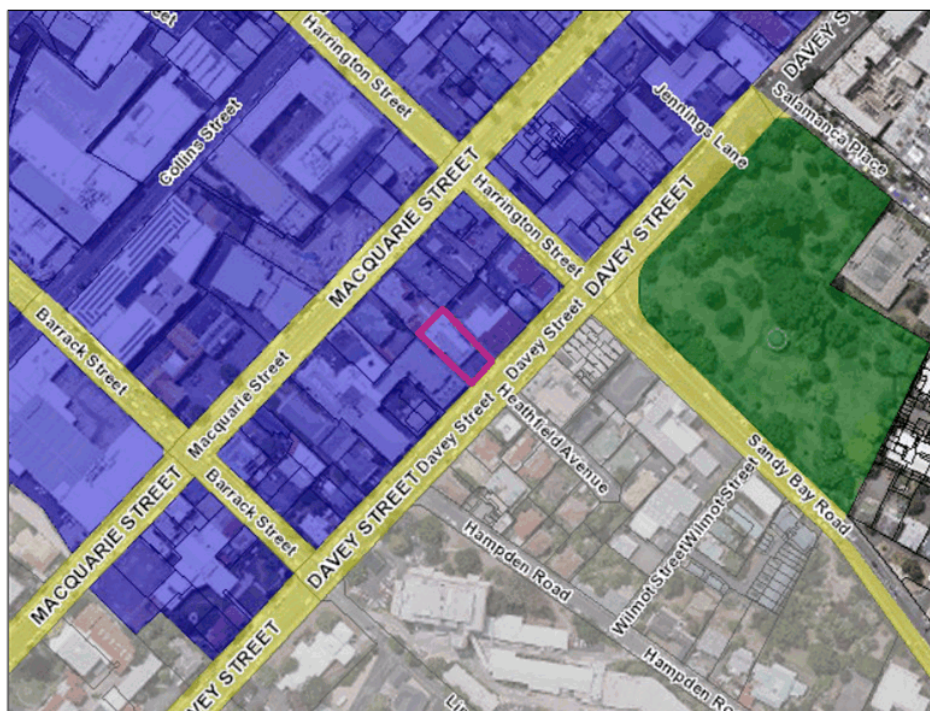


Figure 23: Site Zoning (source: www.theLIST.tas.gov.au © the State of Tasmania).

- 3.2.4 The Planning Report includes assessment of the proposal elements of the Hobart Interim Planning Scheme 2015 (HIPS 2015) including topics listed below that are included in this Urban Form Supporting Statement for reference and relance to the urban form assessment also.
- Desired Future Character Statements
 - Development Standards, including:
 - Building height
 - Setback
 - Design
 - Passive Surveillance

3.3 BUILT FORM CONSIDERATIONS

3.3.1 The proposed development is considered as follows within four settings:

- **Urban context:** *Proposed building scale and massing relative to local urban context;*
- **Street frontage:** *Proposal within Davey Street frontage & viewed from Harrington Street;*
- **Heritage:** *Proposal relative to heritage context;*
- **Public Realm:** *Proposed building scale and massing relative to adjacent public realm;*

3.4 PROPOSED BUILDING SCALE & MASS RELATIVE TO LOCAL URBAN CONTEXT

- 3.4.1 The proposal building is illustrated in the context of surrounding urban context in the figure below. This figure presents a context height study diagram, with the proposal highlighted in colour and the surrounding, existing urban built form shown in monochrome.
- 3.4.2 The buildings immediately next to the proposal, (within the urban block bounded by the streets of Davey St, Macquarie St, Harrington St and Barrack St), have a smaller scale and massing than the proposal, predominantly in a range of 2-3 storeys, with the occasional single storey building and some 4 storey building elements (such as within neighbouring St Helen's Private Hospital complex).
- 3.4.3 In the urban blocks on the opposite sides of these surrounding streets, including on Davey Street, Macquarie Street and Harrington Street there are multiple examples of taller buildings (ranging from 5 to over 10 storeys building height) that have a greater scale and mass of building form, these examples are highlighted in grey in the figure below.
- 3.4.4 These examples of buildings with greater building scale and mass show the surrounding context for the proposed building form, including in the approach to the staggered, stepping of the building form and profile with the lower building element to address the primary street frontage, with a taller building element set back within the plot and urban block.
- 3.4.5 When considered in this setting, it is clear that the proposal has a form that is consistent with building forms in the local urban context.

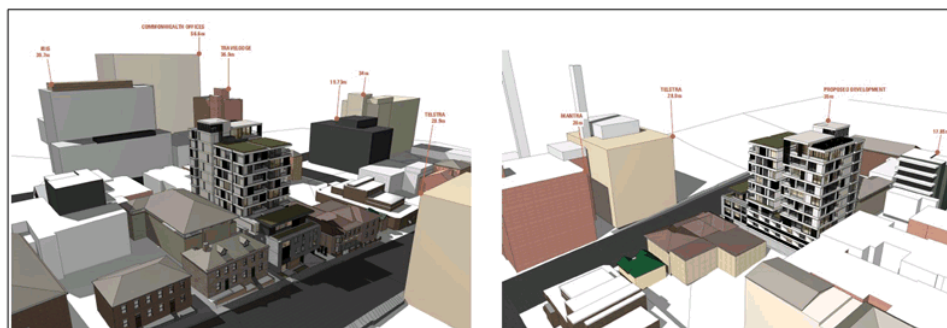


Figure 24: Context height study, illustrative view showing proposal set within the surrounding urban form shown for context (source: JAWS Architects)

3.5 PROPOSAL WITHIN DAVEY STREET FRONTAGE

- 3.5.1 The Davey Street frontage is a primary consideration for the proposed development as the primary street frontage from which the new building will be viewed, accessed and experienced.
- 3.5.2 The figure below presents a street elevation illustration showing the proposal in the setting of an extended section of Davey Street, including beyond Harrington Street to the north of the site. The

elevation drawing shows the proposal in the context of both the immediate street frontage to Davey Street (with coloured façades of buildings that have direct frontage to the street) and also in the context of buildings in the surrounding local urban context (shown in grey / outline image).

- 3.5.3 The proposal recognises the scale, form and height of neighbouring buildings that have direct frontage to Davey Street and responds with the front element of the proposal that extends to four storeys above natural ground level. The height of this front built form element of the proposal is comparable with the height of neighbouring buildings, including heritage buildings of historic character, as illustrated in the elevation drawings in the figures below.



Figure 25: Context height study, illustrative Davey Street elevation, including proposal (bounded in red), with surrounding urban form shown for context (source: JAWS Architects)

- 3.5.4 The proposal has a taller element of built form that extends to 11 storeys in building height. This is set back within the site, building height extends to 10 storeys after a 15m set back from the site / street boundary, with an additional rooftop (11th storey) setback a further 15m (30m setback from street edge). This building element may be taller than the immediately neighbouring buildings on Davey Street but it is comparable to the height, scale and mass of other existing buildings in the local urban context as illustrated by the taller buildings indicate in outline form in the elevation drawings in figures above and below.



Figure 26: Context height study, enlarged to show illustrative street elevation with the proposal in context of neighbouring buildings with frontage to Davey Street (source: JAWS Architects)

- 3.5.5 The figure below presents further, more detailed illustration of the proposed building form in elevation, as viewed from Davey Street.
- 3.5.6 The scale of the front four storey building element has strong correlation with the building height of neighbouring building at 67 Davey Street (part of the 'St Helens Private Hospital').

- 3.5.7 Furthermore, the appearance of the proposed building is broken down in appearance and form, with different architectural articulation of the ground floor and third floor, compared to the first and second levels. The third floor is indented compared with second floor, and differing material and colour palettes help to break up the mass of the building form on that has closest relationship to the street edge.
- 3.5.8 The appearance of the taller building element (that extends to 10-11 storeys) set back within the site is differentiated from the front building element, with lighter colour palette, and greater extent of glazing and fenestration. This difference in appearance further helps to break down the overall scale and mass of the proposal.



Figure 27: Davey Street elevation illustration, showing proposal and existing neighbouring buildings (source: JAWS Architects)

3.6 PROPOSAL VIEWED FROM NEARBY HARRINGTON STREET

3.6.1 The illustration presented in the figure below shows the proposal in the local urban context, as seen in elevation form, viewed from Harrington Street (to the north east of the site).

3.6.2 This extended street elevation illustration presents a context height study, that shows the height of the proposal is of a comparative height and scale to other tall existing buildings in the local area, including as illustrated from left to right in the section illustration:

- The Mantra Hotel building of 6 storeys building height on the adjacent, opposite side of Davey Street (corner of Davey and Sandy Bay Road);
- Elements of the 'St Helens Private Hospital Building;
- The Travelodge building at 167 Macquarie Street (c.10-11 storeys building height); and
- The 188 Collins Street building (c. 15 buildings storeys height).

3.6.3 The illustration in the figure below also shows how the proposed building has a stepped and staggered form, with the four storey element of building that has immediate frontage to Davey Street, with taller building elements setback, in stepped form within the site, and set towards the centre of the overall urban block (as bounded by Davey Street and Macquarie Street in this elevation illustration).

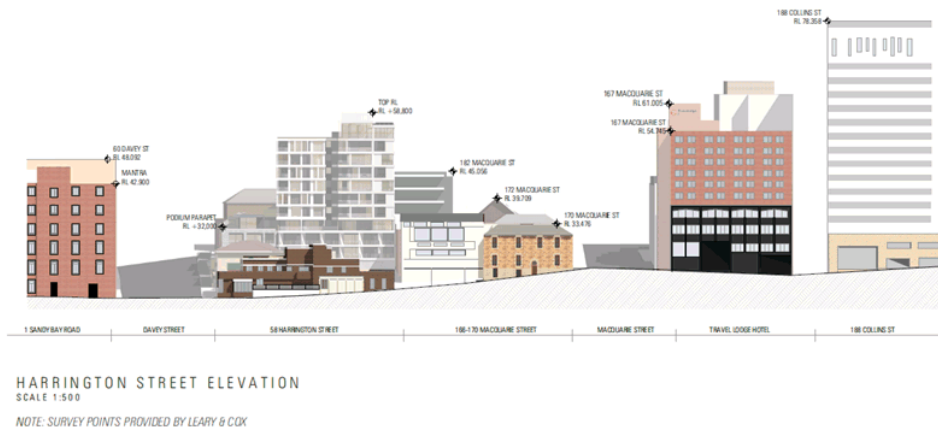


Figure 28: Context height study, illustrative Harrington Street elevation view showing proposal set within the surrounding urban form shown for context (source: JAWS Architects)

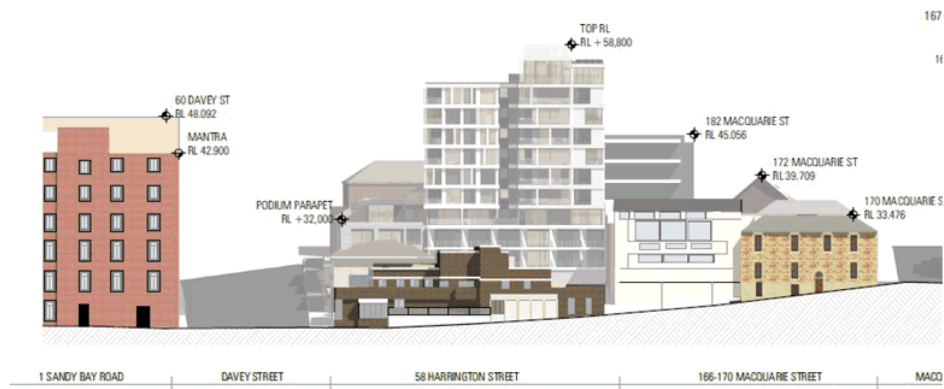


Figure 29: Enlarged, context height study, illustrative Harrington Street elevation view showing proposal set within the surrounding urban form shown for context (source: JAWS Architects)

3.7 PROPOSAL SCALE & MASSING RELATIVE TO ADJACENT PUBLIC REALM

- 3.7.1 The proposed buildings can be considered not only in relation to existing buildings but also in relation to the immediately adjacent public realm of streets and spaces. Two notable public realm considerations for the urban setting of the proposal site are streets and open spaces. The application site has direct frontage to Davey Street, a wide city street, and is located close to St. David's Park, to the north east of the site across Davey Street.
- 3.7.2 Wide city streets and public parks provide valuable public realm and setting for built form including the proposed development. The figure below illustrates the proposal site (in blue) relative to the context of urban form buildings, streets and public open space (St David's Park).

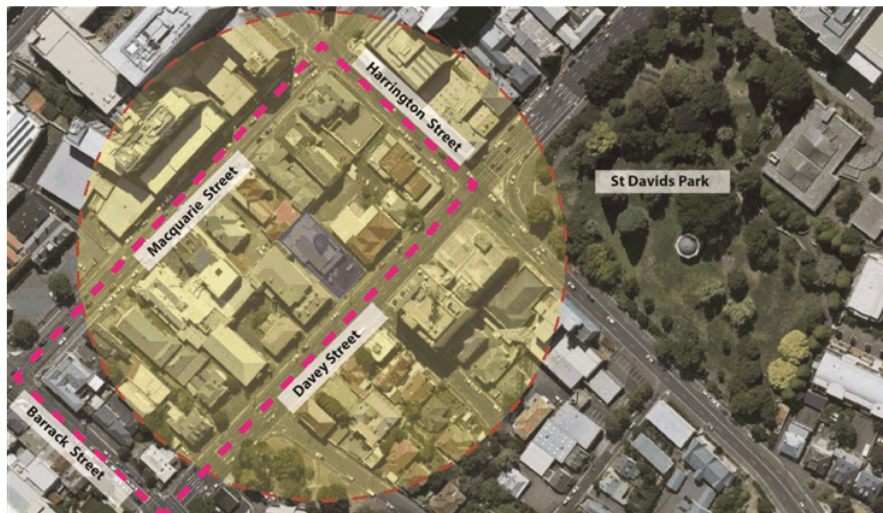


Figure 30: Aerial photo illustrating urban context including public realm of city streets and St David's Park within 100m radii of site (source: The LIST, www.theLIST.tas.gov.au © the State of Tasmania).

3.7.3 Considering these public realm features in turn:

- **Streets:** Davey Street is a wide city street of circa 22m (boundary edge to boundary edge, either side of the street including footpaths and carriageway) in front of the site. This street consists of public footpaths on both sides of the vehicular carriage way. The street has a strong urban character, lined on both sides by a variety of built form including many buildings of heritage and historic character. Davey Street, as a wide street, has the potential to accommodate new buildings of the scale of the proposal.
- **Spaces:** St David's Park is to the north east of the site, on the opposite side of Davey Street. The entrance to the park on the corner of Davey Street and Harrington Street/Sandy Bay Road is within 100m of the site as illustrated in the figure above.

St David's Park is one of Hobart's most important public open spaces, a rich landscape setting and heritage asset for the city. The scale of the park is significant, larger than some of the individual urban blocks that surround it. The park has an approximate area of 1.8 hectares, measuring up to approximately 140m width between Sandy Bay Road and Salamanca Place, and up to approximately 150m width between Davey Street and the Salamanca Mews residential apartments.

3.7.4 The setting of Davey Street and nearby St David's Park provides valuable public realm in front of, and close to the application site. This open space setting is important to note in relation to plot ratio considerations, notably the wider public open space in front of a plot the greater the capacity for the site to accommodate taller built form.

3.7.5 Views of the public realm setting of the site, as viewed from Davey Street and this street edge of St David's Park are presented in the following figures.

View 1: From Corner of Davey Street and Sandy Bay Road (see Figure 31)

3.7.6 This view is taken from close to the entrance to St David's Park, at the corner of Davey Street and Sandy Bay Road, facing in a south west direction looking towards the proposal site.

Existing view

3.7.7 The existing view is framed by the Mantra Hotel building to the left side of the photograph, which at 6 storeys building height provides a strong built form edge to the corner of the street. Davey Street rises up as it heads in a south west direction, with existing built form lining the street edge with limited building setback and a regular pattern of building height. Taller built form is visible, including the Ibis Styles Hotel, to the right of the photo, one block away on Macquarie Street.

Proposal view

3.7.8 The photomontage image presented under the existing view shows the proposed building positioned in the setting of the existing urban form. The front building block element of the proposal has a scale and mass comparable to neighbouring buildings that also directly front onto Davey Street. The taller elements of the proposal are clearly visible, albeit stepped back from the street frontage and set back within the urban block. The nearest comparable building of similar height in this view is the Ibis Styles Hotel, to the right of the photo, one block away on Macquarie Street.

View 2: From Davey Street approaching St David's Park (see Figure 32)

Existing view

- 3.7.9 The existing view is framed by the St David's Park to the left side of the photograph, with matures both within the park and lining Davey Street providing shade and a sense of enclosure to this side and stretch of Davey Street. Davey Street in the foreground is on relatively level ground, before it rises up as it heads in a south west direction. Existing built form lining the street edge opposite the park has zero or very limited building setback and a varied pattern of building height and architectural character including heritage buildings, church with spire and more modern additions.

Proposal view

- 3.7.10 The photomontage image presented under the existing view shows the proposed building positioned in the setting of the existing urban form. The top level of the front building block element of the proposal is just visible with a scale and mass comparable to neighbouring buildings that also directly front onto Davey Street. The taller elements of the proposal are clearly visible, immediately beyond the church tower and spire in the middle ground. The taller elements of the proposal again appear stepped back from the Davey Street frontage and set back within the urban block.



BEFORE: Corner of Davey Street and Sandy Bay Road



AFTER: Corner of Davey Street and Sandy Bay Road

Figure 31: View of site and proposal, 'before & after' images. Top image showing existing view of the site in urban context, facing south west along Davey Street from corner of St David's Park (the intersection between Sandy Bay Road and Davey Street). Bottom image showing illustrative rendered image of the proposal, set in urban context. (source: JAWS Architects)



BEFORE: Davey Street



AFTER: Davey Street

Figure 32: View of site and proposal, as 'before and after' images. Top image showing existing view of the site in urban context, facing south west along Davey Street from corner of St David's Park (as Salamanca Place joins Davey Street). Bottom image showing illustrative rendered image of the proposal, set in urban context. (source: JAWS Architects)

3.8 PROPOSAL RELATIVE TO HERITAGE CONTEXT

3.8.1 The site, as situated within the extent of the H1 Heritage Precinct is illustrated in the figure below.

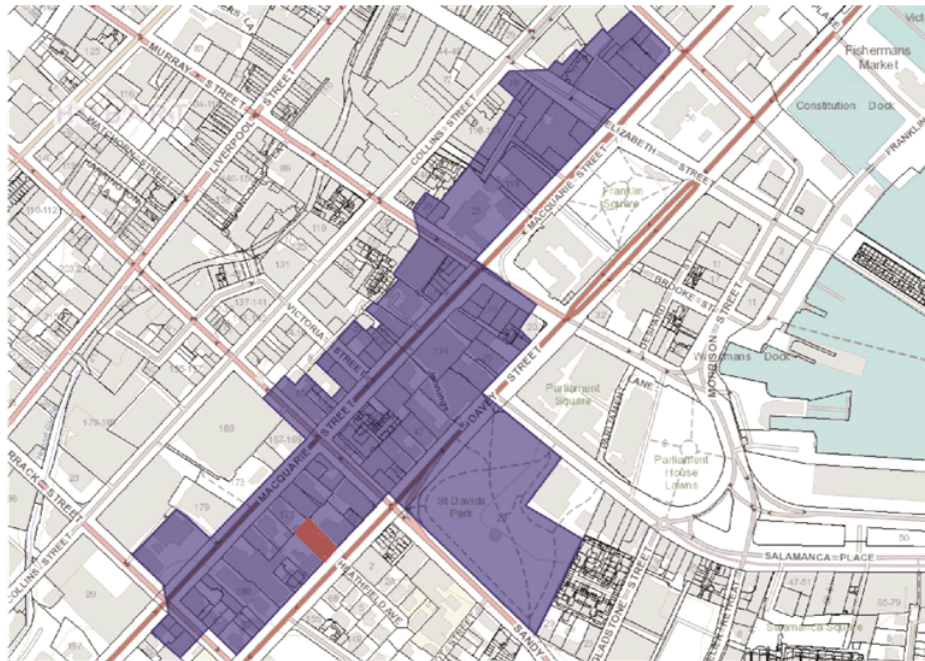


Figure 33: Extents of Heritage Precinct H1 - City Centre (in blue), site in red.
(Source: www.theLIST.tas.gov.au © the State of Tasmania).

- 3.8.2 Acknowledging the sites location within the Heritage Precinct H1 and with immediately neighbouring buildings of historic heritage value, the proposal has been designed with a front building block of lower building height, creating a proposed street frontage presence that is comparable to the scale and mass of the existing, neighbouring heritage buildings, as illustrated in figures below.
- 3.8.3 The front elevation of the proposed building has a pattern of fenestration that has rhythm and scale similar to neighbouring heritage buildings, notably the frontage comparable scale and fenestration pattern to neighbouring 67 Davey Street, as illustrated in the proposed street elevation in the figure below.
- 3.8.4 The taller building element of the proposal is set back within the plot and the urban block, limiting the direct impact upon the heritage buildings that predominantly have a street frontage presence. The more contemporary extension to the St Helens private hospital provides a neighbouring precedent for taller building form set back within the urban block.
- 3.8.5 Further detail of the heritage setting and the proposal response to heritage is provided in the heritage reports prepared by Paul Davies Pty Ltd, submitted as part of the development application.



Figure 34: Plan view showing proposal (bounded in red), with surrounding urban form shown for context, including heritage buildings & plots in blue (source: JAWS Architects)



Figure 35: Context height study, enlarged to show illustrative street elevation with the proposal in context of neighbouring buildings with frontage to Davey Street (source: JAWS Architects)

4. SUMMARY

- 4.1.1 This urban form supporting statement presents analysis of the existing urban form and the assessment of the proposed development at 63 Davey Street, in the local urban context of this part of central Hobart.
- 4.1.2 With consideration of the above listed features the proposed built form has been designed in a way that is considered to be compatible within the urban form setting of Davey Street and surrounding urban streets and blocks. The variety of buildings (in terms of character, age, scale, mass, height and appearance) is notable throughout the central area of Hobart, including in the local urban context around the site on Davey Street and nearby city streets including Macquarie Street, Harrington Street and Collins Street.
- 4.1.3 The proposed building form and heights are compatible with the existing pattern of urban form presented by existing buildings to streets and urban blocks in the local urban context of the site, as illustrated in the figures and associated commentary in this statement.



15 May 2020

Ben Ikin
Hobart City Council
GPO Box 503
HOBART TAS 7001
(Submitted through e-Portal)

Dear Ben,

FURTHER INFORMATION - 63 DAVEY STREET, HOBART

I am writing in response to your letter of the 17/06/19 requesting further information in response to the proposed development at 63 Davey Street, Hobart (PLN-19-319).

This application has been on hold pending the outcome of the 58 Harrington Street Appeal. In response to that decision, the application has been revised as per the accompanying documents which also addresses the outstanding RFI below.

Stormwater Management Code

SWFi1

To enable the Council to assess the application against clause E7.7.1 of the Stormwater Management Code of the Hobart Interim Planning Scheme 2015, please provide the following:

a) A concept stormwater treatment report prepared by a suitably qualified person, including associated plans and calculations, demonstrating that the proposed stormwater system will achieve the State Stormwater Strategy targets. Council notes that a carpark treatment should target fine sediments and hydrocarbons.

Previous advice from Council's engineer (Robin Cooper) confirmed that SW treatment is not required as the carpark is located on the basement level and will not be exposed to direct rainwater that would require drainage. This position has been adopted for a number of other developments where basement car parking is provided.

As per the stormwater report provided as part of the submission, stormwater runoff from the site will not increase over existing and the proposed system is suitable to cater for the development.

b) Please provide a plan and indicative long section detailing the proposed stormwater connection. This plan must show any potential clashes with other services, including third party private services, walls, and crossovers.

Please refer to the revised civil documents.

TasWater - TW1

With regard to the TasWater RAI, please refer to the attached advice from the civil engineers which responds to points 1 & 2.

3. Provide a construction management plan detailing how all existing customers will maintain current levels of TasWater sewer service during the proposed construction and re-alignment of the sewer main.

Advice from TasWater has indicated that a CMP is not required at this stage and a note has been added to the attached civil plans to indicate that sewer service to adjoining properties will be maintained at current levels during the proposed construction and realignment.

A construction management plan will be provided as part of design development for building approval.

4. The location of the property water connection / water meter assembly must provide for unfettered access to enable reading, testing, inspection, maintenance and exchange without impediment and must be kept clear of obstructions at all times. The current location of the water meter seems to be accessible via a garage door. TasWater would prefer the meter set be located free of access constraints. Alternatively, an access plan must be provided stating how to access, maintain or replace water meters 24/7/365 and enable the water meters to be read 365 days a year between 7am and 7pm.

As detailed on the civil plans, the property water meter is located beyond the garage door and will be accessible at all times, without obstruction.

5. The proposal requires works on the adjacent property 186 Macquarie St, HOBART (C.T. 110411/1) and thus the applicant is required to satisfy Land Use Planning and Approvals Act 1993 (No. 70 of 1993) - Section 52. What if applicant is not the owner? Please provide written confirmation that this has been satisfied.

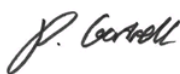
6. Please provide a set of title documents for 186 Macquarie St, HOBART (C.T. 110411/1) - Folio Plan, Folio Text, Schedule of Easements and Council Certificate Page (note that sometimes a Schedule or Council Certificate Page may not be available - so delete).

Please find attached copy of the title documents for 186 Macquarie Street.

Confirmation from the engineer has indicated that works may be required on the adjoining property at 186 Macquarie Street, due to the need to upgrade an existing sewer main connection. Further details regarding this potential upgrade will be determined during design development. The landowners have been notified in accordance with S52 of the Land Use Planning and Approvals Act 1993.

If you have any further queries in relation to any of the above, please contact me on 6234 9281.

Yours sincerely,



Phil Gartrell
Planner
IRENEINC PLANNING & URBAN DESIGN



7 July 2020

Cameron Sherriff
Hobart City Council
GPO Box 503
HOBART TAS 7001
(Submitted through e-Portal)



Dear Cameron,

FURTHER INFORMATION - 63 DAVEY STREET, HOBART

I am writing in response to your letter of the 19/06/20 requesting further information in response to the proposed development at 63 Davey Street, Hobart (PLN-19-319).

Tas Water

TW1

Information to satisfy the enclosed additional information request from TasWater (TasWater Reference No. TWDA 2019/00782HCC dated 19 June 2020).

Please see attached revised civil documentation prepared by Aldanmark.

Planning

PLN Fi1

Clarify the proposed use/s of the development. Please provide a more detailed break down of the proposed uses and how they intend to operate in conjunction with one another. Where necessary please make changes to the supporting planning report before resubmitting.

A revised planning report is currently being prepared and will be submitted shortly to address PLN Fi1.

Parking and Access

PA1

Please provide documentation for assessment against Clause E6.7.1 A2/P2.

Advice:

Council consider that the access proposed to service the right of way on the title is an existing access off of the title of 63 Davey Street and the proposed new access constitutes a second access which requires assessment against E6.7.1 A2/P2.

A revised response to Clause E6.7.1 A2/P2 will be provided in the revised planning report, which will be submitted shortly.

smithstreetstudio

ireneinc

49 Tasma St, North Hobart, TAS 7000

Tel (03) 6234 9281

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PLANNING TAS PTY LTD TRADING AS IRENEINC PLANNING & SMITH STREET STUDIO PLANNING & URBAN DESIGN

ABN 78 114 905 074

PA2.1

Please provide scaled and dimensioned drawing(s) demonstrating the vehicular access design, or a design that provides safe and efficient access.

To satisfy Hobart Interim Planning Scheme 2015 clause E6.7.2 Acceptable Solution A1/P1, Tasmanian Standard Drawings and AS/NZS 2890.1:2004 Section 3, the scaled and dimensioned design drawings must include:

- *Plan view showing the location and dimensioned width of the combined vehicular crossover with the neighbouring property.*

Please refer to the revised civil documentation which addresses PA2.1.

PA5.1

Please Scaled and dimensioned plan(s) showing the layout of commercial vehicle facilities for loading, unloading or manoeuvring designed to comply with AS/NZS 2890.2:2002 or a design which ensures that parking areas enable safe, easy and efficient use.

To satisfy Hobart Interim Planning Scheme 2015 clauses E6.7.13 Acceptable Solution A1 the scaled and dimensioned design drawings must include:

- *A layout of commercial vehicle facilities for loading, unloading or manoeuvring that is designed to comply with AS/NZS 2890.2:2002*

Where the design drawing(s) do not comply with the above clauses, provide a certification by a suitably qualified engineer that the design is safe and ensures ease of access, egress and manoeuvring on site. This will then be assessed under performance criteria of the Hobart Interim Planning Scheme 2015.

Advice:

Council's cleansing and solid waste unit have advised that the number of residential / visitor accommodation units proposed are unable to be collected at the kerb by Council's waste trucks.

It is noted that the consultant planning report states that the TIA indicates waste vehicles can reverse into the driveway and drive out in a forward direction or collection can occur using the parking lane. As this AS2890.2 Section 3.2.3, please provide documentation from the Road Authority (Department of State Growth) that they are satisfied that the proposed waste vehicle movements will not compromise the safety and convenience of vehicular traffic, cyclists, pedestrians and other road users; and that they are aware of the scale of waste removal proposed from the kerb.

Please see attached letter from Milan Prodanovic addressing PA5.1. Advice from State Growth has been requested and will be provided along with the revised planning report shortly.

Stormwater Code

SW Fi1

To enable the Council to assess the application against the relevant provisions of the Stormwater Management Code of Hobart Interim Planning Scheme 2015, please provide:

(a) A concept stormwater treatment report prepared by a suitably qualified person, including associated plans and calculations, demonstrating that the proposed stormwater system will achieve the State Stormwater Strategy targets. Council notes that a carpark treatment should target fine sediments and hydrocarbons.

(b) Please provide a plan and indicative long section detailing the proposed stormwater connection. This plan must show any potential clashes with other services, including Third party private services, walls, and crossovers.

Advice for submitted plans: It is noted on the submitted 'Detailed Ground Floor Plan' 18E513 C1.01 Rev G:

Please confirm that this pit does or does not connect to the manhole mentioned on 186 Macquarie St, and also, if this manhole contains private (maybe shared?) stormwater infrastructure, show where this drains to (perhaps the kerb and gutter outlet on the downhill / north east side of the 186 Macquarie St cross over?). If this is the case provide details of accommodating this infrastructure in the redevelopment of the 186 Macquarie St access, and the proposed development.

Please show the stormwater kerb and gutter connection for 61 Davey Street it appears to be in approximately the location you propose the new subject site connection. Please provide the proposed clearance from the crossover for 61 Davey Street, noting the wing is not the standard width. From the stormwater long sections on C2.02 RevD it is not clear that the required clearance from the two bundles of conduits can be achieved. Please comment on the feasibility of these proximities and any possible alternative connection location should it be required.

The calculated flows are larger than generally accepted for new development (<12L/s) to kerb and gutter. Please confirm that your proposed flow rate of 17.2L/s in the proposed designed alignment will be contained within the gutter.

Please refer to the revised civil documents and revised stormwater report which address points (a) and (b).

Engineering Road - Infrastructure in a Road Reservation

ENGR Fi2

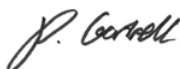
To ensure that the Council's road infrastructure is protected please provide:

1. Show and label the location and extent of retaining walls, footings, excavations adjacent or within the highway reservation, including modification or demolition of existing structures.
2. Show existing and proposed long and cross sections of footpath in accordance with TSDR11v1.

Please refer to the revised civil documentation.

If you have any further queries in relation to any of the above, please contact me on 6234 9281.

Yours sincerely,



Phil Gartrell
Planner
IRENEINC PLANNING & URBAN DESIGN



63 Davey Street Heritage Considerations

The following review considers the statutory heritage provisions for the site and area that affect potential proposals for development on the site. The assessment has been prepared by Paul Davies, Heritage Architect.

STATUTORY LISTINGS AND CONTROLS

HISTORIC CULTURAL HERITAGE ACT 1995 (As amended)

The property at 63 Davey Street is not listed on the Tasmanian Heritage Register.

The adjacent properties at 61 and 65 Davey Street are listed on the Tasmanian Heritage Register and are described as the RAAF Memorial Centre and Johnsons Edgehill Terraces. The properties to the rear of the site in Macquarie Street are also listed on the Tasmanian Heritage Register.

As the Act only applies to listed sites there are no considerations under the Act that require addressing.

HOBART INTERIM PLANNING SCHEME

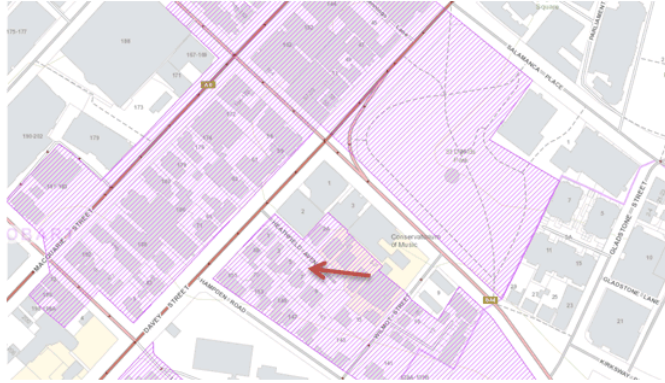
The Hobart Interim Planning Scheme 2015 applies to the site.

The site is not heritage listed.

The site is located within the H1 Heritage Precinct under the Hobart Interim Planning Scheme 2015.

The subject property is also located within an area - Central Hobart - identified in the Hobart Interim Planning Scheme 2015 Table E13.4 Places of Archaeological Potential as having potential to contain archaeological remains and therefore application is also required to address the provisions in the planning scheme for Places of Archaeological Potential.

The adjoining sites are listed items on the heritage schedule of the Planning Scheme.



Extract from Hobart Planning Scheme showing the site and the heritage precinct

The following table sets out the relevant heritage provisions of the Planning Scheme with comment on how they apply to the site and how they have been considered.

Table: Response to Heritage Precinct Attributes set out in the Hobart Planning Scheme.

Precinct Attribute Table E 13.2	Application to 63 Davey Street
It contains some of the most significant groups of early Colonial architecture in Australia with original external detailing, finishes and materials demonstrating a very high degree of integrity, distinctive and outstanding visual and streetscape qualities.	The site does not represent Colonial architecture and consequently cannot contribute to the heritage value of the streetscapes within the precinct. There are no heritage values on the site or the buildings currently located on it.
The collection of Colonial, and Victorian buildings exemplify the economic boom period of the early to mid nineteenth century.	This applies to surrounding buildings, currently the use of the front of the site as a carpark detracts from these values.
The continuous two and three storey finely detailed buildings contribute to a uniformity of scale and quality of street space.	The form of building on the site is not one that is recognised as an attribute of the area nor does it form part of the significant built fabric of the precinct.
It contains a large number of landmark residential and institutional buildings that are of national importance.	It is not a landmark residential or institutional building of national importance and cannot represent this value.
The original and/or significant external detailing, finishes and materials demonstrating a high degree of importance.	Not applicable.

ASSESSMENT OF HERITAGE IMPACT AGAINST HOBART INTERIM PLANNING SCHEME 2015 HERITAGE OBJECTIVES & CONTROLS

Considering the heritage precinct overlay applying to the subject site, any redevelopment must be assessed against the provisions of the Historic Heritage Code under the Hobart Interim Planning Scheme, 2015, as the site is located within the H1 City Centre Heritage Precinct.

The relevant planning objectives and controls are contained within Section E13.0 Historic Heritage Code of the Hobart Interim Planning Scheme 2015. These objectives and controls are addressed below.

The Hobart City Centre has also been identified as a place of archaeological sensitivity.

As noted earlier, it is also important to consider the height controls as it is difficult to separate the concepts of heritage and height and scale when considering development.

The design of the new building has responded to the setting of Davey Street.

The approach of developing a streetfront form that is responsive to the streetscape is sound and required under the code.

The question that then follows is how does the proposal overall, that is behind the streetscape form, relate to the site, the city and the precinct.

The Interim Planning Scheme establishes heights for precincts and building envelopes and has a range of overlays that have the potential to impact other more straightforward controls. On this site the height and envelope controls are quite clear, but they are overlaid with the heritage controls.

The two areas of control in the Planning Scheme for this development are the height and envelope controls and the heritage overlay.

The height controls, while not a guarantee of a specific height, set out the intent of Council in zoning the site. There is a very clear expectation that even with the heritage overlay that this area is capable of development beyond the scale of what is currently provided. This can be stated with certainty as Council established the height controls with the understanding that the area had a heritage overlay. If, Council, as a result of the heritage overlay, had determined that a lower height should apply across the precinct, there would have been no difficulty in embedding

that in the Planning Scheme. That did not happen and the precinct has the current height limits and envelope controls.

A sound approach to managing height is to establish streetfront heights and setbacks and to reinforce the patterns of historic built form through referencing current forms. The scheme provisions require the first 15 metres of the site (in depth) to specifically consider adjacent heritage items (which establishes a setback control) but the Scheme does not establish a height control in this zone, rather it looks to a contextual solution to fit into the streetscape. The proposal has worked with the adjacent heights to determine the infill form and scale using the 15 metre setback control.

The articulation and management of form in this proposal is successful and achieves a balance on the site between heritage values and new forms.

A matter to consider is how the development is seen from key public locations. For this site they include views along Davey Street and from Macquarie Street (behind the buildings fronting Macquarie Street).

Davey Street is a main one-way arterial road with the predominant views up the hill. The front form fits within the streetscape and the rear higher form is setback as required.

Views down Davey Street, which are pedestrian views, will see the larger form of the building, but this will be impacted by inevitable development (even modest development) on adjacent sites that will remove some views of the side of the building.

The built form will also be seen looking from Macquarie Street where the built form will be seen set back and well behind the streetscape buildings.

The following section addresses the specific scheme controls that affect the site:

Table: Response to Heritage Precinct Development Standards set out in the Hobart Planning Scheme.

E13.8 Development standards for Heritage Precincts	
E13.8.1 Demolition	
Objective: To ensure that demolition in whole or in part of buildings or works within a heritage precinct does not result in the loss of historic cultural heritage values unless there are exceptional circumstances.	<p>The building to be demolished does not represent precinct heritage values and has no heritage significance.</p> <p>This objective is not affected by the proposal.</p>
Performance Criteria P1: Demolition must not result in the loss of any of the following:	Refer to comment above.
(a) buildings or works that contribute to the historic cultural heritage significance of the precinct;	
(b) fabric or landscape elements, including plants, trees, fences, paths, outbuildings and other items, that contribute to the historic cultural heritage significance of the precinct;	Refer to comment above.
E13.8.2 Buildings and Works other than Demolition	
Objective: To ensure that development undertaken within a heritage precinct is sympathetic to the character of the precinct.	<p>In summary the precinct values are respected by scaling the building to the street frontages to be consistent with the precinct significant built forms, using materials that respond to the character of the precinct and activating the street frontage. The building design has been developed to create well modelled and articulated forms that respond to the finer grain of at least parts of the precinct.</p> <p>The matter of overall scale is complex as the planning controls have specifically anticipated development of larger scale than presently exists (or the controls would not have been created as they now exist). A key consideration is how the potential scale of development provided for under the current controls can be located and managed on the site with the proximity of heritage buildings around (but not on) the site.</p> <p>Setting any higher forms back behind a streetscape element is a key design method to address scale.</p>

Performance Criteria P1: Design and siting of buildings and works must not result in detriment to the historic cultural heritage significance of the precinct, as listed in Table E13.2. Performance Criteria P2: Design and siting of buildings and works must comply with any relevant design criteria / conservation policy listed in	<p>The design has been developed in relation to precinct values.</p> <p>This is seen in the use of the lower street height forms to create a complimentary street edge condition to complete the streetscape form in the area, the recessive use of materials and the siting of the larger parts of the building well set back from principal view lines.</p> <p>Maintaining the height controls for the rear sections of the building is consistent with the intent of the Planning Scheme provisions.</p>
Table E13.2, except if a heritage place of an architectural style different from that characterising the precinct.	Not relevant
Performance Criteria P3: Extensions to existing buildings must not detract from the historic cultural heritage significance of the precinct.	Not relevant
<p>Performance Criteria P4: New front fences and gates must be sympathetic in design, (including height, form, scale and materials), and setback to the style, period and characteristics of the precinct.</p> <p>Acceptable solution A4: New front fences and gates must accord with original design, based on photographic, archaeological or other historical evidence.</p>	Not relevant
<p>Performance Criteria P5: The removal of areas of landscaping between a dwelling and the street must not result in the loss of elements of landscaping that contribute to the historic cultural significance or the streetscape values and character of the precinct.</p> <p>Acceptable solution A5: Areas of landscaping between a dwelling and the street must be retained.</p>	Not relevant
E13.8.3 Subdivision	
Objective: To ensure that subdivision within a Heritage Precinct is consistent with historic patterns of development and does not create potential for future incompatible development.	Not relevant

Performance Criteria P1: Subdivision must not result in any of the following: (a) detriment to the historic cultural heritage significance of the precinct, as listed in Table E13.2; (b) a pattern of subdivision unsympathetic to the historic cultural heritage significance of the precinct; (c) potential for a confused understanding of the development of the precinct; (d) an increased likelihood of future development that is incompatible with the historic cultural heritage significance of the precinct.	Not relevant
Performance Criteria P2: Subdivision must comply with any relevant design criteria / conservation policy listed in Table E13.2.	Not relevant
Performance Criteria P3 & P4	Not relevant to the H1 Heritage Precinct
E13.10 Development standards for Places of Archaeological Potential	
E13.10.1 Building, Works and Demolition	
Objective: To ensure that building, works and demolition at a place of archaeological potential is planned and implemented in a manner that seeks to understand, retain, protect, preserve and otherwise appropriately manage significant archaeological evidence.	A detailed archaeological assessment will be prepared for the site to guide future actions and management of possible archaeological resources. The assessment addresses separately all of the relevant Scheme requirements.
Performance Criteria P1: Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:	See above

<p>(a) the nature of the archaeological evidence, either known or predicted;</p> <p>(b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;</p> <p>(c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition;</p> <p>(d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;</p> <p>(e) measures proposed to preserve significant archaeological evidence 'in situ'.</p> <p>Acceptable solution A1: Building and works do not involve excavation or ground disturbance.</p>	See above
E13.2.2 Subdivision	
Objective: To ensure that subdivision does not increase the likelihood of adverse impact on a place of archaeological potential.	See above
<p>Performance Criteria P1: Subdivision must not impact on archaeological resources at Places of Archaeological Potential through demonstrating either of the following:</p> <p>(a) that no archaeological evidence exists on the land;</p> <p>(b) that there is no significant impact upon archaeological potential.</p>	See above

<p>Clause 22.4.1 P5 - Building height within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:</p> <p>(a) not unreasonably dominate existing buildings of cultural heritage significance; and</p> <p>(b) not have a materially adverse impact on the historic cultural heritage significance of the heritage place;</p>	<p>This provision (in part) requires the street front elements of new development to have a specific relationship to any adjoining heritage items. The adjoining buildings to either side in Davey Street are heritage items of approximately two storey scale and the control requires that a new building, to a depth of 15 metres on the site should not dominate those heritage elements.</p> <p>The proposal has addressed this by providing a form of the appropriate and related scale to the two heritage elements in the front part of the site with greater height set back behind the 15 metre line in the control.</p>
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SUMMARY

The proposal has been developed within the context of the site, the precinct and the Scheme controls. It has developed a form that infills the street frontage and sets back the larger element behind the required 15 metre setback. The building height complies with the Scheme provisions.

There is always a question of scale and how a new larger form integrates into an existing smaller scaled precinct. This has been addressed in the Scheme by establishing specific height controls and considering street infill and building adjacent to heritage items. These provisions are specifically included to address the addition of new larger built forms and this proposal has worked within that framework to create a built form that, though larger than existing developments fits within the framework that is established and the context of the area.

Paul Davies

B Arch MBEnv Bldg Cons AIA Chartered Architect Heritage Consultant

January 2019



63 Davey Street

Application of Planning Scheme Provisions

At a pre-DA presentation to the Hobart Planning Panel for a proposed development for this property, a discussion took place between the panel members and the applicant as to how the Planning Scheme provisions apply to the site with particular reference to height and the heritage overlay. To reduce the discussion to its central issue, the panel chair (Mr Curtis) suggested that the heritage overlay took precedence over other controls and that height, which was the focal point of the discussion, needed to respond to the existing character of the precinct irrespective of other controls.

The author of this response (Paul Davies) expressed an interpretation of the Planning Scheme that was different to this where the heritage overlay, while part of the Scheme, has to be read and interpreted in relation to all of the controls that apply to the site, particularly where other controls provide very specific controls that relate (in this case) to height and where the heritage controls are generic.

The Panel requested a further response to this question from Mr Davies to assist in Council's eventual determination of the proposal, noting that at this time the presentation was a pre-lodgement briefing.

There are a range of controls that apply to the site however, for the purpose of this assessment only those that address heritage, height, setbacks and related matters are considered as the assessment of general amenity is not relevant to the current discussion.

The fundamental problem that arises in the Scheme is the potential conflict between zoning and heritage. This is explored in this short document in an attempt to ascertain how the Scheme provisions should be applied to this site.

ZONING

The site is zoned Central Business Zone which is the most dense and intense zone within the Council area. The whole of the surrounding block and parts of adjoining blocks share this zoning but not sites to the east of Davey Street. Davey Street in this location forms the edge of the zone.

The zoning is clearly intentional (that is, it is not a default zoning that has caught a site by accident) as the mapping of the zone is very specific and this site is not on the perimeter of the zone. This is seen in the way the edges of the zone are carefully mapped around sites and not just following street alignments.

The site also does not form part of a transitional area where there is a significant change in character from one zone to another.

The zone purpose statement is set out in the Scheme:

22.1.1 Zone Purpose Statements

22.1.1.1 To provide for business, civic and cultural, community, food, hotel, professional, retail and tourist functions within a major centre serving the region or sub-region.

22.1.1.2 To maintain and strengthen Hobart's Central Business District and immediate surrounds including, the waterfront, as the primary activity centre for Tasmania, the Southern Region and the Greater Hobart metropolitan area with a comprehensive range of and highest order of retail, commercial, administrative, community, cultural, employment areas and nodes, and entertainment activities provided.

22.1.1.3 To provide a safe, comfortable and pleasant environment for workers, residents and visitors through the provision of high quality urban spaces and urban design.

22.1.1.4 To facilitate high density [residential development](#) and visitor accommodation within the activity centre above ground floor level and surrounding the core commercial activity centre.

22.1.1.5 To ensure [development](#) is accessible by public transport, walking and cycling.

22.1.1.6 To encourage intense activity at pedestrian levels with shop windows offering interest and activity to pedestrians.

22.1.1.7 To encourage a network of arcades and through-site links characterised by bright shop windows, displays and activities and maintain and enhance Elizabeth Street Mall and links to it as the major pedestrian hub of the CBD.

22.1.1.8 To respect the unique character of the Hobart CBD and maintain the [streetscape](#) and townscape contribution of places of [historic cultural heritage significance](#).

22.1.1.9 To provide a safe, comfortable and enjoyable environment for workers, residents and visitors through the provision of high quality spaces and urban design.

The statements are focussed on the future development of Hobart and seek, for the site and its surrounds as well as the broader area of the zone, high density residential development and visitor accommodation around the core of the city, near public transport and facilities with high quality building and urban design.

The zone objectives address heritage values at 22.1.1.8 where development is required to:

"respect the unique character of the Hobart CBD and;

maintain the [streetscape](#) and townscape contribution of places of [historic cultural heritage significance](#)."

This is the only reference to heritage under the zoning controls.

The wording of the objective is important as the statement specifically refers to 'places' and what is of [historic cultural heritage significance](#). The Scheme states that 'places' include individual heritage sites and heritage precincts consequently, the site falls under this objective.

The [historic cultural heritage significance](#) of each precinct is defined in the precinct statement (see section on heritage overlay) as set out in the Heritage Code.

The heritage precinct that covers the area is H1 City Centre. It is a broad precinct that covers part of the Central Business Zone but also extends into other zones beyond it.

HEIGHT IN THE PLANNING SCHEME

Specific height controls apply to the zone as well as height objectives.

Clause 22.4.1 Objective: That [building height](#):

- (a) contributes positively to the [streetscape](#) and [townscape](#);
- (b) does not unreasonably impact on historic heritage character;
- (c) does not unreasonably impact on important views within the [urban amphitheatre](#);
- (d) does not unreasonably impact on [residential amenity](#) of land in a [residential](#) zone; and
- (e) provides significant community benefits if outside the [Amenity Building Envelope](#).

(a) contributes positively to the [streetscape](#) and [townscape](#);

- Streetscape is defined in the general Zone provisions (but not the heritage code) as: *the visual quality of a street depicted by [road](#) width, street planting, characteristics and features, public [utilities](#) constructed within the [road](#) reserve, the setbacks of buildings and structures from the [lot](#) boundaries, the quality, scale, bulk and design of buildings and structures fronting the [road](#) reserve.*

For the purposes of determining [streetscape](#) with respect to a particular [site](#), the above factors are relevant if within 100 m of the [site](#)."

It is clearly focussed on the immediate street setting and while it refers to building design it does not refer to heritage values.

- Townscape means *"the urban form of the city and the visual quality of its appearance, it includes the urban landscape and visual environment of the city. As a concept it strives to give order to the form of the city, the pattern of landscape and [development](#) of the urban landscape."*

This is a broad and conceptual understanding of how the city fits together. • This is a relatively subjective test as 'positively' is not defined. It must relate to the zone objectives which relate to the overall character of the zone.

- It is a question of detailed design as a well-designed building will respond better than a poorly designed building.
- If the design of the required lower streetscape part of a development is appropriate it satisfies the streetscape requirement, if the potentially higher rear form complies with the height controls and is also well designed it can satisfy the townscape requirement.

- If a building is within the height and setback controls it must by definition be capable of making a positive contribution unless there are exceptional site circumstances to consider. An assessment of the actual design is of course required.

(b) does not unreasonably impact on historic heritage character;

This can be understood in the following way:

- The statement is qualified, it does not say have NO impact, it acknowledges that there is likely to be an impact as any development will have some impact. The term used is 'not unreasonably impact'. This is subjective and depends on what the individual considers to be reasonable or conversely unreasonable.
 - It is necessary to refer to other parts of the Scheme to assist in understanding what may be reasonable or unreasonable as it is not simply a matter of the taste of an individual assessor. It can be safely concluded that a development outside the defined controls will most likely not be reasonable as the controls must be intended to define the limits of 'reasonable'.
 - It cannot however be assumed that simply complying with empirical controls will automatically be reasonable. That does not factor in the need to respond to setting and character in a well-designed and nuanced way.
 - The key that is reinforced in the Scheme is the need to strictly limit street front development by carefully fitting it into existing heights and setbacks and then addressing scale and larger forms behind. Whether this is an ideal outcome is not in question, it is what the Scheme mandates by a detailed set of specific controls. Conversely, if the Scheme did not want to see greater heights in locations such as the Davey Street location, the Scheme provisions could have: adopted a different zoning; applied a different height control; set out a different amenity envelope; added specific height provisions into the heritage overlay; etc. There is a fundamental assumption in the construction of the Planning Scheme that allows height across the zone provided the specific controls of the Scheme are followed.
 - A valid conclusion to determining what is a reasonable level of impact (to reverse the emphasis in the objective) is to start with - on the one hand - a complying built form, then consider other planning and amenity matters and - on the other hand - look at the potential of the location, not just the immediate site for development to ascertain how the intended scale and form that the zone anticipates may be managed across the area to then determine how a specific development can fit into a future change of scale and character (around heritage values) or 'stand-alone' if other development did not take place.
- It cannot be assumed that future development on adjacent or nearby sites will or will not occur but both scenarios need to be considered.
- In summary the test is not whether there is any adverse heritage impact, it is whether any impact is reasonable within the framework of the whole Planning Scheme.

(c) does not unreasonably impact on important views within the [urban amphitheatre](#);

- Important views are mapped in the Scheme at figure 22.6. This is the only definitive analysis of views in the Scheme.

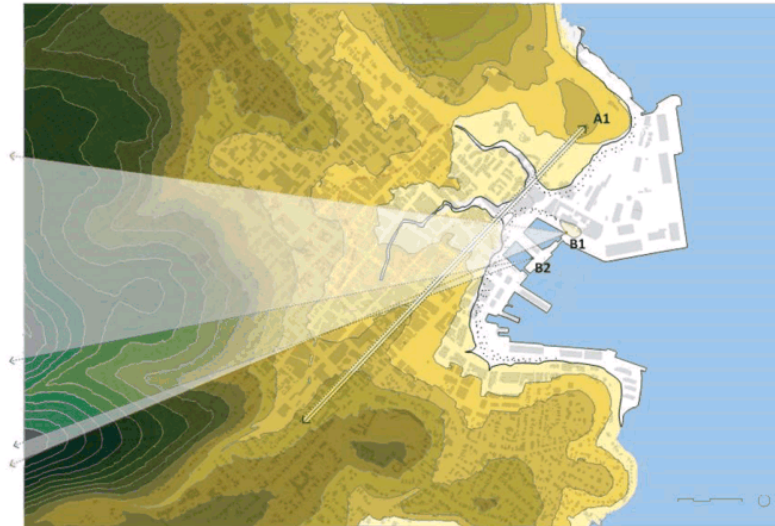


Fig. 22.6 View Lines and View Cones

Legend :

A 1 : Macquarie Street to / from Cenotaph
View line width = street width

B 1 : Hunter Street (above Hunter Island) to kunanyi (Mount Wellington)
Cone Width : 22° 21' at horizon, 32° extent of arc
Cone Elevation : 7° 55', Base of cone : 6° 41'
View Point :
E : 474822.332
N : 658943.174

B 2 : Franklin Wharf (Constitution Dock edge - 10 m from SE corner) to face of kunanyi (Mount Wellington)
Angle from horizontal : 8° 34'
Building edges (left) : 81° 49' (upper) 85° 16' (lower)
Building edges (right) : 81° 33' (upper) 82° 30' (mid) 85° 16' (lower)
View Point :
E : 474685.740
N : 658836.092

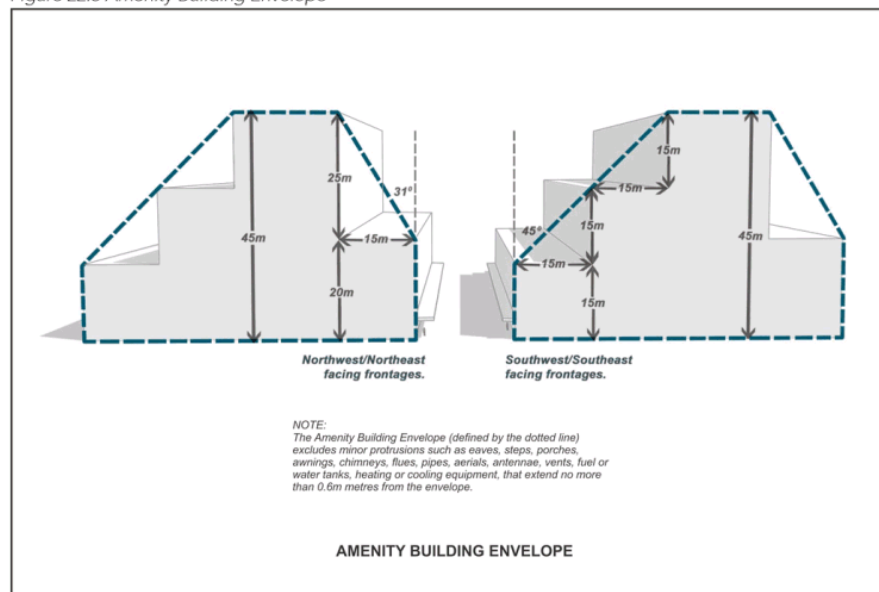
- The urban amphitheatre is defined as "means the setting of central Hobart including the layered rise of landforms rising from the water plane datum to the landform horizons (see Figures 22.7, 22.8 and 22.9)." The diagrams explain the concept of an amphitheatre, however it is conceptual and the diagrams offer little assistance in how development should take place or relate to the amphitheatre form.
- The proposed development does not fall within the viewlines of the two nominated locations for important views in Figure 22.6.
- There are locations in Davey Street where the new built form will affect horizon and hilltop views, that is inevitable as any development will potentially do this depending on where the viewer is located. However, there are no nominated or identified view locations within the Scheme that are affected.
- Any street front development will affect longer views.

- (d) does not unreasonably impact on [residential amenity](#) of land in a [residential zone](#); and
- This does not apply as the area is not currently residential
- (e) provides significant community benefits if outside the [Amenity Building Envelope](#).
- The current proposal is not outside the Amenity Building Envelope and consequently this does not apply.

Amenity Building Envelope

The Amenity Building Envelope is set out in the Planning Scheme as a base height and form control. While it references heritage, it applies across the whole of the zone and is not specific to or outside the heritage overlay.

Figure 22.3 Amenity Building Envelope



Footnotes

The Amenity Building Envelope has been developed with regard to heritage, streetscape and sense of scale, wind tunnelling effects and solar penetration.

The 20m height at the northwest/northeast facing frontages maintains a 1:1 ratio of street building height for the purposes of townscape aesthetics and maintaining a human scale.

The 15m height and subsequent 45 degree building envelope angle at southwest/southeast facing frontages maintains sufficient solar penetration to the opposite side of the street and also helps to control air and wind turbulence.

The Amenity Building Envelope is shown by the thick dotted line. The 15m setbacks for the 'steps' of development shown within the envelope are suggestive only. Development does not have to comply with the suggested 15m setbacks in order to comply with the envelope.

The Amenity Building Envelope controls are very specific in how they were developed and apply. The above extract from the Scheme has been underlined to mark the comments that relate to heritage, town and streetscape, aesthetics and scale. The control is intended to guide development on sensitive sites.

In detail the control says:

<i>Amenity envelope Control Statement</i>	<i>Comment</i>
<i>1 The Amenity Building Envelope has been developed with regard to heritage, streetscape and sense of scale</i>	<p>This is a control that has considered, presumably in some detail, how to infill within the central zone for amenity issues and:</p> <ul style="list-style-type: none"> - heritage - streetscape - sense of scale. <p>These are the matters that have to be addressed under the Scheme and within the Heritage Code. This is a specific control, that if complied with provides a framework to design new larger development in the zone.</p> <p>It is not limited to non-heritage areas or to just heritage sites, it applies across the Zone. There is no qualification on where the envelope may or may not apply.</p>
<i>2 The 20m height at the northwest/northeast facing frontages maintains a 1:1 ratio of street:building height for the purposes of townscape aesthetics and maintaining a human scale.</i>	<p>This sets out that the scale of the city (generally) at streetfront, using the 20 metre control (as a maximum and noting that it is less in other situations), achieves the desired townscape aesthetic and human scale that Council desire for Hobart.</p> <p>This is not a specific reference to heritage, but given the density of heritage sites within the city and the way in which streetfront controls and setbacks are established around those sites (specific controls for building adjacent to heritage sites), the primary concern of Council in the city is managing street front height with suitable setbacks to larger development.</p> <p>It is noted that a 15 metre height is included for sites that will overshadow the street and this is to achieve an environmental outcome, where the 20 metre height is about urban form.</p>
<i>3 Development does not have to comply with the suggested 15m setbacks in order to comply with the envelope.</i>	<p>The discretion here is valid as setbacks do need to be considered on a site by site basis even if there is a minimum setback recommended.</p>

This control does not negate heritage considerations but as it is developed specifically with regard to how to protect heritage values (amongst others), it must be given considerable weight in determining how new larger developments should fit into the city.

Heritage Code

The subject site is not a heritage item and consequently the provisions that apply to an individual place do not have relevance. It is also not a place on the Tasmanian Heritage Register and as a consequence the Tasmanian Cultural Heritage Act has no direct relevance to the development.

The site is within a heritage precinct and the heritage code applies. Its stated purpose is:

E13.1 Purpose

E13.1.1 To recognise and protect the [historic cultural heritage significance](#) of places, precincts, landscapes and areas of archaeological potential by regulating [development](#) that may impact on their values, features and characteristics.

E13.2 Application

E13.2.1 This code applies to [development](#) involving land defined in this code as any of the following:

- (b) a [Heritage Precinct](#);
this is defined in the Scheme as
means an area shown on the planning scheme maps as a [heritage precinct](#) and described in Table E13.2 as having particular [historic cultural heritage significance](#) because of the collective heritage value of individual places as a group for their [streetscape](#) or [townscape](#) values.

The purpose is broad, and it is achieved by regulating development. That regulation is set out in the Scheme in its totality. The values, features and characteristics are those set out in the precinct character statement as identified in clause E13.2.1. There is no other material available beyond the Table E13.2 statement on which to base an assessment.

This is not then an open-ended consideration. For an individually listed place the THC data sheet may provide (though generally not) some understanding of heritage values or there may be potential to provide a site specific heritage assessment. However, for a precinct, the statement in the Scheme provides that information.

The purpose in the Heritage Code goes beyond the zone objectives as it refers to a collective value of places and references both streetscape and townscape, however it is also limited by reference to Table E13.2 to which it gives considerable weight.

Clause 13.8.2 addresses works in a heritage precinct

The objective set out in clause 13.8.2 is: "To ensure that [development](#) undertaken within a [heritage precinct](#) is sympathetic to the character of the precinct".

There are no acceptable solutions that are relevant. The relevant performance criteria under this clause are:

Performance Criteria
P1 Design and siting of buildings and works must not result in detriment to the historic cultural heritage significance of the precinct, as listed in Table E13.2.
P2 Design and siting of buildings and works must comply with any relevant design criteria / conservation policy listed in Table E13.2, except if a heritage place of an architectural style different from that characterising the precinct.

Reference to Table 13.2 is the mandatory basis for applying the performance criteria and assessing a proposal.

That table with commentary is set out below. As discussed, there is no reference to height or reference to townscape in the Heritage Precinct controls. It is also very difficult to address P2 as there

are no design criteria or specific conservation policies in the table. A lack of precision or information in the Scheme provisions cannot be used as a default control.

Criterion P1 must also be considered within the framework of the Scheme controls and specifically Table E13.2. The criterion actually does not allow other matters to be considered.

I would suggest the overall drafting of the Scheme is quite clumsy and imprecise, that language within the document is not used consistently and there is no easy way to follow the intent of some of the Criteria but, despite that, the process of assessment of heritage values on a non-heritage listed site within a heritage precinct within the central business zone and how development may or may not affect heritage values and the zone objectives and controls is reasonably clearly set out.

The precinct statement, with the relevance to addressing the zone objectives is:

This precinct is significant for reasons including:

Reason for significance (Scheme provision)	Relevance to the Zone objective (Comment)
1 It contains some of the most significant groups of early Colonial architecture in Australia with original external detailing, finishes and materials demonstrating a very high degree of integrity, distinctive and outstanding visual and streetscape qualities.	<p>This statement is the only one that relates to the precinct values. It notes that the buildings (across the whole of the precinct and not just the area within the Central Business Zone) are in groups (at least in part). The reference to their integrity relates mainly to their individual qualities - external details, finishes and materials - and the final statement is about the "distinctive and outstanding visual and streetscape qualities."</p> <p>The zone objective states "respect the unique character of the Hobart CBD and; maintain the streetscape and townscape contribution of places of historic cultural heritage significance."</p> <p>The zone objective differs from the precinct character statement as it adds a townscape element in addition to streetscape. This means the two matters while having a relationship are separate considerations. From this it can also be drawn that the precinct statement does not consider townscape, it limits its comments to streetscape but does look to protecting the group value of places of significance.</p> <p>The zone objectives are also framed differently to the heritage character statements as they seek to maintain the contribution of the heritage elements both (presumably) individually and collectively <u>as part of</u> the unique character of the Hobart CBD. The character of the Hobart CBD is not however limited to its heritage values.</p> <p>This reading of the Scheme provisions is further supported by the height controls for the zone that limit streetscape heights quite severely (and appropriately) and then establish a much higher height potential behind a streetscape setback control. This is an intentional and designed response</p>

	to the character of the CBD including its heritage character.
2 The collection of Colonial, and Victorian buildings exemplify the economic boom period of the early to mid-nineteenth century.	<p>This is a statement of fact that is correct but offers no assistance in understanding the precinct except that these elements need to be retained and respected.</p> <p>It is also of interest that while there is a heritage precinct overlay on part of the Central Business Zone, that many of the major heritage buildings, that form part of the collective value of the city, are not within a precinct.</p> <p>The way in which the city is heritage listed appears quite random.</p> <p>There is no assistance in this statement in considering the proposed development.</p>
3 The continuous two and three storey finely detailed buildings contribute to a uniformity of scale and quality of street space.	<p>This relates to the street frontage and initial building depth that requires consideration of the two to three storey scale and how buildings fit into the streetscape (or street space). The height controls specifically address this issue with a street height control and a setback control to ensure that infill sites address this.</p> <p>Street space is not a term that is defined in the Scheme.</p>
4 It contains a large number of landmark residential and institutional buildings that are of national importance.	<p>This is a statement of fact that is correct but offers no assistance in understanding the precinct except that these elements need to be retained and respected.</p> <p>There is no assistance in this statement in considering the proposed development.</p>
5 The original and/or significant external detailing, finishes and materials demonstrating a high degree of importance.	<p>This is a statement of fact that is correct but offers no assistance in understanding the precinct except that these elements need to be retained and respected.</p> <p>It relates to individual properties and not the precinct as a whole.</p> <p>There is no assistance in this statement in considering the proposed development.</p>

Applying the Scheme Provisions

Based on the above assessment I understand the Scheme to address this site in the following ways:

- 1 The zone objectives are the primary controls for the site.

I conclude this as the Scheme intentionally included this site and block with its relatively high concentration of heritage sites within this zone with the specific set of controls on height, street front height and setbacks. This need not have taken place and the area could have been given a different zoning or a different set of controls within the zoning. As it was not, it must be read that the zone controls apply and are intended and appropriate.

- 2 The zone controls on heights, setback etc are relevant and apply to the site.

The specific zone controls on height set out how to locate development of complying height and with regard to streetscape heights and setbacks. The scheme anticipates that an infill development will adopt the streetfront heights, with prescribed setbacks to allow greater height at a suitable distance from the street frontage.

This does not guarantee any particular development will be approved but it sets the framework for siting and height that is expected.

- 3 The Heritage Code applies to the site as it is within a heritage precinct and the considerations relate to Table 13.2 as they are the referenced matters to consider. The Scheme, rightly or wrongly, limits heritage considerations in this way.
- 4 The Scheme controls on views are not relevant to the site as essential views are mapped and the site is outside the Scheme mapping.

Conclusion

The core question is whether it is possible to design a building that is built to and complies with the height limit and setback controls, as well as other precinct controls, in this location that could have an acceptable heritage impact. While the detailed design of the building must be a key consideration, the initial consideration of appropriateness has to be a conceptual one that relates to overall scale and massing. If this cannot be satisfied, then the actual design does not need to be considered.

As I understand Council's position, the heritage code takes precedence over other controls as the area is a heritage precinct and unless it can be demonstrated that the height, form and mass does not have an adverse heritage impact, it is not possible to achieve approval.

It is my understanding and reading of the Scheme provisions that this is not a correct application of either the heritage code or the Scheme in general.

Putting aside the anomalies that exist in Planning Schemes due to the complexity of areas and the inability of any Scheme to address every situation, with a Scheme that is relatively recent it must be taken at face value where specific controls are set out. Where there are specific controls such as the height and setback controls and a definition of heritage values (Table E13.2) these must take precedence in any assessment over non-specific general controls.

The zone, irrespective of the heritage precinct overlay, sets objectives that promote denser development with height limits to achieve that and then provides for heritage sites and areas by establishing specific setbacks, streetfront heights, provision on what to do when building adjacent to heritage items (where the development site is not an item), addressing the streetfront, etc. These controls apply to individual heritage items and to sites within the heritage precinct. They establish a clear framework on how to approach new development.

The zone objectives do require the character of the zone area to be respected - both heritage and non-heritage - and for streetscape and townscape values of heritage places to be maintained. This does not however, specifically address height as an issue.

The height controls then set out objectives that, with regard to heritage, state that development "*does not unreasonably impact on historic heritage character*". There is a Scheme expectation that development that otherwise fits under the Scheme controls may have some impact on heritage character (most likely related to scale and design) and that this can be acceptable. There is no

general planning or specific heritage prohibition on building to or near the height limits and setbacks that are in the Scheme.

The heritage code requires the Scheme character statement for the precinct to be used to assess the potential for impact on heritage values. The code does not allow for broader or more wide-ranging considerations. Correctly the Scheme sets limits on considerations based on the material that is set out within it. The character statement for the Davey street Precinct offers very little assistance on the matter of height and form within the area except to acknowledge the streetscape character that presently exists in parts of the overlay. It is silent on other considerations.

I would conclude that the Planning Scheme allows a building of the height and general siting that is proposed to be capable of approval by Council subject to the detailed assessment of form, materiality, amenity and the numerous other planning controls within the Scheme that have to be addressed as a matter of course. Conversely, there is no prohibition or restriction on higher or larger buildings being approved within the heritage overlay area based on the Heritage provisions within the Planning Scheme.

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May 2019



TRAFFIC IMPACT ASSESSMENT

**PROPOSED
RESIDENTIAL APARTMENT AND
VISITOR ACCOMMODATION
DEVELOPMENT**

**63 DAVEY STREET
HOBART**

APRIL 2020



TRAFFIC IMPACT ASSESSMENT

PROPOSED RESIDENTIAL APARTMENT AND VISITOR ACCOMMODATION DEVELOPMENT

63 DAVEY STREET
HOBART

APRIL 2020

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CONTENTS

	Page Number
1. INTRODUCTION	4
2. SITE DESCRIPTION	5
3. DEVELOPMENT PROPOSAL	6
4. EXISTING ROAD AND TRAFFIC ENVIRONMENT	8
4.1 Road Characteristics	8
4.2 Traffic Activity	9
4.3 Crash Record	10
5. TRAFFIC GENERATION BY THE DEVELOPMENT	12
6. TRAFFIC ASSESSMENT AND IMPACT	14
6.1 Operational Impact of Increased Traffic Activity	14
6.2 Assessment of Available Sight Distances	14
6.3 Internal Traffic Access, Circulation and Car Parking	16
6.4 Public Transport Services	21
7. SUMMARY AND RECOMMENDATIONS	23

ATTACHMENTS:

- Attachment A - Design drawings of proposed layout of visitor accommodation and residential apartment development
- Drawings of civil design, access and on-street management



REFERENCES:

- Australian Standard AS 1742.2-2009 – Manual of uniform traffic control devices Part 2: Traffic control devices for general use
- AUSTROADS – Guide to Road Safety Part 6: Road Safety Audit (2009)
- Road Traffic Authority NSW – Guide to Traffic Generating Developments, 2002
- Road and Maritime Services (Transport) - Guide to Traffic Generating Developments; Updated traffic surveys (August 2013)
- AUSTROADS – Guide to Road Design Part 4A: Unsignalised and Signalised Intersections (2009)
- AUSTROADS – Guide to Traffic Management Part 6: Intersections, Interchanges and Crossings (2009)
- Australian Standard AS 2890 – Parking Facilities, Part 1 – Off-street car parking
- Australian Standard AS 2890 – Parking Facilities, Part 2 – Off-street commercial vehicle facilities
- Hobart Interim Planning Scheme 2015

1. INTRODUCTION

A planning application will be lodged with the Hobart City Council for a multi-storey residential apartment and visitor accommodation development, including a lobby/reception/office space, at 63 Davey Street in Hobart.

This Traffic Impact Assessment (TIA) report has been prepared in support of the proposed development.

The TIA report considers the existing road and traffic characteristics along Davey Street in the area of the development site. An assessment is made of the traffic activity that the development will generate and the effect that this traffic will have on Davey Street.

Consideration is given to the access design and available sight distances along Davey Street at the junction of the driveway to the development site. An assessment is also made of the driveway arrangements, internal vehicle traffic circulation and parking provisions within the development site having regard to current applicable Australian standards and the requirements of the Hobart Interim Planning Scheme (2015).

The report is based on the Department of State Growth (DSG) - Traffic Impact Assessment Guidelines. The techniques used in the investigation and assessment incorporate best practice road safety and traffic management principles.

2. SITE DESCRIPTION

The proposed development site is located on the northern side of Davey Street, around 70m to the west of the Harrington Street intersection.

The site lies within the Central Business Zone of Hobart. Development in the surrounding area is mixed with business and commercial, visitor accommodation, hospital and residential uses.

The location of the development site has been highlighted on the extract from the street atlas for this area, seen in Figure 2.1.

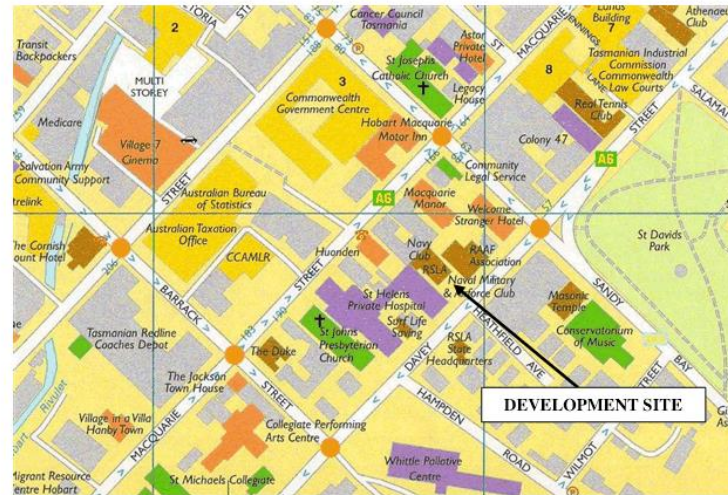


Figure 2.1: Extract of street atlas showing location of proposed apartment development site

3. DEVELOPMENT PROPOSAL

The proposed development at 63 Davey Street is for the construction of a multi-storey building which will have residential and visitor accommodation apartments.

The Ground Floor Level of the building will include the driveway off Davey Street, reception/staff area, lifts and stairwell, storage, waste and equipment rooms, and a bicycle storage room.

There will be 21 serviced – visitor accommodation apartments on Level 1 and 2. The residential apartments will occupy Level 3 to 10 with five apartments on Level 3, four apartments on each of Level 4 to 9, one apartment on Level 10. There will be 29 residential apartments with 2-bedrooms and one with three-bedrooms.

Two car lifts will provide access from the Ground Floor Level to the car parking spaces on the two Basement Levels. There will be 17 standard and four small car parking spaces plus two motorcycle parking spaces on each level – a total of 42 car parking spaces and four motorcycle parking spaces. One car parking space on each level has been designated as a disabled car parking space.

The vehicle access to the on-site car parking area will be via a 5.8m to 6.0m wide driveway off Davey Street at the eastern end of the site. There will be a separate pedestrian access into the building off Davey Street next to the driveway as well as through the foyer at the western side of the building.

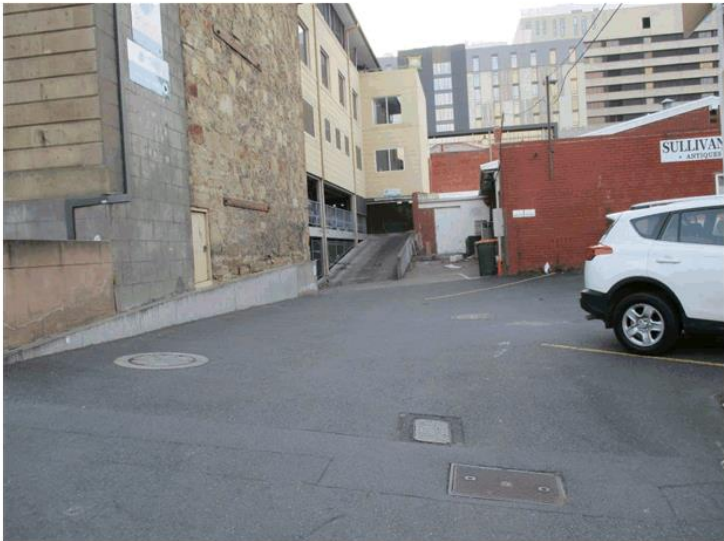
There is a right of way access along the western side boundary to the property which is currently used as the access to the two-level St Helens Hospital car park as well as the small car park (nine car parking spaces) on the development site.

A view of the development site is seen in Photograph 3.1 and the right of way access to the hospital car park is seen in Photograph 3.2.

Design drawings of the proposed development site layout and services/civil design are included with this report as Attachment A.



Photograph 3.1: View of development site from Davey Street



Photograph 3.2: View of access to St Helens Hospital car park at western side of development site

4. EXISTING ROAD AND TRAFFIC ENVIRONMENT

4.1 Road Characteristics

The one road of relevance to the proposed apartment development with respect to vehicular traffic and access is Davey Street.

Davey Street has a straight horizontal alignment on an upgrade to the west of around 11%.

It is a one-way street with four marked traffic lanes as well as parking lanes and footpaths along both sides of the street. The footpath along the development site frontage has a width of 2.65m.

The 50km/h urban speed limit applies to Davey Street.

A view of the geometric character of Davey Street in the area of the development site is seen in Photograph 4.1.



Photograph 4.1: View to west along Davey Street with development site ahead on right between buildings

4.2 Traffic Activity

In order to have knowledge of current traffic volume along Davey Street passing the development site, peak hour turning traffic volume surveys were undertaken during the 8:00am – 9:00am and 4:30 – 5:30pm periods on Tuesday 22 May 2018. The results from this survey have been summarised in Figures 4.1 and 4.2.

The survey recorded the passing traffic volume in the nearest (right hand) traffic lane on Davey Street, separate from the other three lanes as well as the traffic volume using the driveway to the development site and hospital. Nearly all the vehicles using the driveway were to and from the hospital car park (lower level).

As can be seen from Figures 4.1 and 4.2, the traffic volume along Davey Street past the development site was 2,381 vehicles/hour and 2,657 vehicles/hour, respectively during the morning and afternoon peak hour periods. Around 13% and 27% of the total Davey Street traffic volume used the right-hand lane in each peak hour period.

The survey also recorded 13 vehicles entering and exiting the shared driveway to the development site and St Helens Hospital during both peak traffic periods.

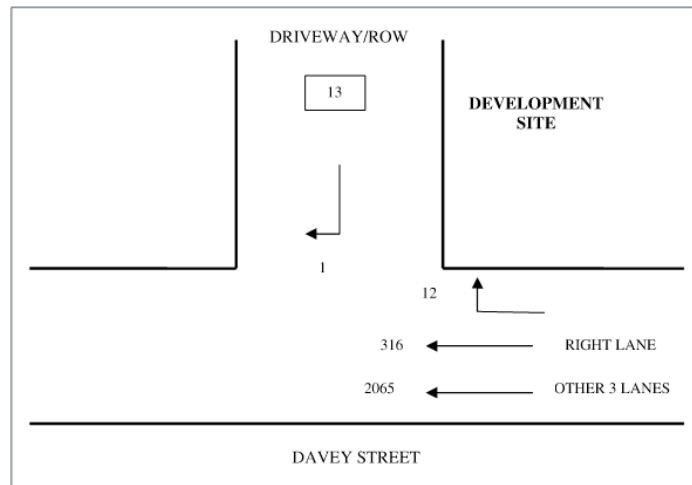


Figure 4.1: Turning traffic volumes at junction of Davey Street and development site driveway - 8:00am to 9:00am

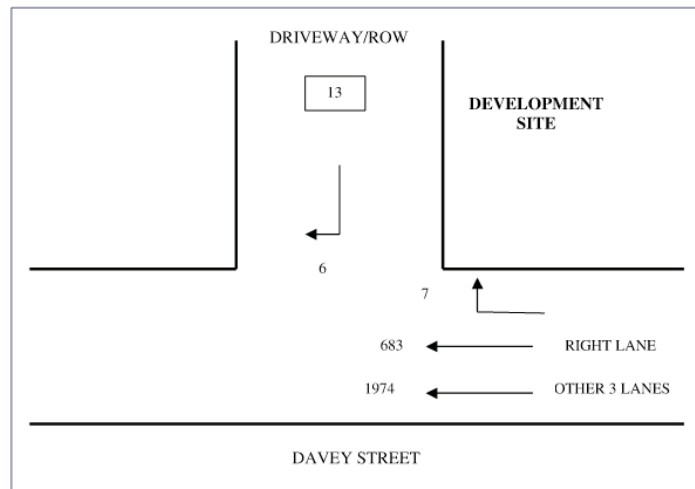


Figure 4.2: Turning traffic volumes at junction of Davey Street and development site driveway - 4:30pm to 5:30pm

These traffic volumes on Davey Street were compared with the lane counts from the traffic signal loop detectors in Davey Street at the Barrack Street intersection for the same day.

The loop detectors recorded around 10% higher volumes in the morning peak hour compared with the manual count; the higher count possibly being due partly to the loops not distinguishing combination vehicles (car- trailer or similar, counted as two vehicles) and traffic entering from Hampden Road while the afternoon count was virtually the same as the manual count.

4.3 Crash Record

All crashes that result in personal injury are required to be reported to Tasmania Police. Tasmania Police record all crashes that they attend. Any crashes that result in property damage only, which are reported to Tasmania Police, are also recorded even though they may not visit the site.

Details of reported crashes are collated and recorded on a computerised database that is maintained by DSG.

Information was requested from DSG about any reported crashes along Davey Street between Harrington Street and Barrack Street, including the intersections at each end, over the last five and half years since January 2013.

Advice has been received that the crash database has record of 59 reported crashes along this section of Davey Street.

Of these crashes, 32 crashes occurred at the Davey Street/Harrington Street intersection. 20 crashes were angle collisions between vehicles heading straight ahead on the two adjacent legs of the intersection with 10 resulting in injury. Such a high crash record and severity rate with a fairly consistent crash pattern at this intersection requires investigation by the road and traffic authorities; possibly requiring a consideration of 'see through' effects or intergreen signal timings.

There have been only nine reported collisions at the Davey Street/Barrack Street intersection; five were angle collisions and only one resulted in injury.

There has been one collision at the Hampden Road/Barrack Street junction involving a pedestrian who sustained minor injury.

The other 17 crashes were midblock collisions with 5-6 incidents in each of the three sections of Davey Street from Harrington Street to Heathfield Avenue, to Hampden Road, and to Barrack Street. Eight crashes were rear end type collisions and five were parking incidents. One rear end collision required first aid attention; all other midblock collisions resulted in property damage only.

5. TRAFFIC GENERATION BY THE DEVELOPMENT

As outlined in Section 3 of this report, the development being proposed is the construction of 51 residential and visitor accommodation apartments on the site at 63 Davey Street. The residential apartments will have two bedrooms (29 apartments) and three bedrooms (1 apartment). There will also be 21 visitor accommodation apartments.

The other relevant detail about the proposed development is that there will be parking on-site for 42 cars and four motorcycles, with all parking spaces to be allocated to the residential apartments.

In considering the traffic activity that each apartment will generate when occupied, guidance is normally sought from the New South Wales, Road Traffic Authority document – Guide to Traffic Generating Developments. The RTA guide is a nationally well accepted document that provides advice on trip generation rates and vehicle parking requirements for new developments.

The updated 'Technical Direction' to the Guide dated August 2013 advises that the trip generation for residential dwellings in regional areas of New South Wales is 7.4 trips/dwelling/day.

This is consistent with findings by this consultant for dwellings in Tasmania. Surveys in the built-up areas of Tasmania over a number of years have found that typically this figure is 8.0 trips/dwelling/day with smaller residential units generating around 4 trips/unit/day and larger units generating around 6 trip/unit/day.

Peak hour traffic surveys were also undertaken on Sandy Bay Road in 2015 at the 20 apartments in the Governor's Square development at 74 Sandy Bay Road which have car parking access off Sandy Bay Road. The traffic generation by these Governor's Square apartments during the peak hour was 3.75 vehicles/apartment/hour. These apartments each have two bedrooms.

In addition to the above, the following points are also relevant in estimating the traffic generation by the proposed development:

- the proposed apartments will have two bedrooms, one apartment with three bedrooms, and the apartments will have at least one car parking space on-site (40% of the apartments will have two spaces);
- the development site is very close to the Hobart CBD (just over 500m walking distance to the Liverpool Street/Collins Street intersection);
- the development site is very close to all route bus services at the central bus station in Elizabeth Street (around 550m walking distance);

The apartments are therefore expected to generate around the same traffic activity as the Grosvenor Square apartments.

For the purpose of this assessment, a slightly higher traffic generation rate of 4.5 vehicles/apartment/day will be assumed.

Applying this trip generation rate to the 30 residential apartments, the traffic generation is expected to be around 135 vehicles/day and some 14 vehicles/hour during peak traffic periods when fully developed and occupied, based on the peak hour traffic being the typical 10% of the daily traffic volume.

6. TRAFFIC ASSESSMENT AND IMPACT

This section of the report evaluates the impact of the traffic that will be generated by the proposed apartment development on passing Davey Street traffic volumes.

An assessment has been made of the adequacy of available intersection sight distances along Davey Street at the driveway junction; consideration has been given to the proposed internal site layout with respect to traffic circulation and parking as well as pedestrian accessibility and safety.

6.1 Operational Impact of Increased Traffic Activity

The proposed apartment development is expected to generate around 135 vehicles/day and 14 vehicles/hour at peak traffic times of the day.

The two-way traffic activity generated by the proposed development will not have any change in intensity of traffic activity or impact on the Davey Street traffic flow.

The traffic volume using the existing driveway which currently serves the development site as well as the adjacent hospital car park is virtually the same in peak traffic periods as will use the future driveway to the proposed apartment building. As seen in Figures 4.1 and 4.2, there were 13 two-way vehicle movements at the existing driveway in each peak hour.

Vehicles turning movements are currently and will in the future be to and from the right-hand traffic lane in Davey Street which carries nearly 700 vehicles/hour in peak traffic periods.

Intersections and junctions reach capacity when the total conflicting approach traffic volumes are around 1,500 vehicles/hour. The conflicting traffic volume at the new driveway will be around half this volume and there will not be an operational issue.

The traffic on Davey Street passes the development site in platoons. Vehicles entering Davey Street from the driveway need to wait for the platoon to pass to obtain a gap in the traffic stream. Once each platoon has passed (with green phase to Harrington Street) there are more than sufficient opportunities and time to enter Davey Street.

6.2 Assessment of Available Sight Distances

Consideration has been given to the available sight distances along Davey Street from the proposed driveway to the development.

The view along Davey Street for motorists entering from the location of the proposed driveway are seen in Photograph 6.1.

In assessing the sight distance, the requirements of Clause E6.7.2 A1 would apply in this case. It states: *the location, sight distance, width and gradient of an access must be designed and constructed to comply with section 3 – “Access Facilities to Off-street Parking Areas and Queuing Areas” of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking.*

AS 2890.1 details the required sight distances to approaching vehicles on public roads from private driveways, such as is under consideration in this assessment.

Free vehicle speeds in Davey Street past the development site would be around 50 - 55km/h. The desirable driveway sight distance is 69m for approach vehicle speeds of 50km/h from a point 2.5m back from the edge of road (at the property boundary), and 76m for approach speeds of 55km/h.

A driver exiting the site will be able to see much further than 76m along Davey Street with the advantage of a clearer line of sight due to the driveway to the adjacent property as well as the view over the footpath to the right side traffic lanes on Davey Street approaching from Harrington Street.

As can be appreciated from the view in the Photograph 6.1, it would normally be possible to see beyond the Harrington Street intersection, i.e. distances of over 100m.



Photograph 6.1: View to east along Davey Street from driveway to development site

6.3 Internal Traffic Access, Circulation and Car Parking

Following input into the design of the trafficable areas and having due regard to the requirement of AS 2890, the proposed layout and design of the driveway, circulation area and parking arrangements which will service the apartment building is shown on the development site layout drawings in Attachment A.

Relevant design elements of the proposed site layout related to traffic are discussed below.

Access driveway and traffic circulation

There will be one driveway off Davey Street which will service access to the proposed building on the development site.

Details of the proposed new driveway and gutter crossover as well as proposed changes to the existing driveway gutter crossover are detailed on the attached drawings.

The proposed new driveway off Davey Street will have a width of 6.0m at the frontage boundary and into the building, and 5.8m past the proposed two car lifts. Apart from a short section of grade transition at the frontage boundary of up to 6%, the driveway into the building will have a fairly flat grade of 3% or less).

The new driveway width is sufficient to allow all vehicles to simultaneously enter and exit the driveway to/from Davey Street, passing one another along the driveway; therefore, enter and exit the site in a forward direction to and from the car lifts.

The new driveway will have a security gate located some 6m into the site from the frontage property boundary allowing the car to stop clear of the footpath while activating the opening of the security gate.

The driveway layout is quite satisfactory to safely and efficiently accommodate the expected traffic activity.

The existing driveway to the development site, which also serves the St Helens Hospital car park will remain and continue to provide access only to the hospital car park.

The St Helens Hospital car park has two levels of parking. The lower level is a secured staff car park with around 18 car parking spaces. Currently, the entry and exit movements are via the shared driveway with the development site.

The upper level has some 15 car parking spaces for visitors. Entry to this level of the car park is via the shared driveway and up a ramp with the exit via the driveway on the western side of the adjacent hospital building.

With the construction of the proposed building on the development site, the driveway width (including the right of way) to the hospital car park will be around 3.5m. This will be sufficient for traffic use that the driveway will receive.

The peak hour surveys, detailed in Section 4.2 of this report, found the traffic movement in both periods were in one direction, some 13 vehicles entering in the morning peak hour and a similar number of vehicles departing in the afternoon peak hour.

The visitor car park would receive use at other times during the day with traffic entering via the eastern driveway and departing via the western hospital driveway.

Having regard to the advice in Section 3.2.2 of AS 2890.1, the traffic volume using the driveway during each hour, with the flow being predominantly in one direction throughout the day, will not be high enough to meet AS 2890.1 requirements for driveway passing areas (around 14 vehicles/hour compared with minimum of 30 vehicles/hour before passing areas are required).

Alternatively, there also is sufficient manoeuvring space for cars to exit the lower car park and turn left up the ramp to the upper car park and exit back to Davey Street via the western hospital driveway; effectively operate as a one way traffic flow through the hospital car park site.

It is recommended that the St Helens Hospital be requested to impose the one way traffic operation such that the eastern hospital driveway is the entry only to the two car parking levels.

Car lift operation

It is understood a decision has not been made about the supplier of the car lifts for the building. From discussions with one Australian supplier of car lifts, it is understood the travel speed of the car lift between floor levels depend on the cost outlay; it can vary from 6m/minute to 12m/minute.

The service rate will also depend on the location of the lift (what level it is at the time of demand by an arriving vehicle). The lift operation can be programmed to be waiting at Basement Levels in the morning peak hour and Ground Floor Level during the afternoon peak hour to best service the peak direction vehicle movements.

As a worst case, the service time for arriving cars at the Ground Floor Level would be around 2.5 - 3 minutes (lift from lower Basements Level) and at best it would be around 0.5 minutes (lift waiting at Ground Floor Level).

The worst-case situation would service cars at a rate of around 20 vehicles/hour, however the service rate would be significantly better than this with half the traffic movement being to and from Basement Level 1.

At an average service rate of 2 minutes, one lift would service 30 vehicles/hour and the two lifts would service twice this rate of vehicle arrivals

and departures. The two lifts will therefore efficiently service the expected traffic generation by the development which is expected to be only 14 vehicles/hour, with only one to two cars expected to be waiting at the busiest times.

Car parking supply

Clause E6.6.5 of the Hobart Interim Planning Scheme 2015 states that for a development in the Central Business Zone, the acceptable solution for the number of car parking spaces on the site is:

AI

- (a) No onsite parking is provided; or*
- (b) onsite parking is provided at a maximum rate of 1 space per 200m² of gross floor area for commercial uses; or*
- (c) onsite parking is provided at a maximum rate of 1 space per dwelling for residential uses; or*
- (d) onsite parking is required operationally for an essential public service, including, hospital, police or other emergency service.*

The proposed development will have 30 residential apartments and 42 car parking spaces. Only Clause E6.6.5 A1(c) would be applicable in this case in which case the proposed development will have 12 additional car parking spaces to that specified in this clause.

The performance criteria for Clause E6.6.5 are:

PI

Car parking provision:

- (a) is in the form of a public car parking station provided as part of a development which utilises a major existing access; or*
- (b) must not compromise any of the following:*
 - (i) pedestrian safety, amenity or convenience;*
 - (ii) the enjoyment of 'al fresco' dining or other outdoor activity;*
 - (iii) air quality and environmental health;*
 - (iv) traffic safety.*

In considering these performance criteria, P1(a) does not apply.

In regard to P1(b), this TIA report has addressed the matters referred to in (i) and (iv).

Pedestrian safety matters are addressed below, and traffic safety is considered in different sections of the report which discuss the expected traffic generation, mix of conflicting traffic movements, intersection sight distances

and driveway access to Davey Street, all of which have been found to be quite satisfactory.

In regard to P1(b) (ii) and (iii):

- the proposed development will not have any impact or bearing to any outdoor activity; and
- the use and activity resultant from the proposed development will not have any adverse effects on air quality or environmental health.

The additional 12 car parking spaces proposed in this development will therefore not result in any adverse traffic amenity, safety or environmental outcomes. The proposed car parking supply is therefore supported.

On-site parking area design

All the resident parking spaces on the site will be compliant with AS 2890.1.

The required turn paths of vehicles have been checked and found to be adequate for three-point turns by B85 cars for all manoeuvres to and from all parking spaces.

The specific dimensions that have been assessed include the following:

- All standard parking spaces will be 5.4m long and 2.4m wide in accordance with User Class 1A for residential parking (as detailed in Figure 2.2 of AS 2890.1 for 90-degree parking);
- The three small car parking spaces will be at least 5.0m long and 2.4m wide, in accordance with Section 2.4.1 (a) (iii) of AS 2890.1. A small car is a 50th percentile car on public roads, which has a length of up to 4.45m; therefore, having six of the 42 car parking spaces designated for small cars is quite acceptable;
- One disabled car parking space is proposed on each parking level (residential developments don't require disabled car parking spaces). The disabled parking spaces will be in accordance with requirements of AS 2890.6 with an adjacent shared area (the shared area will be 2.1m wide (less than 2.4m) but sufficient to meet user needs).
- There will be at least a 300mm clearance to the side walls and obstructions for door opening and manoeuvring (as detailed in Figure 2.2 of AS 2890.1);
- The width of the parking aisle will be 6.0m (more than the 5.8m required in Figure 2.2 of AS 2890.1 for Class 1A 90-degree parking);
- The offset in the parking aisle will be sufficient to not compromise the required turning path for entry and exit to adjacent parking spaces other than the need to have the small car parking spaces (Bay No.7-9) due to the maximum available bay length of 5.0m;

- There will be at least a 1.0m extension to the ends of the parking aisle for cars to reverse out of parking spaces (as detailed in Figure 2.3 of AS 2890.1);
- The height clearance will be a minimum of 2.2m in the trafficable areas on the two Basement Levels and 3.5m along the access driveway in the building on the Ground Floor Level;

With all dimensions meeting the requirements of AS 2890.1, the parking spaces will be compliant with the standard and meet the Acceptable Solution for Clause E6.7.5.

There will also be a secured bicycle storage room on the Ground Floor Level for those residents that will have a bicycle for transport and recreational purposes.

On-street parking

The construction of the new driveway to the development site will require the removal of one metered parking bay at the eastern end of the development site. It will also require the relocation of another parking space to the west by around 1m.

The location of that parking bay is highlighted in Photograph 6.2.

The attached drawings detail the proposed changes to the parking spaces.



Photograph 6.2: View to east along Davey Street showing development site frontage and location of proposed driveway

Pedestrian Traffic

The development site is located within short walking distance of all services and shopping facilities in the Hobart CBD. Therefore, the building is expected to generate a significant pedestrian movement to and from the site.

Pedestrians will be able to access the apartment block directly from Davey Street, away from the driveway, either through the entry foyer or the side passageway next to the driveway.

Consideration has been given to the required sight triangle between motorists exiting the driveway and pedestrians approaching along the Davey Street footpath, as indicated in Figure 3.3 of AS 2890.1.

There is an existing driveway serving the adjacent property located adjacent to the property boundary between the development site and the property to the east.

The design drawings (and 3D artist impressions) indicate that a low wall (with low landscaping) will be constructed along this side boundary. The wall will be no higher than around 600mm and will ensure there is an unobstructed line of sight to/from pedestrians, as required.

The layout at the front of the site and the provision of the access and pedestrian sight line measures will ensure pedestrian safety and convenient access with good amenity.

Waste collection/servicing

The collection of domestic waste is expected to occur through arrangements with Council.

There will be a waste storage area within the building for all occupants.

An area has also been designated at the front of the building but inside the property from where the bins can be easily wheeled to the waste collection vehicles, either belonging to council or a private contractor.

The collection service could occur from the parking lane with the bins wheeled from the temporary external bin storage area to the back of the garbage truck in the parking lane.

6.4 Public Transport Services

Metro Tasmania currently operates regular route bus services along Davey Street (outbound) to the southern suburbs and South Hobart area.

However, the Elizabeth Street bus station is around 550m walking distance from the development site. Route bus services to and from all suburbs in the greater Hobart area are available at this bus station.

Normally the accepted maximum walking distance between bus stops and residential dwellings is 400m. In this central business area, it would be quite acceptable to walk the additional 150m.

7. SUMMARY AND RECOMMENDATIONS

This Traffic Impact Assessment has been prepared in support of the planning application to the Hobart City Council for the construction of an apartment building at 63 Davey Street in Hobart.

There will be 21 visitor accommodation apartments and 30 residential apartments in the building. 29 residential apartments will have 2-bedrooms and one will have three-bedrooms.

Two car lifts will provide access from the Ground Floor Level to the car parking spaces on the two Basement Levels where there will be a total of 42 car parking spaces and four motorcycle parking spaces.

The assessment has reviewed the existing road and traffic environment along Davey Street in the area of the development site.

Davey Street is a one-way street with four marked traffic lanes and parking along both sides of the street.

Peak hour turning traffic volume surveys were undertaken during the 8:00am – 9:00am and 4:30pm – 5:30pm periods on Davey Street outside the development site on Tuesday 22 May 2018. The survey found the traffic volume along Davey Street past the development site was 2,381 vehicles/hour and 2,657 vehicles/hour during the morning and afternoon peak hour periods, respectively. Around 13% and 27% of the total Davey Street traffic volume used the right-hand lane in each period.

There were 13 vehicles entering and exiting the shared driveway to the development site and St Helens Hospital car park during both peak traffic periods.

The crash database has record of 59 reported crashes along Davey Street between Harrington Street and Barrack Street, including the intersections at each end, over the last five and half years since January 2013.

Of these, 32 crashes occurred at the Davey Street/Harrington Street intersection, there were nine crashes at the Davey Street/Barrack Street intersection, and 17 were midblock crashes with 5-6 collisions in each of the three sections of Davey Street from Harrington Street to Heathfield Avenue to Hampden Road to Barrack Street. Eight were rear end type collisions and five were parking incidents.

The main concern with the crash history is the high crash record and severity rate at the Davey Street/Harrington Street intersection. This intersection requires investigation by the road and traffic authorities for some solutions which will reduce the crash rate.

It has been estimated that the proposed development, when fully completed and occupied will generate some 135 vehicles/day and around 14

vehicles/hour during peak traffic periods, based on the peak hour traffic being the typical 10% of the daily traffic volume.

The design drawings detail the proposed new driveway and gutter crossover as well as proposed changes to the existing gutter crossover which will service access to the St Helens Hospital car park.

With the construction of the proposed building on the development site, the driveway width (including the right of way) to the St Helens Hospital car park, along the western boundary of the development site, will be around 3.5m.

It is recommended that the St Helens Hospital be requested to impose the one way traffic operation such that this hospital driveway is the entry only to the two car parking levels.

The proposed new driveway off Davey Street into the building will be sufficient to accommodate the expected traffic activity generated by the proposed development.

Vehicles entering and exiting the development site driveway will turn right to and from the right-hand traffic lane in Davey Street which carries nearly 700 vehicles/hour in peak traffic periods.

Intersections and junctions reach capacity when the total conflicting approach traffic volumes are around 1,500 vehicles/hour. The conflicting traffic volume at the new driveway will be around half this volume and there will not be an operational issue.

There are more than sufficient opportunities and time for vehicles to enter Davey Street once each vehicle platoon has passed (during green phase to Harrington Street).

An assessment has been undertaken of the available sight distances at the junction of the development site driveway with Davey Street. The available sight distances are more than sufficient to meet AS 2890.1 requirements and hence the planning scheme.

Consideration has been given to the proposed layout and design of the internal driveway, traffic circulation provisions and parking arrangements, having regard to accepted practices and relevant Australian Standards.

It has been concluded the design is satisfactory in meeting the requirement of AS 2890.1 and therefore the Planning Scheme.

The proposed two car lifts in the building will efficiently service the movement of cars between the ground floor level and the two Basement Levels. The service rate of the two lifts will be much higher than the traffic movement to and from the site and therefore queueing will be minimal at one, occasionally two cars waiting at the busiest time of the day.

As the development site is located within the Central Business Zone, the planning scheme has a requirement for maximum parking supply as the

acceptable solution. The proposed 42 car parking spaces will exceed this maximum by 12 parking spaces.

In considering the relevant performance criteria, it has been concluded the proposed development will not have any impact or bearing on any outdoor activity while the use and activity resulting from the proposed development will not have any adverse effects on air quality or environmental health.

The proposed parking supply, with the additional 12 car parking spaces, will not result in any adverse traffic amenity, safety or environmental outcomes and the proposed car parking supply is therefore supported.

The construction of the new driveway to the development site will require the removal of one metered parking bay at the eastern end of the development site and a slight relocation of the adjacent parking bay to the west, as detailed on the design drawings.

The building is expected to generate a significant pedestrian movement to and from the site as it is located within a short walking distance to all services and shopping facilities in the Hobart central business area,

The Elizabeth Street bus station is also around 550m walking distance from the development site from where all route bus services to the greater Hobart region start and finish.

The driveway design to the development site and a low wall to be constructed along the side boundary, no higher than 600mm will ensure there will be a sufficient pedestrian sight triangle between exiting vehicles and pedestrians on the Davey Street footpath. This wall will also assist in addressing this issue for the driveway to the adjacent property on the eastern side.

The waste collection service for the building can occur from the parking lane with bins wheeled from the bins area to the back of the garbage truck, with the service to be provided by either council or private contractor.

Overall, it has been concluded that the proposed apartment development can be supported on traffic grounds as it will not give rise to any adverse safety or operational traffic issues with the implementation of the proposed measures.

ATTACHMENT A

- Design drawings of proposed layout of visitor accommodation and residential apartment development
- Drawings of civil design, access and on-street management



29 June 2020

Phil Gartrell
Planner
Ireneinc Planning and Urban Design
49 Tasma Street
NORTH HOBART TAS 7001

Dear Phil

**PROPOSED RESIDENTIAL APARTMENT DEVELOPMENT
63 DAVEY STREET, HOBART**

I refer to the letter dated 19 June 2020 from the Hobart City Council requesting further information regarding the proposed development at the above address.

In regard to item PA5.1, the council has advised:

Advice:

- *Council's cleansing and solid waste unit have advised that the number of residential / visitor accommodation units proposed are unable to be collected at the kerb by Council's waste trucks.*
- *It is noted that the consultant planning report states that the TIA indicates waste vehicles can reverse into the driveway and drive out in a forward direction or collection can occur using the parking lane. As this AS2890.2 Section 3.2.3, please provide documentation from the Road Authority (Department of State Growth) that they are satisfied that the proposed waste vehicle movements will not compromise the safety and convenience of vehicular traffic, cyclists, pedestrians and other road users; and that they are aware of the scale of waste removal proposed from the kerb.*

The Department of State Growth Crown Landowner Consent regarding this development does not raise any concern with parking and waste collection.

It appears the council has based all this request on the statement in the Department of State Growth Crown Landowner Consent related to 202 Macquarie Street (see attached):

- *Please note that traffic flows on Macquarie Street are under constant review and that the current kerb site parking arrangement could change at any time. Therefore, development should not rely on these parking spaces in the longer term.*

My understanding from this statement is simply that kerbside parking restrictions along Macquarie Street could change at some time in the future. It does not state that kerbside servicing of properties along the street will not be possible.

The changes to kerbside parking along both sides of Macquarie Street and Davey Street will have to take into account that there are many businesses and dwellings along both sides of the street that require ongoing servicing, in particular waste

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collection, for the kerbside. All these properties are set up for kerbside waste collection and it will be impossible to consider, let alone implement, alternative arrangements.

The reality of the Department of State Growth advice is that there may be clearway restrictions imposed along one or both sides of Macquarie Street and Davey Street in the future or a bus lane on the left side of the road during peak traffic periods.

These restrictions will be only part-time restrictions, applied when required for the traffic conditions. Therefore, there will be other significant periods of the day when kerbside waste collection will be able to occur. The Department of State Growth would not propose or will not be able to apply restrictions along Macquarie Street and Davey Street which would not allow kerbside waste collection at different times of the day or night.

If required, this could be allowed under Road Rule 158 or Road Rule 313A, if this does not exist at present. This matter, in particular the statement in the Crown Landowner Consent for 202 Macquarie Street regarding parking, detailed above, has been discussed with traffic engineers at the Department of State Growth.

They have confirmed with me that their advice is intended to relate only to the car parking supply for any development and that any shortfall in the car parking supply within the development site should not depend on currently available car parking along Davey Street or Macquarie Street; it does not relate in any way to waste servicing of the site.

A large waste collection company in Tasmania has advised me that most of its work in the Hobart central business area and surrounds, including kerbside waste collection, is undertaken outside of peak hour traffic and also outside normal work hour periods.

The Hobart City Council does collect domestic waste from multiple unit developments from the kerbside, in accordance with its *Development Application Guidelines – Waste Management* (see attached).

Arrangements at this proposed development have been designed to allow for kerbside collection of domestic waste by council.

I therefore conclude the council has misinterpreted the advice from the Department of State Growth.

In addition, the proposed development adequately provides for all kerbside waste collection, without the need for trucks to enter the building (the Traffic Impact Assessment report does not refer to waste collection vehicles reversing in the development site driveway) .

Yours sincerely

Milan Prodanovic



24 July 2020

Phil Gartrell
Planner
Ireneinc Planning and Urban Design
49 Tasma Street
NORTH HOBART TAS 7001

Dear Phil

**PROPOSED RESIDENTIAL APARTMENT DEVELOPMENT
63 DAVEY STREET, HOBART**

I refer to the letters dated 19 June 2020 and 22 July 2020 from the Hobart City Council requesting further information regarding the proposed development at the above address.

In regard to item PA5.1 (19 June 2020), the council has advised:

Advice:

- *Council's cleansing and solid waste unit have advised that the number of residential / visitor accommodation units proposed are unable to be collected at the kerb by Council's waste trucks.*
- *It is noted that the consultant planning report states that the TIA indicates waste vehicles can reverse into the driveway and drive out in a forward direction or collection can occur using the parking lane. As this AS2890.2 Section 3.2.3, please provide documentation from the Road Authority (Department of State Growth) that they are satisfied that the proposed waste vehicle movements will not compromise the safety and convenience of vehicular traffic, cyclists, pedestrians and other road users; and that they are aware of the scale of waste removal proposed from the kerb.*

The Department of State Growth Crown Landowner Consent regarding this development does not raise any concern with waste collection.

It appears the council has based this request on the statement in the Department of State Growth Crown Landowner Consent related to 202 Macquarie Street (see attached):

- *Please note that traffic flows on Macquarie Street are under constant review and that the current kerb site parking arrangement could change at any time. Therefore, development should not rely on these parking spaces in the longer term.*

My understanding from this statement is simply that kerbside parking restrictions along Macquarie Street could change at some time in the future. It does not state that kerbside servicing of properties along the street will not be possible.

The changes to kerbside parking along both sides of Macquarie Street and Davey Street will have to take into account that there are many businesses and dwellings

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along both sides of the street that require ongoing servicing, in particular waste collection, from the kerbside. All these properties are set up for kerbside waste collection and it will be impossible to consider, let alone implement, alternative arrangements.

The reality of the Department of State Growth advice is that there may be clearway restrictions imposed along one or both sides of Macquarie Street and Davey Street in the future or a bus lane on the left side of the road during peak traffic periods.

These restrictions will be only part-time restrictions, applied when required for the traffic conditions. Therefore, there will be other significant periods of the day when kerbside waste collection will be able to occur. The Department of State Growth would not propose or will not be able to apply restrictions along Macquarie Street and Davey Street which would not allow kerbside waste collection at different times of the day or night.

If required, this could be allowed under Road Rule 158 or Road Rule 313A, if this does not exist at present.

This matter, in particular the statement in the Crown Landowner Consent for 202 Macquarie Street regarding parking, detailed above, has been discussed with traffic engineers at the Department of State Growth (DSG).

The above advice has now been confirmed, that the DSG advice is intended to relate only to the car parking supply for any development and that any shortfall in the car parking supply within the development site should not depend on currently available car parking along Davey Street or Macquarie Street; it does not relate in any way to waste servicing of the site.

A copy of the advice from DSG is attached.

The Hobart City Council does collect domestic waste from multiple unit developments from the kerbside, in accordance with its *Development Application Guidelines – Waste Management* (see attached).

Arrangements at this proposed development have been designed to allow for kerbside collection of domestic waste by council.

Council has misinterpreted the advice from the Department of State Growth. In addition, the proposed development adequately provides for all kerbside waste collection, without the need for trucks to enter the building (the Traffic Impact Assessment report does not refer to waste collection vehicles reversing in the development site driveway).

Mixed in with the above is the following:

Advice: In addition to the above and in terms of the information submitted to date, please note that Council's Road Engineers have expressed concern with regard to the width of the proposed widened driveway crossover. Their advice is that the proposed driveway crossover is too wide (10.96m), and that a crossover over 8m in width is unlikely to be accepted by the road authority. It is requested



that you consider separating the crossover from the adjoining property's, so that both crossovers are less than 8 metres. If this change is to be adopted for the proposal, plans should be amended accordingly and resubmitted for assessment.

In response to this, there are two driveways in this area serving two adjacent properties. The design drawings show the separation between the two driveways is not more than 0.5m.

If the gutter crossover was constructed as for two separate driveways, the wings for each driveway will intrude into each driveway by one metre. It is totally unacceptable to have cars bouncing across the wing of kerb face when undertaking entry and exit manoeuvres to and from Davey Street.

Furthermore, the proposed design of the gutter crossover for these two driveways is fully in accordance with Standard Drawing TSD-R09-v1 (attached).

This is not a 10m gutter crossover to a single two driveway for car traffic, therefore, DSG will not have any interest in this driveway detail. The proposed gutter crossover and driveway design was included in the documents on which DSG provided consent for the development at 63 Davey Street and no comment was made about this detail

Yours sincerely

Milan Prodanovic

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Statement of Historical Archaeological Potential

63 Davey Street
HOBART TASMANIA

For John Tellyros

July 2018

Contents:

<u>1. INTRODUCTION</u>	<u>1</u>
1.1. INTRODUCTION AND BRIEF.....	1
1.2. LIMITATIONS.....	3
<u>2. STATUTORY HERITAGE REQUIREMENTS</u>	<u>4</u>
2.1 HOBART INTERIM PLANNING SCHEME 2015.....	4
2.2. TASMANIAN HERITAGE REGISTER.....	6
2.3. OTHER STATUTORY HERITAGE REGISTERS/LISTS.....	6
2.4. ABORIGINAL HERITAGE ACT 1975.....	6
<u>3. ARCHAEOLOGICAL METHODOLOGY.....</u>	<u>7</u>
<u>4. HISTORICAL BACKGROUND OF THE SUBJECT SITE</u>	<u>9</u>
4.1. RESEARCH METHODOLOGY	9
4.2. HISTORICAL OVERVIEW	10
Summary of development of the subject site.....	35
<u>5. CURRENT SITE OBSERVATIONS AND ASSESSMENT OF PRIOR DISTURBANCE</u>	<u>41</u>
General site observations	41
Likely specific disturbance events.....	43
<u>6. THE LIKELY SIGNIFICANCE AND RESEARCH POTENTIAL OF ARCHAEOLOGICAL REMAINS</u>	<u>44</u>
<u>7. ARCHAEOLOGICAL ZONING PLAN AND POLICIES.....</u>	<u>47</u>

This document was written by Brad Williams (BA.Hons Archaeology, G.Dip Maritime Archaeology, MA Cultural Heritage Management, G.Dip Environmental Planning) Historical Archaeologist, Heritage Consultant and Director of Praxis Environment. Praxis Environment is a division of Praxis Synergy Pty. Ltd. ACN 63 700 818.

Unless otherwise stated, all photographs were taken by Brad Williams, July 2018

Unless otherwise stated, the north point (or approximate) of maps and plans is to the top of the page.

Cadastral information depicted in this document must not be relied upon without verification by a Surveyor.

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1. Introduction

1.1. Introduction and brief

This report has been commissioned by Mr. John Tellyros in order to accompany an application to the Hobart City Council for a proposed redevelopment of the place known as 63 Bathurst Street, Hobart.

The subject site is on the northern side of Davey Street Hobart near the intersection of Harrington Street and comprises of C/T 54396/1 (PID 5660956) and is currently the site of a 1960s building which until recently housed the Navy Club and more recently an antiques store. The building covers approximately 2/3 of the site and the remainder being a small carpark near the street frontage.

The site is not listed on the Tasmanian Heritage Register, nor is it a Heritage Place as defined by Table E.13.1 of the Hobart Interim Planning Scheme 2015, although it is within the Places of Archaeological Sensitivity as defined by Figure E.13.1 of the scheme, therefore the provisions of Part E.13.10 of the scheme is applicable. Accordingly, the brief for this project was to develop a **statement of historical archaeological potential** as the basis for archaeological planning in any future development of the subject site.

Although not listed on the Tasmanian heritage Register, the archaeological approach in this document has been developed with regard to the Tasmanian Heritage Council's Practice Note 2 – *Managing Historical Archaeological Significance in the Works Application Process*¹, and the Tasmanian Heritage Council's *Guidelines for Historical Archaeological Research on Registered Places*² as a means of demonstrating a sound and best-practice approach.

¹ <http://www.heritage.tas.gov.au/media/pdf/2%20Practice%20note%20-%20Archaeology.pdf>

² <http://www.heritage.tas.gov.au/media/pdf/Archae%20ResGlines%20%20FINAL%20-%20June%202009.pdf>

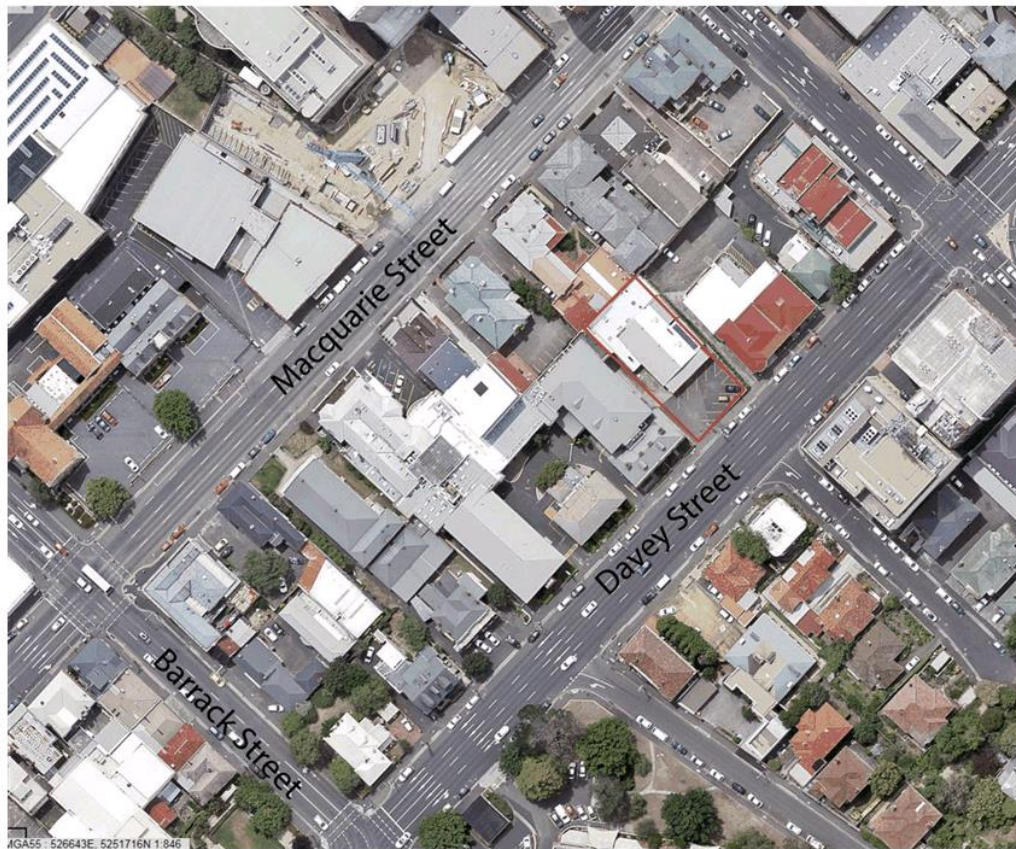


Figure 1.1 – A recent aerial image of the area – the subject site depicted in red. www.thelist.tas.gov.au



Figure 1.2 – Cadastral boundaries of the subject site and surrounds – the subject site depicted in red. www.thelist.tas.gov.au

1.2. Limitations

This document has the following stated limitations:

- This document is largely a predictive analysis (i.e. non-invasive) of the possible archaeological resource and might be subject to further on ground testing to verify findings if deemed necessary by any stakeholder.
- All depictions of the location of site features are approximate. A surveyor should be engaged if any party requires exact confirmation of locations.
- The depiction of expected archaeological features in this report largely relies on the accuracy of historical surveys and data – no guarantee of the accuracy of this historical data is given.
- The scope of this project only included historic heritage values. Consideration of Aboriginal heritage values was outside the scope.

2. Statutory heritage requirements

This report has been commissioned to consider the historical archaeological potential of the subject site arising from any applicable statutory listings. The following statutory heritage responsibilities that relate to historical archaeology are to be met in any development of the subject site:

2.1 Hobart Interim Planning Scheme 2015

The place is within the area defined in Figure E13.1 of the Hobart Interim Planning Scheme 2015 (the *scheme*) as a *Place of Archaeological Potential*, therefore the provisions of Part E13.10 are applicable.

Part E13.10 of the scheme details the *Development Standards for Places of Archaeological Potential*, with the following *Objectives*:

13.10.1: Building, Works and Demolition: To ensure that building, works and demolition at a place of archaeological potential is planned and implemented in a manner that seeks to understand, retain, protect, preserve and otherwise appropriately manage significant archaeological evidence.

13.10.2: Subdivision: To ensure that subdivision does not increase the likelihood of adverse impact on a place of archaeological potential.

The scheme prescribes *Performance Criteria* for each of these *Objectives* and pursuant to Part E.13.5 of the scheme, the Planning Authority may require the following to accompany any application for development of a place of archaeological potential in order to assess the proposal against the performance criteria:

- (f) a statement of archaeological potential;
- (g) an archaeological impact assessment;
- (h) an archaeological method statement;

Under the definitions of the scheme:

(f) means:

a report prepared by a suitably qualified person that includes all of the following:

- a. a written and illustrated site history;

- b. overlay plans depicting the main historical phases of site development and land use on a modern base layer;
- c. a disturbance history.
- d. a written statement of archaeological significance and potential accompanied by an archaeological sensitivity overlay plan depicting the likely surviving extent of important archaeological evidence (taking into consideration key significant phases of site development and land use, and the impacts of disturbance).

(g) means:

a report prepared by a suitably qualified person that includes a design review and describes the impact of proposed works upon archaeological sensitivity (as defined in a statement of archaeological potential).

(h) means:

a report prepared by a suitably qualified person that includes the following where relevant to the matter under consideration:

- a. strategies to identify, protect and/or mitigate impacts to known and/or potential archaeological values (typically as described in a Statement of Archaeological Potential);*
- b. collections management specifications including proposed storage and curatorial arrangements;*
- c. identification of measures aimed at achieving a public benefit;*
- d. details of methods and procedures to be followed in implementing and achieving (a), (b) and (c) above*
- e. expertise to be employed in achieving (d) above;*
- f. reporting standards including format/s and content, instructions for dissemination and archiving protocols.*

The current document aims to fulfil those points in a consolidated manner in the assessment of the proposed development to assist the planning authority to make an informed assessment against the performance criteria of the scheme.

2.2. Tasmanian Heritage Register

The subject site is not listed on the Tasmanian Heritage Register therefore is not subject to the provisions of the *Historic Cultural Heritage Act 1995*. Nonetheless, the archaeological approach in this document has been developed with regard to the Tasmanian Heritage Council's Practice Note 2 – *Managing Historical Archaeological Significance in the Works Application Process*³, and the Tasmanian Heritage Council's *Guidelines for Historical Archaeological Research on Registered Places*⁴ as a means of demonstrating a sound and best-practice approach.

2.3. Other statutory heritage registers/lists

The subject site is not listed on any of the following statutory registers:

- The National Heritage List
- The Commonwealth Heritage List
- The World Heritage List

Nor is it included in any buffer zones arising from those lists, therefore is not subject to the historic heritage provisions of the respective Acts which enable statutory input into development of places on those lists.

2.4. Aboriginal Heritage Act 1975

An assessment of any possible Aboriginal heritage values is not part of the brief for this report; nonetheless the provisions of the Aboriginal Heritage Act 1975 are applicable to the place. A search of the Tasmanian Aboriginal Heritage sites register (Job # 14745213) did not identify any registered Aboriginal relics or apparent risk of impacting Aboriginal relics (search valid until 13/2/19). The Tasmanian Government Unanticipated Discovery Plan – Procedure for the management of unanticipated discoveries of Aboriginal relics in Tasmania must be adhered to in the event that any Aboriginal heritage items are discovered during the course of any works.

³ <http://www.heritage.tas.gov.au/media/pdf/2%20Practice%20note%20-%20Archaeology.pdf>

⁴ <http://www.heritage.tas.gov.au/media/pdf/Archae%20ResGlines%20%20FINAL%20-%20June%202009.pdf>

3. Archaeological Methodology

This statement of archaeological potential is derived from a process which identifies the potential of the site to yield archaeological remains, the significance of any remains, and their potential to yield meaningful information about the site, and which might contribute to relevant key archaeological and historical themes. The following briefly outlines the methodology followed:

Determining general archaeological potential: Through a desktop analysis of historical data and secondary sources, as well as non-invasive site observations, an understanding of the evolution of the site has been gained which has allowed an assessment of the archaeological potential (however significant) of any part of the site - resulting in substantiated predictions of the likelihood of finding *something* upon any particular part of the site.

This has been done by analysing primary source material, summarizing the developmental history of the site and developing a chronological narrative detailing an overview of the history of all known features to have ever existed on the site. Where possible, developmental overlays have been developed from historic maps, plans, photographs and other visual documentation. This overlay has been supported by other observations providing supplementary information, and also includes processes such as demolition and disturbance which may have removed or destroyed potential remains – and may have diminished the archaeological potential.

Assessing the significance and potential of any likely archaeological resources to yield meaningful information: Upon understanding the archaeological potential through desktop and site analysis, the next step was to understand its relationship to any aspect of the identified significance of the place – e.g. do the remains have the potential to demonstrate an aspect of the significance of the site or related key historic theme? The potential for any of the archaeological remains to demonstrate important aspects of the history of the site, whether in a state, regional or thematic context, is to be considered.

Understanding possible impact of development and formulation of management strategies: Based on any identified archaeological potential and significance of the site, consideration will be given as to whether the proposed development will impact upon any likely archaeological remains and if necessary broad management strategies will be proposed to manage any impact.

Table 1 (below) demonstrates the steps of this assessment:

Methodology for formulation of the statement of archaeological potential		
	If 'no'	If 'yes'
1. Archaeological potential. Are you likely to find something if you dig here? (i.e. a <u>Statement of Archaeological Potential</u>).	Further action may not be required, although a contingency plan may be required for unexpected finds.	The significance of the archaeological potential should be investigated.
2. Significance. Could anything you find here greatly contribute to our understanding of the site or related significant theme?	Further action may not be required.	The likely integrity of the archaeological remains should be investigated.
3. Integrity. Are any archaeological remains likely to be intact?	Further action may not be required, although a contingency plan is required for unexpected integrity.	The likelihood of significant archaeological remains is confirmed.
4. Impact Will proposed works impact upon the significant archaeological remains? i.e. an <u>Archaeological Impact Assessment</u> .	Further action may not be required, although a contingency plan may be required for unexpected impacts.	An <u>Archaeological Method Statement</u> will be required to detail how impact will be managed/mitigated.

4. Historical background of the subject site

4.1. Research methodology

For this initial assessment of archaeological potential, the depiction of the physical history of the site will be the main consideration – with other aspects of site history (i.e. social histories, economic history, associations *et. al.*) likely to be more useful in any post-investigation analysis of findings (i.e. artifact assessment), therefore beyond the scope of the current document. Similarly, the history of other townscape developments is beyond the scope of the current document however may be useful in further detailed analysis of future archaeological findings.

The following overview of the known physical development history of the site aims to aid in the prediction of the likely archaeological remains. This does not represent a comprehensive site history, and has been limited to a history of the physical development of the site as relevant to the archaeological resource.

Primary sources

Broadly, the primary sources consulted in the development of the statement of archaeological potential include:

- Hobart City Council building files (AE417 series, Tasmanian Archive and Heritage Office).
- Historic maps, photographs (NS and PH series) - Tasmanian Archive and Heritage Office.
- Department of Primary Industry, Parks, Water and Environment (DPIPWE) aerial photo collection (Service Tasmania).
- DPIPWE – Land Data Branch, historic map collection (basement)
- DPIPWE – Land Data Branch, titles.
- Historic newspapers, via the National Library of Australia's *Newspapers Online* portal.

Secondary sources

No secondary source documents are known to exist which are of particular relevance to the history or archaeology of the subject site.

In order to gain an overview of what once existed on the site, as the basis for predicting archaeological remains, the following is a brief overview of the historical development of the site based on primary source documents (the subject site depicted in red) as well as overviews drawn from the secondary sources as detailed above. Note that this is a brief historical overview, concentrating solely on physical development, sufficient only for

basic archaeological planning. As per above, further historical research is required in order to refine a detailed archaeological research design, which is provided here in Section 5. Such detail is also required to supplement the interpretation of archaeological findings – requiring an iterative process of the assessment of findings against further historical and comparative research from both primary and secondary sources, which should be provided in an archaeological method statement and post-excavation analysis.

4.2. Historical overview

The land was the home of the Mouheneener people for tens of thousands of years, prior to displacement by European settlers in 1804.

The subject area forms part of a 1 acre 2 rood 4 perch lot granted to the prominent Hobart merchant David Lord. David Lord inherited his father's Van Diemen's Land estate in 1824. At the time, it was believed to be worth about £50,000 and included considerable landholdings⁵. For this reason, it is extremely difficult to trace the history of this allotment during Lord's ownership.

The subject site appears to have been first developed prior to 1832 as part of Lord's larger allotment, with a survey from around that time depicting a building and outbuilding (see Figure 4.2). That survey is known to be reasonably accurate in its depiction of the presence of buildings, but not wholly accurate in the precise size, shape and location. The 1825 panorama of Hobart Town by Augustus Earle (see Figure 4.3) depicts buildings in this area which may represent pre-1825 development of the subject site that is more clearly reflected on the c1832 survey. A similar arrangement of buildings (depicted just outside the subject site) is depicted in the later 1830s Frankland survey (see Figure 4.4) and partially within the subject site on the c1841 census map (see Figure 4.5 – based largely on the Frankland survey). Neither of these surveys are particularly accurate and are only indicative of the presence of buildings rather than their precise locations. Nonetheless, the highly accurate Sprent survey of c1843 depicts a large timber building on the subject site which is probably indicative of these earlier depictions (see Figure 4.6). Note that the Sprent survey only depicts buildings which were clearly visible from public vantage points, therefore has probably omitted the rear outbuilding that is depicted on the earlier surveys.

⁵ Susan Allen, 'Lord, David (1785–1847)', Australian Dictionary of Biography, National Centre of Biography, Australian National University, <http://adb.anu.edu.au/biography/lord-david-2369/text3111>, published first in hardcopy 1967, accessed online 29 July 2018.

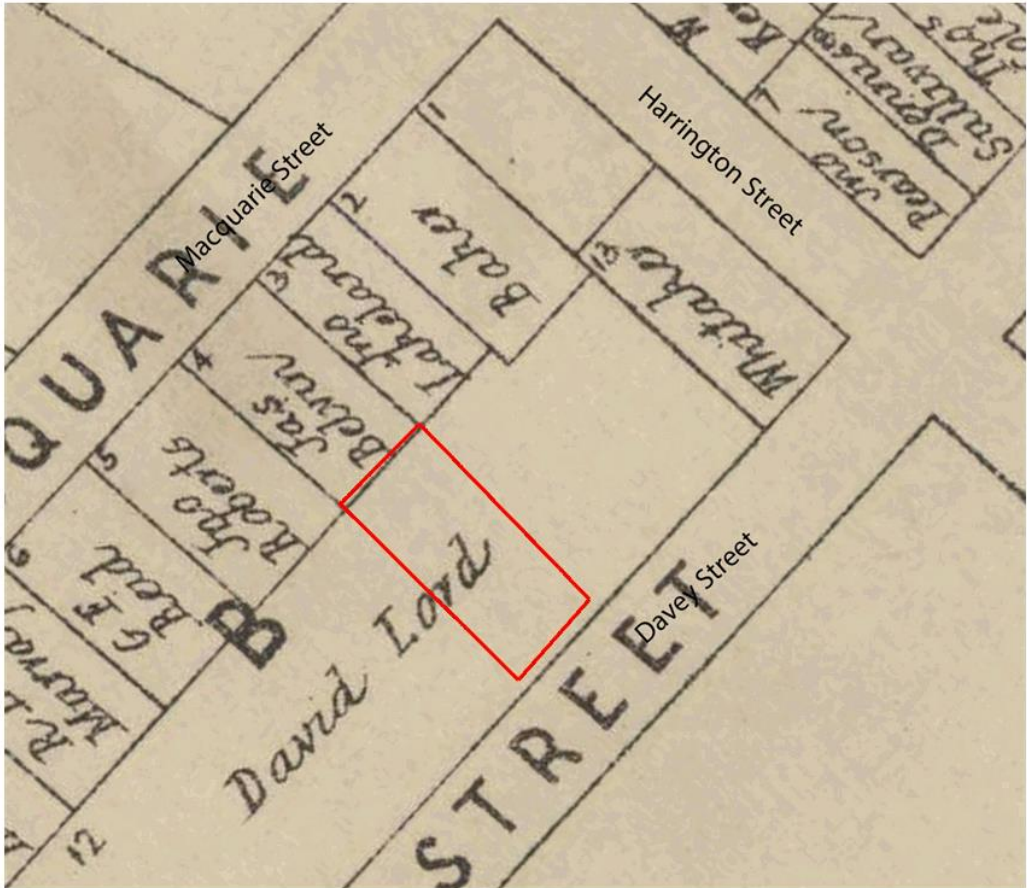


Figure 4.1 – A plan or original lot grants and purchase grants, drawn 1909 which reflects earlier allocations showing the subject site as part of a larger grant to David Lord. Tasmanian Archive and Heritage Office, AF391-1-31.





Figure 4.3 – Excerpt from the c1825 panorama of Hobart Town by Augustus Earle, showing Heathfield in the foreground with a cluster of smaller buildings near/on the subject site (denoted approximately by the red arrow). State Library of New South Wales a1541002h.



Figure 4.4 – Excerpt from Frankland's 1839 map of Hobart and surrounds. State Library of Tasmania, Allport Stack 912.94661MAP.

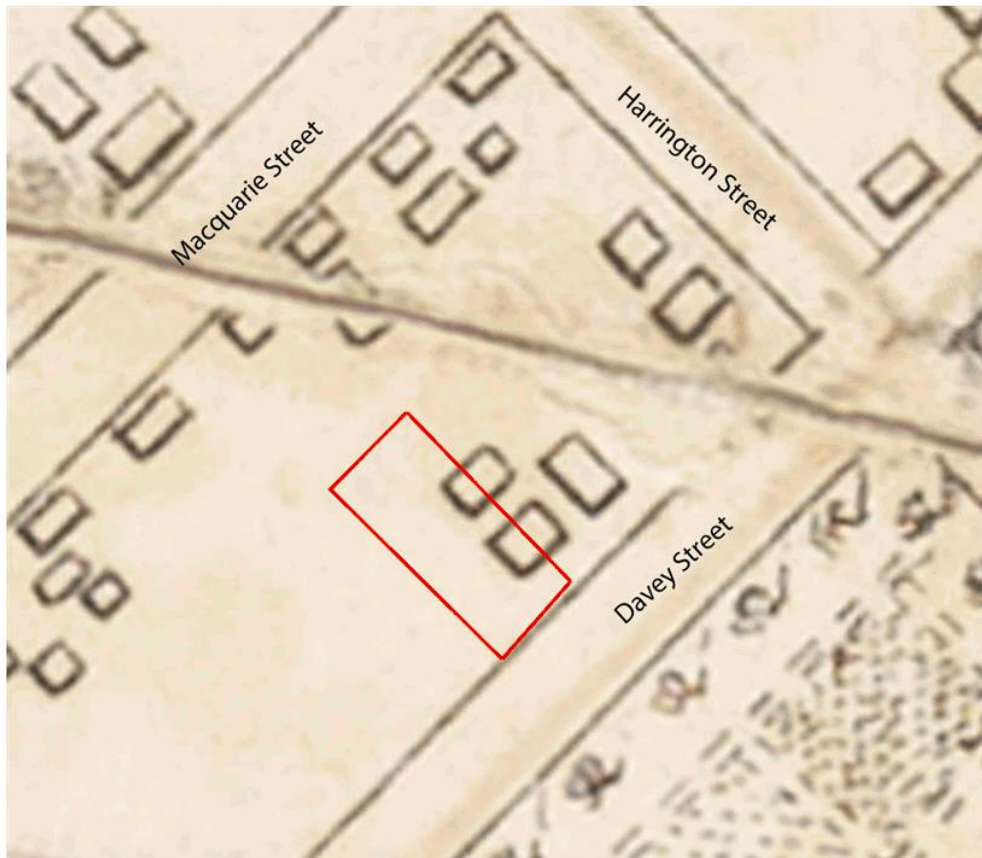


Figure 4.5 - Excerpt from the 1841 census map of Hobart and surrounds. Tasmanian Archive and Heritage Office, CS08-17-578.

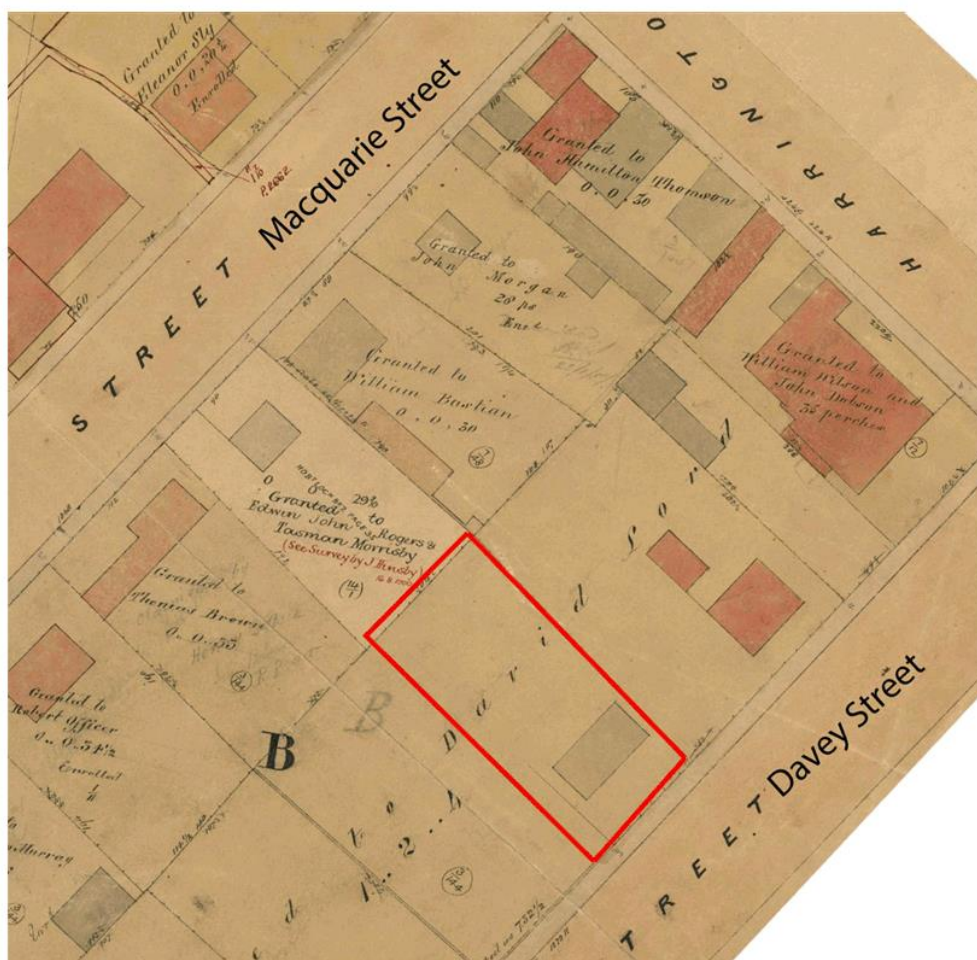


Figure 4.6 - Excerpt from Sprent's c1845 map of Hobart and surrounds (www.thelist.tas.gov.au).

Despite the precise evolution of the Lord landholdings being difficult to trace, land tenure records, cross-referenced with Valuation Roll data for Hobart Town show that by 1871, the property now known as 63 Davey Street was in the possession of John Strange Dandridge, at that time Superintendent at Oyster Cove. Dandridge had married Matilda Prout, daughter of colonial artist John Skinner Prout, in 1848⁶. Later c19th depictions of the building indicate what appears to be a reasonably simple single-storey building with a hipped roof (see Figures 4.7-4.8).

⁶ Tasmanian Archive & Heritage Office (TAHO) RGD33/1/3 #833
Praxis Environment 2018

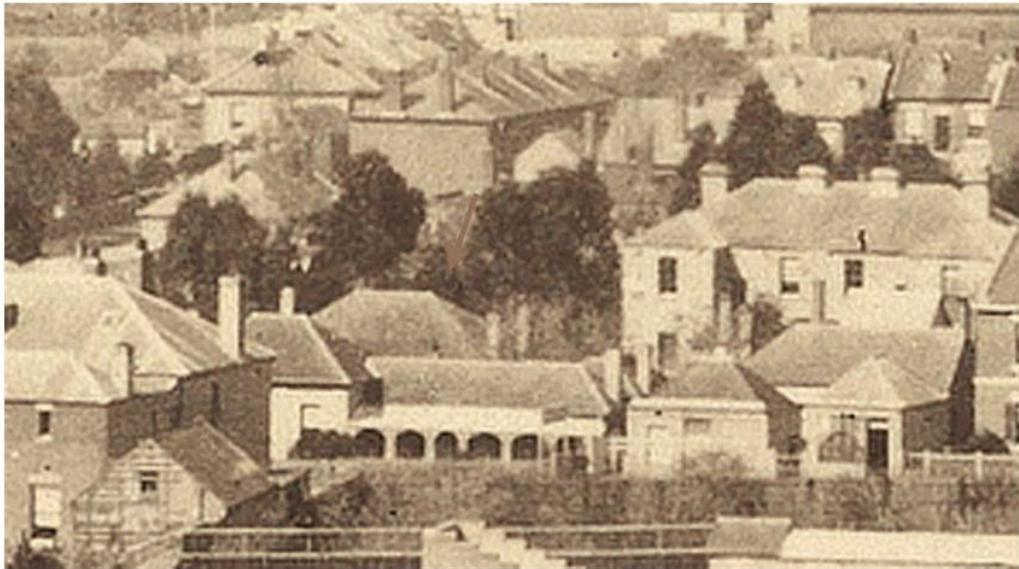


Figure 4.7 - Excerpt from Alfred Abbott's 1878 panorama of Hobart, showing the rear of the subject site (denoted by the red arrow).
Tasmanian Archive and Heritage Office AUTAS001136156486



Figure 4.8 – 'Balloons eye view of Hobart' showing the Davey Street frontage of the building within the subject site (denoted by the red arrow). Australasian Sketcher 10 May 1879

Dandridge died in March 1874⁷, leaving the bulk of his estate to his children by Matilda⁸. Images of the subject site from that time show a consistent pattern of development to the (likely) pre-1830 building, being a single storey residence with a hipped roof.

The property remained in the hands of Dandridge's trustees, until it was sold by the Public Trustees in October 1916 to Herbert Combes, a surveyor, for £825. Prior to the sale, the house had been occupied by one of Dandridge's trustees, Edwin Cradoc Nowell⁹. It appears that by 1907 the building had been renovated with bay windows installed to the frontage which were not evident in the 1879 sketch and not shown on earlier depictions of the footprint of the building (Sprent's survey would surely have depicted these if present). This survey shows a building to the rear of the main dwelling on the site. It is unclear whether this represents the second building implied by the inaccurate c1830s surveys and may have been omitted from the Sprent survey (Sprent only surveyed buildings which could be seen from the street). That building is best depicted on the 1946 aerial photograph and appears to be at least two phases of building each with a distinct individual roof form and these survived until the 1953 construction of the Navy Club building.

⁷ TAHO RGD35/1/8 #1938

⁸ Department of State Growth (DSG) Deeds Library Memorial 11/8307

⁹ *Tasmanian Government Gazette* 5 December 1910 p.2208



Figure 4.10 – 1907 Metropolitan Drainage Board survey showing the subject site and surrounds (Hobart Sheet 9).

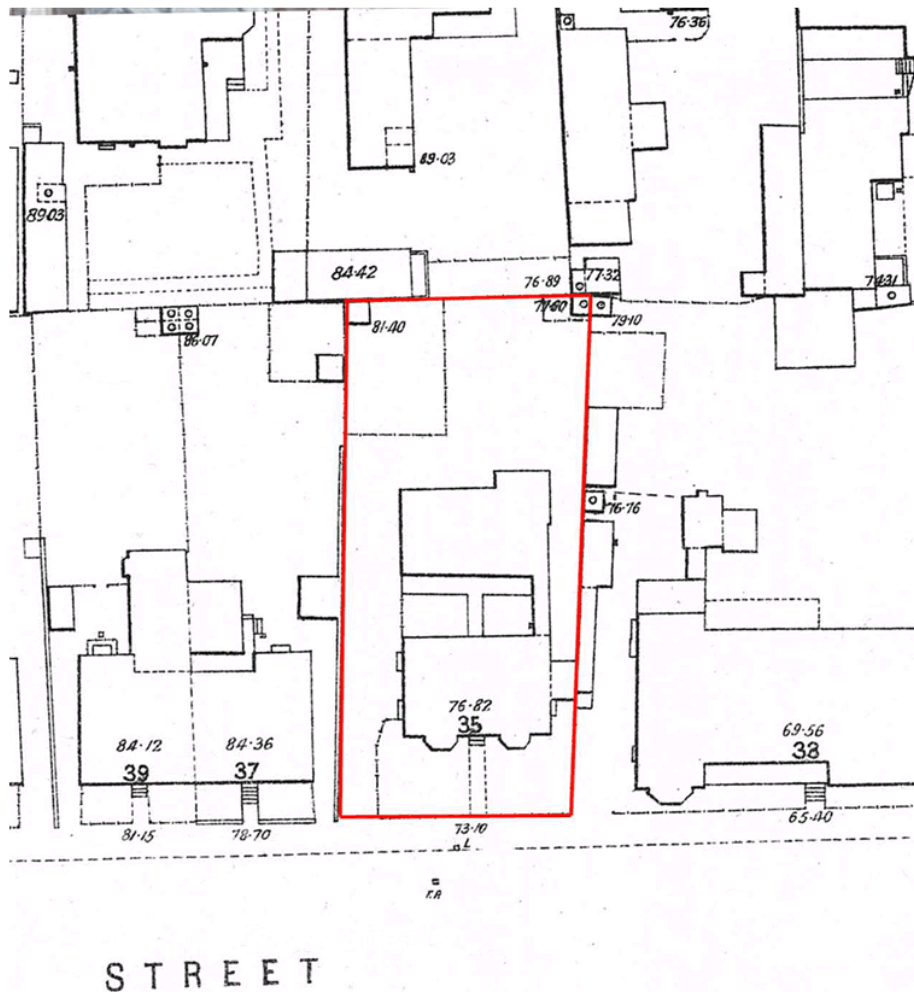


Figure 4.10a – Detail of 1907 Metropolitan Drainage Board survey showing the subject site and surrounds (Hobart Sheet 9).

Combes died in Oct 1919, leaving the property in equal shares to his children Rita and Herbert Combes (junior). The property stayed in the Combes family until May 1949, when Edmund Raymond Fergusson (widower of the late Rita Fergusson nee Combes) sold the property to Bertrand Lucien Dechaineux and Maxwell Ernest Mathers for £2,560¹⁰. Dechaineux and Mathers purchased the property as trustees of the Ex-Naval Mens' Association of Tasmania Hobart Sub-branch¹¹. In July 1954, the trustees transferred ownership to the Naval

¹⁰ DSG Mem 19/1385; Mem23/7496; Mem24/4013; Mem24/4014

¹¹ DSG Mem 24/4015

Memorial House¹². Note that the more generic term 'Navy Club' will be used in this document for simplicity – and reflecting the later name of the site.



Figure 4.11 – A c1934 photograph of 63 Davey Street, entitled 'house of Mrs. Furguson'. Tasmanian Archive and Heritage Office NS1298/1/1809.

¹² DSG Mem 28/1098



Figure 4.12 - The subject site taken from the 1946 aerial run of Hobart (Run 1, 10894).

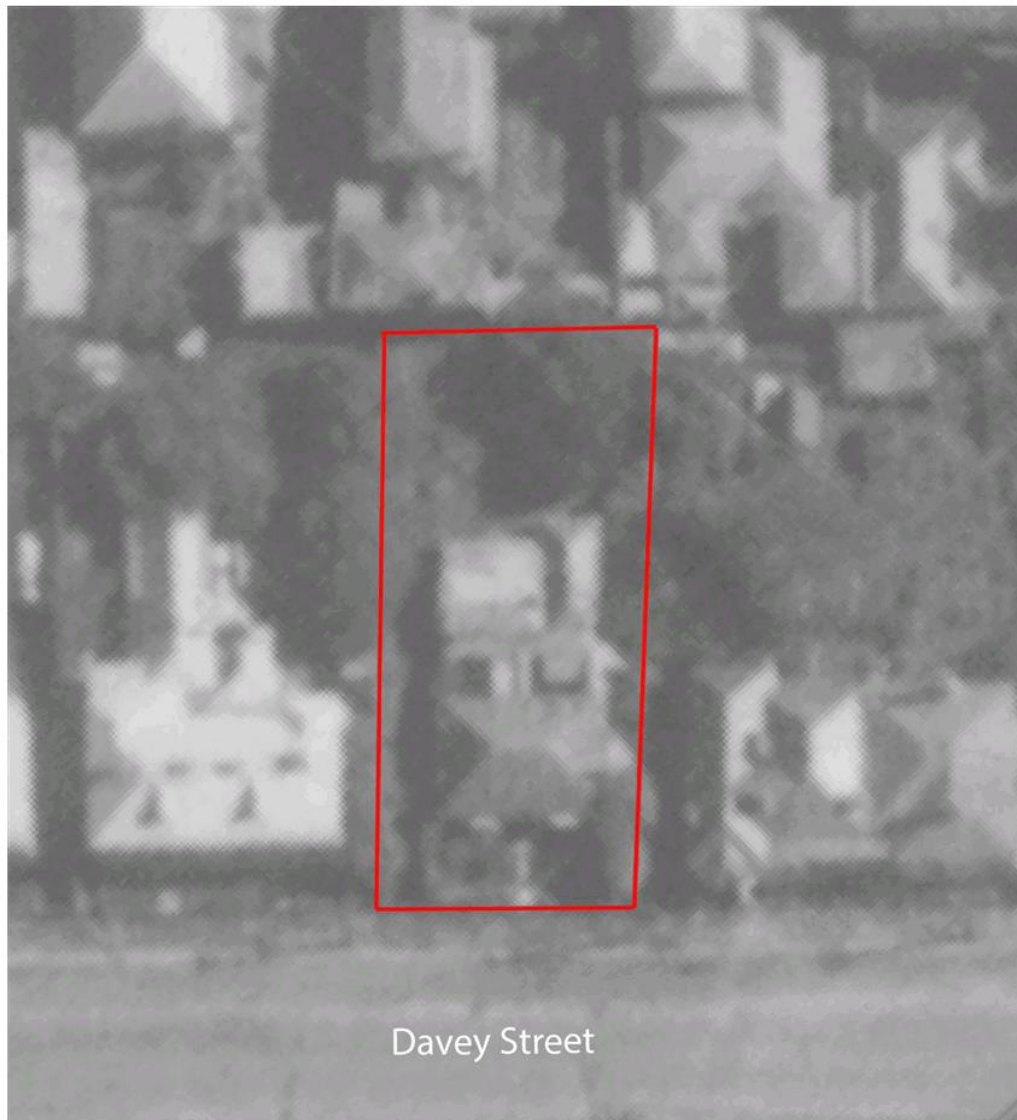


Figure 4.12a - The subject (detail) site taken from the 1946 aerial run of Hobart (Run 1, 10894).



Figure 4.13 – Excerpt from a 1953 oblique aerial photograph of the area, the subject site denoted by red arrow. Tasmanian Archive and Heritage Office PH30-1-5204.



Figure 4.14 – Excerpt from a 1953 oblique aerial photograph of the area, the subject site denoted by red arrow. Tasmanian Archive and Heritage Office PH30-1-5205.

In 1953, plans were approved for a purpose built *Naval Memorial House* at 63 Davey Street which retained the existing dwelling on the streetfront (the outbuilding at rear removed), but involved the addition of a hall at rear, with a terrace to the east and a canteen, kitchen and toilets along the western edge. The building was enlarged in its first few years of life with 1957 plans showing the addition of a billiards room, additional kitchen and ladies lounge. This filled the entire rear portion of the site to the boundaries. That extended area was renovated in 1971 and converted to a member's bar and committee room. At that time a bulk store and foyer was added. The earlier building remained standing at that time and is notated on the plans as a 'rental premises'.

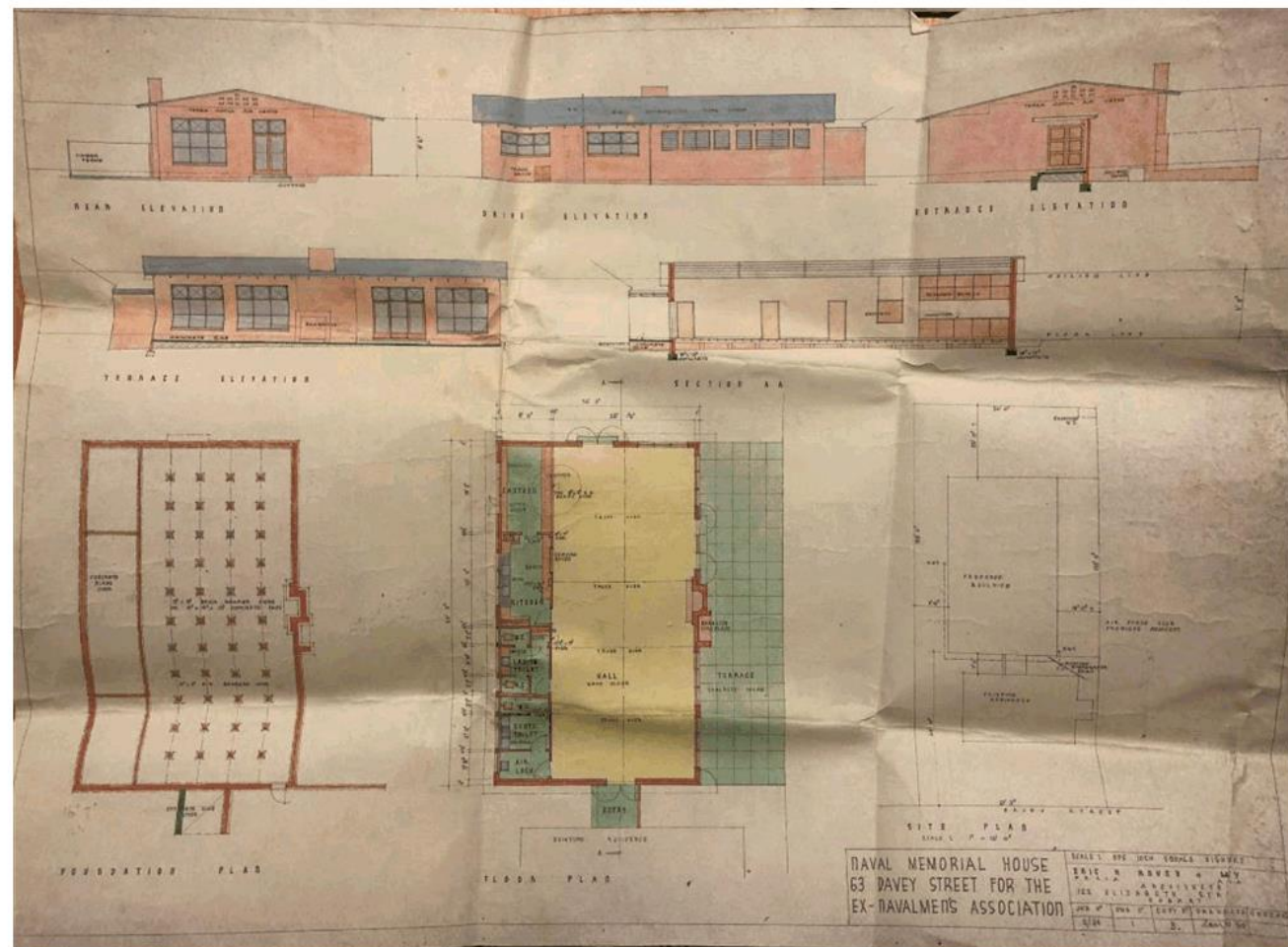


Figure 4.15 – Floor and site plan for the Naval Memorial House, 1953. Tasmanian Archive and Heritage Office AE417/2/4689.

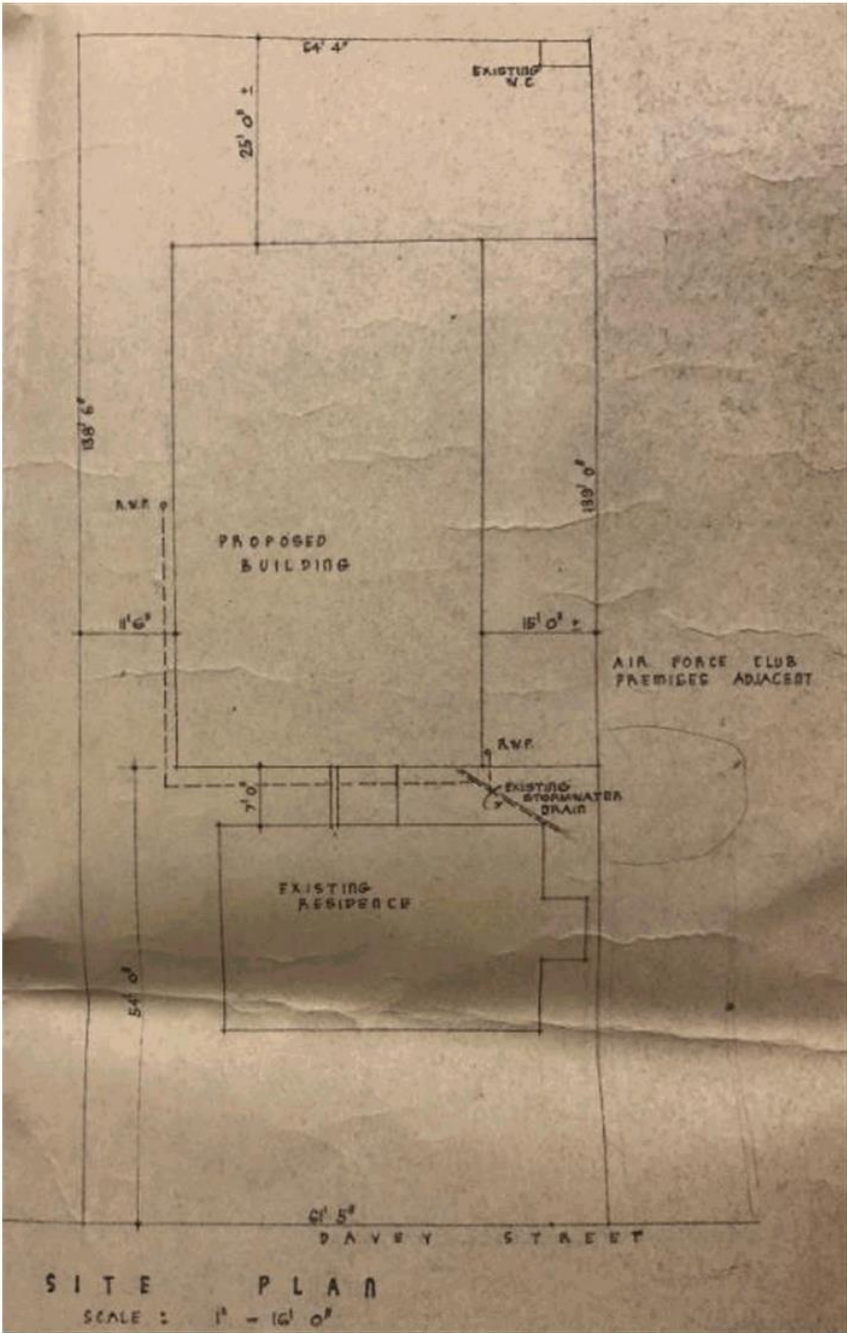


Figure 4.15a – Site plan for the Naval Memorial House, 1953. Tasmanian Archive and Heritage Office AE417/2/4689.

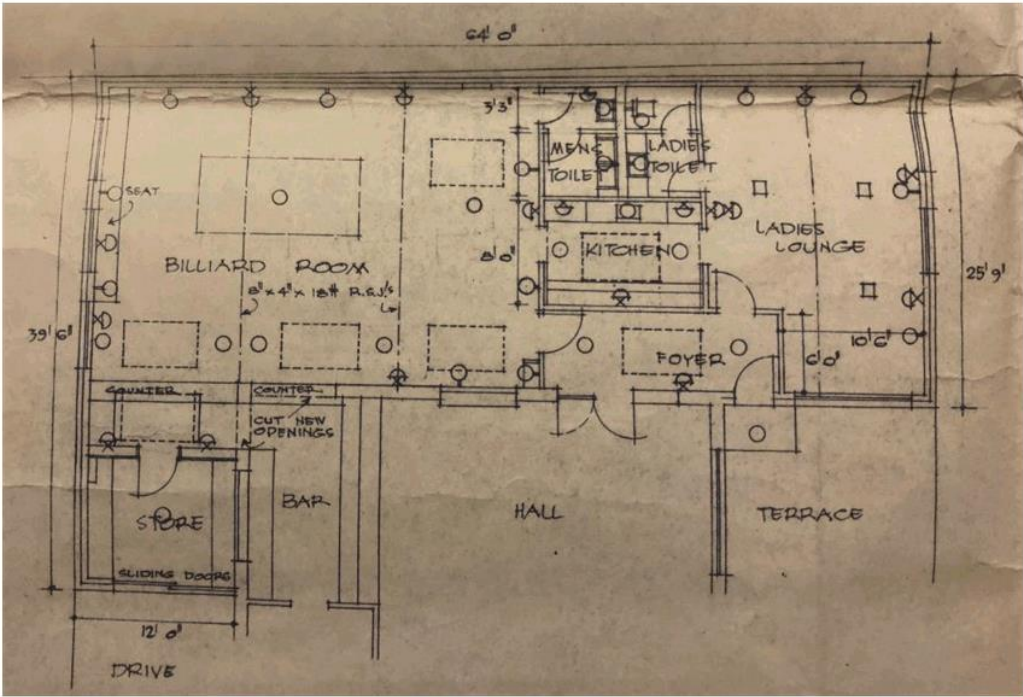


Figure 4.16 – Additions to the Naval Memorial House, 1957. Tasmanian Archive and Heritage Office AE417/2/2454



Figure 4.17 - The subject site taken from the 1958 aerial run of Hobart (Hobart Run 5 T332-12 March 1958).

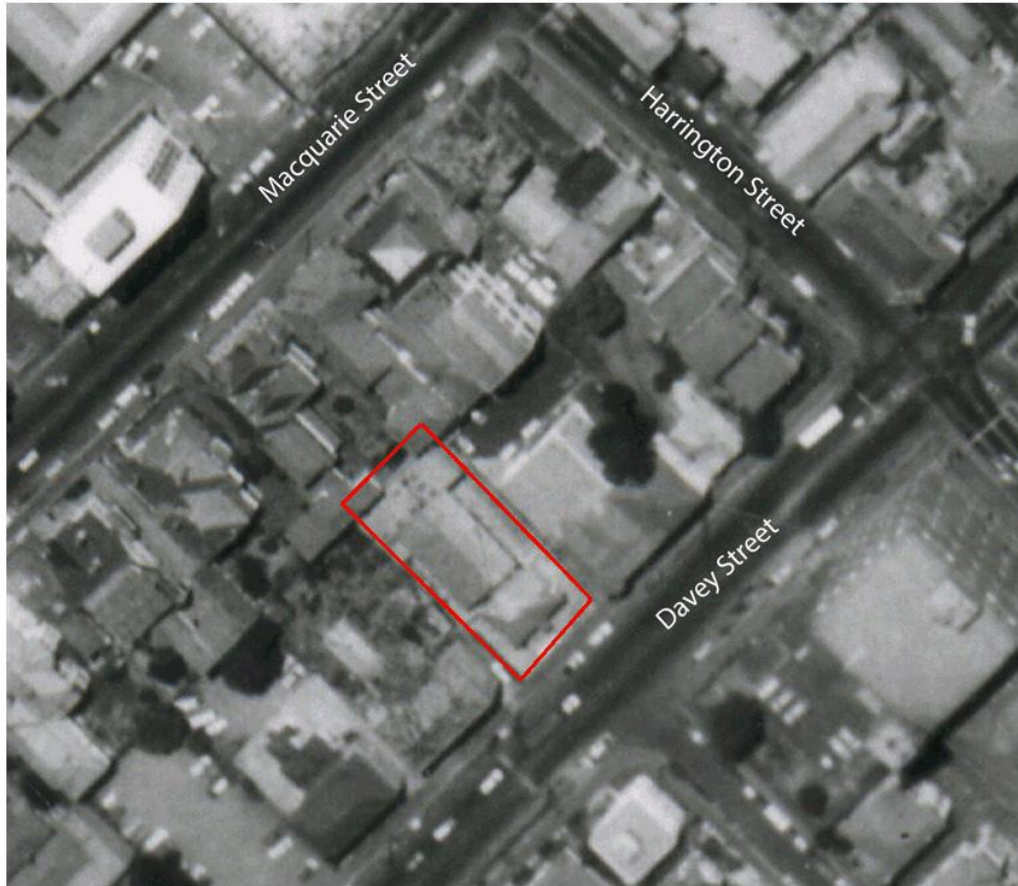


Figure 4.18 - The subject site taken from the 1968 aerial run of Hobart (Hobart Run 6-153 February 1968).

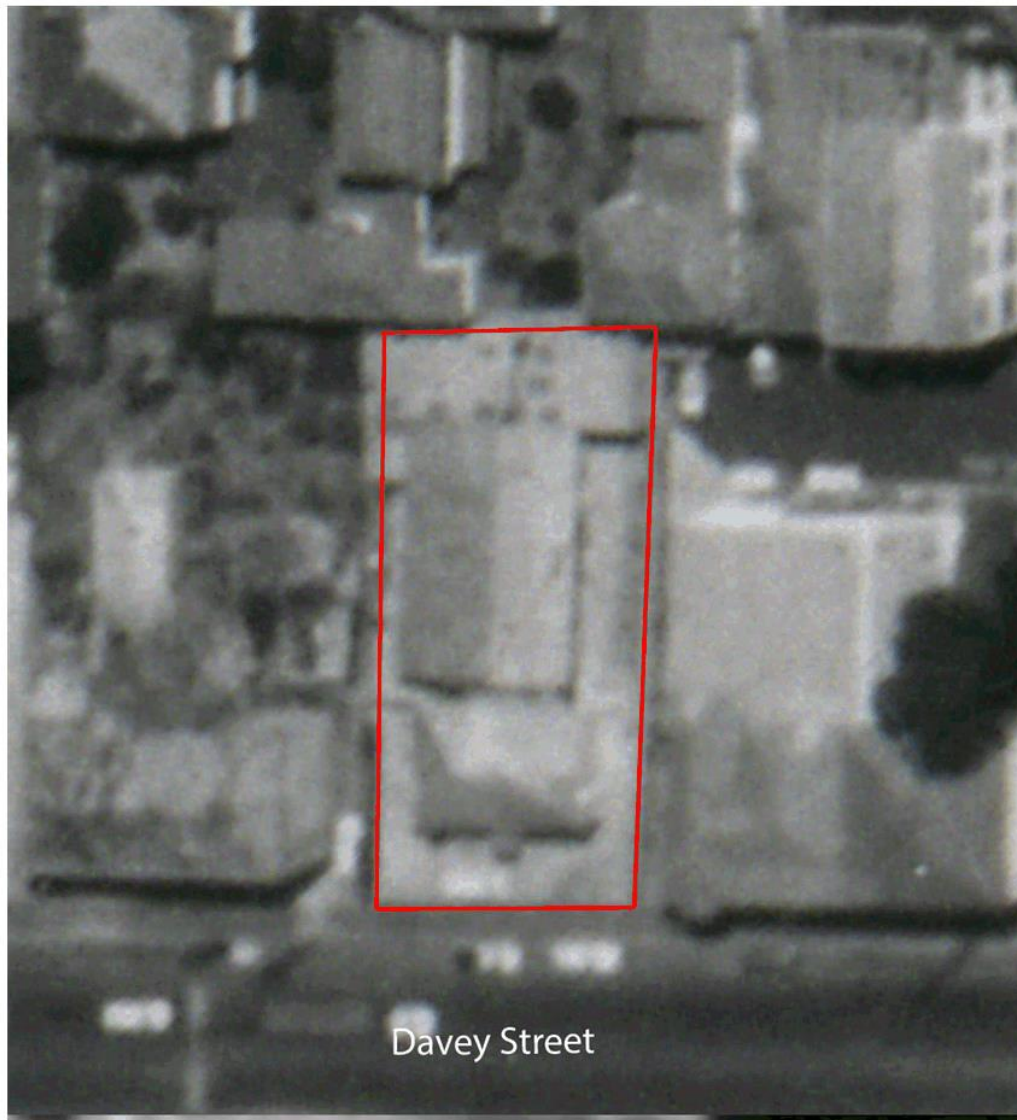


Figure 4.18a - The subject site (detail) taken from the 1968 aerial run of Hobart (Hobart Run 6-153 February 1968).

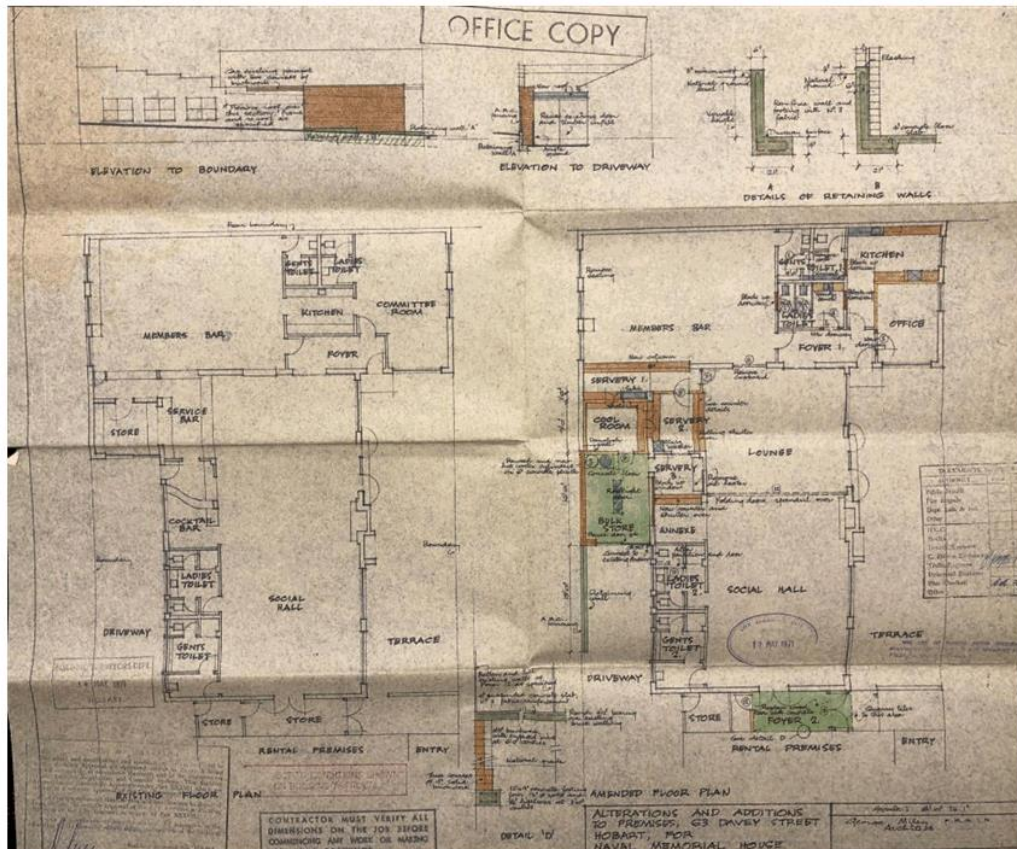


Figure 4.19 – Additions to the Naval Memorial House, 1971. Tasmanian Archive and Heritage Office AE417/5/971.

Further renovations were undertaken in 1979 as a staged development, with a new entrance hall added to the eastern side as the first stage, then demolition of the residence at front to make way for a new stage, secretary's office and store. Those plans indicate that the carpark is to be excavated and four spaces created – although comparison of the site levels currently with that historically (see Section 5) suggests that the carpark was built up – not excavated.

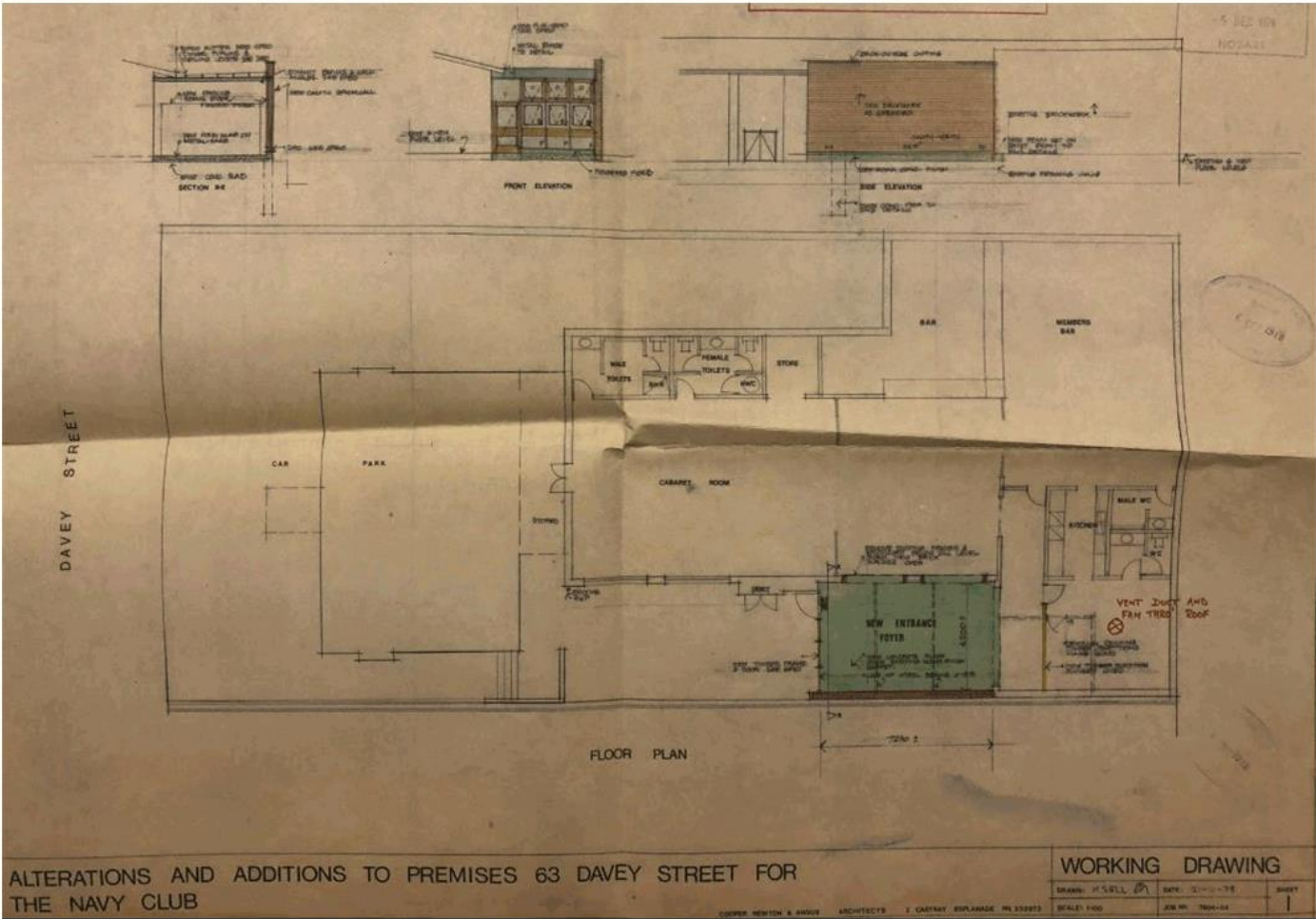


Figure 4.20 – Additions to the Naval Memorial House (Stage 1), 1979. Tasmanian Archive and Heritage Office AE417/6/2955.

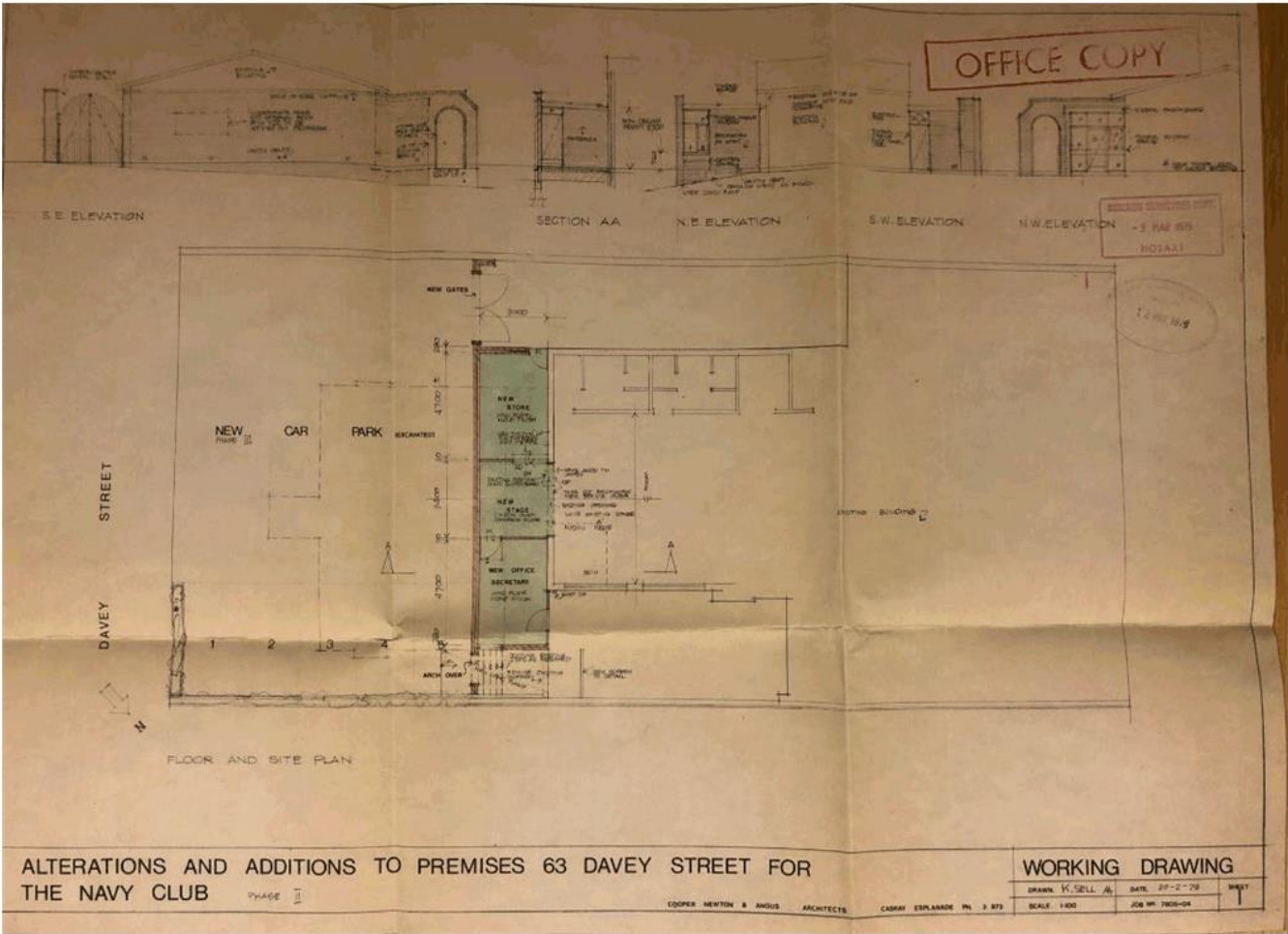


Figure 4.21 – Additions to the Naval Memorial House (Stage 2), 1979. Tasmanian Archive and Heritage Office AE417/6/3134.

Summary of development of the subject site

The above historical summary shows that:

- The site was probably developed around 1830 with a dwelling facing Davey Street and at least one outbuilding near to the rear of that dwelling.
- The site remained largely unchanged until the 1953 construction of the Navy Club (and by later names) building and at that time the outbuildings were removed (but the dwelling retained).
- The Navy Club was enlarged in 1957, 1971 and 1979.
- The residence was retained until 1979 when it was demolished for a carpark and front extension to the Navy Club building.

The following figures show overlay plans of known historic development on the subject site, drawn from the survey plans depicted above which are considered to have the greatest accuracy:



Figure 4.22 – Overlay of the c1830s depiction of the buildings on/near the subject site (blue). Note that the accuracy of these surveys is known to be low – merely depicting the *presence* of buildings, rather than necessarily an accurate location.



Figure 4.23 – Overlay of the mid-1840s depiction of the buildings on the subject site (green), as per the Sprent survey in relation to the subject site (red). This survey is known to have a very high accuracy but does not necessarily show outbuildings that were not visible from the street.

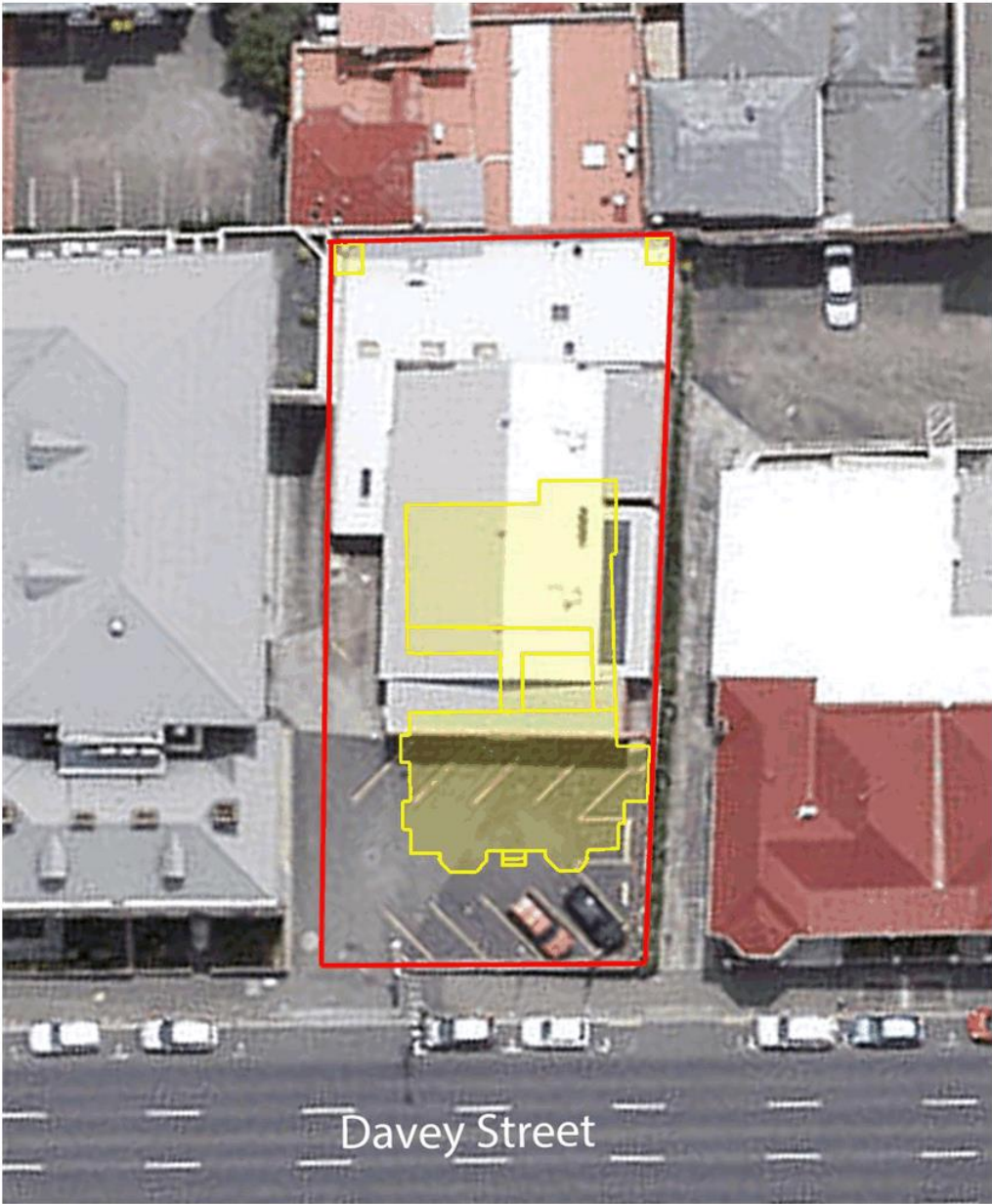


Figure 4.24 – Overlay of the of the pre-1908 depiction of the buildings on the subject site (yellow) in relation to the subject site (red).
This survey is known to have a very high accuracy.

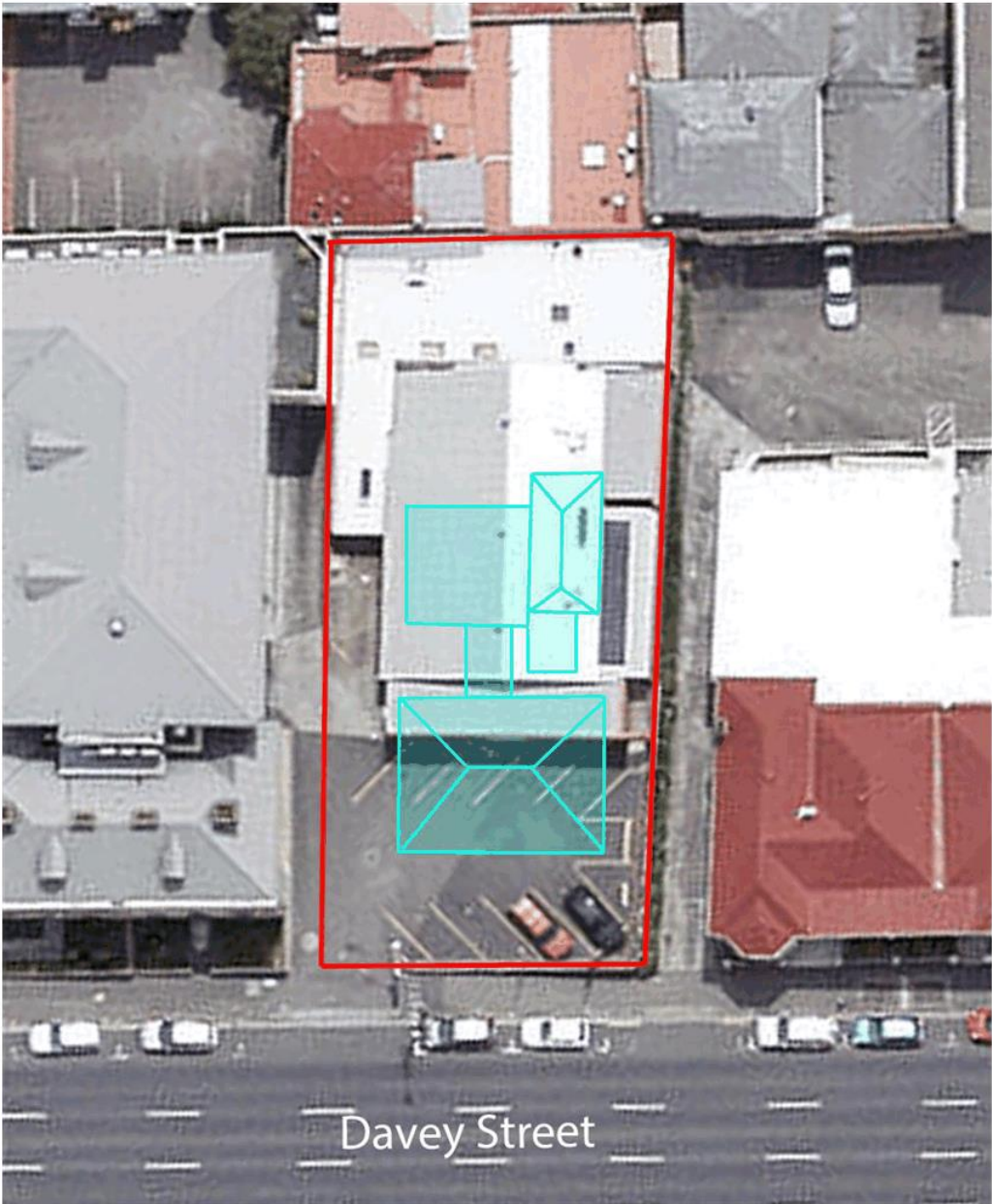


Figure 4.25 - The form of the site during the mid-twentieth century (i.e. pre-1953) based on the 1948 aerial photograph.



Figure 4.26 – Composite overlay of the footprint of all known pre-1946 buildings and site features (colours as per coding above) in relation to the subject site (red).

5. Current site observations and assessment of prior disturbance

As per the methodology outlined in Section 2.1, Section 3.3 has formed a desktop assessment of the factors which have influenced the development of the possible archaeological resource within the subject site over a 180+ year period.

However, it is critical to understand other factors, in particular site disturbance, which may have impacted upon the archaeological potential of the site and its ability to provide meaningful archaeological remains which answer research questions such as those above.

This section will review site observations and likely scenarios which would have resulted in disturbance, in order to assist in understanding the likelihood of the survival of archaeological remains.

General site observations

Little insight into the archaeological potential of the site can be gained through site observation, given that the site is largely covered with buildings. However, the forecourt (carpark) of the site which was the location of much of the footprint of the house is clear of buildings. Whilst the 1979 plans note that the carpark area is to be 'excavated', the current ground level appears to have been heightened when compared to the 1934 photograph of the building, with a retaining wall on the eastern boundary and the carpark surface some 500mm higher than the footpath level of Davey Street. The 1934 photograph shows that historic ground level within the site followed the natural contour (as evident by the front fence) therefore it is much more likely that this part of the site has been filled rather than cut. The bricks on this retaining wall are consistent with that of the 1979 extension which suggests that the work was all done at that time.



Figure 5.1 – The current Davey Street frontage (Google Streetview).



Figure 5.2 – The same view in 1934. Tasmanian Archive and Heritage Office NS1298/1/1809.

Likely specific disturbance events

Whilst the observations above give little real detail on possible disturbance, a disturbance history can also be built from a desktop assessment - i.e. known events which are likely to have impacted upon archaeological remains. Section 4 has detailed the evolution of the site from the historical information which is available. The possible impact upon archaeological remains deriving from each of these events will be detailed below:

Demolition of the earlier buildings and construction of the current buildings

No information has been found in the historical research undertaken for the current document which gives any clue as to the extent or method of demolition of the earliest buildings on the site – the outbuildings (possibly c1830s) in c1953 and the dwelling itself in 1979. The only hint can be gained from Figures 5.1 and 5.2 which suggest that filling of the site was done, which suggest that demolition of the main dwelling may not have been thorough and there may be foundation remains and occupational debris beneath this fill.

Whilst the method and extent of demolition or removal of subsurface material of the outbuilding c1953 is not known, the foundation design of the portion of the Navy Club building built at that time shows that the building is on strip footings on a 18" wide by 12" deep concrete beam to the perimeter of the building, with 9"x9" brick piers dotted 4'-wide for the length of the building on 15" square and 12" deep concrete pads. The structural section of those plans show that the natural ground level of the site appears to have been retained with only localised cutting at the rear of the building (outside the footprint of the earlier building). The building was built up with a substantial underfloor area – again suggesting that the site was not broadly excavated. Whilst this would have had localised impact dotting the site, it would not have required widespread excavation and there may be remains of those earlier buildings and occupational debris beneath the current building.

Subsequent service trenches etc.

A search of underground asset registers (via *Dial Before You Dig*) revealed that there are no major underground assets in the subject site (logical, that site being an extant building) with the exception of a NBN cable crossing the carpark site. There may be minor service assets in the subject site (i.e. services within the building and carpark) or lines of disused/redundant services however it is unlikely that these have caused major disturbance to archaeological remains.

6. The likely significance and research potential of archaeological remains

As depicted above, the subject site has a very simple development history, only one significant historical development phase – the c1830s construction of the dwelling facing Davey Street and outbuildings at rear, all of which survived until around 1953 until gradual demolition by construction and expansion of the Navy Club building to become what it is today.

It is likely that the demolition of those earlier buildings and construction of the current (former) Navy Club buildings has not destroyed all earlier archaeological remains. Given the demolition of the buildings and formation of a carpark over any remains in the late 1970s, any such remains would be limited to low-level structure (i.e. foundations, possible lower courses of the buildings) and any subsurface features such as basements, wells, cesspits etc. – although no such structures have been determined through historical research (i.e. no such structures are described in early accounts of the buildings), although are considered possible. There is also the possibility of artefactual remains relating to the habitation and use of the buildings as per the thematic discussion below.

The site may also yield information on site formation processes which have acted upon the site, both pre and during construction (e.g. alteration of the natural landform, construction rubble), use (e.g. occupation deposits), demolition (e.g. demolition rubble) and post-demolition use (e.g. fill and disturbance).

Remains associated with the residence and domestic occupation are also considered to be of high archaeological potential due to their earliness and long-use period (some 140+ years) and have the potential to demonstrate 19th century domestic life in the area (and wider Tasmania for that matter).

From a wider regional perspective, archaeological data and remains yielded from the subject site, whether coupled with other Hobart/Tasmanian data, has the potential to strengthen a comparative dataset for research into intra-colonial society through comparison with mainland (and indeed inter-colonial society on an international level). For example early inner-city working-class communities such as Broadway, Cumberland/Gloucester Streets and the Rocks (Sydney) and Little Lonsdale Street (Melbourne) and portside working-class areas such as Port Adelaide, all of which have had substantial archaeological works undertaken which include hotel sites and early inner-city housing and would provide useful datasets for the inter-colonial analysis of any Tasmanian data which would in-turn add to the depth and scope of the analysis of those collections on the range of themes as outlined above (and others).

From a temporal perspective, any remains from the earlier occupation of the early domestic habitation of the housing represent a formative period of the settlement of Hobart and are likely to be of significance when considering their research potential. Archaeological investigations and analysis of the former residential component of the site, dating from c1830, has the potential to add depth to other similar such analyses of early-mid Victorian Hobart domestic sites, including investigations such as that undertaken as part of the Menzies Centre (Liverpool/Campbell Streets) excavations, which investigated several prominent 1820s-onwards inner city residences, including Crowther's (Godden Mackay Logan/Arctas). Similarly, investigations at Peter Degraives house in Collins Street (Hadleys Hotel development, Godden Mackay Logan) and preliminary investigations at the original Hobart Port Officer's residence at 100 Salamanca Place (Praxis Environment) have investigated early inner city residential sites. Forthcoming reports on excavations on other Hobart domestic sites such as Kemp's house (36 Argyle Street), Judge Pedder's house (173 Macquarie Street), Crowther's house/surgery (177 Macquarie Street) will also act to build upon knowledge and provide comparative datasets of early Hobart residences. There have been few examples of archaeological investigations into wider communities around the Hobart CBD, i.e. investigations which cover a wide number of adjacent sites representing different functions (such as the Whale Fishery Inn and adjacent housing). Notable examples however are the range of Wapping investigations (e.g. Austral Archaeology 1996, 1998, 2002, 2009) and the forthcoming report on the Montpelier Retreat excavations undertaken by Austral Tasmania in 2015. Results from the Bathurst/Watchorn Streets site could add to that range of data derived from those other inner city colonial enclaves.

Consistent with the 'Tiered research question' approach outlined in the Tasmanian Heritage Council's *Guidelines for Historical Archaeological Research on Registered Places*¹³, the following questions could be investigated in the archaeological remains expected to be present within the subject site:

Tier 1 Questions: These questions outline the essential knowledge base needed for any site research or significance evaluations. Such questions are often empirical in nature, and straightforward answers can be sought and often identified – generally limited to a physical knowledge of that particular place. Questions relevant to the subject site may include:

- How closely did the buildings and site features conform to the historic plans?
- What construction methods were used in the buildings and other infrastructure?

¹³ <http://www.heritage.tas.gov.au/media/pdf/Archae%20ResGlines%20%20FINAL%20-%20June%202009.pdf>
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- What evidence of alteration of the natural landscape and cultural interventions to the site is archaeologically determinable (e.g. filling of the site, demolition events, site formation processes etc.).
- Are the distinct use/development phases of the buildings distinguishable?
- Can the layout and function of the buildings, and indeed individual rooms or yard spaces be ascertained?
- How thoroughly were the buildings demolished?

Answers to these questions provide a foundation of information about the structure, type, use and duration of site occupation which enables the researcher to consider a second tier of questions.

Tier 2 Questions: Conclusions that can be drawn about a site that connect the material remains found on a site to specific behavior. For instance, can hotel activities be linked to particular trade, use or entertainment activities on the site. Do artifacts relate to the lifeways of the households that lived and/or worked on the site? For instance, do any artifacts represent class, gender, taste and health/hygiene of those living/working on the site? Particularly if artifacts can be specifically dated, and with supplementary historical research, artifact assemblages from this site may contribute knowledge and provide tangible connectedness to known hoteliers, workers, patrons, inhabitants etc., and how they lived. How do these demonstrate the mix of hospitality, residential and commercial uses of the site?

Tier 3 Questions: These questions represent the highest level of inquiry. Such questions associate the activities and behavior at individual sites with broad social, technological and cultural developments – which can be of interest on local, national or global lines of enquiry. Whilst these questions posed for a single site may not reach conclusions in the short term (as Tier 1 and 2 questions might) – the collection of data can contribute to future research by the provision of a comparable dataset. The goal of such research is to develop increasingly refined and tested understandings of human cultures within broader theoretical or comparative contexts. Lines of wider enquiry that findings from within the subject site may contribute to are:

- Are there class or status differences evident in the material culture of the inhabitants of this area (subject to further historical research) when compared to, say, other early Hobart residential dwellings/enclaves or sites in contemporary rural areas and/or other cities?
- Did any changes in material culture through time in the residences coincide with wider Tasmanian or local events or technology (e.g. urbanisation/development of Hobart, railway/port upgrades, start of rubbish collection etc.)?

7. Archaeological zoning plan and policies

As per the methodology outlined in Section 3, this section has built a chronology of site development which has detailed the physical evolution of the site and events/processes which would have acted to build the archaeological record. Section 6 has discussed the likely significance of those archaeological remains and what they may yield in terms of research potential alongside key historic, regional, thematic and temporal lines of enquiry. Section 6 has provided an assessment of the events which are likely to have impacted upon the integrity of those archaeological remains.

From the above, it is therefore plausible to propose that due to the site being the location of early development, which has probably not been subject to substantial disturbance, it may yield archaeological remains which have the potential to contribute to a knowledge of important Tasmanian heritage themes as per the research framework in Section 6.

The site may yield physical remains of those buildings, as well as artifacts relating to the occupation and use of those buildings, which may yield information which is not readily available (or available at all) from historical sources.

Note that the overlay plans of known early building footprints as depicted in Figures 4.22 - 4.26 do not cover the entire subject site (i.e. are concentrated towards the eastern and northern edges) it is feasible to propose that parts of the subject site have different abilities to yield building remains and remains of concentrated habitation. This is not to imply that archaeological remains are only found within building footprints, but the concentration of such remains is likely to be less the further away from building footprints (noting that there may still be remains of ancillary features and other occupational debris outside building footprints).

Based on the known and likely early building footprints, the following archaeological zoning plan is proposed for the subject site:

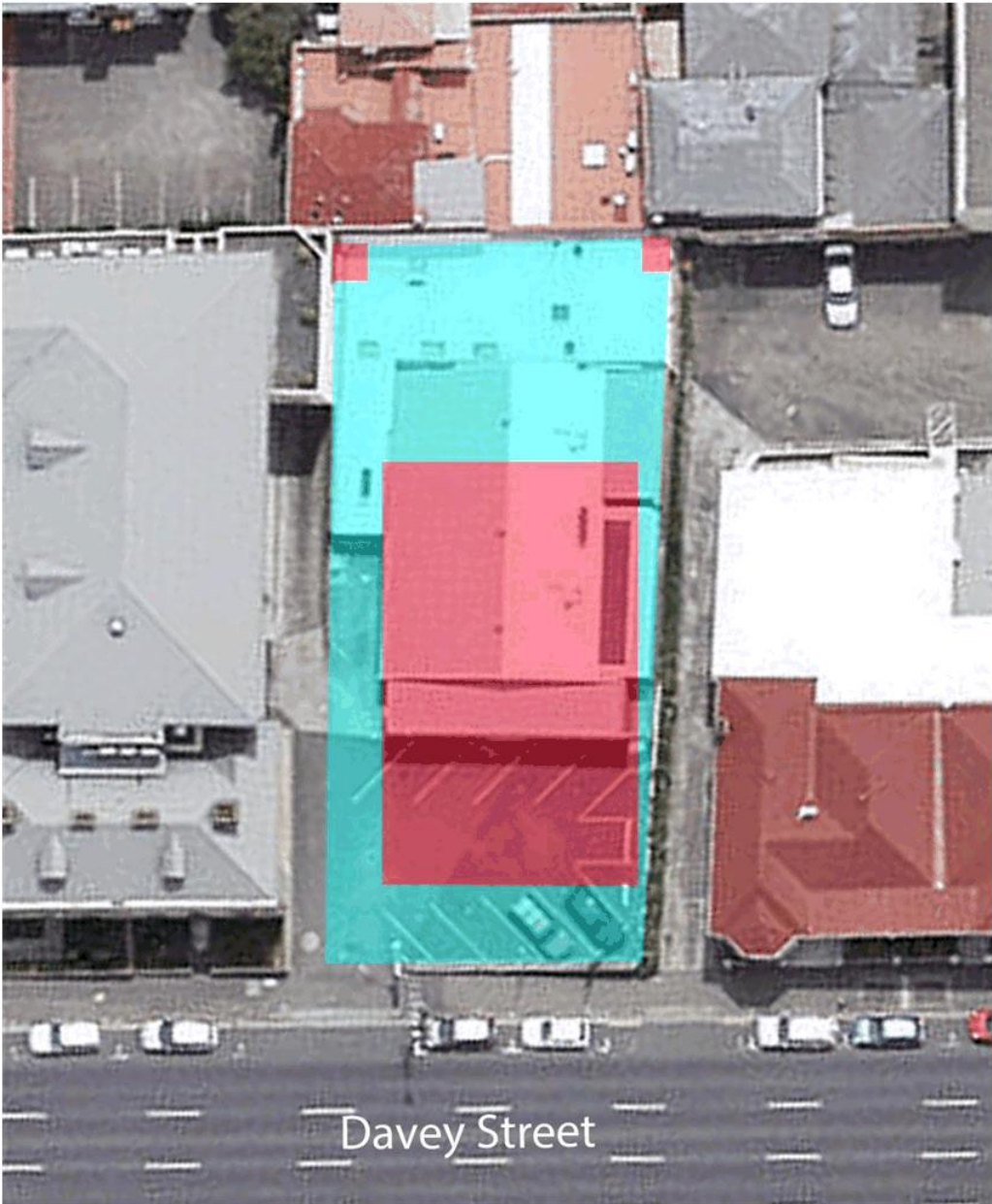


Figure 7.1 – Archaeological zoning plan for the subject site. Red denoting areas of high archaeological potential and blue depicting areas of low archaeological potential.

The following table considers the archaeological remains which may be found within each specific area.

Area	Likely remains	Likely integrity	Significance/potential
Red	<p>Foundations and other structural remains associated with the c1830 dwelling and outbuildings (including what appears to be two privies at the rear of the site).</p> <p>Artifacts relating to the early domestic occupation of those buildings.</p> <p>Deposits and debris which may provide information on site formation processes (e.g. demolition, fill etc.).</p>	Likely to be largely archaeologically intact, given the lack of evidence of any substantial disturbance, except for some localised disturbance at the time of construction of the current buildings.	Of high archaeological potential and historical interest in demonstrating the evolution of the site, the layout and construction of the early buildings and the material culture of those using/inhabiting the buildings over some 140+ years.
Blue	Remains likely to be limited only to scant occupational debris or possible landscape elements (e.g. paths, drains etc.) associated with areas of the site limited to open spaces or ancillary/minor development.		Of low or no archaeological potential given the lack of any substantial historical development in this area.

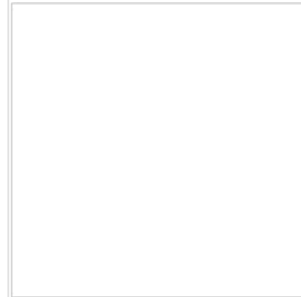
Accordingly, the following archaeological management policies are recommended:

1. Any excavation proposed in areas of **high archaeological potential** must be preceded by an archaeological impact assessment, and if necessary an archaeological method statement, which details measures to be taken to avoid or mitigate impact upon the archaeological resource. That method statement must be in accordance with industry standard (e.g. the Tasmanian Heritage Council's Practice Note 2 – *Managing Historical Archaeological Significance in the Works Application Process*) and implemented in the works process.
2. No archaeological input is required for excavation in areas of **low archaeological potential**, however any unexpected finds must be reported to a qualified historical archaeologist who is to assess their significance and deal with any significant finds as per (1) above.

Planning: #179370

Property

63 DAVEY STREET HOBART TAS 7000

**People**

Applicant

*

Tellyros Klonis Unit Trust, by their Agent, Ireneinc Planning
and Urban Design49 Tasma Street
49 Tasma Street
NORTH HOBART TAS 7005
62349281
tim@ireneinc.com.au

Owner

*

Tellyros Klonis Unit Trust

49 Tasma Street
49 Tasma Street
NORTH HOBART TAS 7005
62349281
tim@ireneinc.com.au

Entered By

PHIL GARTRELL
49 TASMA STREET
NORTH HOBART TAS 7000
03 6234 9281
tim@ireneinc.com.au**Use**

Multiple dwellings

Details

Have you obtained pre application advice?

- No

If YES please provide the pre application advice number eg PAE-17-xx

Are you applying for permitted visitor accommodation as defined by the State Government Visitor
Accommodation Standards? Click on help information button for definition. If you are not the owner of the
property you MUST include signed confirmation from the owner that they are aware of this application.

*

<input type="radio"/> No		
Is the application for SIGNAGE ONLY? If yes, please enter \$0 in the cost of development, and you must enter the number of signs under Other Details below. *		
<input type="radio"/> No		
If this application is related to an enforcement action please enter Enforcement Number <input type="text"/>		
Details		
What is the current approved use of the land / building(s)? *		
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Please provide a full description of the proposed use or development (i.e. demolition and new dwelling, swimming pool and garage) *		
<input type="text" value="Demolition and construction of multiple dwellings and visitor accommodation"/>		
Estimated cost of development *		
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		Site area (m2)
Existing floor area (m2)	Proposed floor area (m2)	<input type="text" value="809"/>
Carparking on Site		
		N/A
Total parking spaces	Existing parking spaces	<input type="text" value="0"/> <input type="text" value="0"/> <input type="text" value="Other (no selection chosen)"/>
Other Details		
Does the application include signage? *		
<input type="radio"/> No		
How many signs, please enter 0 if there are none involved in this application? *		
<input type="text" value="0"/>		
Tasmania Heritage Register		
Is this property on the Tasmanian Heritage Register? *		
<input type="radio"/> No		
Documents		
Required Documents		
Title (Folio text and Plan and Schedule of Easements) *		
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Plans (proposed, existing) *		
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Supporting Documents		
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<input type="text" value="Heritage Report"/>		
<input type="text" value="63 Davey St Heritage Review.pdf"/>		
<input type="text" value="Supplementary Heritage Report"/>		

Supplementary - Davey Street Heritage 21May2019.pdf
Civil Plans
190409 CIV 18E51-3 C.pdf
Stormwater Report
190305 SR 18E51-3.pdf

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

SEARCH OF TORRENS TITLE

VOLUME	FOLIO
54396	1
EDITION	DATE OF ISSUE
3	29-Aug-2013

SEARCH DATE : 16-Jan-2019

SEARCH TIME : 02.00 PM

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Diagram 54396

being the land described in Conveyance No. 28/1098

Derivation : Part of 1A-2R-4Ps Gtd to D Lord

Prior CT 4877/14

SCHEDULE 1

M428321 TRANSFER to YIANNIS TELLYROS and VASILIOS KLONIS
Registered 29-Aug-2013 at 12.02 PM

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

B786232 BURDENING EASEMENT: a right of carriageway
(appurtenant to Lot 1 on Plan No. 110411) over the
"Right of Way Variable Width" on Diagram No. 54396
Registered 20-Mar-1995 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

**FOLIO PLAN**

RECORDER OF TITLES

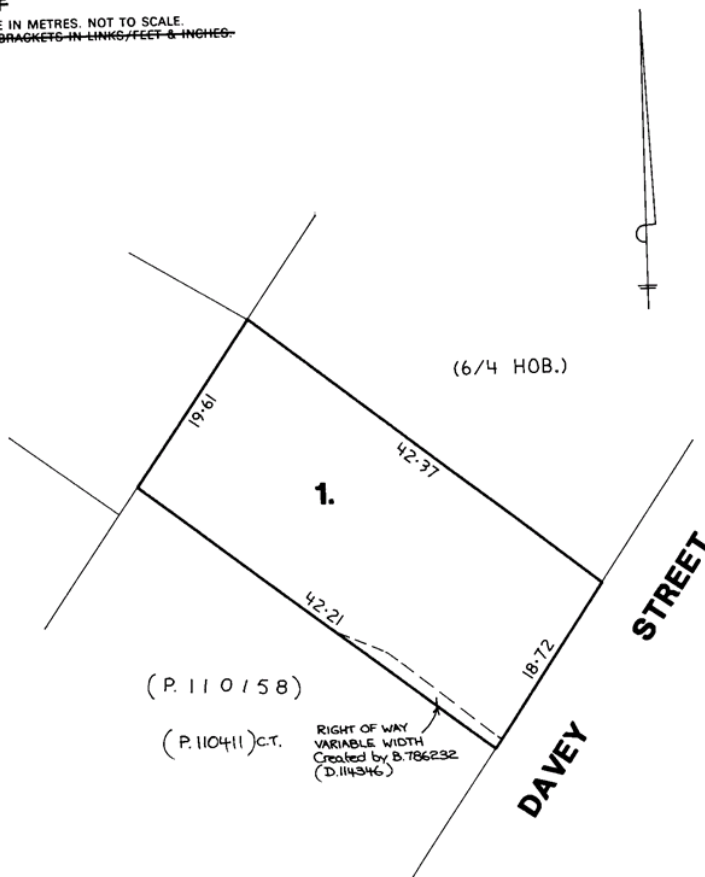
Issued Pursuant to the Land Titles Act 1980



APPROVED <i>[Signature]</i> RECORDER OF TITLES	29 APR 1992	CONVERSION PLAN CONVERTED FROM 28/1098	REGISTERED NUMBER D.54396
FILE NUMBER A.10566	GRANTEE: PART OF 1-2-4 GTD. TO DAVID LORD		DRAWN S.J.G. 15.4.92

SKETCH BY WAY OF ILLUSTRATION ONLY

CITY/TOWN OF **HOBART (SEC.B)**
~~LAND DISTRICT OF~~
~~PARISH OF~~
 LENGTHS ARE IN METRES. NOT TO SCALE.
~~LENGTHS IN BRACKETS IN LINKS/FEET & INCHES.~~



**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



SEARCH OF TORRENS TITLE

VOLUME 110411	FOLIO 1
EDITION 9	DATE OF ISSUE 18-Aug-2014

SEARCH DATE : 03-Jun-2020

SEARCH TIME : 10.22 AM

DESCRIPTION OF LAND

City of HOBART
 Lot 1 on Plan 110411
 Being as to the land marked EXYZF on P110411 the land formerly
 described in Conveyance 60/8687
 Derivation : part of 1a 2r 4ps and part of 34.5ps gtd to D
 Lord & R Officer (respectively) and whole of lot 35610 gtd to
 St Helens Hospital P/L
 Derived from A14739
 Prior CTs 110158/1, 110158/2, 3395/57 and 80982/1

SCHEDULE 1

C453465 TRANSFER to HEALTHSCOPE LIMITED Registered
 09-Jul-2003 at 12.01 PM

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 BENEFITING EASEMENT: Right of Way (appurtenant to the land
 marked ABCDHJK on P110411) over the Right of Way
 shown on P110411
 BENEFITING EASEMENT: Right of Drainage (appurtenant to the
 land marked ABCDEFGHJRLMK on P110411) over the
 Drainage Easement shown on P110411
 SP5246 FENCING PROVISION in Schedule of Easements (relating
 to the land marked DEFGH on P110411)
 B786232 BENEFITING EASEMENT: a right of carriageway over the
 "Right of Way Variable Width" on Diagram No. 54396
 Registered 20-Mar-1995 at noon
 B786233 BENEFITING EASEMENT: a right of carriageway over the
 "Right of Way" on Plan No. 114191
 B786233 BENEFITING EASEMENT: Pipeline Rights over the
 "Pipeline Easement 1.50 wide" and over the "Oxygen
 Storage Compound Easement" on Plan No. 114191
 Registered 20-Mar-1995 at 12.01 PM
 B730150 ADHESION ORDER under Section 477A of the Local
 Government Act 1962 Registered 11-Mar-1994 at noon
 C278107 LEASE to HPH Developments Pty Ltd of a leasehold



RESULT OF SEARCH

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



estate for the term of 20 years from 14-Dec-1999
Registered 27-Mar-2001 at noon
Leasehold Title(s) issued: 110411A/1

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



OWNER		PLAN OF TITLE		REGISTERED NUMBER
FOLIO REFERENCE CT 3395-57 & F/R 80982-1 & 110158-142		LOCATION HOBART		P 110411
GRANTEE		FIRST SURVEY PLAN No. SP.5246, 50530D, P.110158		APPROVED 10 MAR 1994
CAUTION --- COUNCIL APPROVAL REQUIRED FOR FURTHER SUBDIVISION		COMPILED BY L.T.D.		Recorder of Titles
SCALE 1:500		LENGTHS IN METRES		
MAPSHEET MUNICIPAL CODE No. 21	LAST UPI No.	LAST PLAN No.	ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN	

SKETCH BY WAY OF ILLUSTRATION ONLY AS RELATES TO THE LAND MARKED (D.3395-57) DERIVED FROM F/R 110158/142
 COMPILED PLAN AS RELATES TO THE LAND MARKED (A.3395-57 & F.R.80982-1)

Department of State Growth

Salamanca Building Parliament Square
4 Salamanca Place, Hobart TAS
GPO Box 536, Hobart TAS 7001 Australia
Email permits@stategrowth.tas.gov.au Web www.stategrowth.tas.gov.au
Ref: D19/120703



Phil Gartrell
Ireneinc Planning and Urban Design on behalf of Tellyros Klonis Unit Trust
49 Tasma Street
HOBART TAS 7000

Dear Mr Gartrell

Crown Landowner Consent Granted – 63 Davey Street, Hobart

I refer to your recent request for Crown landowner consent relating to the development application at 63 Dave Street, Hobart for the modification of existing crossover and the construction of new crossovers onto Davey Street.

I, Andrew Hargrave, Manager Asset Management, State Roads, the Department of State Growth, having been duly delegated by the Minister under Section 52 (1F) of the *Land Use Planning and Approvals Act 1993* (the Act), and in accordance with the provisions of Section 52 (1B) (b) of the Act, hereby give my consent to the making of the application, insofar as it affects the State road network and any Crown land under the jurisdiction of this Department.

The consent given by this letter is for the **making of the application only** insofar as that it impacts Department of State Growth administered Crown land and is with reference to your application dated 24 May 2019.

In giving consent to lodge the subject development application, the Department notes the following applicable advice:

Other types of works (pipeline, etc.) OR Construction of infrastructure in the road reserve/on Crown land (Works permit required)

In giving consent to lodge the subject development application, the Department notes that the works in the State road network will require the following additional consent:


The consent of the Minister under Section 16 of the *Roads and Jetties Act 1935* to undertake works within the State road reservation.

For further information please visit <http://www.transport.tas.gov.au/road/permits> or contact permits@stategrowth.tas.gov.au.

- 2 -

The Department reserves the right to make a representation to the relevant Council in relation to any aspect of the proposed development relating to its road network and/or property.

Yours sincerely



Andrew Hargrave
MANAGER ASSET MANAGEMENT

Delegate of
Minister for Infrastructure
Jeremy Rockliff MP

18 June 2019

cc: General Manager, Hobart City Council



Enquiries to: Cindy Elder
☎: (03) 6238 2836
✉: coh@hobartcity.com.au
Our Ref: **F19/103826**

31 July 2019

Phil Gartrell
Ireneinc
49 Tasma Street
North Hobart 7001

Via Email: tim@ireneinc.com.au

Dear Mr Gartrell

NOTICE OF LAND OWNER CONSENT TO LODGE A PLANNING APPLICATION

Site Address: **63 Davey Street**

Description of Proposal: **Adjustment of footpath levels at proposed and
reinstated vehicle accesses within footpath Davey
Street Road Reservation**

Phil Gartrell

Applicant Name:

PLN (if applicable): **PLN 19-319**

I write to advise that pursuant to Section 52 of the *Land Use Planning and Approvals Act 1993*, I grant my consent on behalf of the Hobart City Council as the owner/administrator of the above land for you to make application to the City for a planning permit for the development described above and as per the attached documents.

Please note that the granting of the consent is only for the making of the application and in no way should such consent be seen as prejudicing any decision the Council is required to make as the statutory planning authority or as the owner/administrator of the land.

Hobart Town Hall
50 Macquarie Street
Hobart TAS 7000

Hobart Council Centre
16 Elizabeth Street
Hobart TAS 7000

City of Hobart
GPO Box 503
Hobart TAS 7001

T 03 6238 2711
F 03 6234 7109
E coh@hobartcity.com.au
W hobartcity.com.au

CityofHobartOfficial
ABN 39 055 343 428
Hobart City Council

Yours faithfully

A handwritten signature in black ink, appearing to read "N D Heath", written in a cursive style.

(N D Heath)
GENERAL MANAGER

Attachment: Land Owner Consent

City of **HOBART****LAND OWNER CONSENT TO
LODGE A PLANNING APPLICATION**

Site Address: **63 Davey Street**

Description of Proposal: **Adjustment of footpath levels at proposed and reinstated vehicle accesses within footpath Davey Street Road Reservation**

Applicant Name: **Phil Gartrell**

PLN *(if applicable)*: **PLN 19-319**

The land indicated above is owned or is administered by the Hobart City Council.

The applicant proposes to lodge an application for a permit, pursuant to the *Land Use Planning and Approvals Act 1993*, in respect to the proposal described above.

Part or all of the application proposes use and/or development on land owned or administered by the City located on the Davey Street Highway reservation at 63 Davey Street the proposal being to potentially alter the footpath levels at proposed and reinstated vehicle crossovers and changes to on street parking, (as shown on the attached plans).

Being and as General Manager of the Hobart City Council, I provide written permission to the making of the application pursuant to Section 52(1B)(b) of the *Land Use Planning and Approvals Act 1993*.

A handwritten signature in dark ink, appearing to read 'N D Heath'.

(N D Heath)
GENERAL MANAGER

Date: 31/7/19

This consent is for the making of a planning application only, and does not constitute landlord consent for the development to occur.

Attachments/Plans: Aldanmark dwg c1.01, 300% Zoom of Aldan mark C1.01

Application Referral Cultural Heritage - Response

From:	Sarah Waight
Recommendation:	Proposal is unacceptable.
Date Completed:	
Address:	63 DAVEY STREET, HOBART 186 MACQUARIE STREET, HOBART ADJACENT ROAD RESERVE
Proposal:	Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Infrastructure and Access Works
Application No:	PLN-19-319
Assessment Officer:	Cameron Sherriff,

Referral Officer comments:

This proposal is over two sites with the demolition and new work on the property of 63 Davey Street. There is associated infrastructure work on the site of 186 Macquarie Street (St Helens Hospital)

The site of 63 Davey Street is located in the Hobart 1 Heritage Precinct. It is one (1) of three (3) properties that are not heritage listed in this block that has 23 heritage listed places.



Subject site with 61 and 65 Davey Street (aka as 186 Macquarie St) either side (both heritage listed properties). Source: Council image

The proposal is also located in a Place of Archaeological Potential.

This precinct is identified as having heritage significance and the Scheme articulates this with the following statements of significance:

This precinct is significant for reasons including:

1. *It contains some of the most significant groups of early Colonial architecture in Australia*

- with original external detailing, finishes and materials demonstrating a very high degree of integrity, distinctive and outstanding visual and streetscape qualities.*
- 2. The collection of Colonial, and Victorian buildings exemplify the economic boom period of the early to mid nineteenth century.*
- 3. The continuous two and three storey finely detailed buildings contribute to a uniformity of scale and quality of street space.*
- 4. It contains a large number of landmark residential and institutional buildings that are of national importance.*
- 5. The original and/or significant external detailing, finishes and materials demonstrating a high degree of importance.*

Project proposal

The proposal is for the demolition of the existing building, excavation and construction of a building that has four floors on the street frontage. Set back 15 metres, but contiguous to the four floor element is a tower that has an additional seven (7) floors. In total, above and including ground level the building has a total of eleven (11) floors. Below ground is a basement carpark accessed via a car lift.

Works are also proposed as part of new and existing infrastructure connections requiring access to adjacent heritage listed properties. This work is not considered to impact on heritage values and is acceptable.

The proposal is supported by the following documentation:

Statement of Archaeological Potential by Praxis Environment, dated July 2018

Report titled '63 Davey Street Heritage Considerations', by Paul Davies, dated January 2019 (not updated)

Report titled '63 Davey Street Application of Planning Scheme Provisions', by Paul Davies, dated May 2019 (not updated)

Report titled '63 Davey Street, Planning submission to Hobart City Council', by Ireneinc (includes response to the Historic Heritage Code and references the above Heritage Reports by Paul Davies), dated July 2020

Relevant heritage provisions

The project must be assessed against the following clauses:

E13.8.1 P1 - Demolition - Heritage Precinct

E13.8.2 P1 - Buildings and works - Heritage Precinct

E13.10.1 P1 - Building, works and demolition - Development Standards for Places of Archaeological Potential

22.4.1 P5 - Building Height - Development Standards for Buildings and Works - Central Business Zone

22.4.3 A3 - Design - Development Standards for Buildings and Works - Central Business Zone

Representations:

A total of 393 representations were made, 366 against and 27 for, the proposal. The following heritage matters were raised in the representations against the proposal:

- "This proposed development is too tall, too bulky and inappropriate to the area. It will further reduce views. It is not sympathetic to the surrounding buildings or history of the area. There is an excess of student accommodation in Hobart and will be for some time. The building - because of its height and bulk - is not aesthetically pleasing. It's a triumph of greed over need, of profit over design. It should be rejected. If not, it should be limited to 3 floors. If approved it will open up the adjacent site to a high building. That is a poor planning outcome."
- "It is not compatible with the streetscape and damages the views along Davey Street."
- "This proposal is too high and intrusive, given its location within a significant Heritage

precinct, its proximity to a number of heritage listed properties, and its prominent location within a highly significant streetscape."

- "The location of the building is of particular relevance given that it's a prominent site within a Heritage Precinct and within a street of especially high townscape, streetscape, and heritage quality. More acknowledgement needs to be given in the design to the rich cultural heritage associated with the area."
- "Hobart has wonderful heritage buildings and people come to Tasmania to view the wonderful buildings - particularly in the Salamanca, Hobart Waterfront and Davey Street area. ... Sandstone, Georgian and splendid!"
- "The proposed 43 metre building is totally out of scale with its surrounding buildings, which are mainly colonial and heritage listed. This proposal fails to protect and enhance the heritage values of the precinct. This is one of Hobart's most important historic streetscapes – possibly the most important after the Macquarie/Murray/Davey Street precinct. The entire block is free of high rise buildings, and only three sites in the block are not on the Heritage Register. As such, it holds a special place in the hearts of Hobartians and is much admired by tourists, who can no longer experience such marvelous, mostly intact streetscapes as this one in many other cities. The development would completely overshadow the heritage-listed colonial properties up Davey Street. It would be impossible to hide the proposed bulk of the structure from the Davey-Harrington Street intersection. A set-back would be of no value - the bulk of this building would be an imposition on this heritage precinct."
- "This development is way outside this limit and would present a major blight on the entire precinct given the elevated location."
- "This proposal is not only inappropriate in terms of height, bulk and blocking views of kunanyi but is also extremely ugly and has no positive design features."
- "This proposed development will really spoil the city scape of Hobart."
- "Hobart need to remain "Hobart". We do not want to be just another city with an ugly city scape. Keep development within appropriate heights and appearances keeping our current heritage in mind."
- "They .. will ruin the aesthetics of Hobart from so many areas. Also what impact will the build have on the older buildings in the area? So much damage can be caused to them. Surely there's space out of inner Hobart for development to go on, please keep our city sacred."
- "Hobart should refrain from following the tragic path to ugly development. Be unique! Dare to be different like David Walsh. Keep Hobart, it's history and it's unique story strong for the future. There is no place for High Rise Towers anywhere here for any reason."
- "I am thoroughly tired of continual applications for high rise developments that are too high for our streetscape."
- "Hobart's great asset is it's overall form and how it sits in the magnificent scenery of the river and mountain."
- "Please stop this development. It's too tall and spoils the views of the mountain."
- "and somehow Tasmania became this extraordinary repository for 200 years of architecture and aesthetics, wonderfully piled on top of one another. Including a unique "Tasmanian provincial" style seen in the 19thC pubs."
- "It is vital to the future economy of Tasmania that Hobart retain its heritage, its townscape and its streetscape. Without those, the city will lose its appeal to visitors both Australian and international."
- "Hobart's character and biggest asset for residents and tourism is its "human scale". If we lose that we are just another city."
- "A Hobart that is classic, beautiful and in keeping with our history- that is what we need to be aiming for."
- "Yet another proposed high rise building to spoil the skyline of Hobart!"
- "just too much for a site in a Heritage Precinct."
- "Building appears incongruous with scale of existing buildings and streetscape. Also potential for similar scale proposal to occur for Welcome Stranger site."

- "The essence of a past that needs to be preserved for our children and grandchildren."
- "What does this Council not understand about maintaining the heritage of a city? There are unsympathetic buildings in the CBD - and once built they tend to remain. However, we are still so very fortunate in Hobart that not all has been ruined - we still have views across rooftops and minimum shadowing of streets. This will not be the case if developments such as these are permitted to go ahead. Does this Council want to be known as the one that professes to maintain the heritage of our city yet whose actions belie those words? Or does it want to be known as the Council that realised Hobart is one of the few cities in Australia that had streets of heritage?"
- "Please respect the building height recommendation, once this area is ruined then Hobart loses so much of the charm that makes Hobart so special."
- "...once this area is ruined then Hobart loses so much of the charm that makes Hobart so special."
- "It is not compatible with the streetscape and damages the views along Davey Street. This proposal is too high and intrusive, given its location within a significant Heritage Precinct, its proximity to a number of heritage listed properties, and its prominent location within a highly significant streetscape. It fails to reinforce the traditional urban form of the City that steps down from the Macquarie Street ridge to Sullivans Cove. The proposal needs to more appropriately acknowledge its context and to moderate its overall height and urban form accordingly. The location of the building is of particular relevance given that it's a prominent site within a Heritage Precinct and within a street of especially high townscape, streetscape, and heritage quality. More acknowledgement needs to be given in the design to the rich cultural heritage associated with the area. The proposal presents a significant departure from the traditional pattern of development in the area."
- "I do not think the design as presented has any consideration for the surrounding area. I believe any such building should be more sympathetic to the Heritage values so important in our City."
- "This area is predominantly low to medium rise."
- "I do not think the building as designed is fitting for the area. I firmly believe that a new building on this site should be sympathetically designed in more of a "heritage-style" to reflect and respect the surrounding buildings and Davey Street."
- "The scale of the building bears no relationship to any other building on Davey Street and especially on that side of the road, where no building is over 3 or four storeys high. This area of Davey Street holds some historic buildings and a modern tall structure amongst them would detract from the feeling of the area."
- "Hobart wants quality development, development which enhances those buildings which hold our history.."
- "This development will only serve to trash Hobart's heritage status, a unique feature to Hobart which no other capital city in Australia can boast or indeed cash in on."
- "I felt sorry to think that the proposed development would overpower all the 3 and 4 storey buildings in the vicinity. As you drive up the hill, all the current buildings are of a similar scale. I came home to look at the application and was horrified to see that it will 'stick up like a sore thumb' - totally the wrong scale for the area."
- "this building is too large/tall and does not relate at all to buildings in the area."
- "We object to the above application on the basis that it blocks line of sight from St Davids Park to Mount Wellington."
- "The proposal is higher than anything else in the block bounded by Davey, Barrack, Macquarie and Harrington Streets and therefore out of character with the rest of the block. It will be the dominant building from every angle and detract from its surroundings. The diagram on page 45 of document 2441275.pdf shows the proposed development's scale relative to buildings to the northwest, the Ibis, Commonwealth Offices and Travelodge, giving the impression that the proposal is in keeping with existing structures. It's misleading in that those three buildings are not in this block, and from any viewpoint on the surrounding streets this proposal would appear taller than anything around it. The artist's impressions (using photos) of the visual impact on pages

15-19 document 2441277.pdf curiously include proposed additions to the Welcome Stranger Hotel, a separate issue, and has no bearing on this application, and its inclusion gives a false impression."

- "This part of Davey Street must be protected from out-of-scale developments. We have very few areas of Hobart which reflect our earlier history."
- "I strongly oppose this proposed development, its scope, scale and overall footprint will be detrimental to Hobart and its heritage. I strongly support the developer being required to submit a proposal that is aligned with the location, is considerably lower in height and does not destroy heritage, cultural and social values of this area in the City of Hobart."
- "We are steadily losing our skyline and streetscapes and will lose even more of the 'feel' and character that makes Hobart special, liveable and a draw for tourists. Let us be smart in our development - it is those cities around the world that are envied and most visited and , most importantly, good for locals to live in and use."
- "If this proposed building were to be half its height it would still be too tall. It shares many parallels with the empress towers in battery point, including being an insult to our city."
- "This proposal is excessively obtrusive and is not in keeping with the surrounding environment. It sticks out like a sore thumb.."
- "This heavy 'clunky' development proposal represents a form of 'high-rise by stealth'. This area of Hobart is a surviving example of a late nineteenth century building style. It is a lighter, more sophisticated form of residential development which contrasted with New Town and 'downtown Hobart'. Wealthy professionals were building homes for their families with more space around them; fresh air; with views toward the mountain and surrounding wooded countryside. The homes in this area are a significant record of a phase in Hobart's architectural history which was moving on from the 'heavy' Victorian stone buildings to a 'lighter' more decorative style using brick, timber, and stucco. Any contemporary development should complement that lighter style in Hobart's architectural history. There is more to 'heritage architecture' than the heavy 19th century Victorian stone buildings which have hitherto been thought worthy of preservation. We should be thankful that so many residences in this freer, lighter style have survived into the 21st century."
- "Hobart is well loved for its low street scape and should not be changed. All the heritage buildings add to its character and are why I love living here. The modern buildings being built now are ugly and spoil the character of it all."
- "The development as proposed does nothing to enhance the Heritage Precinct and is completely out of scale with the surrounding area."
- "While I am not against development in the CBD, this proposed development is totally inappropriate in regards to the heritage Precinct of Macquarie Street and its dominate position. It will obscure the view of Kunyoni because of its height and is totally not in character off the area and its surrounding streetscape."
- "I strongly feel that this proposed development is not consistent with the recommendations made in the Leigh Woolley report, and will impact strongly on the viewscape of the Davey St, St David's Park area in a way that will markedly degrade its current and historical character."
- "The 63 Davey St site ... is still a relatively intact, historic streetscape (apart from the modern Family Law Court). Council should be doing its utmost to retain this historic streetscape. Planned to be as high as the Welcome Stranger proposal, it is even more prominent due to its position further up Davey Street"
- "The proposed development is clearly double the height of the existing buildings immediately surrounding this site. This is not okay! Think of the long term heritage value of this area."
- "This will be another "eye sore" in our Heritage Precinct."
- "This building is not sensitive to the area in any aspect; height, heritage, streetscape and general context."
- "The proposal is yet another attempt to corrupt the heritage values of a special Heritage Precinct near the heart of the city. For over six decades the community has recognised

the particular qualities of this streetscape. Key buildings were classified by the National Trust back in the '60s, and new infill buildings such as the extensions to St Helen's Hospital were carefully conceived to provide some acknowledgement of the scale and context of the streetscape. This particular proposal flagrantly ignores the values of the Heritage Precinct. ...The planning report includes diagrams which meaninglessly compare the height and form of the proposed building with the taller buildings on the ridge of Macquarie Street, and the former government office building at 188 Collins Street. This type of comparison conveniently ignores the actual location of the proposal in Davey Street, on a site surrounded by heritage buildings and in the middle of an identified Heritage Precinct."

- "Despite the rear wall of our heritage listed building being on the rear boundary and therefore abutting the rear wall of the proposed development, I had not been advised by the proponent of the plans. Given the construction of this building goes to our boundary wall, with engineering works going 2 floors underground and adjacent to this wall, we are surprised that has been no consultation. I assume that, if successful, any remediation or damage to our property will be addressed by the developer."
- "The focus of the design is to accommodate the heritage streetscape to Davey St by use of the podium frontage. The result, however, is that there is a sheer 10 storey building against my heritage building in Macquarie St. This effectively will take away our link to the sky. The proposal fits with the planning building height envelope only by monsterring our building in Macquarie St."
- "The proposal is higher than anything else in the block bounded by Davey, Barrack, Macquarie and Harrington Streets and therefore out of character with the rest of the block. It will be the dominant building from every angle and detract from its surroundings."
- "Davey Street is generally considered to be Australia's longest preserved urban heritage streetscape and townscape of unrivalled heritage value."

Of the 27 representations made in favour of the proposal, the following heritage/design and streetscape related matters were raised:

- "The design appears to be modern yet fits into the area nicely."
- "I strongly believe that they would be sympathetic to the Heritage area and would build a development that would look and feel outstanding for the current environment."
- "It appears to sit comfortably in this location at the gateway to the CBD."
- "The tiered approach undertaken in the application is harmonious to the street landscape and fills a streetscape hole with an attractive and complimentary design while eliminating a less attractive parking area at the front of the street."
- "The stepping back of this development reduces the bulk and perceived size of the building dramatically."

Recent Tribunal decision:

The recent Tribunal decision for the Welcome Stranger Hotel at 58 Harrington Street Hobart (Hexa Pacific Pty Ltd v Hobart City Council and Ors [2020] TASRMPAT 1) provides useful interpretation of the clauses of the Historic Heritage Code relevant to this proposal. The Tribunal decision, upheld Council's decision to refuse that proposal based on non-compliance with clauses 22.4.1 P5 and E13.8.2 P1.

Heritage Assessment:

The objective of clause E13.8.1 Demolition states:

Objective:

To ensure that demolition in whole or in part of buildings or works within a heritage precinct does not result in the loss of historic cultural heritage values unless there are exceptional circumstances.

Clause E13.8.1 P1 states:

Demolition must not result in the loss of any of the following:

- (a) buildings or works that contribute to the historic cultural heritage significance of the precinct;*
- (b) fabric or landscape elements, including plants, trees, fences, paths, outbuildings and other items, that contribute to the historic cultural heritage significance of the precinct; unless all of the following apply;*
 - (i) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;*
 - (ii) there are no prudent or feasible alternatives;*
 - (iii) opportunity is created for a replacement building that will be more complementary to the heritage values of the precinct.*

Response:

The building of 63 Davey Street, is of a scale and siting that results in it being subservient to and sits recessively in this highly important streetscape. However, it dates to 1979 and has a carpark to the front and little architectural merit and it does not make a positive contribution to the stated historic cultural heritage significance of the precinct. In this instance (a) and (b) of E13.8.1 P1 must be satisfied prior to meeting sub-clauses (i), (ii) and (iii). For the reasons outlined above it is concluded that clause E13.8.1 P1 is satisfied.

Clause E13.8.2 P1 states:

Design and siting of buildings and works must not result in detriment to the historic cultural heritage significance of the precinct, as listed in Table E13.2.

Response:

Assessment of this proposal must consider the building as a whole within the Heritage Precinct.

A Heritage Precinct is defined in E13.3.1 Definition of Terms in the Historic Heritage Code as:

"means an area shown on the planning scheme maps as a heritage precinct and described in Table E13.2 as having particular historic cultural heritage significance because of the collective heritage value of individual places as a group for their streetscape or townscape values."

Streetscape is defined in 4.1 of the Scheme as:

"means the visual quality of a street depicted by road width, street planting, characteristics and features, public utilities constructed within the road reserve, the setbacks of buildings and structures from the lot boundaries, the quality, scale, bulk and design of buildings and structures fronting the road reserve."

For the purposes of determining streetscape with respect to a particular site, the above factors are relevant if within 100 m of the site."



Part of the streetscape on Davey Street with the subject site in the centre. Source: Council image

For the purposes of assessing this proposed building against E13.8.2 P1 it is: a tiled/stone square facade element fronting Davey Street, that has three floors, a separate apartment element with a darker horizontal overhanging roof form, set back and in, that forms a street front element of four stories, a higher tower 15 metres back from the street frontage with a 'penthouse' and service structure on top. Overall, the building has a height of approximately 36 metres above the ground level at the street frontage, and a RL of 58.8. The proposal is shown below. The taller 'greyed out' buildings behind (eg Commonwealth Centre and Ibis Hotel) are not relevant in the consideration of clause E13.8.2 P1 as they are outside the Heritage Precinct and not in the streetscape as defined. The tower component of the proposed building is also 'greyed out' but this must not be misconstrued as 'being in the background' or outside the Heritage Precinct and therefore not part of this proposal.



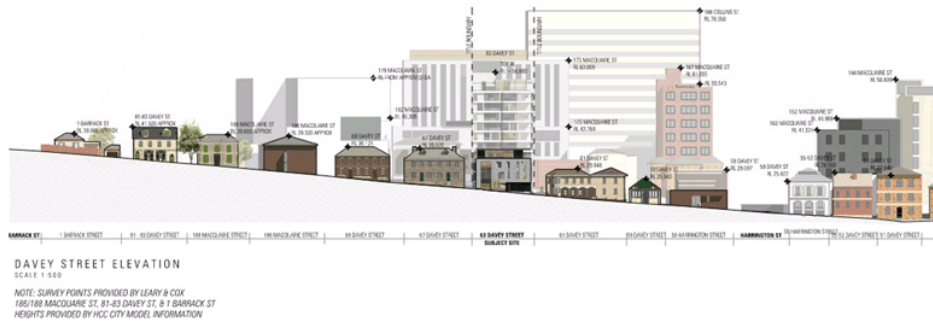


Image: The subject site is in the centre. The 11 storey building behind is shown as 'greyed out' and the buildings outside the Heritage Precinct in Macquarie Street are also shown as 'grey out' which could be misconstrued as an existing building or not part of this proposal. Source: Screenshot from applicant's documentation.

In consideration of clause E13.8.2 P1, detriment means "damage or loss to such value or thing". This is stated in *Hexa Pacific Pty Ltd v Hobart City Council and Ors* [2020] TASRMPAT 1 at [83].

Comparisons with the Welcome Stranger at 58 Harrington Street proposal and subsequent Tribunal decision must be drawn carefully. The sites are near (a heritage listed building separates them) and both are located in the same Heritage Precinct. The Welcome Stranger proposal differed by having two tower components of 10 floors and 13 floors and was located on a corner site. This proposal has a tower component of 11 floors.

However, in relation to that decision and clause E13.8.2 P1, the Tribunal stated "The Proposal is to be located within an area of the Precinct where the streetscape largely comprises buildings that fall within the description set out in Statement 3 for the Heritage Precinct. In the Tribunal's view, the Proposal whilst comprising elements of different heights and setbacks, includes two tower elements which introduce a development scale so at odds in the location with the identified statements of significance (and in particular Statement 3), and would result in the Heritage Precinct as a whole being detrimentally impacted." *Hexa Pacific Pty Ltd v Hobart City Council and Ors* [2020] TASRMPAT 1 at [92].

The current proposal is, based on height measurement, approximately 4.3 metres lower than the Welcome Stranger proposal when the measurement is taken from the street ground level. The subject site is up the hill from the Welcome Stranger site with the subject site dropping down Davey Street approximately 1.6 metres across the street frontage. The RL of the top of the Welcome Stranger was 63.00, while in comparison the RL at the highest point for the current proposal is 58.80.

This proposal differs from the Welcome Stranger proposal in that it is 11 floors high (including the ground floor) and measures approximately 36 metres above the ground level at the street frontage. The following image shows the proposal inserted into the streetscape.



BEFORE: Davy Street



The above image demonstrates an obvious change in the historic streetscape. Source: Applicant's supporting documentation

In summary the proposal is of a scale that is at odds with the streetscape that is within an area of the precinct that largely comprises buildings that fall within the description within the statements of significance of '*continuous two to three storey finely detailed buildings*'. That is,

within the block of Davey, Barrack, Macquarie and Harrington Street the buildings of the precinct are characterised by one, two and three storey buildings around the edge with the maximum height of any building to the rear is 5 storeys. Where the uniformity of streetscape and scale is so central to the heritage values of this block, a building that is taller by the extent proposed cannot enhance the heritage values because it will be out of scale and context with its surroundings. In this respect, there is detriment to an element of a wider precinct and therefore there will be detriment in this case to the precinct values as a whole. The proposal does not satisfy E13.8.2 P1.

The proposal is located in the Central Business Zone and the Development Standards for Buildings and Works 22.4.1 Building Height apply. The proposal does not satisfy 22.4.1 A5 and must therefore be assessed against 22.4.1 P5.

The objective of clause 22.4.1 Building Height is:

That building height:

- (a) contributes positively to the streetscape and townscape;*
- (b) does not unreasonably impact on historic heritage character;*
- (c) does not unreasonably impact on important views within the urban amphitheatre;*
- (d) does not unreasonably impact on residential amenity of land in a residential zone; and*
- (e) provides significant community benefits if outside the Amenity Building Envelope.*

Clause 22.4.1 P5 states:

Building height within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

- (a) not unreasonably dominate existing buildings of cultural heritage significance; and*
- (b) not have a materially adverse impact on the historic cultural heritage significance of the heritage place;*
- (c) for city blocks with frontage to a Solar Penetration Priority Street in Figure 22.2, not exceed the Amenity Building Envelope illustrated in Figure 22.3, unless it can be demonstrated that the overshadowing of the public footpath on the opposite side of the Solar Penetration Priority Street does not unreasonably impact on pedestrian amenity.*

Response:

The heritage listed buildings in Davey Street are shown below. The discussion in relation to 22.4.1 P5 follows.



Adjacent heritage listed building at 61 Davey Street. Source: Council image



Adjacent heritage listed building at 65 Davey Street. Source: Council image

The adjacent heritage listed buildings have the following attributes: simple uncomplicated, well mannered, restrained and modest design, cohesive character and scale, symmetry or regular rhythm, clear horizontal lines, and a fenestration pattern of traditional sash windows of similar proportions. In addition, they have narrow eaves and a simple roof form that has a practical purpose, but also offers an aesthetic function to delineate proportions and define the area between the walls and a pitched roof. Each heritage listed building is also solidly anchored to the ground.

65 Davey Street has two storeys with attic windows, 61 Davey Street has two storeys. This proposal is four storeys. There are no four storey buildings in this section of Davey Street. One

of the characteristics in this block is that the buildings step down Davey Street in an orderly fashion and this can be seen in eaves line of each building and this is demonstrated in the applicant's documentation of the streetscape (see above). Even the recent infill to St Helen's Hospital, respects this pattern, and overall, the listed buildings exhibit a modulated height and rhythm that is rare in Hobart.

While the physical measurement of height difference might be considered minor, the new proposal has design features which contribute to the building having a taller perceived or apparent height, thus leading to it dominating and asserting itself within the existing streetscape.

In summary, the design features that give the proposal a greater perceived and less respectful height in this well mannered streetscape are as follows:

- The proposal is approximately 8.4 metres above the eaves line of the adjacent heritage listed property at 61 Davey Street.
- The proposal has three levels of square, sharp edged and contemporary lines which contrast with the subtle modulated elevations of the adjacent heritage listed buildings to create a more prominent and monolithic form
- The projecting solid eaves of the darker 'mini penthouse' is a contemporary form that is heavier than any roof form of the heritage listed places adding to the height and heaviness of the four storey form.
- The proposal has a deep undercroft at ground level for vehicular and pedestrian access which results in the street façade being elevated above the ground and appearing higher than it actually is.
- The large vertical window configuration over two floors provide a verticality to the three storey portion that gives the building even great height.



BEFORE: Davy Street



AFTER: Davy Street

The above image demonstrates an obvious change in building height in the historic streetscape. Source: Applicant's supporting documentation

In addition, the four storey element including the dark 'mini penthouse' on top will obscure the roof scape including chimneys of adjacent buildings. A close look at the applicant's submitted documentation demonstrates how much taller in the streetscape it will appear.



The above image demonstrates an obvious change in the historic streetscape and shows the real and perceived height of the front four storey element. Source: Applicant's supporting documentation

Not only is it higher than adjacent buildings, but the design of the proposal will result in it appearing even higher and more out of scale and proportion, 'stealing the thunder' of existing heritage listed buildings. It projects further into the streetscape and asserts itself, making its presence felt in all directions. It is sharper and of a form that is more prominent, flamboyant and 'monolithic' than the adjacent polite heritage listed buildings such that it will detract from and be more prominent than the heritage listed buildings. This has an unreasonable impact on the historic heritage character of heritage places such that they are obscured, appear dominated and lesser in scale.

A building that was two or two and half storeys high would be a more appropriate response where the heritage and streetscape values are the most significant in Hobart.

It is concluded that the proposal unreasonably dominates the adjacent buildings by virtue of the height of the lower element that is four storeys high and has a materially adverse impact on the restrained heritage qualities of the adjacent places through its height different design, form, fenestration pattern vertical facade treatment and alternative roof form by upstaging the adjacent buildings. The proposal does not satisfy 22.4.1 P5.

Clause 22.4.1 A3 states:

The facade of buildings constructed within 15m of a frontage and not separated from a place listed in the Historic Heritage Code by another building, full lot (excluding right of ways and lots less than 5m width) or road (refer figure 22.5 i), must:

- (a) include building articulation to avoid a flat facade appearance through evident horizontal and vertical lines achieved by setbacks, fenestration alignment, design elements, or the outward expression of floor levels; and*
- (b) have any proposed awnings the same height from street level as any awnings of the adjacent heritage building.*

Response:

The facade of the buildings on the lower four storey element (ie that part of the proposal within 15 metres of the frontage) does not have a flat facade and has a mix of design elements, fenestration alignments and evident vertical lines and some expression of floor levels as required in (a) of the clause. The adjacent buildings do not have awnings, therefore (b) is not relevant in this case. While the facade of the four storey element remains problematic from a height point of view and as discussed above, it does meet the acceptable solution of 22.4.1 A3.

Clause E13.10.1 states:

Buildings, works and demolition must not unnecessarily impact on archaeological resources at places of archaeological potential, having regard to:

- (a) the nature of the archaeological evidence, either known or predicted;*
- (b) measures proposed to investigate the archaeological evidence to confirm predictive statements of potential;*
- (c) strategies to avoid, minimise and/or control impacts arising from building, works and demolition;*
- (d) where it is demonstrated there is no prudent and feasible alternative to impacts arising from building, works and demolition, measures proposed to realise both the research potential in the archaeological evidence and a meaningful public benefit from any archaeological investigation;*
- (e) measures proposed to preserve significant archaeological evidence 'in situ'.*

Response:

The Praxis Environment report identifies an area of high archaeological potential and this is denoted in an area of red in figure 7.1 (p.48) of the Praxis report. It is acknowledged that the disturbance history may be greater than observations and historical records of the site, however, the report suggests taking a cautious approach and that structural remains associated with the c.1830 dwelling and outbuilding relating to the potential remains of the domestic occupation of the site. The report recommends that "Any excavation proposed in areas of high archaeological potential must be preceded by an archaeological impact assessment, and if necessary an archaeological method statement, which details measures to be taken to avoid or mitigate impact upon the archaeological resource. That method statement must be in accordance with industry standard (e.g. the Tasmanian Heritage Council's Practice Note 2 – Managing Historical Archaeological Significance in the Works Application Process) and implemented in the works process." This can be achieved by a condition of permit and as such the proposal can satisfy E13.10.1 P1.

Summary:

The proposal fails to satisfy E13.8.2 P1 and 22.4.1 P5 of the Historic Heritage Code of the Scheme and is recommended for refusal.

Reasons for refusal:

1. The proposal does not meet the acceptable solution of the performance criterion with respect to clause E13.8.2 P1 of the Historic Heritage Code of the *Hobart Interim Planning Scheme 2015* because the proposal results in detriment to the historic cultural heritage significance of the precinct through its design and siting.
2. The proposal does not meet the acceptable solution of the performance criterion with respect to clause 22.4.1 P5 of the Historic Heritage Code of the *Hobart Interim Planning Scheme 2015* because the proposed building unreasonably dominates and has a materially adverse impact on adjacent existing buildings of cultural heritage significance through its height.

Sarah Waight
Senior Cultural Heritage Officer
1 October 2020

Application Referral Development Engineering - Response

From:	Cam Cecil
Recommendation:	
Date Completed:	
Address:	63 DAVEY STREET, HOBART 186 MACQUARIE STREET, HOBART ADJACENT ROAD RESERVE
Proposal:	Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Infrastructure and Access Works
Application No:	PLN-19-319
Assessment Officer:	Cameron Sherriff,

Referral Officer comments:

SUMMARY:

- The application is for a multi-storey (11 floors including ground level) apartment complex with 51 apartments in total. Floors 1 & 2 (21 apartments) are for visitor accommodation, whilst 3-10 (30 apartments) are for residential use.
- 2 basement levels are proposed which will contain 42 car-parking spaces, 4 motorcycle parking spaces, and a bicycle store. Two car lifts on the ground floor level will be used to gain vehicle access to the basement levels.
- All of the car-parking spaces are to be allocated for residential use.
- Pedestrian sight distance is currently inhibited on the northern side of the access due to the proposed fence height, and a condition limiting fence height will be required for the application to be supported.
- Vehicle sight distance is inhibited by the kerb-side parking spaces, however the straight alignment and gradient of Davey Street assists in this respect.
- Council's Cleansing and Solid Waste Services unit have assessed the proposal and advised they will not undertake collection for the development. The applicant has thus proposed private collection from the road carriageway which has been endorsed by the Department of State Growth (as part of the TIA).

Discretions:

- E6.7.2 Vehicle and pedestrian sight distances
- E6.6.5 Number of parking spaces
- E6.7.13 Facilities for Commercial Vehicles

PLANNING PERMIT INFORMATION:

In a council related engineering context, the proposal can be supported in principal subject to the following conditions and advice:

General Conditions:

ENG 1: Pay Costs

ENG 3A: Access & parking designed and constructed

ENG 3B: Access & parking designed prior to
 ENG 4: Access and parking constructed, sealed and drained prior to use
 ENG 5: Number of spaces
 ENG 8: Parking space user class and signage
 ENG 12: Construction waste management plan
 ENG 13: Waste management plan
 ENG 13: Fence sight line
 ENG sw1: Stormwater drained to council infrastructure
 ENG sw4: New connection design
 ENG sw7: Stormwater detention and treatment
 ENG r1: Structures supporting highway reservation
 ENG r3: Design of road infrastructure
 ENG tr1: Signage and linemarking design
 ENG tr2: Construction traffic management plan

Advice:

Dial before you dig
 Fees and charges
 Building Permit
 Plumbing Permit
 Access
 Redundant Crossovers
 Work within the Highway Reservation
 Road Opening Permit (Occupation of the Public Highway)
 New Stormwater Connection

DETAILED ASSESSMENT:**E5.0 Road and railway access code**

E5.1 Purpose			E5.1.1 The purpose of this provision is to: (a) protect the safety and efficiency of the road and railway networks; and (b) reduce conflicts between sensitive uses and major roads and the rail network.
E5.2 Application of this Code	YES	NO	New vehicle crossing proposed
			This Code applies to use or development of land:
	Yes	No	(a) that will require a new vehicle crossing, junction or level crossing; or
	Yes	No	(b) that intensifies the use of an existing access; or
	Yes	No	(c) that involves a sensitive use, a building, works or subdivision within 50m metres of a Utilities zone that is part of:
	Yes	No	(i) a rail network;
	Yes	No	(ii) a category 1 - Trunk Road or a category 2 - Regional Freight Road, that is subject to a speed limit of more than 60km/h kilometres per hour.
Clause for Assessment			Comments / Discussion (in bold)

Clause 5.5.1 Existing road accesses and junctions NOT APPLICABLE		<p><u>Documentation submitted to date appears not to invoke clause E5.5.1.</u></p> <p>No intensification of existing road accesses and/or junctions proposed.</p>
Clause 5.5.2 Existing level crossings NOT APPLICABLE		<p><u>Documentation submitted to date appears not to invoke clause E5.5.2.</u></p> <p>No intensification of an existing level crossings proposed.</p>
Clause 5.6.1 development adjacent to roads and railways NOT APPLICABLE		<p><u>Documentation submitted to date appears not to invoke clause E5.6.1.</u></p> <p>No development adjacent to category 1 or category 2 road proposed.</p>
Clause 5.6.2 road accesses and junctions ACCEPTABLE SOLUTION		<p>The road and access junctions must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <u>Documentation submitted meets the Acceptable Solution for clause E5.6.2 A2</u></p> <p>Acceptable solution - A1 No new access or junction to roads in an area subject to a speed limit of more than 60km/h. - N/A</p> <p>Acceptable solution - A2 SATISFIED No more than one access providing both entry and exit, or two accesses providing separate entry and exit, to roads in an area subject to a speed limit of 60km/h or less.</p> <ul style="list-style-type: none"> • There will be only one access providing both entry and exit. • The section of existing layback (outside of the ROW servicing 186 Macquarie) that comprised part of the double cross-over shared with 186 Macquarie Street is being removed making the former access to the site non-serviceable. • A new double crossover is proposed adjacent to the North-Eastern boundary
Clause 5.6.3 new level crossings NOT APPLICABLE		<p><u>Documentation submitted to date appears not to invoke clause E5.6.3.</u></p> <p>No new level crossings proposed.</p>

Clause 5.6.4 sight distance at accesses and junctions			<p>The SISD values in HIPS are excessive and do not accord with the recommendations of Austroads or AS 2890.1. The sight distance has therefore been assessed under HIPS E6.7.2 (ie. AS 2890.1/Austroads)</p> <p>The sight distance at accesses and junctions must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p><u>Documentation submitted meets the Acceptable Solution for clause E5.6.4.</u></p> <p><u>Acceptable solution - A1:</u> Sight distances at: (a) an access or junction must comply with the Safe Intersection Sight Distance shown in Table E5.1; and (b) rail level crossings must comply with AS1742.7 Manual of uniform traffic control devices - Railway crossings, Standards Association of Australia.</p>
NOT APPLICABLE			

E 6.0 Parking and Access Code

E6.1 Purpose			E6.1.1
			The purpose of this provision is to:
	Yes	N/A	(a) ensure safe and efficient access to the road network for all users, including drivers, passengers, pedestrians and cyclists;
	Yes	N/A	(b) ensure enough parking is provided for a use or development to meet the reasonable requirements of users, including people with disabilities;
	Yes	N/A	(c) ensure sufficient parking is provided on site to minimise on-street parking and maximise the efficiency of the road network;
	Yes	N/A	(d) ensure parking areas are designed and located in conformity with recognised standards to enable safe, easy and efficient use and contribute to the creation of vibrant and liveable places;
	Yes	N/A	(e) ensure access and parking areas are designed and located to be safe for users by minimising the potential for conflicts involving pedestrians, cyclists and vehicles; and by reducing opportunities for crime or anti-social behaviour;
	Yes	N/A	(f) ensure that vehicle access and parking areas do not adversely impact on amenity, site characteristics or hazards;
	Yes	N/A	(g) recognise the complementary use and benefit of public transport and non-motorised modes of transport such as bicycles and walking;
	Yes	N/A	(h) provide for safe servicing of use or development by commercial vehicles.

E6.2 Application of this Code	YES	—	This code applies to all use and development.
Clause for Assessment			Comments / Discussion (in bold)
Clauses 6.6.5 Number of Car Parking Spaces - CBD			<p>The parking number assessment must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p>Documentation submitted to date does not satisfy the Acceptable Solution for clause E6.6.5 and as such, shall be assessed under Performance Criteria.</p> <p>Acceptable solution - A1: NOT SATISFIED</p> <p>(a) No on-site parking is provided; NOT MET</p> <p>(b) on-site parking is provided at a maximum rate of 1 space per 200m² of gross floor area for commercial uses; N/A</p> <p>(c) on-site parking is provided at a maximum rate of 1 space per dwelling for residential uses; NOT MET</p> <p>(d) on-site parking is required operationally for an essential public service, including, hospital, police or other emergency service. N/A</p> <ul style="list-style-type: none"> • The proposal includes 42 car parking spaces (2x DDA compliant) for 30 residential apartments and 21 visitor accommodation apartments (51 total). The TIA states that all car parking spaces will be for the residential users. • The AS requires 30 spaces for the residential apartments. • 42 spaces exceeds the AS of 30 spaces <p>Performance Criteria - P1: SATISFIED</p> <p>Car parking provision:</p> <p>(a) is in the form of a public car parking station provided as part of a development which utilises a major existing access; or - NA</p> <p>(b) must not compromise any of the following:</p> <p>(i) <i>pedestrian safety, amenity or convenience</i></p> <ul style="list-style-type: none"> • The proposed singular access for 42 car-parking spaces causes a concentration of vehicle movements across the footpath. • The sight distance to pedestrians does not comply with the Australian Standard due to the height of the proposed boundary fence exceeding 1.2m. • Pedestrian safety and convenient use of the footpath will therefore be compromised. CONDITION FOR MAXIMUM BOUNDARY FENCE HEIGHT <p>(ii) <i>the enjoyment of 'al fresco' dining or other outdoor</i></p>

		<p>activity</p> <p>None near the proposed development</p> <p>(iii) <i>air quality and environmental</i></p> <p>This is not compromised (beyond what is typically accepted for a multi-storey apartment complex)</p> <p>(iv) <i>traffic safety.</i></p> <ul style="list-style-type: none"> • There are kerb-side parking spaces adjacent to the access that inhibit the sight distance to vehicles on Davey Street. • Davey Street is heavily trafficked, particularly during peak hours. • The gradient of Davey Street is in a favourable direction and improves the sight distance. • Traffic safety is not compromised beyond what is typical for an access servicing a multi-storey apartment complex. • The design has been assessed by a consulting traffic engineer and has been found to be acceptable (refer TIA).
<p>Clause 6.7.1 number of vehicle accesses</p> <p>ACCEPTABLE SOLUTION</p>		<p>The parking number assessment must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p><u>Documentation submitted meets the Acceptable Solution for clause E6.7.1</u></p> <p>Acceptable solution A1: SATISFIED</p> <p><i>The number of vehicle access points provided for each road frontage must be no more than 1 or the existing number of vehicle access points, whichever is the greater.</i></p> <p>The existing southern access (that forms part of a double crossover also servicing 186 Macquarie) is being removed, and a new access is proposed on the northern side - there will be only one access.</p> <p>Acceptable solution A2: SATISFIED</p> <p><i>In the Central Business Zone and Particular Purpose Zone 10 (Royal Hobart Hospital) no new vehicular access is provided unless an existing access point is removed.</i></p> <p>An access is being removed (per the above)</p>
<p>Clause 6.7.2 design vehicle access</p> <p>PERFORMANCE CRITERIA</p>		<p>The design of the vehicle access must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p><u>Documentation submitted to date does not satisfy the Acceptable Solution for clause E6.7.2 and as such, shall be assessed under Performance Criteria.</u></p>

Acceptable Solution - A1: **NOT SATISFIED**

Design of vehicle access points must comply with all of the following:

(a) in the case of non-commercial vehicle access; the location, sight distance, width and gradient of an access must be designed and constructed to comply with section 3 – “Access Facilities to Off-street Parking Areas and Queuing Areas” of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking

NOT MET

The sight distance does not comply with AS2890.1

Performance Criteria - P1: **SATISFIED**

Design of vehicle access points must be safe, efficient and convenient, having regard to all of the following:

(a) avoidance of conflicts between users including vehicles, cyclists and pedestrians;

Vehicles and Cyclists

- There are kerb-side parking spaces adjacent to the access that inhibit the sight distance to vehicles on Davey Street.
- Davey Street is heavily trafficked, particularly during peak hours.
- The gradient of Davey Street is in a favourable direction and improves the sight distance.
- Traffic safety is not compromised beyond what is typical for an access servicing a multi-storey apartment complex.
- The design has been assessed by a consulting traffic engineer and has been found to be acceptable (refer TIA).

Pedestrians

- The proposed singular access for 42 car-parking spaces causes a concentration of vehicle movements across the footpath.
- The sight distance to pedestrians does not comply with the Australian Standard due to the height of the proposed boundary fence exceeding 1.2m.
- Pedestrian safety and convenient use of the footpath will therefore be compromised. **CONDITION FOR MAXIMUM BOUNDARY FENCE HEIGHT**

(b) avoidance of unreasonable interference with the flow of traffic on adjoining roads;

- Assessed by a consulting traffic engineer and found to be acceptable (refer TIA).

(c) suitability for the type and volume of traffic likely to be generated by the use or development;

- The width and gradient of the access is acceptable for servicing the 42 parking spaces proposed.

		<ul style="list-style-type: none"> Assessed by a consulting traffic engineer and found to be acceptable (refer TIA). (d) ease of accessibility and recognition for users. The location of the access will permit easy use The access is consistent with surrounding properties and as such ease of recognition is acceptable
Clause 6.7.3 vehicle passing ACCEPTABLE SOLUTION		<p>Vehicle passing must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <u>Documentation submitted to date appears to be able to satisfy the Acceptable Solution for clause E6.7.3.</u></p> <p>Acceptable solution - A1: - SATISFIED</p> <p>Vehicular passing areas must:</p> <p>(a) be provided if any of the following applies to an access:</p> <p>(i) it serves more than 5 car parking spaces; 42 spaces</p> <p>(ii) is more than 30 m long; 45m</p> <p>(iii) it meets a road serving more than 6000 vehicles per day; Yes</p> <p>(b) be 6 m long, 5.5 m wide, and taper to the width of the driveway;</p> <p>(c) have the first passing area constructed at the kerb;</p> <p>(d) be at intervals of no more than 30 m along the access.</p> <p>The width of the access and internal driveway is sufficient to permit vehicle passing (6m)</p>
Clause 6.7.4 on site turning ACCEPTABLE SOLUTION		<p>On-site turning must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <u>Documentation submitted to date appears to satisfy the Acceptable Solution for clause E6.7.4.</u></p> <p>Acceptable solution - A1: SATISFIED</p> <p>On-site turning must be provided to enable vehicles to exit a site in a forward direction, except where the access complies with any of the following:</p> <p>(a) it serves no more than two dwelling units;</p> <p>(b) it meets a road carrying less than 6000 vehicles per day.</p> <p>Vehicles will be able to exit the site in a forwards direction</p>

<p>Clause 6.7.5 layout of parking area</p> <p>ACCEPTABLE SOLUTION</p>		<p>The layout of the parking area must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p>Documentation submitted to date appears to satisfy the Acceptable Solution for clause 6.7.5.</p> <p>Acceptable Solution A1: SATISFIED</p> <p>The layout of car parking spaces, access aisles, circulation roadways and ramps must be designed and constructed to comply with section 2 "Design of Parking Modules, Circulation Roadways and Ramps" of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking and must have sufficient headroom to comply with clause 5.3 "Headroom" of the same Standard.</p> <p><i>Car Parking Space Dimensions (AS2890.1 Fig 2.2 = 2.4x5.4m Class 1A)</i></p> <p>6 parking spaces for small vehicles have been proposed. These can be accepted under the Australian Standard providing they are appropriately marked CONDITION FOR SIGNAGE AND LINEMARKING</p> <p><i>Car Parking Space Design Envelope (AS2890.1 Fig 5.2 300mm clearance on side)</i></p> <p>OK</p> <p><i>Headroom: (AS2890.1 Fig 5.3 = 2.2m clearance)</i></p> <p>OK</p> <p><i>Parking Space Gradient (5%)</i></p> <p>OK</p> <p><i>Aisle Width (AS2890.1 Fig 2.2 = 5.8m Class 1A)</i></p> <p>OK</p> <p><i>Garage Door Width & Apron (AS2890.1 Fig 5.4 = 2.4m wide => 7m wide apron)</i></p> <p>N/A</p> <p><i>Parking Module Gradient (manoeuvring area 5% Acceptable Soln, 10% Performance)</i></p> <p>OK</p> <p><i>Driveway Gradient & Width (AS2890.1 Section 2.6 = 25% and 3m)</i></p> <p>OK</p> <p><i>Transitions (AS2890.1 Section 2.5.3 = 12.5% summit, 15% sag => 2m transition)</i></p> <p>OK</p> <p><i>Vehicular Barriers (AS2890.1 Section 2.4.5.3 = 600mm drop, 1:4 slope)</i></p> <p>N/A</p> <p><i>Blind Aisle End Widening (AS2890.1 Fig 2.3 = 1m extra)</i></p> <p>OK</p>
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<p>Clause 6.7.6 surface treatment</p> <p>ACCEPTABLE SOLUTION</p>			<p>The surface treatment must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <u>Documentation submitted to date does satisfy the Acceptable Solution for clause E6.7.6.</u></p> <p>Acceptable Solution - A1: SATISFIED Parking spaces and vehicle circulation roadways must be in accordance with all of the following; (a) paved or treated with a durable all-weather pavement where within 75m of a property boundary or a sealed roadway; (b) drained to an approved stormwater system, unless the road from which access is provided to the property is unsealed.</p> <p>Submitted plans indicate a concrete surface treatment drained to an approved stormwater system</p>
<p>Clause 6.7.7 Lighting of parking area Planner and health unit to assess</p>	—	—	Planner to assess
<p>Clause 6.7.8 Landscaping Planner to assess</p>	—	—	Planner to assess
<p>Clause 6.7.9 motor bike parking</p> <p>ACCEPTABLE SOLUTION</p>			<p>The motor bike parking must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <u>Documentation submitted to date does satisfy the Acceptable Solution for clause E6.7.9.</u></p> <p>Acceptable Solution A1: - SATISFIED The design of motorcycle parking areas must comply with all of the following: (a) be located, designed and constructed to comply with section 2.4.7 "Provision for Motorcycles" of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking; (b) be located within 30 m of the main entrance to the building.</p> <ul style="list-style-type: none"> Four motorcycle parking spaces have been proposed

<p>Clause 6.7.10 bicycle parking</p> <p>ACCEPTABLE SOLUTION</p>		<p>The bicycle parking must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <u>Documentation submitted to date does satisfy the Acceptable Solution for clause E6.7.10.</u></p> <p>Acceptable Solution A1: SATISFIED The number of on-site bicycle parking spaces provided must be no less than the number specified in Table E6.2.</p> <p>Acceptable Solution A2: SATISFIED The design of bicycle parking spaces must be to the class specified in table 1.1 of AS2890.3-1993 Parking facilities Part 3: Bicycle parking facilities in compliance with section 2 "Design of Parking Facilities" and clauses 3.1 "Security" and 3.3 "Ease of Use" of the same Standard.</p> <p><i>Table E6.2 sets out the number of bicycle parking spaces required. The requirement for spaces for a use or development listed in the first column of the table is set out in the second and forth columns of the table with the corresponding class set out in the third and fifth columns. If the result is not a whole number, the required number of (spaces) is the nearest whole number. If the fraction is one-half, the requirement is the next whole number.</i></p> <p>User Class: Visitor Accommodation</p> <p>Visitor Accommodation = 1 for each 40 accommodation rooms (Employee/resident bicycle parking requirement) and 1 for each 30 accommodation rooms (Visitor/customer/student bicycle parking requirement)</p> <ul style="list-style-type: none"> 2 spaces are required and a bicycle store with sufficient room has been proposed
<p>Clause 6.7.11 bicycle end trip</p> <p>Planner to assess</p>	—	Planner to assess
<p>Clause 6.7.12 siting of car parking</p> <p>Planner to assess based on DE assessment of Clause 6.7.5 layout of parking area</p>	—	Planner to assess

<p>Clause 6.7.13 facilities for commercial vehicles</p> <p>PERFORMANCE CRITERIA</p>		<p>The facilities for commercial vehicles must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p><u>Documentation submitted to date does not satisfy the Acceptable Solution for clause E6.7.13 and as such, shall be assessed under Performance Criteria.</u></p> <p>Acceptable Solution A1: NOT SATISFIED</p> <p>Commercial vehicle facilities for loading, unloading or manoeuvring must be provided on-site in accordance with Australian Standard for Off-street Parking, Part 2 : Commercial. Vehicle Facilities AS 2890.2:2002, unless:</p> <p><i>(a) the delivery of all inward bound goods is by a single person from a vehicle parked in a dedicated loading zone within 50 m of the site;</i></p> <p>There is no loading zone within 50m</p> <p><i>(b) the use is not primarily dependent on outward delivery of goods from the site.</i></p> <p>N/A</p> <p>Performance Criteria - P1: SATISFIED</p> <p>Commercial vehicle arrangements for loading, unloading or manoeuvring must not compromise the safety and convenience of vehicular traffic, cyclists, pedestrians and other road users.</p> <ul style="list-style-type: none"> • The traffic engineering consultant has advised that private collection will be undertaken from the kerb-side, and that the associated risk and interruption to convenience is tolerable • The Department of State Growth has endorsed the TIA with proposed private collection from the carriageway <p>On this basis, the proposed commercial vehicle arrangements (ie. waste collection) can be supported. CONDITION FOR WASTE MANAGEMENT PLAN</p>
<p>Clause 6.7.14 access to a road</p> <p>ACCEPTABLE SOLUTION</p>		<p>The access to a road must satisfy the Acceptable Solutions of the Hobart Interim Planning Scheme 2015 (HIPS 2015).</p> <p><u>Documentation submitted to date does appear to satisfy the Acceptable Solution for clause E6.7.14.</u></p> <p>Acceptable Solution A1:</p> <p>Access to a road must be in accordance with the requirements of the road authority. - SATISFIED</p>

Clause 6.7.15 access to Niree Lane NOT APPLICABLE			The access to Niree Lane must satisfy either Acceptable Solutions or Performance Criteria for each clause of the Hobart Interim Planning Scheme 2015 (HIPS 2015). <u>Documentation submitted to date appears not to invoke clause E6.7.15.</u> No development proposed within Niree Lane.

URBAN DESIGN ADVISORY PANEL

MINUTES

MINUTES OF A MEETING OF THE URBAN DESIGN ADVISORY PANEL
HELD AT 11:00 AM ON THURSDAY 27 AUGUST 2020
RIVERVIEW ROOM AND VIA TEAMS

PLN-19-319 63 DAVEY STREET AND 186 MACQUARIE STREET, HOBART

Description:

Demolition, New Building for 30 Multiple Dwellings and 21 Student Accommodation Units including Carparking, and Associated Works.

This planning application proposes the redevelopment of the site at 63 Davey Street, Hobart, involving the demolition of the existing single storey building at 63 Davey Street, including removal of the existing eight parking spaces in the forecourt immediately off Davey Street. The new works are to facilitate use and development for 51 apartments providing a mix of 30 residential and 21 student accommodation apartments.

Comments:

The proposed building is located in the Central Business Zone and significantly, is in a Heritage Precinct.

The Panel noted the changes made to the design (especially in regard to overall height) since the proposal was last presented to the Panel as a pre-application in January 2019.

The Panel supports the Davey Street podium elevation and finds the massing, materials and height of this part of the proposed building to be well considered and a positive contribution to the streetscape.

The Panel does however consider that at ground level the extent of landscaping could be more substantial and that the use of quality materials (especially paving) must be extended to the full frontage of the site including the driveway and service areas.

**URBAN DESIGN ADVISORY PANEL
MINUTES
27/08/2020**

On the question of landscaping generally, there remains a lack of detail and any approval should include appropriate conditions regarding the engagement of a landscape architect and the submission of detailed landscaping plans for approval.

The principal concern of the Panel remains the overall height of the proposal within the context of the values of the Heritage Precinct within which the proposal is located.

The height of the tower elements will cause the proposal to be prominent in the townscape and streetscape, adversely impacting the qualities of the Heritage Precinct, especially the Davey Street streetscape and St Davids Park. These concerns in the opinion of the Panel can only be mitigated with a further very significant reduction in height.

In particular the Panel considers that 22.4.1 objective (b) and P1.1 (a) of the City of Hobart Planning Scheme have not been met. Similarly E13.8.2 P1 has not, in the opinion of the Panel, been met.

13 19 Allison Street, West Hobart - Partial Demolition and Alterations
File Ref: F20/116006

Memorandum of the Manager Development Appraisal of 30 October 2020 and attachment.

Delegation: Committee



City of **HOBART**

MEMORANDUM: CITY PLANNING COMMITTEE

19 Allison Street, West Hobart - Partial Demolition and Alterations

At its meeting on 19 October 2020, the Council's City Planning Committee resolved to defer item 7.1.7 19 Allison Street, West Hobart – Partial Demolition and Alterations. The Committee's resolution was as follows:

That the matter be deferred for two weeks to enable conciliation between the applicant and the neighbours of 17 Allison Street, West Hobart.

A meeting was held between the applicants and the three representors at the Council offices, moderated by the Manager Development Appraisal. This provided an opportunity for the representors to raise their concerns in detail and make proposals about how their concerns would be addressed.

Subsequent to that meeting, the applicants proposed some design alterations to the deck and screening, in an attempt to meet the concerns of the representors. As a result of the discussions, one representor has indicated that she no longer maintains her objection to the proposed works. Another representor has indicated that the proposed alterations were not sufficient to address her concerns. The other representor did not agree to the changes which were proposed and proposed further changes, which were not acceptable to the applicants.

The current proposal by the applicants is for the proposed screening condition to be amended to the following, with an additional advice clause requiring further vegetation:

In addition to the screening shown on the plans which form part of this application, screening must be installed prior to the first use of the deck and maintained, as follows:

- (a) on the side of the deck adjacent to 17 Allison Street;**
- (b) to a height above deck surface level of 1.7m;**
- (c) extending 2m from the point where the deck joins the house; and**
- (d) constructed of white aluminium slats.**

Reason for condition

To provide reasonable opportunity for privacy for the dwelling at 17 Allison Street.

Advice

The owner is encouraged to use pot plants and other vegetation along the full width of the side of the deck adjacent to 17 Allison Street to provide additional privacy.

The applicants have also proposed that the deck is reduced in width by 0.5m to 4m.

PLN 2

The deck must be reduced to a maximum depth (measurement from the house to the front edge) of 4.0m.

Reason for condition

To reduce the visual impacts of the development.

These conditions are supported by the planner who assessed the application and recommended approval. The proposed changes do not make this a “substantially different” application and can be imposed through conditions.

The revised recommendation with this new condition is as follows:

RECOMMENDATION

Pursuant to the *Hobart Interim Planning Scheme 2015*, the City Planning Committee, in accordance with the delegations contained in its terms of reference, approve the application for Partial Demolition and Alterations at 19 Allison Street, WEST HOBART for the reasons outlined in the officer’s report and a permit containing the following conditions be issued:

GEN

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-20-550 - 19 ALLISON STREET, WEST HOBART TAS 7000 - Final Planning Documents except where modified below.

Reason for condition

To clarify the scope of the permit.

PLN 1

In addition to the screening shown on the plans which form part of this application, screening must be installed prior to the first use of the deck and maintained, as follows:

- (a) on the side of the deck adjacent to 17 Allison Street;
- (b) to a height above deck surface level of 1.7m;
- (c) extending 2m from the point where the deck joins the house; and
- (d) constructed of white aluminium slats.

Reason for condition

To provide reasonable opportunity for privacy for the dwelling at 17 Allison Street.

PLN 2

The deck must be reduced to a maximum depth (measurement from the house to the front edge) of 4.0m.

Reason for condition

To reduce the visual impacts of the development.

ADVICE

The following advice is provided to you to assist in the implementation of the planning permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's [website](#) for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

BUILDING PERMIT

You may need building approval in accordance with the *Building Act 2016*. Click [here](#) for more information.

This is a Discretionary Planning Permit issued in accordance with section 57 of the *Land Use Planning and Approvals Act 1993*.

WASTE DISPOSAL

It is recommended that the developer liaise with the Council's Cleansing and Solid Waste Unit regarding reducing, reusing and recycling materials associated with demolition on the site to minimise solid waste being directed to landfill.

Further information regarding waste disposal can also be found on the Council's [website](#).

PRIVACY

The owner is encouraged to use pot plants and other vegetation along the full width of the side of the deck adjacent to 17 Allison Street to provide additional privacy.

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.



Karen Abey
**MANAGER DEVELOPMENT
APPRAISAL**

Date: 30 October 2020
File Reference: F20/116006

Attachment A: PLN-20-550 - 19 ALLISON STREET WEST HOBART TAS
7000 - Planning Committee or Delegated Report with
attachments ↓ 

**APPLICATION UNDER HOBART INTERIM PLANNING SCHEME 2015**

Type of Report: Committee
Committee: 19 October 2020
Expiry Date: 20 November 2020
Application No: PLN-20-550
Address: 19 ALLISON STREET , WEST HOBART
Applicant: KRISTY LITTLE
71 NELSON ROAD
Proposal: Partial Demolition and Alterations
Representations: Three (3)
Performance criteria: General Residential Zone Development Standards; Historic Heritage Code

1. Executive Summary

1.1 Planning approval is sought for Partial Demolition and Alterations, at 19 Allison Street West Hobart.

1.2 More specifically the proposal includes:

- Demolition of an existing elevated deck attached to the rear of the dwelling.
- A new, larger deck (6.85m wide by 4.5m deep) extending from the rear of the dwelling at the same level with a finished floor level of 2.7m above ground level. The existing and proposed decks correspond with the primary floor level of the dwelling, which at the rear is equivalent to first floor level.
- The proposed deck maintains the existing side boundary setbacks of the dwelling at 1.3m and 1.9m.
- The deck includes a 1.7m high aluminium horizontal slat screen along its south-western side. The remaining sides of the deck are surrounded by a 1m high glass balustrade.
- Composite wood decking is proposed.

1.3 The proposal relies on performance criteria to satisfy the following standards and codes:

- 1.3.1 General Residential Zone Development Standards - Building Envelope; Privacy

- 1.3.2 Historic Heritage Code - Demolition and Buildings/Works in a Heritage Precinct
- 1.4 Three (3) representations objecting to the proposal were received within the statutory advertising period between 07/09 and 21/09/2020.
- 1.5 The proposal is recommended for approval subject to conditions.
- 1.6 The final decision is delegated to the City Planning Committee, because more than two and less than five objections to the application have been received, and the officer recommendation is for approval.

2. Site Detail



Image 1: Aerial view of the subject property and surrounds.

- 2.1 19 Allison Street, West Hobart is a 600m² residential property occupied by a single weatherboard dwelling located to the front of the lot. The site exhibits a degree of cross slope both from left down to right and also from front down to rear, meaning that what appears as a single storey dwelling at the front becomes a two storey dwelling at the rear. The property is surrounded by residential properties with similar characteristics and falls inside the West Hobart 3 Heritage Precinct.

3. Proposal

- 3.1 Planning approval is sought for Partial Demolition and Alterations, at 19 Allison Street West Hobart.

3.2 More specifically the proposal is for:

- Demolition of an existing elevated deck attached to the rear of the dwelling.
- A new, larger deck (6.85m wide by 4.5m deep) extending from the rear of the dwelling at the same level with a finished floor level of 2.7m above ground level.
- The existing and proposed decks correspond with the primary floor level of the dwelling, which at the rear is equivalent to first floor level.
- The proposed deck maintains the existing side boundary setbacks of the dwelling at 1.3m and 1.9m.
- The deck includes a 1.7m high aluminium horizontal slat screen along its south-western side. The remaining sides of the deck are surrounded by a 1m high glass balustrade.
- Composite wood decking is proposed.

4. Background

4.1 None relevant.

5. Concerns raised by representors

- 5.1 Three (3) representations objecting to the proposal were received within the statutory advertising period between 07/09 and 21/09/2020.
- 5.2 The following table outlines the concerns raised in the representations received. Those concerns which relate to a discretion invoked by the proposal are addressed in Section 6 of this report.

Concerns about impact upon light and sunlight capture.
Concerns about impacts upon views and outlook.
Concerns about non-compliance with scheme standards - setbacks, height and privacy. The deck has not been designed to minimise the impacts caused.
Concerns about the visual impact caused by the proposed deck.
Concerns about privacy impacts from overlooking from the deck into habitable rooms and private open spaces.
Concerns that any screen planting required by Council will impact solar access to adjacent properties.
Concerns about screening and the overall proposal not being considerate of heritage values.
Concerns about impact upon current levels of residential amenity through the introduction of a larger deck and the potential for its unsociable use.
Concerns about the non-residential use of the property for visitor accommodation without approval.

6. Assessment

- 6.1 The *Hobart Interim Planning Scheme 2015* is a performance based planning scheme. To meet an applicable standard, a proposal must demonstrate compliance with either an acceptable solution or a performance criterion. Where a proposal complies with a standard by relying on one or more performance criteria, the Council may approve or refuse the proposal on that basis. The ability to approve or refuse the proposal relates only to the performance criteria relied on.
- 6.2 The site is located within the General Residential Zone of the *Hobart Interim Planning Scheme 2015*.
- 6.3 The existing use is Residential (single dwelling). The proposal maintains this use. A single dwelling is a No Permit Required Use in the Zone.
- 6.4 The proposal has been assessed against:
- 6.4.1 Part D - 10 General Residential Zone
 - 6.4.2 E13.0 Historic Heritage Code
- 6.5 The proposal relies on the following performance criteria to comply with the applicable standards:

6.5.1 General Residential Zone Development Standards:-

*Setbacks and Building Envelope – Part D 10.4.2 P3**Privacy – Part D 10.4.6 P1*

6.5.2 Historic Heritage Code:-

*Demolition - Heritage Precinct - Part E 13.8.1 P1**Buildings and Works - Heritage Precinct - Part E 13.8.2 P1; P3*

6.6 Each performance criterion is assessed below.

6.7 Setbacks and Building Envelope – Part D 10.4.2 P3

6.7.1 The acceptable solution A3 at clause Part D 10.4.2 requires development to be contained within the shape produced by the acceptable building envelope as it responds to the characteristics of the site, along with development that is within 1.5m of a side boundary being a total length of 9m or one third the length of the boundary line, whichever is the lesser.

6.7.2 The proposal includes all parts of the proposed deck and screen contained within the shape of the envelope as it applies to the site. The proposed deck however maintains the existing 1.3m side setback of the dwelling and this setback occurs for the full 21m length of this side of the dwelling. The additional 4.5m of deck brings the total length within 1.5m to 25.5m, equating to 42% of the length of the corresponding 61m side boundary line.

6.7.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.

6.7.4 The performance criterion P1 at clause Part D 10.4.2 provides as follows:

The siting and scale of a dwelling must:

(a) not cause unreasonable loss of amenity by:

(i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining lot; or

(ii) overshadowing the private open space of a dwelling on an adjoining lot; or

(iii) overshadowing of an adjoining vacant lot; or

(iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining lot; and

(b) provide separation between dwellings on adjoining lots that is compatible with that prevailing in the surrounding area.

- 6.7.5 The proposed deck maintains the existing setbacks of the dwelling from both side boundaries. These setbacks are typical for dwellings in the area which is characterised by older dwellings on reasonably narrow lots where much of the lot frontage is occupied. Whilst there is some variation in side setbacks, typical distances are not substantially greater than what is being maintained here, and there are numerous instances of more minimal setbacks.

There are no adjoining vacant lots.

Taking into account the orientation and topography of adjoining lots and the positioning/orientation of the dwellings and open space areas upon them, along with the presence of well-established and vegetated gardens and boundary line vegetation, the availability of vantage points to gain a clear view of the proposed deck is limited. To the south-west and west, where the natural ground level is more elevated, the proposed deck corresponds with thickly vegetated gardens and boundary line vegetation (Plates 1, 2 and 3) The proposed deck is well-screened from these properties and given the existing circumstances and the more-or-less open form of the proposed deck, visual impacts are minimised and overshadowing would not affect primarily protected areas to any extent that could be deemed to be unreasonable.



Plate 1: The adjoining property (21 Allison Street) opposite the existing proposed deck.



Plate 2: The adjoining property (2 Blackwood Avenue) opposite the proposed, extended deck location.



Plate 3: Looking back (towards the front of the site) at the existing deck, dwelling and south western side boundary from ground level.

- 6.7.6 To the east/north-east (Plate 4), the existing dwelling on this adjoining property has a single window opposite the existing deck which would similarly correspond with the initial part of the proposed deck. This window (Plate 5) is set below the balustrade level of the existing deck, with essentially half the window below and half above the deck surface level.



Plate 4: Looking from the existing deck to the adjacent property to the east/north-east. The adjacent window is obscured by the side balustrade. This neighbour's screened deck can be seen in the middle of the image and boundary line vegetation is to the left.



Plate 5: The adjacent window as viewed from the existing deck.

- 6.7.7 On the subject site, between the existing deck and the shared boundary line, is a lattice structure that butts up to the side of the existing deck

(Plate 6). Growing on this structure is an ornamental grape which has extended up to be entwined in the balustrade of the existing deck. This vegetation, when not devoid of leaves, assists to screen the appearance of the deck when viewed from the window in the adjacent dwelling. Discussions with the applicant confirm an intent to retain the lattice structure and vine adjacent to the new deck. Beyond the rear of this adjacent dwelling is a batten screen along the length of a rear deck, before boundary line vegetation extends for effectively the full length of this boundary obscuring the adjacent property's rear yard space from view from the existing deck.



Plate 6: The lattice structure (partially obscured by the tree in the foreground), existing deck, side boundary line and adjacent dwelling's window in context and as viewed from ground level.

- 6.7.8 The view of the proposed deck from the adjacent property, either from the side window or the rear yard is limited. The window is not large, nor does it contribute to the primary outlook from this end of the adjacent dwelling. Whilst the subject site, and with it the existing dwelling and the proposed deck, is situated upon higher ground level, the more or less open form of the deck and the limited opportunity for a direct view from the adjacent site are such that no unreasonable visual impact by way of apparent scale, bulk or proportions would result. The proposed deck is also not considered likely to cast any unreasonable degree of shadow onto this adjoining property. Limited angled shadowing towards the side of the dwelling on the adjacent property may result in the latter parts of the

shorter days of the year, however local topography around the distant Mount Stuart hillside further to the north/north-west and the orientation of the subject and adjacent sites and the development upon them combine to make it likely that access to sunlight at this time would already be compromised and would not be of a high quality or frequency.

6.7.9 The proposal complies with the performance criterion.

6.8 Privacy – Part D 10.4.6 P1

6.8.1 The acceptable solution A1 at clause Part D 10.4.6 requires decks with surface levels above 1m to have a minimum setback of 3m from side boundaries and 4m from rear boundaries unless having a permanently fixed screen to 1.7m above surface level with a maximum transparency of 25%.

6.8.2 The proposal includes a deck with a surface level 2.7m above natural ground level, with side setbacks of 1.3m and 1.9m, with a 1.7m high horizontal slat screen along the side with the 1.3m setback. The deck has a rear setback of approximately 30m.

6.8.3 The proposal does not comply with the acceptable solution; therefore assessment against the performance criterion is relied on.

6.8.4 The performance criterion P1 at clause Part D 10.4.6 provides as follows:

A balcony, deck, roof terrace, parking space or carport (whether freestanding or part of the dwelling) that has a finished surface or floor level more than 1 m above natural ground level, must be screened, or otherwise designed, to minimise overlooking of:

- (a) a dwelling on an adjoining lot or its private open space; or*
- (b) another dwelling on the same site or its private open space; or*
- (c) an adjoining vacant residential lot.*

6.8.5 The proposed deck is non-compliant for privacy where at a setback of 1.9m from the side boundary to the east/north-east. No screen has been proposed along this side of the deck as the applicant seeks to retain a distant outlook to the north/north-east. On the adjacent property, and as mentioned earlier, corresponding with this side of the proposed deck is a window in the rear side of the adjacent dwelling, a screened deck and boundary line vegetation, which extends for the remainder of the side boundary line. The height offset between the two adjoining dwellings is

such that there is no direct outlook from one to another on the same horizontal plane. The existing deck and the proposed deck surface level is elevated enough from the window in the side of the adjacent dwelling that the outlook from users of the deck is not directly into the interior of the room inside but instead out and across this adjacent dwelling and site (Plate 7). Nevertheless, if one wished to they could stand at the edge of the deck and look down into window of the adjacent dwelling and the room inside, albeit the field of view would be limited and as experienced during an inspection of the site the view into this window was at least to some extent obscured by reflection. Whilst it would be possible to look into this window from the deck in this way, this is not the typical behaviour of any reasonable person using a deck and this certainly does not look to be unavoidable as a direct consequence of the deck's design.



Plate 7: Horizontal view through the end of the existing deck balustrade with the adjacent window offset in height beyond.

- 6.8.6 There are no further privacy concerns in terms of overlooking from the deck into the adjoining property's deck which is well screened to substantial height or the private open space beyond which is well-screened by boundary line vegetation growing on the adjacent site.

Despite the conclusions above, it is considered that privacy impact could be better managed with the inclusion of a section of screening adjacent to the window in the side of the adjacent dwelling. Such screening would need not be overly tall, and would simply need to overlap the top of the

adjacent window, approximately 1.3m or to the top of the rear edge of the adjacent dwelling's skillion roof, and would need only to extend in length to correspond with the rear corner of the adjacent dwelling, or for the length of this side of the existing deck. The proposed glass balustrade could then extend further to the rear of the deck where privacy impact is not a concern. Primary outlook from the deck would be maintained, and given its limited length would prevent the addition of any unreasonable visual impact. Such a screen should give some comfort to representors concerned about privacy impact to this side of the deck. A screen of this extent has been discussed with the applicant who indicated a willingness to accept a requirement for such an addition to the design.

- 6.8.7 With the inclusion of a screen as described above the proposal complies with the performance criterion. This can be achieved by way of a condition on any permit issued for the proposal.

6.9 Demolition (Heritage Precinct) - Part E 13.8.1 P1

- 6.9.1 There is no acceptable solution for demolition within a Heritage Precinct.
- 6.9.2 The proposal includes removal of the existing deck.
- 6.9.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.
- 6.9.4 The performance criterion P1 at clause Part E 13.8.1 provides as follows:

Demolition must not result in the loss of any of the following:

- (a) buildings or works that contribute to the historic cultural heritage significance of the precinct;*
- (b) fabric or landscape elements, including plants, trees, fences, paths, outbuildings and other items, that contribute to the historic cultural heritage significance of the precinct;*
- unless all of the following apply;*
- (i) there are, environmental, social, economic or safety reasons of greater value to the community than the historic cultural heritage values of the place;*
- (ii) there are no prudent or feasible alternatives;*
- (iii) opportunity is created for a replacement building that will be more*

complementary to the heritage values of the precinct.

- 6.9.5 The Council's Cultural Heritage Officer states that:

Demolition of an existing deck at rear of a house in a heritage precinct. Drawings indicate that a new deck is proposed in the same location. Plans held at Hobart City Council indicate that the area of building where development is proposed is well beyond the footprint of early and original fabric. The proposed deck is similar in size and location to the existing structure and the proposed demolition and development are not anticipated to result in the loss of cultural heritage values. It is very unlikely that the proposed development will be discernible from the street.

The proposed development satisfies E 13.8.1 P1.

- 6.9.6 The officer's report is included as an attachment to this report.

- 6.9.7 The proposal complies with the performance criterion.

- 6.10 Buildings and Works (Heritage Precinct) - Part E 13.8.2 P1 and P3

- 6.10.1 There is no acceptable solution for buildings and works within a Heritage Precinct.

- 6.10.2 The proposal includes a new deck at the rear of the property within Heritage Precinct

- 6.10.3 There is no acceptable solution; therefore assessment against the performance criterion is relied on.

- 6.10.4 The performance criteria P1 and P3 at clause Part E 13.8.2 provide as follows:

P1

Design and siting of buildings and works must not result in detriment to the historic cultural heritage significance of the precinct, as listed in Table E13.2.

P3

Extensions to existing buildings must not detract from the historic

cultural heritage significance of the precinct.

- 6.10.5 Demolition of an existing deck at rear of a house in a heritage precinct. Drawings indicate that a new deck is proposed in the same location. Plans held at Hobart City Council indicate that the area of building where development is proposed is well beyond the footprint of early and original fabric. The proposed deck is similar in size and location to the existing structure and the proposed demolition and development are not anticipated to result in the loss of cultural heritage values. It is very unlikely that the proposed development will be discernible from the street.

The proposed development satisfies E 13.8.2 P1 & P3.

- 6.10.6 The officer's report is included as an attachment to this report.

- 6.10.7 The proposal complies with the performance criterion.

7. Discussion

- 7.1 Planning approval is sought for Partial Demolition and Alterations, at 19 Allison Street West Hobart.
- 7.2 The application was advertised and received three (3) representations. The representations raised concerns including privacy, visual bulk, overshadowing and residential amenity impacts. These matters have been considered in context with the proposal's degree of compliance with relevant development standards. The proposal is subject to performance criteria that consider such impacts, and for the most part the impacts generated are not considered to be unreasonable. Nonetheless, the matter of privacy to the north-eastern side of the deck has been deemed worthy of increased attention, and as previously detailed in the assessment it has been concluded that more could be done to reduce this impact. As such a condition requiring some additional screening to part of the north-eastern side of the deck is recommended for inclusion on any permit issued for the development.
- 7.3 The proposal has been assessed against the relevant provisions of the planning scheme and is considered to perform well.
- 7.4 The proposal has also been assessed by the Council's Cultural Heritage Officer who has raised no objection to the proposal.

7.5 The proposal is recommended for approval.

8. Conclusion

8.1 The proposed Partial Demolition and Alterations at 19 Allison Street, WEST HOBART satisfies the relevant provisions of the *Hobart Interim Planning Scheme 2015*, and as such is recommended for approval.

9. Recommendations

That: Pursuant to the *Hobart Interim Planning Scheme 2015*, the City Planning Committee, in accordance with the delegations contained in its terms of reference, approve the application for Partial Demolition and Alterations at 19 Allison Street, WEST HOBART for the reasons outlined in the officer's report and a permit containing the following conditions be issued:

GEN

The use and/or development must be substantially in accordance with the documents and drawings that comprise PLN-20-550 - 19 ALLISON STREET WEST HOBART TAS 7000 - Final Planning Documents except where modified below.

Reason for condition

To clarify the scope of the permit.

PLN 1

Screening to a height above deck surface level equivalent to that of the rear end of the skillion roof section on the back of the adjacent dwelling at 17 Allison Street, with no more than 25% uniform transparency must be installed and maintained along that part of the north-eastern edge of the deck for a length equivalent to the current north-eastern end of the existing deck prior to first occupation.

Prior to the issue of any approval under the *Building Act 2016*, revised plans must be submitted and approved showing screening in accordance with the above requirement.

All work required by this condition must be undertaken in accordance with the approved revised plans.

Reason for condition

To provide reasonable opportunity for privacy for dwellings.

ADVICE

The following advice is provided to you to assist in the implementation of the planning

permit that has been issued subject to the conditions above. The advice is not exhaustive and you must inform yourself of any other legislation, by-laws, regulations, codes or standards that will apply to your development under which you may need to obtain an approval. Visit the Council's [website](#) for further information.

Prior to any commencement of work on the site or commencement of use the following additional permits/approval may be required from the Hobart City Council.

BUILDING PERMIT

You may need building approval in accordance with the *Building Act 2016*. Click [here](#) for more information.

This is a Discretionary Planning Permit issued in accordance with section 57 of the *Land Use Planning and Approvals Act 1993*.

WASTE DISPOSAL

It is recommended that the developer liaise with the Council's Cleansing and Solid Waste Unit regarding reducing, reusing and recycling materials associated with demolition on the site to minimise solid waste being directed to landfill.

Further information regarding waste disposal can also be found on the Council's [website](#).



(Cameron Sherriff)

Development Appraisal Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.



(Ben Ikin)

Senior Statutory Planner

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

Date of Report: 28 September 2020

Attachment(s):

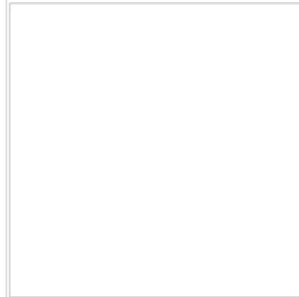
Attachment B - CPC Agenda Documents

Attachment C - Planning Referral Officer Cultural Heritage Report

Planning: #209958

Property

19 ALLISON STREET WEST HOBART TAS 7000

**People**

Applicant

*

KRISTY LITTLE

0447255063

Miss.ristyk@gmail.com

Owner

*

Travis Little

71 Nelson Road

SANDY BAY TAS 7005

0477167962

travlittle@gmail.com

Owner

*

KRISTY LITTLE

0447255063

Miss.ristyk@gmail.com

Entered By

KRISTY LITTLE

0447255063

Miss.ristyk@gmail.com

Use

Single dwelling

Details

Have you obtained pre application advice?

☒ Yes

If YES please provide the pre application advice number eg PAE-17-xx

Meeting with Ben Ikin

Are you applying for permitted visitor accommodation as defined by the State Government Visitor Accommodation Standards? Click on help information button for definition. If you are not the owner of the property you MUST include signed confirmation from the owner that they are aware of this application.

*

☐ No

Is the application for SIGNAGE ONLY? If yes, please enter \$0 in the cost of development, and you must enter the number of signs under Other Details below.

*

☐ No

If this application is related to an enforcement action please enter Enforcement Number

Details

What is the current approved use of the land / building(s)?

*

 Residential

Please provide a full description of the proposed use or development (i.e. demolition and new dwelling, swimming pool and garage)

*

 Replacement of deck

Estimated cost of development

*

 9000.00

Existing floor area (m2)

Proposed floor area (m2)

Site area (m2)

 13.70

 30.80

 600

Carparking on Site

N/A

Total parking spaces

Existing parking spaces

 Other (no selection chosen)

 2

 2

Other Details

Does the application include signage?

*

 No

How many signs, please enter 0 if there are none involved in this application?

*

 0

Tasmania Heritage Register

Is this property on the Tasmanian Heritage Register?

☐ No

Documents

Required Documents

Title (Folio text and Plan and Schedule of Easements)

*

Title plan.pdf

Plans (proposed, existing)

*

Deck plans 19 Allison Street.pdf

Covering Letter

Cover letter deck - HCC application.pdf

Supporting Documents

Photos or Montages

View of neighbouring deck and property.pdf

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

SEARCH OF TORRENS TITLE

VOLUME 44568	FOLIO 1
EDITION 4	DATE OF ISSUE 18-Mar-2020

SEARCH DATE : 28-Aug-2020

SEARCH TIME : 09.10 AM

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Diagram 44568

Being the land described in Conveyance No. 61/5382

Derivation : Part of 0A-3R-24Ps. Gtd. to J. Moir

Prior CT 4675/100

SCHEDULE 1M495007 TRANSFER to KRISTY ANNE LITTLE and TRAVIS MATTHEW
LITTLE Registered 10-Dec-2014 at noonSCHEDULE 2

Reservations and conditions in the Crown Grant if any

E205345 MORTGAGE to Perpetual Corporate Trust Limited

Registered 18-Mar-2020 at 12.01 PM

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



APPROVED 29 JUN 1990 <i>Michael Smith</i> RECORDER OF TITLES	CONVERSION PLAN CONVERTED FROM 65/4308	REGISTERED NUMBER D. 44568
FILE NUMBER Y.12046	GRANTEE: PART OF LOT 13, 0-3-24 GTD. TO JOHN MOIR.	DRAWN <i>AA</i> 8/6/90

OS-4 2082

SKETCH BY WAY OF ILLUSTRATION ONLY

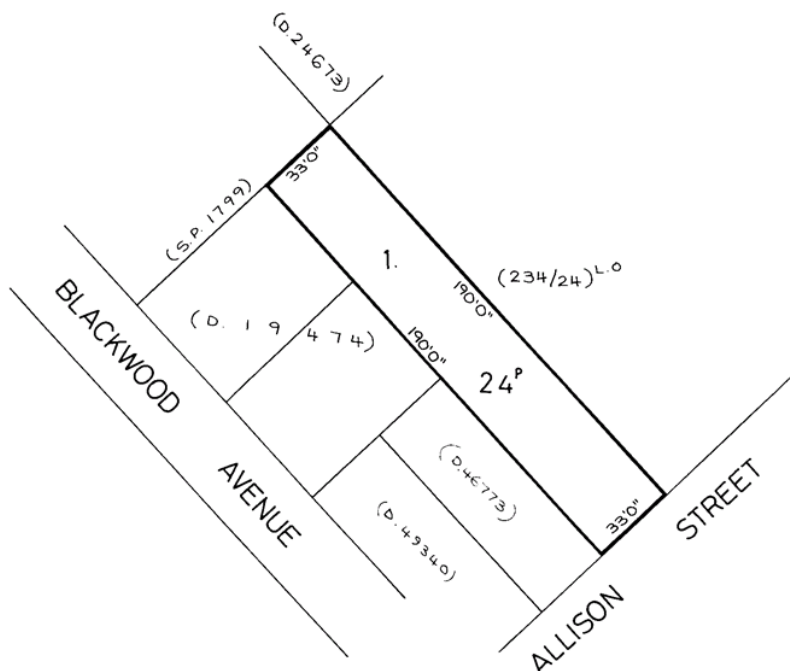
CITY/TOWN OF HOBART (SEC. A6)

LAND DISTRICT OF

PARISH OF

LENGTHS ARE IN METRES, NOT TO SCALE.

LENGTHS IN BRACKETS IN LINKS/FEET & INCHES.



K.A & T.M Little
19 Allison Street
West Hobart
Tasmania, 7000
Ph. 0447 255 063

24th August, 2020

Planning Department
Hobart City Council
GPO Box 503
Hobart, Tasmania, 7001
Australia

Re: Planning Application: Deck replacement – 19 Allison Street, West Hobart

To Whom It May Concern,

I write in relation to the current planning application for our residence at 19 Allison Street, West Hobart.

The existing deck built by the previous owners has reached end of life with some deterioration and needs replacing. We are seeking approval to replace the deck with some alterations to make it more usable, safe and aesthetically pleasing for our family.

As part of the replacement, we plan to install privacy screening on the South West side of the deck. We are not proposing the installation of privacy screening along the North East side of the deck. As there are no internal windows in the kitchen/dining area facing North East, significant light into this living area is provided via the glass windows and doors leading to the deck area. Erecting 1.7m high privacy screening along the North Eastern side of the deck would significantly impact the natural solar light coming into the house during the morning hours of the day as well as negatively impact the visual aspect over West Hobart. On this basis, we are requesting approval to install transparent glass balustrading on this edge of the deck.

The existing deck design at our property was built without the inclusion of privacy screening. Our revised deck will not be positioned any closer towards the neighbouring property at 17 Allison Street. Furthermore the owner of 17 Allison Street has installed their own privacy screening on their deck, this along with the existing greenery screening along the fence line, limits any view of their deck and garden from our property and the proposed deck.

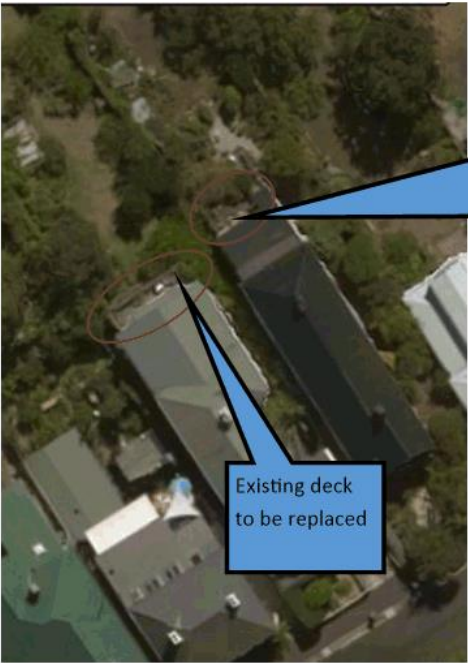
We are confident that the proposed revisions to our deck will not impact on the privacy or visibility of the neighbouring property and we have also consulted with the owner of the neighbouring property over our planned design.

Should there be any further concerns, we would be willing to look into the installation of additional greenery screening from ground level in conjunction with Planning Officers and the owner of the neighbouring property if deemed to be a requirement.

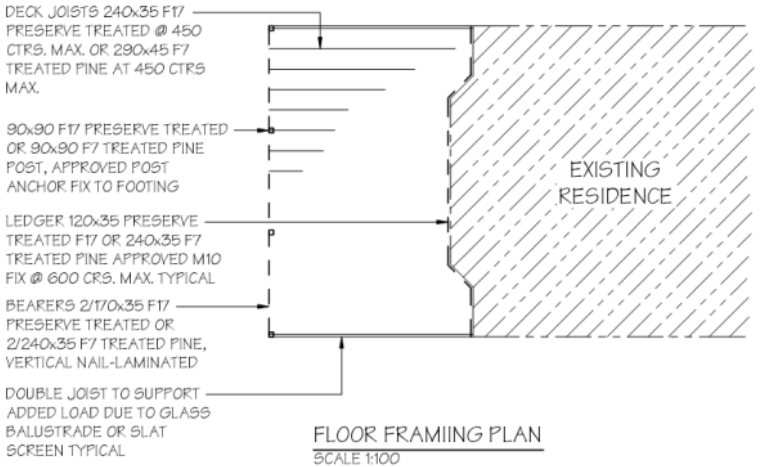
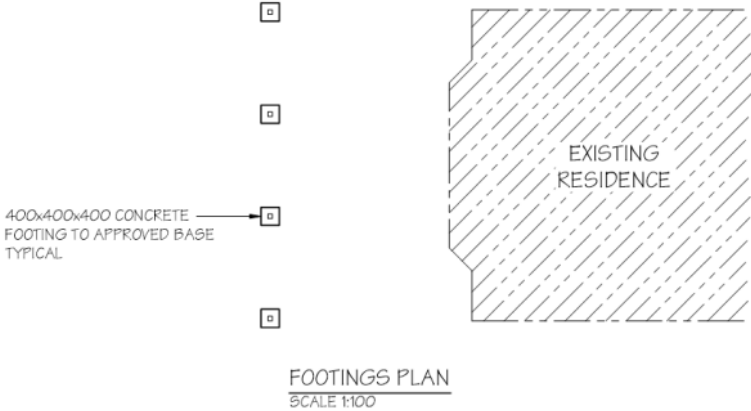
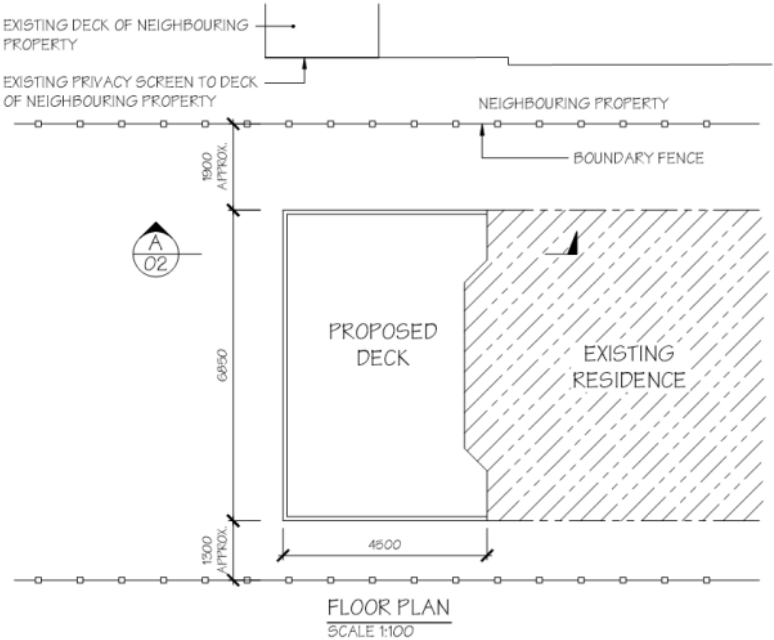
Please feel free to contact us should you require any further information regarding this application.

Yours sincerely,

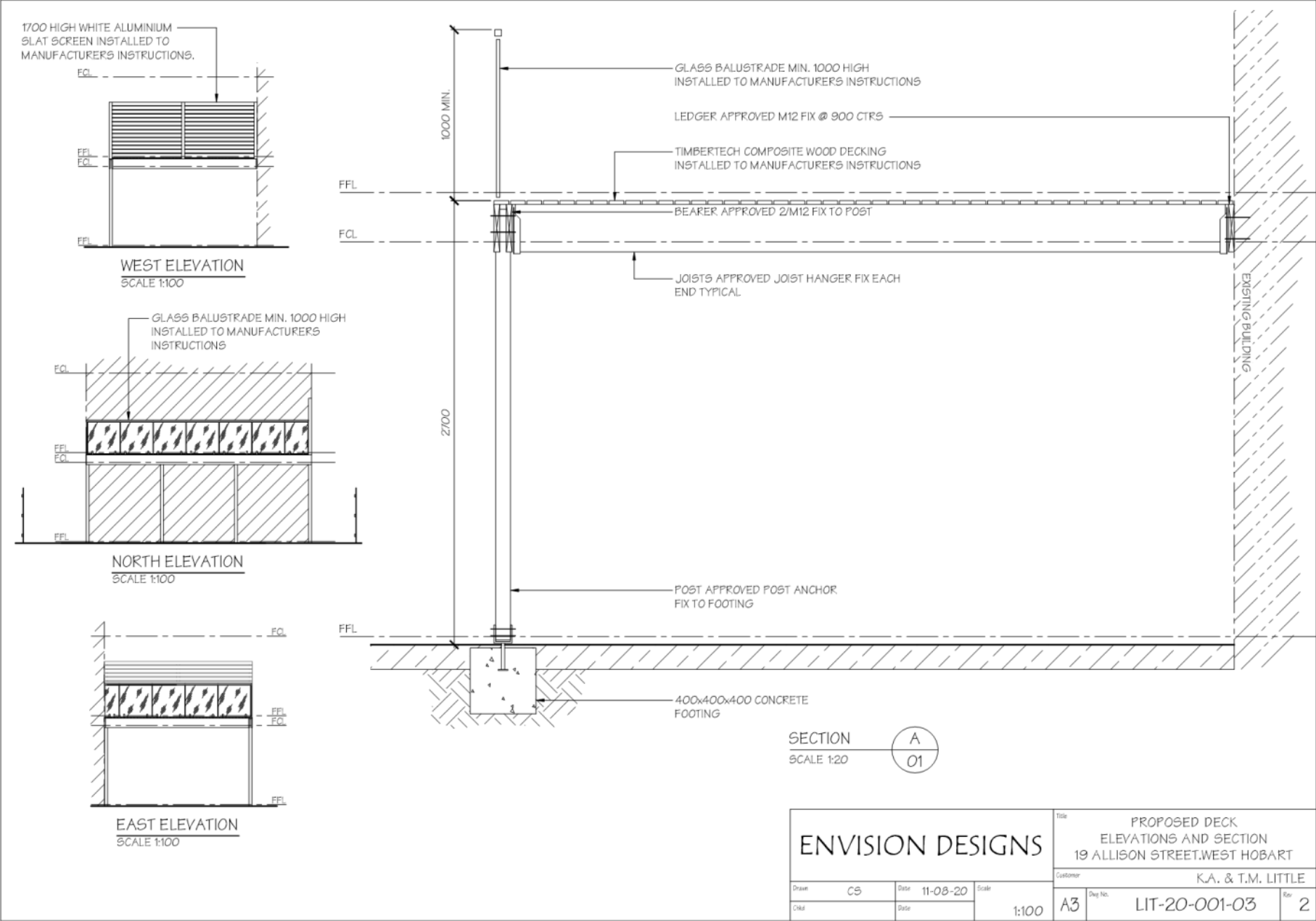
Kristy & Travis Little
Owners - 19 Allison Street, West Hobart







ENVISION DESIGNS			Title PROPOSED DECK - FLOOR PLAN, FLOOR FRAMING & FOOTINGS PLAN 19 ALLISON STREET, WEST HOBART		
Customer K.A. & T.M. LITTLE					
Drawn CS	Date 11-08-20	Scale 1:100	A3	Shee No. LIT-20-001-02	Rev 2
Check	Date				



Application Referral Cultural Heritage - Response

From:	Megan Baynes
Recommendation:	Proposal is acceptable without conditions.
Date Completed:	
Address:	19 ALLISON STREET, WEST HOBART
Proposal:	Partial Demolition and Alterations
Application No:	PLN-20-550
Assessment Officer:	Cameron Sherriff,

Referral Officer comments:

Demolition of an existing deck at rear of a house in a heritage precinct. Drawings indicate that a new deck is proposed in the same location. Plans held at Hobart City Council indicate that the area of building where development is proposed is well beyond the footprint of early and original fabric. The proposed deck is similar in size and location to the existing structure and the proposed demolition and development are not anticipated to result in the loss of cultural heritage values. It is very unlikely that the proposed development will be discernible from the street.

The proposed development satisfies E 13.8.1 P1 and E 13.8.2 P1 & P3.

MB
CHO
23
09
2020

14 42-44 Burnett Street, North Hobart - PLN-20-633
File Ref: F20/115977

Memorandum of the Director City Planning of 30 October 2020.

Delegation: Council



City of **HOBART**

MEMORANDUM: CITY PLANNING COMMITTEE

42-44 Burnett Street, North Hobart - PLN-20-633

Background

The *Land Use Planning and Approvals Act 1993* requires that an application is determined based on the planning scheme which is in effect as at the date of the decision, not the date of the application. This can make it very difficult for developers if there is a change to the applicable scheme after the date of the application and before the date of determination.

The *Hobart Interim Planning Scheme 2015* has recently been amended, with the amendments to take effect on 19 November 2020. The amendments introduce various amenity improvements to apartment-style developments.

The amendments were properly advertised, then assessed and approved by the Tasmanian Planning Commission. The Development Appraisal Unit has communicated with applicants and their consultants about the nature of the amendments and the timing of them.

Current Application

The application is broadly described as follows:

The proposal is to demolish existing structures at 40 Burnett Street and 42-44 Burnett Street and construct a new seven storey building (two below ground floors and five above ground floors) that is primarily for 31 multiple dwellings. The ground floor is proposed to also contain commercial space with direct frontage to Burnett Street.

The site is located between Argyle and Elizabeth Street. There is an existing single storey building on the site at 42-44 Burnett Street. This was most recently used as Novus Windscreen repairs.

This applicant presented the proposed applicant to a “pre-application” meeting of the Urban Design Advisory Panel (UDAP) on 27 August 2020, which is a voluntary process. At that meeting, it was made clear that if the applicant wished to have the application determined prior to the commencement of the amendments to the Scheme then an application should be made as soon as possible.

It was clear at the UDAP meeting that the applicant had committed significant resources to the preparation of the application, including consulting with various experts regarding the proposal.

UDAP was broadly supportive of the pre-application proposal, although some suggestions were made in relation to the design.

An application was made on 22 September 2020. UDAP is due to consider the application as submitted on 5 November 2020.

If the application is not able to be determined prior to 19 November 2020 then a redesign may be necessary and the costs of preparing the application to date may be wasted.

Delegations

If there are 6 or more representations then the application would need to be determined by the Council. The advertising period does not end until 11 November 2020, so it is too early to tell whether or not there will be 6 or more representations. Given the location of the proposed development, it is possible that there will be at least 6 representations.

The officer assessment has not been completed and so it is too early to say whether it will be recommended for approval or refusal. If recommended for refusal then the application would need to be determined by the Council.

The back-up delegation to the Director City Planning would only be activated in the event that there is a tied vote. This is to prevent the scenario where the Committee considers the application on 16 November 2020 and has tied votes, so that there is no decision on the application. There would be insufficient time to call a special Council meeting to be held prior to 18 November 2020.

A planned special Council meeting on 18 November is an alternative to the proposed delegations.

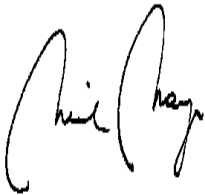
RECOMMENDATION

That:

1. There is a specific delegation made by the Council to the City Planning Committee to determine this application, as follows:
 - a. The Council, pursuant to section 6(3) of the *Land Use Planning and Approvals Act 1993*, delegates to the City Planning Committee, the power to determine planning application reference PLN-20-633 for the property at 42-44 Burnett Street, North Hobart.
 - b. The Council, pursuant to section 6(3) of the *Land Use Planning and Approvals Act 1993*, delegates to the Director City Planning the power to determine planning application reference PLN-20-633 for the

property at 42-44 Burnett Street, North Hobart in circumstances where the application has been considered by the City Planning Committee and the Committee has not determined the application due to the vote on a motion being tied. The application must be determined by following the recommendation of the Council's officers, as specified in the report provided to the Committee.

As signatory to this report, I certify that, pursuant to Section 55(1) of the Local Government Act 1993, I hold no interest, as referred to in Section 49 of the Local Government Act 1993, in matters contained in this report.

A handwritten signature in black ink, appearing to read 'Neil Noye', with a stylized, cursive script.

Neil Noye
DIRECTOR CITY PLANNING

Date: 30 October 2020
File Reference: F20/115977